

Design Review Meeting



1518 W Dravus St.

Design Review Meeting

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1518 W Dravus St.

Project Information

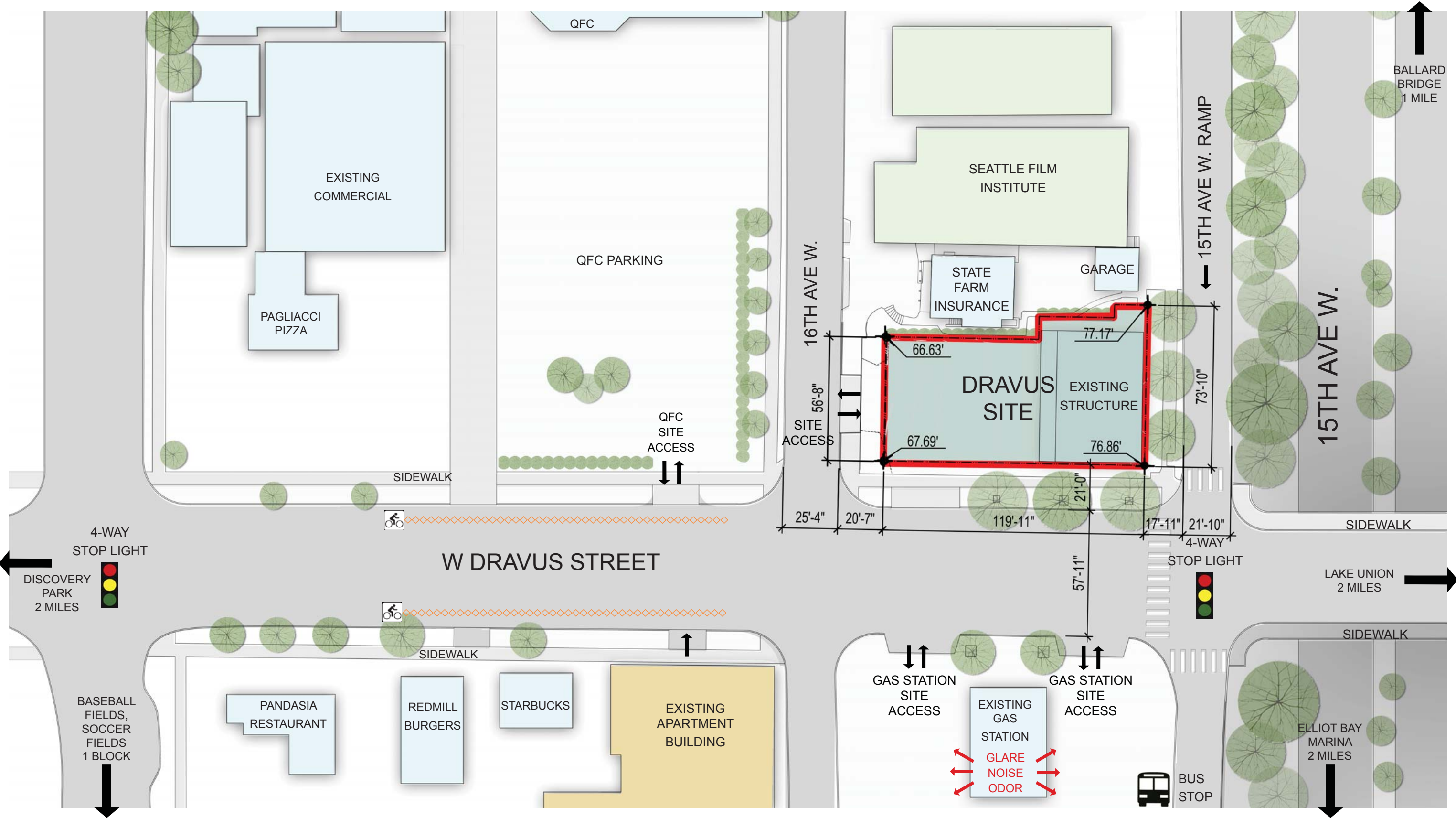
Project Description:	This proposal is for a 7-story residential building with at grade parking. Parking will be below grade on the east side of the building and accessed from a drive aisle along the west side of the building.		
Property Address:	1518 West Dravus Street, Seattle WA 98119		
Project Number:	3017929		
Parcel Number:	2770603065		
Zone:	SM/D 40-85 (Seattle Mixed-Dravus)		
OverLay:	Airport Height – Outer Approach Surface		
ECA:	Liquefaction, Abandoned Landfill		
Lot Area:	7,424 SF		
Adjacent Properties:	The property is adjacent to a two story commercial bldg. to the N. & surrounded by streets on three sides.		
	<u>Requirements</u>	<u>Project Data</u>	
FAR: 23.48.009 Table A	Not subject to FAR limits	31,694 sf	
Height Limit: SLUC 23.48.10	40', 45' if LEED Gold & Affordable Housing. Only residential uses above 40'-0", 85'-0" beyond 50'-0" setback from Dravus St. property line	45' and LEED gold & Affordable Housing Residential uses above 40'-0". 65'-0" beyond 27'-0" setback	
Amenity Area: SLUC 23.45.522	5% of total gross floor area	5% of total gross floor area	
Permitted Uses: SLUC 23.45.504 Table A	Residential & Office	Residential	24,602 38 units
		Parking	6,100 sf, 19 spaces
		Amenity Area	992 sf
		Total	31,694 sf
Street Level Development Standards: SLUC 23.48.014	Projects not on a designated Class 1 pedestrian street on the land use map do not require commercial use at the ground floor	This project is not on a designated Class 1 pedestrian street and therefore a residential lobby and parking is permitted on the ground floor	
Development Departures:			
1.	Requesting departure for reducing the setback from 50'-0" to 27'-0" to allow an additional two levels of housing units and provide an iconic architectural building at this important neighborhood corner		
2.	Requesting departure for increasing the allowable setback at the corner of 16th Ave W & W Dravus St. to create an active pedestrian plaza		
3.	Requesting a departure for decreased aisle width in the parking garage due to site restrictions		
4.	Requesting a departure for parking at street level due to site restrictions		

Vicinity Map

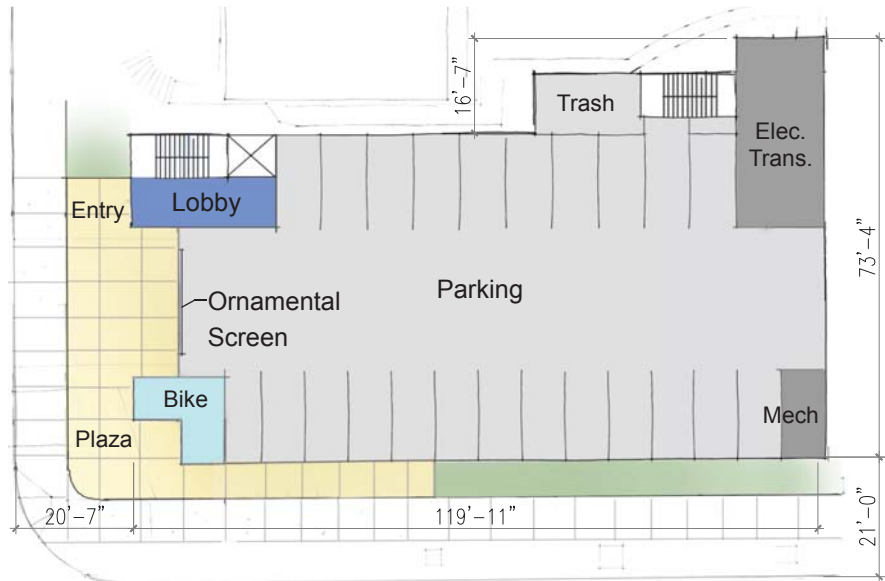


1518 W Dravus St.

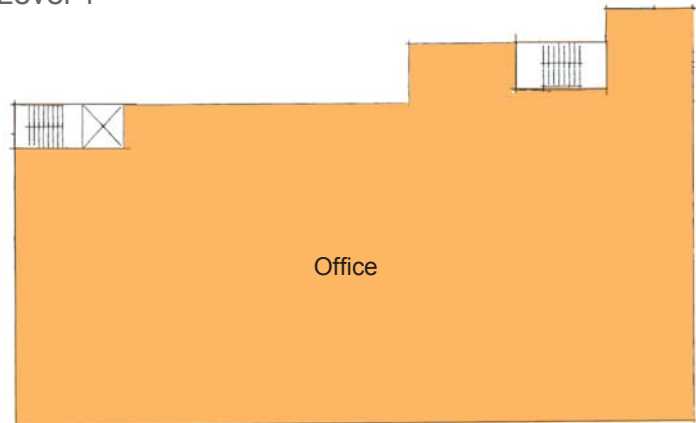
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27'-0" SETBACK SCHEME - recommended at EDG



