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EDG MEETING 09/15/2014

EARLY DESIGN GUIDANCE    DPD #3017625    9510 Stone Ave North - Seattle WA

911 Western Ave #318  
Seattle, WA 98104  
206.287.0136  
www.cobbarch.com

**E. Cobb Architects**

**SITE INFORMATION:**

ADDRESS - 9510 Stone Ave North  
PARCEL # - 4310701290  
OWNER/APPLICANT- Stone North LLC  
AGENT - E. Cobb Architects, Josh Johns

**EXISTING SITE:**

LOT SIZE - 150'X100' - 15,000 sqft  
USE - Office, Coverd Storage and Open Storage  
TREES/LANDSCAPE - No significant trees, site is primarily buildings and gravel parking and storage  
SITE SLOPE - 11' maximum grade change from SE corner to NW corner.

**LANDUSE:**

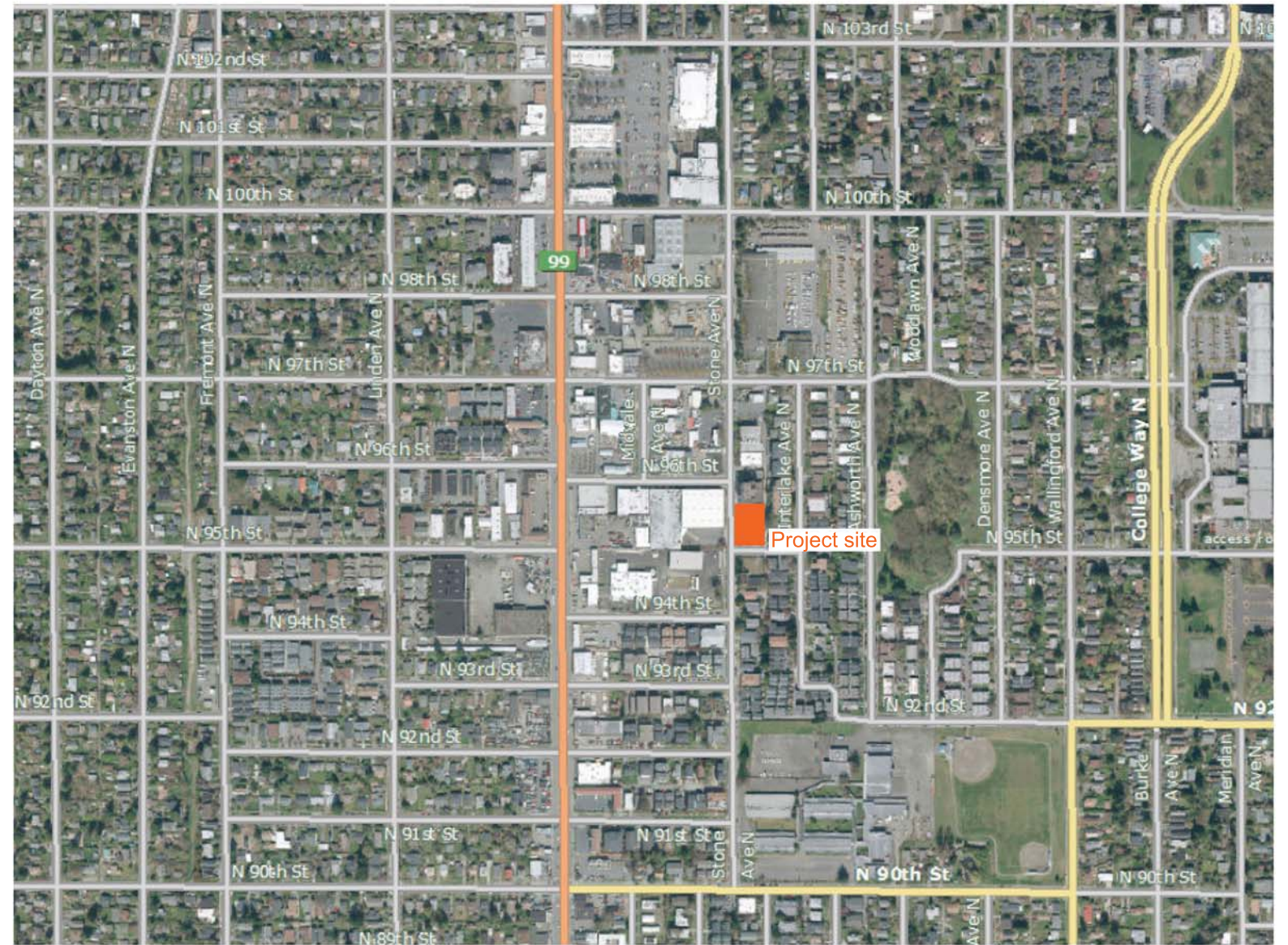
- C2-65 zone (65' maximum building height)
- 4.25 FAR for Residential only, 4.75 FAR for mixed used buildings
- Urban Village overlay - Aurora -Lictin Springs - Residential Urban Village
- No ECA designations
- Parking - no required parking for residential or office use per urban village overlay

**PROPOSED PROGRAM:**

PARKING SPACES- Between 9-66 depending on design option  
STREET LEVEL COMMERCIAL RETAIL/OFFICE - 3-6 units  
RESIDENTIAL UNITS - 60-74 units

**AREA:**

PARKING AREA - Between 2500- 18,000 sqft  
STREET LEVEL RETAIL/OFFICE AREA - 7000-9,600 sqft  
RESIDENTIAL AREA - 54,000-57,650 sqft



**DEVELOPMENT OBJECTIVES:**

- Architectural Presence** – Create a high degree of architectural presence. Form a strong street edge and focal corner at Stone ave N and N 95th st.
- Human Scale** – Create street level interaction along Stone Ave N and N 95th St.
- Modern design in an evolving neighborhood** – Modern design - focus on quality of light/space with simple, durable materials and detailing.
- Flexible mixed use design** – Provide a wide range of residential unit configurations, flexible street level commercial space and sensible onsite parking provisions.

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**ZONING INFORMATION:**

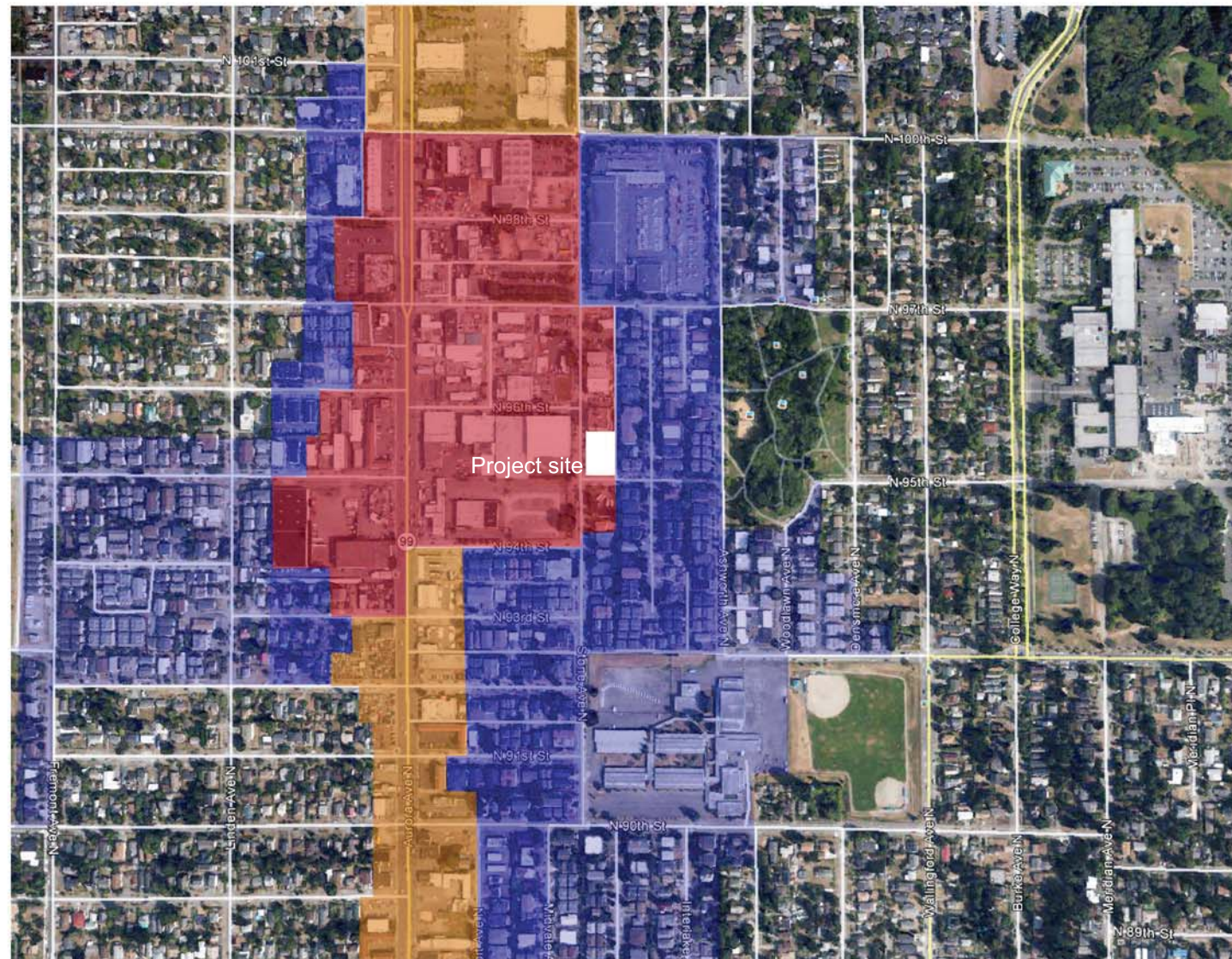
ADDRESS - 9510 Stone Ave North

PARCEL # - 4310701290

ZONING - C2-65

OVERLAY - Residential Urban Village

LOT SIZE - 150'X100' = 15,000 sqft



Vicinity Map - Adjacent zoning and example commercial building types

- C2-65
- C1-65
- LR3
- LR2

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**23.47A.004 PERMITTED USES**

Commercial uses - Office, Retail, Live/work

**23.47A.006.3 Conditional Use**

-Residential use within a C2 zone

**23.47A.012 Structure Height**

-65' from average DPD building grade

-An additional 4' is allowed for open railings, planters, skylights, clerestories, greenhouses, solariums, parapets and firewalls

**23.47A.014 Setbacks**

-The structure may built to the alley ROW for commercial use. Residential use must be setback 15' from the CL of the alley up to 40' above grade. The setback increases 2' for every 10' of additional building height beyond 40'.

