

ISOLA 15TH BALLARD  
7530 15TH AVENUE NW, SEATTLE, WA



DESIGN REVIEW RECOMMENDATION

NORTHWEST BOARD  
DPD# 3017926

OCTOBER 5, 2015

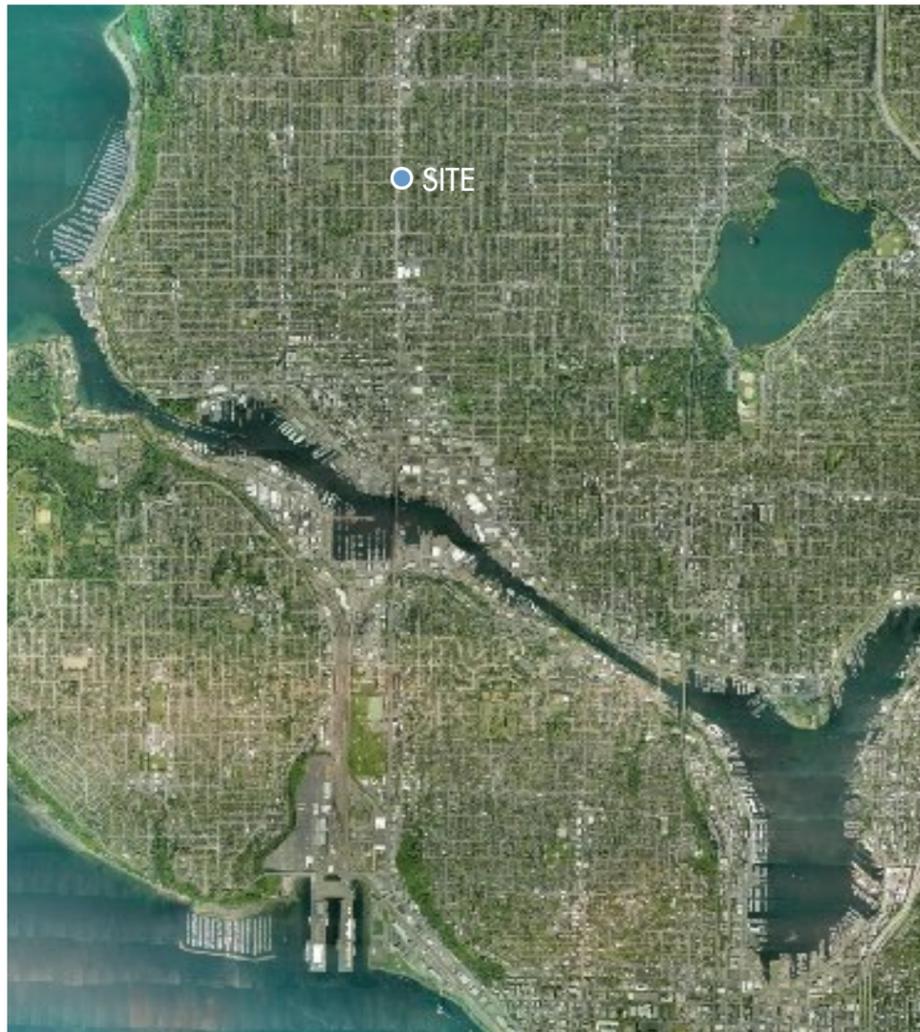


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## PROJECT VISION

Set in a growing and eclectic area of north Ballard, this project combines durable and richly textured materials with a clean, modern architecture, resulting in a project whose live/work homes embrace the commercial character of 15th Ave NW while also establishing a quieter interior-focused community of townhouses. Street life along 15th will be enhanced through thoughtfully crafted retail spaces, while a lushly planted courtyard will mark entry to the residential portion of the site. Along NW 75th Street, the buildings will help transition from the activity on 15th to the single family properties beyond. Outdoor amenity spaces are provided with private yards and shared greenspace at ground level, along with private rooftop decks atop each home. The decks will take advantage of broad territorial views with potential for views of the Olympic Mountains, the ship canal, and downtown.



SEATTLE AREA SITE MAP

## PROGRAM OBJECTIVES

- Provide a vibrant residential environment.
- Provide viable and flexible commercial space along 15th Ave NW.
- Allow for at least one parking space per home, in excess of the requirements.
- Engage the street edge and provide an improved pedestrian experience.
- Take advantage of territorial views to the west and south.
- Remain sensitive to site-specific micro zoning.

## PROJECT PROGRAM

|                            |                              |
|----------------------------|------------------------------|
| Number of Units:           | 53                           |
| Live-Work Units            | 20                           |
| Townhouses                 | 23                           |
| Area of Residential Uses:  | 48,732 SF                    |
| Area of Commercial Uses:   | 27,300 SF                    |
| Parking and Utility Areas: | 20,409 SF                    |
| Total Area:                | 96,471 SF                    |
| Number of Parking Spaces:  | 70 stalls in at-grade garage |
| Floor Area Ratio:          | 3.25 max / 2.53 provided     |
| Number of Stories:         | 4 stories                    |

## OWNER

Isola Homes  
1518 1st Avenue S  
Suite 301  
Seattle, WA 98134

## ARCHITECT

Nicholson Kovalchick Architects  
310 1st Avenue S  
Suite 4S  
Seattle, WA 98104

## DPD CONTACT

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206-684-0814

## PROPOSED

## CONTENTS

|    |                                   |
|----|-----------------------------------|
|    | <b>INTRODUCTION</b>               |
| 3  | Project Goals & Program           |
|    | <b>CONTEXT ANALYSIS</b>           |
| 5  | Neighborhood & Zoning             |
| 6  | Existing Site Map                 |
| 8  | Streetscapes                      |
|    | <b>DESIGN PROPOSAL</b>            |
| 11 | EDG Concept                       |
| 12 | Vicinity Site Plan                |
| 13 | Floor Plans                       |
| 18 | Elevations                        |
| 24 | Sections                          |
| 26 | Live/Work Elevations              |
| 28 | Podium Townhouse Elevations       |
| 30 | At-Grade Townhouse Elevations     |
| 32 | Renderings                        |
| 34 | 15th Ave Storefronts              |
| 35 | 75th Street & Residential Entries |
| 36 | Renderings                        |
| 38 | Response to EDG Guidance          |
| 41 | Landscape Plan                    |
| 42 | Planting Palette                  |
| 44 | Courtyard Landscaping             |
| 46 | Unopened R.O.W. Treatment         |
| 47 | Glazing Study                     |
| 48 | Lighting Plan                     |
| 49 | Signage Concept                   |
|    | <b>APENDIX</b>                    |
| 51 | Departures 1, 2, & 3              |
| 54 | Shadow Studies                    |

# CONTEXT ANALYSIS

NEIGHBORHOOD ZONING & DEVELOPMENT

NEIGHBORHOOD ANALYSIS

EXISTING SITE PLAN

EXISTING STREETSCAPES

# CONTEXT ANALYSIS

## NEIGHBORHOOD ZONING & DEVELOPMENT

### EXISTING SITE

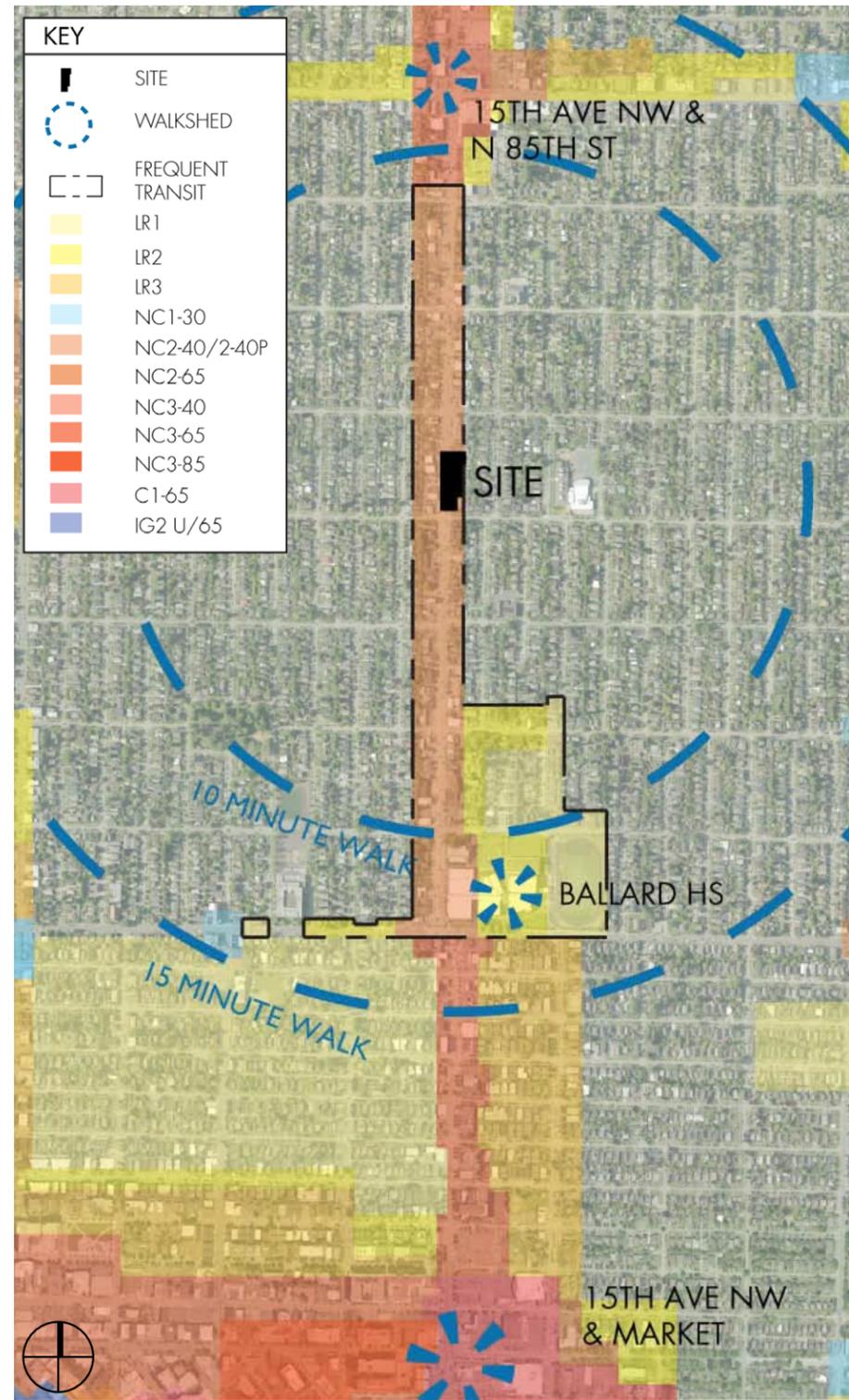
- Located at the NE corner of 15th Ave NW & NW 75th St.
- Existing Development to be replaced:
  - 7500: Westernco Donut House
  - 7510: Stop-N-Go
  - 7514: The Greener Cleaners
  - 7518: Galway Traders
  - 7522: Unfinished concrete shell
  - 7530: Ballard Muffler & Auto Center
- Site is approximately 340' long and 70' wide at the south, 113' wide at the center, and 127' wide at the north.
- Site slopes approximately 12' from the northwest to the southeast.
- There is an unopened city right-of-way along the east side of the property.

### ZONING & OVERLAY

- Zoned NC2-40/2-40P in its entirety.
- Within a verified Frequent Transit Overlay
- Parcels to the North, South, and West also zoned NC2-40
- Parcels to the East zoned SF 5000

### NEIGHBORHOOD DEVELOPMENT

- Location is midway between neighborhood nodes at Ballard HS & NW 85th Street.
- Usage along 15th is in transition, with a mix of new development, older structures, adult entertainment venues, and the occasional unused building.
- Whittier Elementary is two blocks east of the site, and several small parks are nearby.