Level 1



Level 2

1518 W Dravus St.

REVISED SCHEME



REVISIONS:

Setbacks from 16th ave W & W Dravus St. simplify form

Narrowed driveway

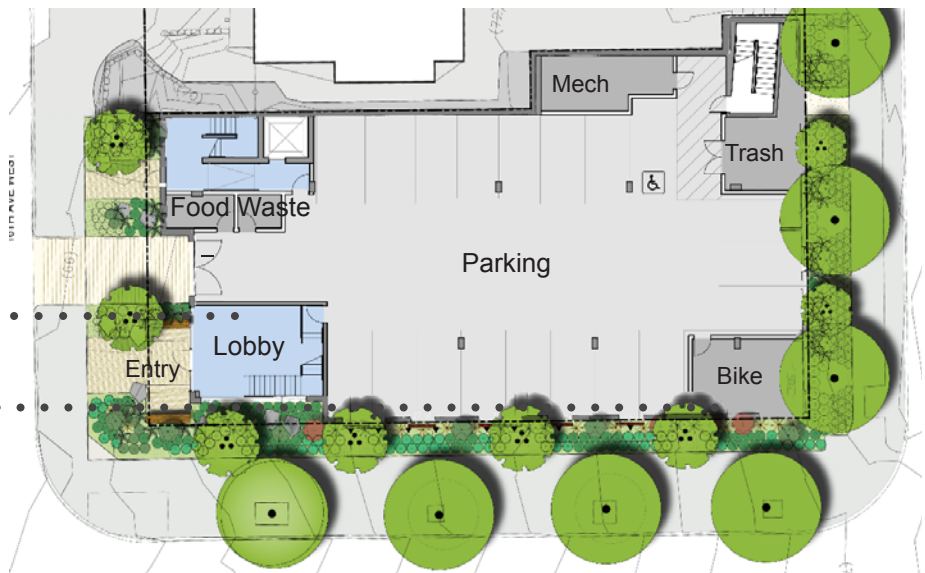
Lobby moved to corner of 16th Ave W & W Dravus St.

Bike Storage Relocated

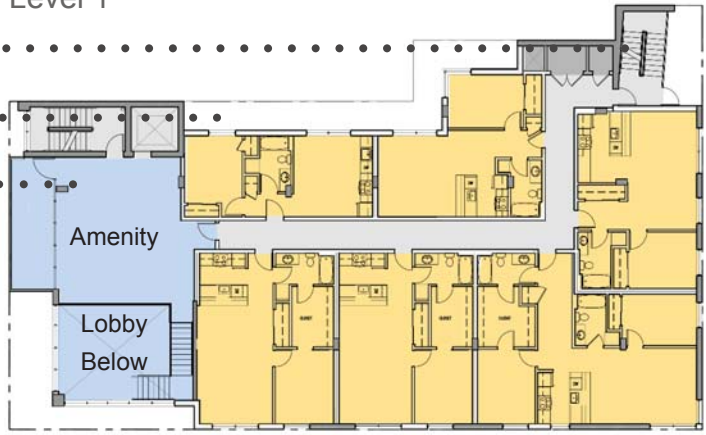
Revised Stair Location

Setback added on Level 2

Amenity and Residential replace office on Level 2



Level 1



Level 2

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Creating a Neighborhood Icon

- The board recommends the structure present a strong design concept at the corner of 16th Ave. W & Dravus St. The architectural massing & composition should be of an iconic nature.

The building is located on the prominent corner or W Dravus St & 16th Ave W. The site is highly visible and will serve as an entry into the neighborhood. An open stair & grand architectural form will create an iconic, memorable feature that will also create a backdrop for signage.

The neighborhood of Interbay is industrial and located near the Balmer rail yard. The design draws on these features by incorporating forms and materials from the storage containers and cranes of the rail-yard. The corner entry mullion design has also been inspired by the train tracks, themselves.



NE VIEW FROM 15TH AVE



SW VIEW





1518 W Dravus St.

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Developing an Open Space

- The board recommended the development of an open space at the corner of 16th Ave W & W Dravus St.
- The board also had the following recommendations for the space:



LANDSCAPE PLAN



SW VIEW

Response to Street Characteristics

- The board suggests that the design responds to the characteristics of each street.

16th Ave W

Neighborhood Street



- Entrance to lobby & garage located here to encourage pedestrian activity
- Canopies and balcony create pedestrian scale
- Landscape and green space contribute to the neighborhood feel of the street
- Balconies and lobby create an active street-level

W Dravus St.

Commercial Connector



- High traffic patterns along Dravus inspire movement and shifting within the forms of the south facade.
- Vines, trees, & shrubs soften the hardscapes and create buffer between apartment units and Dravus St.
- Parking on Level 1 raises residential units above Dravus St.

15th Ave W

Off-Ramp



- Shrubs, trees, and vines on exterior stair screen create buffer between apartment units and this busy offramp
- Open stair and architectural 'crane' feature create iconic symbol at entry into Interbay Neighborhood



1518 W Dravus St.

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Concrete



Green Screen



LANDSCAPE PLAN

AEP Span- Box Rib
Cool Metallic Champagne



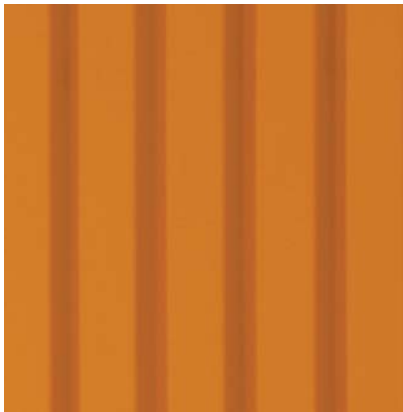
AEP Span- Box Rib
Cool Weathered Copper



AEP Span- Box Rib
Cool Regal White



AEP Span- Box Rib
Cool Terra Cotta



SW VIEW

Lighting Legend

- ① Recessed canopy downlighting
- ② Sconce above door
- ③ Down lighting at open stair