**23.47A.024 Amenity Area**

-5% of gross floor area in the residential use.

-All units will have access to a common amenity space (250sqft min)

-Private balconies must be 60sqft and 6' minimum in each horizontal direction to qualify as amenity space.

**23.47A.016 Landscape Requirements**

-Green factor of 0.30

-No landscape buffer or setback required between the building and alley if parking is enclosed.

**23.47A.030 Required Parking**

-Zero required on site parking per the Residential Urban Village overlay and proximity to public transit.

**23.58A.014 Affordable Housing Incentive or additional FAR**

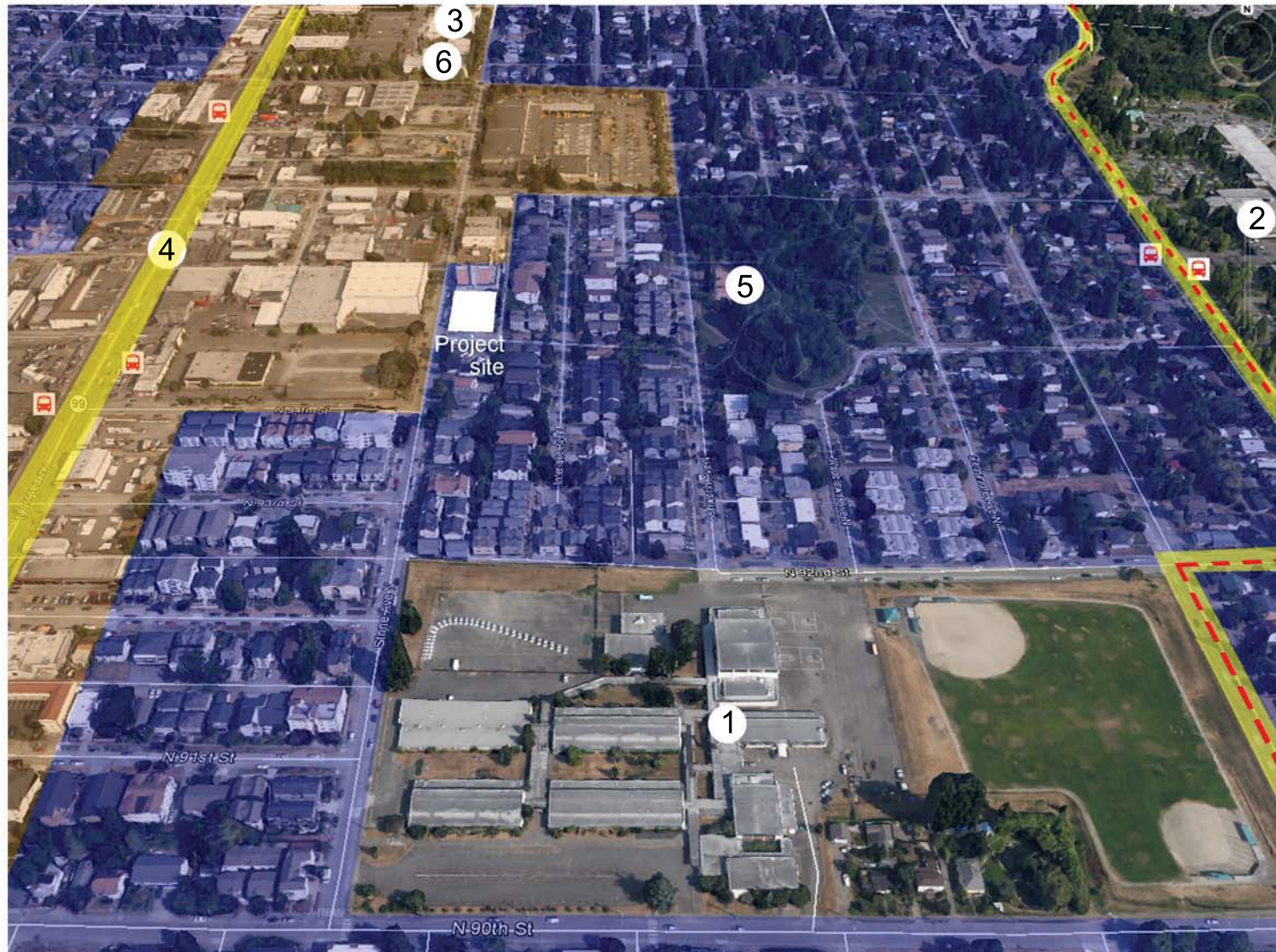
-Not pursued.

**23.54.040 Solid waste and recyclable materials storage and access**

- Residential between 50-100 units - 375sqft

-Commercial - less than 15,000sqft, 125 \* 50% = 63sqft

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**Vicinity map - Transportation and Points of Interest**

- Bus routes
- Bike route (Dedicated lane or Sharrow per master plan)
- Existing residential use
- Existing commercial use

**TRANSPORTATION & POINTS OF INTEREST STUDY DESIGN CUES:**

- Stone Ave North acts as the primary north/south pedestrian circulation for the neighborhood traveling to school, grocery stores or the movie theatre. Highly visible commercial street level use will benefit Stone Ave north.
- Residential entry on North 95th St – Vehicular traffic along N. 95th St is minimal as the roadway dead ends to the east of the site. Where North 95th St ends a pedestrian path continues to Licton-Springs Park.
- Provide a variety of apartment configurations to accommodate a diverse demographic. Consider a variety of unit sizes – studio, 1,2 and three bedroom units to accommodate college students to families.
- Hwy 99 serves the vehicles and not the pedestrians of Licton-Springs. Residents need walkable commercial options on the edge of the residential zone. Although the ground floor commercial spaces may currently rent better as office or live/work space we should plan on an easy transition to future retail and restaurant type spaces as the neighborhood evolves.



**1- Wilson Pacific elementary and middle school** - Scheduled to break ground in 2014. The daily activities and events at the school will create additional pedestrian and vehicular traffic along Stone Ave North. Street level services within walking distance to the school would benefit the neighborhood



**2- North Seattle Community College** -62 acres Community college campus serving approx 18,000 students each year. As the facilities and enrolment continues to grow surrounding housing options also need to increase. The project site is within easy walking distance to NSCC and the unit configurations should consider college students.



**3- Oak Tree Plaza Shopping center** - Major Grocery store, various retail, services and restaurants. Mostly focused on Hwy 99 but within easy walking distance to the project site.



**4- Hwy 99 (Aurora Ave N)** - North/south transportation, public transit, various retail, office and restaurant options. Walking destination from the residential neighborhood for food, retail and transit. The project site is far enough from Hwy 99 to avoid the majority of traffic noise.

