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Northgate Apartments
10720 5th Avenue NE, Seattle, WA 98125
10715 8th Avenue NE, Seattle, WA 98125

RECOMMENDATION MEETING
October 19, 2015
PROJECTS #3017692 & #3019072

DEVELOPMENT OBJECTIVES

PROJECT DESCRIPTION

Located on a through-block site, the Northgate Apartments project will build upon the existing urban fabric in the Northgate Urban Center. The Northgate Apartments will be comprised of two buildings. The 7-story West building will include below-grade parking, retail sales along 5th Avenue, and residential apartments and amenities. The 4-story East building will include residential apartments as well as live-work units. The development objectives for this project are as follows (all values are approximate):

West Building (DPD# 3017692)

Number of residential units:	134 Units
Retail sales and services:	2,848 sf
Number of parking stalls:	
residential (structured)	129
non-residential (structured)	7

East Building (DPD# 3019072)

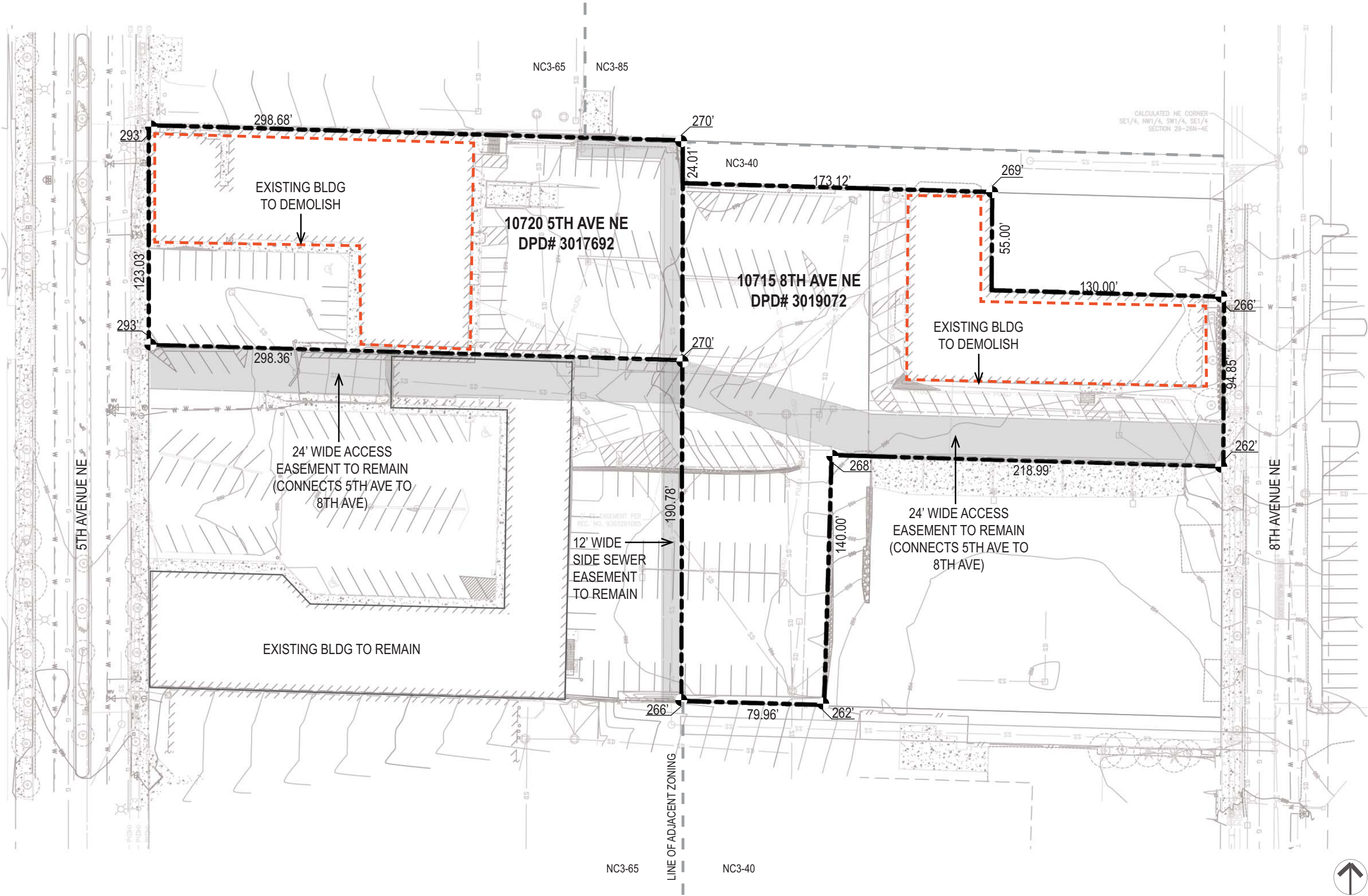
Number of residential units:	83 Units
Number of live/work units:	2 Units
Number of parking stalls:	
residential (surface)	41

Combined Parcels Development Total

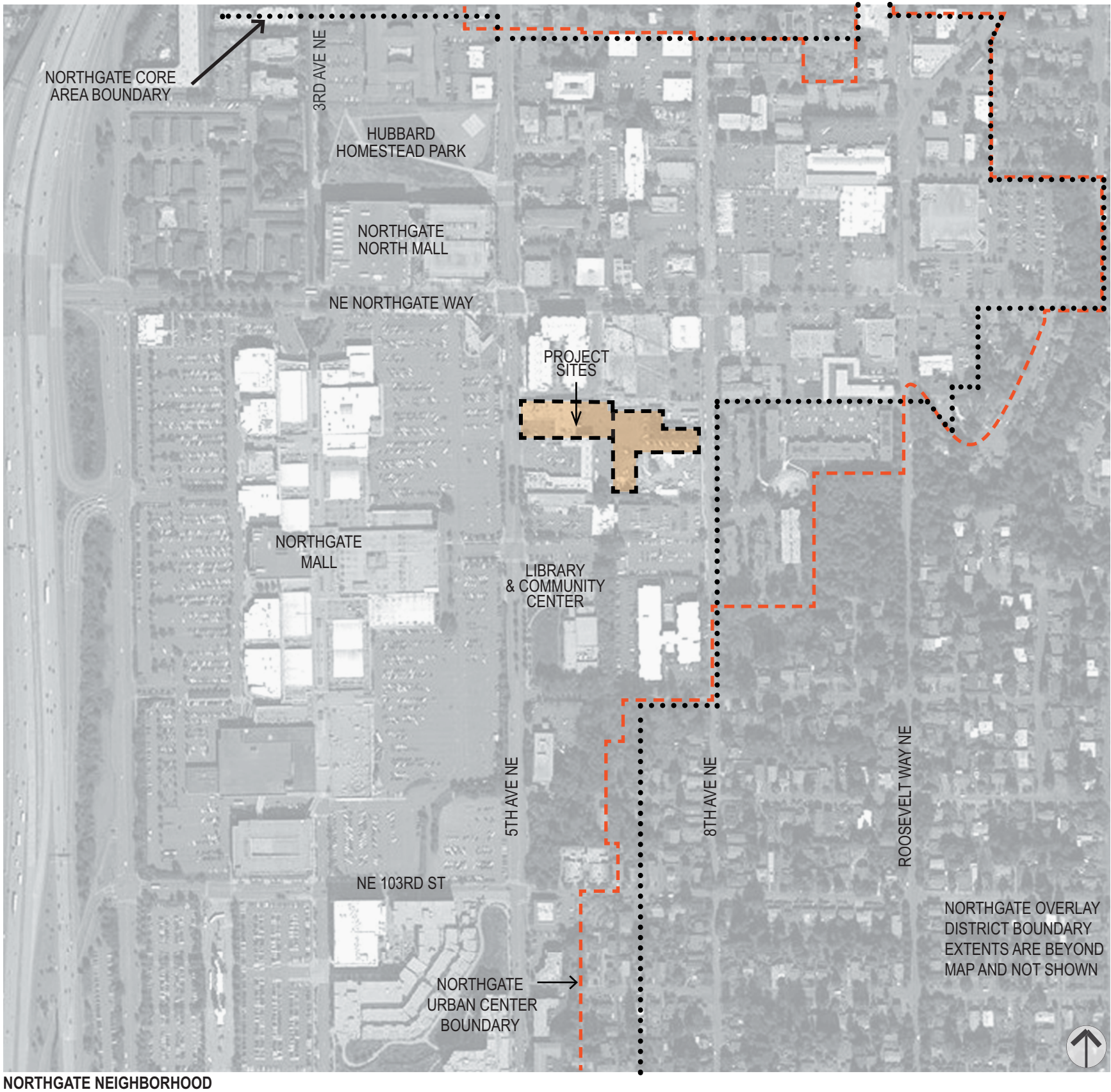
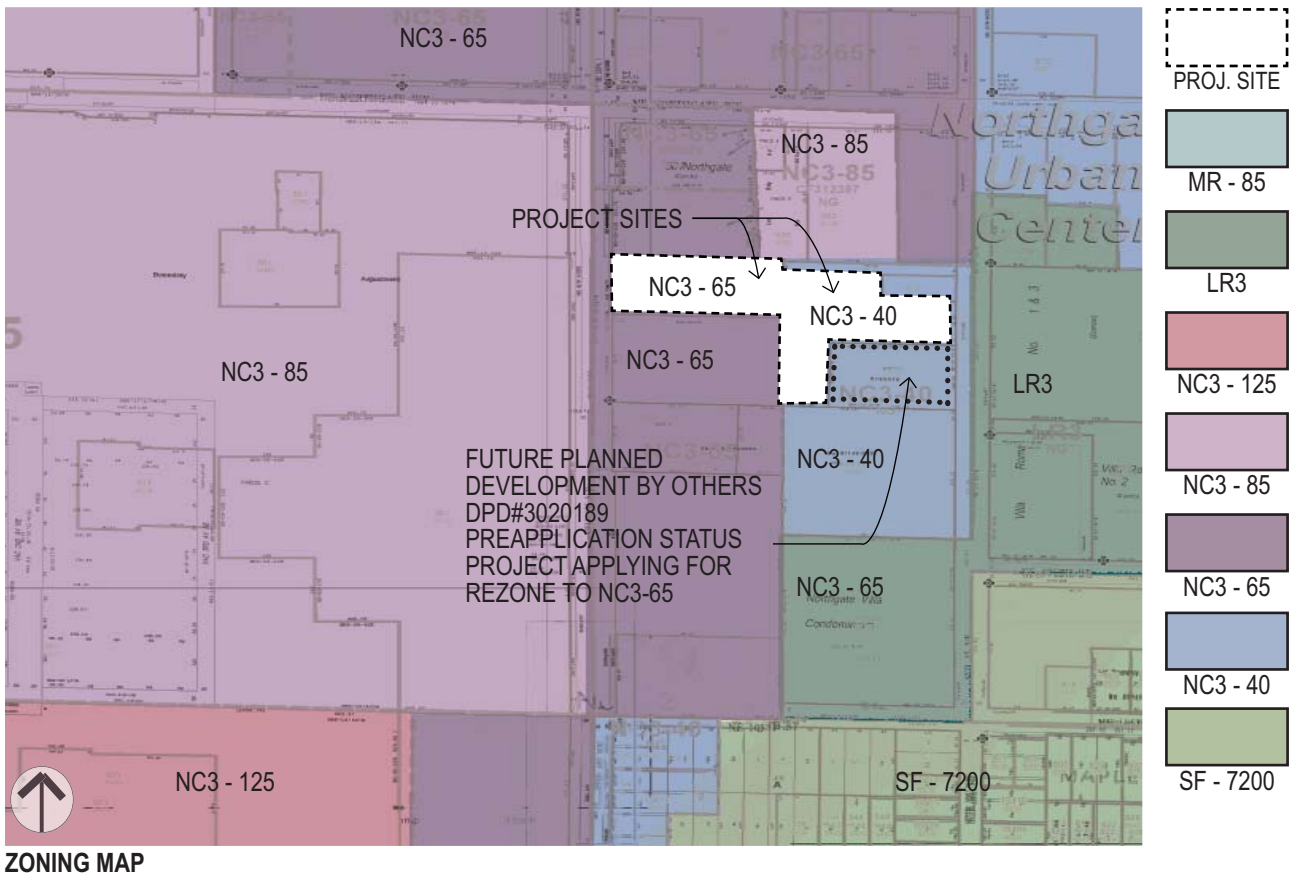
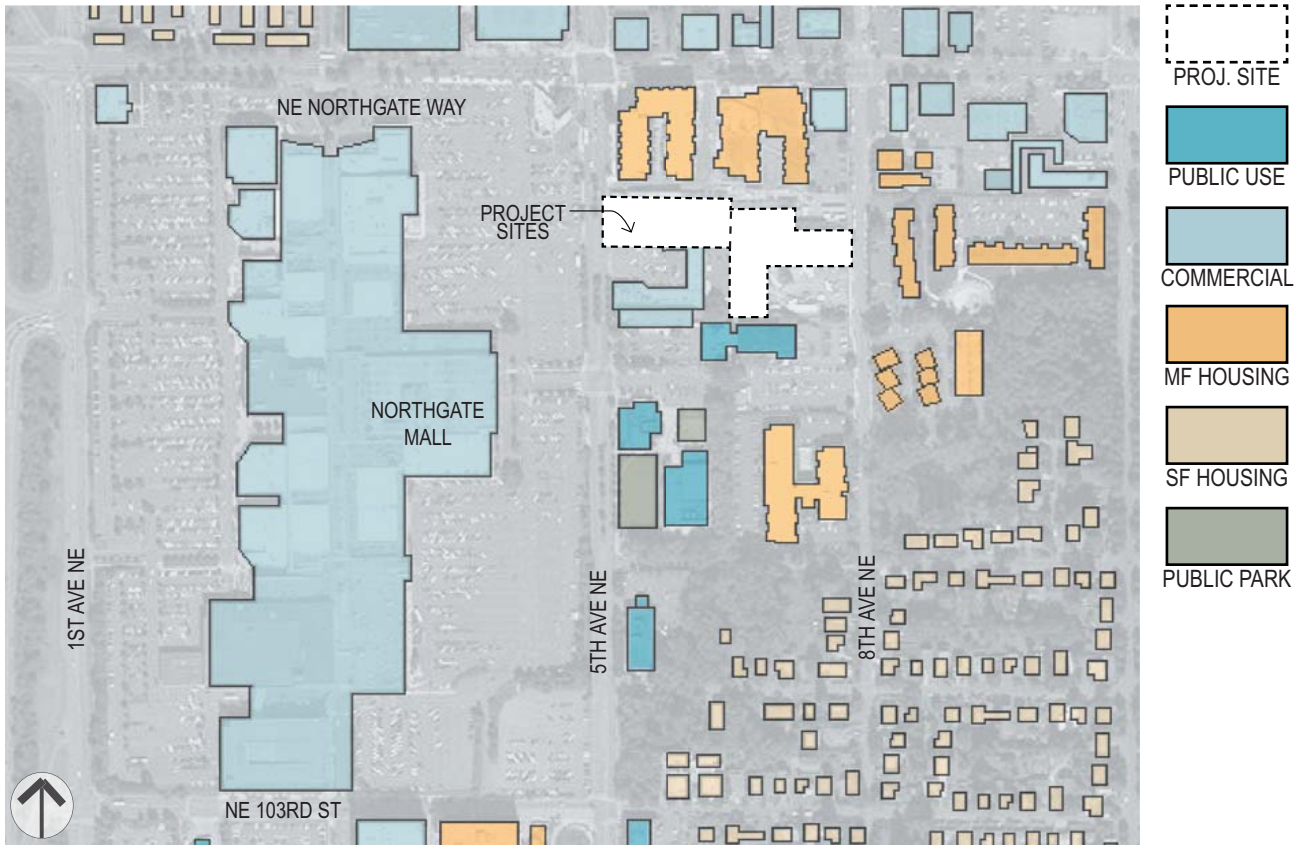
Number of residential units:	217 Units
Number of live/work units:	2 Units
Number of parking stalls:	170
Retail sales and services:	2,848 sf

PROJECT INFORMATION

Zoning / Lot Size:	NC3-40 / 49,680 SF NC3-65 / 36,718 SF
Overall Lot Size:	86,398 SF
Overlay:	Northgate Urban Center & Northgate Overlay District (Within Northgate Core Area)
Pedestrian Zone:	No
Mapped ECA:	Salmon Watershed & Peat
Major Pedestrian St:	5th Avenue (As designated in Northgate Overlay District)
Codes:	Seattle Land Use Code (current edition)