LIGHTING PLAN L1

Signage Locations



Landscape - Plant Species

GROUND FLOOR



VINE MAPLE



SERVICE BERRY



CREeping MAHONIA



SWORD FERN



NINEBARK



WHITE CURRANT



BIRCHLEAF SPIRAEA



REED GRASS



HAIR GRASS



BLUE OAT GRASS



BEAR GRASS



BEACH STRAWBERRY

ROOF



EVERGREEN SPURGE



NEEDLE GRASS



RUSSIAN SAGE



ROSEMARY



REED GRASS



BEACH STRAWBERRY

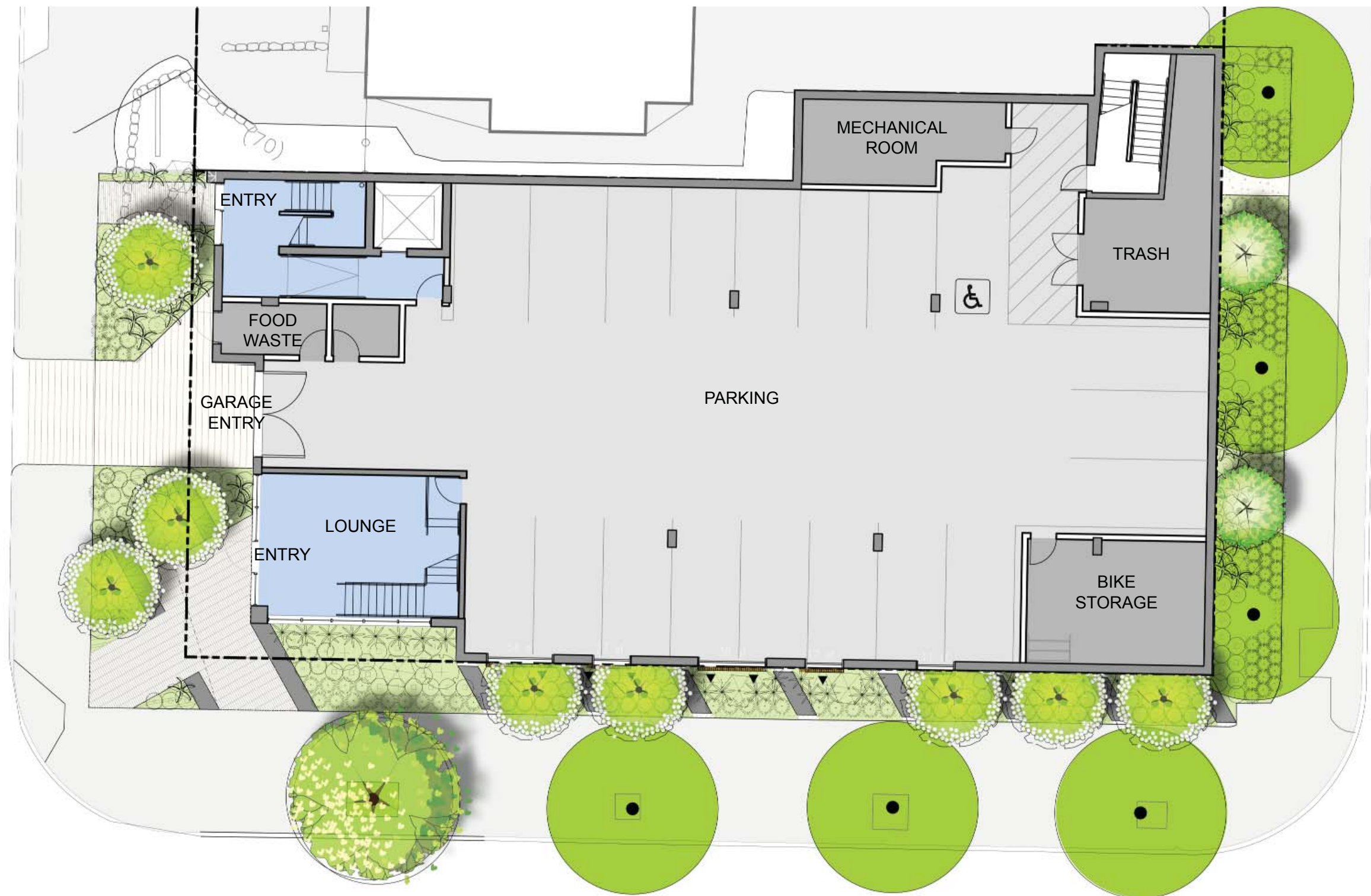


GREEN ROOF - GRASS'



