



## Early Design Guidance

November 24th 2014

## Cubix Apartments

1008 109<sup>th</sup> St., Seattle DPD Project #3017565

■ STRICKER CATO MURPHY ARCHITECTS, P.S. ■



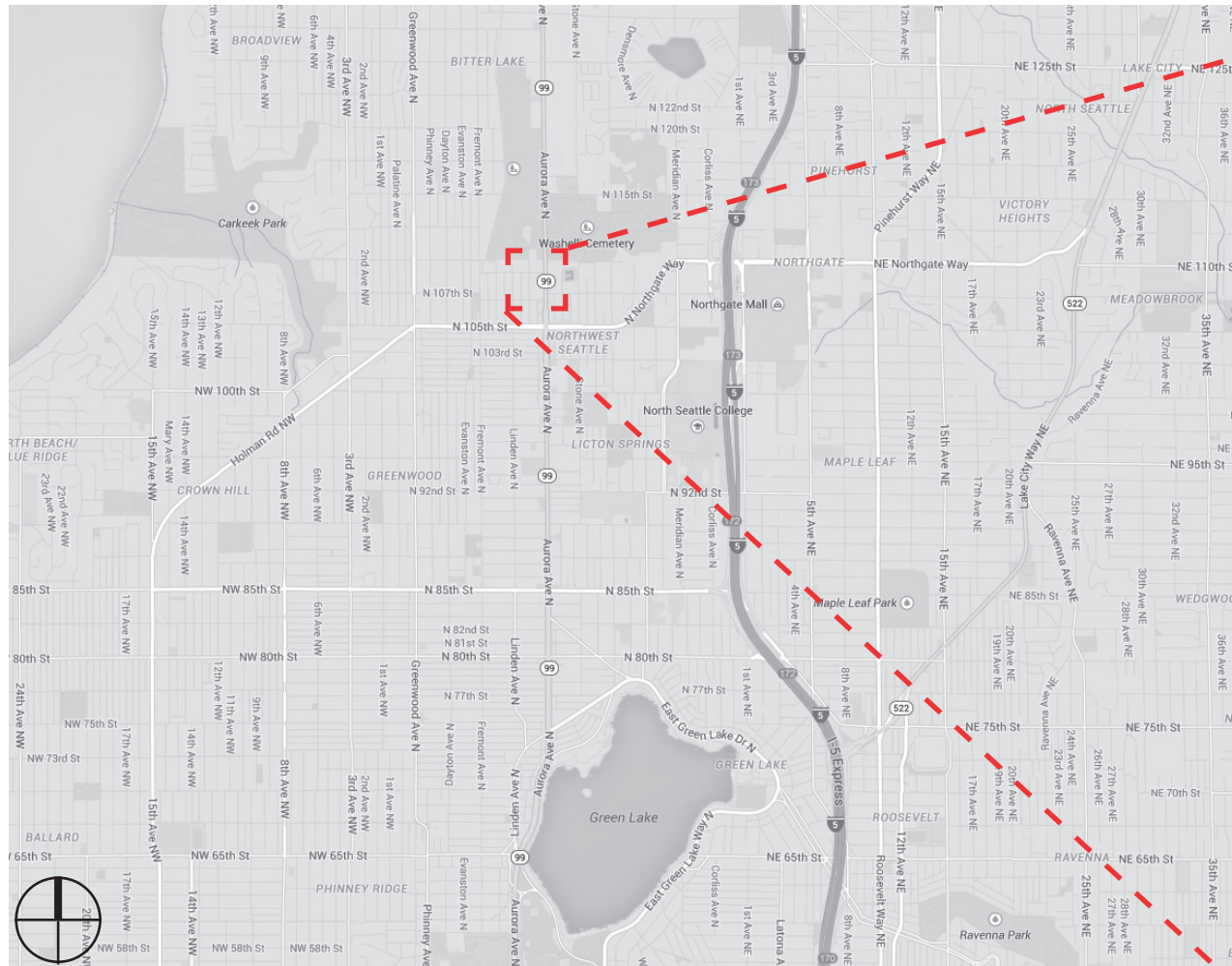


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# Cubix Apartments

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⊕ Nine Block Area Around Site