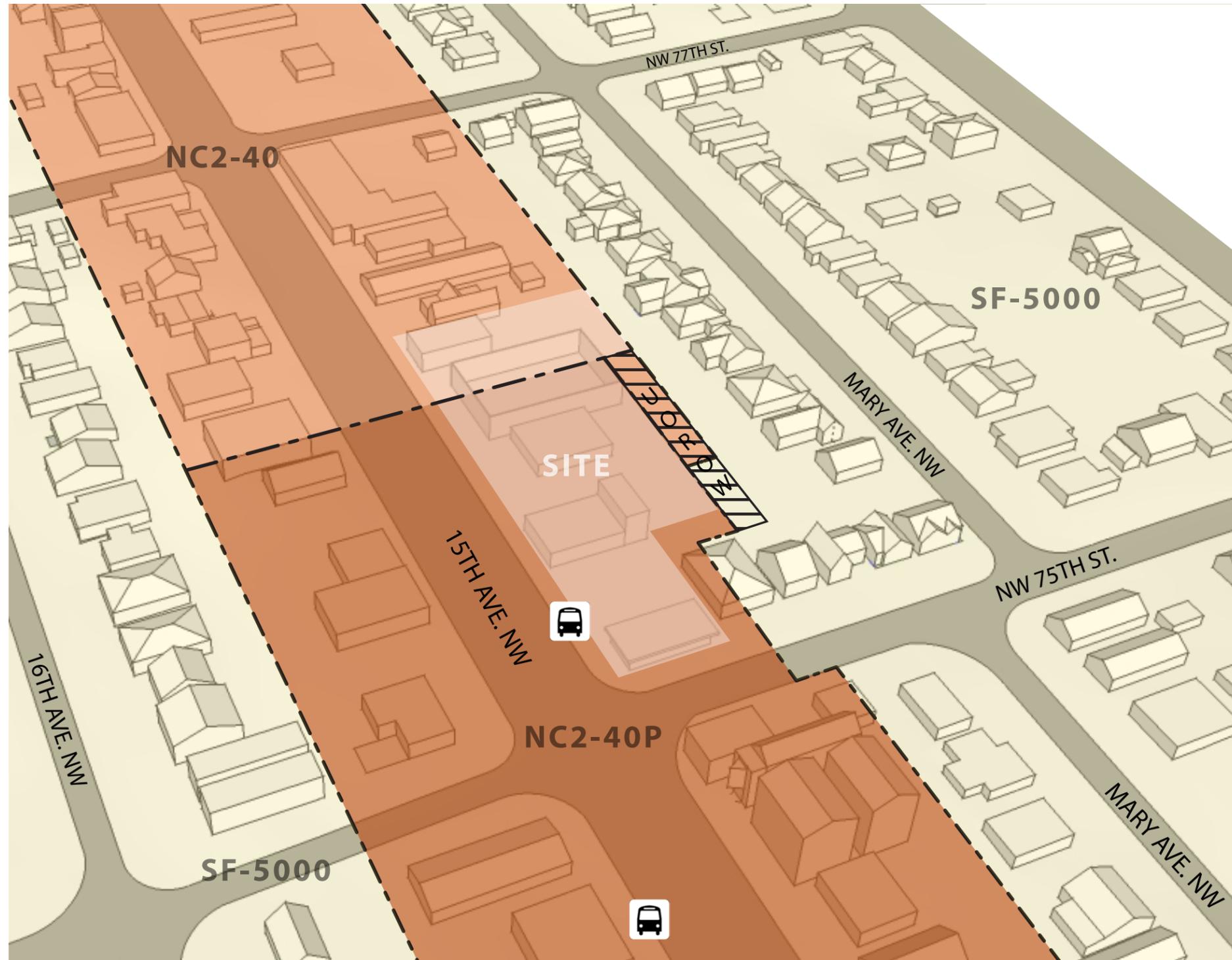
NEIGHBORHOOD ZONING MAP



ADJACENT BUSINESSES - 15TH AVENUE NW

# CONTEXT ANALYSIS

## EXISTING SITE PLAN



LOCAL ZONING AND CONTEXT MAP

### EXISTING SITE CONDITIONS

- Metro Rapid Ride bus stop just north of the intersection.
- An unopened right-of-way extends across half of the eastern property line.
- 15th Avenue NW contains two lanes of traffic in each direction, a center turn lane, and parallel parking along both sides of the street.
- NW 75th Street has one lane of traffic in each direction with no parking adjacent to the site, but transitions to parking on both sides just east of the site.
- The zoning transition occurs within the single family parcel immediately to the east along NW 75th Street.

### CONSTRAINTS

- A portion of the east property line abuts the SF-5000 zoning, requiring that setbacks at these points be maintained.
- There is a slope from northwest to southeast of about 13' across the site.
- Overhead wires along 15th Ave NW will impact views.
- Transitional nature of development along 15th Ave NW.
- Large existing tree located in unopened right-of-way to the east of the site.

### OPPORTUNITIES

- Location at intersection of 15th Ave NW and NW 75th Street affords increased visibility for the project.
- Substantial territorial and mountain views from upper levels to the east, south, and west.
- Frequent transit stop (Rapid Ride) is located at the site for quick access to downtown.
- Nearby schools, grocery stores, churches, recreation areas are appealing to families.

### LEGEND

-  SF-5000
-  NC2-40
-  NC2-40P
-  RAPID RIDE BUS STOP
-  R.O.W.



# CONTEXT ANALYSIS

## EXISTING STREETSAPES



15TH AVE. NW LOOKING WEST



15TH AVE. NW LOOKING SOUTH EAST



15TH AVE. NW LOOKING EAST



15TH AVE. NW LOOKING NORTH WEST

15TH AVE NW TOWNHOMES - DPD # 3017926

DESIGN REVIEW RECOMMENDATION

# DESIGN PROPOSAL

EDG CONCEPT  
VICINITY SITE PLAN  
FLOOR PLANS  
SITE & BUILDING ELEVATIONS  
BUILDING SECTIONS  
LIVE/WORK ELEVATIONS  
PODIUM TOWNHOUSE ELEVATIONS  
AT-GRADE TOWNHOUSE ELEVATIONS  
RENDERINGS  
STOREFRONT AND STREET EXPERIENCE  
RESPONSE TO EDG GUIDANCE  
LANDSCAPING  
GLAZING STUDY  
LIGHTING PLAN  
SIGNAGE CONCEPT

# DESIGN PROPOSAL

## EDG CONCEPT



LEVEL 2 PLAN  
NTS

### BLOCK SECTIONS AND SETBACKS

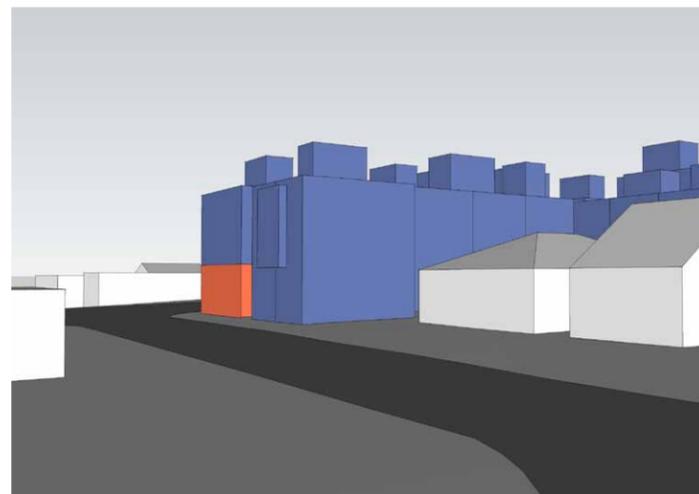
- An existing 15' wide x 170' long unopened right-of-way.
- 15' building setback immediately south of R.O.W. provide a buffer between the project and multiple rear yards.
- Single-family rear yards to east are approximately 60'-70' deep.
- At south end of site, location of zoning change allows no setback, but a 5' setback has been provided.
- Multiple greenspaces, 53 for sale units, 55 parking stalls, and street level commercial live/work space.
- Provides substantial living and commercial spaces, superior greenspace and respect for adjacent properties.

### EAST ELEVATIONS AND WINDOWS

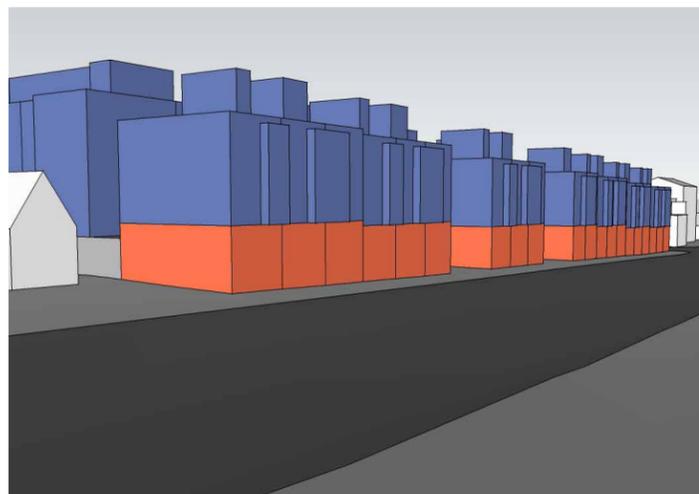
- Development capacity could allow 130 rental apartment units with 65 units facing into single family yards.
- The preferred scheme proposes 12 owner-occupied units facing the east.
- Multiple breaks in the massing allow for additional light onto adjacent properties.
- Plantings along with railings and screening elements will minimize situations where outdoor spaces look directly down into adjacent yards.