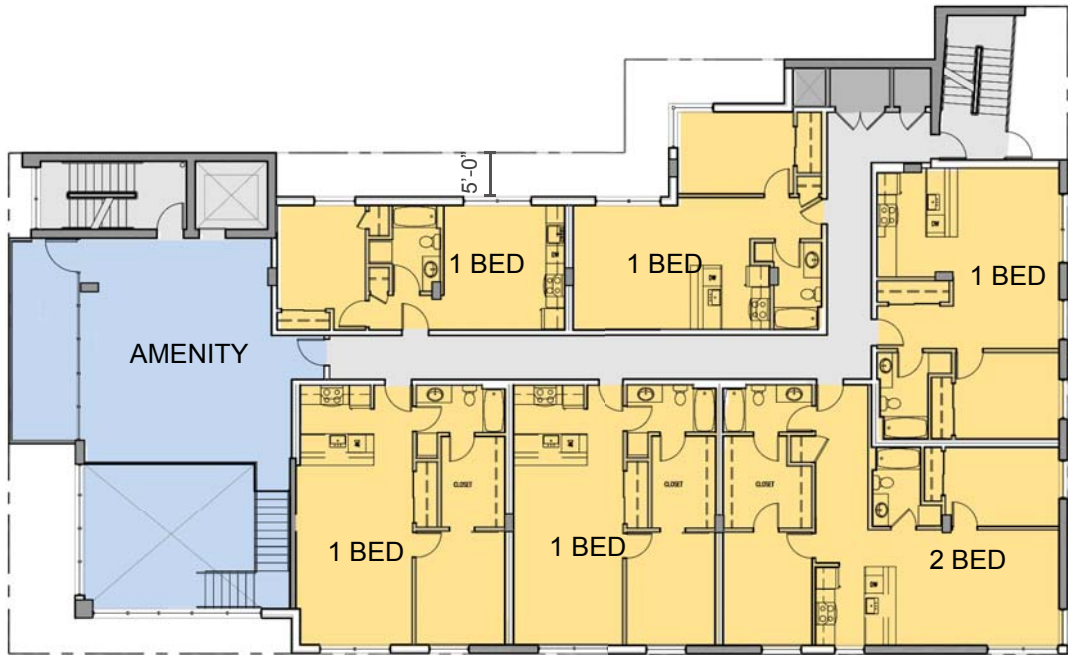
GREEN ROOF - SEDUM

Landscape - Ground Floor Plan

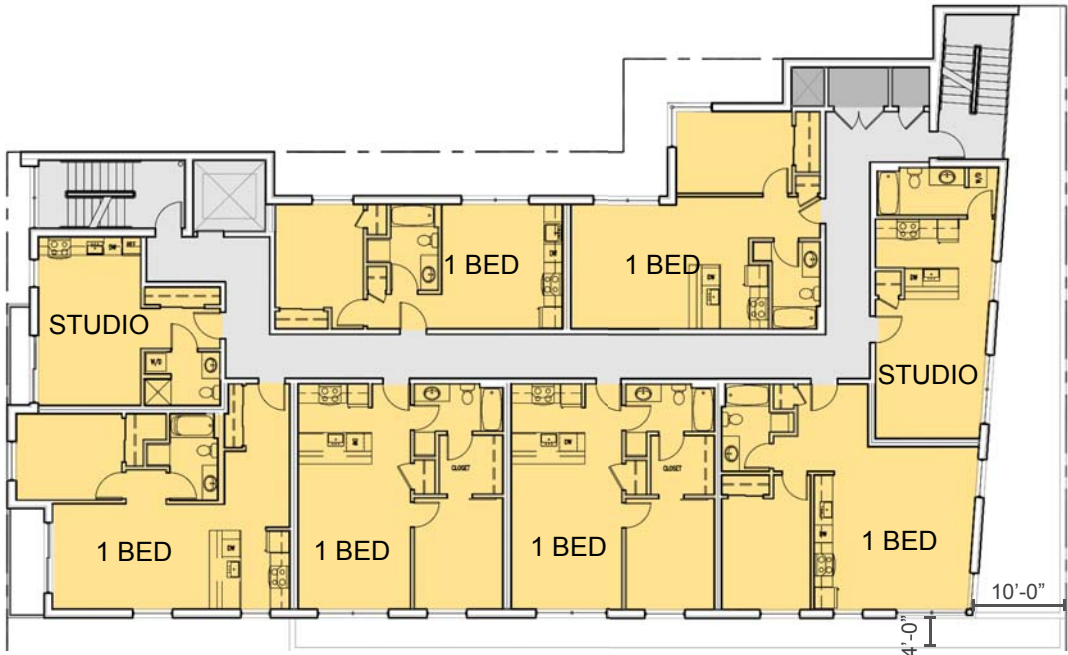


Landscape - Roof Plan

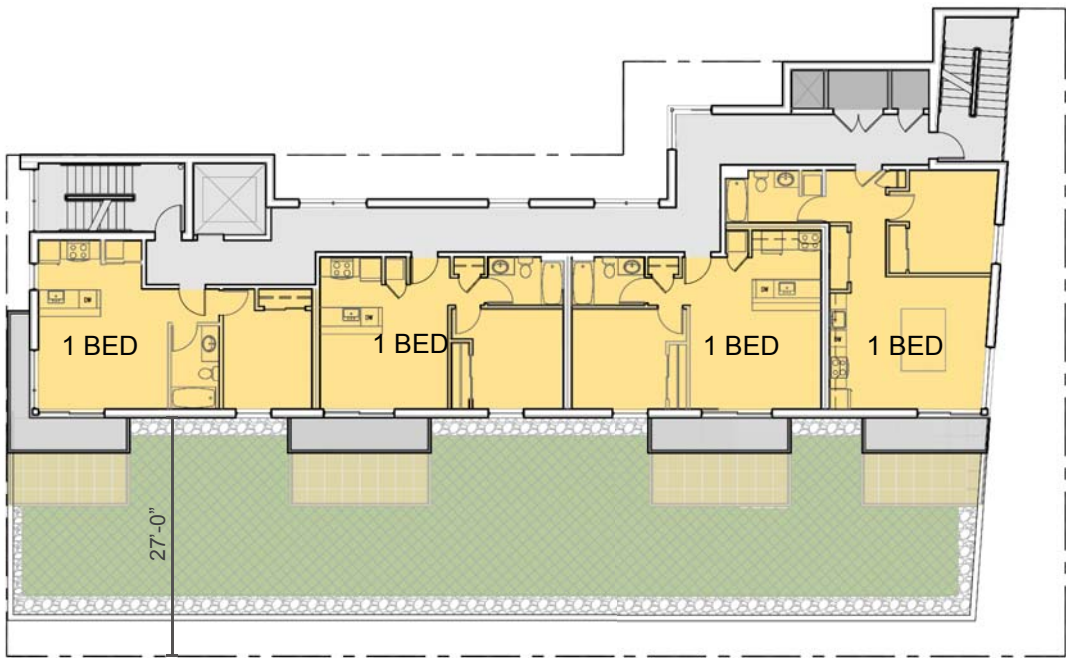




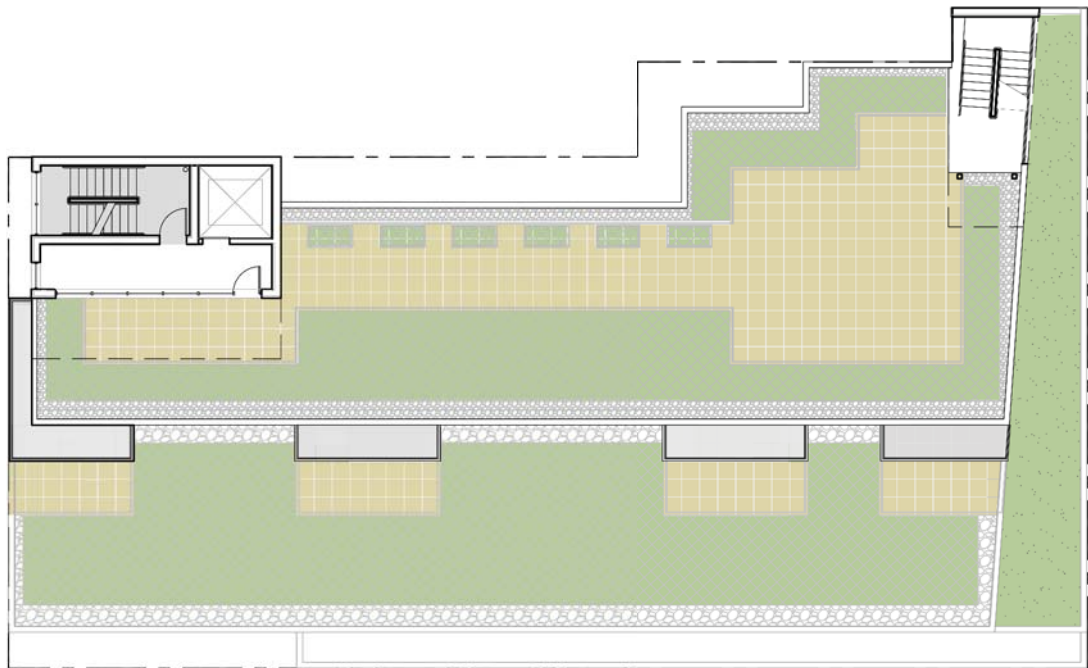
Level 2



Level 5 (Level 3 & 4 sim.)



Level 7 Floor Plan (Level 6 sim.)



Roof Plan

MARCH 21



10AM



12PM



2PM

JUNE 21



10AM



12PM



2PM

DECEMBER 21



10AM



12PM



2PM

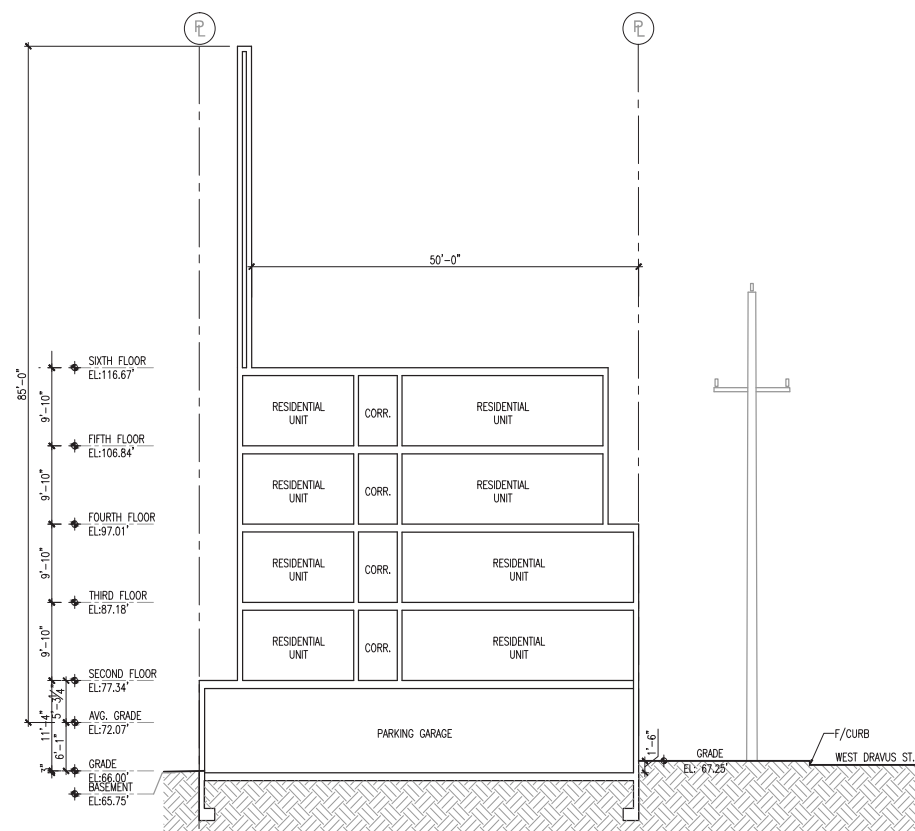
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Development Departure - Setback from W Dravus St.

