



# 701 VALLEY STREET

MULTI-FAMILY RESIDENTIAL BUILDING









PROJECT INFORMATION

PROPERTY ADDRESS

701 Valley Street, Seattle WA

OWNER

701 VALLEY L.L.C.

DEVELOPER

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ARCHITECT

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LANDSCAPE

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PROJECT DESCRIPTION

The proposed project is a 15-story market-rate multi-family residential building approximately 153-feet in height and 145,000-sf. The building will contain 162 residential units and on-site building management and leasing offices. A resident lobby, fitness area, and dog run are located on the first floor, a private outdoor terrace is located on the second floor. Amenity spaces are located at the roof level, including an indoor/outdoor winter garden with operable glass walls and indoor/outdoor kitchen adjacent to an outdoor terrace. Three levels of underground parking accommodate 56 parking spaces. No commercial space is proposed as part of the project.

The project site is 12,163-sf, in the South Lake Union Urban Center and the SM-SLU 175/85-280 zone. The site is under the seaplane flight path, limiting the allowable height. While a curb-cut exists on Aurora Ave, DPD has required that off-street parking access occur on Valley St.

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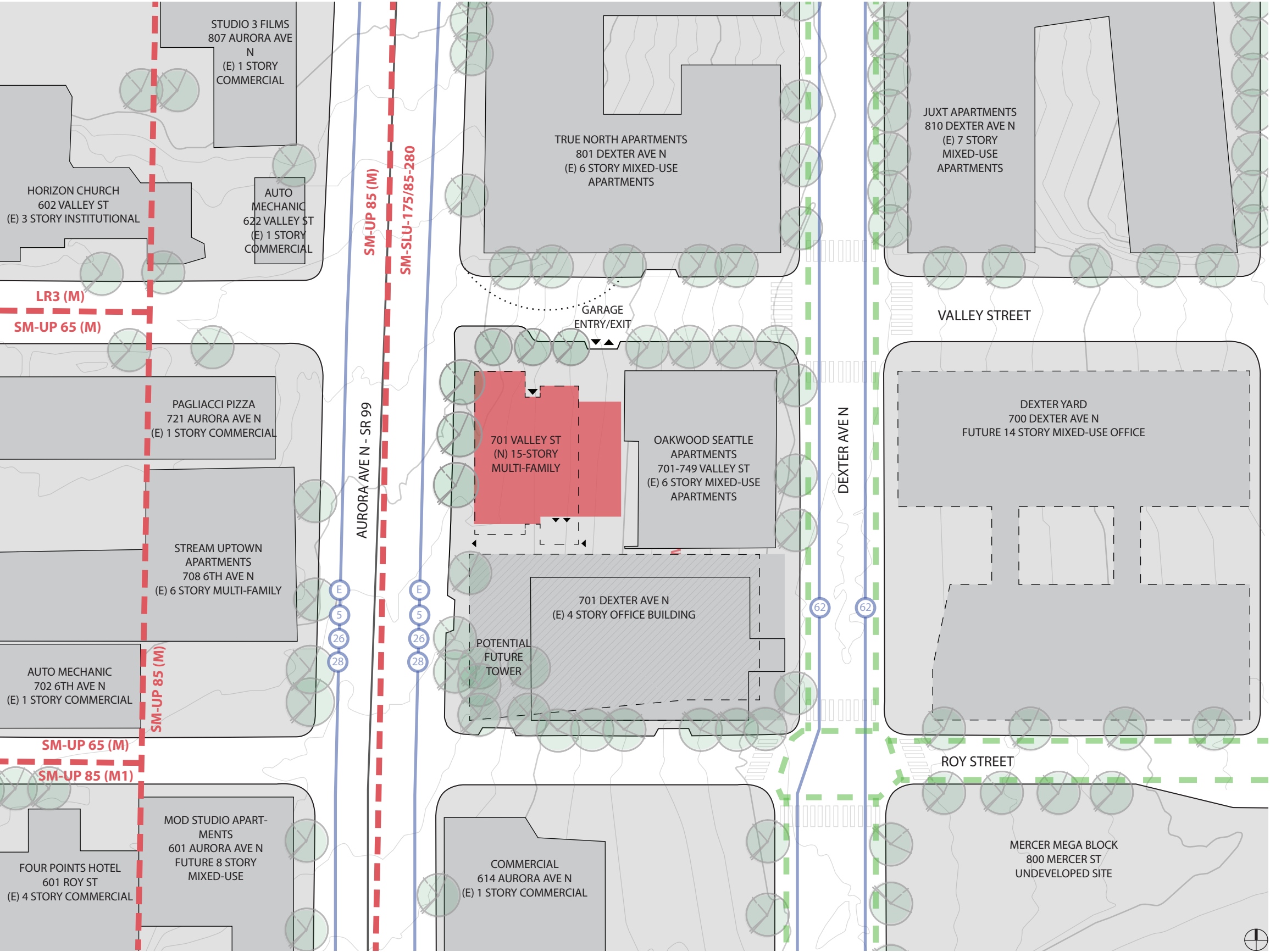


# ZONING ANALYSIS SUMMARY

LAND USE CODE SUMMARY		
PARCEL NO	2249000265	
LOT AREA	12,163-sf (0.28 acres)	
ZONING	SM-SLU 175/85-280, South Lake Union Urban Center, South Lake Union Seaplane Flight Path	
PERMITTED USES	<ul style="list-style-type: none"><li>Residential uses: Permitted Outright</li></ul>	Will comply
STREET LEVEL STANDARDS	<ul style="list-style-type: none"><li>Minimum height for street facing facades: 15'</li><li>Blank facade segments to be ≤30 feet and limited to no more than 70% of the facade width or 78% if street frontage slope exceeds 7.5%</li></ul>	Will comply Valley Street as a 10.5% slope
STRUCTURE HEIGHT	<ul style="list-style-type: none"><li>Maximum structure height as zoned: 250'</li><li>SLU Urban Center Flight Path limits height per map A23.48.010<ul style="list-style-type: none"><li>Rooftop features and mechanical equipment must be under height limit</li></ul></li></ul>	Will comply
FLOOR AREA RATIO	<ul style="list-style-type: none"><li>FAR for all residential uses is exempt</li><li>Underground all residential uses exempt</li><li>Gross floor area:<ul style="list-style-type: none"><li>Upper-level floor area limited to 50% of lot size unless development is prevented from reaching maximum height by Flight Path</li><li>Lots less than 12,500 sq-ft shall have an increase in upper-level floor area from 50% to 67%</li></ul></li><li>Podium:<ul style="list-style-type: none"><li>Podium height is shown on Map A for 23.48.245 and is measured from grade elevation at street lot line<ul style="list-style-type: none"><li>Podium height is 65' per Map A</li></ul></li><li>For podiums of residential uses the average floor area coverage shall not exceed 75% of lot area</li></ul></li></ul>	Will comply
		Will comply
SETBACKS	<ul style="list-style-type: none"><li>No upper level setbacks required</li><li>No facade modulation required for residential uses</li></ul>	
LANDSCAPING STANDARDS	<ul style="list-style-type: none"><li>Green Factor of ≥0.30 required</li></ul>	Will comply
AMENITY	<ul style="list-style-type: none"><li>Residential amenity area equal to 5% of residential gross floor area and meet the following:<ul style="list-style-type: none"><li>All residents have access to the required amenity area</li><li>Amenity areas limited to 50% enclosed</li><li>Common residential amenity areas shall have a dimension of at least 15-ft and a minimum area of 225-sf</li><li>Amenity areas that are provided as landscaping at street level open space shall be counted as twice the actual area</li></ul></li></ul>	Will comply
PARKING	<ul style="list-style-type: none"><li>Parking for residential uses<ul style="list-style-type: none"><li>No parking required within urban centers</li></ul></li><li>Bicycle Parking<ul style="list-style-type: none"><li>Multi-family structures, 1/1 DU long-term, 1/20 DU short-term</li></ul></li></ul>	Will comply



# CONTEXT ANALYSIS



## Neighboring Buildings

- Four-story 701 Dexter Avenue N office building to the south
- Six-story Hue Apartments (717 Dexter Avenue N) located immediately to the east
- Six-story Stream Uptown Apartments (708 6th Avenue N) located across Aurora Avenue N
- Seven-story Juxt Apartments (810 Dexter Ave N) located across the intersection of Dexter Ave and Valley Street to the northeast
- Fourteen-story Dexter Yard mixed-use office building directly across Dexter Ave to the east is currently under construction
- Aurora Avenue N auto-dominated with few buildings addressing the street in a meaningful way

## Street Traffic

- Aurora Avenue N (SR 99) is a regional highway serving as a north-south connection for the Seattle metropolitan area. Bus routes with limited stops use the Aurora corridor (MT #5, #26, #28 & BRT E Line)
- Dexter Avenue N is a minor automobile arterial and principal bicycle route. One bus route (MT #62 serves the corridor
- Valley Street is a one-way street (westbound) serving primarily to access northbound SR 99

## Streetscape

- Pedestrian and bicycle improvements along the Dexter corridor have been made in conjunction with the recent widespread redevelopment of the area. Pedestrian improvements such as new sidewalks, planting beds and street trees have been added to Valley Street adjoining newly constructed buildings
- Frontage along Aurora Avenue N characterized by root bound street trees, heaved sidewalks and curbs, and broken pavement

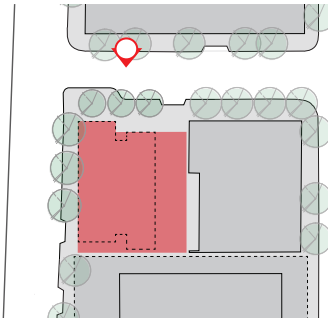
- (N) Building Footprint
- Bus Routes
- Bike Lanes
- Zoning Boundaries
- (N) Building Entry/Exit



# STREET LEVEL RESIDENTIAL ENTRY OFF VALLEY ST

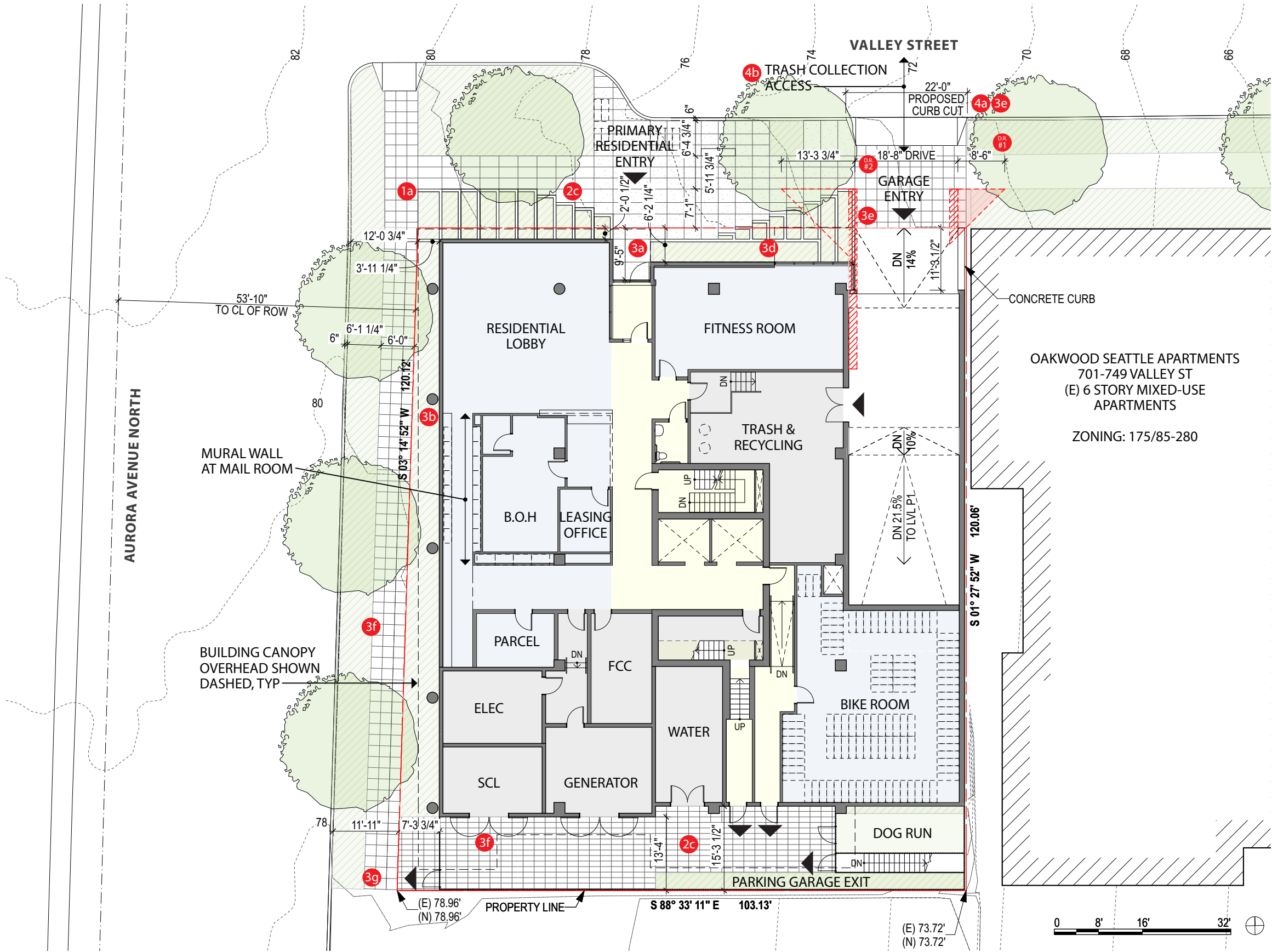
The bullet points below and on the following pages summarize the guidance provided to the design team by the Board at the Early Design Guidance meeting on January 7, 2015. The guidance has been grouped into categories that appear on the right side of the following two-page spreads with a brief description of how the design has been developed in response to the Board's guidance. The red bullets correspond to board priorities and recommendations, and the black bullets correspond to design review guidelines listed as project priorities.

- CS2 B2 Connection to the Street
- CS2 D5 Respect for Adjacent Sites
- PL2 A2 Access Challenges
- PL2 B3 Street-Level Transparency
- PL3 A1 Design Objectives
- PL3 A1c Common Entries to Multi-Story Residential Buildings
- PL3 A4 Ensemble of Elements
- PL4 A1 Serving All Modes of Travel
- DC2 B1 Access Location and Design
- DC2 D2 Texture





STREET LEVEL PLAN



- CS B2 Connection to the Street
- CS D5 Respect for Adjacent Sites
- PL A2 Access Challenges
- PL B3 Street-Level Transparency
- PL A1c Common Entries to Multi-Story Residential Buildings
- PL A4 Ensemble of Elements
- PL A1 Serving All Modes of Travel
- DC B1 Access Location and Design

Massing

1a Tower is positioned at the NW corner of the site

Tower Design

2c Opaque walls are concentrated at the N and S

2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality

Relationship to Street

3a A full height reveal marks the primary entry

3b The lobby level is a continuous horizontal gasket

3c Residential lobby is visible from Dexter Avenue

3d Planters step up the hill and shift towards the entry

3e Curbs, planting, and pavement texture increase car awareness of a pedestrian zone

3f SCL vault is accessed behind the fence, eliminating service doors from the pedestrian experience

3g Access to the rear courtyard is through a secure gate

Access

4a Parking access is from Valley Street

4b Solid waste pickup occurs from parking access off Valley Street

DR #1 Development Standard Departure Request #1, pg 40

DR #2 Development Standard Departure Request #2, pg 41







# ROOF TERRACE AMENITY SPACE



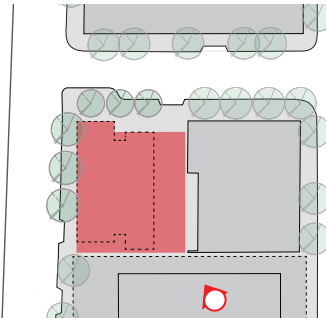
- CS B1 Site Characteristics
- CS D2 Existing Site Feature
- CS D5 Respect for Adjacent Sites
- DC A1 Site Characteristics and Uses
- DC A2 Reducing Perceived Massing
- DC B1 Facade Composition

## Massing

- 1e East-oriented roof terrace with views to Lake Union
- 1f Stepped and shifted slabs respond to topography and the dynamic flight path overlay

## Tower Design

- 2c Opaque walls are concentrated at the N and S
- 2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality
- 2f Massing of the tower creates two slightly unique expressions envisioned as part of a whole