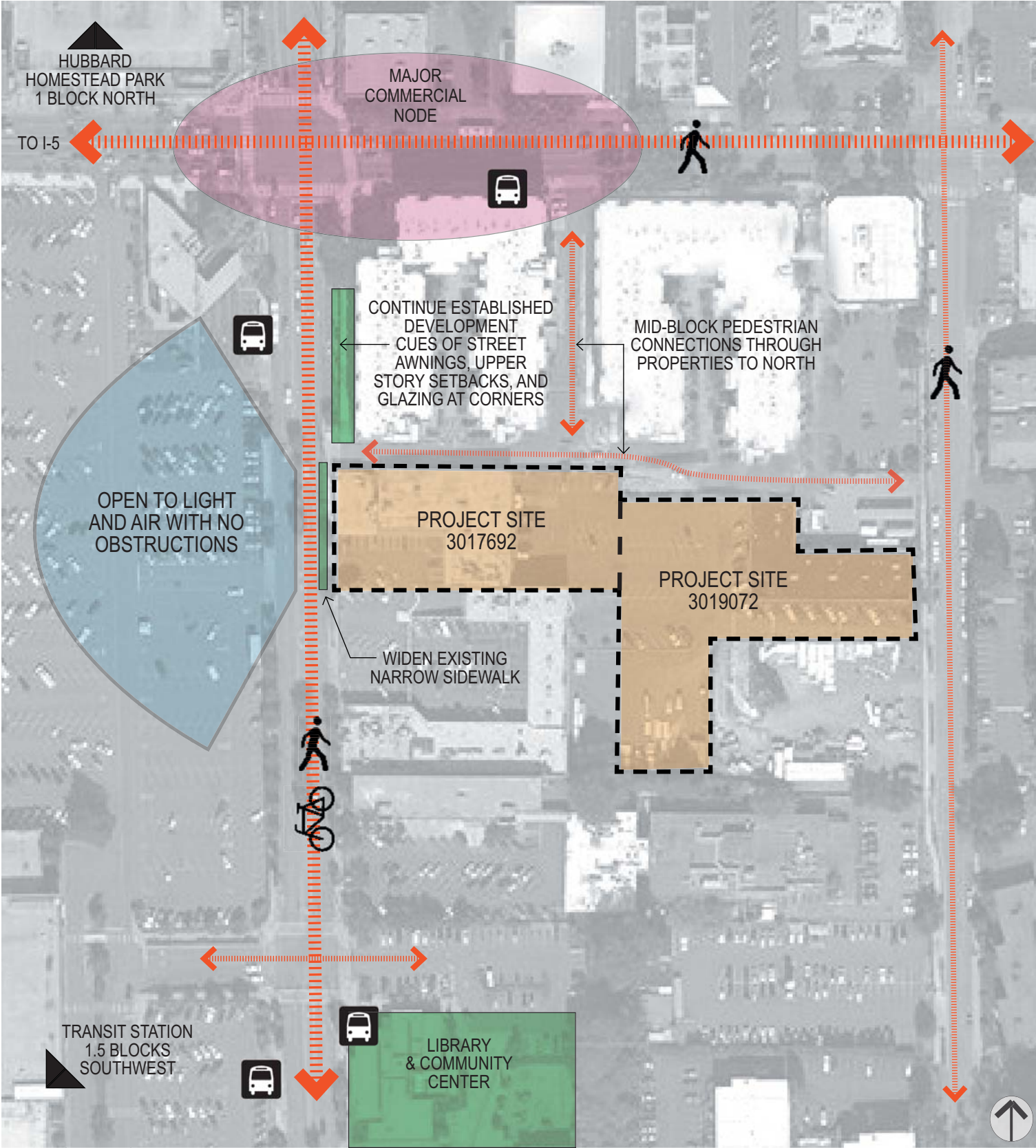


SUMMARY CONTEXT ANALYSIS

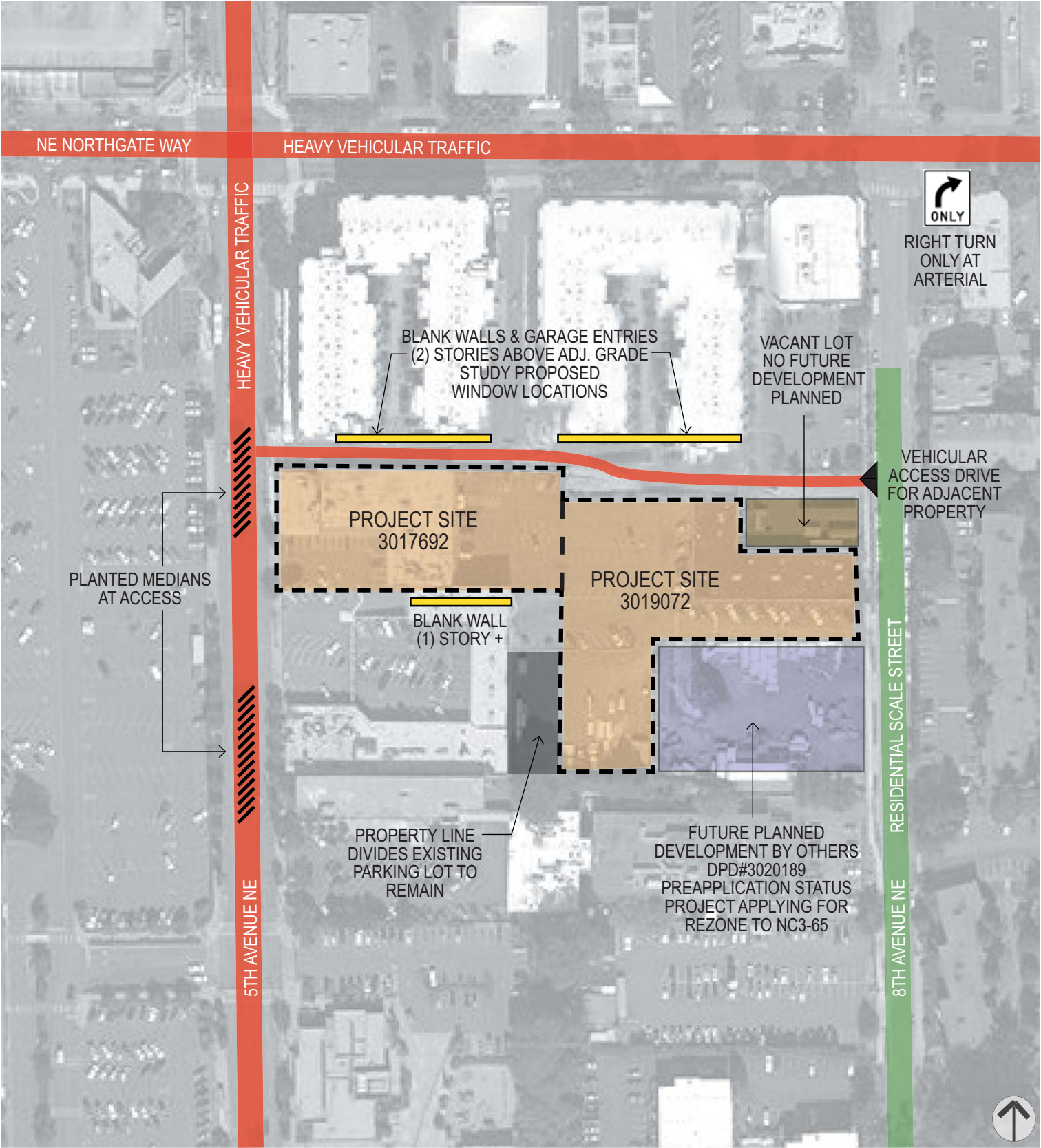




SUMMARY CONTEXT ANALYSIS: OPPORTUNITIES & CONSTRAINTS

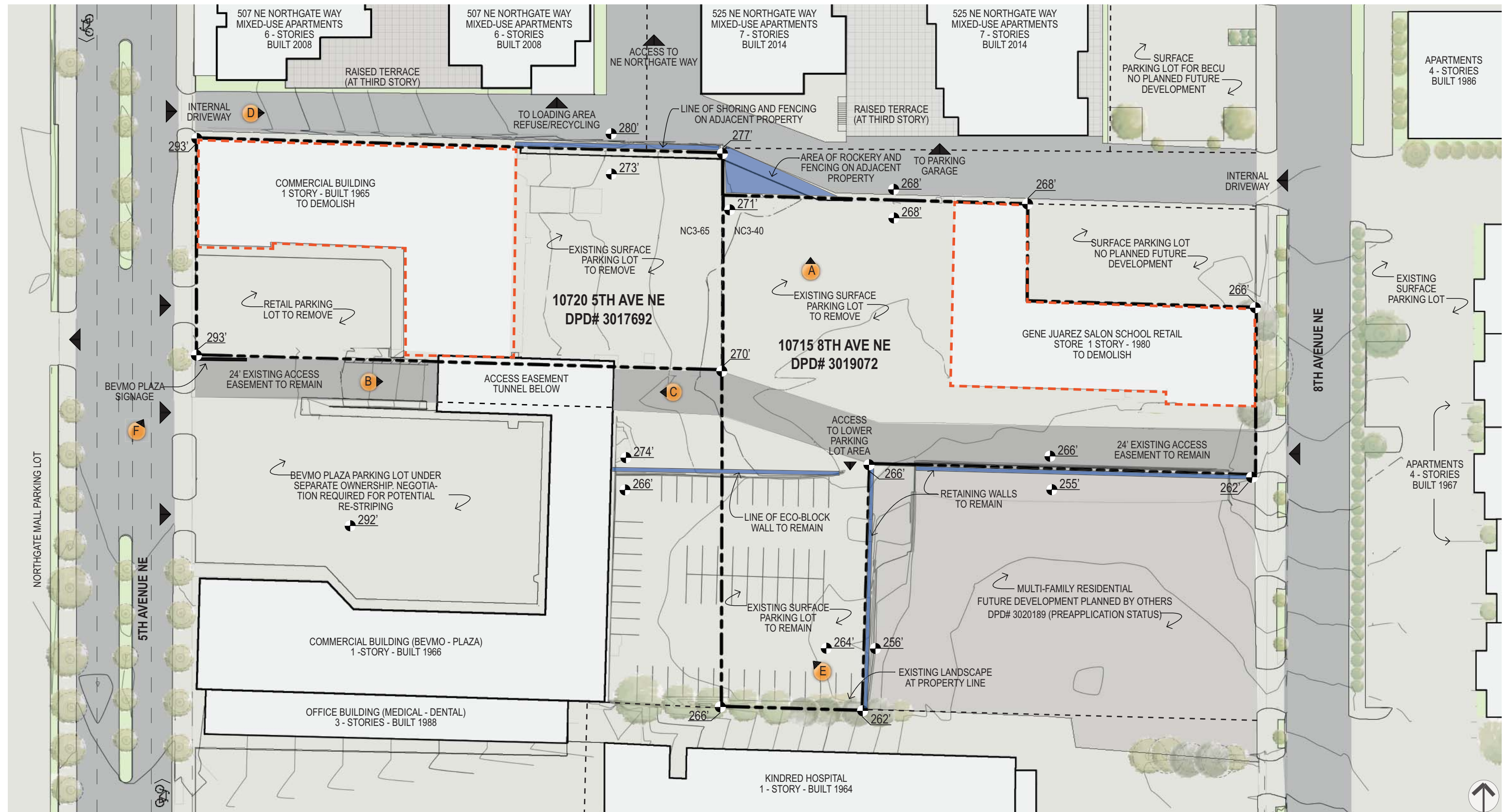


OPPORTUNITIES



CONSTRAINTS

EXISTING SITE CONDITIONS: SITE PLAN



A NORTH PROPERTY LINE

The site abuts two lots (507 & 525 NE Northgate Way), both recently developed mixed-use buildings. The buildings each contain similar amenities and uses as the proposed project. The buildings incorporate raised terraces above two-levels of parking adjacent to the North property line creating multiple blank wall conditions. Existing fencing on top of an exposed shoring add to the overall height of the blank walls.

B EASEMENT LOOKING EAST ON ADJACENT PROPERTY

An existing building on the adjacent property to the South is located above and within the access easement adjacent to the site. The building is to remain, creating both a blank wall condition along the Southern property line, as well as a “tunnel” that must be retained.

C EASEMENT LOOKING WEST ON ADJACENT PROPERTY

An existing building on the adjacent property to the South is located above and within the access easement adjacent to the site. This building is exposed from the Eastern side of the site as well.

D INTERNAL DRIVEWAY TO 507 & 525 NE NORTHGATE WAY

The driveway for access to lots 507 & 525 NE Northgate Way are used for parking access and trash removal for those development projects. It’s anticipated that pedestrians will also use this through block access once the development at 525 is complete, though no sidewalks are provided in the East-West direction.

E ADJACENT SURFACE PARKING LOT TO REMAIN

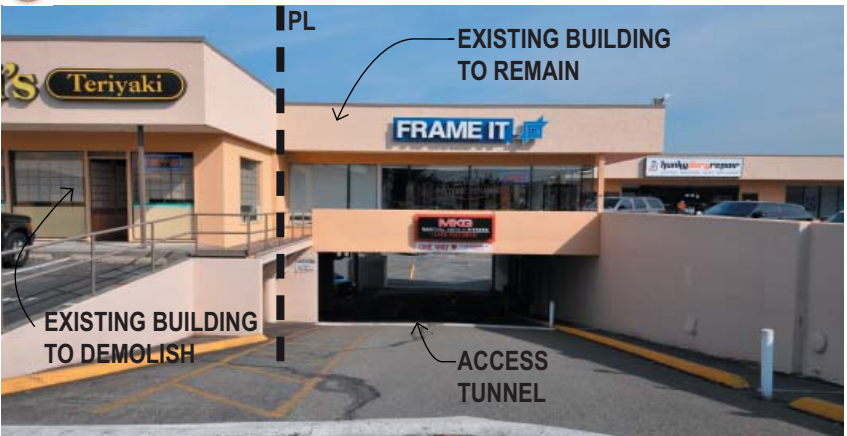
A North-South property line divides an existing surface parking lot so that half the parking area is on the proposed development lot and the remainder on the adjacent property which is under separate ownership. Due to an 8 foot grade differential between the parking area and its Northern edge, the access point to the parking area is in the proposed development and must be retained for shared access.

F PROPERTY SIGNAGE & EASEMENT ENTRY FROM 5TH AVE

Current curb cuts along 5th Avenue NE do not align with the access easement. Current signage is also within the access easement and would need to be addressed as part of the proposed development.



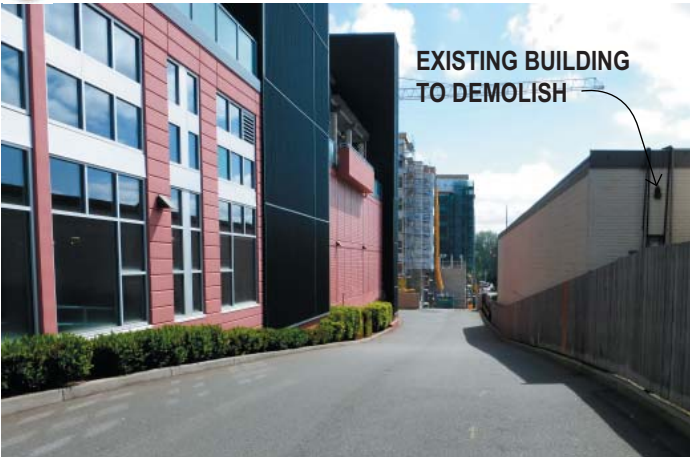
A NORTH PROPERTY LINE



B EASEMENT LOOKING EAST ON ADJACENT PROPERTY



C EASEMENT LOOKING WEST ON ADJACENT PROPERTY



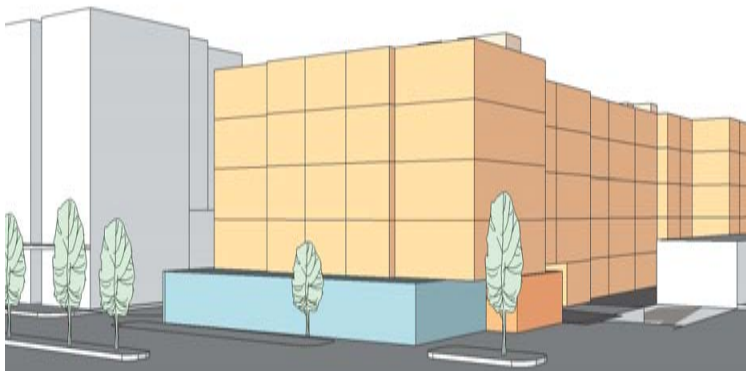
D INTERNAL DRIVEWAY TO 507 & 525



E ADJACENT SURFACE PARKING LOT TO REMAIN

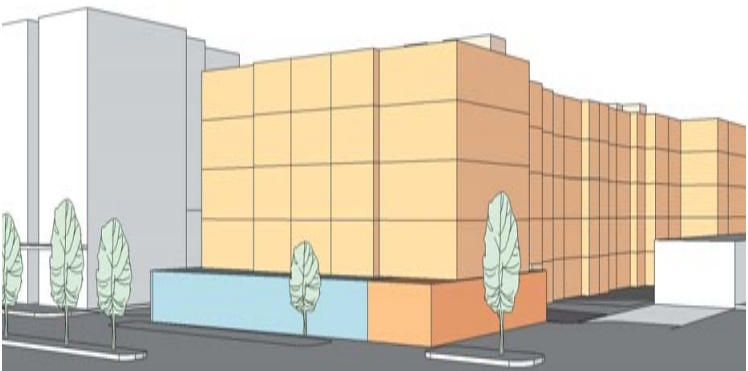


F PROPERTY SIGNAGE & EASEMENT ENTRY FROM 5TH AVE



EDG - OPTION B

DISTINGUISHING FEATURES:
Open plaza area to the Southwest along 5th Avenue NE oriented to retail streetscape. East building massing is held tightly to easement length to maximize center open area opportunities.



EDG - OPTION C (PREFERRED)

DISTINGUISHING FEATURES:
West building is accessed from 5th Avenue NE without needing to cross retail vehicular traffic. East building massing is angled to allow additional landscaping and access to light and air to center of the site.

BOARD COMMENTS:

CORNER RETAIL PLAZA: The board agreed that retail space along 5th Avenue NE should incorporate an entry courtyard as shown in Option B.

EAST / WEST PEDESTRIAN CONNECTIVITY: Board felt that a pedestrian connection located North of the access easement and existing building colonnade, without stairs, provided the best solution for a variety of users.

ENTRIES & ACCESS: Board felt that the residential entry should in located to the West of the retail vehicular entry access drive as proposed in Option B to avoid mingling of pedestrian and vehicular traffic. Board noted the easement should be treated as a “private street” with the use of signage, street lamps, and pedestrian walkways.

8TH AVENUE NE: The massing of the East building should respect the lower residential scale of the 8th Avenue NE streetscape.

LIVE / WORKS: The board felt that the live work use should have its own commercial character with distinct but cohesive entries in coordination with the residential entries. In addition, a landscape barrier should be included between the sidewalk and the live / work units balancing privacy and “eyes on the street”.

BOARD COMMENTS:

NORTH / SOUTH PEDESTRIAN CONNECTIVITY: The board strongly encouraged the inclusion of a North / South pedestrian connection that would connect the property to the North with the East / West connection running adjacent to the access drive. The board agreed that barrier free access was an unlikely attainable due to the existing topography.

PARKING & ACCESS: Board supported Option C placement of vehicular parking access locations along the easement drive, with the retail parking access specifically located behind the retail use. And the residential vehicular access point should not be located directly across from the neighboring tenant space.

ENTRIES & ACCESS: Due to the presence of two streets, a private through street, and multi-directional pedestrian connectivity, wayfinding is critical for all types of users. Project should demonstrate a comprehensive plan for movement for users and all modes of travel.

RESIDENTIAL ENTRIES: Board felt primary residential entries should be located along the street frontage. Alternatively, the project will need to demonstrate the rationale for primary entries being located at the center of the site and how all users will find the residential entry points.