OPTION 1 - CODE COMPLIANT

The images to the right shows possible building development based on the required building setback of 45'-0" for the first 50'-0" and 85'-0" after 50'-0" from the property line.



View from W Dravus St. & 16th Ave W.

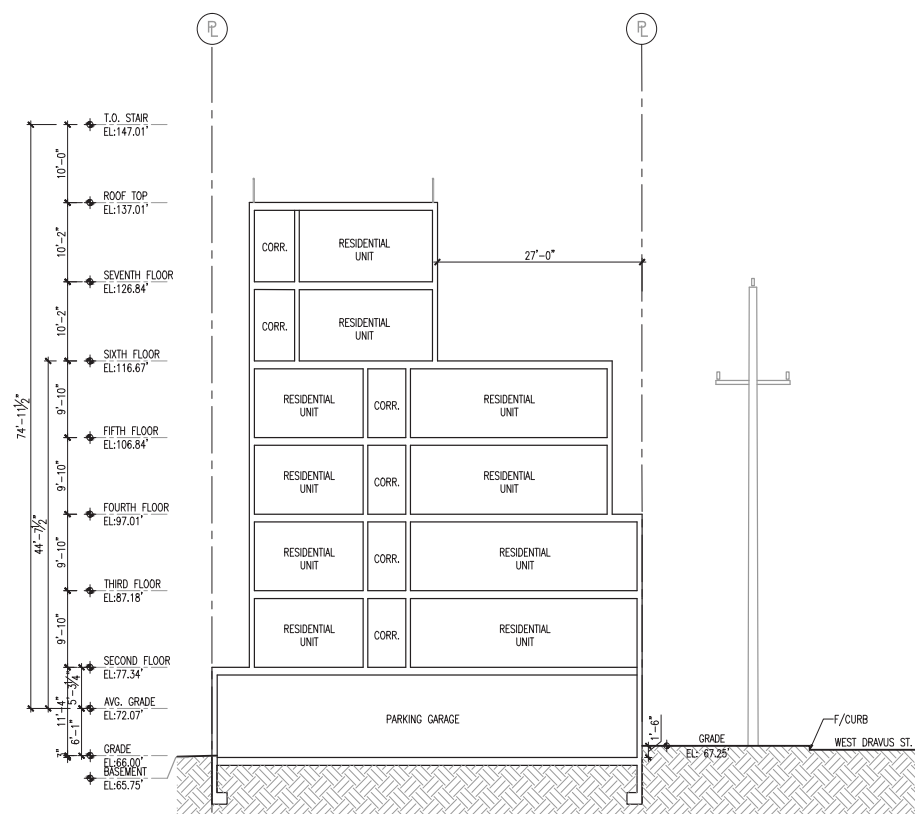


View from W Dravus St. & 15th Ave W.

OPTION 2 - PREFERRED SCHEME

An additional 6th & 7th floor would be added setback 27'-0" from the property line within the 85'-0" height.

The decreased setback has little affect on the overall character of Dravus Ave W. The stepped nature of the building makes a gradual transition to the highest building elevation and creates a gateway or focal point for the neighborhood.



View from W Dravus St. & 16th Ave W.



View from W Dravus St. & 15th Ave W.

Development Departure- Corner Setback

PER SMC 23.48.014.A.3.b

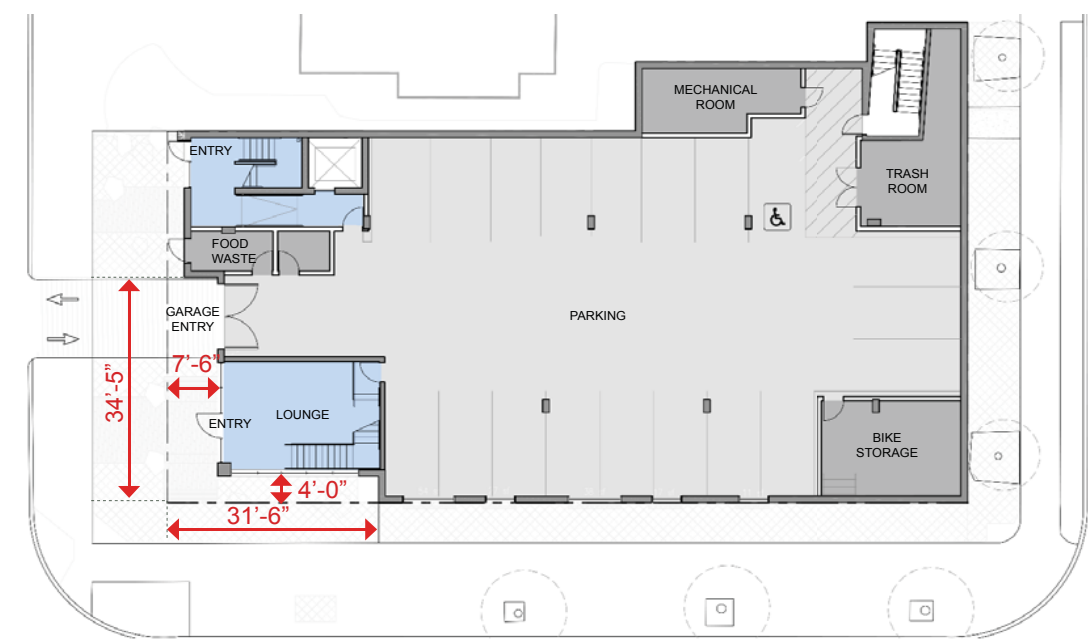
A maximum setback of 12'-0" is permitted, and setback areas must be located 20'-0" away from the corners and landscaped.

PROPOSED

Setbacks are 7'-6" & 4'-1/2" at the corner

CONSIDERATIONS

The board supported a setback at the corner, finding it creates an active pedestrian plaza as well as a successful transition to the existing development to the north.