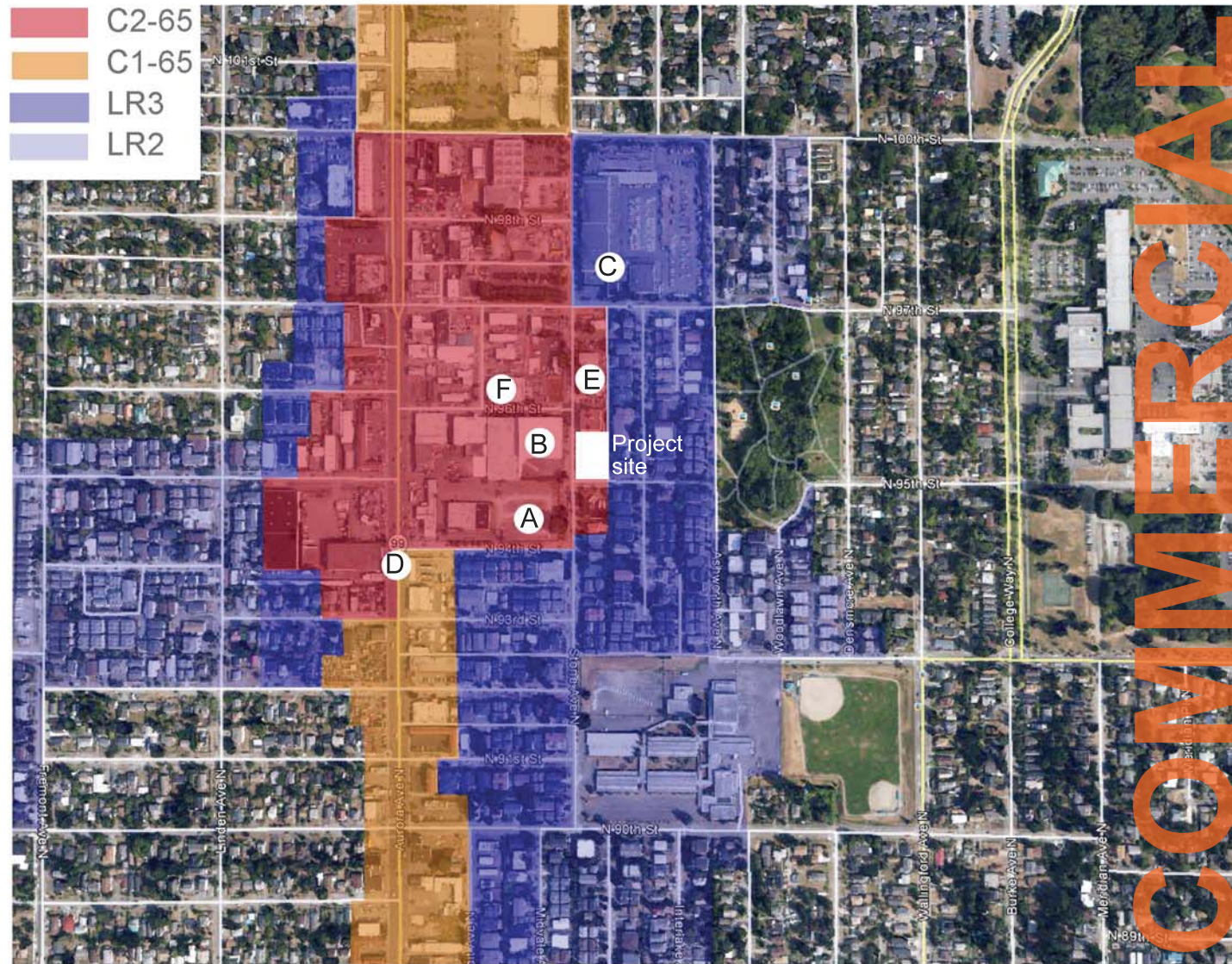


**5- Licton Springs Park** -7.6 acre park with wooded preserves, hiking trails, ponds and a play area. Recent play area renovation and boardwalk pictured above. The park is within 500' of the project site.



**6- AMC Oaktree movie theatre** Destination entertainment center within walking distance of the project site

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**Vicinity Map - Adjacent zoning and example commercial building types**

**Design cues from Commercial building study -**

The majority of surrounding commercial buildings are office and storage facilities with moderate traffic and noise levels compatible with a residential development. The surrounding street level uses are commonly office. The office buildings are somewhat disconnected from the sidewalk and lack interaction with pedestrians for uses such as retail, restaurant and similar services. The commercial services along highway 99 were designed to accommodate vehicles and are not pedestrian friendly. Overall the residents of Licton Springs live on the border of a commercial strip with very few pedestrian compatible commercial options. The intent of the Residential Urban village is to *Promote the development of residential urban villages, which function primarily as compact residential neighborhoods providing opportunities for a wide range of housing types and a mix of activities that support the residential population.* Conclusions for the project site:

- 1 -The existing commercial core is does not necessary support the surround residential neighborhood as a walkable community
- 2 -The street level use should be predominately commercial, connected with the sidewalk and scaled to the pedestrian.
- 3 -The street level use should be flexible to evolve over time. At this point street level office or live/work units may be the highest demand but future demand may evolve to retail and restaurant spaces.
- 4 -The entry to each street level commercial space must be at grade with the sidewalk. This will require steps in the street level units to accommodate the sidewalk elevation change along Stone Ave North and North 95th Street (approx 11' elevation change)

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A- C2-65 - Puget Sound Energy HQ built in 2011. The office/storage facility is only accessed from N 94th St. A planted 5-8' tall site wall faces the length of Stone ave North. Noise from the site is minimal and most traffic in/out moves along N 94th St.



B - C2-65 - Warehouse storage building, long term document storage facility. The nature of long term storage sees minimal pickup/deliveries during 9-5 business hours. The current site produces very little noise and traffic.



C - LR3 - Two Story office complex, Seattle City light HQ building. The office building consumes street parking in the surrounding blocks but after 5pm the area clears out.



D - C2-65 - Aurora Ave N typical commercial strip centered around vehicles. The sidewalks are against the fast moving hwy with no planting strip or street parking to separate pedestrians from traffic. The business are also separated from the sidewalks by parking lots.



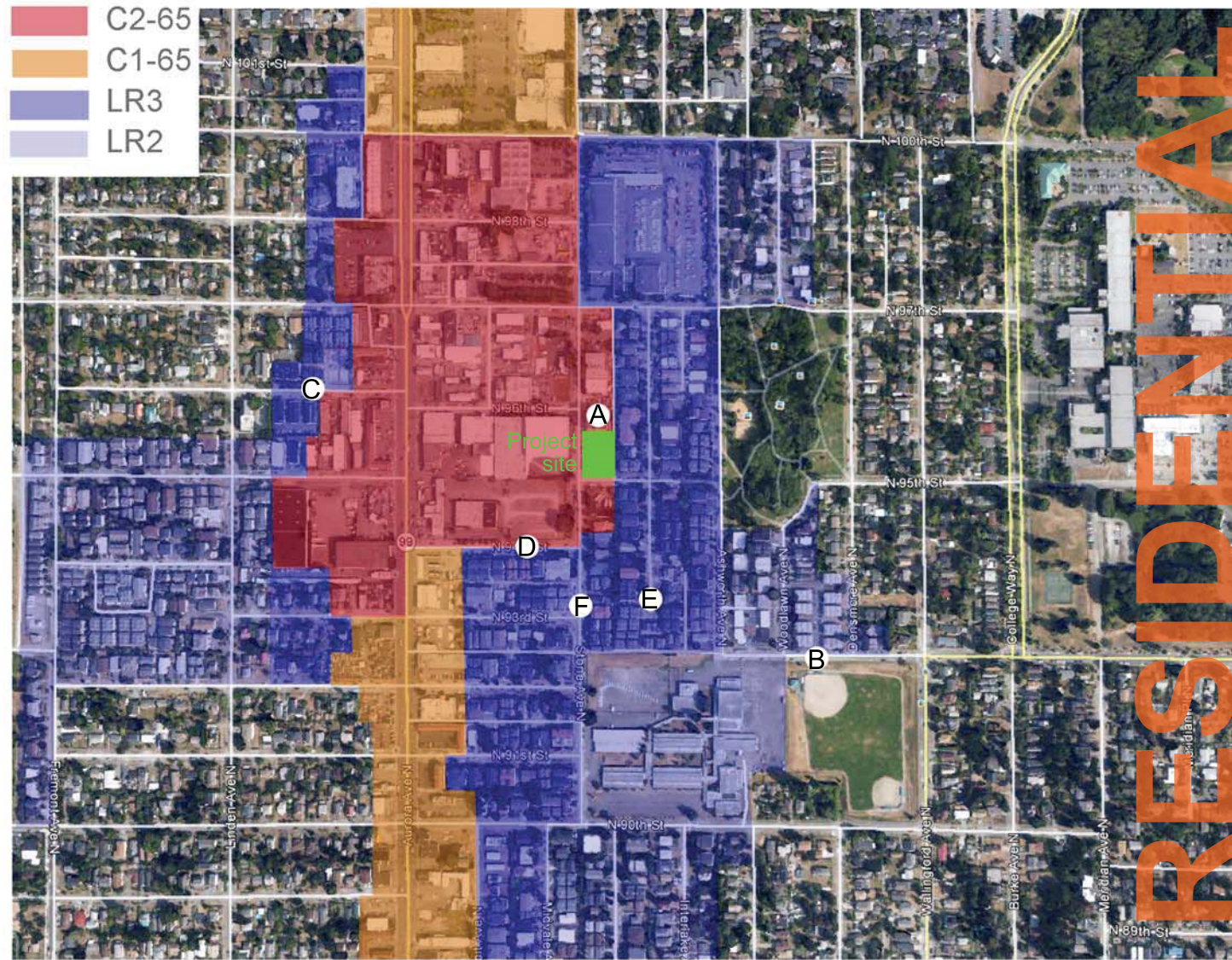
E - C2-65 - Two Story office building - Nelson Electric, The School of Teaching ESL, Sound Business Forms. Great street trees and planting but the raised building elevation and minimal street level windows separates the building from the sidewalk. The building works for office use but retail/services are less likely.



F - C2-65 - Two story office buildings. Office buildings with minimal street front glazing and setback from the street 5'.

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**Vicinity Map - Adjacent zoning and example residential buildings**

**RESIDENTIAL BUILDING STUDY DESIGN CUES:**

- Proposed residential apartments should provide a variety of unit sizes to add diversity within the Licton-Springs housing market (recently dominated by townhome developments)
- Although no parking is required by code the residential units will need onsite parking to compete with similar rentals throughout the neighborhood.
- Modulation of the building elevations will help reduce the perceived mass of the building and create interest.
- Ground floor commercial units must enter at the finished sidewalk elevation for successful street level interaction.
- Integrate the concrete podium and wood structure above to make a cohesive building rather than a box on a base.
- Use the building form and details at the street level to identify important entry points to the building – residential and commercial.
- Consider contrasting materials and vibrant color on the building exterior to create modulation and avoid a monotonous building.



A - C2-65- Recent 6 story residential mixed use building. Step in building facade helps break building mass. The street level commercial units become separated from the sidewalk due to the entry elevation difference. The uniform rhythm of the street level does not identify special entry points at street level. The structure is clearly a framed building resting on a concrete podium, more integration between the street level podium and floors above would create more interest.



B - LR2 - Apartment complex - Many existing multifamily structures have dedicated onsite parking. Although the project is not required to provide parking by City code parking may be necessary to compete with similar rentals throughout the neighborhood. Parking should be located from the alley and below grade to preserve all street front commercial opportunities.



C- LR3- 3 Story Townhomes adjacent to C2-65 zone Many locations in the neighborhood have multifamily residential adjacent to commercial zoning. This example could rely on more street planting as a buffer between the zones rather than relying solely on the 6' wood fence.



D - LR3 - 3 Story Townhomes adjacent to C2-65. Similar example of multifamily residential directly adjacent to C2-65 zoning. The existing C2 use does not appear to negatively impact the housing.