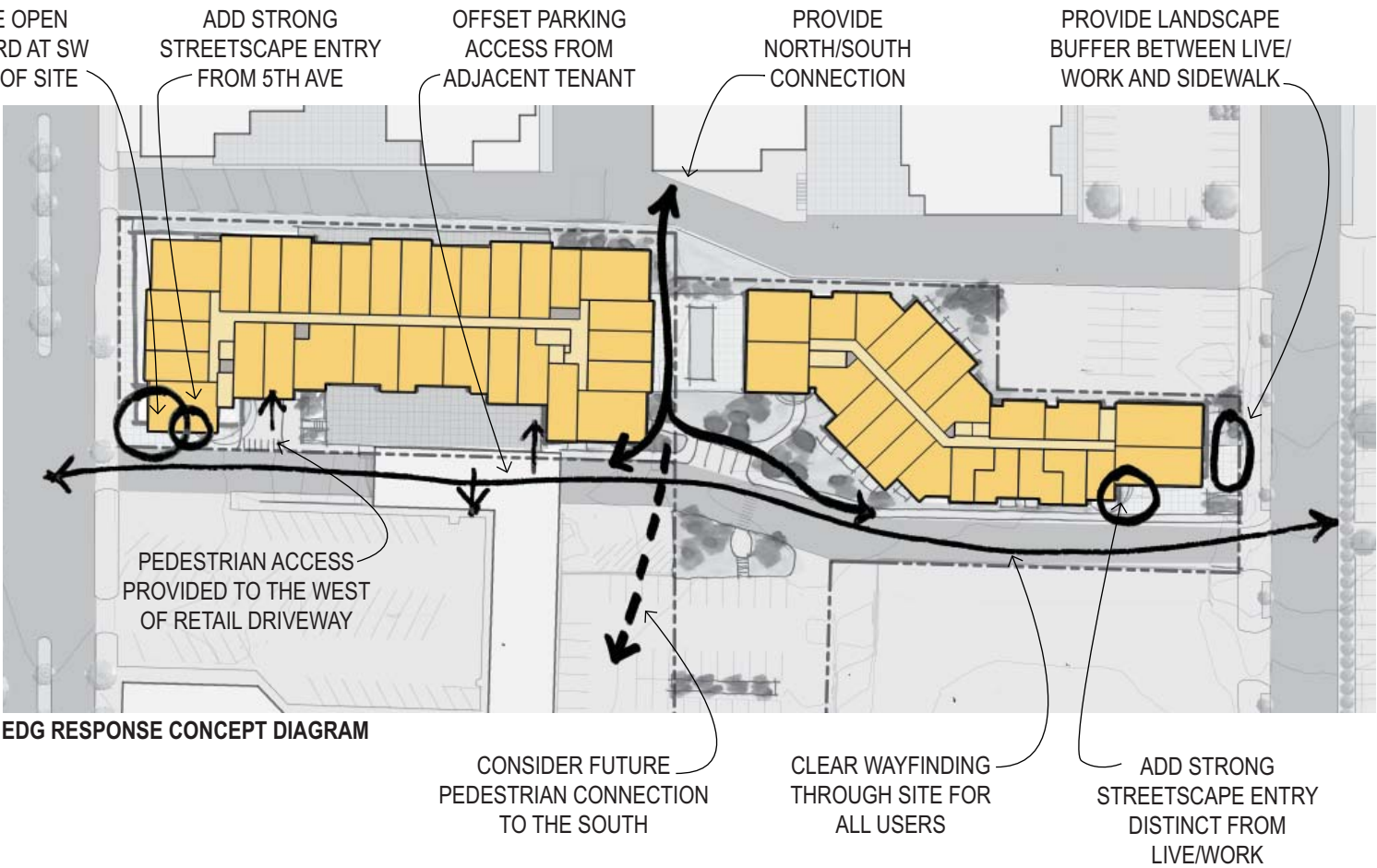


EDG OPTION B TYPICAL FLOOR PLAN



EDG OPTION C TYPICAL FLOOR PLAN

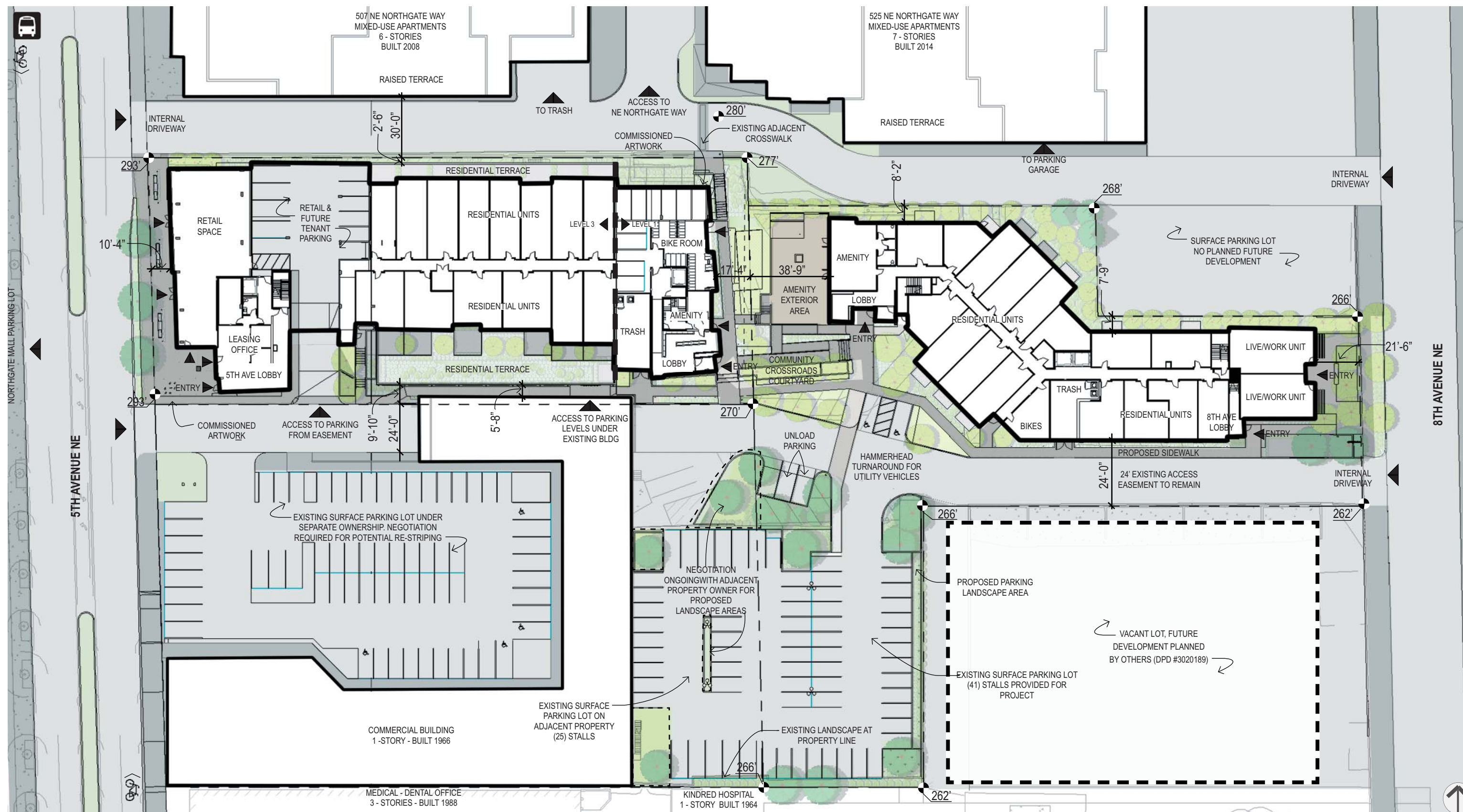
23.47A.005.C	Residential uses limited to 20% of street-level street-facing facades within the Northgate Overlay District.
23.47A.008 & 23.71.008	Blank street facade segments between 2'- 8' above the sidewalk may not exceed 20' in width. Maximum 40% of overall facade width allowed to be blank facades. Required street-level uses (retail) shall equal 60% minimum of frontage lot width. Required street level uses (retail) shall equal 80% minimum of structure width. Street-level street facing facades shall be located within 10 feet of the street lot line unless other approved open spaces are provided. (A minimum of a 12' wide sidewalk shall be provided per Northgate Overlay) Minimum of 60% total facade between 2'- 8' required to be transparent.
23.47A.013	FAR Limits: 40' height zone 3.00 / 3.25 (residential / non-residential) & 65' height zone 4.25 / 4.75 (residential / non-residential)
23.47A.016	Surface Parking Requirements: Landscaped area is required at surface parking areas per 23.47A.016.D1a and table C. Each area must be at least 100 sf and contained with permanent curbs or barriers. Width of area shall be no less than 4 feet.
23.47A.024	Amenity areas required in an amount equal to 5% of total gross floor area in residential use.
23.47A.032 & 23.71.016.F2a	Parking access for a lot abutting two streets shall be from one of the side street lot lines. Parking within a structure shall be separated from street-level street-facing facades by another permitted use.
23.54.015 & 23.71.016	No required parking for Residential use in Urban Center Non-residential use of general sales and service (retail) to meet minimum and maximum stall counts per table A for 23.71.016. A parking waiver for non-residential use for the first 2,500 sf in a structure is allowed per 23.54.015D3 and 23.71.016 At a major pedestrian street (5th Avenue), parking waived for the first 5,000 sf (maximum overall) for street-level retail sales and service uses.
23.71.014	Northgate Overlay District Open Space: NC3-40 zone requires 10% of site area for landscaping and usable public open space. NC3-65 zone requires 15% of site area for landscaping and usable public open space.
23.71.040	NC3-40 zone, density is limited to 1 unit / 600 sf of lot area, unless mixed use present then density increases by 20% NC3-65 zone has no density limit



DEPARTURE REQUEST SUMMARY

DEPARTURE#	LAND USE CODE	PARCEL	CODE ITEM	REQUIREMENT	DEPARTURE REQUESTED (SEE EXHIBITS FOR RATIONALES)
D1	SMC 23.47A.008.A3c (23.71.008.B4)	WEST	Street-level street-facing façade setbacks	Street-level street-facing façade setbacks of no more than 10 feet of street-lot line	Setbacks proposed range from 10' to 11'-7" along retail use frontage, and approximately 31'-0" at the SW corner for a publicly accessed courtyard area
D2	SMC 23.71.008.E3	WEST	Planting strips	Planting strips prohibited along Major Pedestrian Streets	Project proposes a planting strip along 5th Avenue NE between curb and sidewalk
D3	SMC 23.54.030.B2a	WEST	Parking space sizing	Maximum of 25% stalls to be "small", minimum 75% stalls to be "large"	Project proposes (7) non-residential stalls: (2) small / (4) large / (1) van accessible
D4	SMC 23.71.014.A2	WEST	Open space requirements	Minimum of 15% of lot area to be provided as Open Space	Project proposes 5321 SF of total Open Space, equalling 14.5% of lot area.
D5	SMC 23.47A.008.B3	WEST	Non-residential floor to floor heights	Non-residential street uses shall have a floor to floor height of 13'-0"	Project proposes a 12'-6" floor to floor height within the retail area
D6	SMC 23.71.014.B2b	WEST	Usable open space, barrier free access	Usable Open Space shall provide barrier free access	Barrier free access is not provided between project property and adjacent property to the North. Barrier free access is provided within the project site itself.
D7	SMC 23.47A.008.A2c	EAST	Blank walls	Total blank wall segments may not exceed 40% of facade length along street	Blank wall segments proposed total 26'-3", equalling 46.2% of facade length along street.
D8	SMC 23.47A.008.B2a	EAST	Transparency	60% of street-facing street-facade between 2' and 8' above sidewalk grade shall be transparent	Transparent area proposed is 123.91 SF, equalling 45.1% of street-facing street-facing facade.

COMPOSITE SITE PLAN



CONTEXT AND SITE

CS1: Natural Systems and Site Features - Use natural systems and features of the site and its surroundings as a starting point for project design by incorporating strategies for site orientation, ventilation, daylighting, existing land formations and vegetation, and natural drainage systems.

CS2.B: Adjacencies Site, Streets, and Open Spaces - Allow characteristics of sites to inform the design, especially where the street grid and topography create unusually shaped lots. Identify opportunities for the project to make a strong connection to the street. Contribute to the character and proportion of surrounding open spaces.

CS2.C: Relationship to Block, Mid-Block Sites - Continue a strong street-edge where it is already present, and respond to datum lines created by adjacent buildings. Where adjacent properties are underdeveloped, design the party walls to provide visual interest through materials, color, texture, or other means.

CS2.D: Height Bulk and Scale - Review the height, bulk, and scale of neighboring buildings and those anticipated by zoning to determine an appropriate complement and/or transition. Use changes in topography, site shape, and vegetation to successfully respond to adjacent properties. Provide an appropriate transition or complement to the adjacent zone(s). Create a step in perceived height, bulk and scale between the anticipated development potential of the adjacent zone and the proposed development. Respect adjacent properties with design and site planning to minimize disrupting the privacy and outdoor activities of residents in adjacent buildings.

PUBLIC LIFE

PL1.A: Connectivity - Design the building and open spaces to positively contribute to a broader network of opens spaces throughout the neighborhood and seek opportunities to foster human interaction.

PL2.D: Wayfinding - Use design features as a means of wayfinding and provide clear directional signage where needed.

1 RESPONSE: Project proposes signage oriented to vehicular traffic which is visible from the Avenues (parapet and facade locations) and also within the site for clear passage to designated long and short term parking areas (parking signage and monument signage). For pedestrians, the use of human scaled street lighting poles and predictable pathway lighting mark the pedestrian connectivity paths. Entries are marked with articulated awnings containing unique building signage. Specialty paving at important path transitions is also proposed.

PL3.A1: Entries, Design Objectives - Design primary entries to be obvious, identifiable, and distinctive with clear lines of sight and lobbies visually connected to the street. Differentiate residential and commercial entries with design features and amenities specific to each.

PL3.A2: Entries, Ensemble of Elements - Design the entry as a collection of coordinated elements including the door(s), overhead features, ground surface, landscaping, lighting, and other features.

2 RESPONSE: Residential building entries are provided off of the 5th and 8th Avenue NE Streetscapes to give tenants and guests direct access to the residential entries from the public right of way. Additional building entries are located mid-site within the Community Crossroads courtyard engaging the public along the internal pedestrian connectivity pathways. To distinguish commercial / live-work entries with residential entries, strategies of differing awning designs, signage configuration, and architectural building massing is used.

PL3.B: Residential Edges - Provide security and privacy for residential buildings through the use of a buffer or semi-private space between the development and the neighboring streets / buildings. This is particularly important in buildings with ground-level housing, both at entries and where windows are located overlooking the street and sidewalk.

3 RESPONSE: The live / work uses have a 21’ setback from the sidewalk along 8th Avenue NE to preserve the live / work tenant’s personal space as well as providing a greenspace buffer fronting the public realm. In addition, buffer plantings areas are provided along both edges of the live / work patio spaces between future development to the North and the pedestrian access to the residential tower.

PL3.B3: Buildings with Live/Work Uses - Use active and transparent facades and face the non-residential portions of the live/work unit toward the street.

4 RESPONSE: The live / work entries face 8th Avenue NE and have each has direct unique access points from the sidewalk. The commercial uses within the unit front 8th Avenue NE and maintain large front windows and transparency while still creating wall opportunities for business signage and lighting.

PL3.C: Retail Edges - Engage passersby with opportunities to interact with the building interior using glazing and transparency. Allow spaces for activities to extend into the public areas.

5 RESPONSE: Along 5th Avenue NE, the retail frontage maintains direct access from the sidewalk. At the SW corner of the site, open space is provided to separate the residential entry and leasing office from the retail frontage. This open space is utilized for public art / sculpture display.

PL4.A: Entry Locations and Relationships - Provide safe and convenient access points for all modes of travel. Site the primary entry in a location that logically relates to the building uses and clearly connects all major points of access.

6 RESPONSE: Residential building entries are provided off of the 5th and 8th Avenue NE Streetscapes to give tenants and guests direct access to the residential entries from the public right of way. The additional mid-site entries within the Community Crossroads courtyard support the idea that the access easement drive is a “street” by bringing activity to the length of the drive, bringing “eyes on the street” to the central intersection of the publicly accessed North/South and East/West pedestrian connections, strong connections to drop-off and pick-up points, and strong inter-building connections for tenants sharing amenities such as social areas and structured parking.

DESIGN GUIDELINES: RESPONSE



RESPONSE TO GUIDELINES: PLAN OF CENTER ARRIVAL COURT

- | | | | | | |
|---|-----------------|---------------------------------|----|-----------------------|-----------------------------|
| 1 | PL1.A / PL2.D | Connectivity and Wayfinding | 6 | PL4.A | Entry Relationships |
| 2 | PL3.A1 / PL3.A2 | Entries and entry elements | 7 | DC1.B | Vehicular Circulation |
| 3 | PL3.B | Live / Work - Residential Edges | 8 | DC1.C4 | Service Location and Access |
| 4 | PL3.B3 | Live / Work - Facade | 9 | PL1.II/PL2.III/DC1.II | Mid Block Connections |
| 5 | PL3.C | Retail Edges | 10 | PL3.III / PL3.V | Retail Streetscape |

DESIGN CONCEPT

DC1.B: Vehicular Access and Circulation - Choose locations for vehicular access, service uses and delivery areas that minimize conflict between vehicles and non-motorists.

7 **RESPONSE:** All vehicular parking access points are from the easement drive. The retail vehicular parking is located to the East of the retail, leasing office, and residential entry locations so that pedestrian users have direct access to those building amenities from the street without crossing vehicular traffic. The residential structured parking access is located so that is offset from the existing adjacent commercial space to avoid light and glare and protect that entry from traffic.

DC1.C4: Parking and Service Uses - Locate and design service entries away from pedestrian areas or to less visible areas to reduce the impact of these uses on circulation.

8 **RESPONSE:** Due to the presence of the easement drive and the narrow street frontage of the project site, utility uses are placed along the easement for access. Entries are placed as far away from the pedestrian pathways as possible while still providing convenient access for service. The tunnel clearance over the easement does not allow a refuse and recycling vehicle to pass under it. Utility vehicles will need to access the site from 8th Avenue NE to access the service areas for both the East and West buildings. A 60' long hammerhead turnaround space with 20' radius turns is provided so that utility trucks can turn around within the site. Trash pick-up locations and the turn-around access have been reviewed and approved by Seattle Public Utilities.

NORTHGATE DESIGN GUIDELINES

PL1.II: Interior Block Pedestrian Connections - Encourage pedestrian walkways and open spaces. The Northgate Transit Center/future light rail station and adjacent mixed-use transit-oriented development (TOD) with its urban plaza and access to the Thornton Creek Water Quality Channel as well as the Northgate Civic Center and Hubbard Homestead Park are important pedestrian destinations.

PL2.III: Superblock Development - Development goals should include creating a pedestrian friendly environment with street-level commercial uses, connecting to the building to the streetscape, and linking larger sites together with lively pedestrian connections and internal vehicular drives.

DC1.II: Large Scale “Super Block” Development - A network of pedestrian connections should provide walkways to adjacent sites as well as within the site itself. The elements of these walkways shall include lighting, pavement, and plantings.

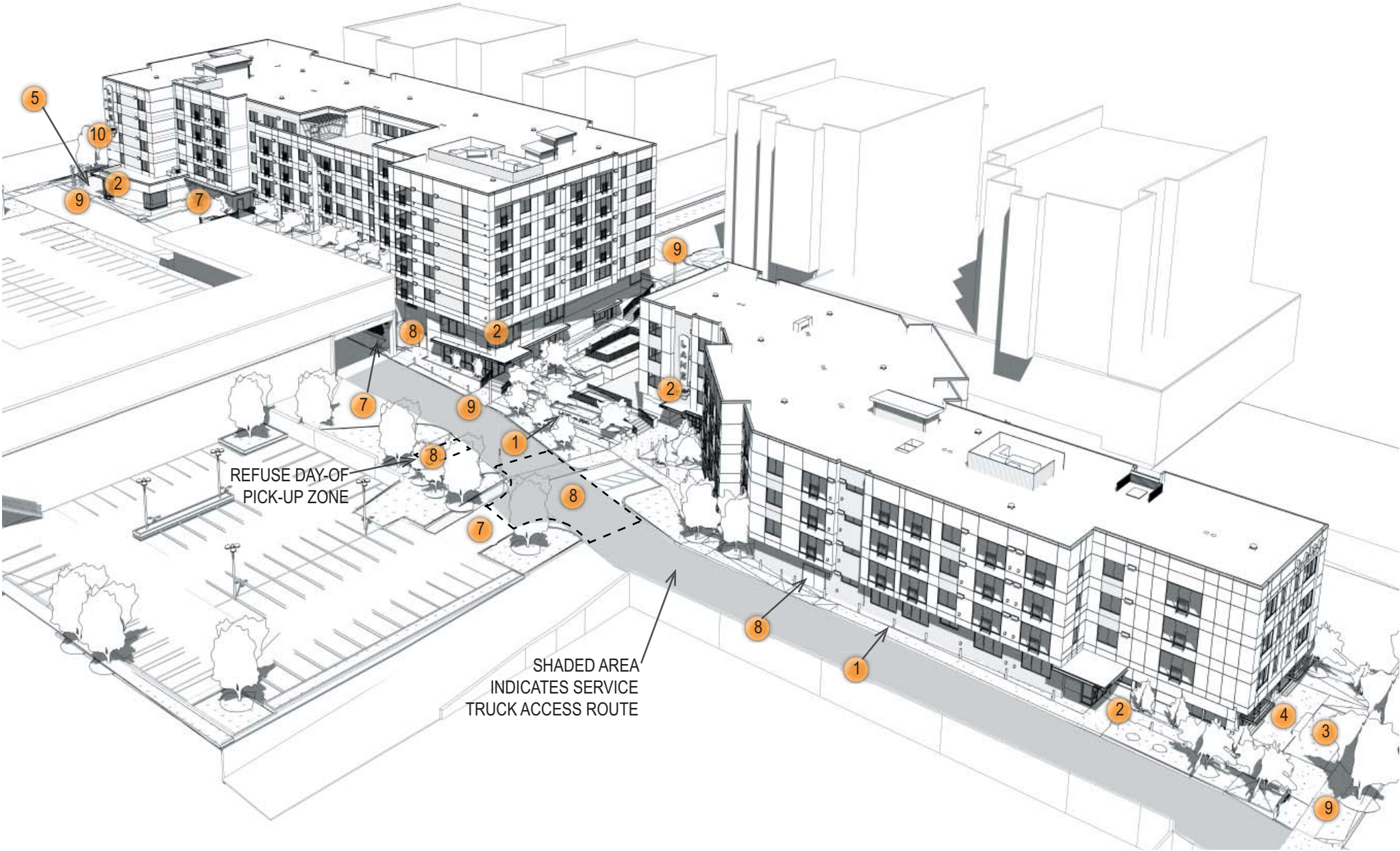
9 **RESPONSE:** Project proposes both a North/South and a East/West pedestrian connection through the site. The North/South connectivity connects the 525 Northgate Way existing sidewalk and intended crosswalk to an active intersection with the East/West connection within the Community Crossroads courtyard mid-site. The East/West pathway connects 8th Avenue to 5th Avenue and follows the easement drive which is intended to be an internal “street” enriched with pedestrian friendly elements such as benches, lighting, and plantings.

PL3.III: Street-level Transparency - Transparency is desirable to provide interaction between the interior occupants and people near the building edge on the street and sidewalk through direct visual connections.

PL3.V: Commercial and Mixed-Use Buildings - Ground floors of buildings should be inviting to the public with open spaces directly connected to the sidewalk and with pedestrian friendly elements such as the use of trees, exterior lighting, and overhead weather protection.