SETBACK DIAGRAM



CORNER PERSPECTIVE

Development Departure - Parking Garage Aisle Width

PER SMC 23.54.030.E.1

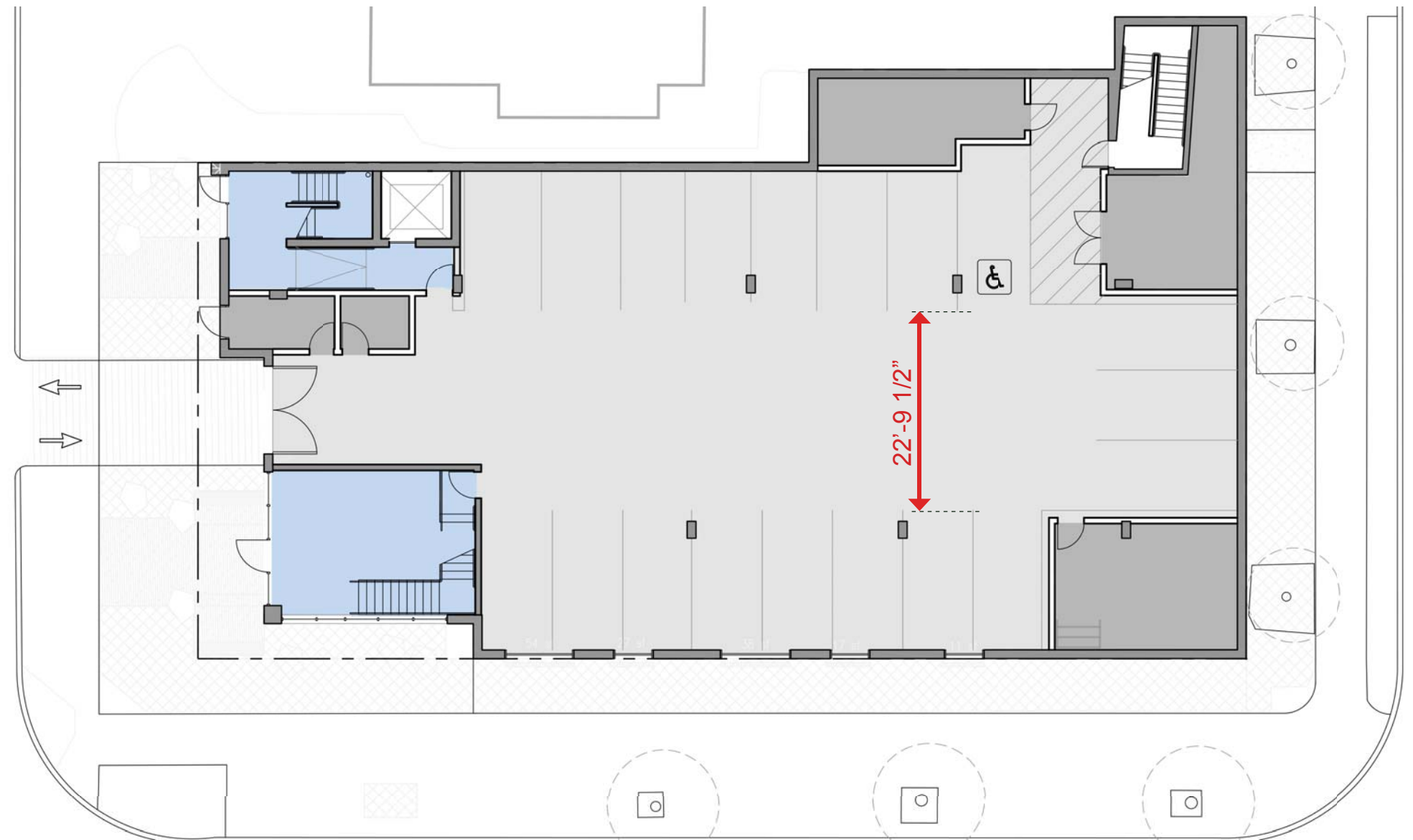
24'-0" required aisle width at ADA van stall

PROPOSED

22'-9 1/2" aisle width at ADA van stall

CONSIDERATIONS

The narrow width of the site limits aisle width. 22'-9 1/2" is the max possible at this location.



Development Departure - Parking at Street Level (1)

23.48.034 - PARKING AND LOADING LOCATION, ACCESS AND CURBCUTS

B.PARKING LOCATION WITHIN STRUCTURES

1.PARKING AT STREET LEVEL

a.EXCEPT AS PERMITTED UNDER SUBSECTIONS 23.48.034.B.1.b AND 23.48.034.B.1.c, PARKING IS NOT PERMITTED AT STREET-LEVEL UNLESS SEPARATED FROM THE STREET BY OTHER USES, PROVIDED THAT GARAGE DOORS NEED NOT BE SEPARATED.

b.DUE TO PHYSICAL SITE CONDITIONS SUCH AS TOPOGRAPHIC OR GEOLOGIC CONDITIONS, PARKING IS PERMITTED IN STORIES THAT ARE PARTIALLY BELOW STREET-LEVEL AND PARTIALLY ABOVE STREET LEVEL WITHOUT BEING SEPARATED FROM THE STREET BY OTHER USES:

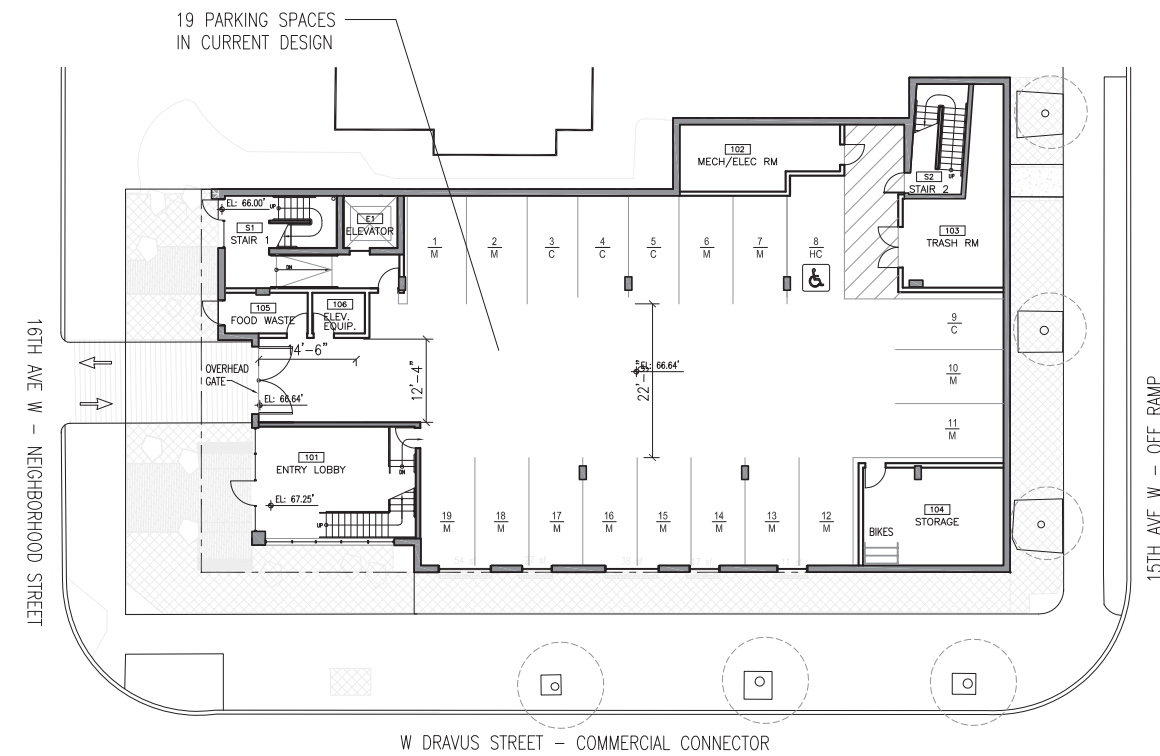
THE TOPOGRAPHIC OR GEOLOGIC CONDITIONS THAT ALLOW PARKING IN STORIES PARTIALLY BELOW STREET-LEVEL AND PARTIALLY ABOVE STREET LEVEL ARE AS FOLLOWS:

THE SITE IS RESTRICTED BY A 10'-0" GRADE CHANGE FROM EAST TO WEST AS WELL AS CONSTRAINED LENGTH & WIDTH DIMENSIONS. DIAGRAMS '6/CS3B-ONE LEVEL PARKING' & DIAGRAM '7/CS3B-TWO LEVEL PARKING' SHOW POSSIBLE BELOW-GRADE LAY-OUTS. BOTH USE A 15% RAMP WHICH IS TOO STEEP FOR PARKING. A 5-6% RAMP WOULD BE PARKABLE BUT ITS 161'-2" RAMP LENGTH WOULD BE UNFEASIBLE FOR THE DIMENSIONS OF THIS SITE.