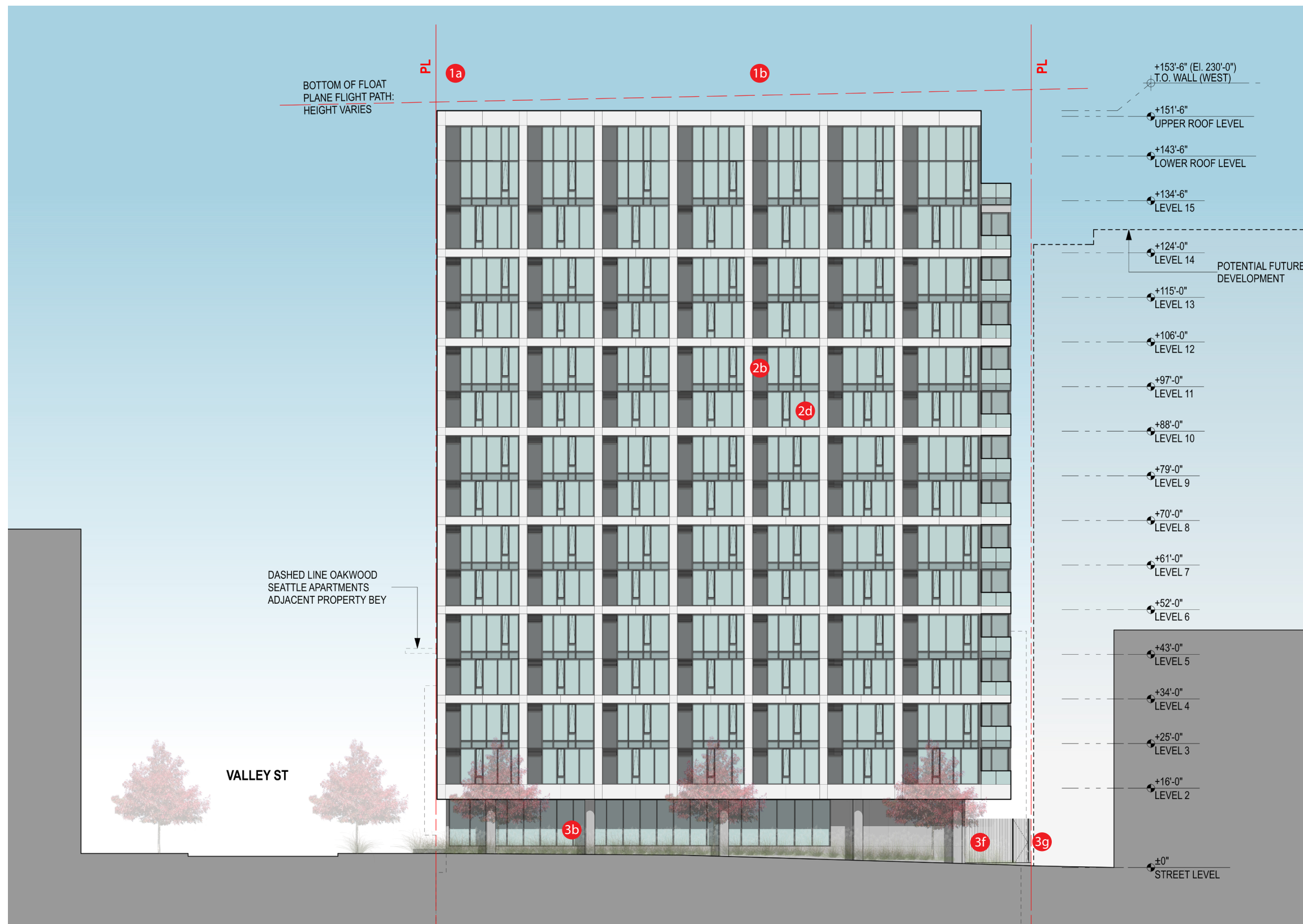
# VIEW FROM AURORA AVE N

- CS B2 Connection to the Street
- CS A4 Evolving Neighborhoods
- PL2 A2 Access Challenges
- PL2 B3 Street Level Transparency
- PL3 A1c Common Entries to Multi-Story Residential Buildings





# WEST ELEVATION (AURORA AVENUE NORTH)



- CS B2 Connection to the Street
- CS A4 Evolving Neighborhoods
- PL A2 Access Challenges
- PL B3 Street Level Transparency
- PL A1c Common Entries to Multi-Story Residential Buildings

## Massing

- 1a Tower is positioned at the NW corner of the site
- 1b The primary building frontage engages Aurora Ave

## Tower Design

- 2b East and west facades are expressed as alternating groups of opaque panels and generous windows
- 2d A cellular grouping of opaque elements creates a unique visual scale

## Relationship to Street

- 3b The lobby level is a continuous horizontal gasket
  - 3f SCL vault is accessed behind the fence, eliminating
  - 3g service doors from the pedestrian experience
- Access to the rear courtyard is through a secure gate

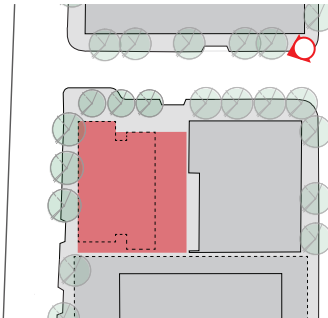
## Materials Key:

- Vision Glass
- Spandrel Glass
- Fiber Cement Panel 1
- Fiber Cement Panel 2 (Not Shown)
- Metal Panel
- Metal Louver
- Aluminum Window Wall System
- Concrete



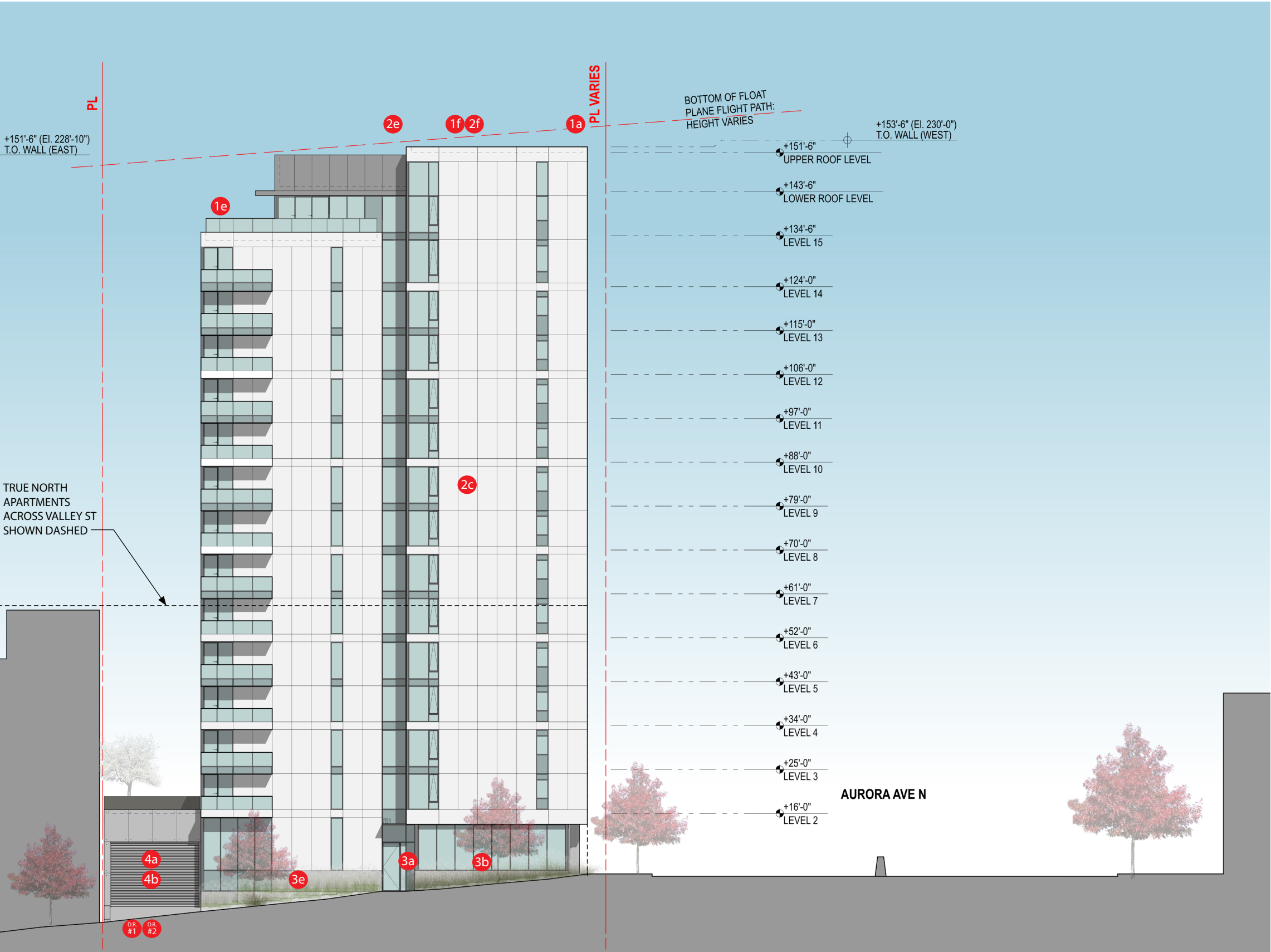
# VIEW FROM VALLEY ST

- CS2 B1 Site Characteristics
- CS2 B2 Connection to the Street
- CS2 D5 Respect for Adjacent Sites
- PL2 A2 Access Challenges
- PL2 B3 Street Level Transparency
- PL3 A1 Design Objectives
- PL3 A4 Ensemble of Elements
- PL4 A1 Serving all Modes of Travel





# NORTH ELEVATION (VALLEY STREET)



- CS B2 Connection to the Street
- CS D5 Respect for Adjacent Sites
- PL A2 Access Challenges
- PL B3 Street Level Transparency
- PL A4 Ensemble of Elements
- PL A1 Serving all Modes of Travel

### Massing

- 1a Tower is positioned at the NW corner of the site
- 1e East-oriented roof terrace with views to Lake Union
- 1f Stepped and shifted slabs respond to topography and the dynamic flight path overlay

### Tower Design

- 2c Opaque walls are concentrated at the N and S
- 2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality

### Relationship to Street

- 3a A full height reveal marks the primary entry  
Residential lobby is visible from Dexter Avenue
- 3b The lobby level is a continuous horizontal gasket
- 3e Curbs, planting, and pavement texture increase car awareness of a pedestrian zone

### Access

- 4a Parking access is from Valley Street
- 4b Solid waste pickup occurs from parking access off of Valley Street
- DR #2 Development Standard Departure Request #2, pg 41

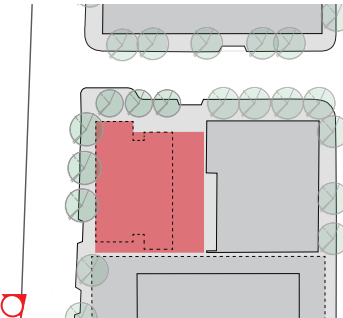
### Materials Key:

- Vision Glass
- Spandrel Glass
- Fiber Cement Panel 1
- Fiber Cement Panel 2
- Metal Panel (Not Shown)
- Metal Louver (Not Shown)
- Aluminum Window Wall System
- Concrete



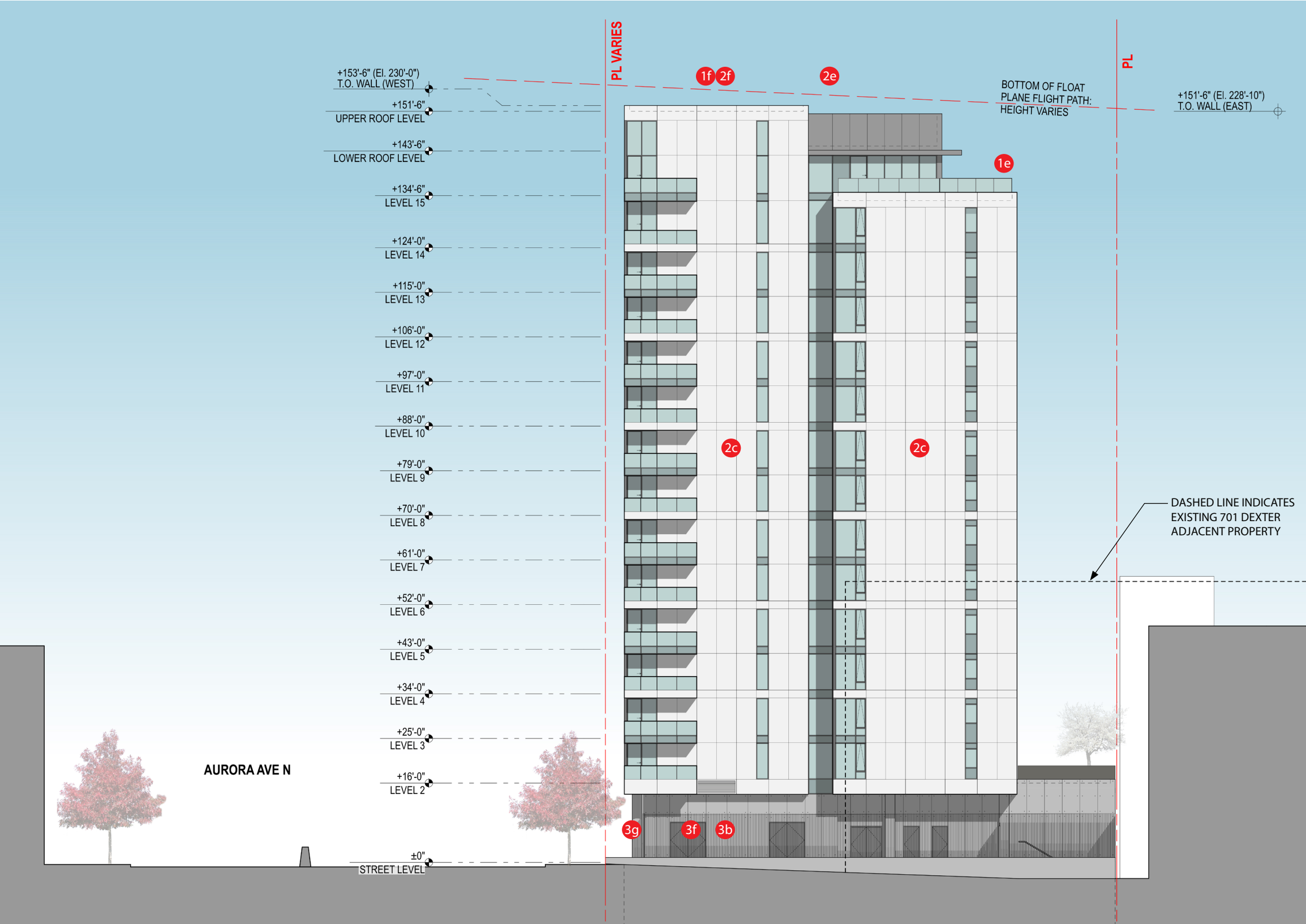
# VIEW FROM AURORA AVE N

- CS1 B1 Site Characteristics
- CS2 B2 Connection to the Street
- CS3 D5 Respect for Adjacent Sites
- PL2 A2 Access Challenges
- PL2 B3 Street Level Transparency
- PL3 A1 Design Objectives
- PL3 A4 Ensemble of Elements
- PL4 A1 Serving all Modes of Travel





# SOUTH ELEVATION (PROPERTY LINE)



- CS A2 Architectural Presence
- CS B1 Site Characteristics
- CS D5 Respect for Adjacent Sites
- PL A1 Serving all Modes of Travel
- PC A1 Site Characteristics and Uses
- BC B1 Facade Characteristics
- BC B2 Blank Walls

## Massing

- 1e East-oriented roof terrace with views to Lake Union
- 1f Stepped and shifted slabs respond to topography and the dynamic flight path overlay

## Tower Design

- 2c Opaque walls are concentrated at the N and S
- 2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality
- 2f Massing of the tower creates two slightly unique expressions envisioned as part of a whole

## Relationship to Street

- 3b The lobby level is a continuous horizontal gasket
- 3f SCL vault is accessed behind the fence, eliminating service doors from the pedestrian experience
- 3g Access to the rear courtyard is through a secure gate

## Materials Key:

- Vision Glass
- Spandrel Glass
- Fiber Cement Panel 1
- Fiber Cement Panel 2
- Metal Panel (Not Shown)
- Metal Louver (Not Shown)
- Aluminum Window Wall System
- Concrete

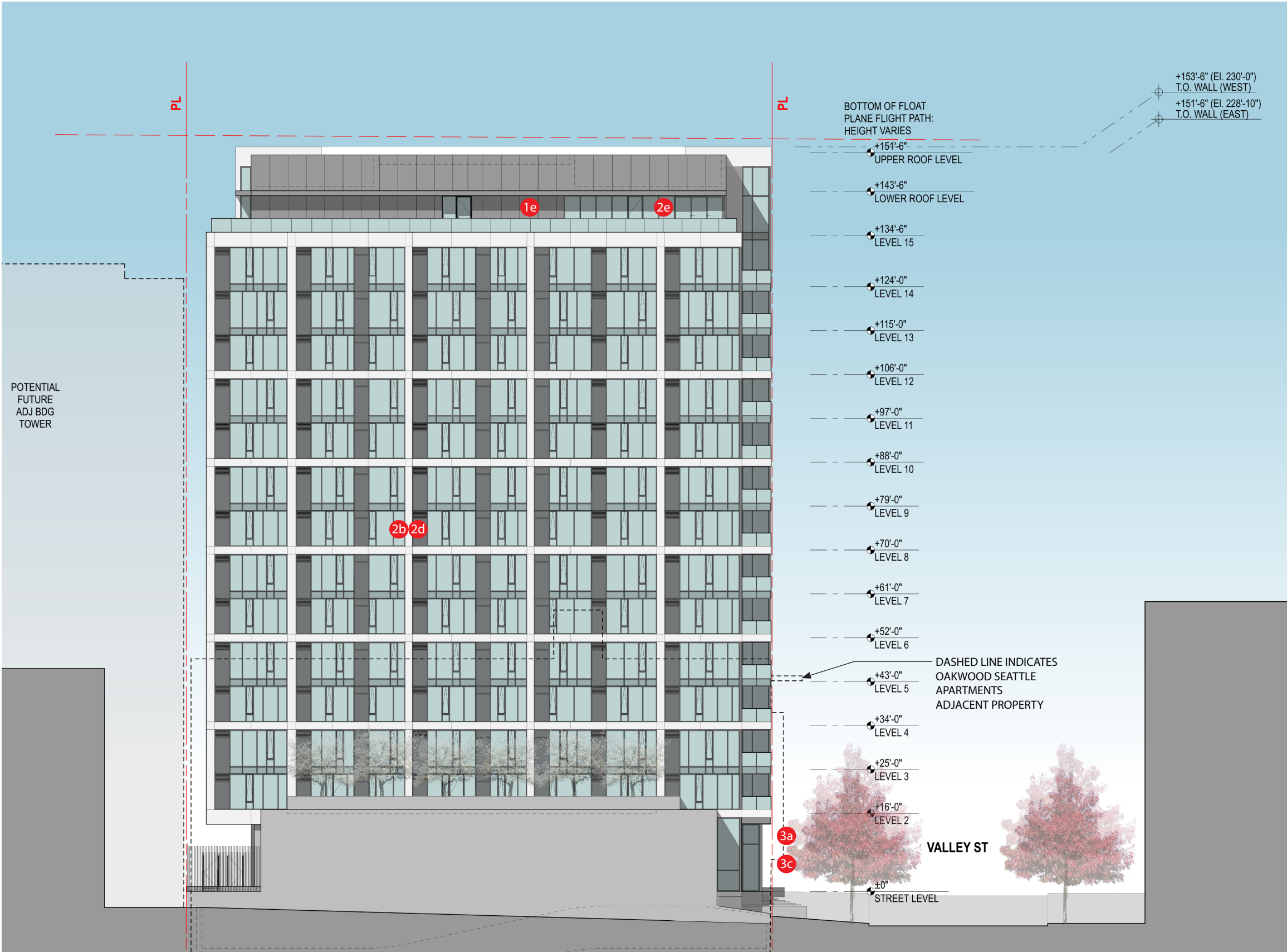


AERIAL VIEW FROM SOUTHEAST





# EAST ELEVATION (PROPERTY LINE)



- CS BT Site Characteristics
- CS B2 Connection to the Street
- PL A1 Serving all Modes of Travel
- PC A1 Site Characteristics and Uses
- PC A2 Reducing Perceived Mass
- PC B1 Facade Composition
- PC D1 Human Scale
- PC D2 Texture