10 **RESPONSE:** 5th Avenue streetscape is envisioned as an active retail sidewalk continuing the pedestrian friendly experience as developed to the North. Project proposes the use of overhead weather protection, clear retail signage, transparency beyond code minimums along the sidewalk and around building corners to eliminate blank wall conditions to the North along the adjacent access drive and opening to the South at the SW corner plaza.

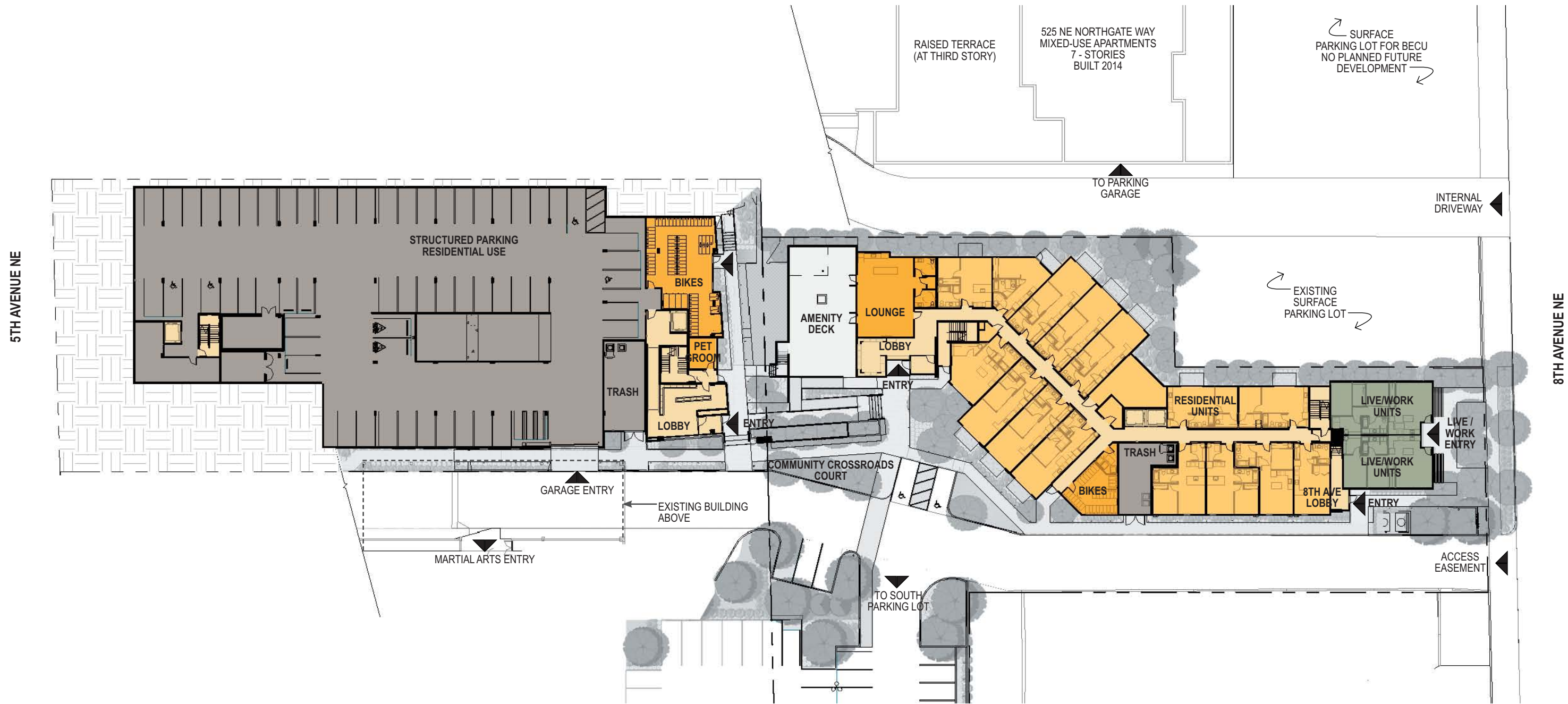
DESIGN GUIDELINES: RESPONSE

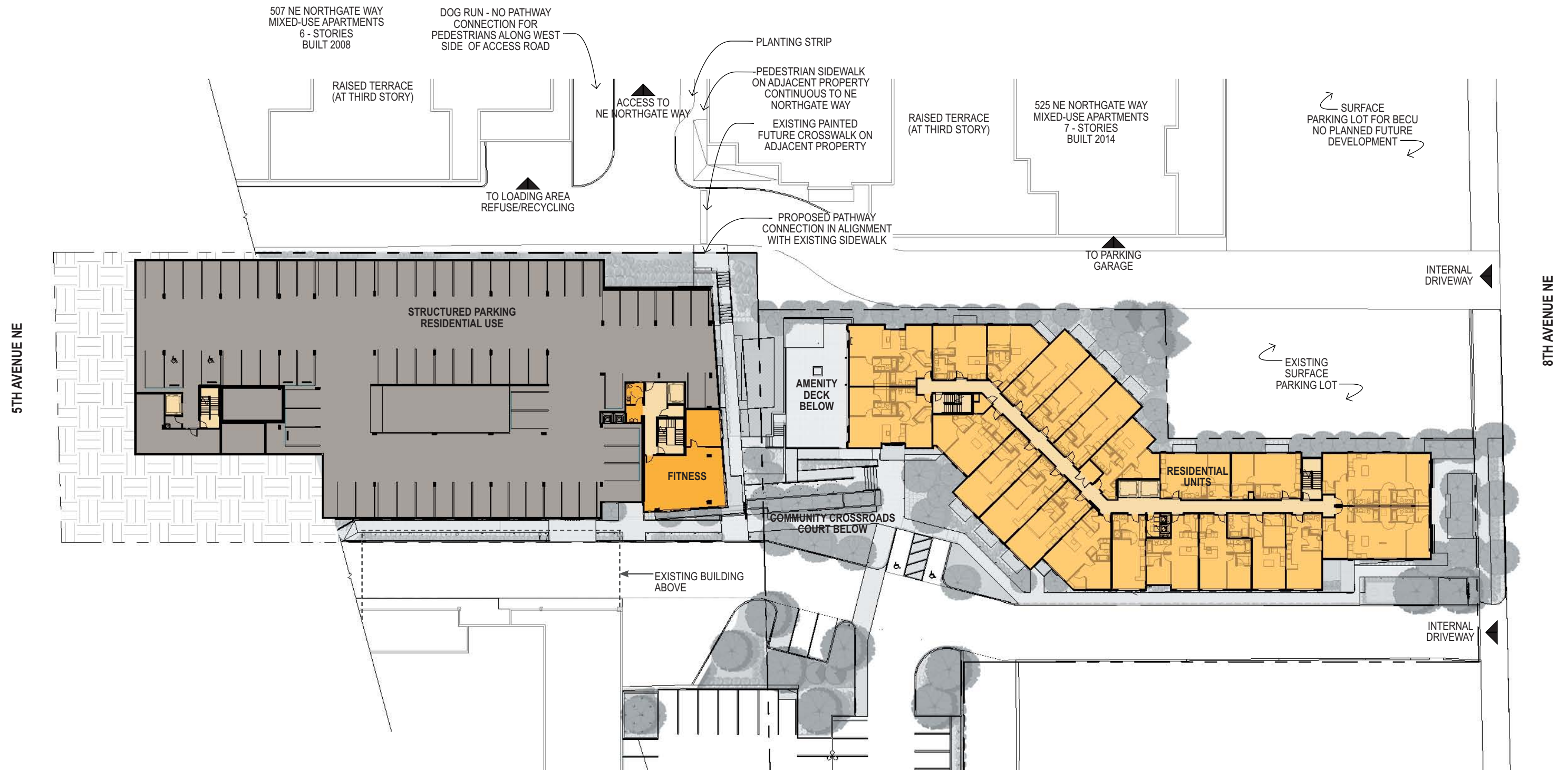


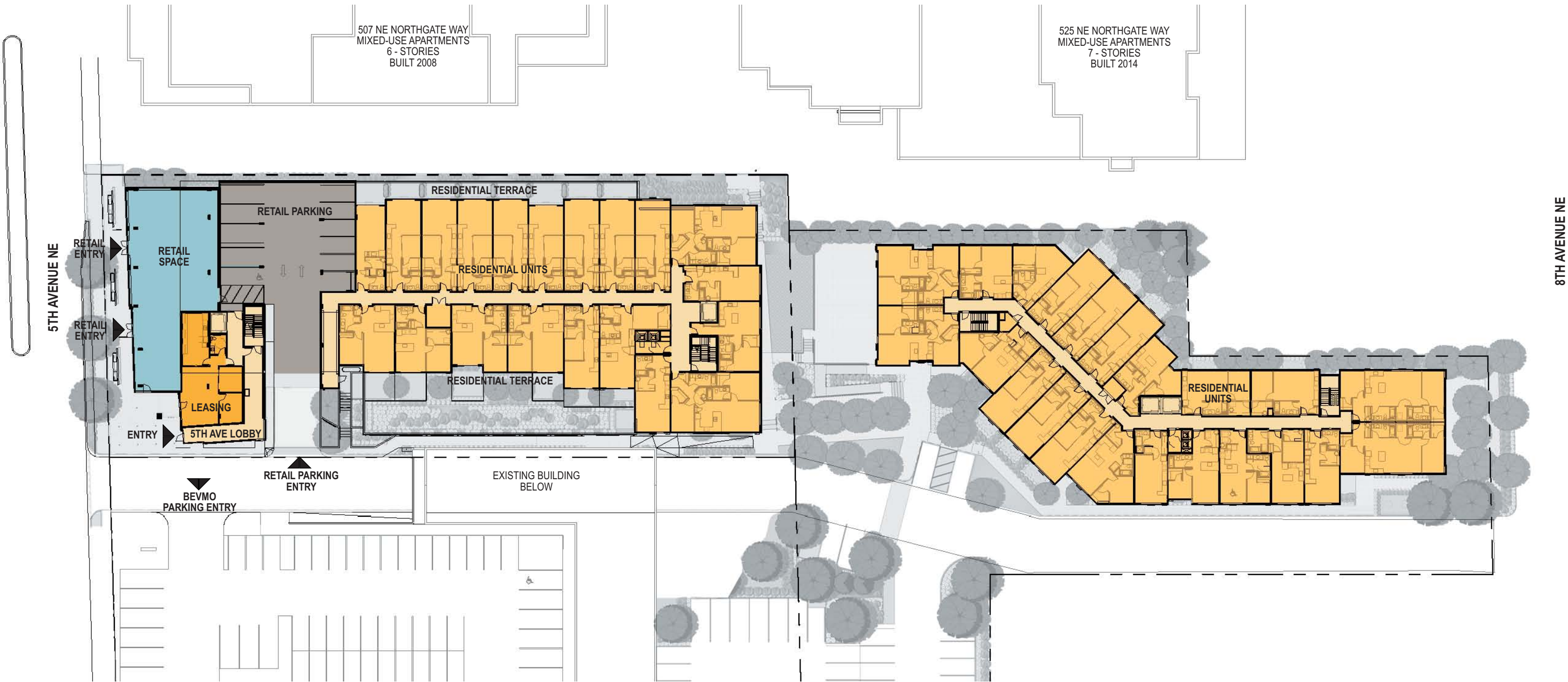
RESPONSE TO GUIDELINES: AXON OF PROJECT SITE LOOKING FROM SE CORNER

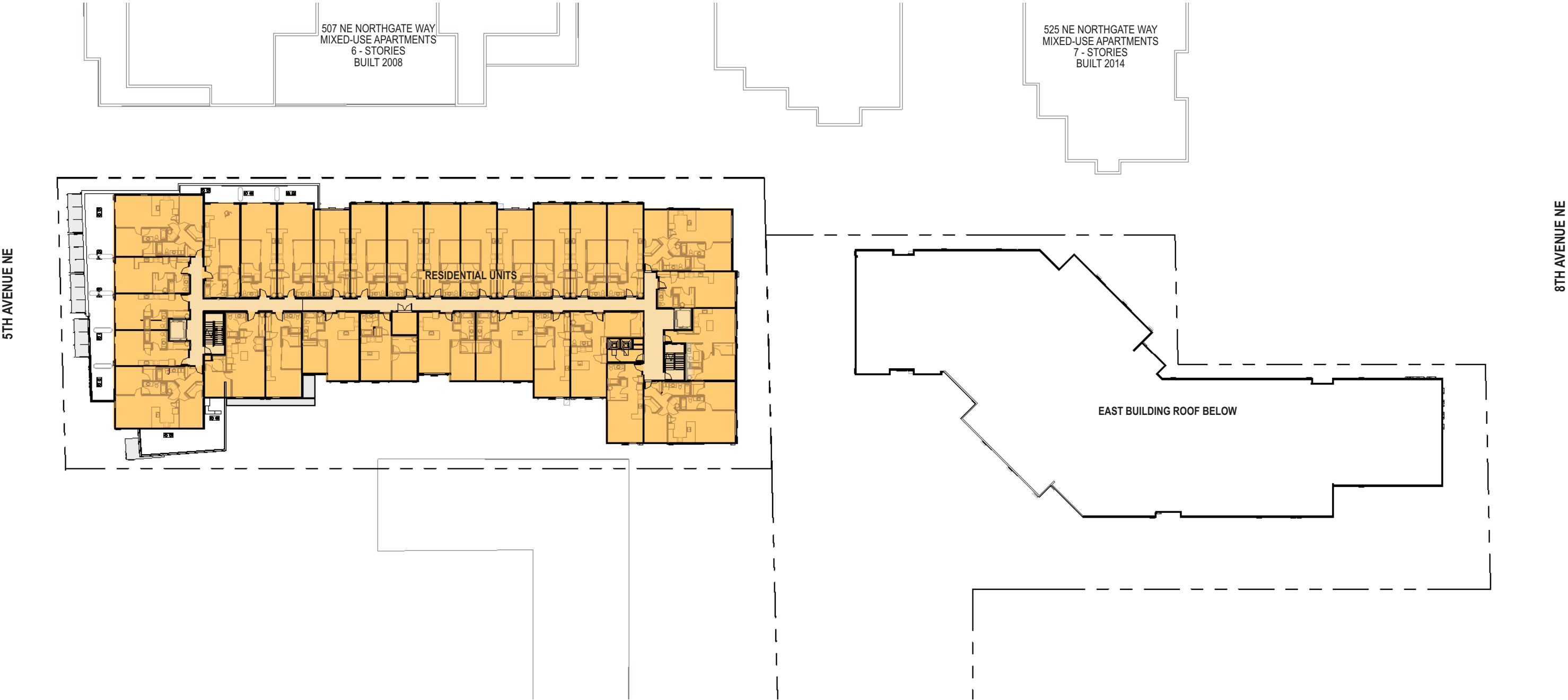
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LANDSCAPE PLANTINGS & CONCEPT IMAGES