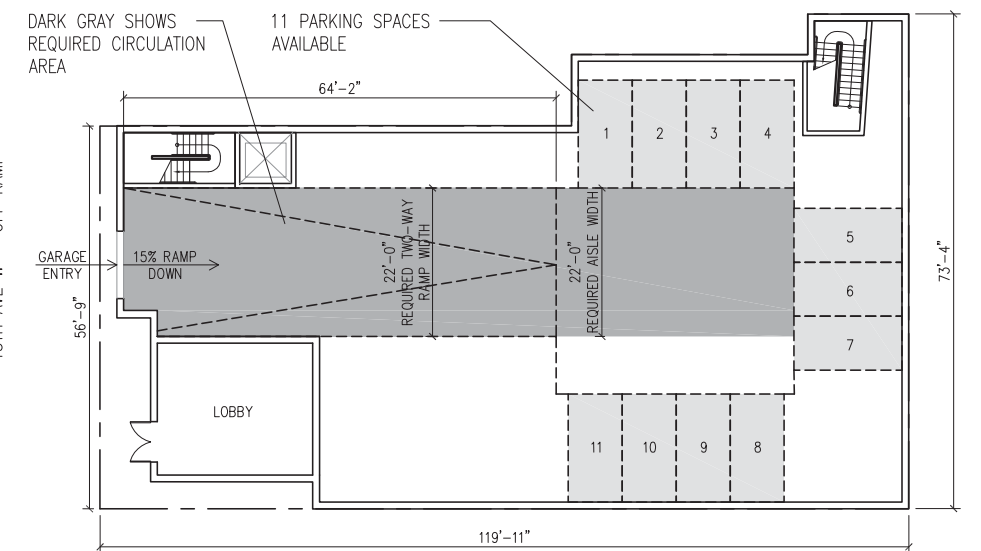
DIAGRAM '6/CS3B-ONE LEVEL PARKING' ILLUSTRATES ONE LEVEL OF PARKING BELOW GRADE. THE 64'-2" LENGTH OF RAMP REQUIRED AND THE 22'-0" REQUIRED WIDTH OF RAMP LIMIT THE NUMBER OF POSSIBLE PARKING SPACES TO 11.

DIAGRAM '7/CS3B-TWO LEVEL PARKING' ILLUSTRATES TWO LEVELS OF PARKING BELOW GRADE. THE 64'-2" LENGTH OF RAMPS ALONG WITH THE 22'-0" TURNING RADIUS & 44'-0" WIDTH REQUIRED FOR RAMPING TO TWO LEVELS LIMITS THE NUMBER OF POSSIBLE PARKING SPACES TO 4 ON LEVEL 1 AND 10 ON LEVEL 2.

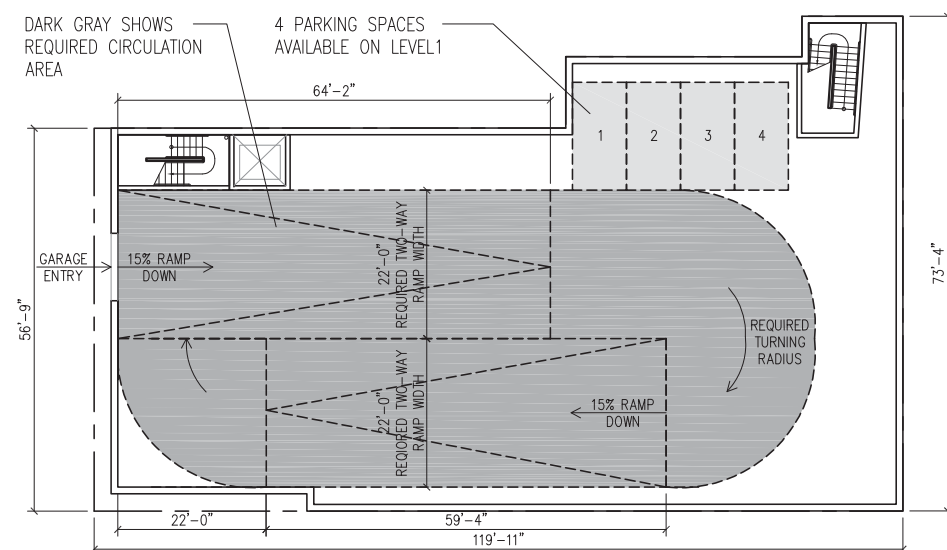
DIAGRAM 8/CS2B SHOWS THE CURRENT DESIGN WHICH WOULD UTILIZE THE SLOPE OF THE SITE & PROVIDE PARTIALLY BELOW GRADE PARKING. PARKING AT GRADE ALONG 16TH AVE W ALLOWS A 12'-0" DRIVEWAY AND NO INTERNAL RAMPING. THIS DESIGN PROVIDES THE REQUIRED PARKING - 19 SPACES.



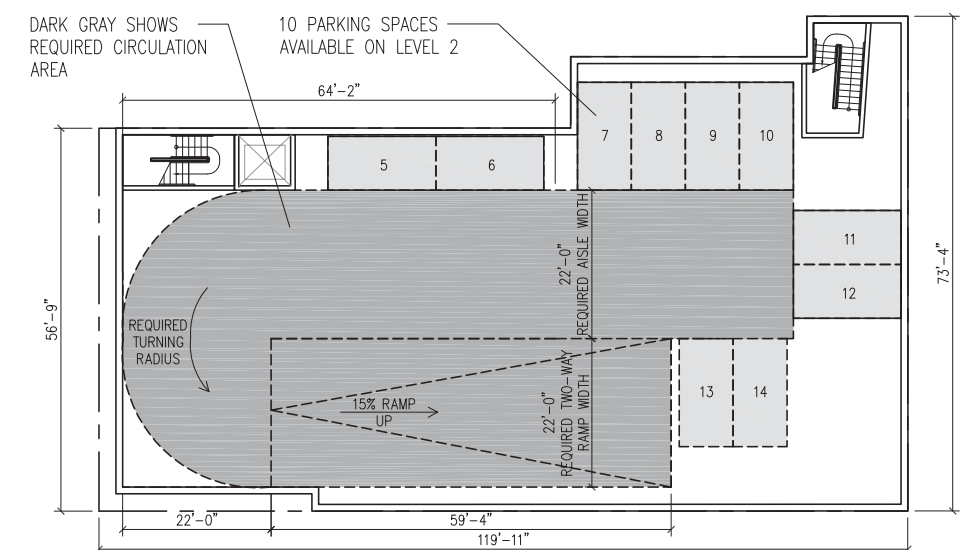
8 CS3b **CURRENT LEVEL 1 PLAN - 19 SPACES**
SCALE: 1/16" = 1'-0"



6 CS3b **ONE LEVEL PARKING DIAGRAM - 11 SPACES**
SCALE: 1/16" = 1'-0"



7 CS3b **TWO LEVEL PARKING DIAGRAM - 14 SPACES**
SCALE: 1/16" = 1'-0"



Development Departure - Parking at Street Level (2)

IN ADDITION, PARKING AT THE STREET LEVEL HAS BEEN BUFFERED BY VARIOUS USES AS FOLLOWS:

LOBBY	61'-10"
TRASH	07'-10"
4'-0" TALL OR LESS	32'-04"
	102'-0" (60% OF FACADE)
PARKING GARAGE WALL	67'-09" (40% OF FACADE)

THE CALCULATIONS ABOVE SHOW ONLY 40% OF THE FACADE IS PARKING GARAGE WALL ALONG THE SOUTH AND WEST ELEVATIONS. THE REMAINING SPACES BORDER THE TRASH ROOM, LOBBY, OR ARE WITHIN 4'-0" HEIGHT AND THEREFORE NOT CONSIDERED A STORY. THE NORTH FACADE IS COMPLETELY BELOW GRADE AS ILLUSTRATED IN 9/CS3.