E - LR3 - Recent Townhome development. The multifamily developments throughout Licton-Springs have increased residential density in the last ten years but primarily with townhomes. An apartment building with a variety of units from studio to three bedrooms would provide more diversity to the housing market.



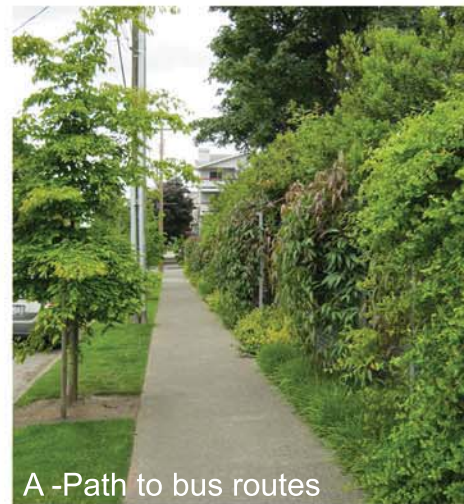
F - LR2 - A mix of SF homes and Duplex/Townhomes. Few single family homes remain within the Lowrise zone as most continue to develop to multifamily housing.

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**Design Cues from Pedestrian circulation analysis:**

-Stone Ave North provides the first pedestrian friendly north-south circulation east of the busy Hwy 99 strip. The majority of pedestrians moving by the project site will be traveling along Stone Ave North and the project should be primarily commercial use at the street level along Stone Ave. The street level residential entrance would be located on North 95th st, still visible from the street corner but not occupying the busier Stone Ave circulation.

-North 95th street becomes a dead end and has limited vehicular traffic and provides a very short path to Licton-Springs park or North Seattle Community College. The existing sidewalk at the project site does not include a planting strip separating the sidewalk from the street. We would like to reclaim part of the recessed parking strip on the north side of north 95th St to provide planting, shade trees (building's southern exposure) and greater separation between pedestrians and cars. Refer to proposed landscape plan on page 15.



A -Path to bus routes

Pedestrian path from the project site to hwy 99 bus routes. The sidewalks are separated from the roadway with developed planting strips and street parking.



D - Stone ave N

North 95th St becomes a dead end for vehicular traffic just east of Interlake Ave N but a pedestrian path continues to connect with Licton-Springs park. The project site is less than a 500' walk to Licton Springs park. N 95th St and Interlake Ave N become dead end streets limiting the traffic along North 95th St.



**PEDESTRIAN CIRCULATION**

Common pedestrian path

Reference images



B-N 97th St

Pedestrian circulation to North Seattle Community College. The continuous sidewalk is separated from the roadway with planting strips and street parking. The path borders Licton-Springs park and is a common path for college students in the neighborhood.



C-N 97th St

PEDESTRIAN

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**View from N 95th St looking North**



The south edge of the project site borders N. 95th St. East of the project site the structures are predominately townhomes and apartment buildings (LR3).

**Design opportunities-**

Street corner with high visibility – Continuous street level commercial use at Stone ave and N 95th st. Consider residential entry on the quieter N 95th st

**Design constraints –**

The sidewalk is currently tight to the street parking curb. Propose a planting strip between sidewalk and street to provide planting, shade trees and pedestrian separation from N 95th St.

**View from N 95th St looking South**



A contractor’s office and storage yard sits directly south of the projects site. The vehicular access to the contractor’s yard is off N 95th St only.

East of the contractor’s office the zoning changes to Lowrise 3 and becomes predominately townhomes and apartments. N 95th St dead ends one block to the east for vehicular traffic limiting the overall traffic on N 95th St.

**Design opportunities-**

Territorial views to the south – Low buildings and LR3 zoning will allow views above the third floor.  
Sun exposure – Excellent sun exposure to the south. Provide glazing and decks with shade protection to utilize passive solar.

**Design constraints-**

Heavy commercial use south of site – The contractor office/storage does produce some noise and traffic but only during regular business hours. Consider street trees and planting on N 95th to help buffer the street level.

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## View from Stone Ave N looking East



- The adjacent building to the north is a mixed use 6 story residential building with ground floor offices.
- The site to the south of N 95th St is currently a contractor's office and storage facility. A small amount of truck traffic to and from the site will be expected during business hours.
- To the south of the construction office the zoning changes to Lowrise 3 and becomes strictly residential homes, apartments and townhomes.

### Constraints –

Currently the west side of Stone ave has essentially no street level use open to the public. Consider office or live/work units as the initial street level use. If the west side of Stone Ave is developed in the future a two sided commercial zone is more conducive to retail and restaurant spaces. The street level should be flexible to meet current and future needs.

### Opportunities-

Adjacent building to the north has ground floor commercial space, continue commercial street level use along Stone Ave. Entrances to commercial spaces should be at the sidewalk elevation. High visibility at Stone and N 95th – Consider commercial space at street level corner and building massing for architectural presence at the corner.

## View from Stone Ave N looking West



-The long term storage has limited truck traffic through the locked gate facing Stone Ave N during business hours only. Aside from the occasional delivery truck the site is extremely quiet.

-The Puget Sound Energy complex has no access from Stone Ave North. The grade change and site wall disconnects the PSE site activity from Stone Ave North. The main function of the site is office, material and vehicle storage. The noise generated on site is minimal during business hours.

### Constraints –

Currently the west side of Stone ave has essentially no street level use open to the public. Consider office or live/work units as the initial street level use. If the west side of Stone Ave is developed in the future a two sided commercial zone is more conducive to retail and restaurant spaces. The street level should be flexible to meet current and future needs.

### Opportunities-

The structures west of Stone Ave N are currently 30' or less allowing for territorial views and sunsets above the third floor. Consider strong western solar exposure and potential future development west of Stone ave. Future buildings could reach 65' blocking views to the west in the future. Stone Ave has some truck traffic during business hours but evenings are quiet. The distance to hwy 99 is enough to marginalize traffic noise on site.

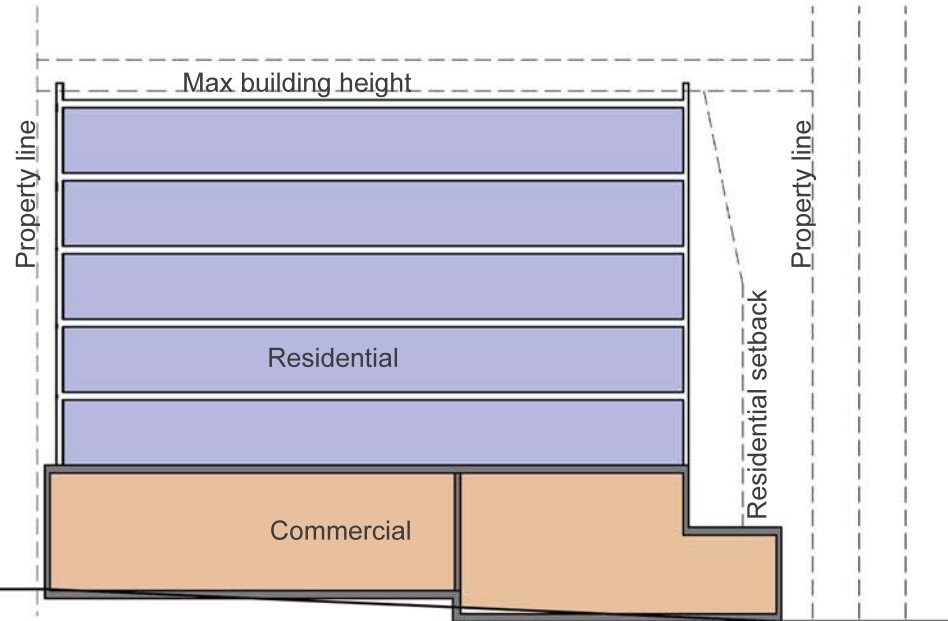
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TYPICAL RESIDENTIAL FLOOR 2-6



EAST-WEST BUILDING SECTION

**OPTION 1 (CODE COMPLIANT OPTION)**

- Building Features**
- 6 stories above grade. Ground floor commercial and parking, 5 floors of residential
  - Solid block massing
- Pros-**
- Parking access from alley, existing curb cut removed
  - Block massing provides a condensed footprint with a larger setback from the alley
- Cons-**
- Minimal outdoor space for individual units
  - Lack of building modulation limits views and eyes on the street
  - Fewer exterior windows limits unit configuration
  - No parking for residential use
- Proposed program:**
- Parking spaces- 9 spaces for commercial use (2500sqft)
  - Street level commercial - (3) units minimum (10,400sqft)
  - Residential units - (59) 1 and 2 bedroom units (58,350sqft)

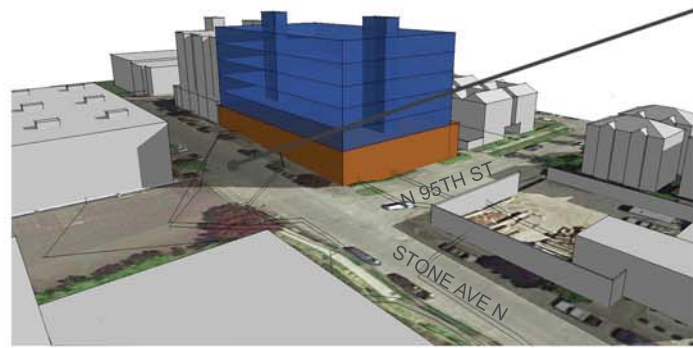


STREET LEVEL COMMERCIAL AND PARKING



SOUTHWEST AERIAL VIEW

**OPTION 1 (CODE COMPLIANT OPTION)**



Excellent morning light to east facing residential units. Stone Ave N. is in shadow from the building.