RED FLOWERING CURRENT



COMMON WHITE SNOWBERRY



COLUMNAR YEW



PRIVET HONEYSUCKLE



SWEET BOX



VINE MAPLE



FOERSTER'S REED GRASS



SPANISH LAVENDER



CAREX



ICE DANCE JAPANESE SEDGE



DWARF EVERGREEN DAY LILY



WESTERN SWORD FERN



BAILEY RED TWIGGED DOGWOOD



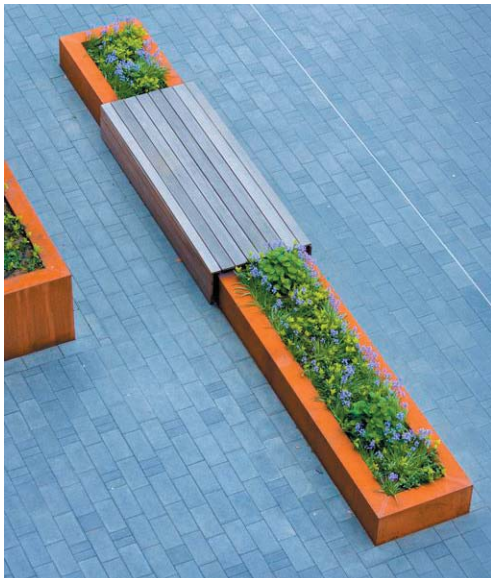
SEDUM GREEN ROOF



GREENSCREENS: PERFORATED DESIGNS INCORPORATED INTO GREENSCREEN ELEMENTS



RETAIL & SITE BENCHES: INCORPORATE PLANTINGS, WOOD SEATING, & SEATING RAIL



ART PIECE AT SW CORNER COURTYARD: USE OF LIGHT WITHIN FEATURE & STRONG VERTICAL COMPOSITION



RAINGARDEN AT LIVE/WORK COURTYARD





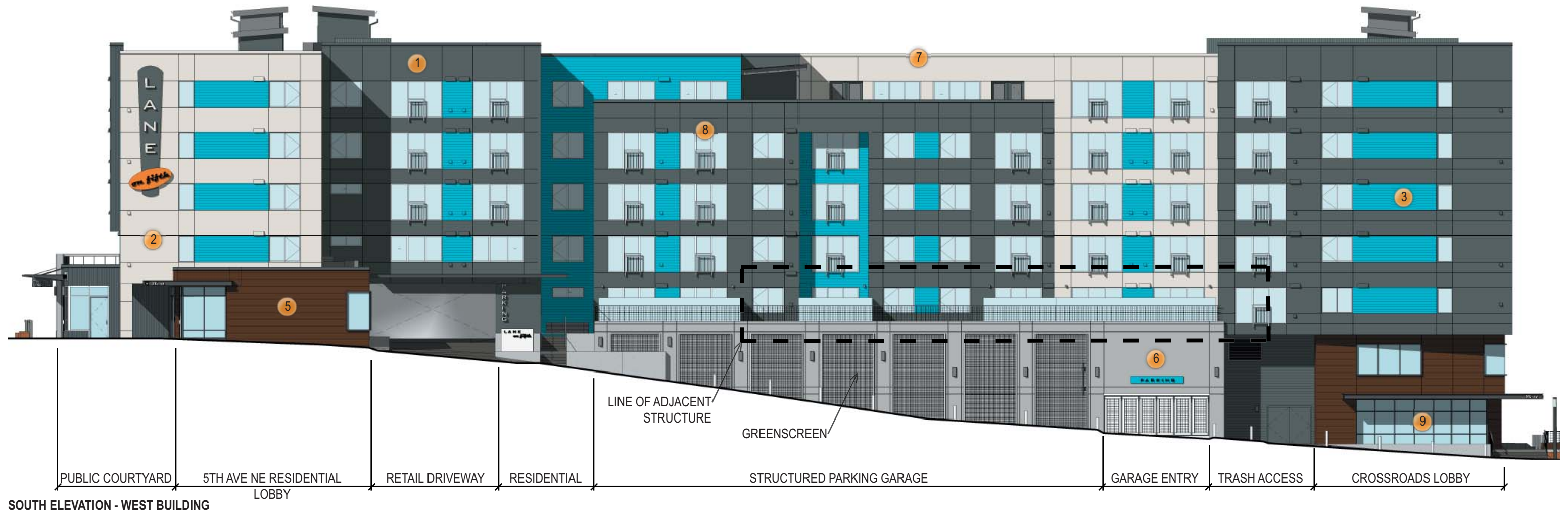
ELEVATIONS & CONCEPTS: WEST BUILDING



RETAIL AWNING: EXPOSED STEEL FRAME AND STAINED WOOD T&G SOFFIT



WOOD SIDING: HORIZONTAL PANEL CONFIGURATION WITH EXPRESSED HORIZONTAL REVEALS. ALIGNMENT WITH WINDOW "PUNCHED" OPENINGS



ELEVATIONS & CONCEPTS: WEST BUILDING



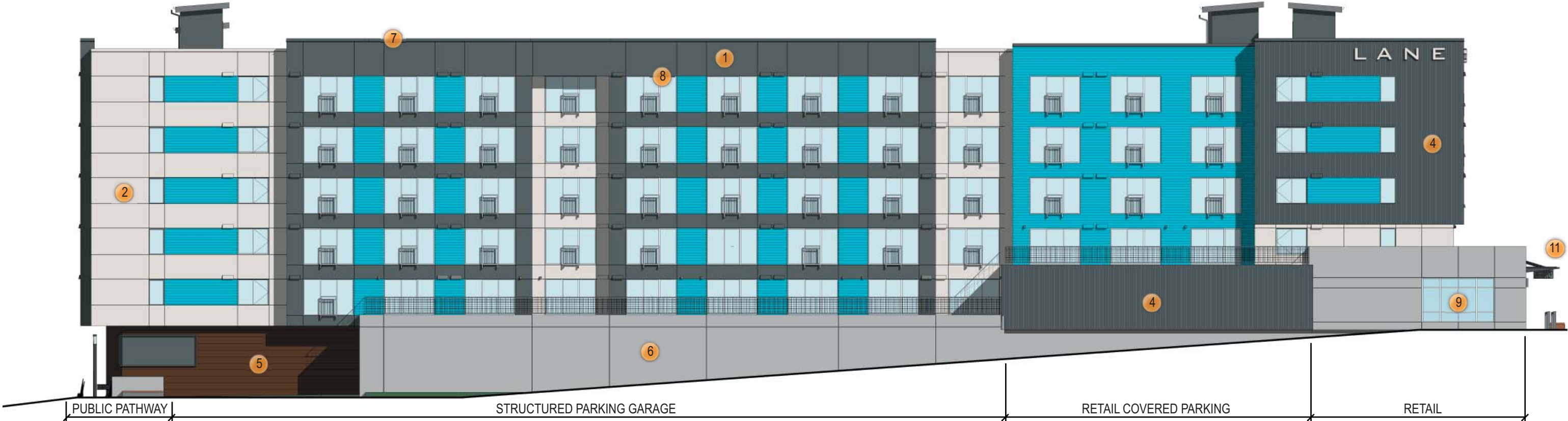
EAST ELEVATION - WEST BUILDING



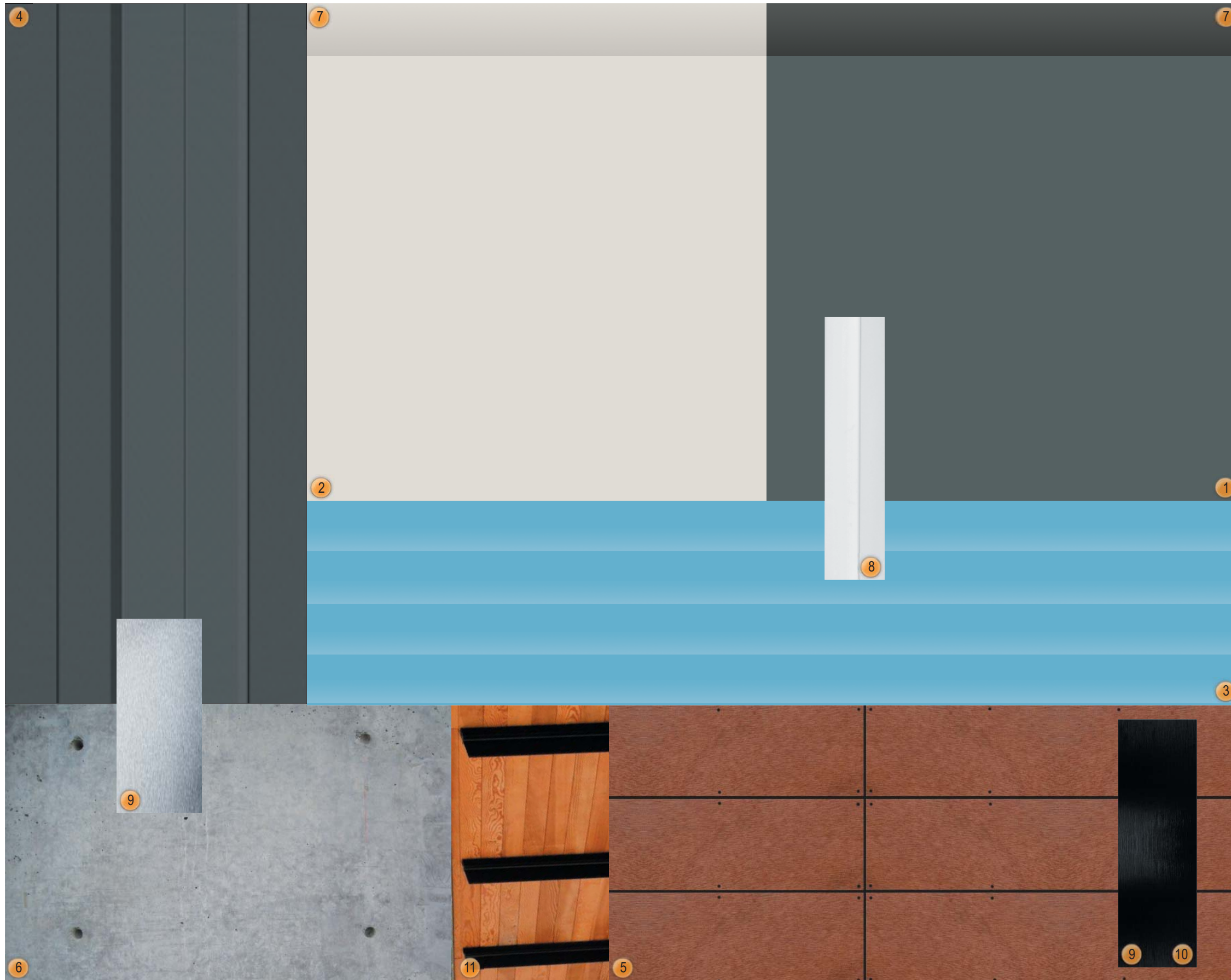
ENTRY AWNINGS: EXPRESSED STEEL C-CHANNEL FRAME WITH FLUSH SOFFIT AND RECESSED LIGHTING



FENESTRATION SURROUND: DEEP STOREFRONT FRAMES AT FEATURE LOCATIONS TO EXAGGERATE PUNCH OPENINGS



NORTH ELEVATION - WEST BUILDING



MATERIAL BOARD: WEST BUILDING

- 1 **CEMENT PANEL SIDING - BODY**
Cement board panel siding with prefabricated flashing reveals and a closed-joint rainscreen system. Paint color is "Roycroft Pewter", SW-2848 by Sherwin Williams.
- 2 **CEMENT PANEL SIDING - BODY**
Cement board panel siding with prefabricated flashing reveals and a closed-joint rainscreen system. Paint color is "Toque White", SW-7003 by Sherwin Williams.
- 3 **CEMENT BOARD LAP SIDING ACCENT**
Cement board lap siding with prefabricated flashing reveals and a closed-joint rainscreen system. Paint color is "Fountain", SW-6787 by Sherwin Williams.
- 4 **METAL PANEL SIDING**
Metal panel 12" vertical siding. Flat pan, no reveal, with 2 pencil ribs. Prestige Series "Cool Zatique II", by AEP-Span
- 5 **WOOD PANEL SIDING**
High density laminate timber panel with expressed horizontal reveals. Panel color is "Copper" Facade, by Parklex
- 6 **EXPOSED CONCRETE WALLS**
Architectural concrete walls with cast reveal joints.
- 7 **FLASHINGS & CAPS**
Metal to match adjacent material colorings
- 8 **RESIDENTIAL WINDOWS**
Vinyl windows, color to be "white"
- 9 **STOREFRONT WINDOWS**
Storefront window system, color to be "clear" at retail locations
Storefront window system, color to be "standard dark bronze" at residential lobbies and common areas.
- 10 **RAILINGS, AWNINGS, & SCREENS**
Metal railings, awnings, and screening components to match standard dark bronze storefront system
- 11 **RETAIL AWNING**
Metal awning structure with wood soffit



ELEVATIONS & CONCEPTS: EAST BUILDING



WEST ELEVATION - EAST BUILDING



FACADE TREATMENT: RECESSED FACADE WITH CONTRASTING MATERIAL SURROUND

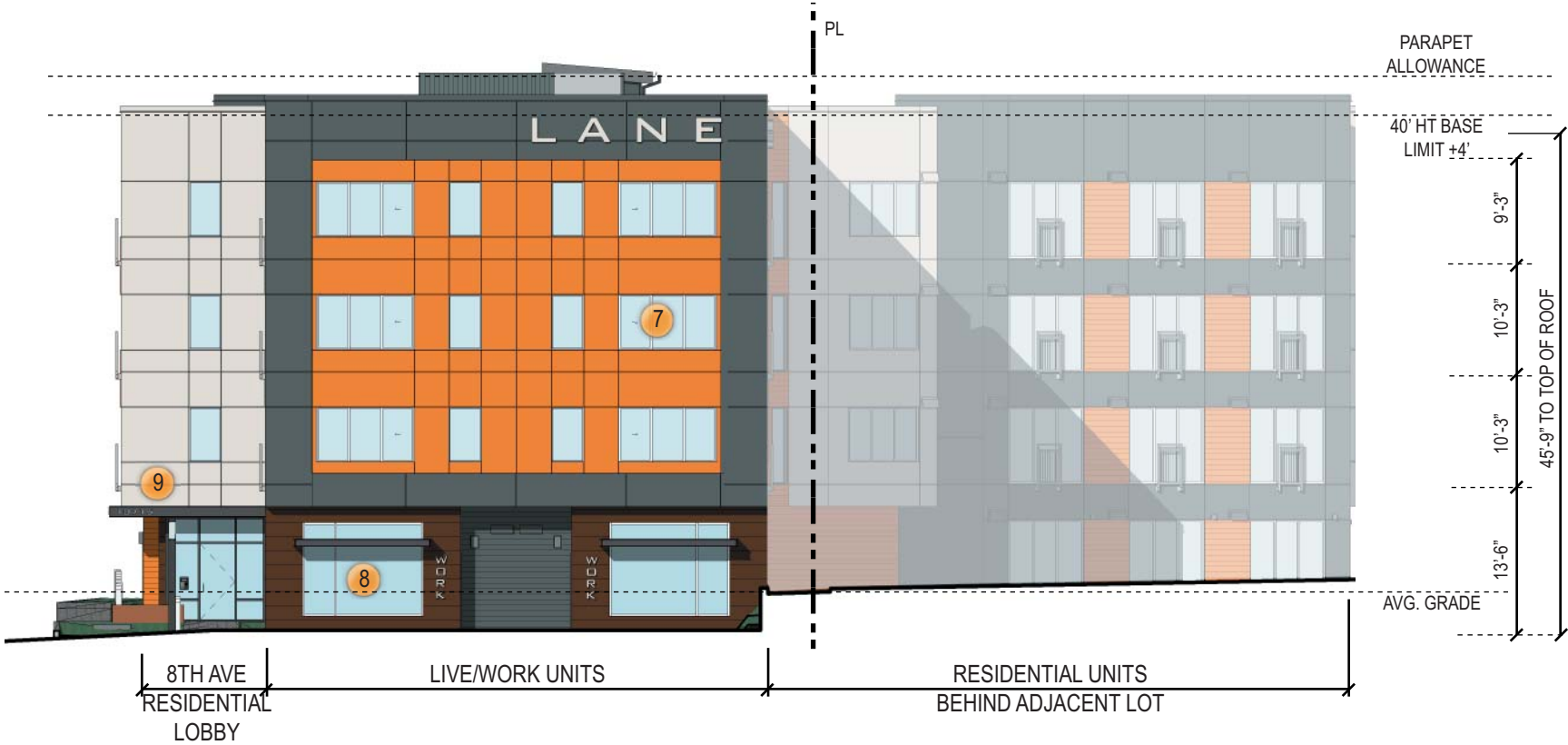


RESIDENTIAL ENTRY: RECESSED FULLY GLAZED STOREFRONT ENTRY WITH AN EXTENDED AWNING ENGAGING THE PATHWAY APPROACH



SOUTH ELEVATION - EAST BUILDING

ELEVATIONS & CONCEPTS: EAST BUILDING



EAST ELEVATION - EAST BUILDING



LIVE/WORK AWNINGS: STEEL SCREENING ALLOWING FILTERED LIGHT ALONG COMMERCIAL USE



LIVE/WORK ENTRY: SHARED RECESSED ENTRY



NORTH ELEVATION - EAST BUILDING



MATERIAL BOARD: EAST BUILDING

- 1 **CEMENT PANEL SIDING - BODY**
Cement board panel siding with prefabricated flashing reveals and a closed-joint rainscreen system. Paint color is "Roycroft Pewter", SW-2848 by Sherwin Williams.
- 2 **CEMENT PANEL SIDING - BODY**
Cement board panel siding with prefabricated flashing reveals and a closed-joint rainscreen system. Paint color is "Toque White", SW-7003 by Sherwin Williams.
- 3 **CEMENT BOARD LAP SIDING ACCENT**
Cement board lap siding with prefabricated flashing reveals and a closed-joint rainscreen system. Paint color is "Amber Wave", SW-6657 by Sherwin Williams.
- 4 **WOOD PANEL SIDING**
High density laminate timber panel with expressed horizontal reveals. Panel color is "Copper" Facade, by Parklex
- 5 **EXPOSED CONCRETE PLANTERS**
Architectural concrete walls with cast reveal joints.
- 6 **FLASHINGS & CAPS**
Metal to match adjacent material colorings
- 7 **RESIDENTIAL WINDOWS**
Vinyl windows, color to be "white"
- 8 **STOREFRONT WINDOWS**
Storefront window system, color to be "clear" at livework locations
Storefront window system, color to be "standard dark bronze" at residential lobbies and common areas.
- 9 **RAILINGS, AWNINGS, & SCREENS**
Metal railings, awnings, and screening components to match standard dark bronze storefront system



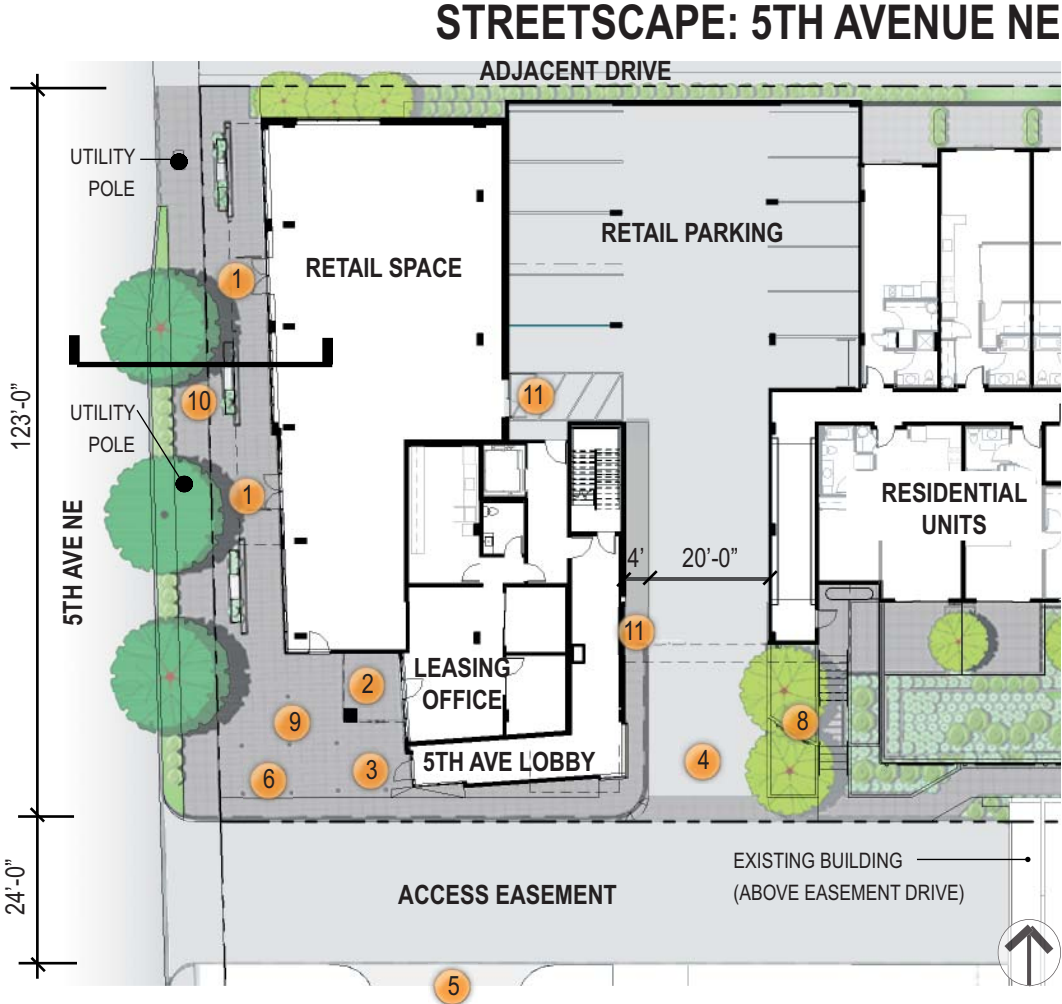






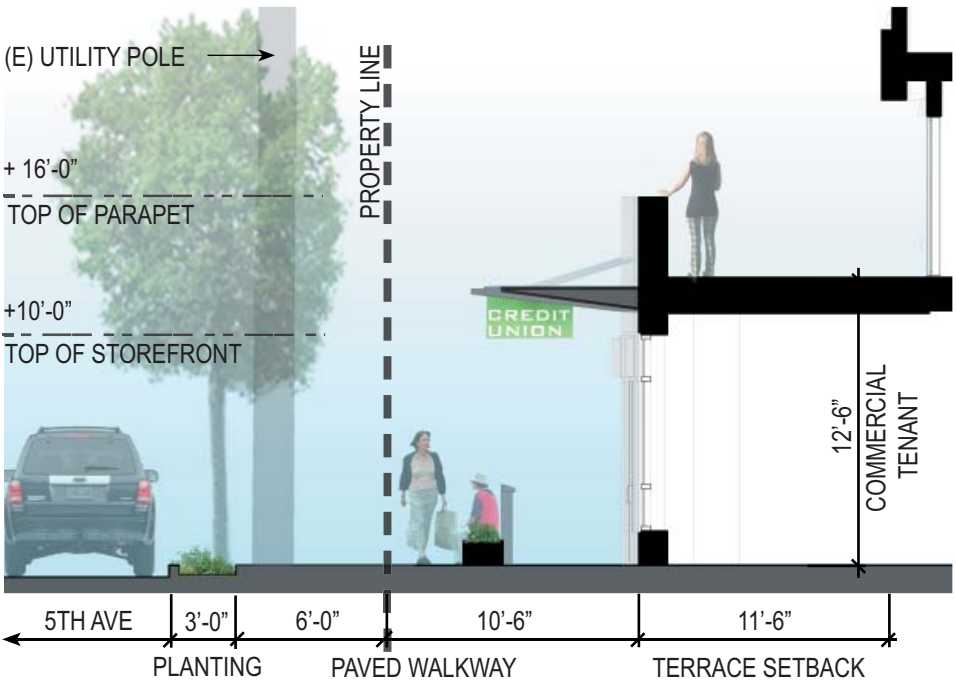
STREETSCAPE ALONG 5TH AVENUE NE LOOKING SOUTH

- 1 COMMERCIAL TENANT ENTRY
- 2 LEASING OFFICE ENTRY
- 3 RESIDENT / GUEST ENTRY
- 4 RETAIL PARKING ENTRY
- 5 ADJACENT RETAIL PARKING LOT ENTRY
- 6 COMMISSIONED ARTWORK
- 7 PRIVATE RESIDENT TERRACE BALCONIES
- 8 BRANDING & WAYFINDING SIGNAGE
- 9 SCORED CONCRETE PAVING
- 10 PUBLIC SEATING AND PLANTERS
- 11 RETAIL CUSTOMER PATHWAYS