b.DUE TO PHYSICAL SITE CONDITIONS SUCH AS TOPOGRAPHIC OR GEOLOGIC CONDITIONS, PARKING IS PERMITTED IN STORIES THAT ARE PARTIALLY BELOW STREET-LEVEL AND PARTIALLY ABOVE STREET LEVEL WITHOUT BEING SEPARATED FROM THE STREET BY OTHER USES, IF:

1)THE STREET FRONT PORTION OF THE PARKING THAT IS AT OR ABOVE STREET-LEVEL DOES NOT ABUT A CLASS 1 PEDESTRIAN STREET REQUIRING STREET-LEVEL USES

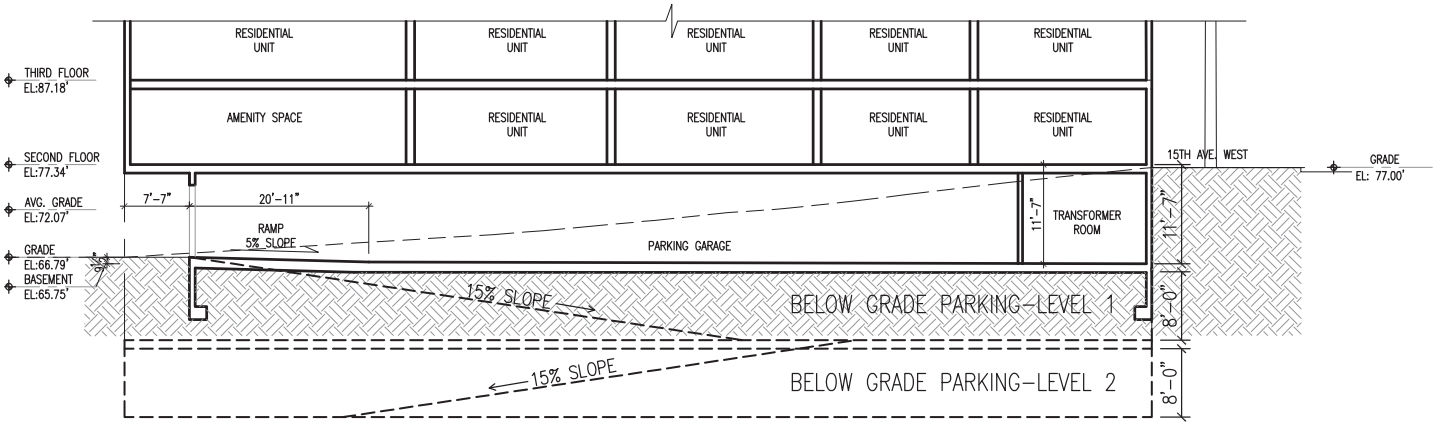
NEITHER W DRAVUS ST OR 16TH AVE W ARE CLASS 1 PEDESTRIAN STREETS

2)THE STREET FRONT PORTION OF THE PARKING THAT IS AT OR ABOVE STREET-LEVEL, EXCLUDING GARAGE AND LOADING DOORS AND PERMITTED ACCESS TO PARKING, IS SCREENED FROM VIEW AT THE STREET-LEVEL

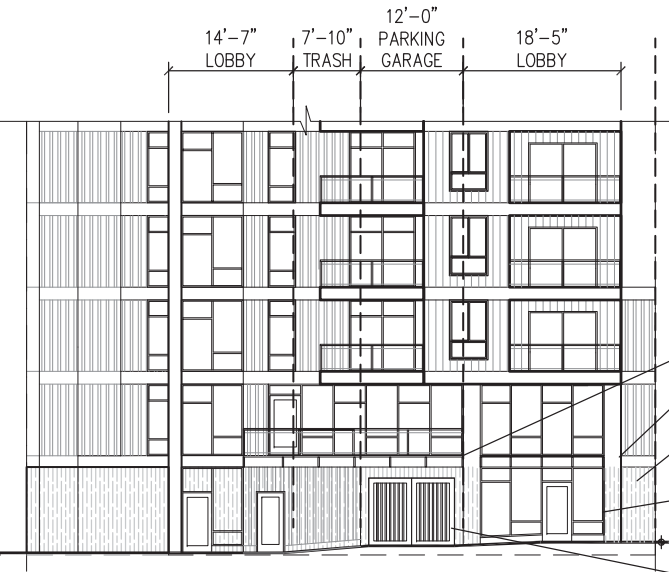
PARKING HAS BEEN SCREENED FROM VIEW AT STREET LEVEL BY A COMBINATION OF ARCHITECTURAL DETAILING & DECORATIVE GRILLES WITH PLANTINGS. IN ADDITION, 60 PERCENT OF THE SOUTH AND WEST FACADES SEPARATE THE PARKING FROM THE STREET BY OTHER USES. SEE 10 & 11/CS3B FOR USES.

3)THE STREET-FACING FACADE IS ENHANCED BY ARCHITECTURAL DETAILING, ARTWORK, LANDSCAPING, STOOPS AND PORCHES PROVIDING ACCESS TO RESIDENTIAL USES, OR SIMILAR VISUAL INTEREST FEATURES.

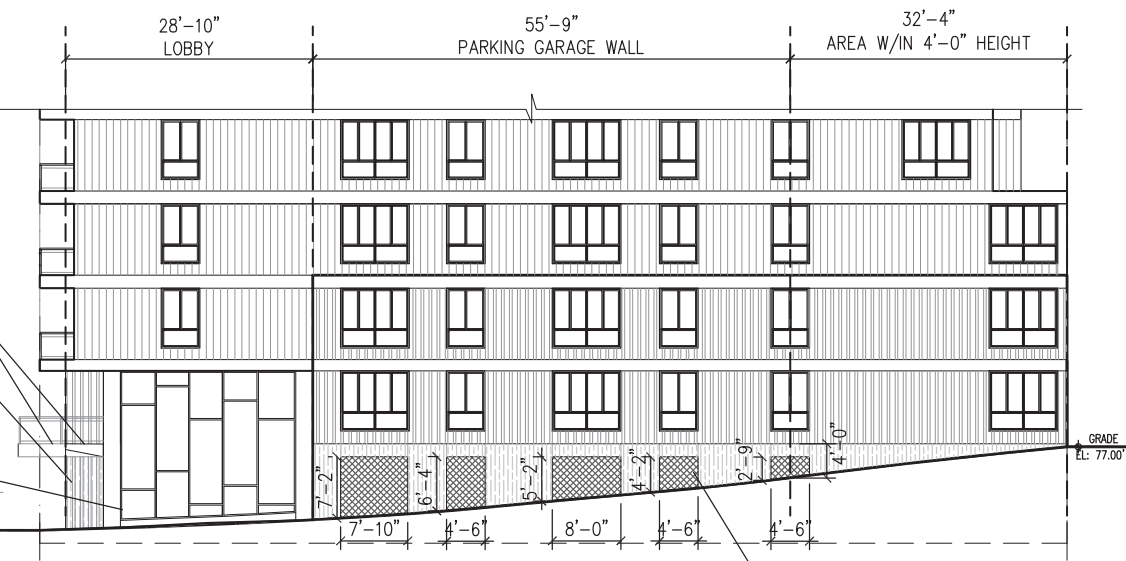
STREET FACING FACADES HAVE BEEN ENHANCED BY THE INCORPORATION OF FORM-LINER CONCRETE DETAILING, MULTIPLE CANOPIES, WOOD STOREFRONTS, & METAL SCREEN OPENINGS WITH PLANTINGS.



9 W / E SECTION
SCALE: 1/16" = 1'-0"



10 WEST ELEVATION
SCALE: 1/16" = 1'-0"



11 SOUTH ELEVATION
SCALE: 1/16" = 1'-0"

(5) DECORATIVE METAL SCREEN WITH PLANTINGS PER LANDSCAPE