## Massing

- 1e East-oriented roof terrace with views to Lake Union

## Tower Design

- 2b East and west facades are expressed as alternating groups of opaque panels and generous windows
- 2d A cellular grouping of opaque elements creates a unique visual scale
- 2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality

## Relationship to Street

- 3a A full height reveal marks the primary entry
- 3c Residential lobby is visible from Dexter Avenue

## Materials Key:

- Vision Glass
- Spandrel Glass
- Fiber Cement Panel 1
- Fiber Cement Panel 2
- Metal Panel
- Metal Louver
- Aluminum Window Wall System
- Concrete



STREET VIEW FROM INTERSECTION OF VALLEY ST & AURORA AVE N









# LEVEL 2 TERRACE & TYPICAL FLOOR PLANS

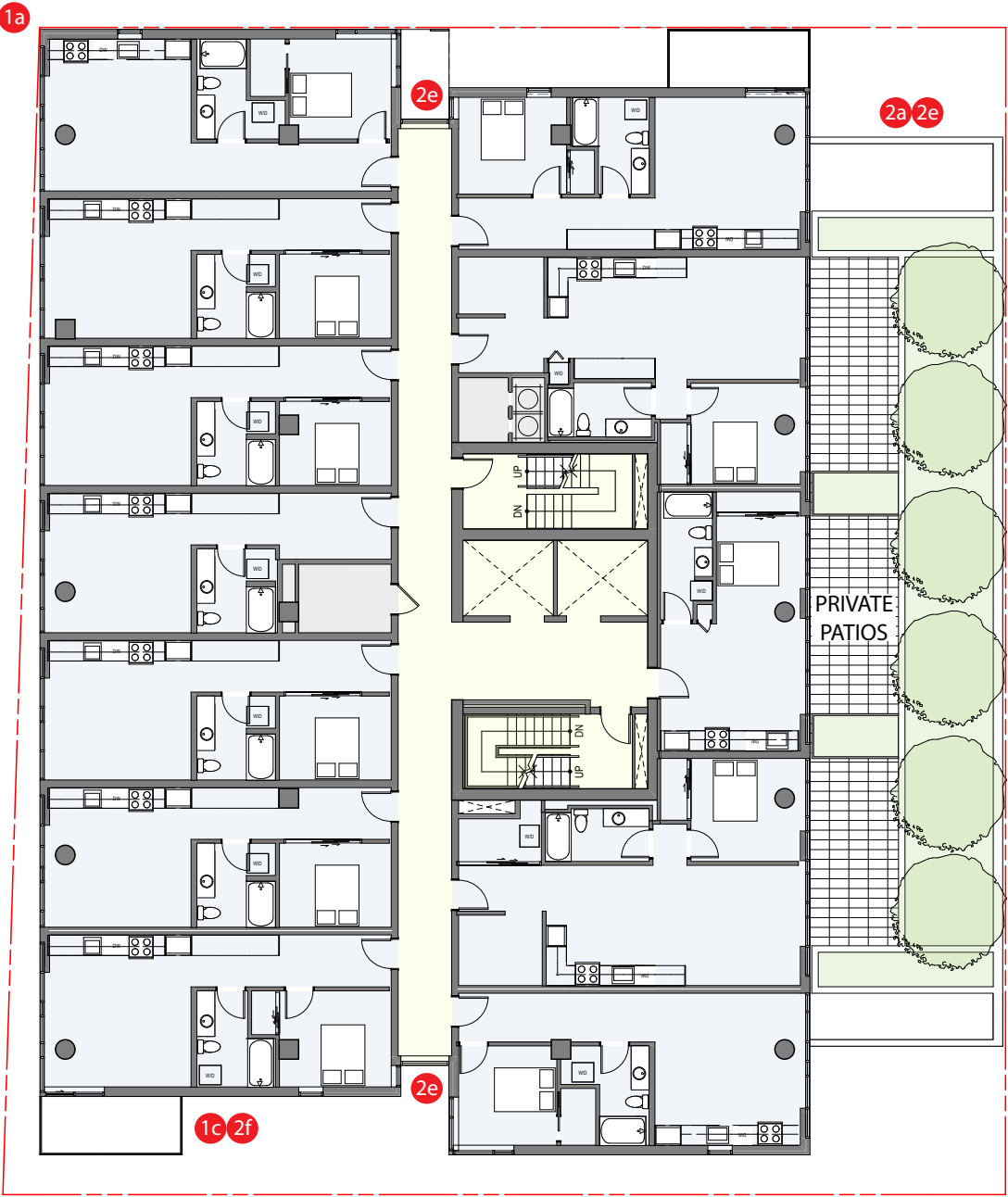
- CS A2 Architectural Presence
- CS B1 Site Characteristics
- CS D5 Respect for Adjacent Sites
- DC A1 Site Characteristics and Uses
- DC A2 Reducing Perceived Massing

### Massing

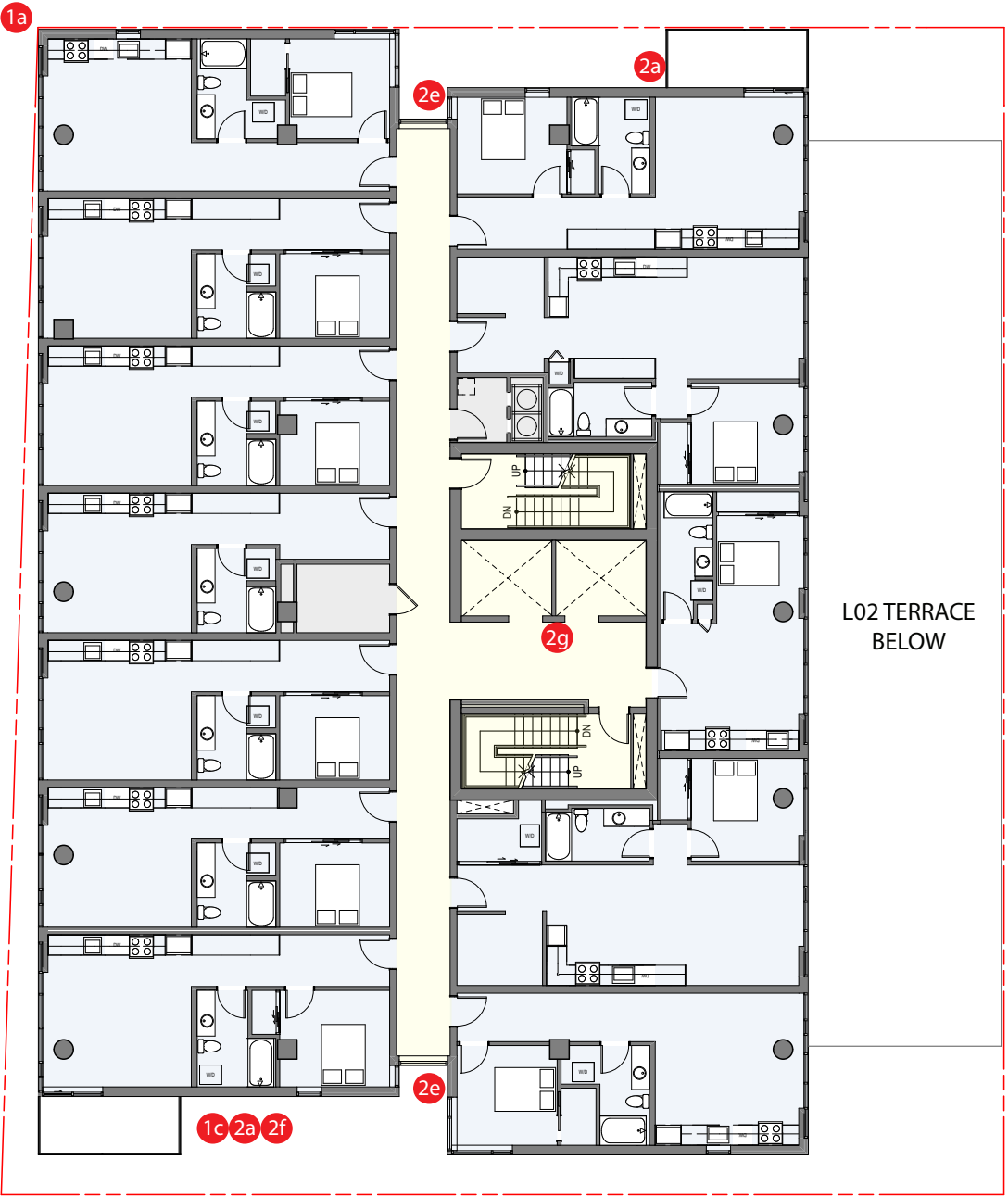
- 1a Tower is positioned at the NW corner of the site
- 1c A reduced footprint creates a uniform, slender tower

### Tower Design

- 2a Concept expressed through masses that shift in plan and step in elevation
- 2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality
- 2f Massing of the tower creates two slightly unique expressions envisioned as part of a whole



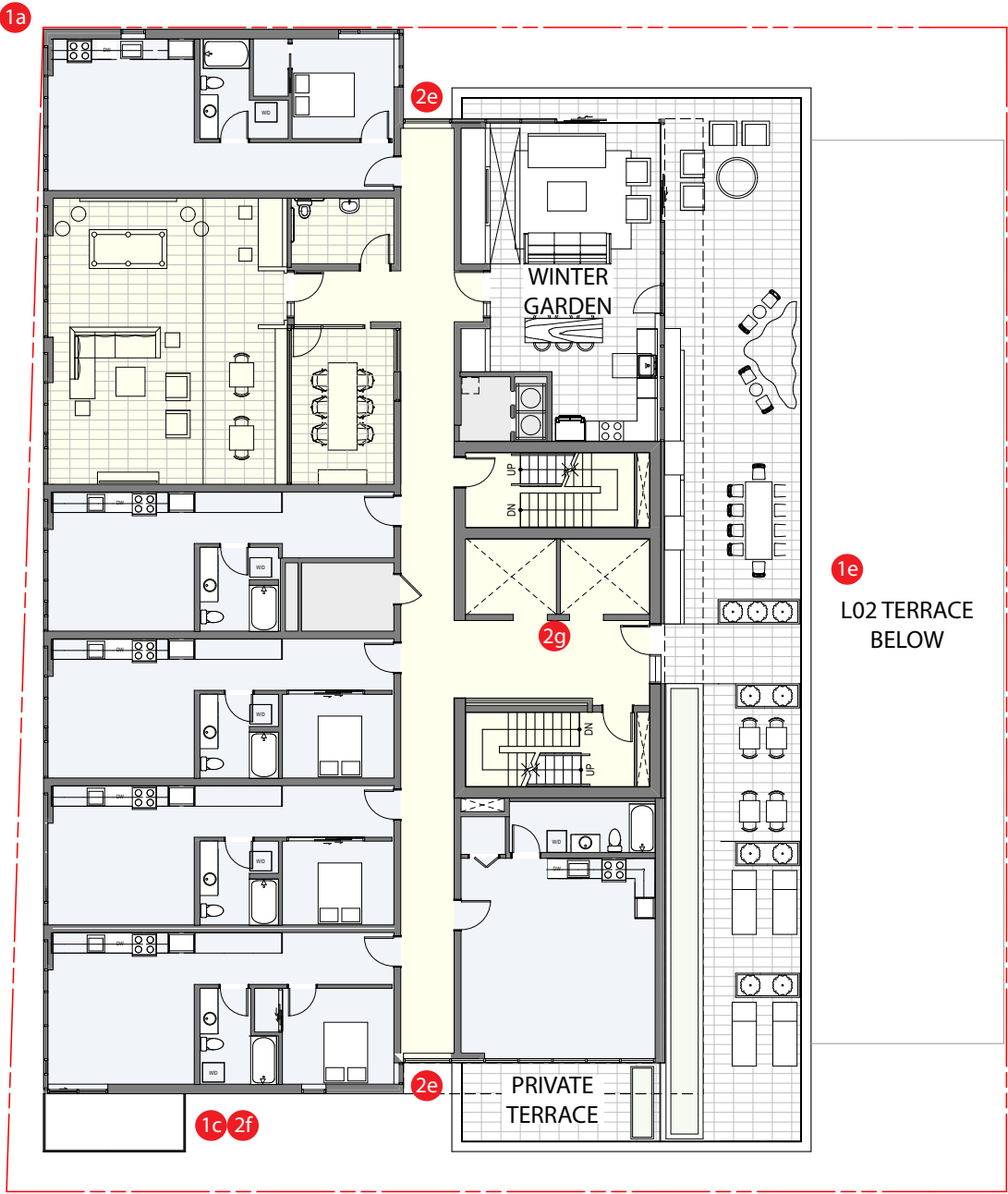
Second Floor & Terrace Plan



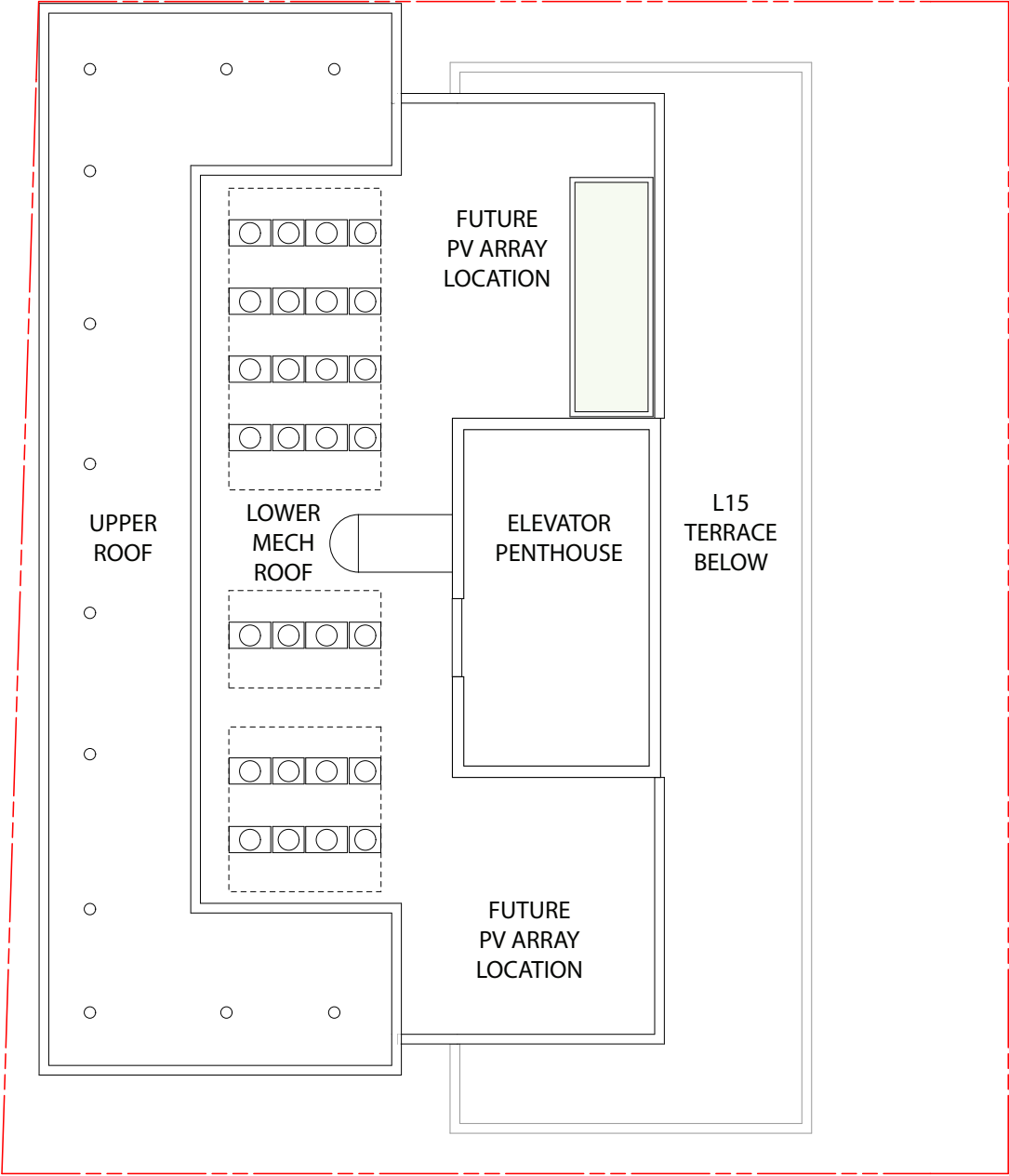
Typical Floor Plan



# LEVEL 15 TERRACE & ROOF PLANS



Level 15 Floor & Terrace Plan



Roof Plan

- CS A2 Architectural Presence
- CS B1 Site Characteristics
- CS D5 Respect for Adjacent Sites
- DC A1 Site Characteristics and Uses
- DC A2 Reducing Perceived Massing

Massing

- 1a Tower is positioned at the NW corner of the site
- 1c A reduced footprint creates a uniform, slender tower
- 1e East-oriented roof terrace with views to Lake Union

Tower Design

- 2a Concept expressed through masses that shift in plan and step in elevation
- 2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality
- 2f Massing of the tower creates two slightly unique expressions envisioned as part of a whole
- 2g An asymmetrical building core creates different unit layouts on each half



## BUILDING SECTION LOOKING NORTH

- CS2 A2 Architectural Presence
- CS2 B1 Site Characteristics
- CS2 D5 Respect for Adjacent Sites
- DC2 A1 Site Characteristics and Uses
- DC2 A2 Reducing Perceived Massing