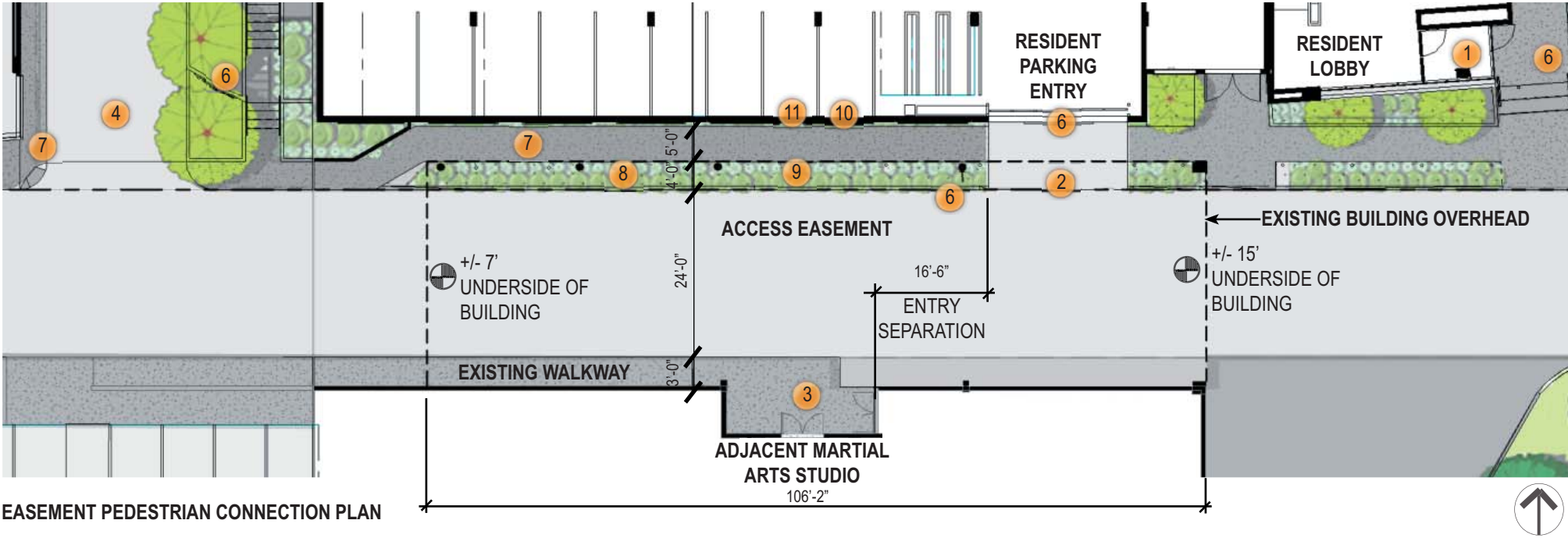
5TH AVENUE STREETSCAPE LOOKING NORTHEAST

STREETSCAPE & PLAZA PLAN ALONG 5TH AVENUE NE



5TH AVENUE SECTION

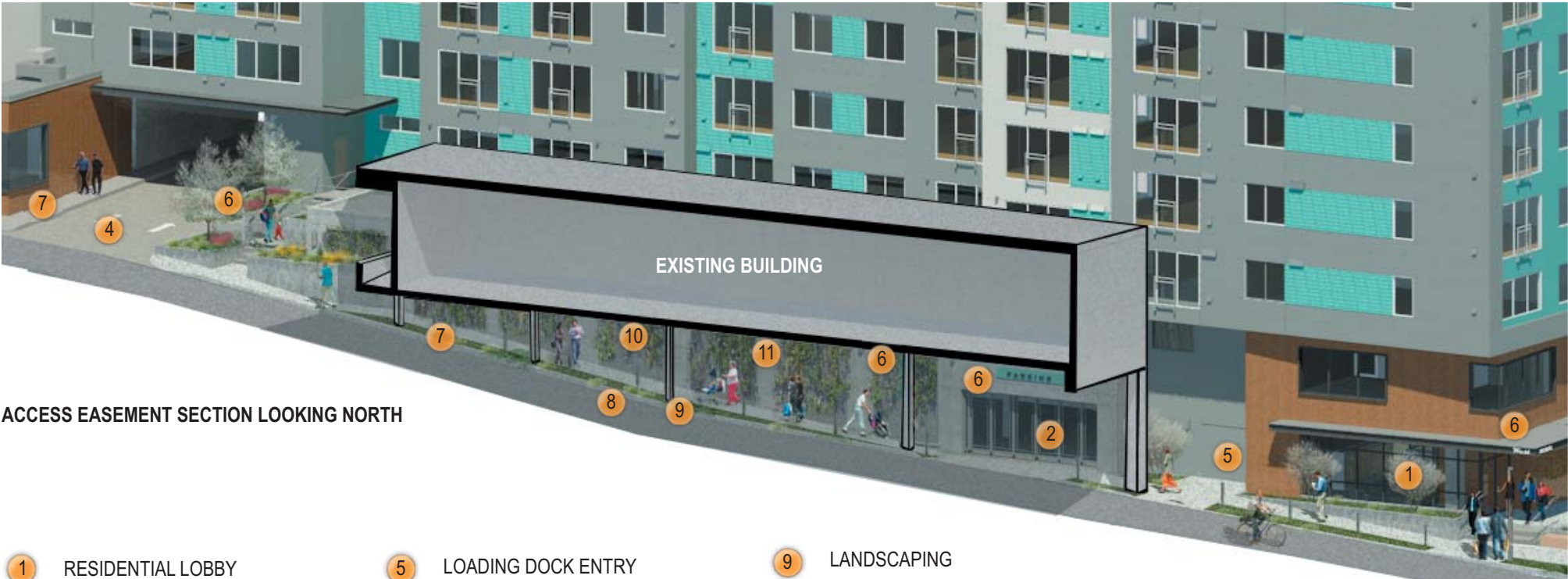
COMMUNITY CONNECTION: ACCESS EASEMENT



EASEMENT PEDESTRIAN CONNECTION PLAN



PEDESTRIAN CONNECTION ALONG ACCESS EASEMENT LOOKING EAST



ACCESS EASEMENT SECTION LOOKING NORTH

- | | | |
|-----------------------------|---|------------------------------|
| 1 RESIDENTIAL LOBBY | 5 LOADING DOCK ENTRY | 9 LANDSCAPING |
| 2 RESIDENTIAL PARKING ENTRY | 6 WAYFINDING SIGNAGE | 10 GREENSCREEN WALL COVERING |
| 3 ADJACENT TENANT ENTRY | 7 PEDESTRIAN SIDEWALK | 11 BUILDING LIGHTING |
| 4 RETAIL PARKING ENTRY | 8 ILLUMINATED BOLLARDS / PATHWAY LIGHTING | |



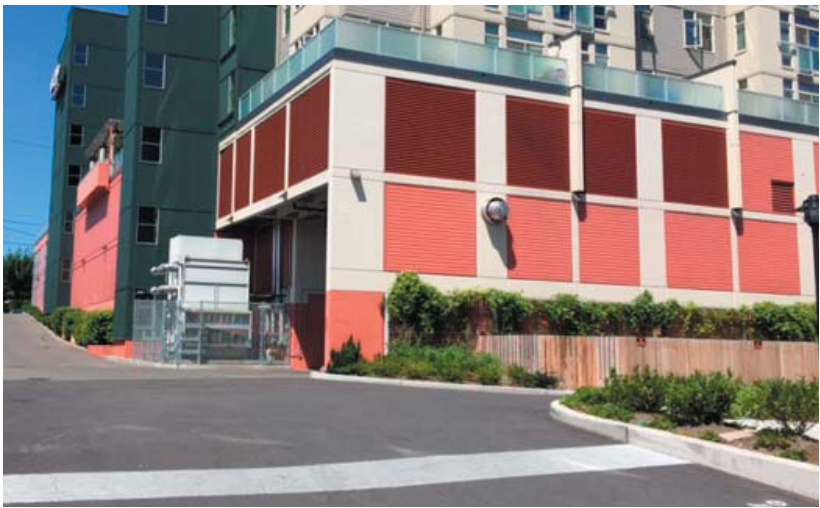
PEDESTRIAN CONNECTION ALONG ACCESS EASEMENT LOOKING WEST



COMMUNITY CROSSROADS: CENTER COURT BIRDSEYE OVERVIEW



- 1 RESIDENTIAL LOBBY
- 2 NORTH PEDESTRIAN CONNECTION
- 3 RESIDENTS OUTDOOR AMENITY
- 4 RESIDENT PET GROOMING ENTRY
- 5 RESIDENT DOG RUN
- 6 BIKE ROOM ENTRY
- 7 RESIDENT FITNESS STUDIO
- 8 BRANDING / ARRIVAL SIGNAGE
- 9 PEDESTRIAN STREET LIGHT POLE
- 10 ILLUMINATED PATHWAY LIGHTING
- 11 SCORED ACCENT CONCRETE PAVING
- 12 BENCH SEATING
- 13 ACCESSIBLE PARKING
- 14 RESIDENTIAL PARKING ENTRY
- 15 LOAD / UNLOAD PARKING STALLS
- 16 SOUTH PARKING LOT PEDESTRIAN CONNECTION
- 17 COMMISSIONED ARTWORK
- 18 REFUSE DAY OF SERVICE PICK-UP ZONE



ADJACENT PROPERTY: SE CORNER OF 507 NORTHGATE WAY



ADJACENT PROPERTY: EXISTING SIDEWALK LOOKING NORTH



ADJACENT PROPERTY: EXISTING "FUTURE" CROSSWALK





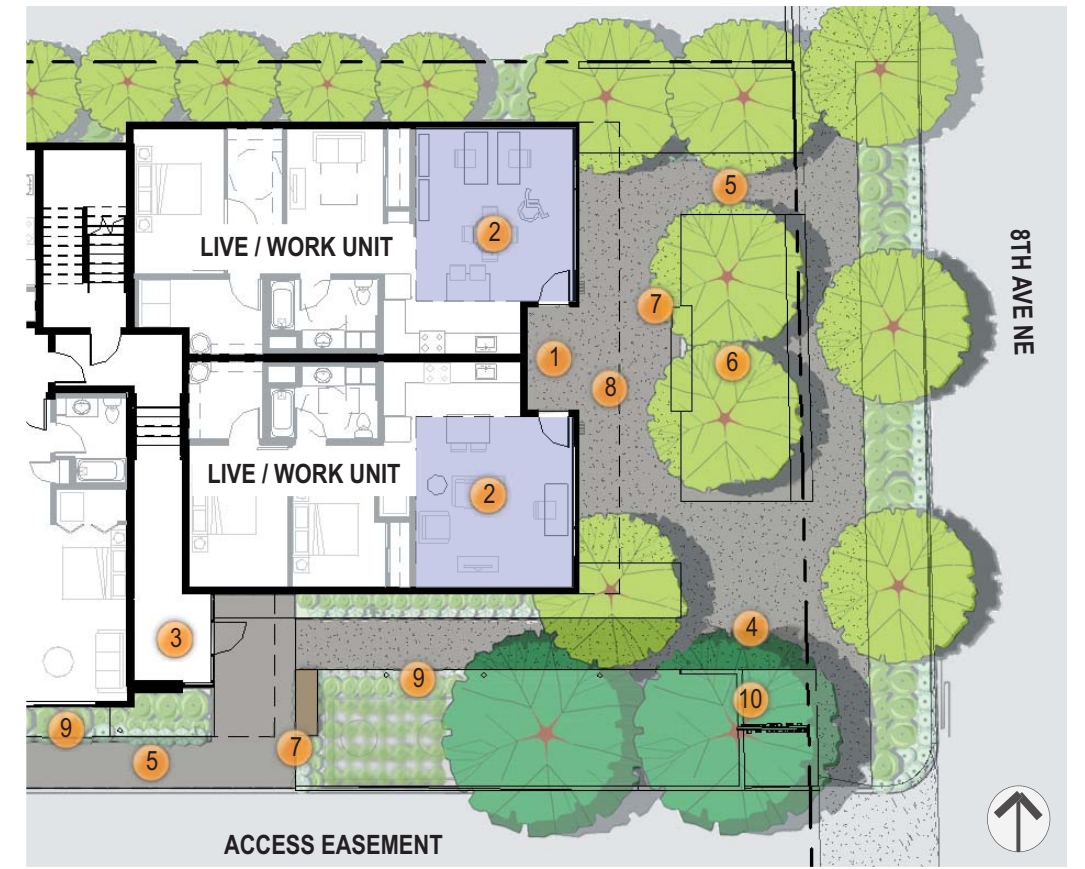




8TH AVENUE NE: LIVE / WORK PLAZA

STREETSCAPE: 8TH AVENUE NE - LIVE / WORK UNITS & ENTRIES

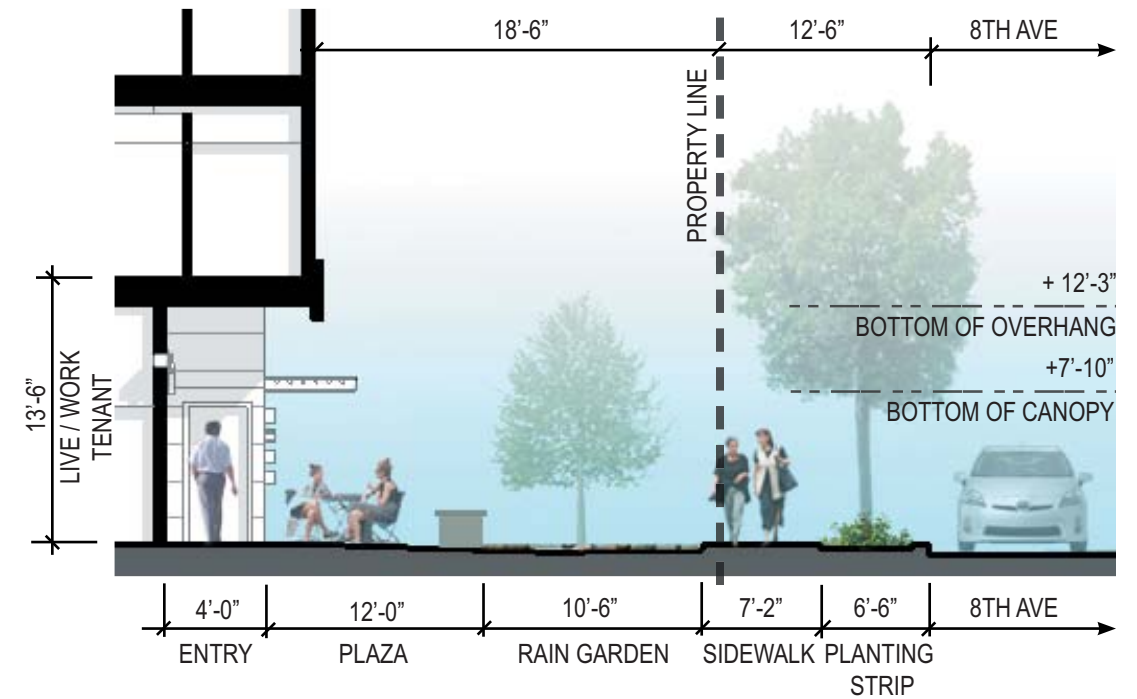
- 1 LIVE / WORK ENTRY
- 2 LIVE / WORK COMMERCIAL AREA
- 3 8TH AVE LOBBY
- 4 LARGE PAVED PEDESTRIAN ENTRY
- 5 PEDESTRIAN SIDEWALK
- 6 RAIN GARDEN
- 7 BENCH SEATING
- 8 SCORED ACCENT CONCRETE PAVING
- 9 ILLUMINATED BOLLARDS / PATHWAY LIGHTING
- 10 MONUMENT SIGNAGE
- 11 BRANDING / WAYFINDING SIGNAGE



8TH AVENUE STREETSCAPE PLAN



8TH AVENUE NE: ENTRIES



8TH AVENUE SECTION

Northgate Apartments

10720 5th Ave NE & 10715 8th Ave NE, Seattle, WA 98125

GRE / NOP Northgate LLC

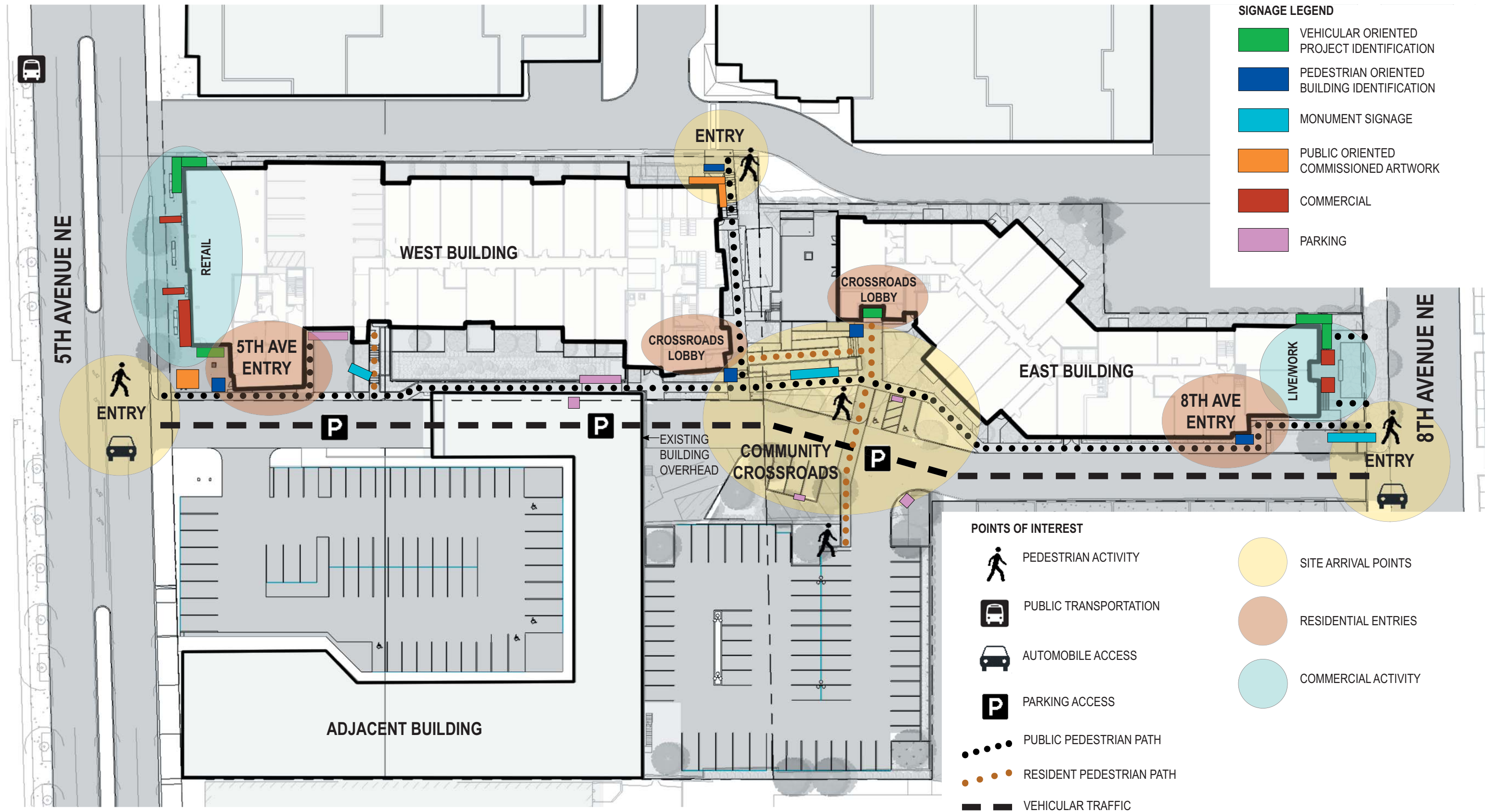
Design Review Recommendation Meeting
October 19, 2015

37

grouparchitect 

www.grouparch.com

WAYFINDING PLAN



SIGNAGE TYPES & CONCEPTS



CONCEPT IMAGE

VEHICLE ORIENTED PROJECT IDENTIFICATION: LOCATED ON PARAPETS AND SEEN FROM SOUTHBOUND 5TH AVENUE NE AND SOUTHBOUND 8TH AVENUE NE