**Building Data:**

Lot Area: 13,282 sf per survey  
 (14,107 sf per King Co)  
 Zone: C1-40  
 Height Limit: 40' (44' with 13' ftf at level one)  
 FAR: 3.25 for mixed use development  
 Setbacks: None required  
 Parking: None required  
 ECA's: None

**Units:**

Modular: 93  
 Current Unit SF: 226  
 Live Work Units : 12  
 Average SF: 247

**Amenity Space for Efficiency Units:  
 (Option #1)**

Gross SF Three Floors: 28,617  
 Amenity Space Req 5%: 1,431  
 Current Planed: 1,464  
 Parking: 13 (9 compact & 4 large)  
 Bike Storage: 75

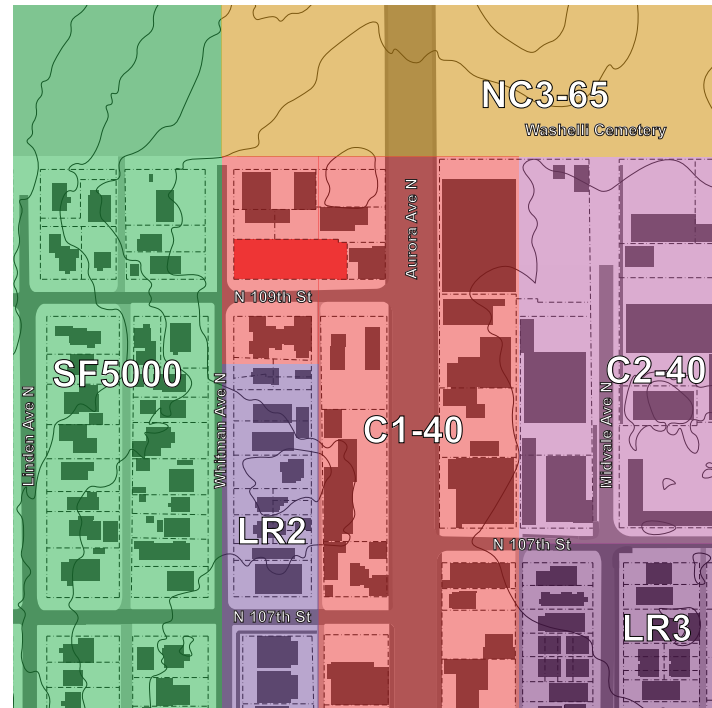
**Cubix Apartments**

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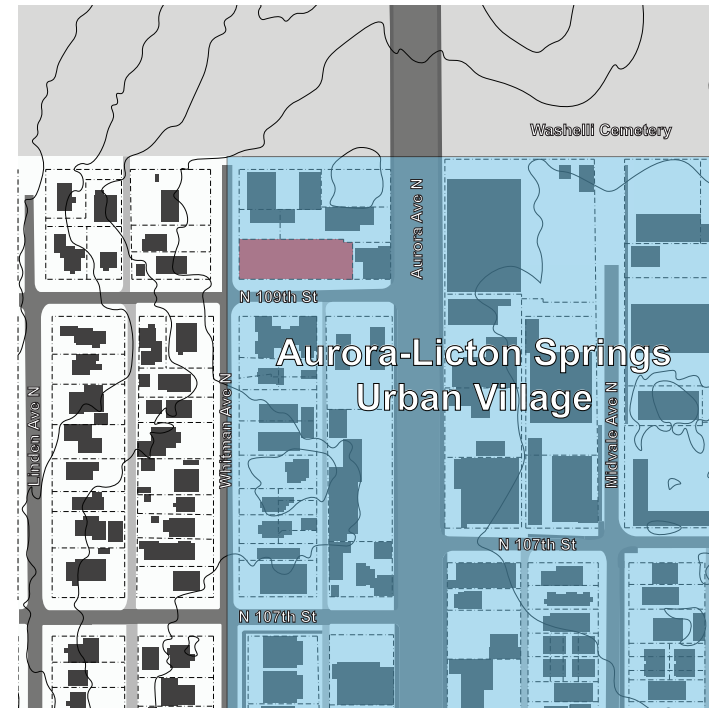
**Proposal  
 Overview**