15TH AVE NW LOOKING NORTH



NW 75TH ST LOOKING WEST



15TH AVE NW LOOKING SOUTH



# DESIGN PROPOSAL

## VICINITY SITE PLAN



# FLOOR PLANS

**COLOR KEY**

- RETAIL
- RESIDENTIAL
- UTILITY
- DECKS
- BUILT-IN PLANTERS



LEVEL 1



# DESIGN PROPOSAL

## FLOOR PLANS

**COLOR KEY**

- RETAIL
- RESIDENTIAL
- UTILITY
- DECKS
- BUILT-IN PLANTERS



LEVEL 2

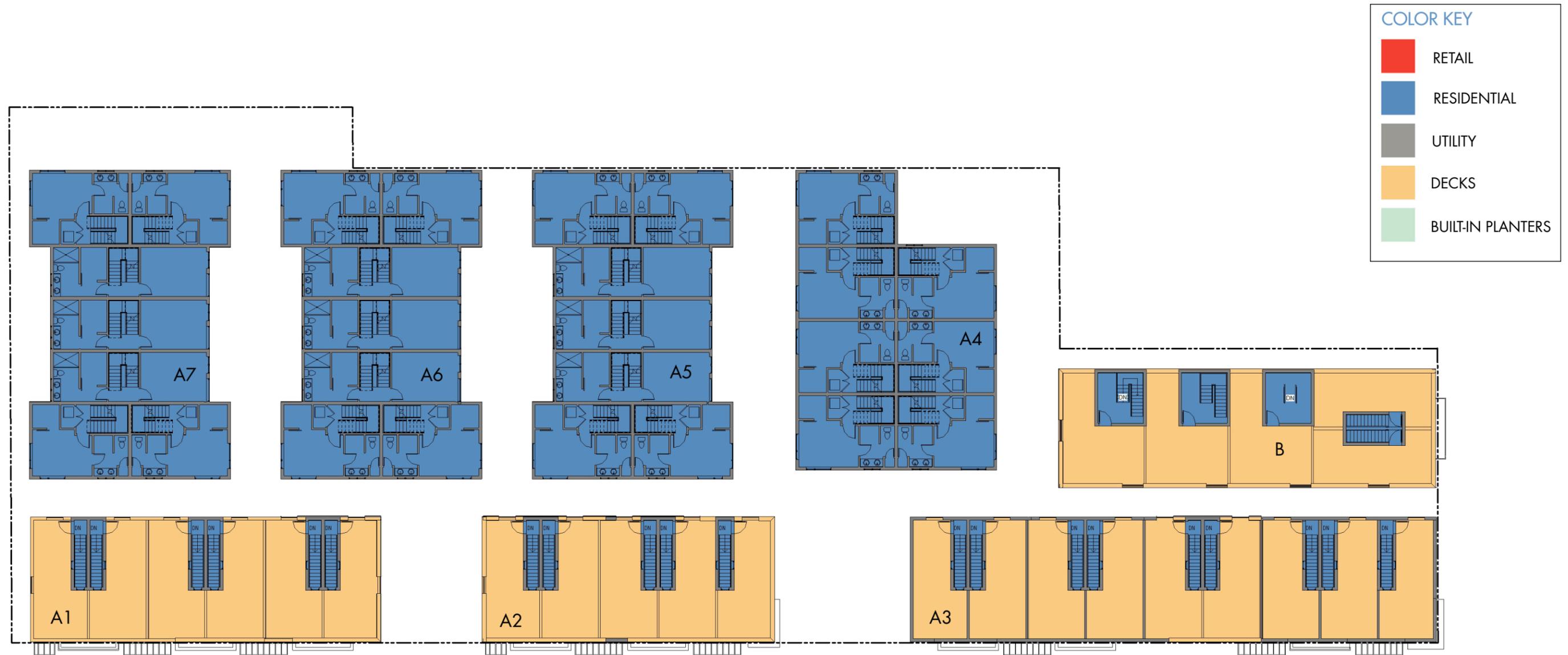


LEVEL 3

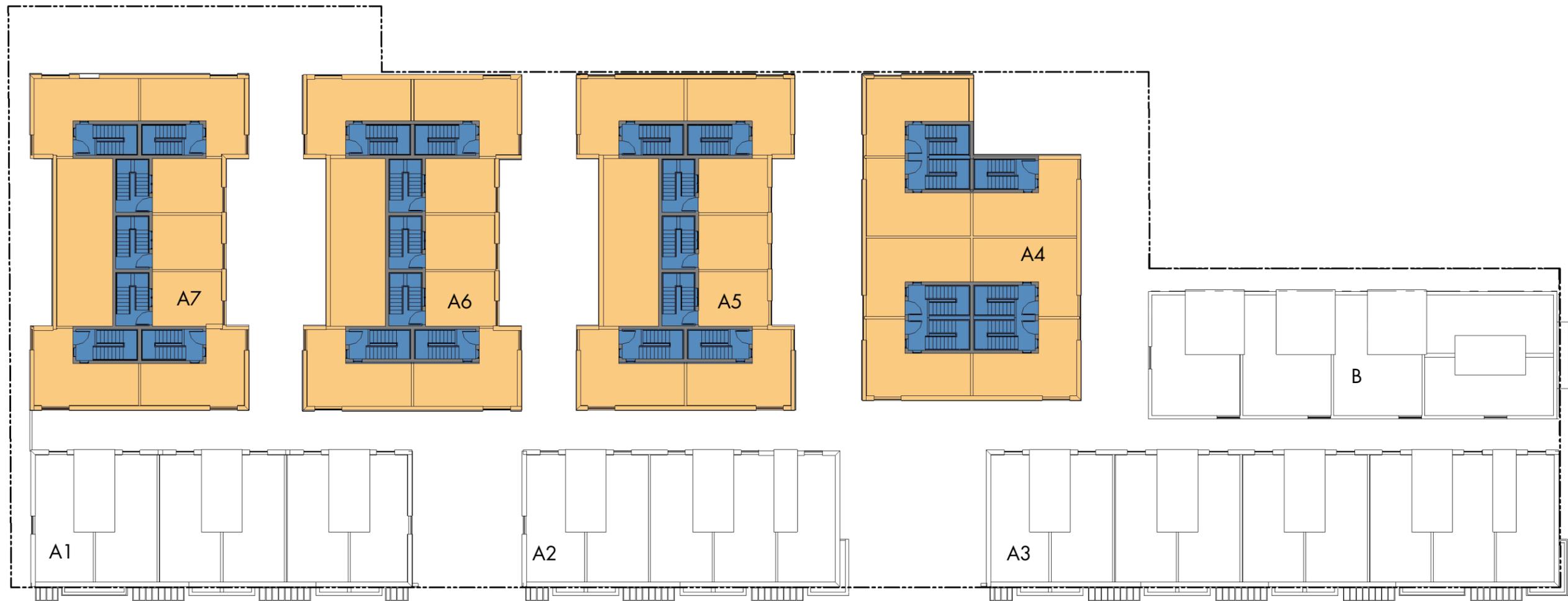


# DESIGN PROPOSAL

## FLOOR PLANS



LEVEL 4 & ROOF



ROOF



# DESIGN PROPOSAL

## STREET ELEVATIONS



FIBER CEMENT PANEL  
⑥

VINYL WINDOW  
⑤

CEDAR SIDING  
⑦

DECK RAILING  
⑩

CEDAR SIDING  
⑦

METAL SHADE

PL

BLADE SIGN

② BRICK

⑨ METAL CANOPY

④ STOREFRONT GLAZING SYSTEM

RETAIL ENTRY

④ EXPOSED CONCRETE

RESIDENTIAL ENTRY

RETAIL ENTRY

④ STOREFRONT GLAZING SYSTEM

### MATERIAL PALETTE



① WOOD FENCING



② FULL BRICK COLOR: COAL CREEK



③ EXPOSED ARCHITECTURAL CONCRETE



④ VINYL STOREFRONT GLAZING SYSTEM COLOR: BLACK



⑤ VINYL WINDOWS, COLOR: WHITE

# STREET ELEVATIONS



|  |  |  |  |  |   |  |  |  |                                   |
|--|--|--|--|--|---|--|--|--|-----------------------------------|
|  | <p><b>6</b><br/>FIBER CEMENT PANELS<br/>COLOR: SW7634<br/>- PEDIMENT</p> |  | <p><b>7</b><br/>CEDAR SIDING<br/>COLOR: WALNUT STAIN</p> |  | <p><b>8</b><br/>FIBER CEMENT PANELS<br/>COLOR: SW7642<br/>- PAVESTONE</p> |  | <p><b>9</b><br/>METAL CANOPY<br/>CEDAR SOFIT</p> |  | <p><b>10</b><br/>DECK RAILING</p> |
|--|--|--|--|--|---|--|--|--|-----------------------------------|

15TH AVE NW TOWNHOMES - DPD # 3017926

DESIGN REVIEW RECOMMENDATION

# DESIGN PROPOSAL

## STREET ELEVATIONS

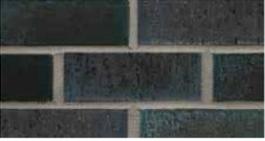


METAL CANOPY ⑨ STOREFRONT GLAZING SYSTEM

RESIDENTIAL ENTRY ⑤ VINYL WINDOW



① WOOD FENCING



② FULL BRICK COLOR: COAL CREEK



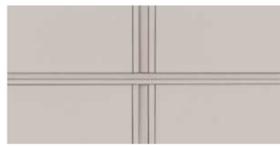
③ EXPOSED ARCHITECTURAL CONCRETE



④ VINYL STOREFRONT GLAZING SYSTEM COLOR: BLACK



⑤ VINYL WINDOWS, COLOR: WHITE



⑥ FIBER CEMENT PANELS  
COLOR: SW7634  
- PEDIMENT



⑦ CEDAR SIDING  
COLOR: WALNUT STAIN



⑧ FIBER CEMENT PANELS  
COLOR: SW7642  
- PAVESTONE



⑨ METAL CANOPY  
CEDAR SOFIT



⑩ DECK RAILING

# DESIGN PROPOSAL

## ELEVATIONS



### MATERIAL PALETTE



①  
WOOD  
FENCING



②  
FULL BRICK  
COLOR:  
COAL CREEK



③  
EXPOSED ARCHITECTURAL  
CONCRETE



④  
VINYL STOREFRONT  
GLAZING SYSTEM  
COLOR: BLACK



⑤  
VINYL WINDOWS,  
COLOR: WHITE

ELEVATIONS



FIBER CEMENT PANEL  
⑥

FIBER CEMENT PANELS  
⑧

FENCE  
①

VINYL WINDOWS  
⑤

CEDAR SIDING  
⑦

EXPOSED ARCHITECTURAL CONCRETE  
③

DECK RAILING  
⑩

PL



⑥ FIBER CEMENT PANELS  
COLOR: SW7634  
- PEDIMENT



⑦ CEDAR SIDING  
COLOR: WALNUT STAIN



⑧ FIBER CEMENT PANELS  
COLOR: SW7642  
- PAVESTONE



⑨ METAL CANOPY  
CEDAR SOFIT



⑩ DECK RAILING

# DESIGN PROPOSAL

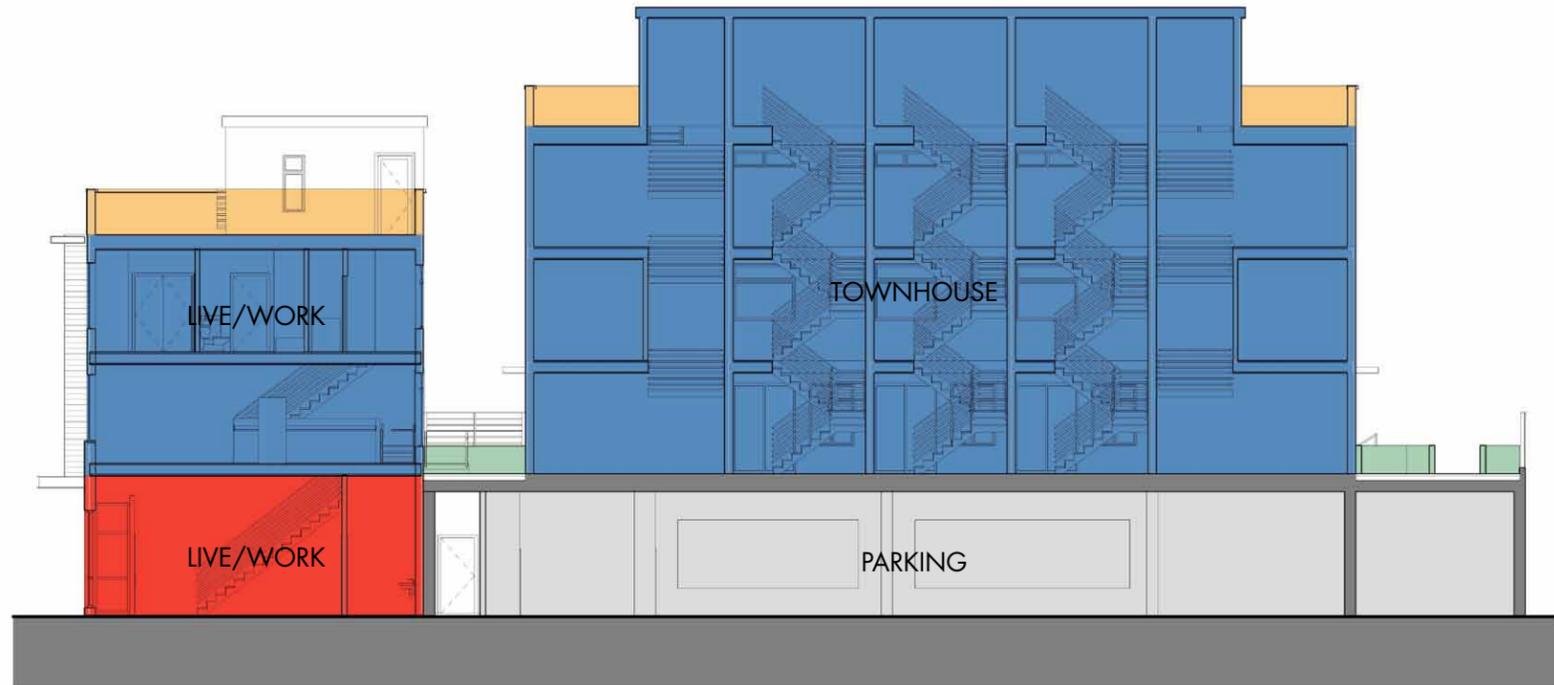
## SECTIONS

| COLOR KEY                             |                   |
|---------------------------------------|-------------------|
| <span style="color: red;">■</span>    | RETAIL            |
| <span style="color: blue;">■</span>   | RESIDENTIAL       |
| <span style="color: gray;">■</span>   | UTILITY           |
| <span style="color: orange;">■</span> | DECKS             |
| <span style="color: green;">■</span>  | BUILT-IN PLANTERS |