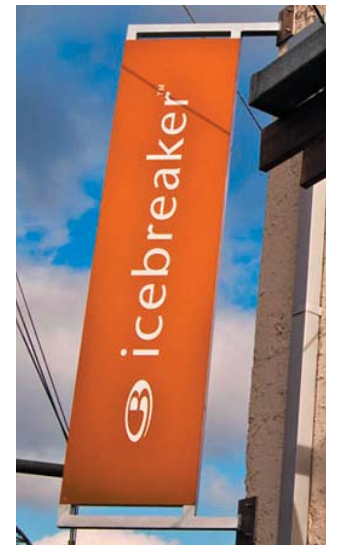


CONCEPT IMAGE

VEHICLE ORIENTED PROJECT IDENTIFICATION: LOCATED ON FACADES AND SEEN FROM NORTHBOUND 5TH AVENUE NE AND ACCESS DRIVEWAY WITHIN CENTRAL ENTRY. SIGNAGE WILL INCORPORATE OWNER'S LOGO, INSPIRED BY 1950'S ICONIC GRAPHICS AND FONTS



VEHICLE ORIENTED PARKING SIGNAGE: LOCATED ON BUILDING FACADES NEAR DRIVEWAY OPENINGS, PLACED AS BLADE SIGNAGE ON AN EASEMENT COLUMN, AND ON STANDALONE SITE SIGNAGE



CONCEPT IMAGE



CONCEPT IMAGE

PEDESTRIAN ORIENTED ADDRESS/BUILDING IDENTIFICATION: LOCATED AT EACH BUILDING ENTRY AWNING, MARKING THE UNIQUE ADDRESS AND UNIQUE NAME OF EACH BUILDING



VEHICLE & PEDESTRIAN ORIENTED MONUMENT SIGNAGE: LOCATED ON NEAR SIDEWALKS AND DRIVEWAYS MARKING THE ENTRY TO THE ACCESS DRIVEWAY AND ARRIVAL WITHIN THE CENTRAL COURT

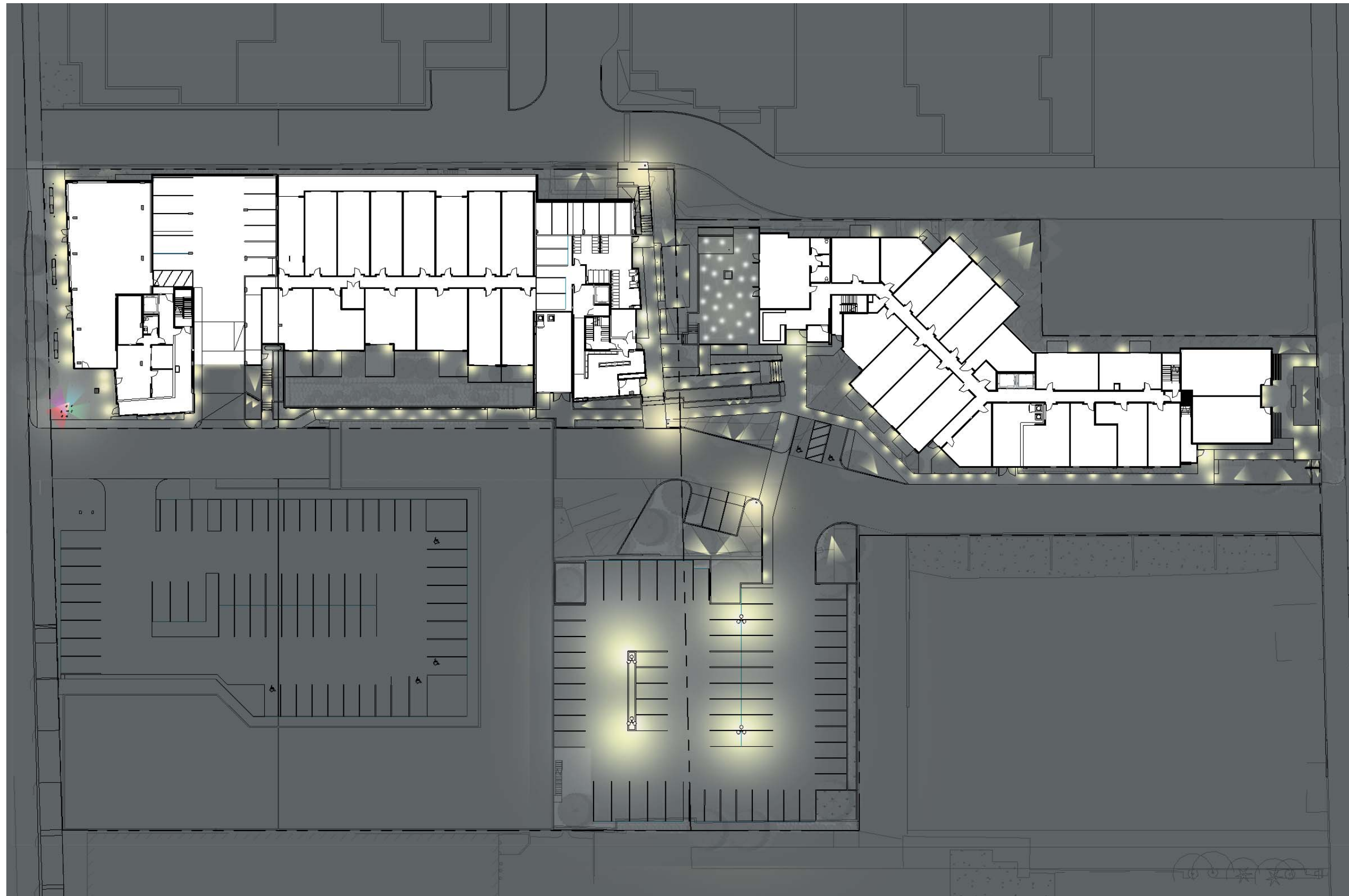


CONCEPT IMAGE



COMMERCIAL LIVEWORK SIGNAGE: LOCATED ON THE FACADE FACING 8TH AVENUE NE NEAR THE LIVEWORK UNIT ENTRIES VISIBLE FROM THE SIDEWALK

EXTERIOR LIGHTING PLAN



FESTIVAL LIGHTING



WALL SCONCE
UNIT PATIOS



RECESSED
SOFFIT CAN



STEP LIGHT



WALL SCONCE
UP/DOWN
LIGHTING



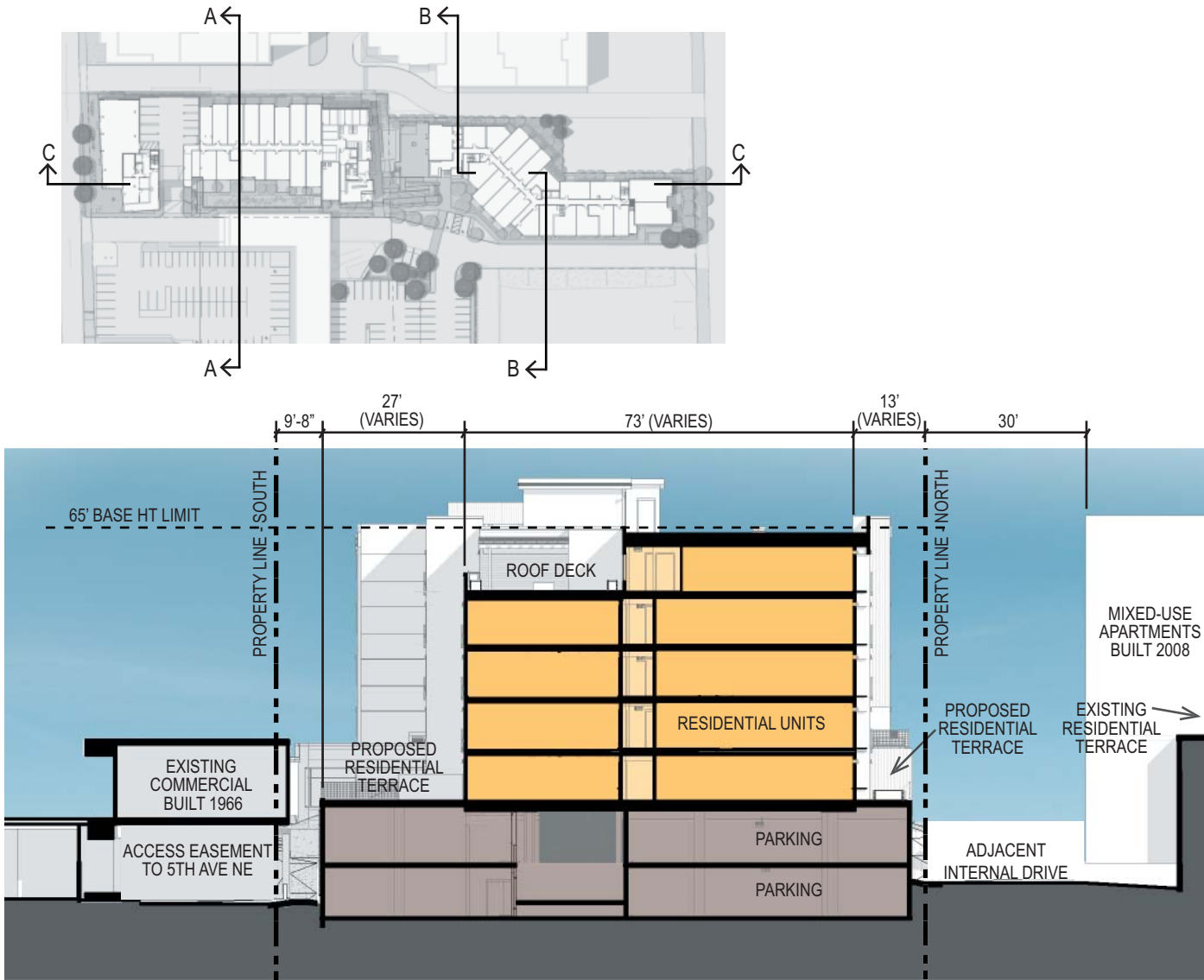
PATHWAY
BOLLARD



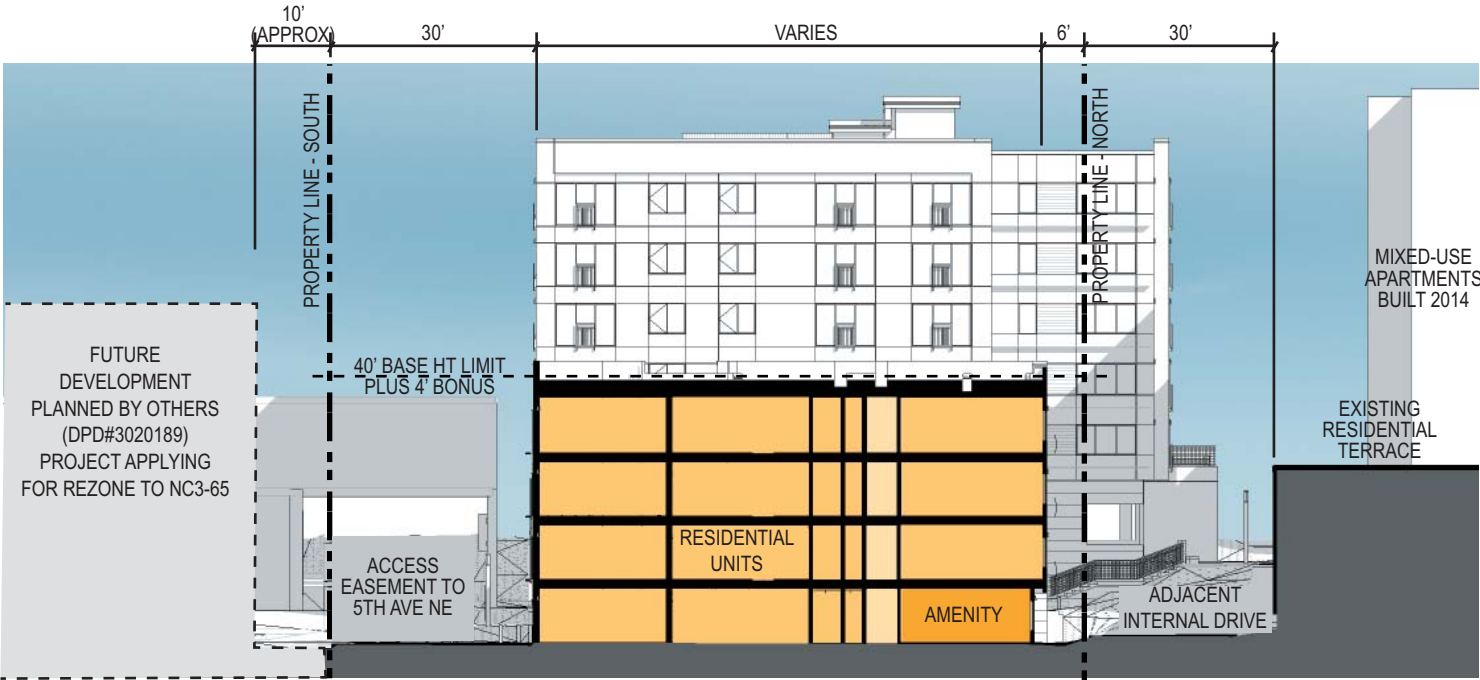
PARKING
POLE LIGHT



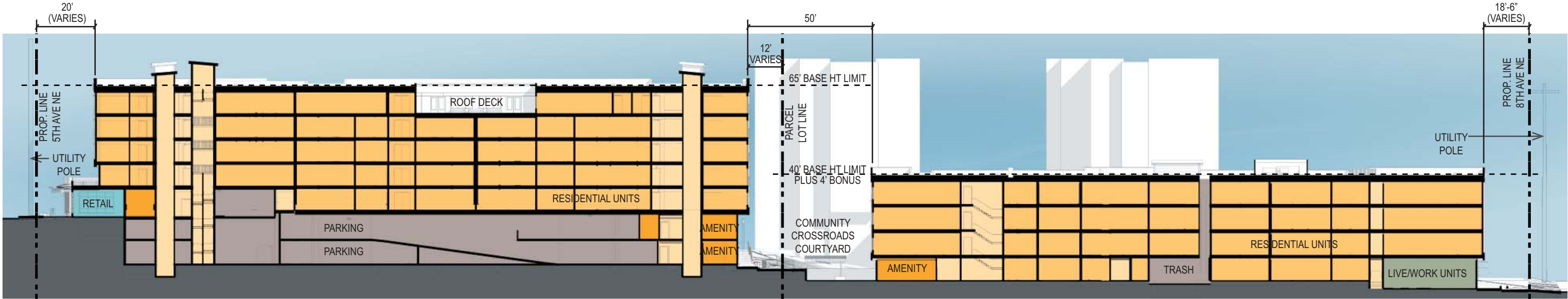
PEDESTRIAN
POLE LIGHT



SECTION A-A: WEST BUILDING SECTION THROUGH EASEMENT DRIVEWAY









SECTION B-B: EAST BUILDING SECTION





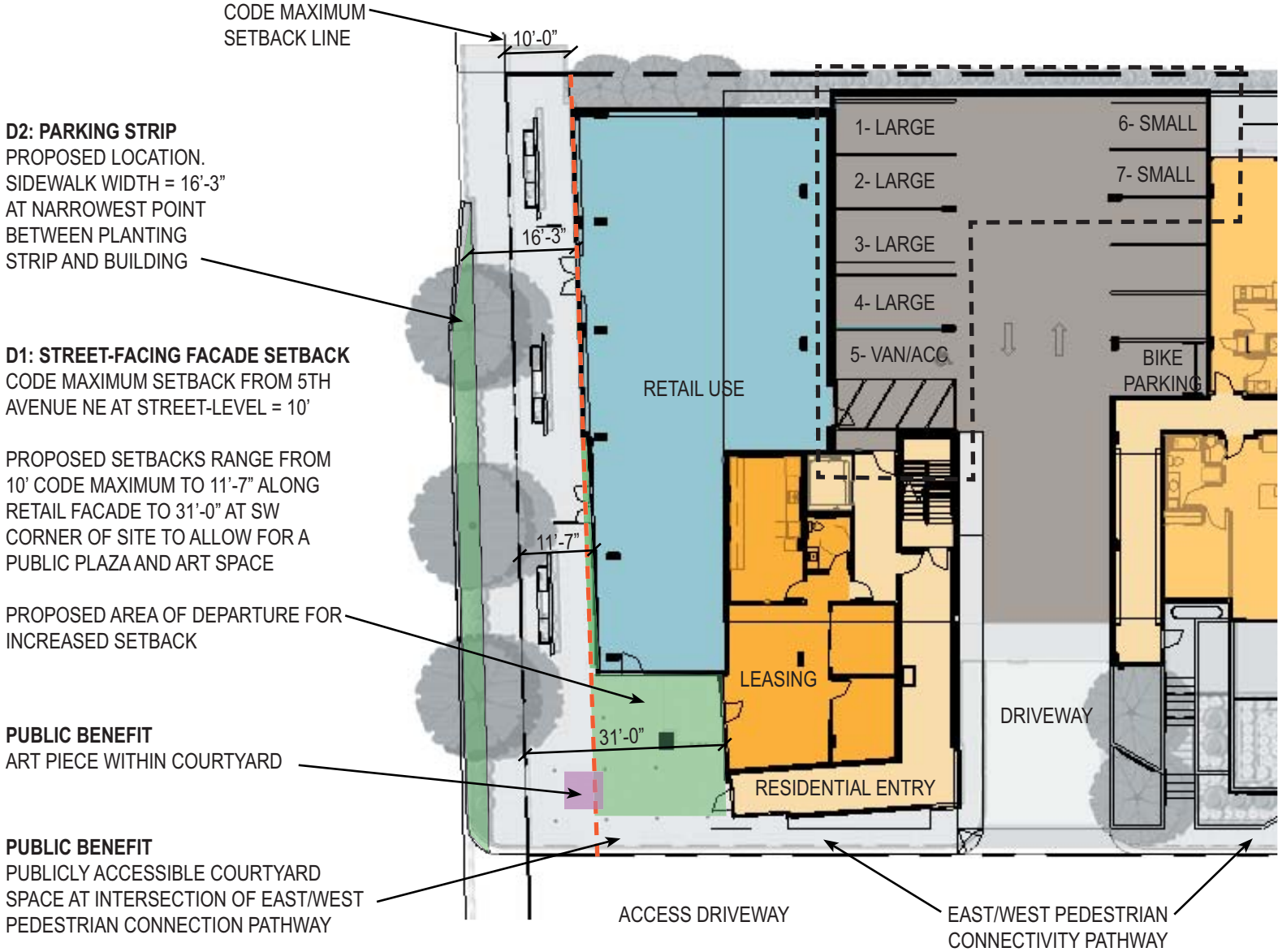
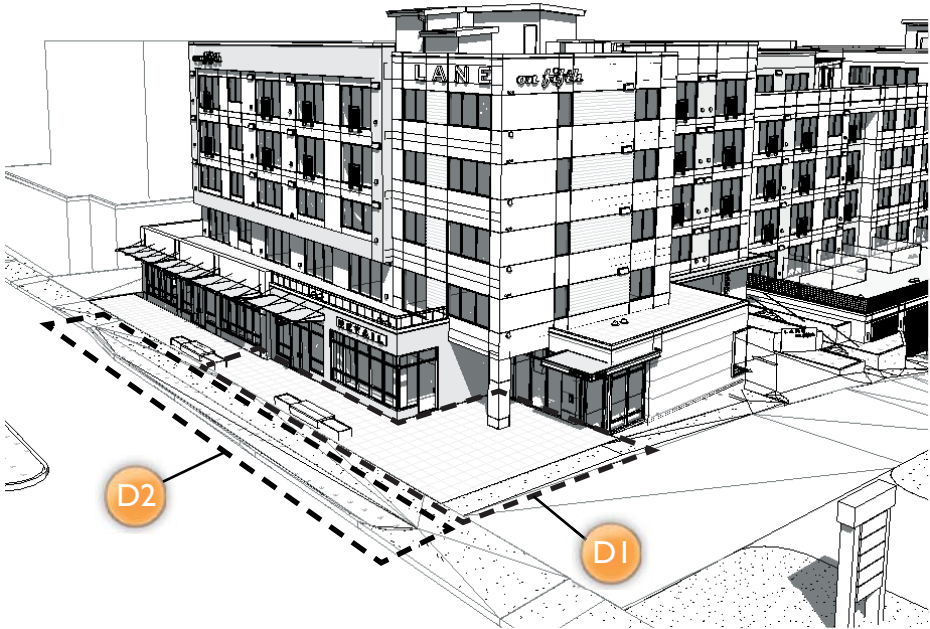
SECTION C-C: SITE SECTION FROM WEST TO EAST