Three-Dimensional Model of Site Area



⊕ Existing Zoning: C1-40



⊕ Urban Village: Aurora - Licton Springs

RadidRide E Line Stop  
0.3 mi either way



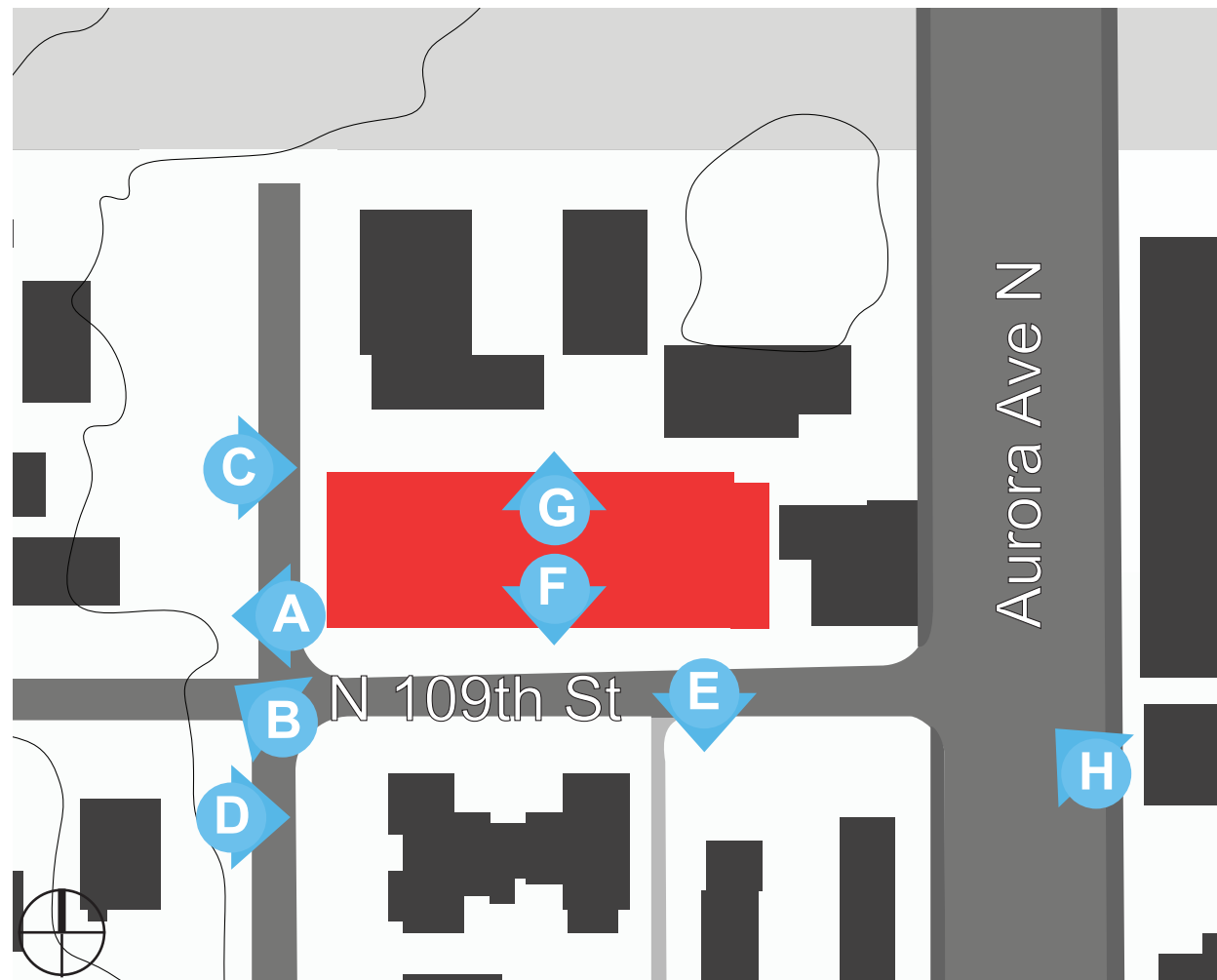
⊕ Local Transit



⊕ Building Types

- 1008 109th St (Subject Property)
- Single Family Residence
- Multi-Family Residence
- Commercial
- Warehouse