SECTION THROUGH PODIUM TOWNHOUSES





SECTION THROUGH PODIUM TOWNHOUSE AND LIVE/WORK

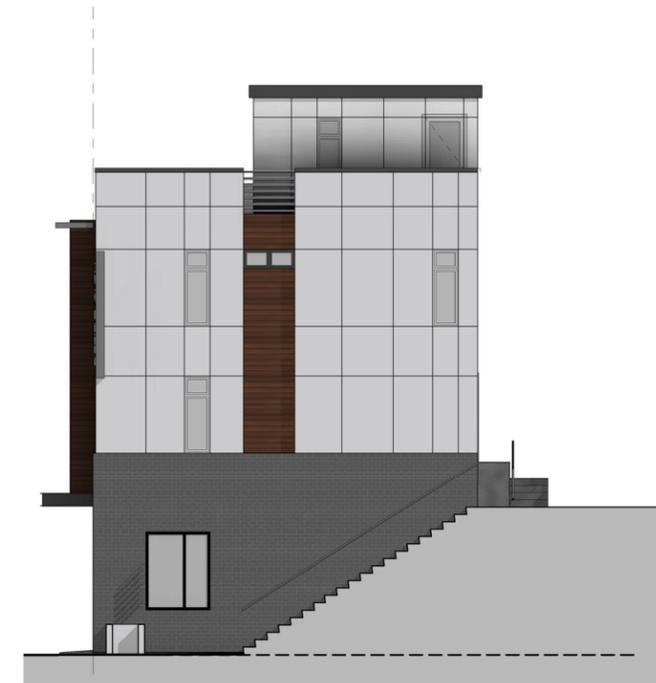


# DESIGN PROPOSAL

LIVE/WORK ELEVATIONS



WEST ELEVATION



SOUTH ELEVATION



EAST ELEVATION



NORTH ELEVATION



15TH AVE NW TOWNHOMES - DPD # 3017926

# DESIGN PROPOSAL

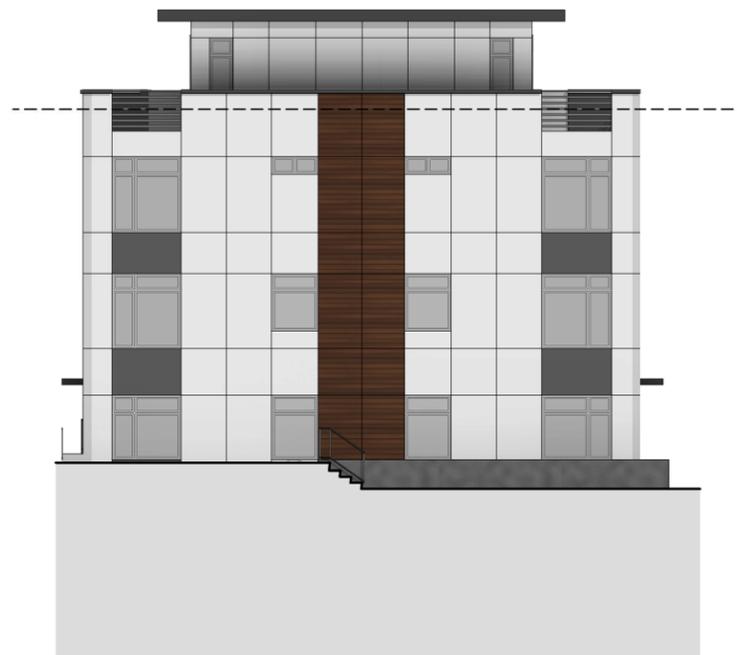
## PODIUM TOWNHOUSE ELEVATIONS



EAST ELEVATION



NORTH ELEVATION



WEST ELEVATION



SOUTH ELEVATION



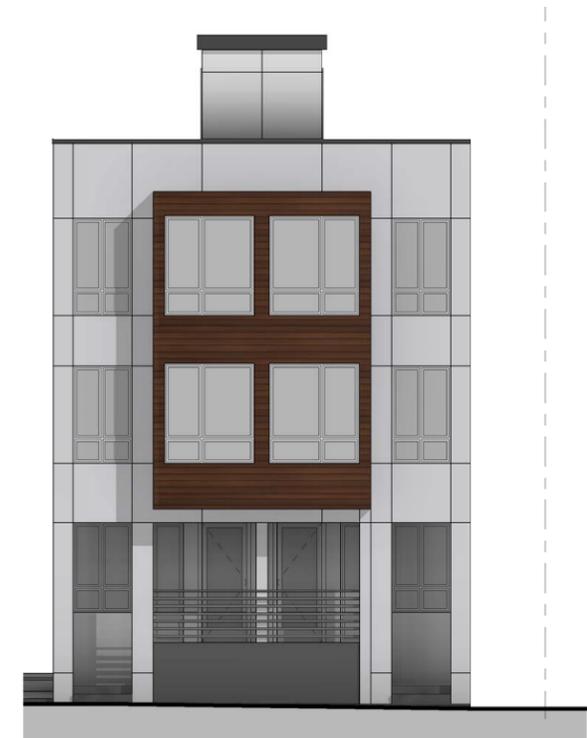
FOREGROUND BUILDING OMITTED FOR PERSPECTIVE RENDERING

# DESIGN PROPOSAL

## AT-GRADE TOWNHOUSE ELEVATIONS



EAST ELEVATION



SOUTH ELEVATION



WEST ELEVATION



NORTH ELEVATION



15TH AVE NW TOWNHOMES - DPD # 3017926

DESIGN REVIEW RECOMMENDATION

RENDERINGS



# DESIGN PROPOSAL RENDERINGS

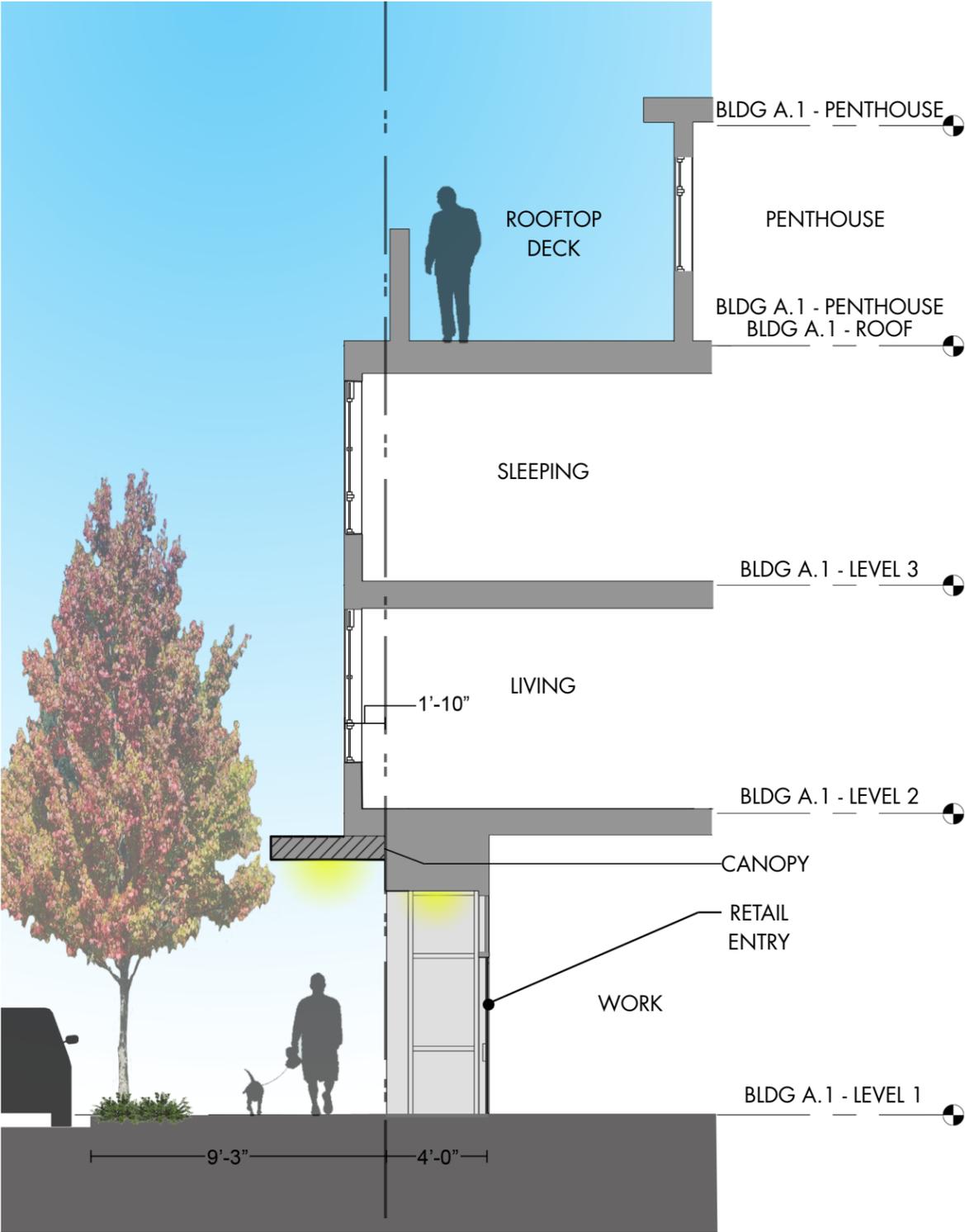


15TH AVE NW TOWNHOMES - DPD # 3017926

DESIGN REVIEW RECOMMENDATION

# DESIGN PROPOSAL

## 15TH AVENUE STOREFRONTS



### 15TH AVE PEDESTRIAN EXPERIENCE

New street trees soften the street edge and add color and shade for pedestrians, while canopies above offer shelter from Seattle’s frequent rainy days. Planting strips work with the street trees to provide a buffer between pedestrians and traffic as well as break up the concrete walk into more human-scaled sections. The canopies have gaps located at each street tree to allow for their growth and create a pleasing engagement between the building and nature. Doorways into the retail spaces are recessed to provide additional shelter and a momentary landing space outside the flow of foot traffic prior to entry, as well as recessed lighting for safety. Signage is attached to the face of the building under the canopies, which contain additional recessed lighting for the sidewalk and provide a safe, glowing light at night.

SECTION THROUGH RETAIL ENTRANCE CANOPY

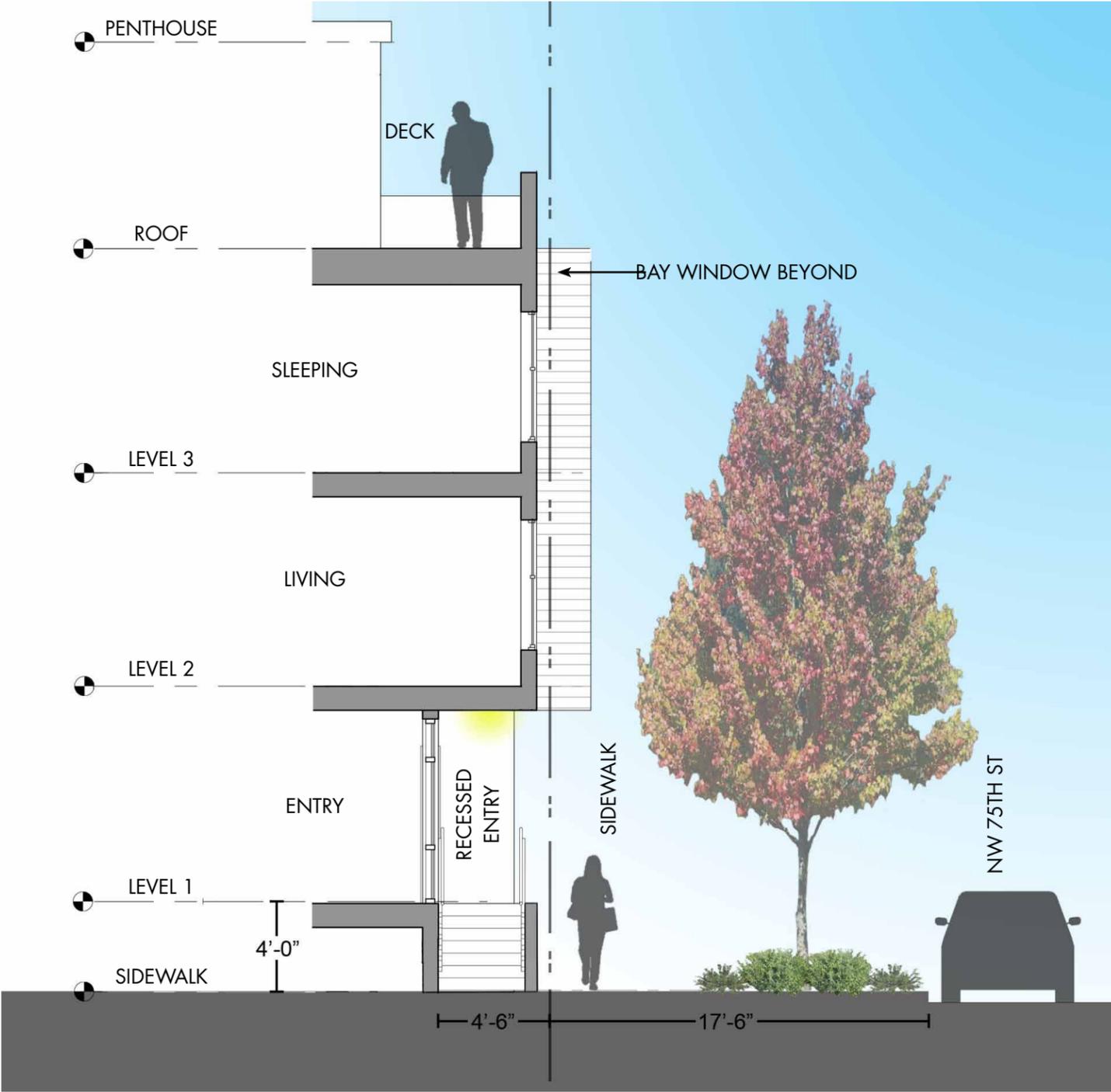
# DESIGN PROPOSAL

## 75TH ST NW STREET & RESIDENTIAL ENTRY



### 75TH PEDESTRIAN EXPERIENCE

The broader sidewalk along 68th, combined with recessed porches at the townhouses, allow for larger planting strips that enhance the residential character of this street. The street trees and colorful plantings proposed along 15th will continue here and serve as a buffer for children that use this walk to reach nearby Whittier Elementary. The townhouse porches are raised to provide privacy for the occupants from passers by, but well lit and open to allow for security and eyes on the street. A walkway separates the two buildings on this street and provides access to the interior facing townhomes as well as secondary access to the commercial spaces facing 15th.