## Massing

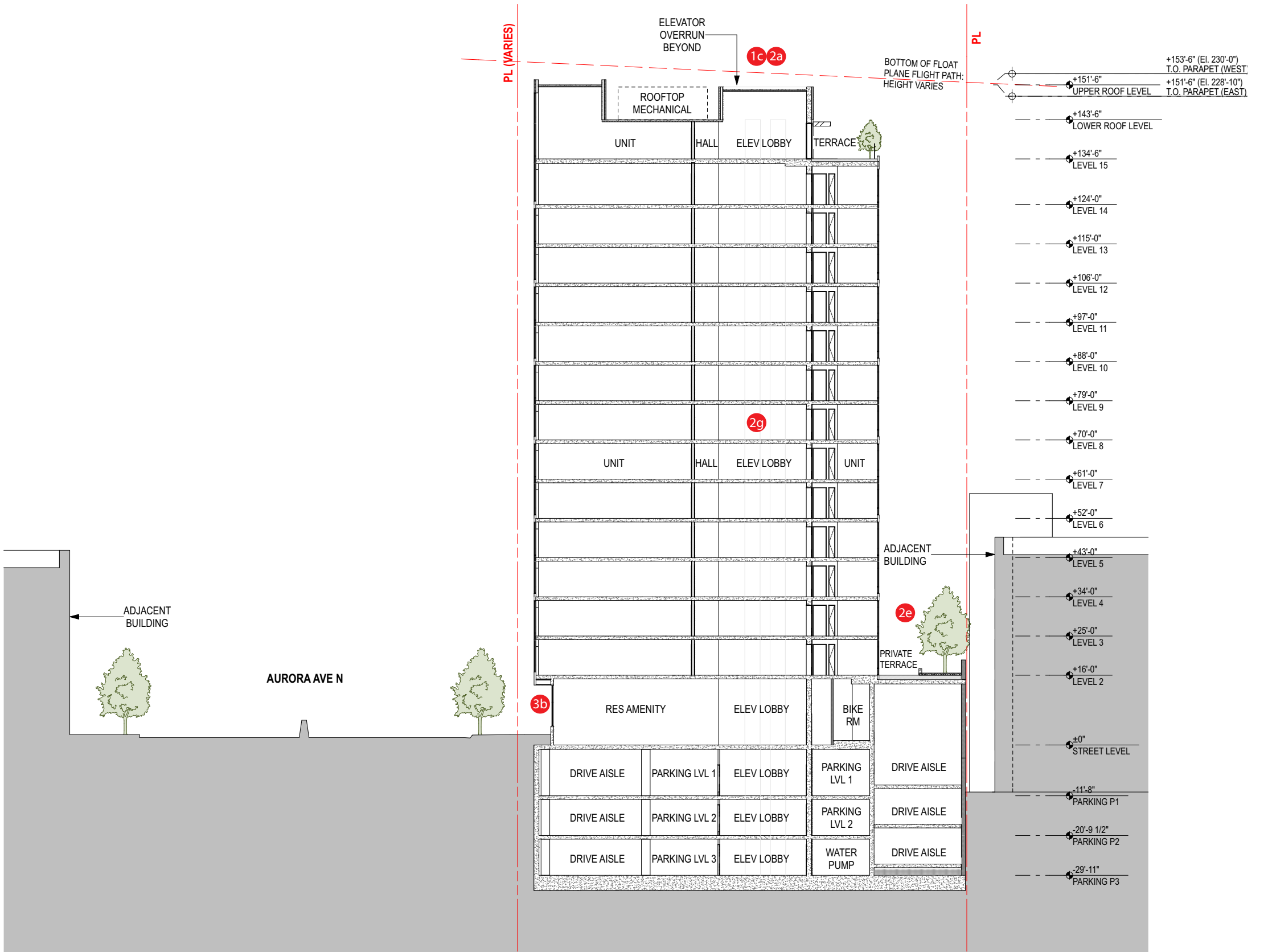
- 1c** A reduced footprint creates a uniform, slender tower

## Tower Design

- 2a Concept expressed through masses that shift in plan and step in elevation
- 2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality
- 2g An asymmetrical building core creates different unit layouts on each half

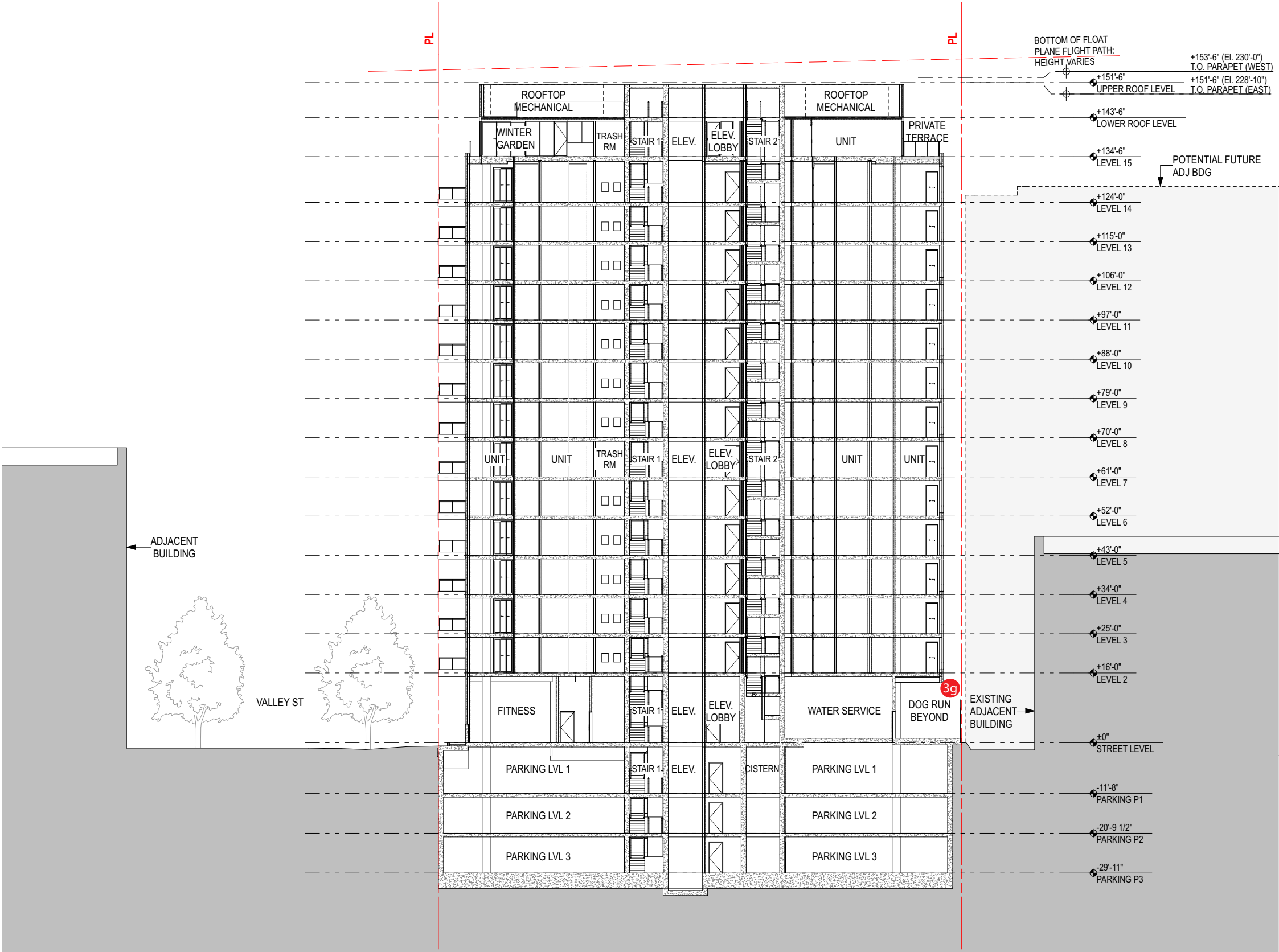
### Relationship to Street

- 3b** The lobby level is a continuous horizontal gasket





# BUILDING SECTION LOOKING EAST



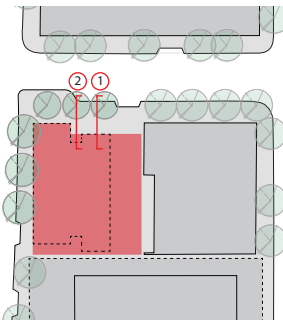
- PLB A2 Access Challenges
- PLB A1 Serving All Modes of Travel
- PLB A2 Connections to All Modes
- DCI A1 Visibility
- DCI B1 Access Location and Design

## Relationship to Street

- 3g Access to the rear courtyard is through a secure gate



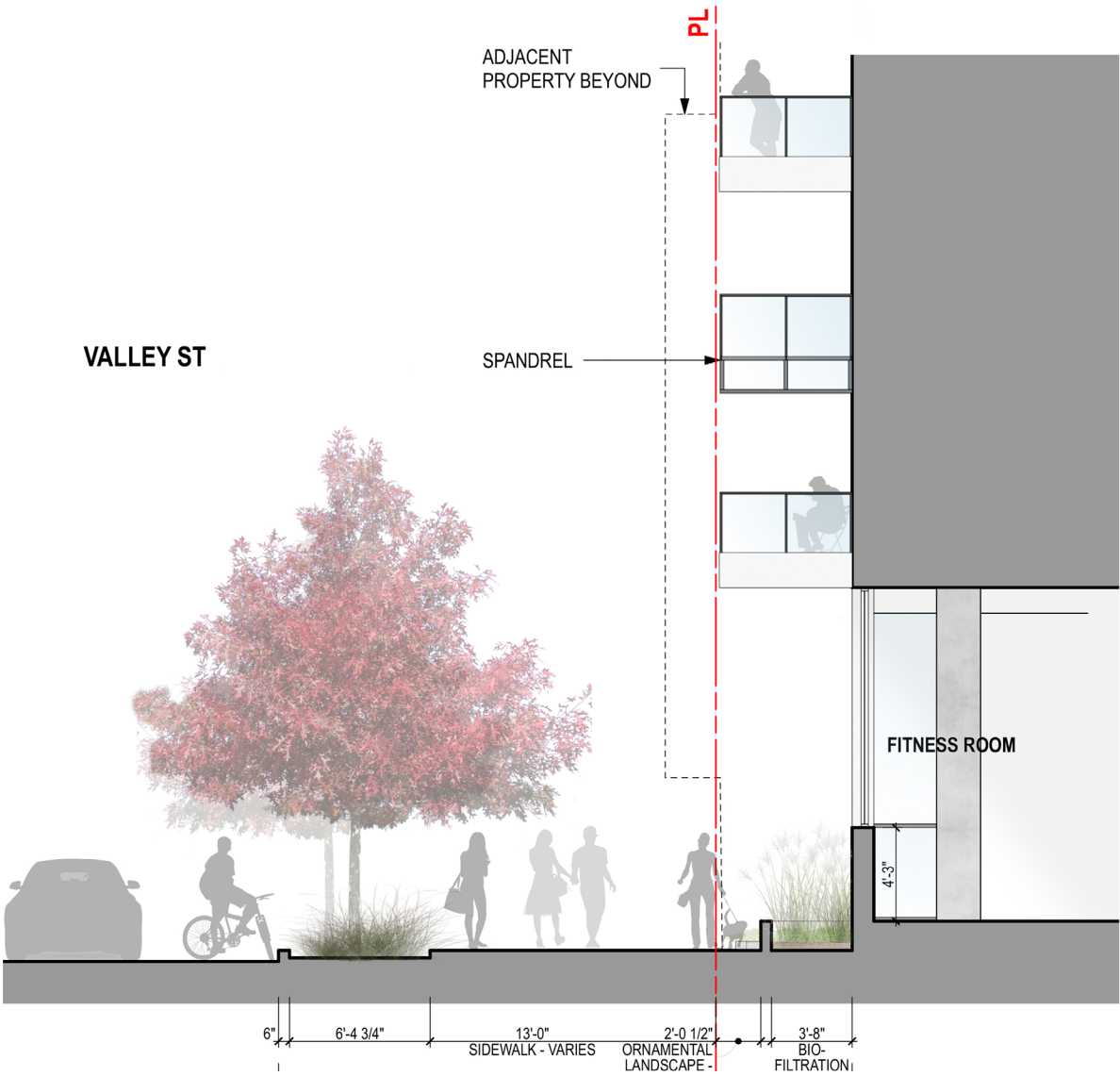
# ENLARGED STREET LEVEL ELEVATIONS AND SECTIONS



- Materials Key:**
- Vision Glass
  - Spandrel Glass
  - Fiber Cement Panel 1
  - Fiber Cement Panel 2
  - Metal Panel
  - Metal Louver
  - Aluminum Window Wall System
  - Concrete



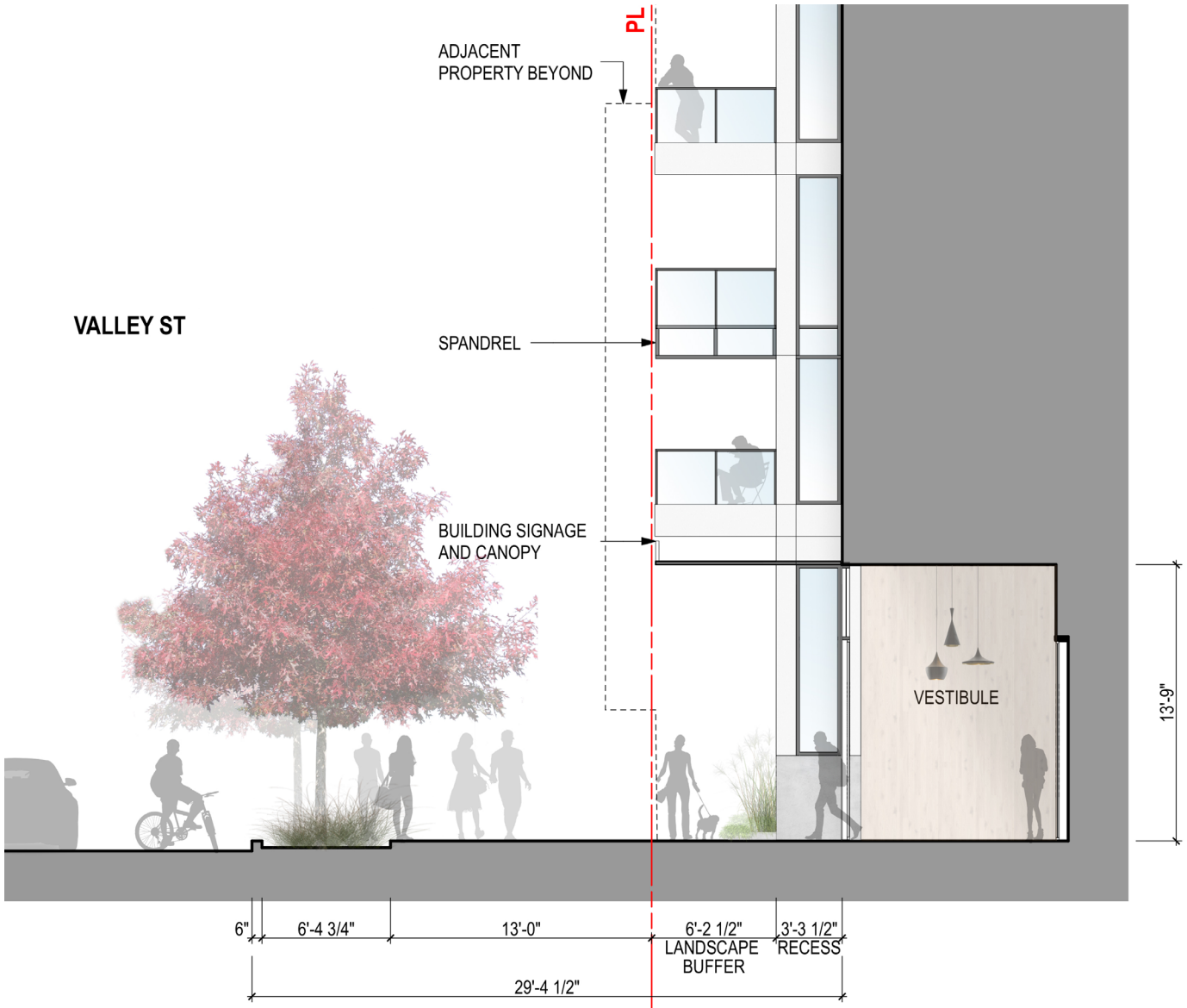
# ENLARGED STREET LEVEL ELEVATIONS AND SECTIONS



Street Level Section at Fitness Room

## 1 Fitness Room Glazing

The concrete base at northeast corner has been raised to be continuous throughout the facade. Fiber cement panels at northeast corner has been redesigned to terminate at the top of the concrete base.



Street Level Section at Residential Entry

## 2 Main Entry off Valley St

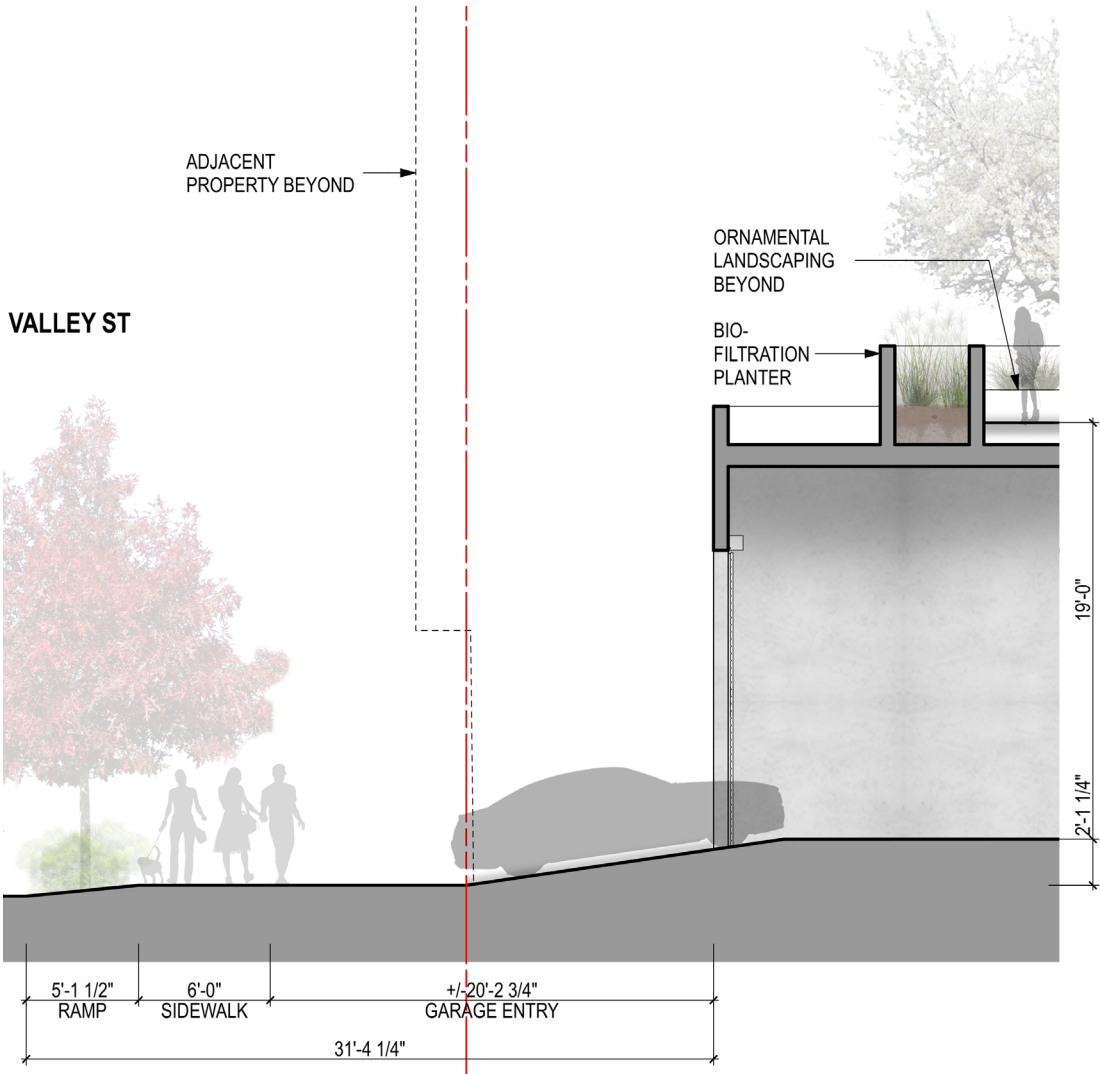
The primary pedestrian entry is located on Valley Street, clearly marked by the building's massing. Given that most pedestrians arriving to the side will be coming uphill from Dexter Avenue to the east, this lobby entrance is both convenient and visible. The stepped landscaped planters also serve to guide the pedestrians to the entrance.