EQUINOX @ 10:AM



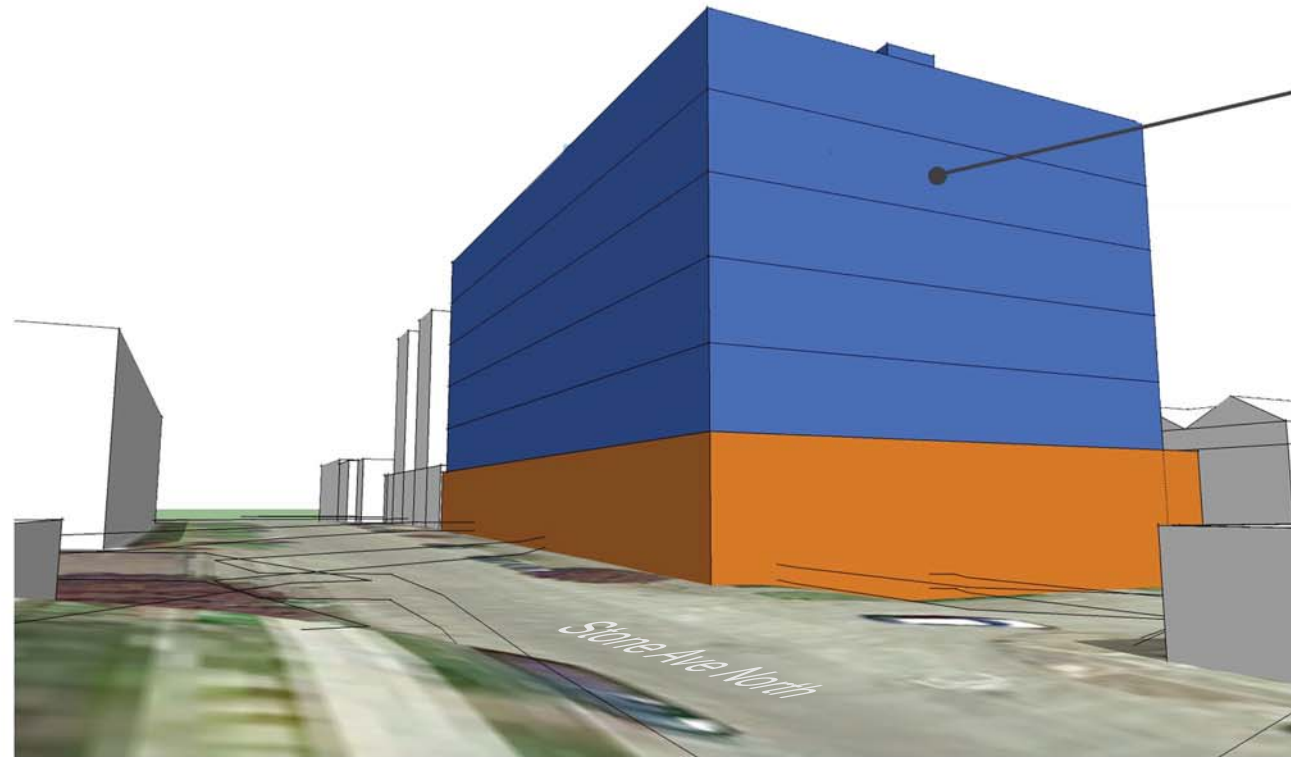
The proposed building will block views to the neighboring building to the north. The ten foot separation between structures will allow for natural light and ventilation but direct southern sunlight to the adjacent building will be greatly reduced.

EQUINOX @ NOON



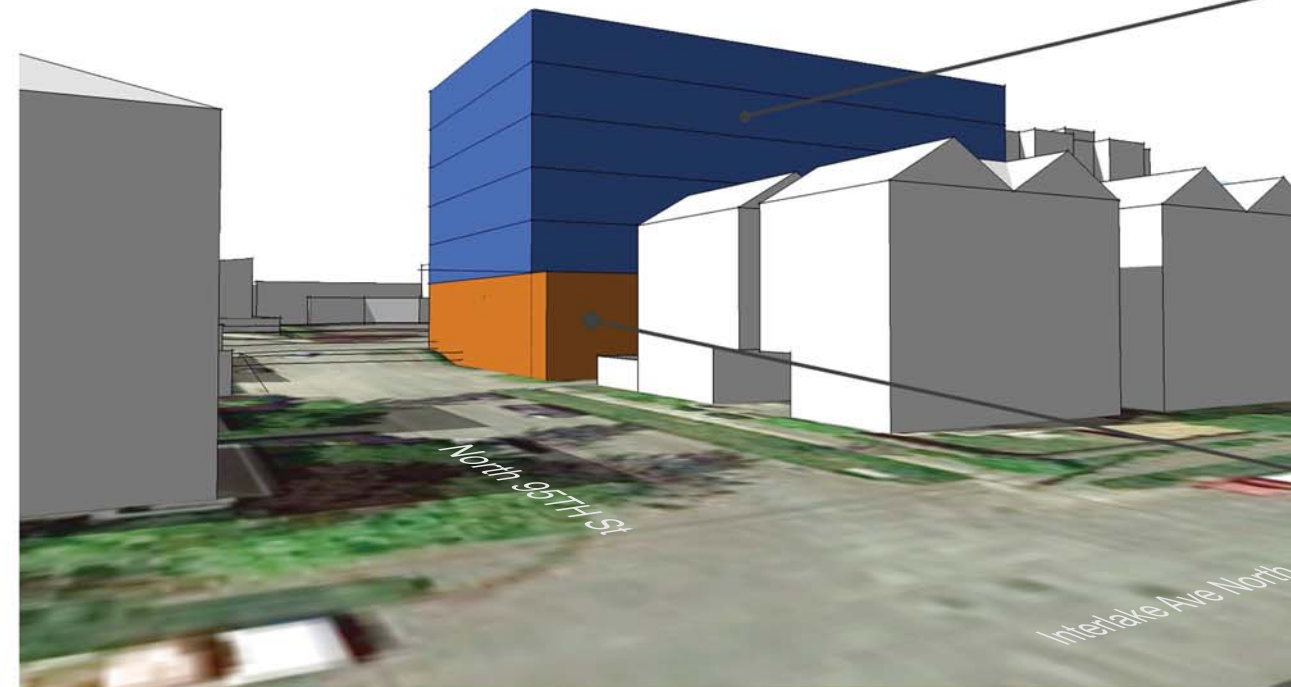
The building's southern exposure will see sun throughout the day, canopies, awnings and street trees would help provide sun control.

EQUINOX @ 2PM



STREET VIEW PERSPECTIVE FROM STONE AVE NORTH

Solid block massing strongly contrasts the smaller scaled multifamily structures and modulation of the adjacent multifamily building to the north



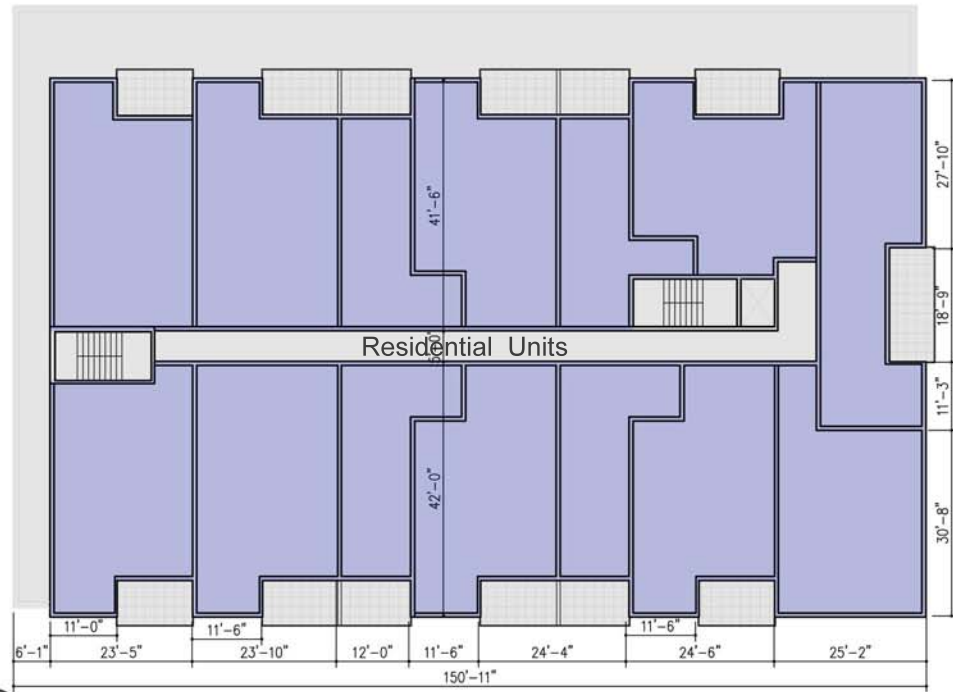
STREET VIEW PERSPECTIVE FROM NORTH 95TH ST AND INTERLAKE AVE NORTH

The alley side of the building (east façade) is visible from multiple angles-close up and from a distance. Although the east façade is not street front the modulation, materials and detailing should be considered highly visible.

Parking access from alley, consider material modulation or planting elements along the podium structure facing the alley to avoid a blank façade.