SECTION THROUGH RESIDENTIAL ENTRY CANOPY

# DESIGN PROPOSAL

RENDERINGS



# DESIGN PROPOSAL

RENDERINGS



15TH AVE NW TOWNHOMES - DPD # 3017926

DESIGN REVIEW RECOMMENDATION

# RESPONSE TO DESIGN GUIDANCE

## DESIGN GUIDELINE PRIORITIES & EDG GUIDANCE

### CS1 NATURAL SYSTEMS & SITE FEATURES

#### CS1-B SUNLIGHT & NATURAL VENTILATION

CS1-B-2. Daylight and Shading: Maximize daylight for interior and exterior spaces and minimize shading on adjacent sites through the placement and/or design of structures on site.

#### CS1-D PLANTS AND HABITAT

CS1-D-1. On-Site Features: Incorporate on-site natural habitats and landscape elements into project design and connect those features to existing networks of open spaces and natural habitats wherever possible. Consider relocating significant trees and vegetation if retention is not feasible.

CS1-D-2. Off-Site Features: Provide opportunities through design to connect to off-site habitats such as riparian corridors or existing urban forest corridors. Promote continuous habitat, where possible, and increase interconnected corridors of urban forest and habitat where possible.

#### EDG GUIDANCE

The Board agreed that the design should minimize shading on adjacent sites through placement and design of structures on-site, and particularly supported the space between each to allow daylight to reach interior and exterior spaces, as well as how the project impacts the adjacent single family structures to the east. The large tree in the unopened right-of-way should be preserved.

#### RESPONSE

The east-west oriented courtyards between the interior buildings has been carried over from the EDG proposal to preserve a higher level of daylight access through the site in the afternoon than a north-south orientation would provide. Planters at the eastern edge of outdoor spaces are provided to serve as a buffer and minimize casual views into the single family yards beyond.

The building atop the southeast corner of the podium has a notch cut out at the corner to accommodate the large existing tree in the unopened alley, and an arborist has been engaged to evaluate and recommend protection of the root zone during construction. Beyond steps taken to minimize impacts within the project site, the developer has also reached out to homeowners along the east property line and is working with them to provide and plant additional screening vegetation in their yards.

### CS2 URBAN PATTERN & FORM

#### CS2-A LOCATION IN THE CITY & NEIGHBORHOOD

CS2-A-2. Architectural Presence: Evaluate the degree of visibility or architectural presence that is appropriate or desired given the context, and design accordingly.

#### CS2-C RELATIONSHIP TO THE BLOCK

CS2-C-1. Corner Sites: Corner sites can serve as gateways or focal points; both require careful detailing at the first three floors due to their high visibility from two or more streets and long distances.

#### CS2-D HEIGHT, BULK & SCALE

CS2-D-3. Zone Transitions: For projects located at the edge of different zones, provide an appropriate transition or complement to the adjacent zone(s). Projects should create a step in perceived height, bulk and scale between the anticipated development potential of the adjacent zone and the proposed development.

CS2-D-4. Massing Choices: Strive for a successful transition between zones where a project abuts a less intense zone.

CS2-D-5. Respect for Adjacent Sites: Respect adjacent properties with design and site planning to minimize disrupting the privacy of residents in adjacent buildings.

#### EDG GUIDANCE

The Board supported the scale of the townhouse structures of the preferred option. They recommended the project transition between or complement the single-family structures to the east, respond to the existing character of 75th Street and the homes and school that it serves, and provide a sunlight/shadow study and window study and relative to the single family homes to the east. As the corner site is highly visible, the detailing of the buildings facing these locations is important.

#### RESPONSE

Because the northern portion of the site directly abuts the single-family lots to the east, the structures over the parking garage maintain the necessary 15' setback from the property line, and the rooftop outdoor space is lined with planters, screening views into yards below. The middle half of the site borders an unopened alley that inherently acts as a buffer space between the existing homes and the new development, and the southernmost building has been voluntarily set back 5-feet from the property line, even though none is required due to the zoning change occurring within the neighboring property.

The requested window study has been provided, however due to the distance between the project and the back of most existing homes, the relationship between these windows is closer to views that would be achieved if the occupants were across a street from one another. In addition, windows on the east side of Building B are minimized and typically high on the wall of the rooms they serve.

Finally, the project engages the corner with a bay and canopy that wrap the corner and acknowledge the commercial character of the ground level space, then breaking down the elevation elements along the 75th facades to accommodate a transition to the street's more residential character.

### PL1 CONNECTIVITY

#### PL1-B WALKWAYS AND CONNECTIONS

PL1-B-1. Pedestrian Infrastructure: Connect on-site pedestrian walkways with existing public and private pedestrian infrastructure, thereby supporting pedestrian connections within and outside the project.

PL1-B-2. Pedestrian Volumes: Provide ample space for pedestrian flow and circulation, particularly in areas where there is already heavy pedestrian traffic or where the project is expected to add or attract pedestrians to the area.

#### EDG GUIDANCE

The Board encouraged a design that will enhance the narrow sidewalk along 15th and pedestrian oriented spaces that enliven the area. Connect on-site walkways to the existing pedestrian infrastructure, and visible access to building entries should be provided.

#### RESPONSE

This project provides multiple pedestrian access points, three along 15th and one on 75th, affording easy access from all approaches and existing walkways. Primary access to the interior of the site is from the entrance courtyard on 15th, which is heavily planted to stand out from the commercial character of adjacent storefronts. The entrances to the commercial spaces have been recessed from the sidewalk to provide a "landing" space for customers to enter and exit without impacting use of the sidewalk.

### PL2 WALKABILITY

#### PL2-B SAFETY & SECURITY

PL2-B-1. Eyes on the Street: Create a safe environment by providing lines of sight and encouraging natural surveillance.

PL2-B-2. Lighting for Safety: Provide lighting at sufficient lumen intensities and scales, including pathway illumination, pedestrian and entry lighting, and/or security lights.

PL2-B-3. Street-Level Transparency: Ensure transparency of street-level uses (for uses such as nonresidential uses or residential lobbies), where appropriate, by keeping views open into spaces behind walls or plantings, at corners, or along narrow passageways.

## EDG GUIDANCE

The Board recommended the Live/Work units provide street-level transparency and flexibility in their design to allow future commercial adaptation. The pedestrian walkways should be evaluated for access to existing infrastructure, visibility of entrances, and safety.

## RESPONSE

The storefronts feature large windows and entrance alcoves that are well lit with recessed lighting above each glass door. Combined with ample windows in the residences above, this provides substantial “eyes on the street” and eliminates opportunities for congregating in dark corners. Access to the interior spaces is controlled through gates and secured doors to prevent unattended spaces hosting unwanted visitors. All pathways will be lit for secure travel while minimizing light and glare to neighbors.

## PL3 STREET-LEVEL INTERACTION

### PL3-A ENTRIES

PL3-A-2. Common Entries: Multi-story residential buildings need to provide privacy and security for residents but also be welcoming and identifiable to visitors.

PL3-A-4. Ensemble of Elements: Design the entry as a collection of coordinated elements including the door(s), overhead features, ground surface, landscaping, lighting, and other features.

### PL3-B RESIDENTIAL EDGES

PL3-B-1. Security and Privacy: Provide security and privacy for residential buildings through the use of a buffer or semi-private space between the development and the street or neighboring buildings.

PL3-B-3. Buildings with Live/Work Uses: Maintain active and transparent facades in the design of live/work residences. Design the first floor so it can be adapted to other commercial use as needed in the future.

## EDG GUIDANCE

The Board discussed the importance of developing an architectural concept that is unified and functional, fitting in well with its surroundings, and encouraging human interaction and activity at the street level.

## RESPONSE

The project responds to the sites needs for two different types of pedestrian entry. Residential access is through interior entrances, with access controlled at the street level by gates and secure doors from the parking garage. Commercial entrances are provided for each space, highly visible, and accessed directly from the sidewalk. The commercial spaces are located at the sidewalk for direct access and identification

from by pedestrians as well as being visible to those driving by. Canopies shelter the entrances and viewing space into storefronts, and provide opportunity for additional lighting should the need arise. These individual commercial spaces could easily be joined together internally to allow an owner of multiple units to create a larger commercial space in response to changes in demand.

## PL4 ACTIVE TRANSPORTATION

### PL4-B PLANNING AHEAD FOR BICYCLISTS

PL4-B-2. Bike Facilities: Facilities such as bike racks and storage, bike share stations, shower facilities and lockers for bicyclists should be located to maximize convenience, security, and safety.

### PL4-C PLANNING AHEAD FOR TRANSIT

PL4-C-2. On-site Transit Stops: If a transit stop is located onsite, design project-related pedestrian improvements and amenities so that they complement any amenities provided for transit riders.

## EDG GUIDANCE

The Board discussed the character of 15th Avenue Northwest, and accessibility for alternative modes of transportation. The Board recommended the inclusion of bicycle racks along the street for commercial customers.

## RESPONSE

Bike racks will be provided in the planting strip just south of the driveway along 15th, providing convenient bike storage for customers and employees of the commercial spaces. Additional bike storage is provided within the parking garage for use by residents.

An existing RapidRide stop is located in front of the site along 15th just north of the intersection, and the project team is working with Metro to meet their needs.

## DC1 PROJECT USES & ACTIVITIES

### DC1-B VEHICLE ACCESS & CIRCULATION

DC1-B-1. Access Location and Design: Choose locations for vehicular access, service uses, and delivery areas that minimize conflict between vehicles and non-motorists wherever possible. Emphasize use of the sidewalk for pedestrians, and create safe and attractive conditions for pedestrians, bicyclists, and drivers

## DC1-C PARKING & SERVICE USES

DC1-C-1. Below-Grade Parking: Locate parking below grade wherever possible. Where a surface parking lot is the only alternative, locate the parking in rear or side yards, or on lower or less visible portions of the site.  
DC1-C-2. Visual Impacts: Reduce the visual impacts of parking lots, parking structures, entrances, and related signs and equipment as much as possible.  
DC1-C-4. Service Uses: Locate and design service entries, loading docks, and trash receptacles away from pedestrian areas or to a less visible portion of the site to reduce possible impacts of these facilities on building aesthetics and pedestrian circulation

## EDG GUIDANCE

The Board noted 15th Avenue Northwest and Northwest 75th Street offer distinct pedestrian characters: one strongly vehicular, the other pedestrian, respectively. The Board agreed that each street façade be designed to respond appropriately. They liked the number of parking spaces proposed, and discussed the potential visual impacts created by the proposed parking podium. The Board requested details of the parking podium be included in the Recommendation packet. Below grade parking was encouraged.

## RESPONSE

Since NW 75th Street is heavily used by families travelling to nearby Whittier Elementary by foot and by car, and any access along this street would be incredibly close to the intersection and traffic signal, the project team agrees with the Board that vehicle access from 15th is preferred, and we have maintained the driveway location. Because this site is within a designated pedestrian zone, access is normally required to be taken from the non-arterial 75th street, requiring the departure proposed at the end of this document.

While this project is only required to provide parking for 17 vehicles, the at-grade parking garage contains 70 stalls (over four times the requirement). The parking garage is located at the rear of the site and fully enclosed to block spillover of light, sound, and odors onto neighboring properties. All service spaces related to the project are located inside the garage, improving the quality of outdoor spaces.

While we recognize the potential for below-grade parking to reduce the scale and massing of a project, the expense of excavation and building such a structure is prohibitive in this situation. Even if building below grade were viable, it would require maximizing the amount of residential space above to offset costs, eliminating any reduction in building massing.

# RESPONSE TO DESIGN GUIDANCE

## DESIGN GUIDELINE PRIORITIES & EDG GUIDANCE

### DC2 ARCHITECTURAL CONCEPT

#### DC2-A MASSING

DC2-A-2. Reducing Perceived Mass: Use secondary architectural elements to reduce the perceived mass of larger projects.

#### DC2-B ARCHITECTURAL & FACADE COMPOSITION

DC2-B-1. Façade Composition: Design all building façades—including alleys and visible roofs— considering the composition and architectural expression of the building as a whole. Ensure that all façades are attractive and well-proportioned.

DC2-B-2. Blank Walls: Avoid large blank walls along visible façades wherever possible. Where expanses of blank walls, retaining walls, or garage façades are unavoidable, include uses or design treatments at the street level that have human scale and are designed for pedestrians.