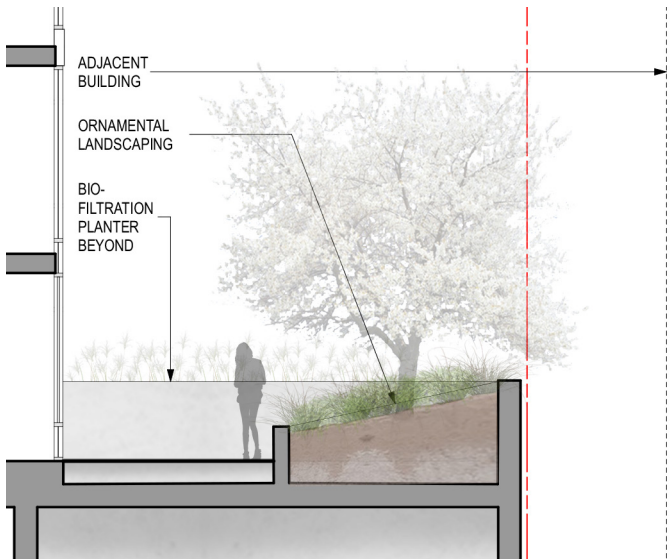
# ENLARGED STREET LEVEL ELEVATIONS AND SECTIONS



Elevation at Garage Entry



Street Level Section at Garage Entry



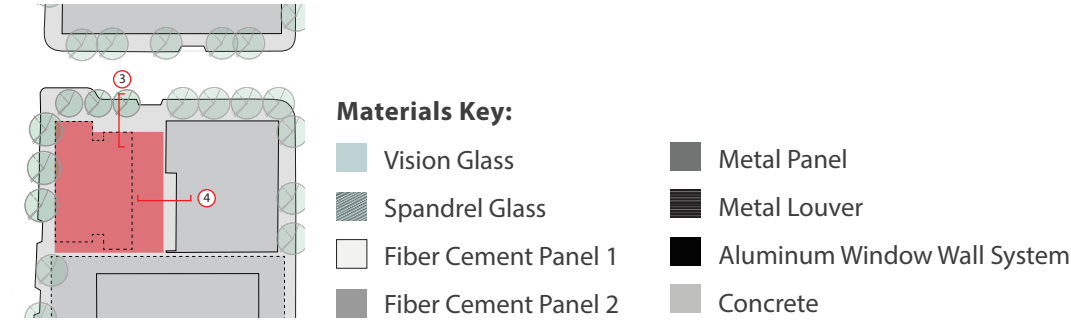
Terrace Level Section at Garage Entry

## 3 Garage Entry off Valley St

The main pedestrian entry is located on Valley street to provide a safe entry. The entry to the parking garage is also located on Valley St, but the curbs, planting, and pavement texture are designed to enhance the pedestrian experience, and increase the awareness that cars are approaching a pedestrian trafficked area.

## 4 Private terraces adjacent to neighboring building

The roof surface of the parking garage access is a landscaped private terrace for these residential units. The bioretention and ornamental planting areas provide privacy from the adjacent residential building, and enhance the visual character of the structure from the neighboring buildings.

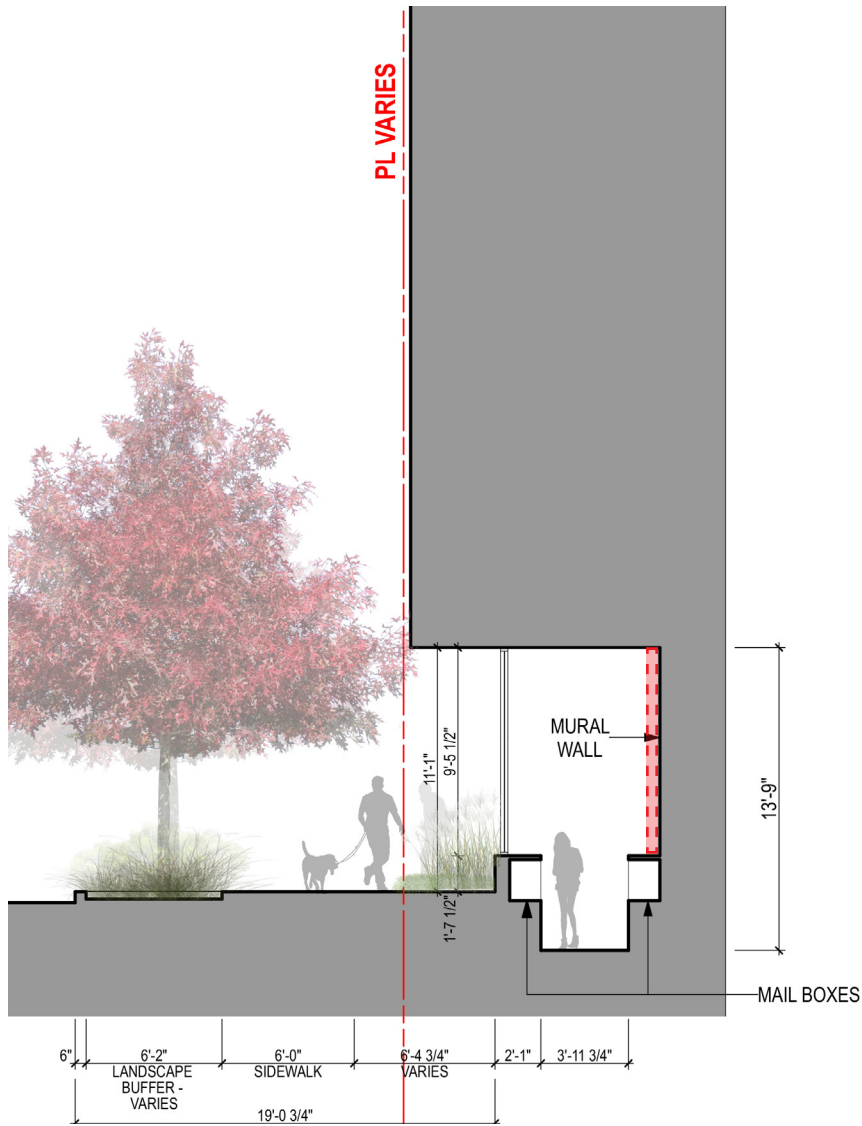




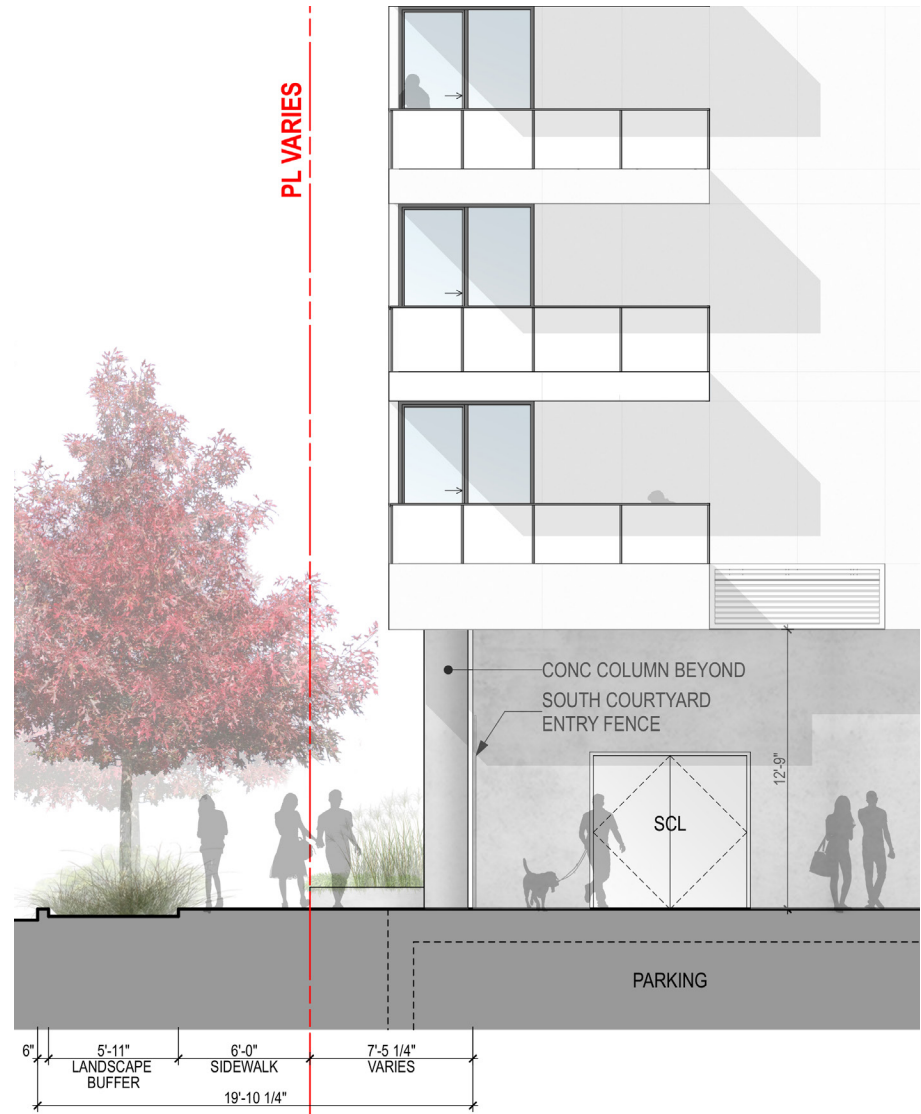
# ENLARGED STREET LEVEL ELEVATIONS AND SECTIONS



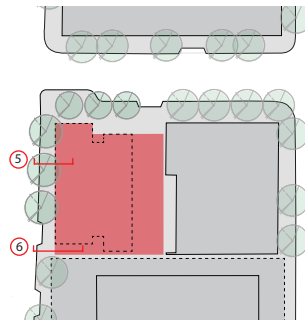
Elevation at Residential Lobby



Street Level Section at Residential Lobby



Section - Elevation at South Courtyard off Aurora



## Materials Key:

Vision Glass	Metal Panel
Spandrel Glass	Metal Louver
Fiber Cement Panel 1	Aluminum Window Wall System
Fiber Cement Panel 2	Concrete

## 5 Residential Lobby Along Aurora

As a welcoming gesture to pedestrians, the sidewalk planters step up the steep hill while shifting back towards the building and its entrance. The western residential slab has been shifted up on columns, revealing the common areas of the lobby level as a continuous horizontal gasket open to the public, pedestrian realm.

## 6 South Courtyard off Aurora Ave N

The site contains a secondary pedestrian access gate at the southwest corner of the site along Aurora. This gate connects to the rear courtyard, which provides access to the bicycle room, dog run and garage stairs. Resident bicyclists or dog walkers arriving from the south may elect to come up Roy Street, which has a more gradual grade than Valley Street.