**Context Analysis  
Vicinity Maps**



# 6 Panoramas

# Early Design Guidance

November 24th 2014





**Cubix Apartments**  
 1008 109th St., Seattle DPD Project #3017565





⊕ Nine Block

1008 109th St

Precision Sheet Metal Fab

JT&A Engineers

Fuzzy Buddy's Dog Day-care

Rose Corner Flower Shop

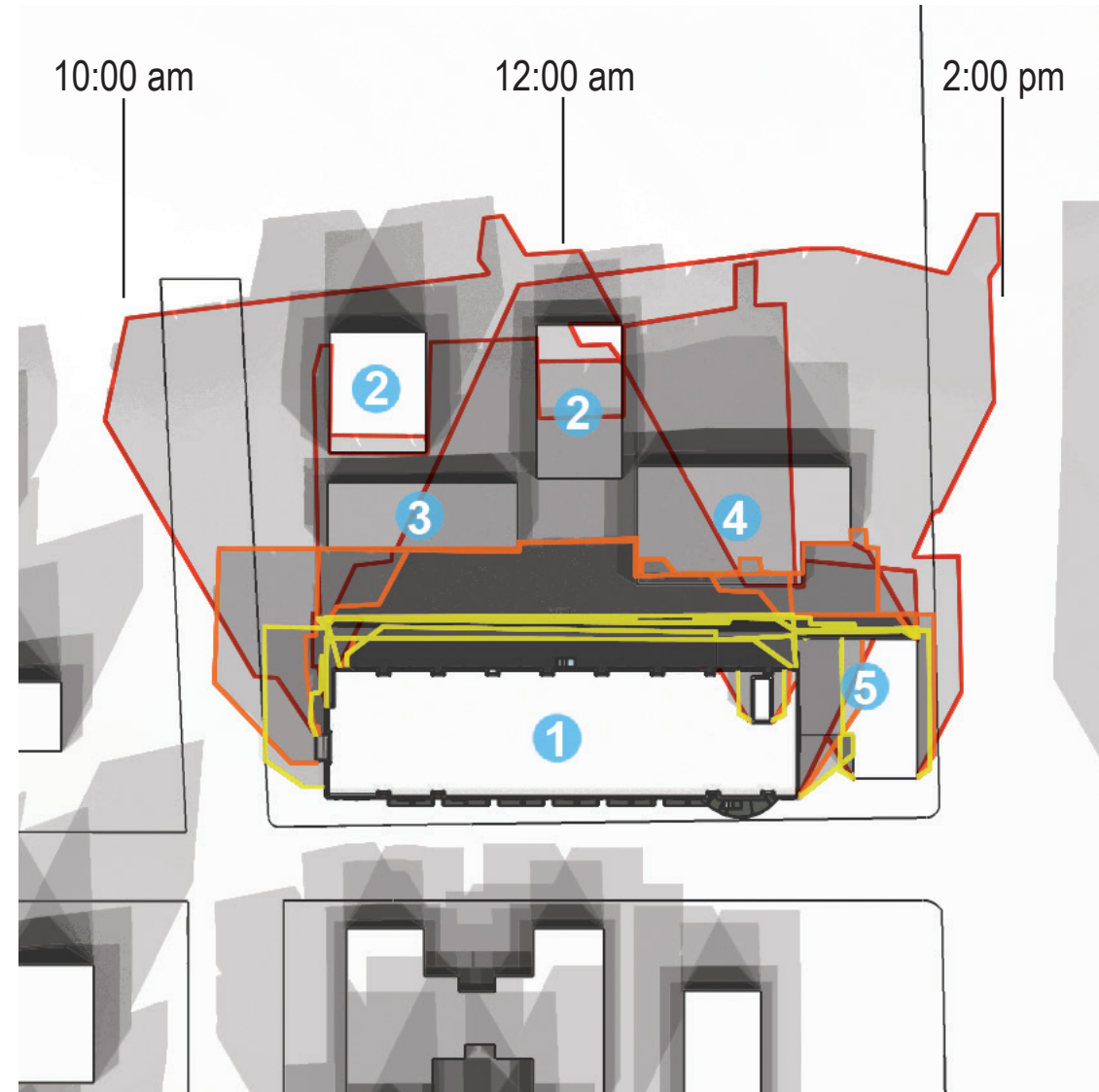
1

2

3

4

5



⊕ Sun Study

■ Winter Solstice (Dec 21)

■ Fall/Spring Equinox (March 21/Sept 21)

■ Summer Solstice (June 21)