#### EDG GUIDANCE

The west, south, and east façades will be highly visible; therefore, the Board recommended that all façades be designed considering the composition and architectural expression of the building as a whole. Large blank walls should be avoided, and durable, maintainable materials should be used.

#### RESPONSE

Upper level bays along both street frontages work in conjunction with metal cornice accents in between to provide significant modulation of the façades. The bays further enhance the design through the use of woodgrained siding, large windows, and metal canopies covering storefronts below. What could have been one long, continuous structure is broken into multiple smaller forms through large openings to accommodate the entry courtyard and driveway on 15th, as well as the pedestrian entrance along 75th.

The residential buildings on the interior are grouped into clusters of 5-7 units instead of long monolithic blocks, allowing for improved access to light and views, both internally and through the site. Stair penthouses to private roof decks are clustered to the interior of the buildings, reducing the perceived height of the project from below.

While blank walls are minimal due to the copious use of windows, a combination of brick, panel, and plank siding in various complementary colors combine to provide additional variety. The north and east concrete walls of the parking garage feature large recesses with textured materials to provide visual relief, while maintaining privacy and protection from noise and odors. The elevations on the north side of the site have been voluntarily set back 5-feet or more from the lot line, providing a buffer from future development, allowing for a walkway at grade, and accommodating generous windows and building modulation on what could be a very plain wall.

### DC4 EXTERIOR ELEMENTS & FINISHES

#### DC4-A EXTERIOR ELEMENTS & FINISHES

DC4-A-1. Exterior Finish Materials: Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.

DC4-A-2. Climate Appropriateness: Select durable and attractive materials that will age well in Seattle's climate, taking special care to detail corners, edges, and transitions.

#### DC4-D TREES, LANDSCAPE, & HARDSCAPE MATERIALS

DC4-D-1. Choice of Plant Materials: Reinforce the overall architectural and open space design concepts through the selection of landscape materials.

DC4-D-3. Long Range Planning: Select plants that upon maturity will be of appropriate size, scale, and shape to contribute to the site as intended.

#### EDG GUIDANCE

The Board indicated that large blank walls should be avoided, and durable, maintainable materials should be used.

#### RESPONSE

Combining durable materials with a crisp, modern design, the project presents a unified and commercially appealing presence at a distance, yet textured and humanly scaled up close. The retail base along 15th is clad in real brick in a dark color that grounds the building and combines with the glass storefront and steel canopies to provide a strong commercial character. The residential spaces above and behind are wrapped in a mix of light gray painted panels, woodgrained siding, and metal accent details for a softer residential feel. As mentioned in DC2 above, the mixture of materials, windows, and building modulation provide for attractive façades that avoid large blank areas.

The pathways and particularly the entry courtyard will feature paved surfaces and a combination of at-grade planting beds and raised planters. These will be filled with native plantings that serve as brightly colored accents, screening elements at selected locations, a feature to soften building forms, and an opportunity to reduce rainwater runoff.

# DESIGN PROPOSAL

## LANDSCAPE PLANS



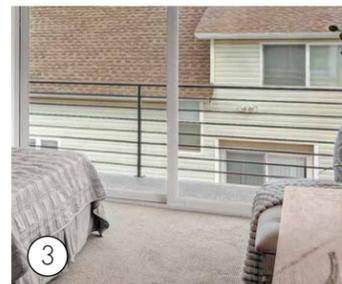
LANDSCAPE SITE PLAN  
NTS



1 WOOD FENCING



2 MESH RAIL



3 DECK RAILING @ ROOF



4 METAL CANOPY - CEDAR



5 MANUF. CONC. PLANTER



6 BUILT-IN CONC. PLANTER



7 PATIO PAVING



8 CONCRETE PLANK PAVERS

# DESIGN PROPOSAL

## PLANTING PALETTE



GOLDEN FULL MOOM MAPLE



BLACK MONDO GRASS



BLACK STEM BAMBOO



BLACK TOWER ELDERBERRY



CHARITY OREGON MAHONIA



DEER FERN



SUN KING ARALIA



PEE WEE OAKLEAF HYDRANGEA



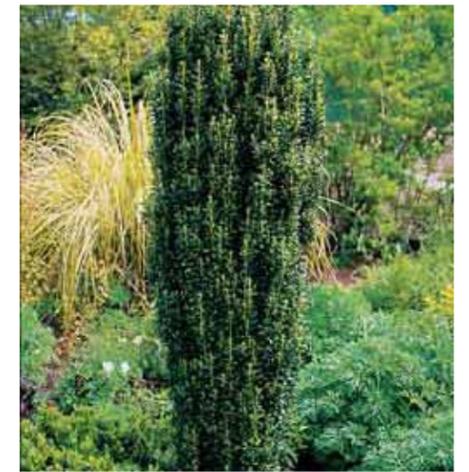
GOLDEN VARIEGATED JAPANESE SWEET FLAG



ASTILBE



JAPANESE PAINTED FERN



SKY PENCIL JAPANESE HOLLY



LONGLEAF MAHONIA



FRAGRANT SWEET BOX



AUCUBA



HINO CRIMSON AZALEA



GOLDEN RUBY BARBERRY



ORANGE SEDGE



CLIMBING HYDRANGEA



AUTUMN FERN



LITTLE BUNNY FOUNTAIN GRASS



SLENDER HINOKI FALSE CYPRESS



BUTTERFLY JAPANESE MAPLE



DAWYCK PURPLE BEECH



HOSTA FIRE AND ICE



LENTON ROSE CINNAMON AND SNOW



JAPANESE FOREST GRASS



SUNSET GLOW BAMBOO



ESPALIER CAMELLIA



FATSIA



LILY OF THE VALLEY BUSH



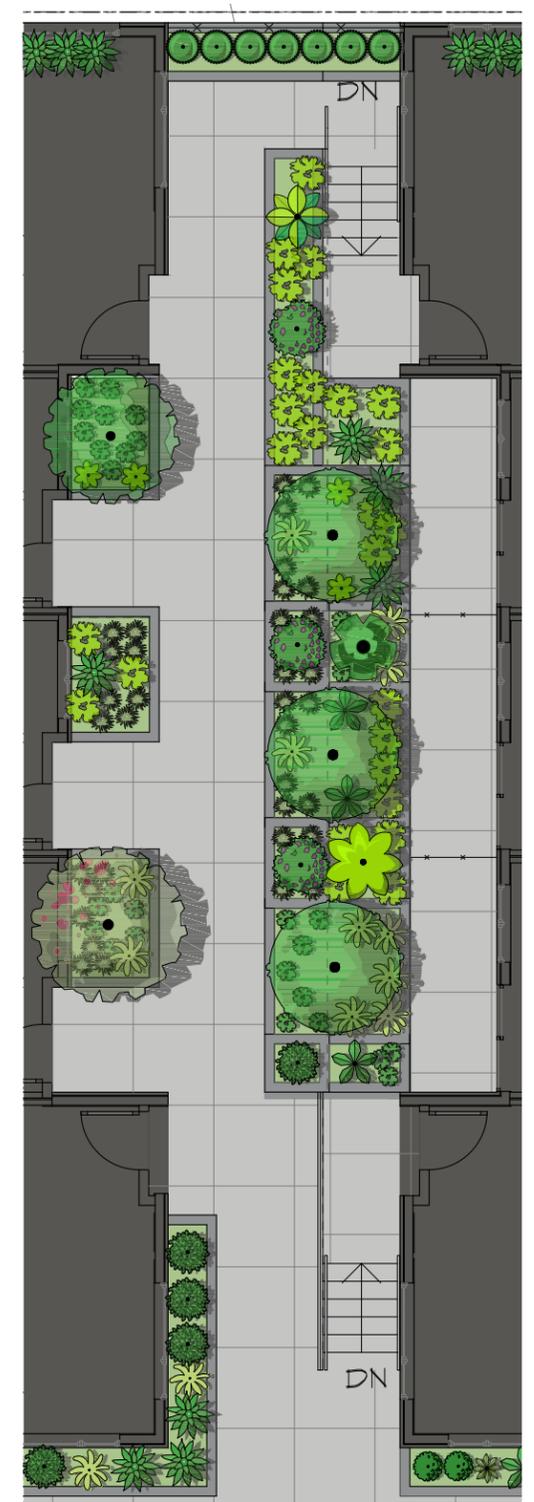
RAMAPO RHODODENDRUN

# DESIGN PROPOSAL

## INTERIOR COURTYARD LANDSCAPING



INTERIOR COURTYARD PATHS AND PLANTING PERSPECTIVE



INTERIOR COURTYARD



ENTRY COURTYARD LANDSCAPING

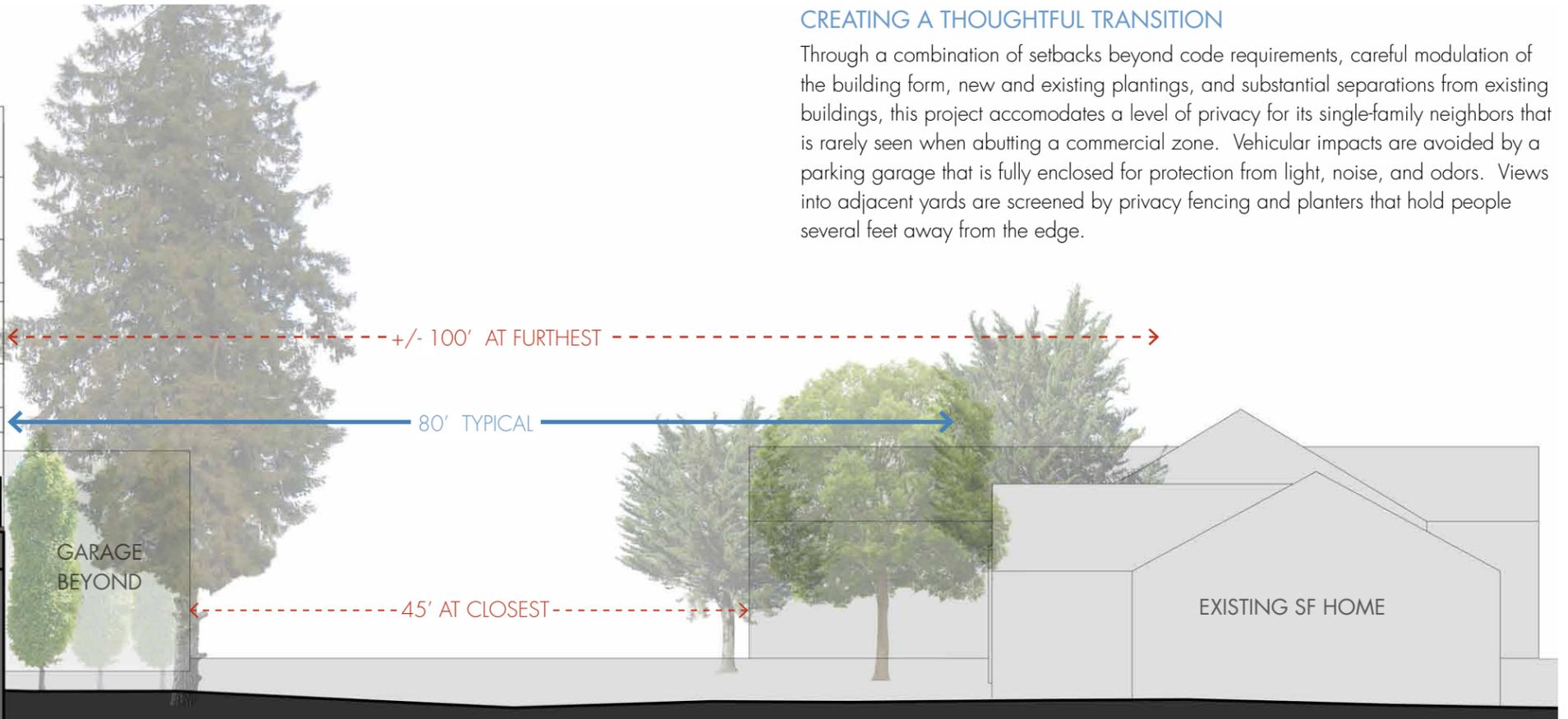


ENTRY COURTYARD SECTION



ENTRY COURTYARD LANDSCAPE

# UNOPENED R.O.W.



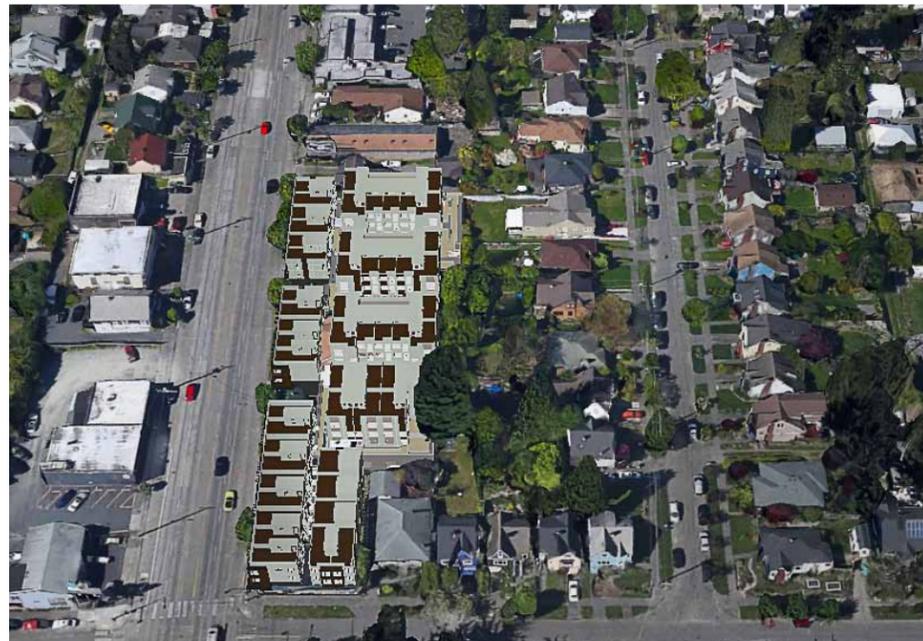
## CREATING A THOUGHTFUL TRANSITION

Through a combination of setbacks beyond code requirements, careful modulation of the building form, new and existing plantings, and substantial separations from existing buildings, this project accommodates a level of privacy for its single-family neighbors that is rarely seen when abutting a commercial zone. Vehicular impacts are avoided by a parking garage that is fully enclosed for protection from light, noise, and odors. Views into adjacent yards are screened by privacy fencing and planters that hold people several feet away from the edge.