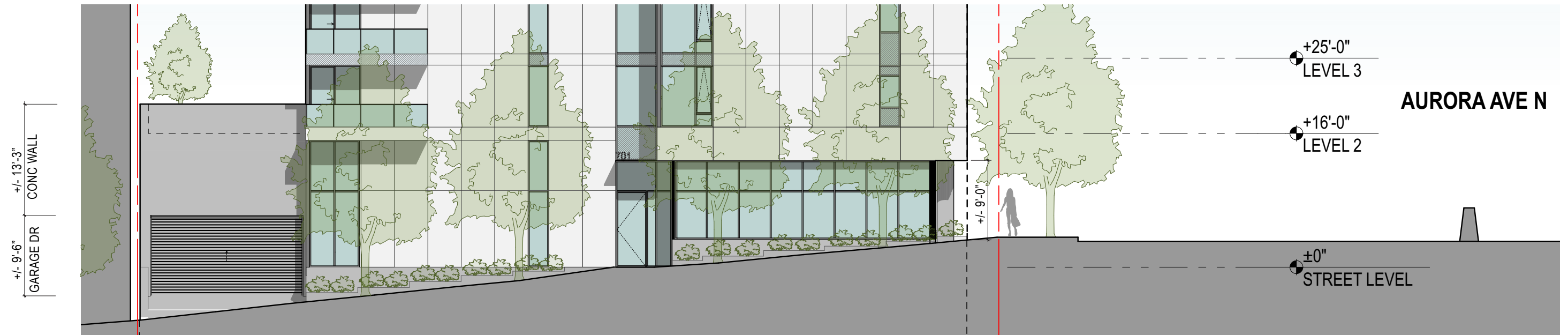


# RESPONSES TO STREETScape GUIDANCE: AURORA AVE

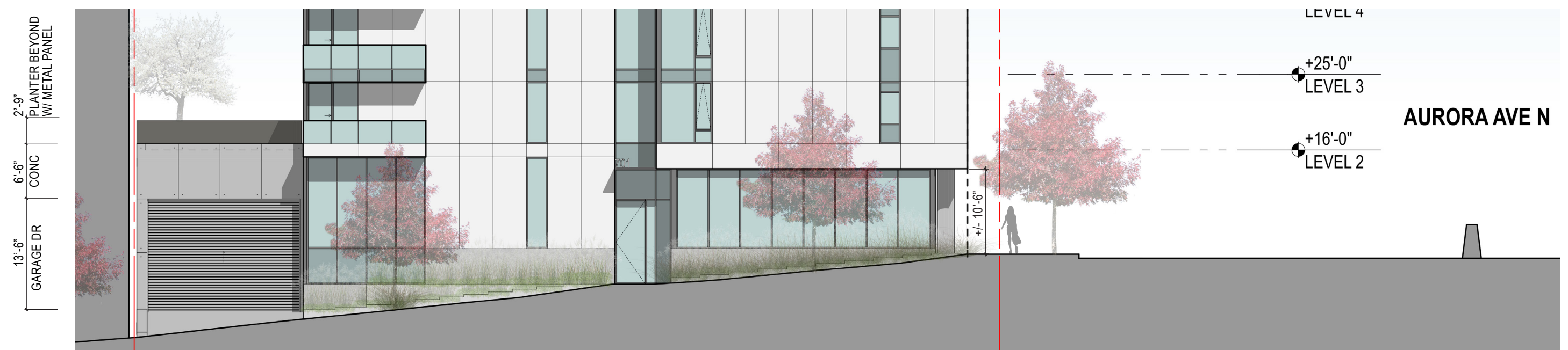




# RESPONSES TO STREETScape GUIDANCE: VALLEY ST



Previous Valley Street Elevation -  
Design Review Meeting #1



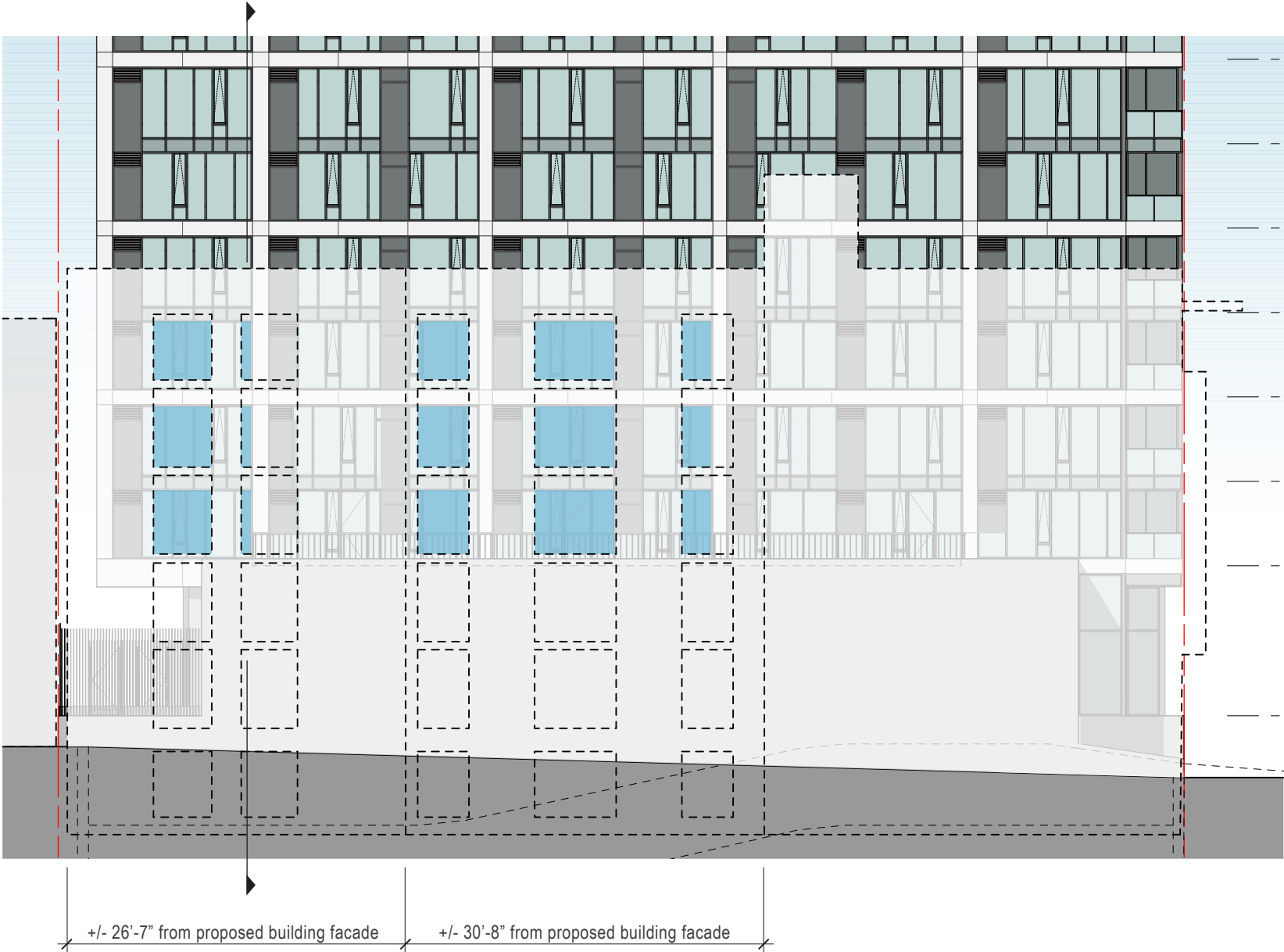
Proposed Valley Street Elevation





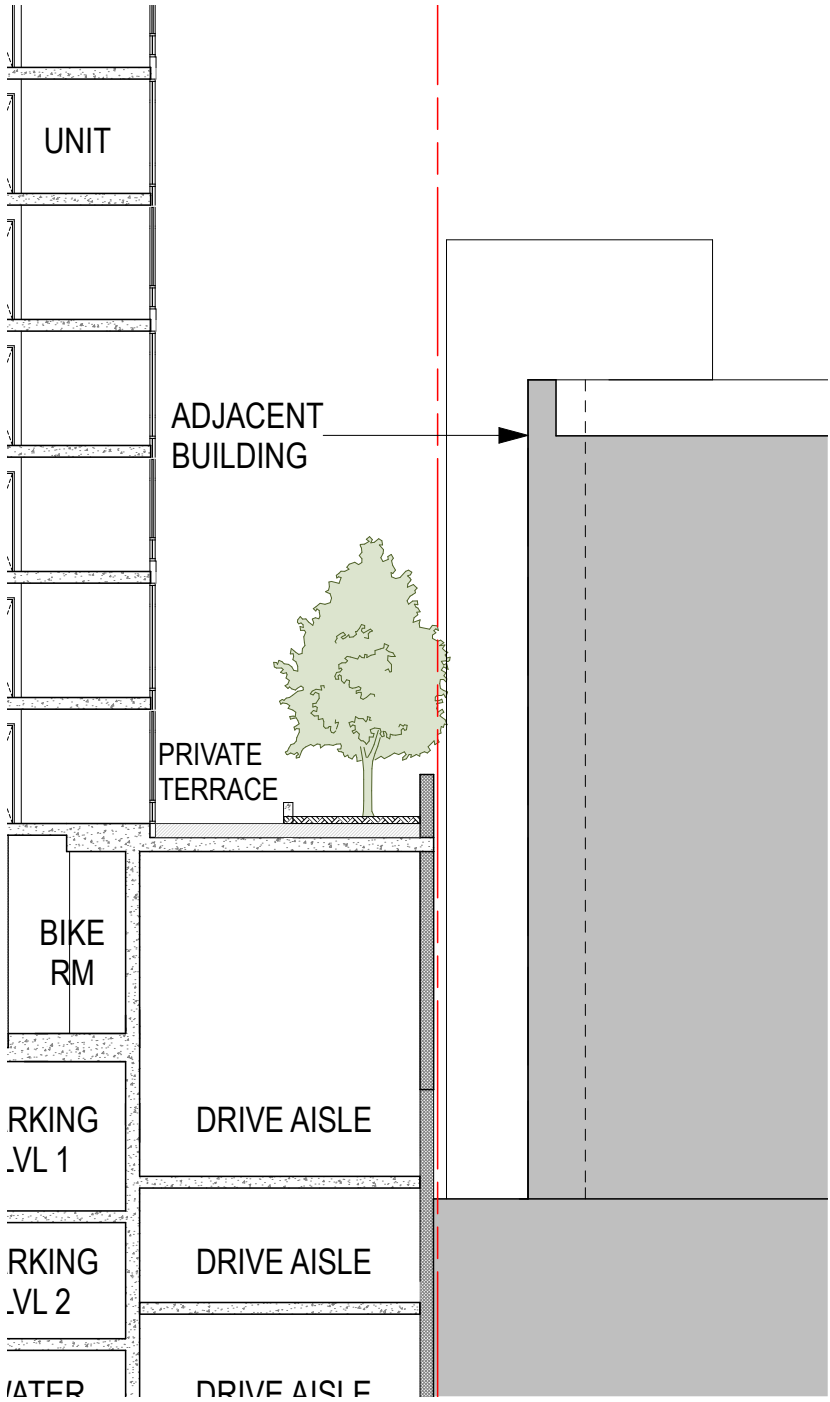


ADJACENCY STUDY: EAST ELEVATION



East Elevation

Light blue illustrates overlapping windows of adjacent and proposed building.

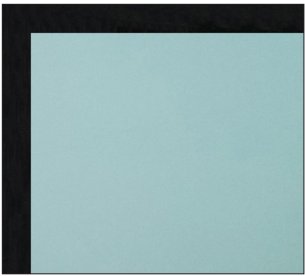


East-West Section



# MATERIAL AND COLOR PALETTE

## MATERIALS



**Material:** Low-e Coated Vision Glass  
**Product:**  
**Color:**  
**Dimensions:**  
  
**Application:**



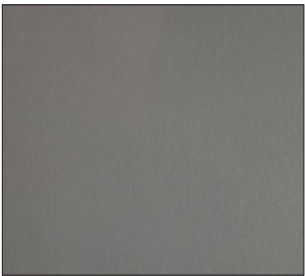
**Material:** Spandrel Glass  
**Product:**  
**Color:**  
**Dimensions:**  
  
**Application:**



**Material:** Integral Color Fiber Cement Panel 1  
**Product:** Swisspearl System  
**Color:** White-ish  
**Dimensions:** Varies  
  
**Application:** Exterior siding



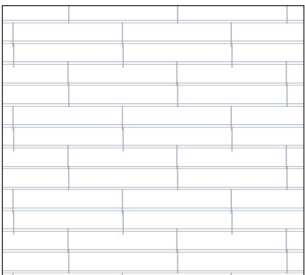
**Material:** Integral Color Fiber Cement Panel 2  
**Product:** Swisspearl System  
**Color:** Sand  
**Dimensions:** Varies  
  
**Application:** Exterior siding



**Material:** Metal Panel  
**Product:**  
**Color:** Gray  
**Dimensions:** Varies  
  
**Application:** Exterior siding, metal louver, and window wall system



**Material:** Concrete  
**Product:** Cast-in-place  
**Color:** Light gray  
**Dimensions:**  
  
**Application:** Exterior perimeter wall at street level

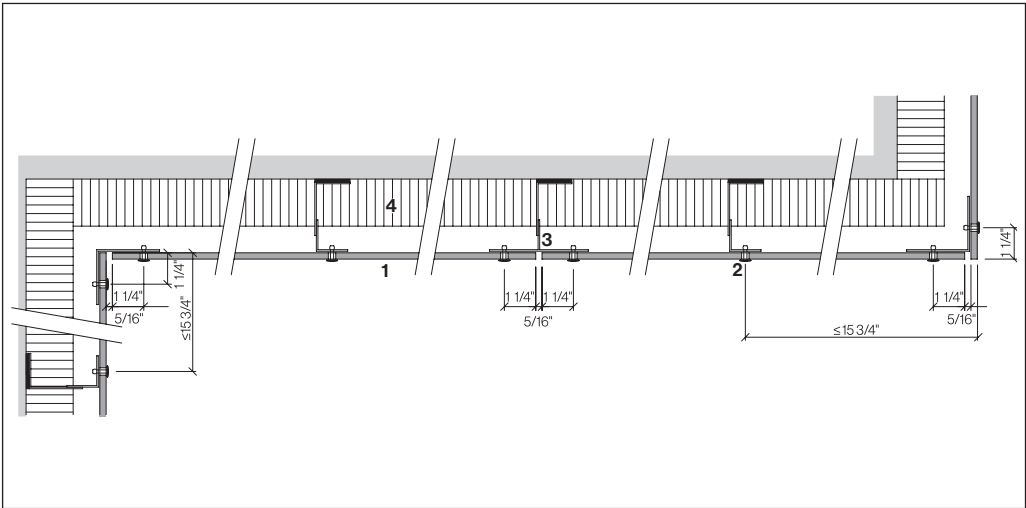


**Material:** Open Air Coiling Grille  
**Product:**  
**Color:** Gray to match concrete  
**Dimensions:** 18'-0"W x 9'-6" H  
  
**Application:** Exterior wall



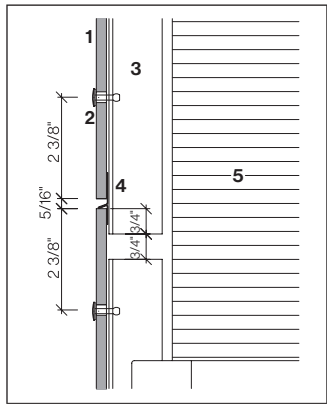
PROPOSED FIBER CEMENT PANEL SYSTEM DETAILS

TYPICAL PLAN DETAIL

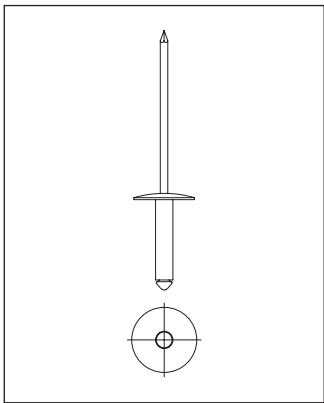


- 1 Swisspearl Largo 5/16"
- 2 Rivet
- 3 Aluminum profile
- 4 Thermal insulation

TYPICAL PANEL JOINTING



- 1 Swisspearl Largo 5/16"
- 2 Rivet
- 3 Panel support profile
- 4 Joint flashing (optional). Always leave -1/16" of the panel width
- 5 Thermal insulation



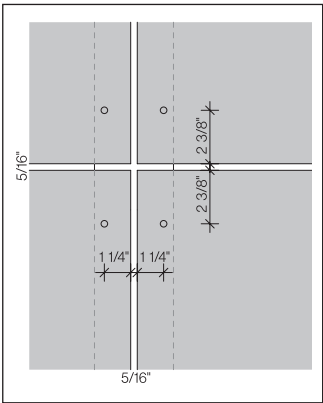
Swisspearl rivet Ø 15 mm

Holes in panel for rivets

Diameter 3/8"

Minimum edge distances

Horizontally 1 1/4"  
Vertically 2 3/8"



Panel edge distances

Panel joints

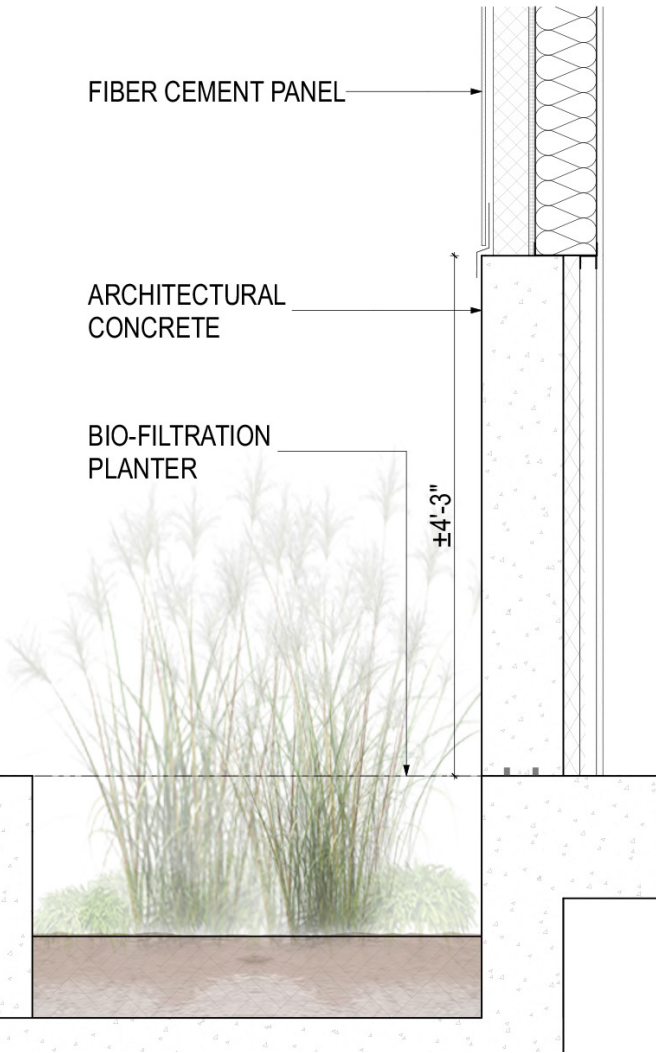
Typical panel joint is 5/16", this allows panel scraps to be used as spacers. Wider joints will make any inaccuracies in the installation less noticeable.