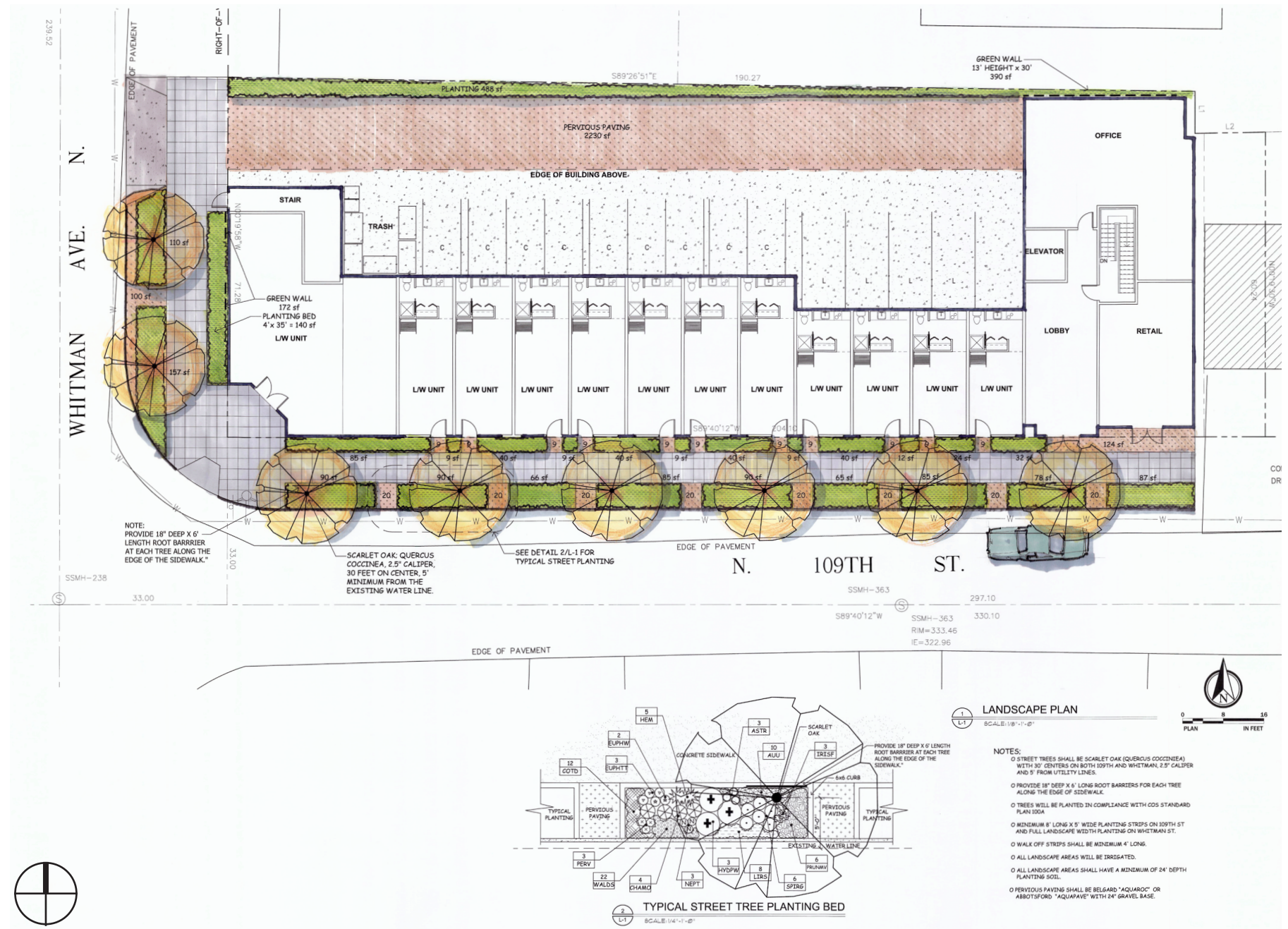
# Cubix Apartments

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Existing Site Conditions

## Sun Study

9



Landscape Plan



SSMH-238  
RIM=34.00  
N IE=10.00  
S IE=10.00

 Proposed Footprint

**Cubix Apartments**  
1008 109th St., Seattle DPD Project #3017565

**Site Plan**  
**Proposed Footprint 11**

### 23.41.004 Applicability

A. Design review required

e. Commercial (C1, C2) Four dwelling units or 12,000 square feet of nonresidential gross floor area, located on a lot in an urban center or urban village 1, or on a lot that abuts or is across a street or alley from a lot zoned single family, or on a lot located in the area bounded by: NE 95th St., NE 145th St., 15th Ave. NE, and Lake Washington