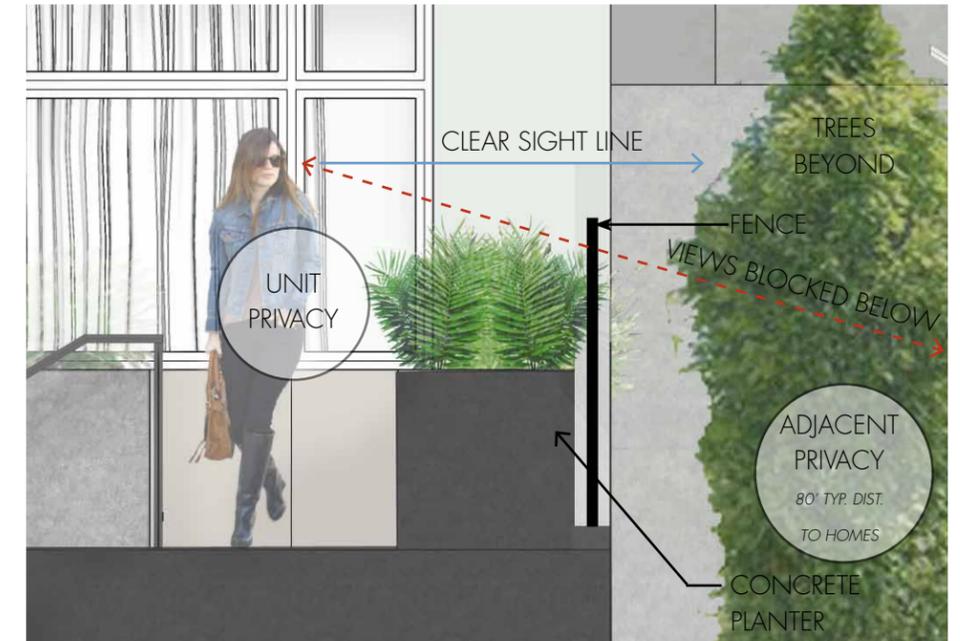
SITE SECTION AT EAST PROPERTY LINE



BIRD'S EYE VIEWS OF EXISTING SF HOMES (NORTHWEST VIEW)



BIRD'S EYE VIEWS OF EXISTING SF HOMES (NORTH VIEW)



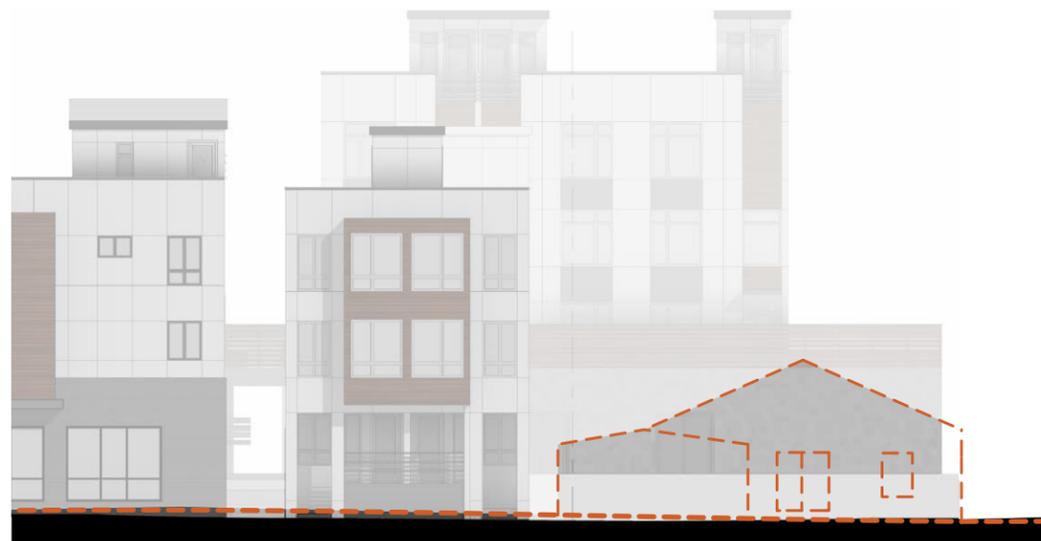
VISIBILITY CONTROL AT SHARED PATIOS/INTERIOR COURTYARDS

# DESIGN PROPOSAL

## GLAZING STUDY



EAST ELEVATION WITH OVERLAY OF ADJACENT FACADES



SOUTH ELEVATION WITH OVERLAY OF ADJACENT FACADES

**COLOR KEY**

- WINDOW OUTLINE
- ADJACENT HOME OUTLINE
- OVERLAY OF PROPOSED AND ADJACENT GLAZING



NORTH ELEVATION WITH OVERLAY OF ADJACENT FACADES  
(ADJACENT BUILDING TO BE DEMOLISHED BY OWNER)

### PROVIDING LIGHT AND VIEWS WHILE RESPECTING PRIVACY

The massing of homes atop the podium has been oriented east/west to minimize the scale of the building, allowing afternoon sun and sky views through the project for the neighbors. Several additional techniques are employed to minimize visual intrusion upon its neighbors to the east: Privacy fencing and a fully enclosed parking garage ensure that direct, eye-level views into neighboring yards are prevented; At Building B, where the neighboring home is closest to the site, windows have been minimized on the east wall and are primarily above eye level to allow light but maintain privacy for all occupants.; Finally, the existing unopened right-of-way along the east property line acts as a 15-foot buffer, Building B is set back an additional 5-feet beyond code requirements, and the developer has offered to provide screening trees on the affected neighbors properties. All these efforts work in combination to establish a respectful relationship to adjacent homes.

15TH AVE NW TOWNHOMES - DPD # 3017926

DESIGN REVIEW RECOMMENDATION

# DESIGN PROPOSAL

## LIGHTING PLAN



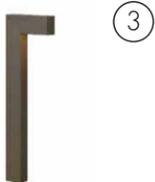
RECESSED LIGHTING



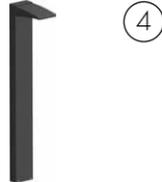
LED UP LIGHT



LED PATH LIGHT



BOLLARD LIGHT FIXTURE



SINGLE LIGHT SCENCE



DOUBLE LIGHT SCENCE



SIGNAGE CONCEPT



EXAMPLE CORNER SIGN



RETAIL SIGN AT CORNER OF 15TH AND 75TH



BUILDING SIGN AT SOUTH WEST CORNER OF BUILDING A1



EXAMPLE BLADE SIGN

# APPENDIX

DEPARTURES 1, 2, & 3  
SHADOW STUDIES

| DEVELOPMENT STANDARD   | REQUIREMENT   | PROPOSED                   | DEPARTURE AMOUNT           | REASON FOR DEPARTURE  | DESIGN REVIEW GUIDELINES  |
|--|---|----------------------------|----------------------------|---|---------------------------|
| #1<br>SMC 23.53.035-B.6<br>Structural Bay Overhang Transparency Requirements | The glass areas of each bay window shall be a minimum of 50 percent of the sum of the areas of all the vertical surfaces of the bay window. At least 60 percent of such required glass area for each bay window shall be on the vertical surface most parallel to the property line | 15th: 40.1%<br>75th: 33.6% | 15th: 19.8%<br>75th: 32.8% | Allowing the SBO glazing to be provided as shown will allow the project to better meet the following guideline:<br>DC2-B1 Facade Composition: 100% of the proposed glazing is located on the facades parallel to the street and sized to maximize the opening, presenting substantial transparency and eyes on the street. The depth of the SBO has been held to 2-feet to provide a more elegant proportioning of the facade, however this is the only place to add glazing and doing so would result in either very narrow slit windows that offer no real additional visibility and intrude on the privacy of neighbors, or extending the SBO's out another foot to allow for glazing while detracting from the preferred massing. | DC2-B1 Facade Composition |



**SBO FACADE COVERAGE DIAGRAMS**

SCALE: 1/16" = 1'-0"



**SOUTH SBO FACADE COVERAGE DIAGRAM**

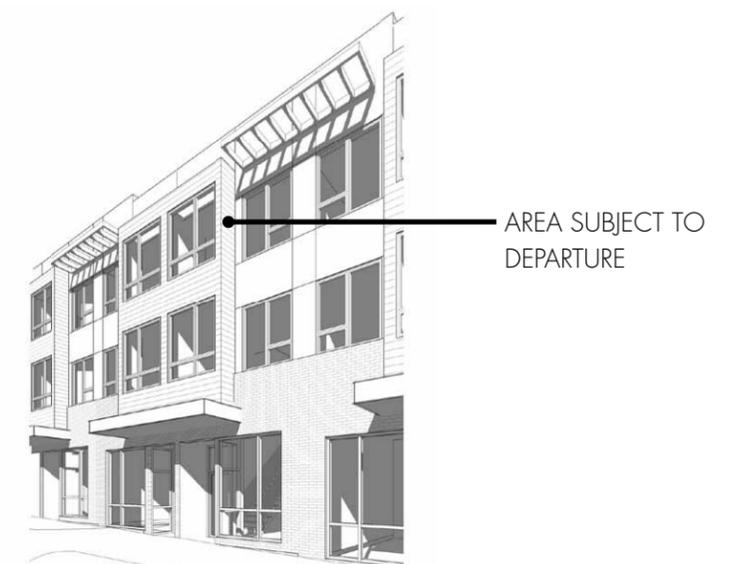
SCALE: 1/16" = 1'-0"

WEST ELEVATION - 15TH AVE NW

|  |                              |          |
|--|------------------------------|----------|
|  | TOTAL AREA (15TH FACADE)     | 10439 SF |
|  | TOTAL SBO AREA (15TH FACADE) | 3172 SF  |
|  | 30.4% SBO                    |          |
|  | TOTAL SBO AREA (ALL FACADES) | 3990 SF  |
|  | TOTAL GLAZING AREA           | 1600 SF  |
|  | 40.1% GLAZING                |          |

SOUTH ELEVATION - NW 75TH ST

|  |                              |         |
|--|------------------------------|---------|
|  | TOTAL AREA (15TH FACADE)     | 2244 SF |
|  | TOTAL SBO AREA (15TH FACADE) | 587 SF  |
|  | 26.2% SBO                    |         |
|  | TOTAL SBO AREA (ALL FACADES) | 715 SF  |
|  | TOTAL GLAZING AREA           | 240 SF  |
|  | 33.6% GLAZING                |         |