WEST PARCEL REQUESTED DEPARTURES (DPD# 3017692)

DEPARTURE #	LAND USE CODE	CODE ITEM	CODE REQUIREMENT	DEPARTURE REQUESTED	DESIGN RATIONALE
1 	SMC 23.47A.008.A3c (23.71.008.B4)	Street-level street-facing façade setbacks	Street-level street-facing façade setbacks of no more than 10 feet of street-lot line	Setbacks proposed range from 10’ to 11’-7” along retail use frontage, and approximately 31’-0” at the SW corner for a publicly accessed courtyard area	During the EDG meeting, the Board supported a courtyard at the SW corner of the site along 5th Avenue NE to support potential exterior commercial uses. The proposed courtyard has direct access to the retail space, publicly scaled artwork, and fronts an articulated residential entry. Along the retail frontage, the additional setback allows for slight material and facade modulation to enhance the streetscape interest.
2 	SMC 23.71.008.E3	Planting strips	Planting strips prohibited along Major Pedestrian Streets	Project proposes a planting strip along 5th Avenue NE between curb and sidewalk	The planting strip provides a desired separation between pedestrians and vehicular traffic, creating an enhanced sense of safety and comfort. The added landscape area supports the vitality of the required street trees and continues the recent pattern of planting strip development installed at the property to the North. The width of sidewalk available for walking is maintained for the full length of the site.
3 	SMC 23.54.030.B2a	Parking space sizing	Max. of 25% stalls to be “small”, min. 75% stalls to be “large”. Code requires (2) small and (5) large stalls	Project proposes (7) non-residential stalls: (2) small / (4) large / (1) van accessible	Due to narrow site constraints and changes in site elevations, the efficiency of the parking area dictates available sizing configurations for the stalls. Allowing a slight deviation in the stall percentage sizing works to minimize the parking use impact on the facades and allows room for a pedestrian pathway on the West side of the retail driveway.
4 	SMC 23.71.014.A2	Open space requirements	Min. of 15% of lot area to be provided as Open Space. Code min. is 5,508 SF (15% of 36,718 SF)	Project proposes 5321 SF of total Open Space, equalling 14.5% of lot area.	An East/West pedestrian connection limits the ability of the West parcel to provide its portion of Open Space. The East parcel (#3019072) provides 8,353 SF of Open Space, for a combined total of 13,674 SF of provided Open Space. (10,476 SF minimum required for combined sites).
5 	SMC 23.47A.008.B3	Non-residential floor to floor heights	Non-residential street uses shall have a floor to floor height of 13’-0”	Project proposes a 12’-6” floor to floor height within the retail area	Overall height limit to maintain all desired program uses and locations resulted in a 12’-6” measurement as the project proposes. Height as proposed will work for proposes retail uses.
6 	SMC 23.71.014.B2b	Usable open space, barrier free access	Usable Open Space shall provide barrier free access	Barrier free access is not provided between project property and adjacent property to the North. Barrier free access is provided within the project site itself.	Barrier-free access is provided to the proposed Open Space from 8th Avenue, to both building entries and project amenities as required by ANSI accessibility codes. A stairway for neighborhood access is provided near the Northern property line to accommodate the 8’ grade change between the two properties. Ramping at this location would exceed 96’ in length and would negatively impact the available landscaping and open space.

EAST PARCEL REQUESTED DEPARTURES (DPD# 3019072)

DEPARTURE #	LAND USE CODE	CODE ITEM	CODE REQUIREMENT	DEPARTURE REQUESTED	DESIGN RATIONALE
7 	SMC 23.47A.008.A2c	Blank walls	Blank wall segments not to exceed 40% of facade length. Code max. length is 22’-9” (40% of 56’-10” LF)	Blank wall segments proposed total 26’-3”, equalling 46.2% of facade length along street.	Blank wall segments at the live/work unit locations provide signage opportunities and a clear architectural entry that is distinct from the residential tower entry. Generous outdoor courtyard and landscaped area between the live/works and sidewalk encourage pedestrian engagement with the space and facade and contribute to street interest. Proposed dimensions of glazing is well proportioned for the facade and provides architectural consistency with adjacent facades.
8 	SMC 23.47A.008.B2a	Transparency	60% of street-facing street-facade between 2’ and 8’ above sidewalk grade shall be transparent. Code min. is 164.88 SF (60% of 274.8 SF)	Transparent area proposed is 123.91 SF, equalling 45.1% of street-facing street-facing facade.	Live/work units are providing a strong commercial space opportunity along 8th Avenue NE and a publicly inviting courtyard space. Glazing is 11’-0” wide for each unit and extends beyond the 8’ header height calculated for code, giving each unit ample visual connections to the sidewalk while still providing signage opportunities and clearly articulated live/work entry locations. Proposed dimensions of glazing is well proportioned for the facade and provides architectural consistency with adjacent facades.



D3

D3: NON-RESIDENTIAL PARKING STALL DISTRIBUTION CALCULATION
PROPOSED NUMBER OF STALLS = 7




REQUIRED MAXIMUM SMALL STALLS NOT TO EXCEED 25% OR 2 STALLS ((7X.25) = 1.75)

REQUIRED MINIMUM LARGE STALLS TO BE LESS THAN 75% OR 5 STALLS ((7X.75) = 5.25)

PROPOSED SMALL (7'-6"X15') STALLS = 2 (MEETS CODE MAX. OF 2 STALLS)

PROPOSED LARGE (8'-6"X19') STALLS = 4 (DOES NOT MEET CODE MIN. OF 5 STALLS)

PROPOSED VAN ACCESSIBLE (8'X19') = 1 (DOES NOT MEET DIMENSIONAL WIDTH OF LARGE STALL DEFINITION, STALL DOES MEET MINIMUM REQUIRED WIDTH FOR A VAN ACCESSIBLE STALL)

DEPARTURE #		LAND USE CODE	CODE ITEM	CODE REQUIREMENT	DEPARTURE REQUESTED	DESIGN RATIONALE
1		SMC 23.47A.008.A3c (23.71.008.B4)	Street-level street-facing façade setbacks	Street-level street-facing façade setbacks of no more than 10 feet of street-lot line	Setbacks proposed range from 10' to 11'-7" along retail use frontage, and approximately 31'-0" at the SW corner for a publicly accessed courtyard area	During the EDG meeting, the Board supported a courtyard at the SW corner of the site along 5th Avenue NE to support potential exterior commercial uses. The proposed courtyard has direct access to the retail space, publicly scaled artwork, and fronts an articulated residential entry. Along the retail frontage, the additional setback allows for slight material and facade modulation to enhance the streetscape interest.
2		SMC 23.71.008.E3	Planting strips	Planting strips prohibited along Major Pedestrian Streets	Project proposes a planting strip along 5th Avenue NE between curb and sidewalk	The planting strip provides a desired separation between pedestrians and vehicular traffic, creating an enhanced sense of safety and comfort. The added landscape area supports the vitality of the required street trees and continues the recent pattern of planting strip development installed at the property to the North. The width of sidewalk available for walking is maintained for the full length of the site.
3		SMC 23.54.030.B2a	Parking space sizing	Max. of 25% stalls to be "small", min. 75% stalls to be "large". Code requires (2) small and (5) large stalls	Project proposes (7) non-residential stalls: (2) small / (4) large / (1) van accessible	Due to narrow site constraints and changes in site elevations, the efficiency of the parking area dictates available sizing configurations for the stalls. Allowing a slight deviation in the stall percentage sizing works to minimize the parking use impact on the facades and allows room for a pedestrian pathway on the West side of the retail driveway.

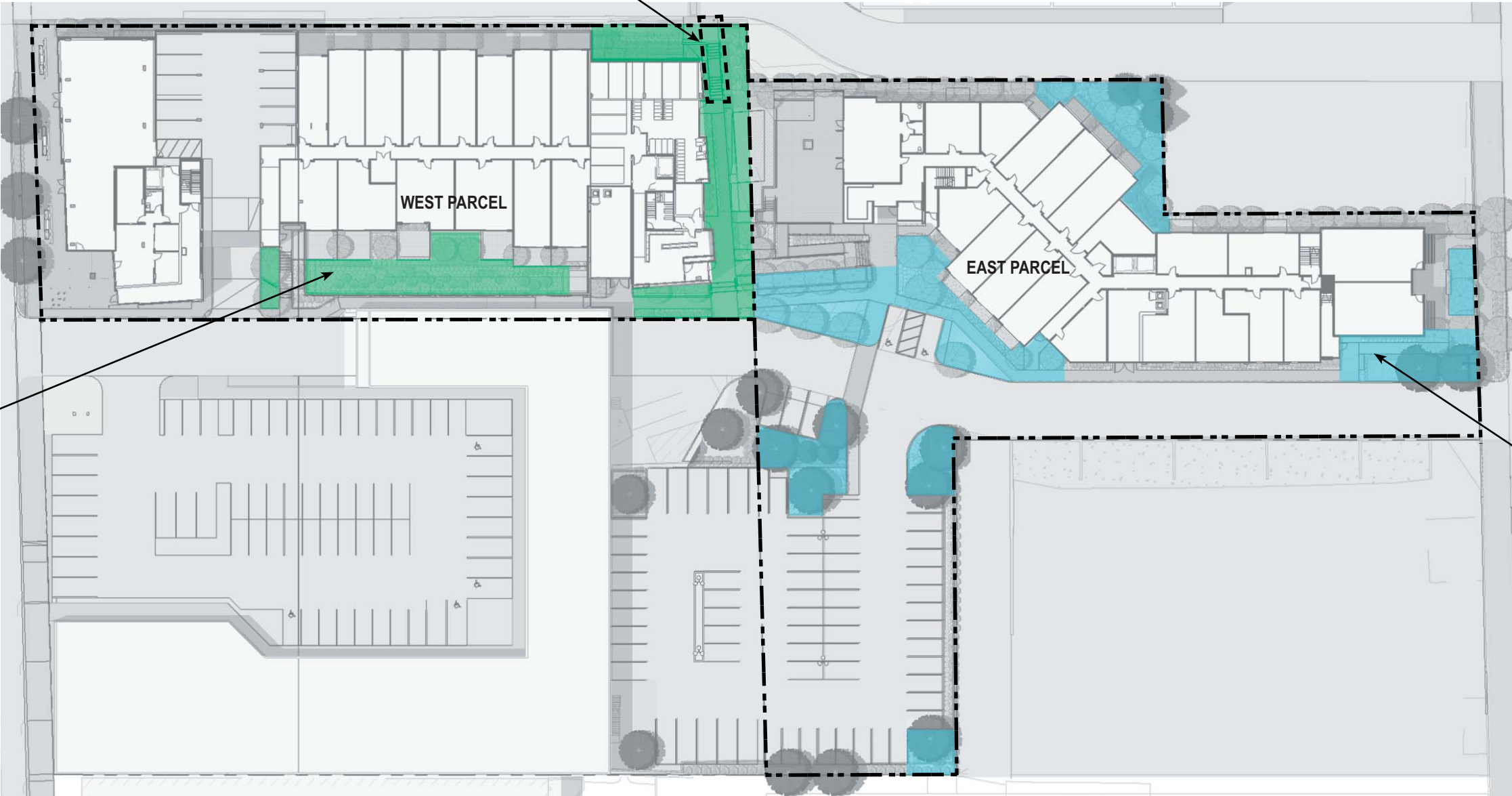
D6: BARRIER FREE OPEN SPACE
STAIRWAY LOCATION TO NEIGHBORING
PROPERTY TO THE NORTH

D4: OPEN SPACE MINIMUM AREA
WEST PARCEL OPEN SPACE
AREA PROPOSED = 5,321 SF
(187 SF LESS THAN CODE MIN.)

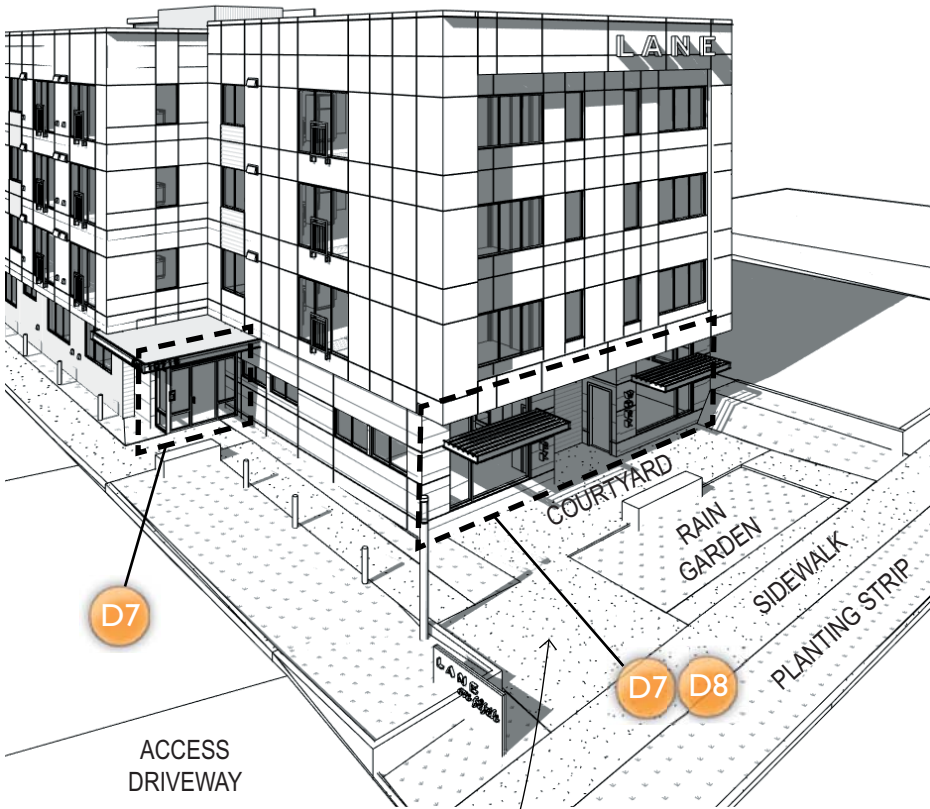
LOCATIONS INDICATED
IN GREEN HATCHING

EAST PARCEL OPEN SPACE
AREA PROPOSED = 8,353 SF
(3,385 SF ABOVE CODE MIN.)

LOCATIONS INDICATED IN
BLUE HATCHING



DEPARTURE #	LAND USE CODE	CODE ITEM	CODE REQUIREMENT	DEPARTURE REQUESTED	DESIGN RATIONALE
4	D4	Open space requirements	Min. of 15% of lot area to be provided as Open Space. Code min. is 5,508 SF (15% of 36,718 SF)	Project proposes 5321 SF of total Open Space, equalling 14.5% of lot area.	An East/West pedestrian connection limits the ability of the West parcel to provide its portion of Open Space. The East parcel (#3019072) provides 8,353 SF of Open Space, for a combined total of 13,674 SF of provided Open Space. (10,476 SF minimum required for combined sites).
5	D5	Non-residential floor to floor heights	Non-residential street uses shall have a floor to floor height of 13'-0"	Project proposes a 12'-6" floor to floor height within the retail area	Overall height limit to maintain all desired program uses and locations resulted in a 12'-6" measurement as the project proposes. Height as proposed will work for proposes retail uses.
6	D6	Usable open space, barrier free access	Usable Open Space shall provide barrier free access	Barrier free access is not provided between project property and adjacent property to the North. Barrier free access is provided within the project site itself.	Barrier-free access is provided to the proposed Open Space from 8th Avenue, to both building entries and project amenities as required by ANSI accessibility codes. A stairway for neighborhood access is provided near the Northern property line to accommodate the 8' grade change between the two properties. Ramping at this location would exceed 96' in length and would negatively impact the available landscaping and open space.



PUBLIC BENEFIT:
PUBLICLY ACCESSIBLE
COURTYARD AND
LANDSCAPING



D7 BLANK WALL SEGMENTS

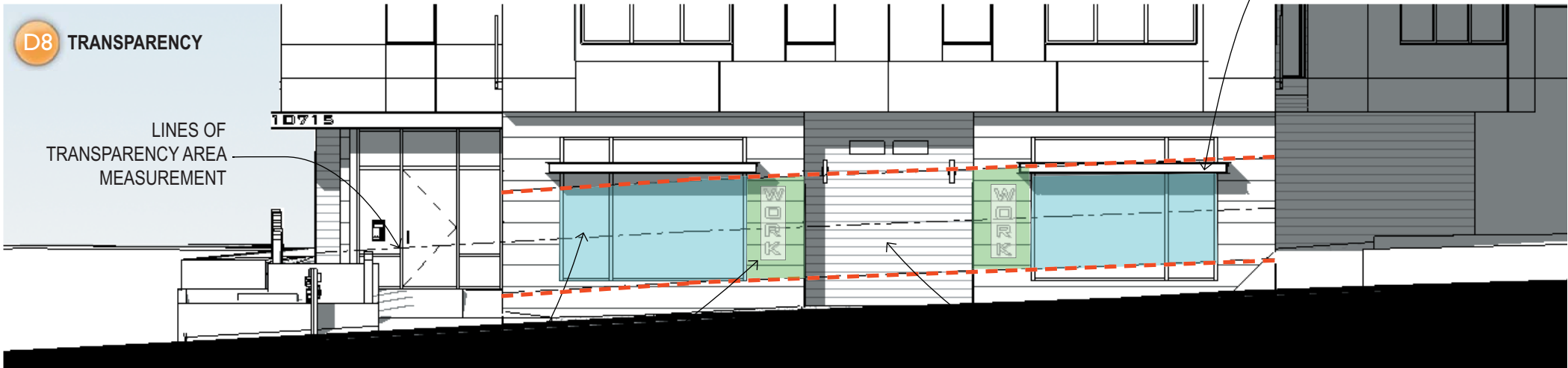
LINE OF BLANK WALL LENGTH MEASUREMENT

CODE COMPLIANT BLANK WALL LENGTH FACADE LOCATIONS (22'-9")

PROPOSED DEPARTURE DIMENSION OF 3'-6"

OVERALL STREET-FACING STREET-LEVEL FACADE LENGTH = 56'-10"

DESIGN BENEFIT:
DISTINCT AND PRIVATE
RECESSED ENTRY
FOR LIVE/WORK UNITS



D8 TRANSPARENCY

LINE OF TRANSPARENCY AREA MEASUREMENT

CODE COMPLIANT TRANSPARENCY AREA (123.91 SF)

PROPOSED DEPARTURE AREA (40.97 SF)

OVERALL NON-RESIDENTIAL USE STREET-FACING STREET-LEVEL FACADE LENGTH

PUBLIC BENEFIT:
AWNINGS

DESIGN BENEFIT:
DISTINCT AND PRIVATE
RECESSED ENTRY
FOR LIVE/WORK UNITS

DEPARTURE #	LAND USE CODE	CODE ITEM	CODE REQUIREMENT	DEPARTURE REQUESTED	DESIGN RATIONALE
7	SMC 23.47A.008.A2c	Blank walls	Blank wall segments not to exceed 40% of facade length. Code max. length is 22'-9" (40% of 56'-10" LF)	Blank wall segments proposed total 26'-3", equalling 46.2% of facade length along street.	Blank wall segments at the live/work unit locations provide signage opportunities and a clear architectural entry that is distinct from the residential tower entry. Generous outdoor courtyard and landscaped area between the live/works and sidewalk encourage pedestrian engagement with the space and facade and contribute to street interest. Proposed dimensions of glazing is well proportioned for the facade and provides architectural consistency with adjacent facades.
8	SMC 23.47A.008.B2a	Transparency	60% of street-facing street-facade between 2' and 8' above sidewalk grade shall be transparent. Code min. is 164.88 SF (60% of 274.8 SF)	Transparent area proposed is 123.91 SF, equalling 45.1% of street-facing street-facing facade.	Live/work units are providing a strong commercial space opportunity along 8th Avenue NE and a publicly inviting courtyard space. Glazing is 11'-0" wide for each unit and extends beyond the 8' header height calculated for code, giving each unit ample visual connections to the sidewalk while still providing signage opportunities and clearly articulated live/work entry locations. Proposed dimensions of glazing is well proportioned for the facade and provides architectural consistency with adjacent facades.