### 23.47A Commercial

23.47A.004 Permitted and prohibited uses

G. Live-work units

1. In all NC zones and C zones live-work units are permitted outright subject to the provisions of this title.

### 23.47A.008 Street-Level Development Standard

3. Height and depth provisions for new structures or new additions to existing structures. Non-residential uses shall extend an average depth of at least 30 feet and a minimum depth of 15 feet from the street-level street-facing facade. If the combination of the requirements of Sections 23.47A.005 or 23.47A.008 and this depth requirement would result in a requirement that an area greater than 50 percent of the structure's footprint be dedicated to non-residential use, the Director may modify the street-facing facade or depth requirements, or both, so that no more than 50 percent of the structure's footprint is required to be non-residential. Non-residential uses at street level shall have a floor-to-floor height of at least 13 feet.

### 23.47A.012 Structure Height

A. The height limit for structures in NC zones or C zones is 30 feet, 40 feet, 65 feet, 85 feet, 125 feet, or 160 feet, as designated on the Official Land Use Map, Chapter 23.32. Structures may not exceed the applicable height limit, except as otherwise provided in this Section 23.47A.012. Within the South Lake Union Urban Center, any modifications or exceptions to maximum structure height are allowed solely according to the provisions of the Seattle Mixed Zone, subsections 23.48.010.B.1, 23.48.010.B.2, 23.48.010.B.3, 23.48.010.E and 23.48.010.F, and not according to the provisions of this Section 23.47A.012. An overlay district may increase or reduce the maximum structure height.

1. In zones with a 30 foot or 40 foot mapped height limit:

a. The height of a structure may exceed the otherwise applicable limit by up to 4 feet, subject to subsection 23.47A.012.A.1.c, provided the following conditions are met:

1) Either

- a) A floor-to-floor height of 13 feet or more is provided for nonresidential uses at street level; or
- b) A residential use is located on a street-level, street-facing facade, and the first floor of the structure at or above grade is at least 4 feet above sidewalk grade; and

2) The additional height allowed for the structure will not allow an additional story beyond the number that could be built under the otherwise applicable height limit.

### 23.47A.024 Amenity area

A. Amenity areas are required in an amount equal to 5 percent of the total gross floor area in residential use, except as otherwise specifically provided in this Chapter 23.47A. Gross floor area, for the purposes of this subsection, excludes areas used for mechanical equipment and accessory parking.

B. Required amenity areas shall meet the following standards, as applicable:

1. All residents shall have access to at least one common or private amenity area;
2. Amenity areas shall not be enclosed;
3. Parking areas, vehicular access easements, and driveways do not count as amenity areas, except that a woonerf may provide a maximum of 50 percent of the amenity area if the design of the woonerf is approved through a design review process pursuant to Chapter 23.41
4. Common amenity areas shall have a minimum horizontal dimension of 10 feet, and no common amenity area shall be less than 250 square feet in size;
5. Private balconies and decks shall have a minimum area of 60 square feet, and no horizontal dimension shall be less than 6 feet.

Council Bill Number: 118201

Ordinance Number: 124608

AN ORDINANCE relating to land use and zoning; establishing a definition for small efficiency dwelling unit; clarifying standards for configuration of dwelling units; amending development standards for congregate residences; amending design review thresholds; clarifying the application of green factor landscaping requirements to congregate residences; amending income eligible household definitions for incentive programs related to small efficiency dwelling units and congregate residences; and modifying vehicle, bicycle and Restricted Parking Zone regulations for small efficiency dwelling units and congregate residences; amending Sections 11.16.315, 23.41.004, 23.45.504, 23.45.508, 23.45.524, 23.47A.004, 23.47A.016, 23.54.015, 23.54.040, 23.58A.004, and 23.84A.008 of the Seattle Municipal Code; and adopting new Sections 23.42.048 and 23.42.049.

3. For the purposes of this subsection 23.42.048.A, a separate or separable area is an area having direct access to the exterior of the building or access to the exterior via hallways and stairways that are primarily ingress/egress routes to the exterior rather than leading to common kitchens and living areas.

B. Small efficiency dwelling units. In all zones small efficiency dwelling units are subject to the following standards. Small efficiency dwelling units are also subject to additional standards specified in the Seattle Building Code and any Director's Rule making interpretation thereof.