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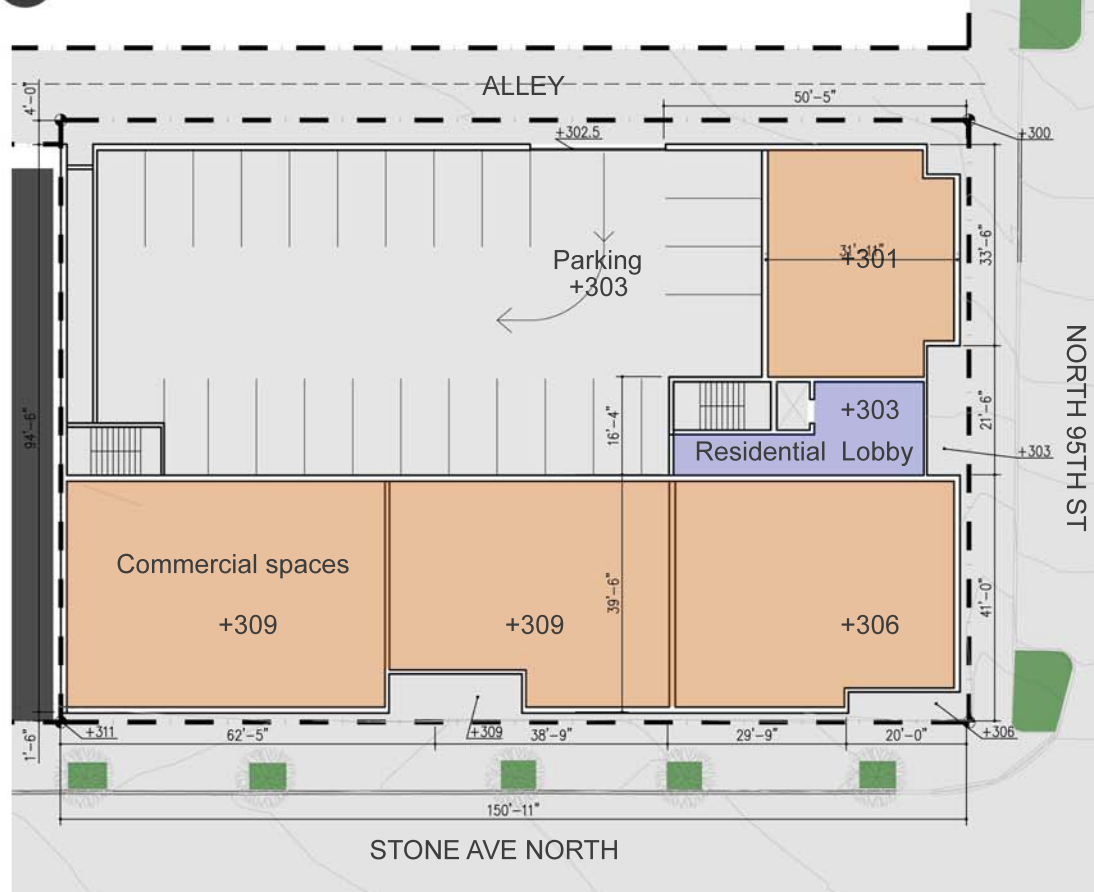
**OPTION 2 - FACADE MODULATION**



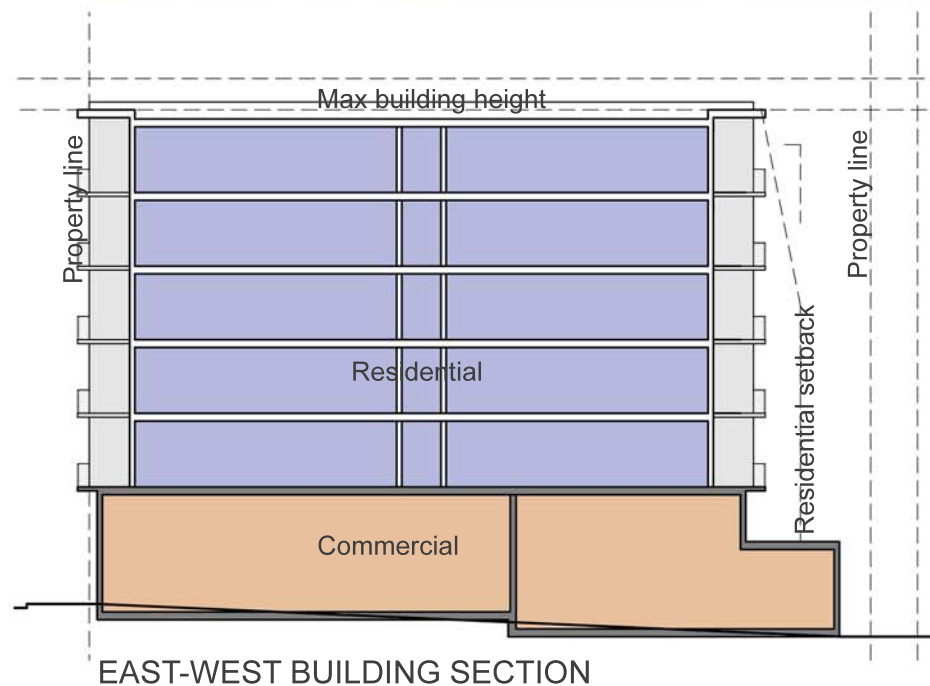
TYPICAL RESIDENTIAL FLOOR 2-6



SOUTHWEST STREET VIEW



STREET LEVEL COMMERCIAL AND PARKING



EAST-WEST BUILDING SECTION

**Building Features**

-6 stories above grade. Ground floor commercial and parking, 5 floors of residential -Building façade modulation

**Pros-**

- Parking access from alley, existing curb cut removed
- Façade modulation creates outdoor spaces for residential units
- Façade modulation reduces the perceived building mass at street and alley facades
- Increased onsite parking (could convert to commercial use in the future as needed).
- A variety of residential unit configurations for diversity
- No required departures

**Cons-**

- The façade modulation helps break the scale of the building but the form still reads as one continuous building form, quite large for the character of the neighborhood.
- Additional parking reduces leasable commercial space as it counts against total FAR above grade

Proposed program: -Parking spaces- 22 enclosed parking spaces (5620sqft) -Street level commercial - (3) units minimum (7,280sqft) -Residential units - (69) Studio, 1,2 and 3 bedroom units (58,350sqft)

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**OPTION 2 - FACADE MODULATION**



EQUINOX @ 10AM



EQUINOX @ NOON



EQUINOX @ 2PM

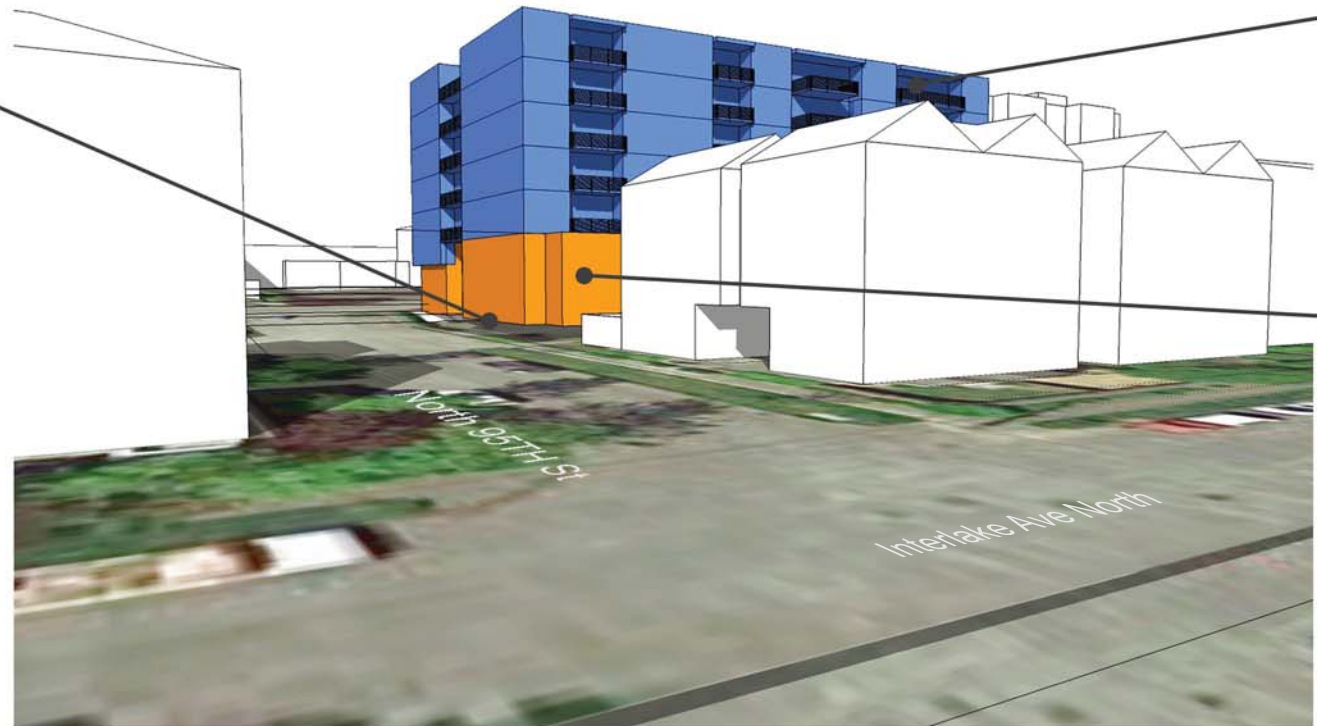
DC2.A  
Modulation breaks the scale of the façade but the overall building still reads as one single mass.

PL1.B.3  
Commercial entry points defined by street level façade modulation. Landscaping, awnings, storefront windows and materials would further define the entry points.

PL1.A.2  
Refer to preliminary landscape plan for proposed planting, shade trees and widened sidewalks.



STREET VIEW PERSPECTIVE FROM STONE AVE NORTH



STREET VIEW PERSPECTIVE FROM NORTH 95TH ST AND INTERLAKE AVE NORTH

Community deck with great views and southern exposure helps define the southern elevation and residential entry point.

CS2.C.1  
Highly visible street corner, establish a strong urban edge to the block

PL3.A.1  
Façade modulation at southern elevation helps identify the residential entry point.