PRODUCT EXAMPLE: FACE FASTENED PANELS



PRODUCT EXAMPLE: ADJACENT TO PLANTINGS

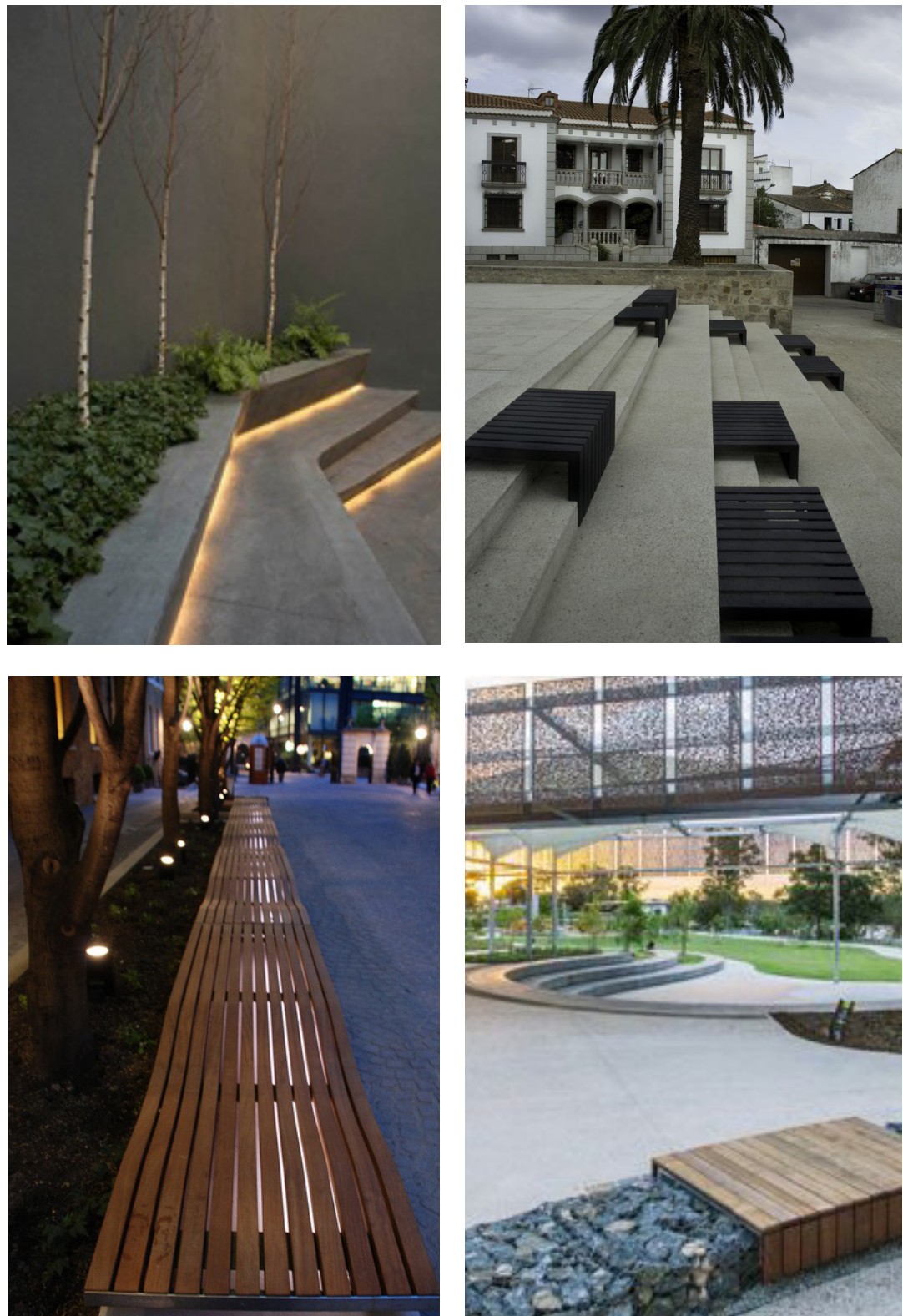


SECTION DETAIL AT FIBER CEMENT PANEL-TO-CONCRETE WALL TRANSITION



# PLANTING AND PAVING

## HARDSCAPE INSPIRATION IMAGES

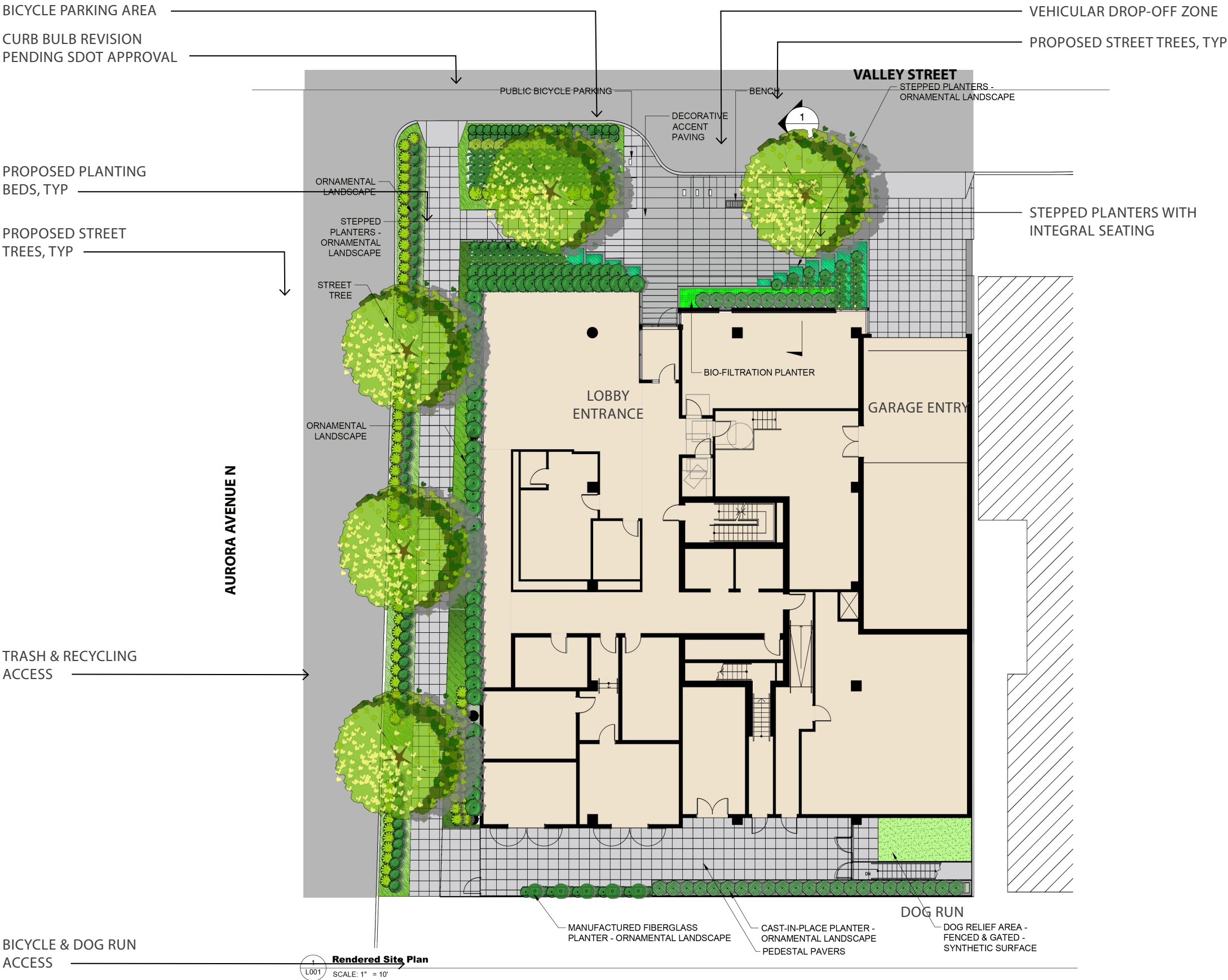


## PLANTING INSPIRATION IMAGES





STREET LEVEL LANDSCAPE PLAN

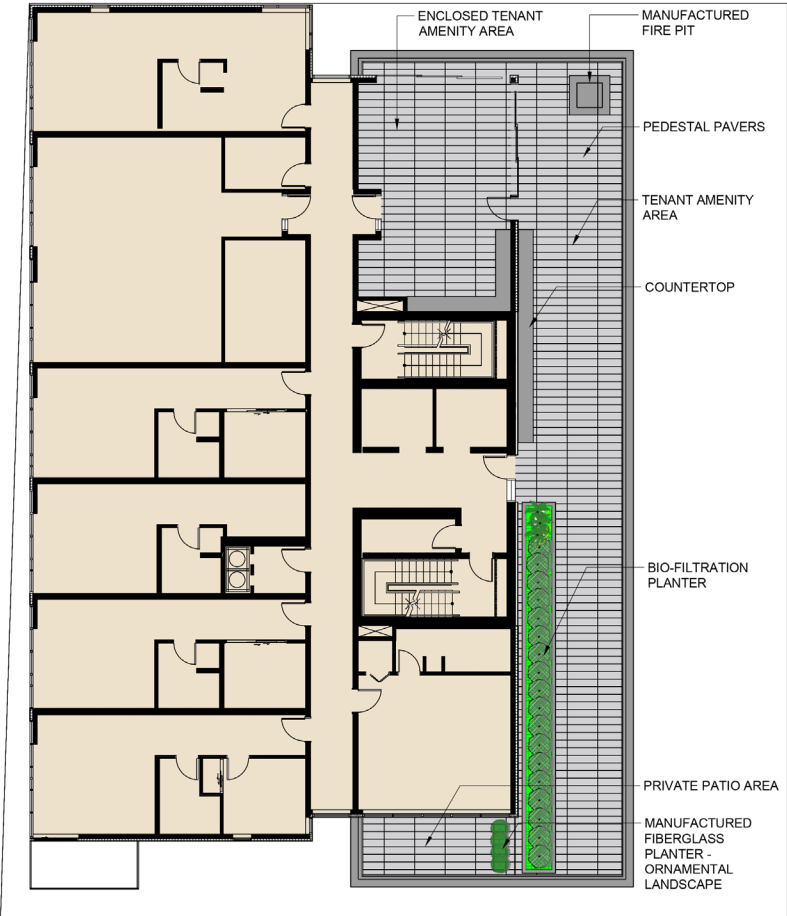




PODIUM & ROOF TERRRACE LANDSCAPE PLANS

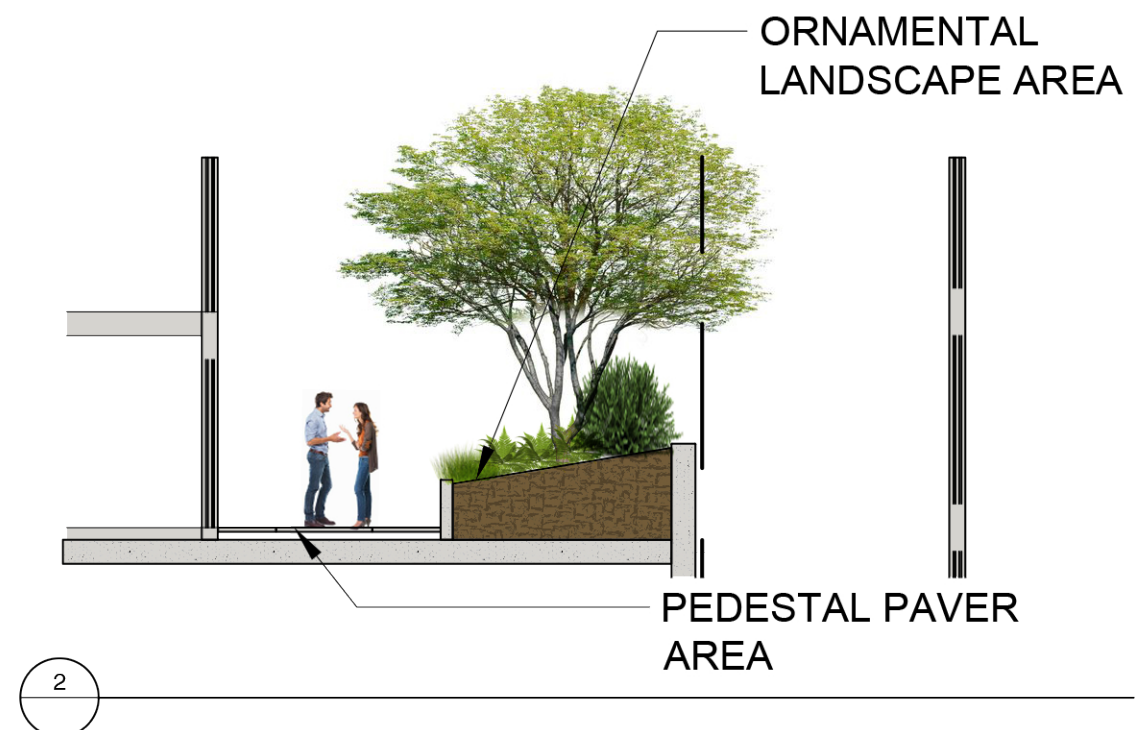
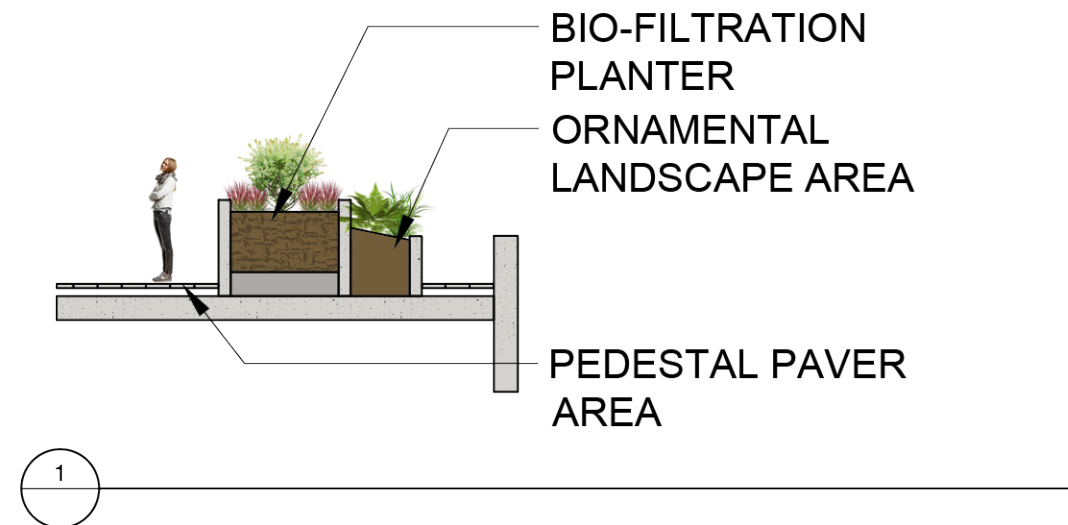
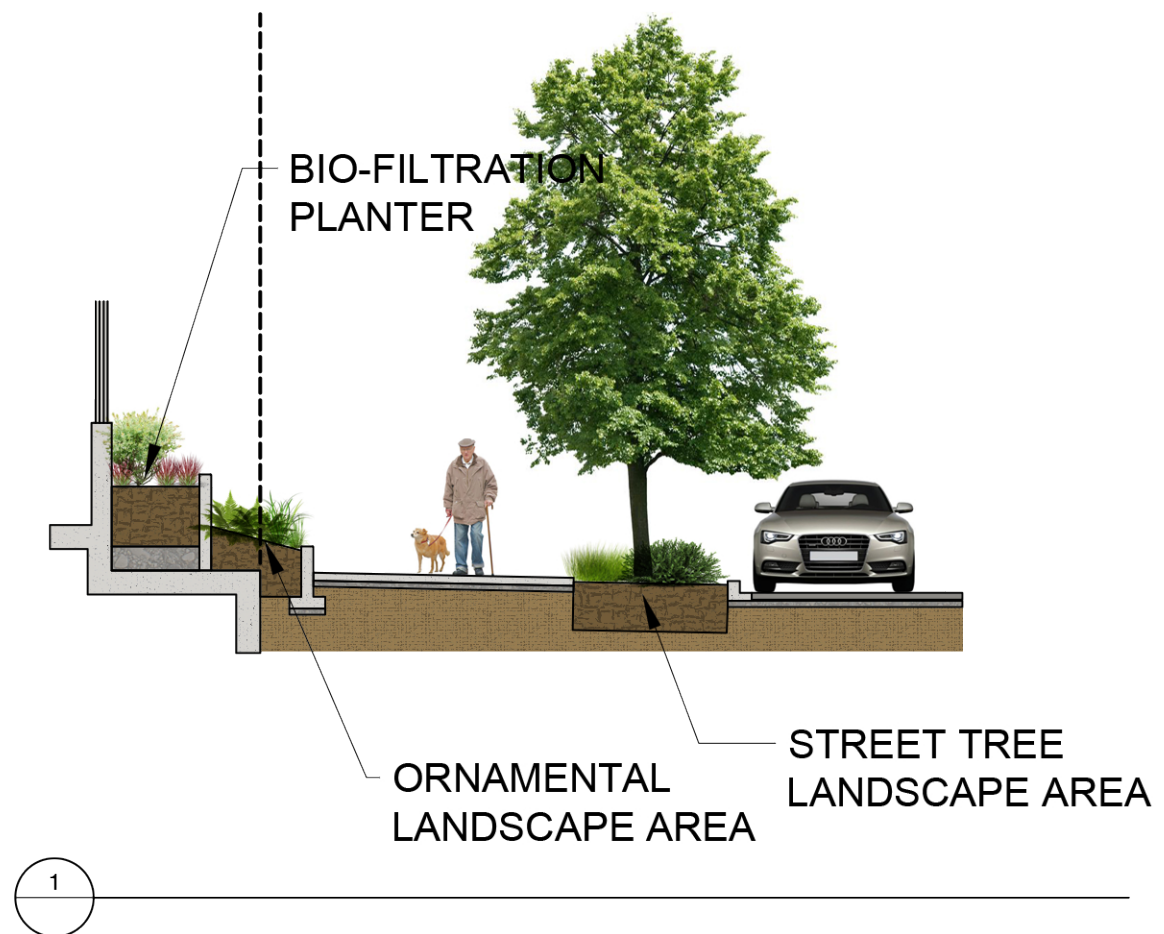