1. Sleeping room net floor area. Each small efficiency dwelling unit shall have a sleeping room that has at least 150 net square feet of floor area. The floor area occupied by bathrooms, cabinets, closets, appliances, and structural features, is not included in calculating the net floor area.

2. Total floor area. The total floor area of a small efficiency dwelling unit, inclusive of bathrooms, cabinets, closets, appliances, and structural features shall be at least 220 square feet.

1. Design review is required for any new multifamily, commercial, or industrial development proposal that exceeds one of the following thresholds in Table A for 23.41.004:

#### Table A for 23.41.004

i. All zones – congregate residences, and residential uses in which more than 50 percent of dwelling units are small efficiency dwelling units.

#### Table B for 23.54.015

Parking for residential uses

II. Residential Use Requirements For Specific Areas

M.

All residential uses in commercial and multifamily zones within urban villages that are not within urban center or the Station Area Overlay District, if the residential use is located within 1,320 feet of a street with frequent transit service, measured as the walking distance from the nearest transit stop to the lot line of the lot containing the residential use.(1)

No minimum requirement

**CS1** The east west orientation of the building and the site provide optimal solar exposure. South facing units will receive maximum natural direct daylighting, while northern units will also receive ample ambient light due to the topography and existing building heights. The existing topography allows for on grade parking with minimal excavation, and a continuous and accessible street facade for community engagement. The use of previous pavement in multiple locations will reduce the impact on the storm sewer system. Street trees and indigenous plantings have been included in the public ROW to provide a sense of place and a connection to the natural surroundings in the neighborhood.

**CS2** The location of the site is one lot off of a major high volume street, which allows us to provide more of a gateway to the neighborhood than a street facade for Aurora. The project is a corner lot which allows for a street front corner retail opportunity to engage the neighborhood and provide a gathering place. The project will have developed sidewalks, which are absent from the surrounding neighborhood. These spaces will provide street front connection to the live work spaces and any retail on the ground floor. Bicycle parking will be provided on the public ROW and within the building. The location is also within 6 min walk to the Rapid Ride E line.

**CS3** The Aurora Licton Springs Urban Village is an established neighborhood but the revitalization of the Aurora corridor with an emphasis on public transit gives the project the opportunity to set an example of a good balance of density and public space for the neighborhood. The modern design and modular construction methods preferred, embody the urban development zeitgeist of Seattle while remaining at the scale and intent of the urban Village.

**PL1** The sites street improvements will be the first in on the block and will set the tone for future improvement and connectivity. The Street front will connect to the Aurora transit corridor as well as providing a safe well lit and covered pathway across the site while engaging with the ground floor retail and live work spaces.

**PL2** The ground level retail and live work units provide an engagement and transparency to the facade from the street. The street improvements of indigenous plants and Scarlett Oak tree's provide a comfortable and safe environment. The previous pavement and varied textures along the sidewalk aide in separating functional zones between the live work occupants and passers by. All amenity's will be at grade with minimal thresholds providing maximum accessibility. The area closest to the building will be covered by a canopy extending most of the length of the facade allowing for weather protection across the site.

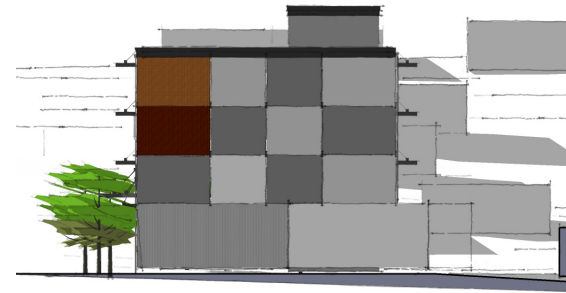
**PL4** Parking access is proposed in the rear of the site providing a prominent pedestrian focused environment for bicycles and building occupants accessing mass transit.

**DC1** Retail spaces have been located at the corners of the site and bookend the live work spaces to provide a fully engaged street presence along the South facade. The main lobby access is located on the front of the building. All parking and service access has been moved to the rear of the site off the one way dead end street as to not interfere with the flow to the main arterial of Aurora.