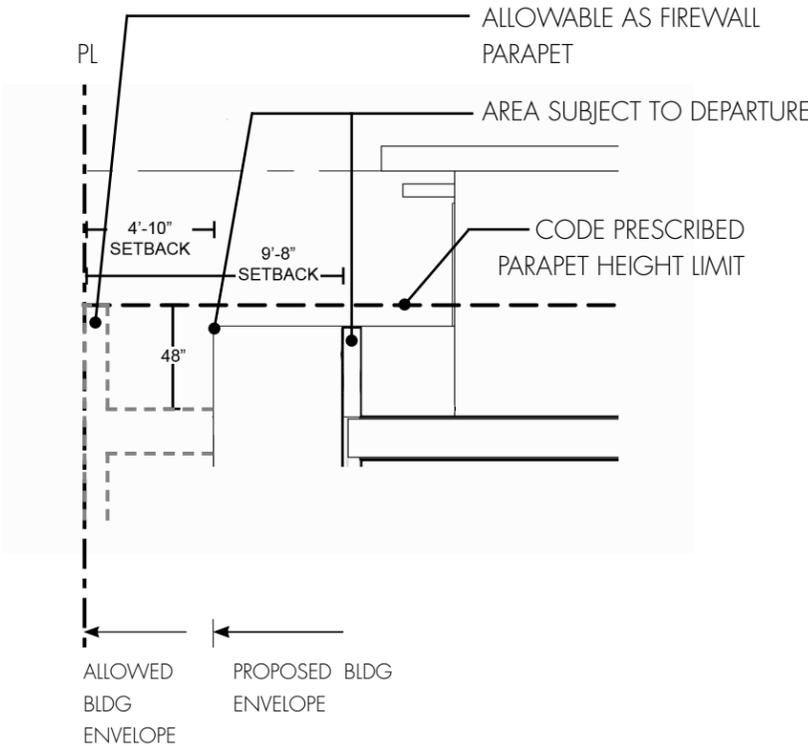
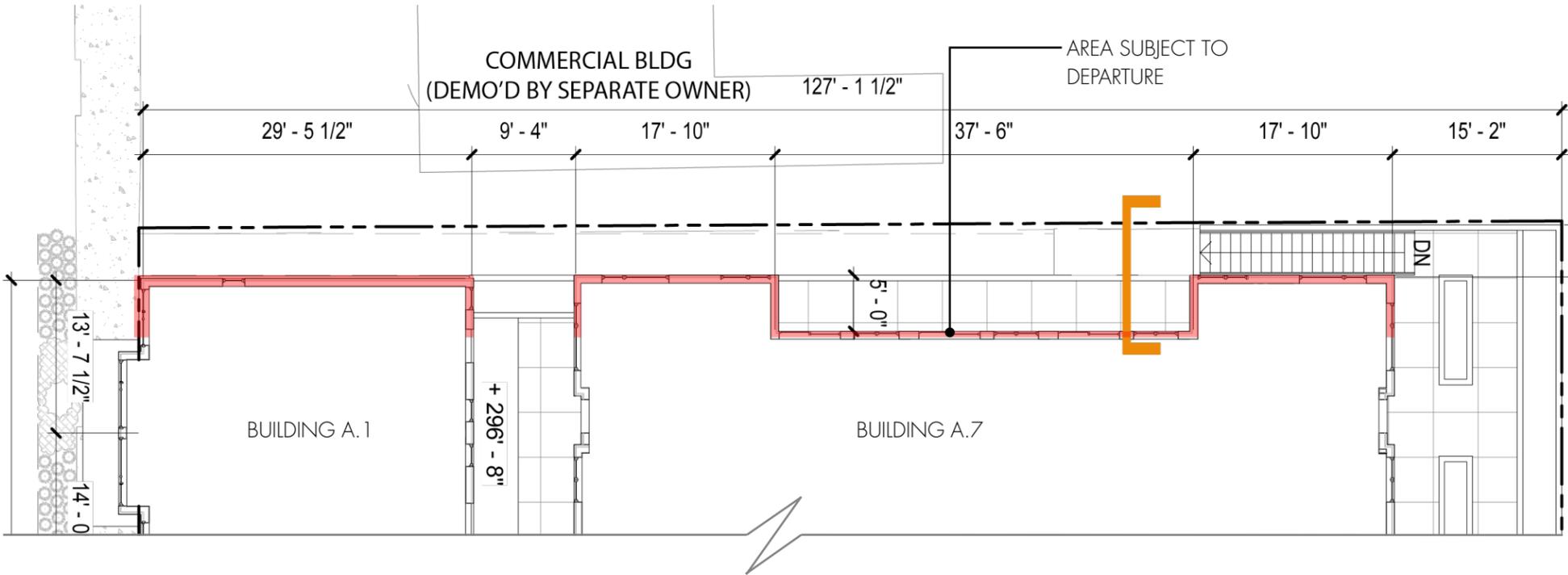


DEPARTURE #1 - STRUCTURAL BAY OVERHANG TRANSPARENCY

# DESIGN PROPOSAL

## DEPARTURE #2

| DEVELOPMENT STANDARD   | REQUIREMENT  | PROPOSED  | DEPARTURE AMOUNT            | REASON FOR DEPARTURE  | DESIGN REVIEW GUIDELINES                                       |
|--|--|---|-----------------------------|---|--|
| #2<br>SMC 23.47A.012-C.7<br>Rooftop Features at North Edge of Roof | Non-firewall parapets shall be located at least 10' from the north edge of the roof unless demonstrated that they will not shade property to the north on January 21st at noon more than would a structure built to maximum permitted height and FAR | Continuous 3-6" parapet located along the roof edge | 3-3" above the height limit | <p>Allowing the parapet for this building to continue uninterrupted will provide for a better waterproofing solution and allow the project to better meet the following guidelines:</p> <p>CS2-d5 Respect for adjacent sites: zoning allows the property to be built to the north property line with a firewall parapet or for a railing to be installed along the entirety of the roof edge, either of which would shadow northern properties far more than the proposed departure which, when combined with the proposed 5' setback will allow for additional open space, light, and air between the structures.</p> <p>DC2-b1 Facade composition: without the departure, the parapet would have a break at an arbitrary point along the top of the east and west walls, creating a visually distracting interruption. In addition, the appearance of building a.7 would vary from buildings a.5 &amp; a.6, taking away from the consistency of the buildings on the top of the podium.</p> | CS2-d5 Respect for adjacent sites<br>DC2-b1 Facade composition |



SECTION

DEPARTURE #2 - ROOFTOP FEATURES AT NORTH EDGE OF ROOF

# DESIGN PROPOSAL

## DEPARTURE #3

| DEVELOPMENT STANDARD                            | REQUIREMENT  | PROPOSED  | DEPARTURE AMOUNT | REASON FOR DEPARTURE  | DESIGN REVIEW GUIDELINES  |
|---|--|---|------------------|---|---|
| #3<br>SMC<br>23.47A.032.A.2.a<br>Parking Access | If access is not provided from an alley and the lot abuts two or more streets, access to parking shall be from a street that is not a principal pedestrian street. | Parking access is provided from the principal pedestrian street | N/A              | <p>Locating the parking access from 15th allows the project to better meet the following Design Review Guidelines:</p> <p>PL1-B-2 Walkways &amp; Connections - Pedestrian Volumes: The sidewalk along 75th is heavily used by parents and children travelling to Whittier Elementary two blocks east, and substantial concern has been voiced about traffic crossing this walkway.</p> <p>DC1-B Vehicle Access &amp; Circulation: The lot depth at the flag extending south to 75th is insufficient to accommodate commercial space, a drive aisle, and parking, which requires the parking be located centrally in the site and reducing this area to circulation only. The curb cut would also be located less than 40-feet from the intersection.</p> <p>DC1-C-2 Parking &amp; Service Uses - Visual Impacts: If the access were taken from 75th as shown in red below, vehicles would be turning and driving along the property line closest to a single family neighbor.</p> | <p>PL1-B-2 Walkways &amp; Connections - Pedestrian Volumes</p> <p>DC1-B Vehicle Access &amp; Circulation</p> <p>DC1-C-2 Parking &amp; Service Uses - Visual Impacts</p> |



DEPARTURE #3 - DRIVEWAY LOCATION 

15TH AVE NW TOWNHOMES - DPD # 3017926

DESIGN REVIEW RECOMMENDATION

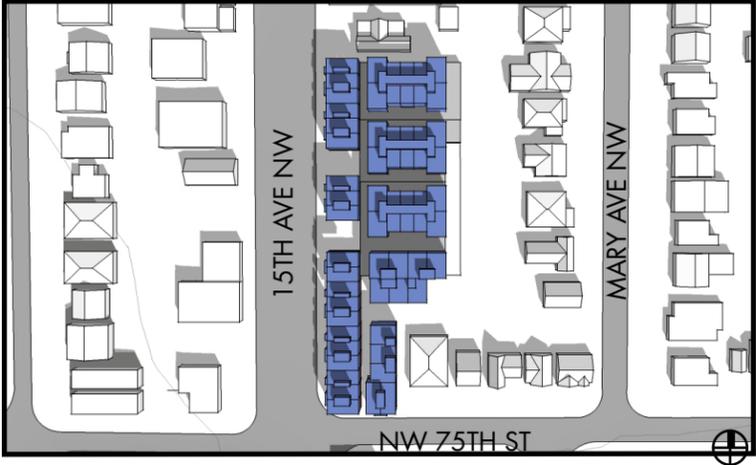
# DESIGN PROPOSAL

## SHADOW STUDIES

MARCH/SEPTEMBER 21 - 10AM



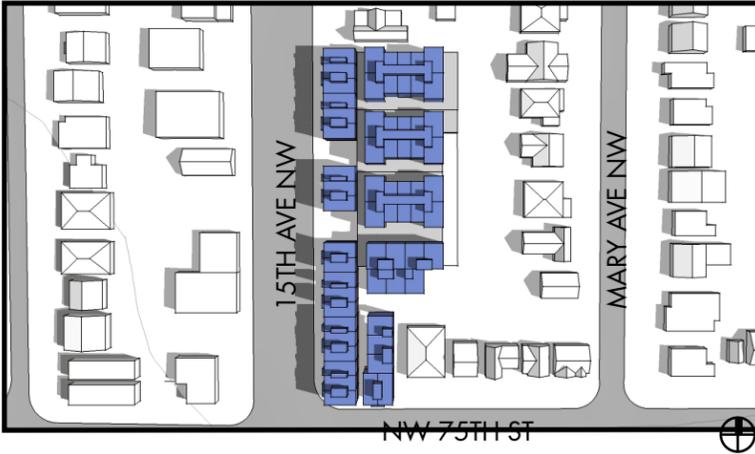
MARCH/SEPTEMBER 21 - 12PM



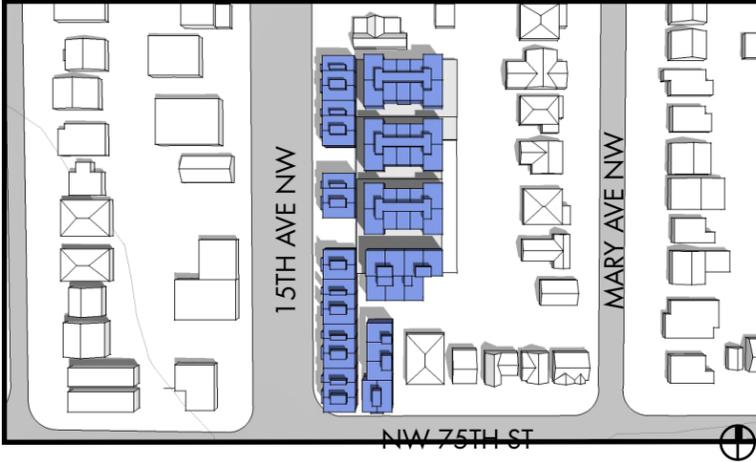
MARCH/SEPTEMBER 21 - 2PM



JUNE 21 - 10AM



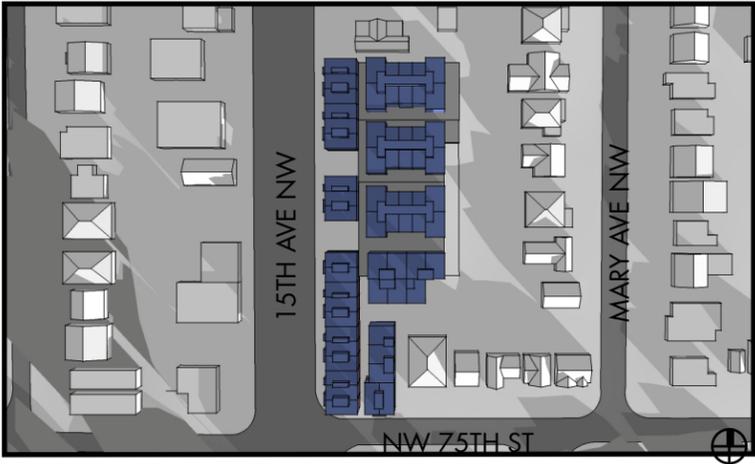
JUNE 21 - 12PM



JUNE 21 - 2PM



DECEMBER 21 - 10AM



DECEMBER 21 - 12PM



DECEMBER 21 - 2PM