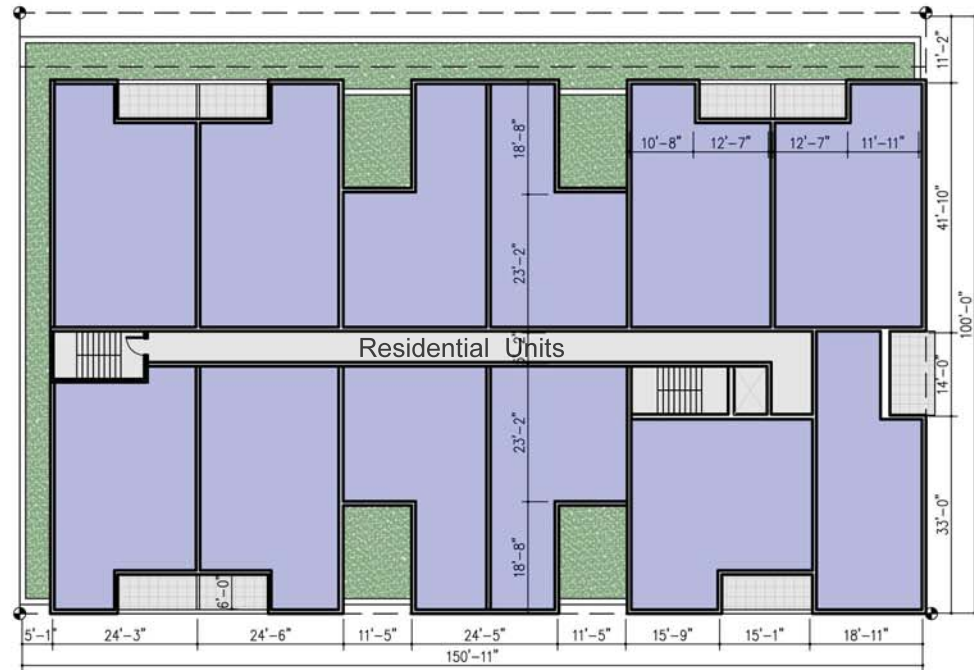
PL2.A.1  
Street level entry points meet sloping sidewalk elevations to ensure access for all

DC2.B.1  
Decks create façade modulation, outdoor space and shade for the east (alley) facing units.

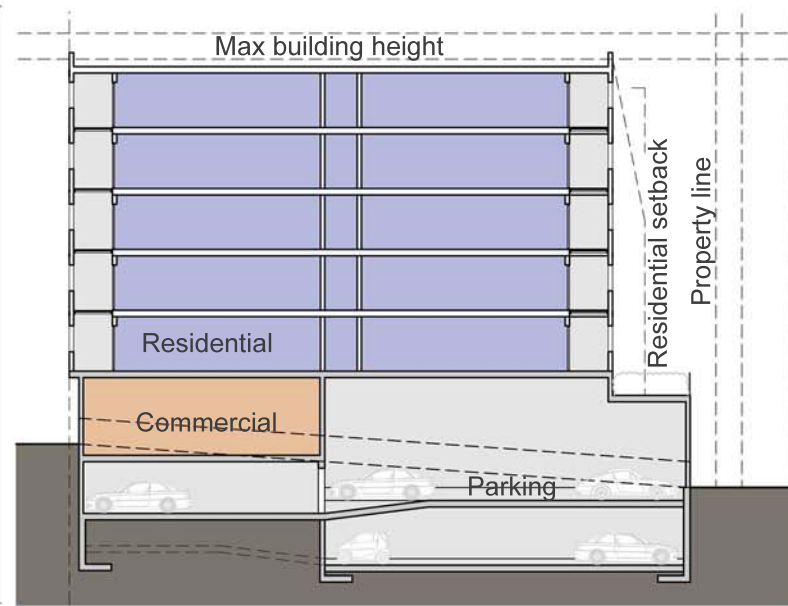
DC2.B.2  
Wall climbing planting at ground floor alley level to conceal parking garage wall and provide a softer transition to the adjacent LR3 zone across the alley.

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**OPTION 3 - MASSING MODULATION**



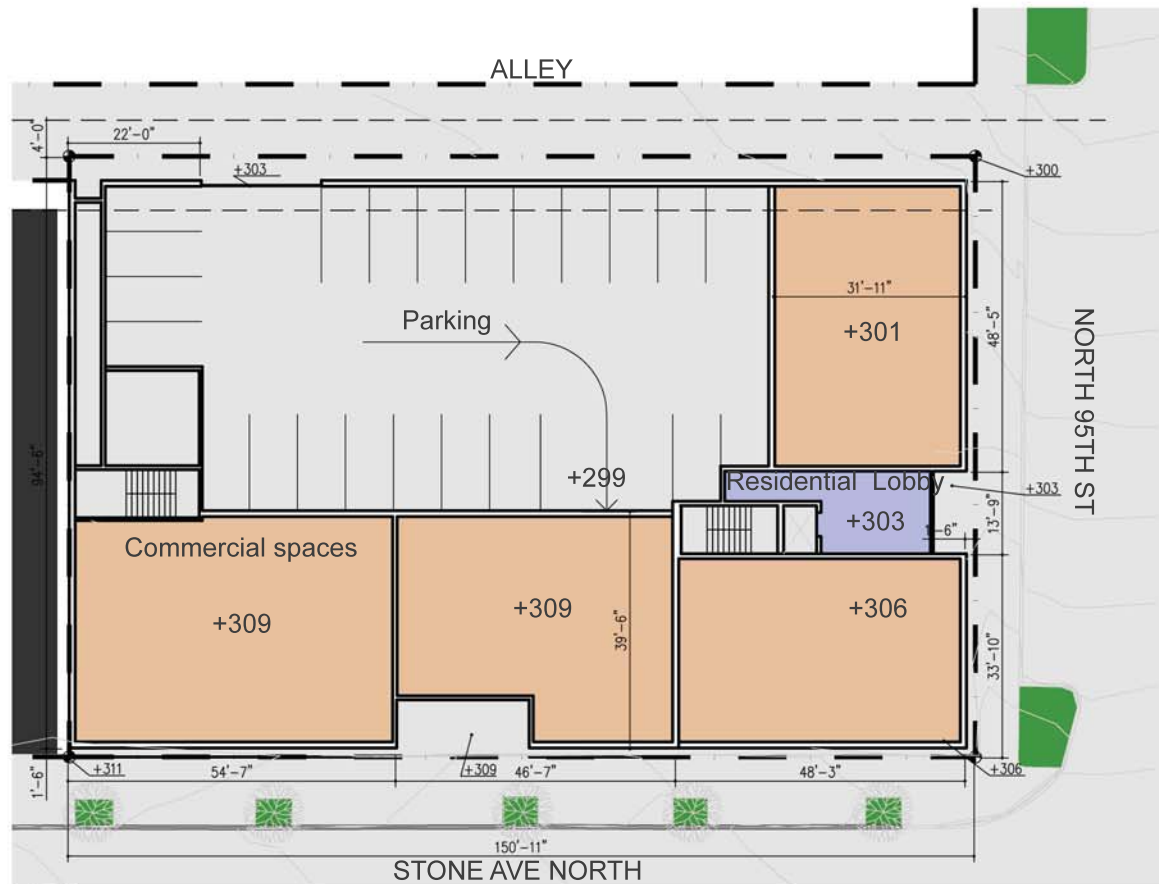
← TYPICAL RESIDENTIAL FLOOR 2-6



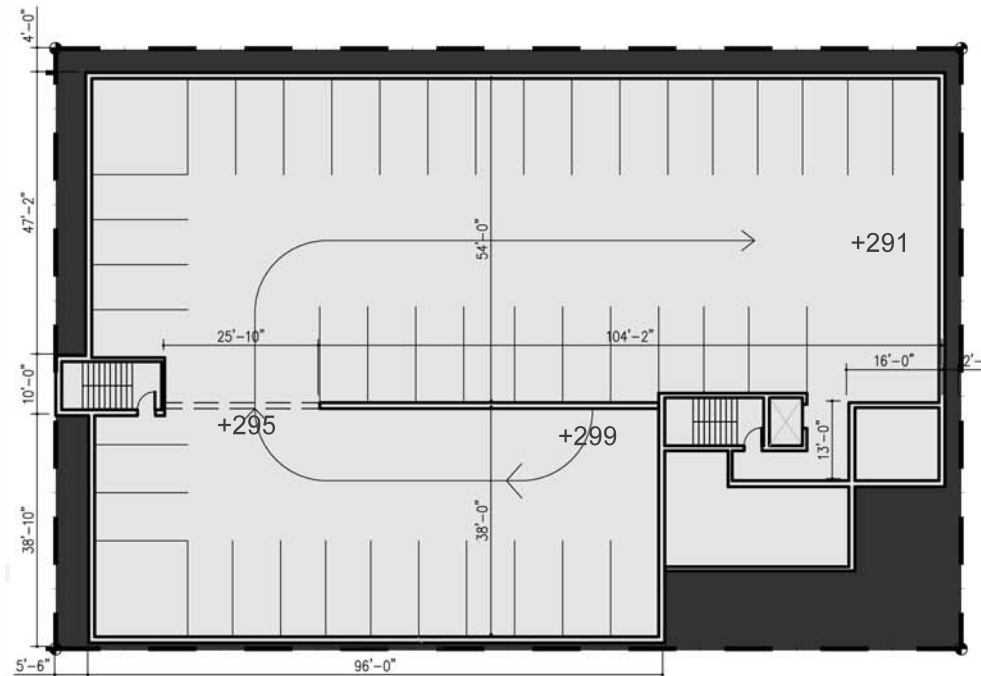
EAST-WEST BUILDING SECTION



SOUTHWEST STREET VIEW



← STREET LEVEL COMMERCIAL AND PARKING



← BELOW GRADE PARKING

**Building Features**

-6 stories above grade. Below grade parking, Ground floor commercial and parking, 5 floors of residential -Building massing modulation

**Pros-**

- Parking access from alley, existing curb cut removed
- Building massing modulation provides more exterior wall area for light/ventilation opportunities.
- Massing modulation reduces the perceived building mass at street and alley facades
- Increased onsite parking to accommodate residential units
- A variety of residential unit configurations for diversity
- No required departures

**Cons-**

- The façade modulation helps break the scale of the building but the form still reads as one continuous building form, quite large for the character of the neighborhood.
- Additional parking reduces leasable commercial space as it counts against total FAR when above grade

Proposed program: -Parking spaces- 64 enclosed parking spaces (5620sqft above grade, 12,400sqft below grade) -Street level commercial - (3) units minimum (7,280sqft) -Residential units - (59) Studio, 1,2 and 3 bedroom units (58,350sqft)

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**OPTION 3 - MASSING MODULATION**



EQUINOX @ 10AM



EQUINOX @ NOON



EQUINOX @ 2PM

DC2.A.2  
Modulation in building massing breaks overall scale of development.