Second Floor & Terrace Plan



Roof Terrace Plan

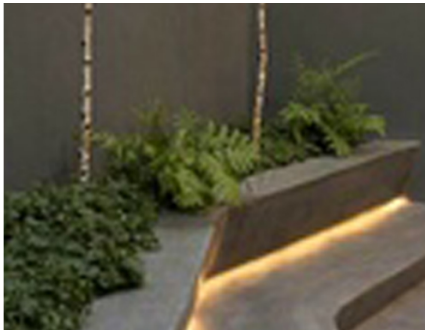




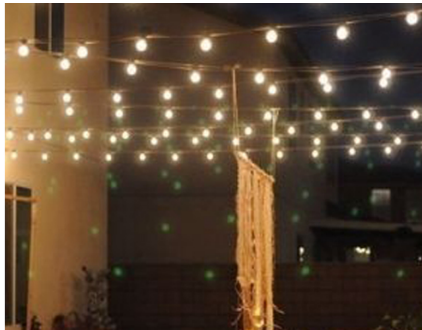


# STREET LEVEL EXTERIOR LIGHTING PLAN

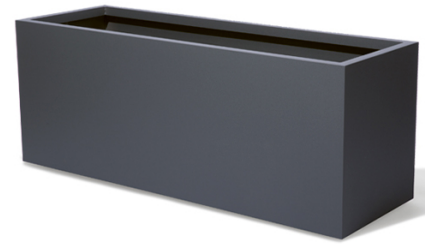
## LIGHTING FIXTURE EXAMPLES



strip light in cast planter walls



suspended festival lights



manufactured planter



manufactured bench



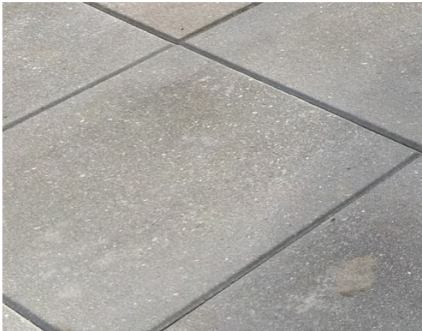
bicycle rack



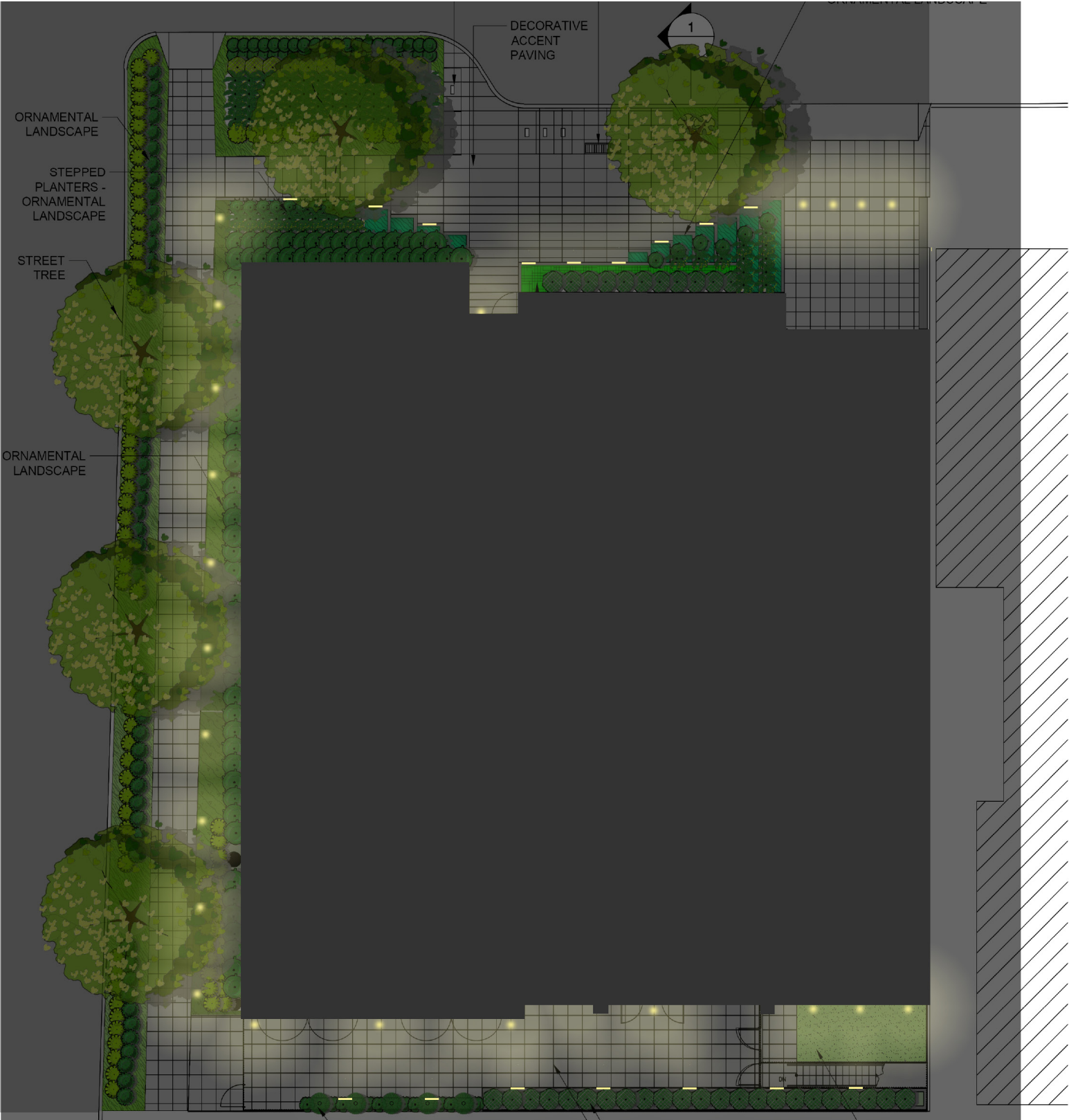
bicycle rack detail



accent paving with ground glass



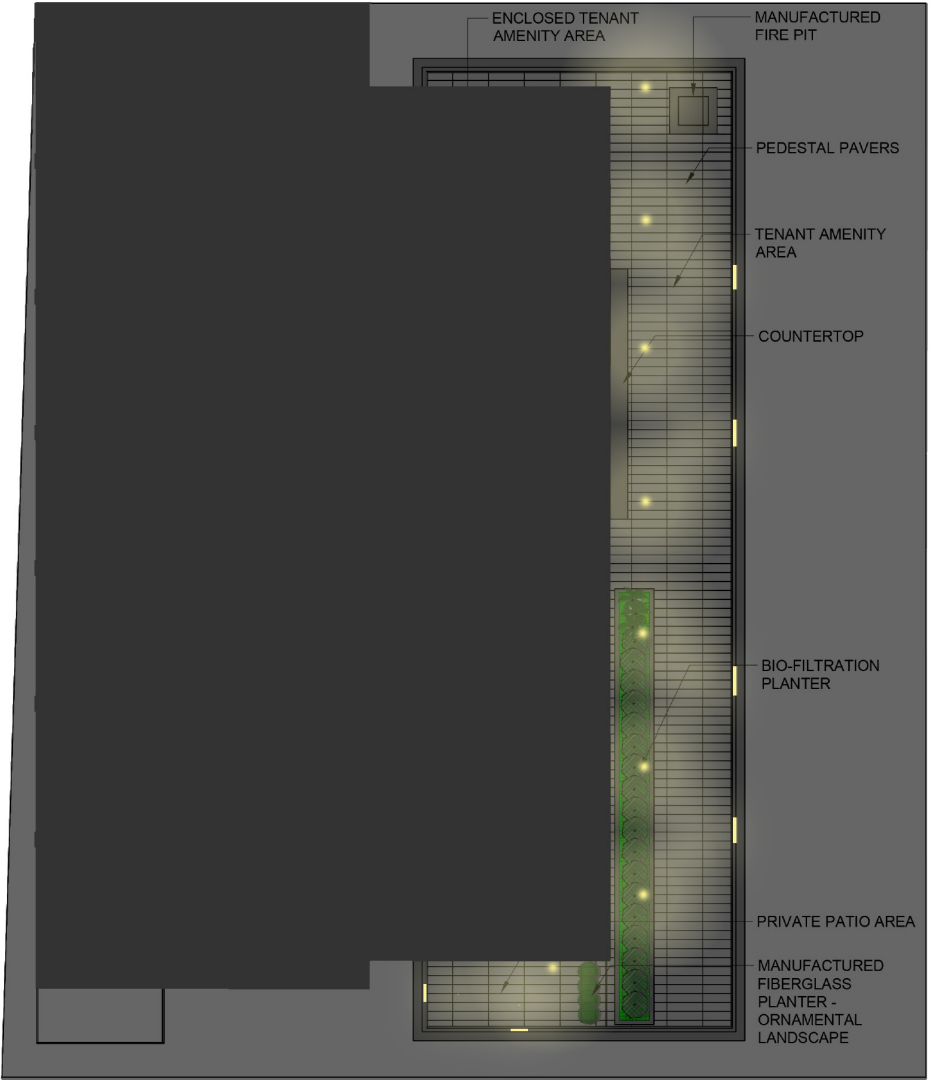
precast pedestal pavers







Second Floor & Terrace Lighting Plan



Roof Terrace Lighting Plan









## Residential Identity Signage

- Wall-mounted
- Integrated with building panel sizes & locations
- Potential for internal or back lighting



## Residential Address Signage

- Integrated with minimalist steel plate entry canopy
- Pin-mounted lettering





# REQUESTED DEPARTURES

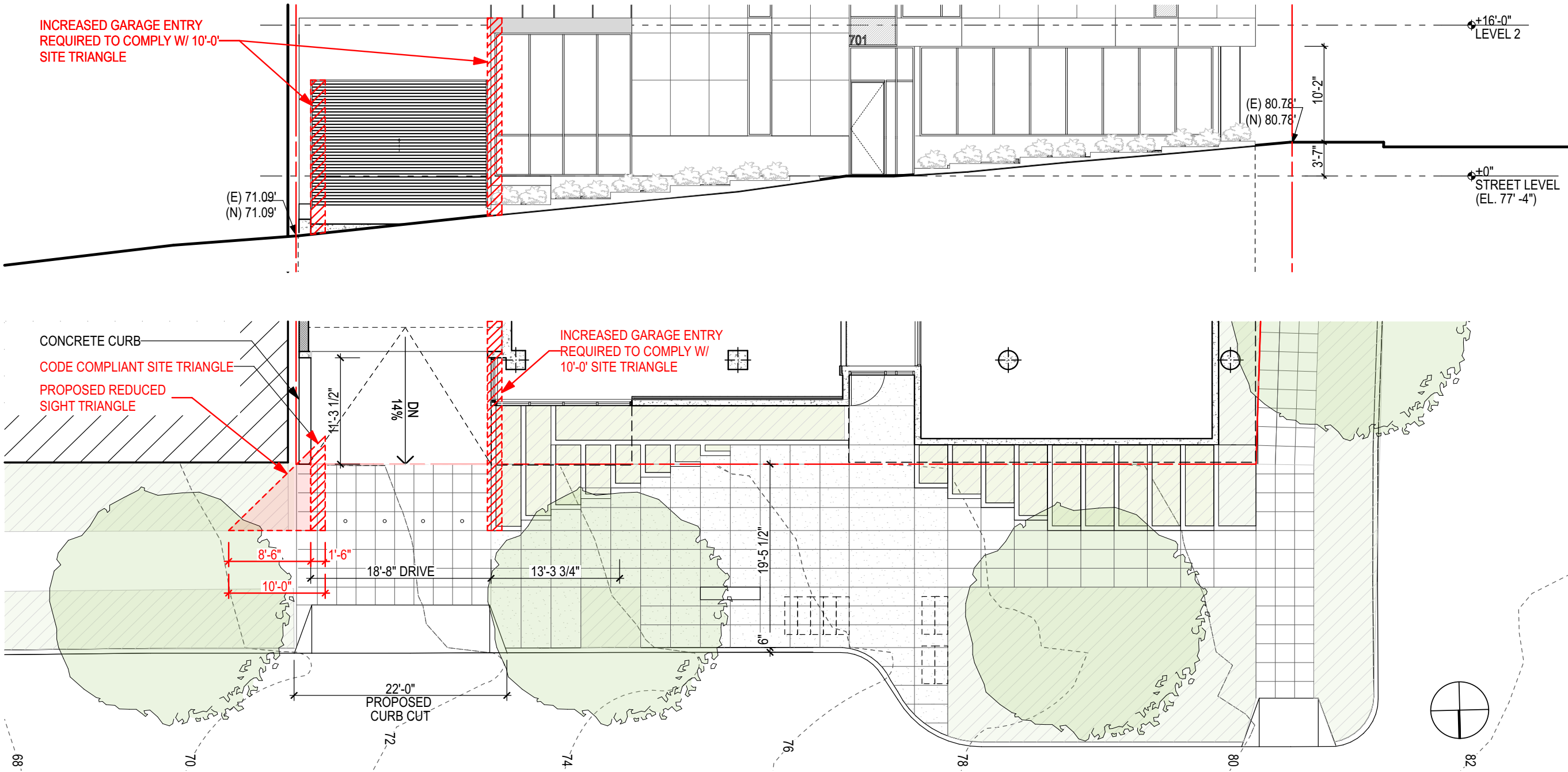
DESIGN STANDARD	DEVELOPMENT STANDARD DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<div>1</div> <div><b>SMC 23.54.030.G.2 Sight Triangle Requirements</b> A ten-foot sight triangle is required at both sides of two-way driveways up to 22-feet wide. When a driveway is located adjacent to a property line, the sight triangle can be achieved by offsetting the driveway or by procuring an easement from the neighbor.</div>	Reduce required sight triangle by 1'-6" to allow an 8'-6" sight triangle at the exit side of the driveway and use textured pavement and at-grade warning lights to address pedestrian safety	Reducing the sight triangle requirement will help to minimize the street level prominence of the parking garage entry. This is particularly important for the project, as Valley Street will be the primary pedestrian approach to the building. To achieve a safe resulting environment, the garage entry is set back from the property line about 11'-6". With the existing sidewalk location this results in nearly 20' from the garage door to sidewalk. The use of textured paving is an effective way of subtly marking intersections at the sidewalk and is a method used by other multi-family projects located in South Lake Union. At-grade, in-slab warning lights are an additional unobtrusive way to signal to pedestrians that a vehicle is approaching, and will supplement the use of textured paving and address low-light level situations. (Design Guidelines CS2-B.2 Connection to Street, DC1-C.1 Below-Grade Parking, DC1-C.2 Visual Impacts)

DESIGN STANDARD	DEVELOPMENT STANDARD DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<div>2</div> <div><b>SMC 23.54.030.D.1.c Driveways</b> Driveways of any length that serve more than 30 parking spaces shall be at least 10 feet wide for one-way traffic and at least 20 feet wide for two-way traffic.</div>	Reduce required driveway width by 1'-4" to allow an 18'-8" driveway at the parking garage entry of Valley St.	Allowing a narrower driveway width on the entrance side of the garage entry will help to diminish the presence of the garage entry and emphasize the pedestrian realm along Valley Street. As noted for Departure Request #2, the emphasis on the pedestrian environment along Valley Street is especially important to the project, as it is the primary approach and entry to the building. (Design Guidelines CS2-B.2 Connection to Street, DC1-C.2 Visual Impacts)



# REQUESTED DEPARTURE #1: SIGHT TRIANGLE

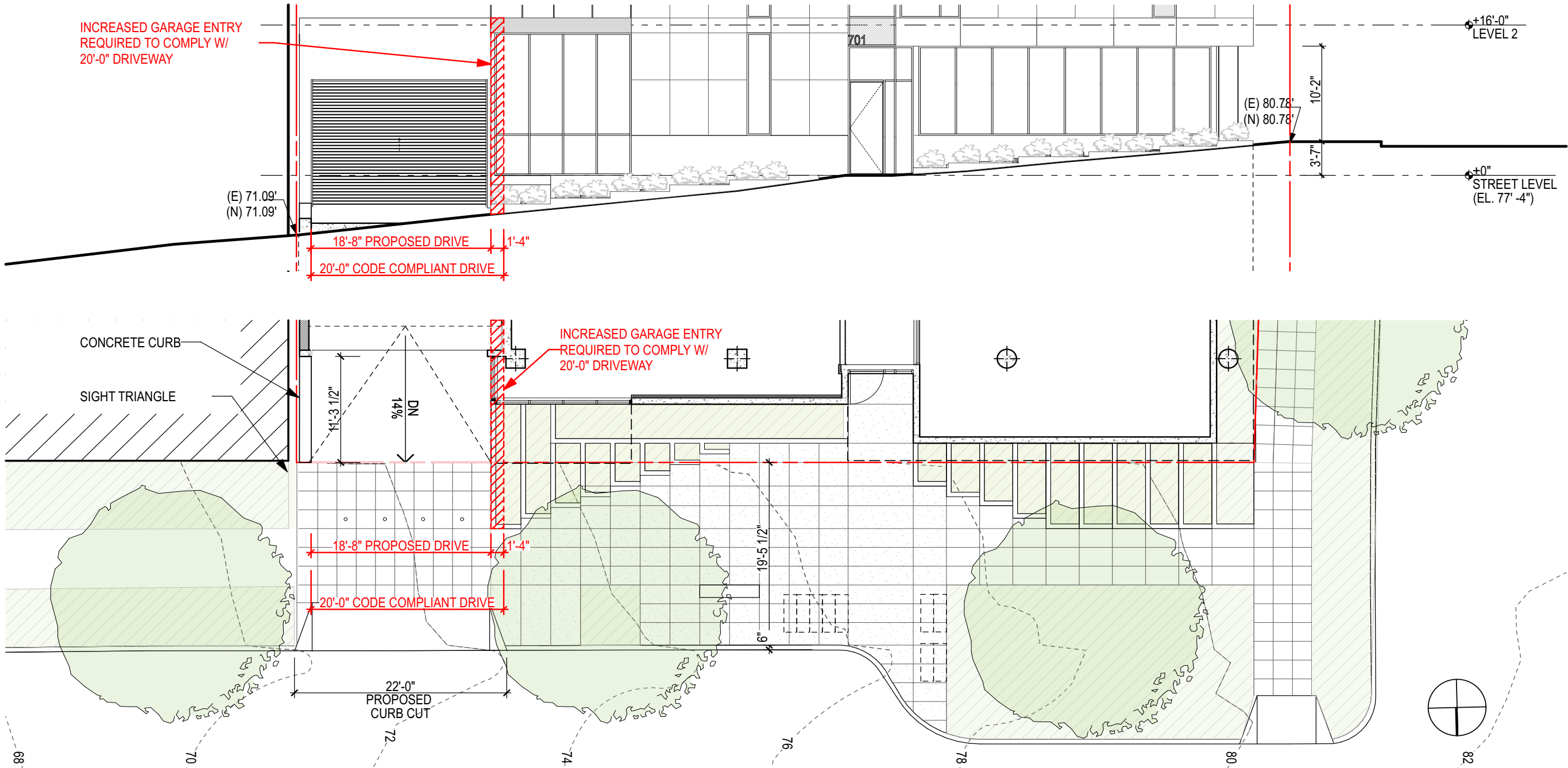
DESIGN STANDARD	DEVELOPMENT STANDARD DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<p>1 <b>SMC 23.54.030.G.2 Sight Triangle Requirements</b></p> <p>A ten-foot sight triangle is required at both sides of two-way driveways up to 22-feet wide. When a driveway is located adjacent to a property line, the sight triangle can be achieved by offsetting the driveway or by procuring an easement from the neighbor.</p>	<p>Reduce required sight triangle by 1'-6" to allow an 8'-6" sight triangle at the exit side of the driveway and use textured pavement and at-grade warning lights to address pedestrian safety</p>	<p>Reducing the sight triangle requirement will help to minimize the street level prominence of the parking garage entry. This is particularly important for the project, as Valley Street will be the primary pedestrian approach to the building. To achieve a safe resulting environment, the garage entry is set back from the property line about 11'-6". With the existing sidewalk location this results in nearly 20' from the garage door to sidewalk. The use of textured paving is an effective way of subtly marking intersections at the sidewalk and is a method used by other multi-family projects located in South Lake Union. At-grade, in-slab warning lights are an additional unobtrusive way to signal to pedestrians that a vehicle is approaching, and will supplement the use of textured paving and address low-light level situations. (Design Guidelines CS2-B.2 Connection to Street, DC1-C.1 Below-Grade Parking, DC1-C.2 Visual Impacts)</p>





# REQUESTED DEPARTURE #2: DRIVEWAY WIDTH

DESIGN STANDARD	DEVELOPMENT STANDARD DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<p><b>1 SMC 23.54.030.G.2 Sight Triangle Requirements</b></p> <p>A ten-foot sight triangle is required at both sides of two-way driveways up to 22-feet wide. When a driveway is located adjacent to a property line, the sight triangle can be achieved by offsetting the driveway or by procuring an easement from the neighbor.</p>	<p>Reduce required sight triangle by 1'-6" to allow an 8'-6" sight triangle at the exit side of the driveway and use textured pavement and at-grade warning lights to address pedestrian safety</p>	<p>Reducing the sight triangle requirement will help to minimize the street level prominence of the parking garage entry. This is particularly important for the project, as Valley Street will be the primary pedestrian approach to the building. To achieve a safe resulting environment, the garage entry is set back from the property line about 11'-6". With the existing sidewalk location this results in nearly 20' from the garage door to sidewalk. The use of textured paving is an effective way of subtly marking intersections at the sidewalk and is a method used by other multi-family projects located in South Lake Union. At-grade, in-slab warning lights are an additional unobtrusive way to signal to pedestrians that a vehicle is approaching, and will supplement the use of textured paving and address low-light level situations. (Design Guidelines CS2-B.2 Connection to Street, DC1-C.1 Below-Grade Parking, DC1-C.2 Visual Impacts)</p>





# REQUESTED DEPARTURES - COMBINED IMPACT TO PEDESTRIAN EXPERIENCE

DESIGN STANDARD	DEVELOPMENT STANDARD DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURES
<p>1 <b>SMC 23.54.030.G.2 Sight Triangle Requirements</b> A ten-foot sight triangle is required at both sides of two-way driveways up to 22-feet wide.</p>	Reduce required sight triangle by 1'-6"	<p>The primary pedestrian approach to the building will be along Valley St from the east. These two requested departures allow for a reduced expression of the parking garage. Without either of these departures, the access to the parking garage would be close to 3'-0" wider than currently shown, negatively impacting the pedestrian experience along Valley St, and given greater prominence to the parking garage access.</p>
<p>2 <b>SMC 23.54.030.D.1.c Driveways</b> Driveways of any length that serve more than 30 parking spaces shall be at least 20 feet wide for two-way traffic.</p>	Reduce required driveway width by 1'-4"	