DC2.B.1  
Enclosed/covered decks provide weather protection, privacy to the residents. From the exterior view the decks appear integral to the building and help conceal clutter on the individual decks.

DC2.C.1  
Massing modulation between commercial podium construction and residential wood framing above creates interest.

Contrasting materials (green/blue) to reinforce the form of the building.

CS1.B.1  
Jogs in building massing allows for additional windows and natural ventilation opportunities. The jogs also reduce the shadows cast to the east and west of the site.

PL1.A.2  
Refer to preliminary landscape plan for proposed planting, shade trees and widened sidewalks.



STREET VIEW PERSPECTIVE FROM STONE AVE NORTH



STREET VIEW PERSPECTIVE FROM NORTH 95TH ST AND INTERLAKE AVE NORTH

Community deck has great views and southern exposure and helps define the southern elevation

PL3.A.1  
Building massing helps identify residential entry.

DC2.B.1  
Simple exterior skin would rely on window placement and detailing to create interest.

CS2.D.4  
Massing modulation at the alley helps reduce building scale adjacent to L3 apartments and provides a more interesting façade.

CS2.D.3  
Opportunity for planting at concrete podium roofing between the residential setback and the alley. Planting would benefit the residential units above as well as soften the façade when viewed from the existing apartments and

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**PREFERRED OPTION 3  
SCHEMATIC MATERIALS**

Flat panel metal siding (horizontal). A natural gray tone on the metal siding would provide contrast to the elements with a vivid color.

Guardrail at west facing decks would use perforated metal panels to match the siding profile. The perforated material would appear seamless with the building siding but the transparency would change at the decks. The perforated metal provides some privacy and shade to the decks and apartments during the daylight hours. The perforated guardrail provides better sight lines to Stone Ave N and with no existing dwellings immediately to the west privacy is less of a concern on the west



South facing decks use open guardrail and greater depth to provide summer shade to the apartment unit. The vertical stack of open decks breaks the southern façade and helps identify the residential entry point.

VIEW OF STONE AVE N AND N 95TH ST ELEVATIONS

Metal window awnings provide additional weather protection and shading on the south façade.

Proposed shade trees at North 95th St provide sun protection for the street level commercial units and the sidewalk.



Cladding at the East facing decks would be solid metal panels to match the siding profile. The opaque deck guardrail will provide privacy between the apartments and the adjacent dwellings across the alley to the east. In addition to privacy the solid guardrail also provides sun/weather protection and conceals items stored on the decks.

Wall climbing planting and planters along the parking garage wall at the alley setback softens the transition to the adjacent LR3 zone.

VIEW OF SOUTHERN ELEVATION AND ALLEY

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**PREFERRED OPTION 3  
SCHEMATIC MATERIALS**

Lower center of building at east (alley) façade.



Elevated stair towers are clad in a contrasting vivid color.

Planters at the concrete podium facing the alley to soften the transition to the adjacent dwelling across the alley.

Proposed right-of-way planting and proposed shade trees per landscape plan (page 18). The project would like to increase the sidewalk/planting width along North 95th St. to provide sun protection the street level, increase pedestrian safety/comfort and provide a buffer from the adjacent use to the south.

AERIAL VIEW OF EAST (ALLEY) FACADE

Clad the "building core" in a vivid color to contrast the neutral metal siding panels.

The recessed decks would be clad with a saturated color to contrast the surrounding metal siding and guardrail panels. The vivid color within the recessed decks would especially stand out at night with interior lights on.

Step in building mass provides opportunity for planters.

Awning at street level for weather and sun protection. Three separate awnings would be used to identify the 3 commercial spaces facing Stone Ave N.



The street level concrete podium could be exposed architectural concrete or a dark charcoal face brick.

AERIAL VIEW OF WEST (STONE AVE N) FACADE

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# Street Level Landscape plan



North 95th ST

- Existing indented parking isle would be changed to planting/trees to match the east side of the alley
- Line of Existing curb (remove existing curb cut), proposed sidewalk design would reduce the width of North 95th st and provide a planting strip and street tree opportunity between the sidewalk and street parking
- Planting beds with ground cover
- Proposed shade trees at North 95th St. Height limited by utility line above (pending relocating low voltage utilities)
- Existing power line and low voltage utilities. Based on initial site meetings with Seattle City light the high voltage power line can be relocate to avoid a required setback on the south side of the building. The remaining lines should be far enough from the property line to trigger any require setback. The contractor will need to coordinate protection and working clearances during construction.

Existing street trees at Stone Ave North to remain.

Enlarge existing planting beds below existing street trees along Stone Ave N typical (ground cover planting)

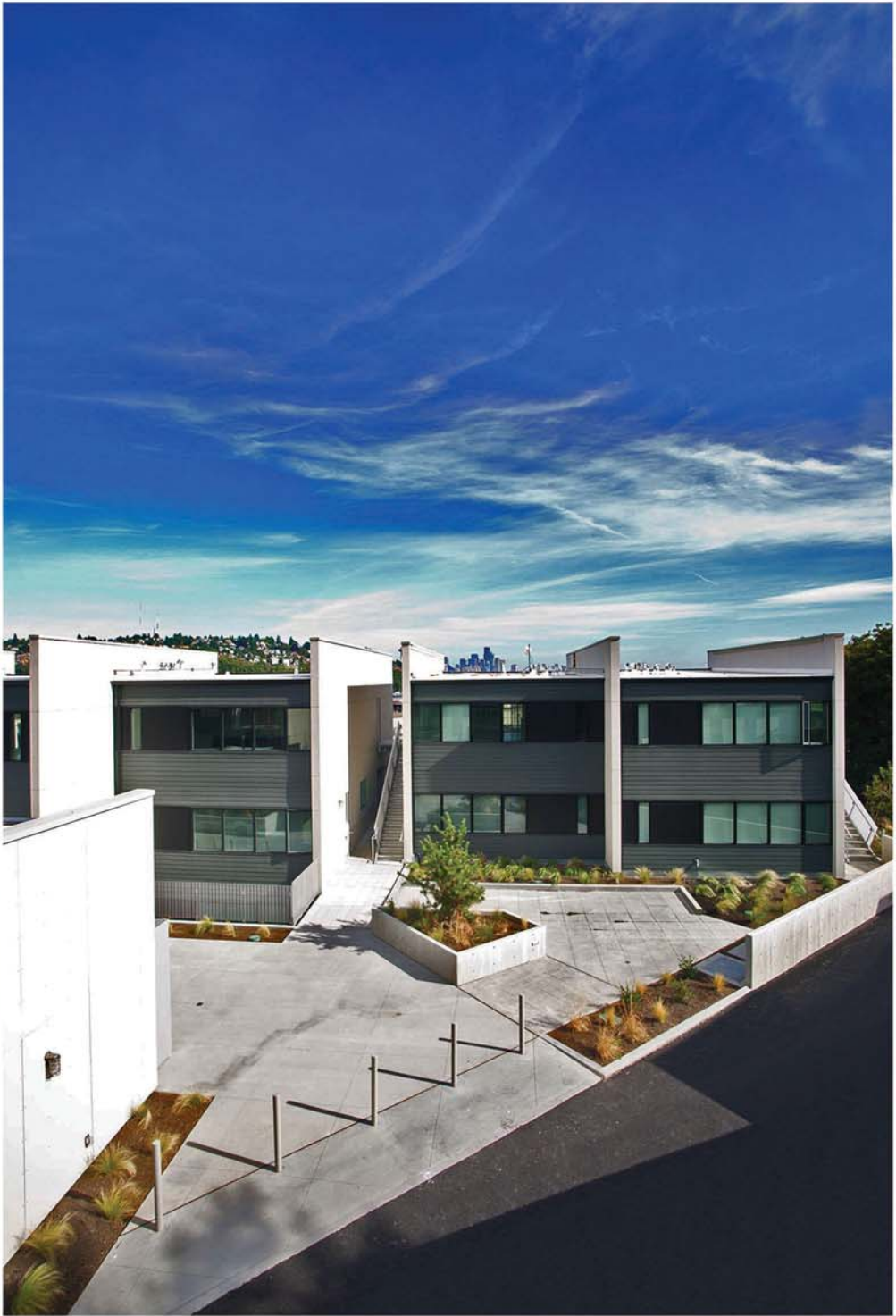


Stone Ave North

911 Western Ave #318  
 Seattle, WA 98104  
 206.287.0136  
 www.cobbarch.com

E. Cobb Architects

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911 Western Ave #318  
Seattle, WA 98104  
206.287.0136  
www.cobbarch.com

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