**DC2** The design intent of the building is perceived and amplified by the preferred modular construction method. The construction time and noise on site will be dramatically reduced, which will benefit the neighboring business's particularly the dog boarding facility to the north. All attempts have been made to modulate and define a character of the design that works with the modular construction but is not rigid and repetitive in its appearance. The first floor is designed at 13' to provide a more open experience on the street facade but all other floors maintain a human scale and the total building is only 40' allowing visual connection from the street to even the highest floors.

**DC3** The project will have a variety of public and private amenity's. Along with the ground floor retail and lobby the occupants will have access to a roof deck on the second floor that looks out to the north. Balcony's are proposed at the end of the public hallways to add light and views.

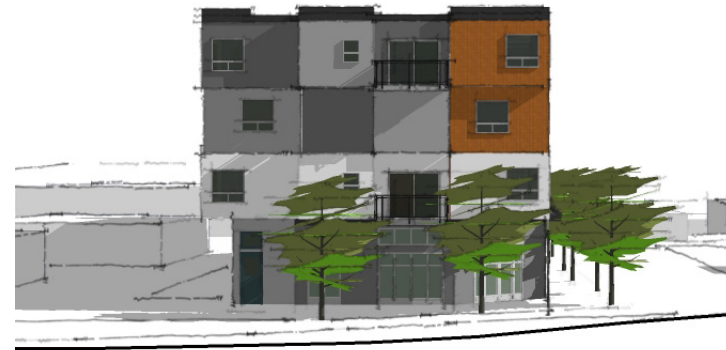
**DC4** Exterior elements and materials have been chosen for their durability in the inclement weather and to provide a palate of sustainable materials. The use of cement board and natural woods will allow for a diverse exterior while maintaining a connection to the industrial and residential nature of the immediate surroundings.



Elevation along Aurora Ave N



Elevation from Back of Site



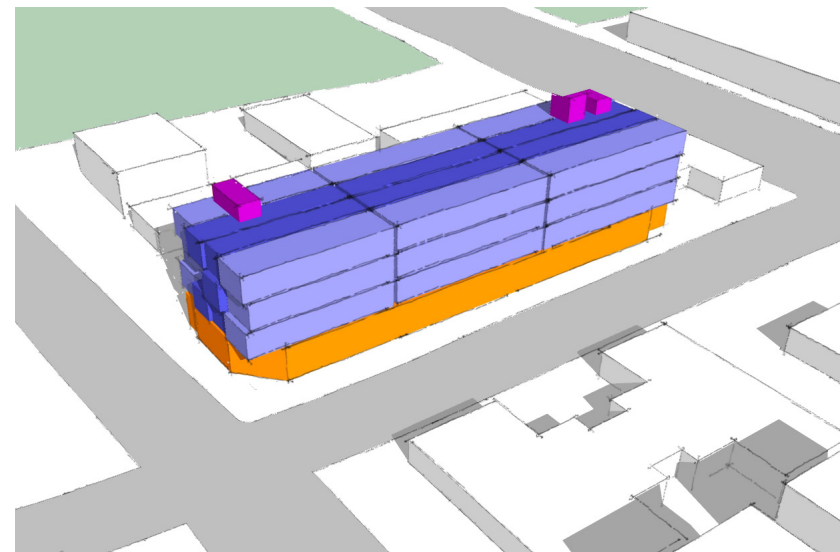
Elevation along Whitman Ave N



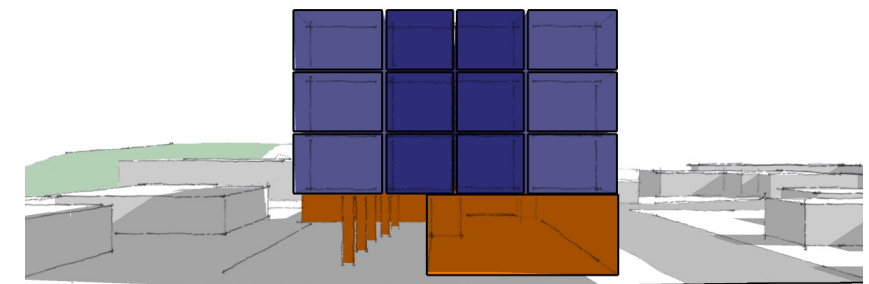
Elevation along 109th St



Perspective From Aurora Ave N



Massing from Whitman Ave N



Section through Massing

# 14 Option #1 - Modular with Parking (Preferred Option)

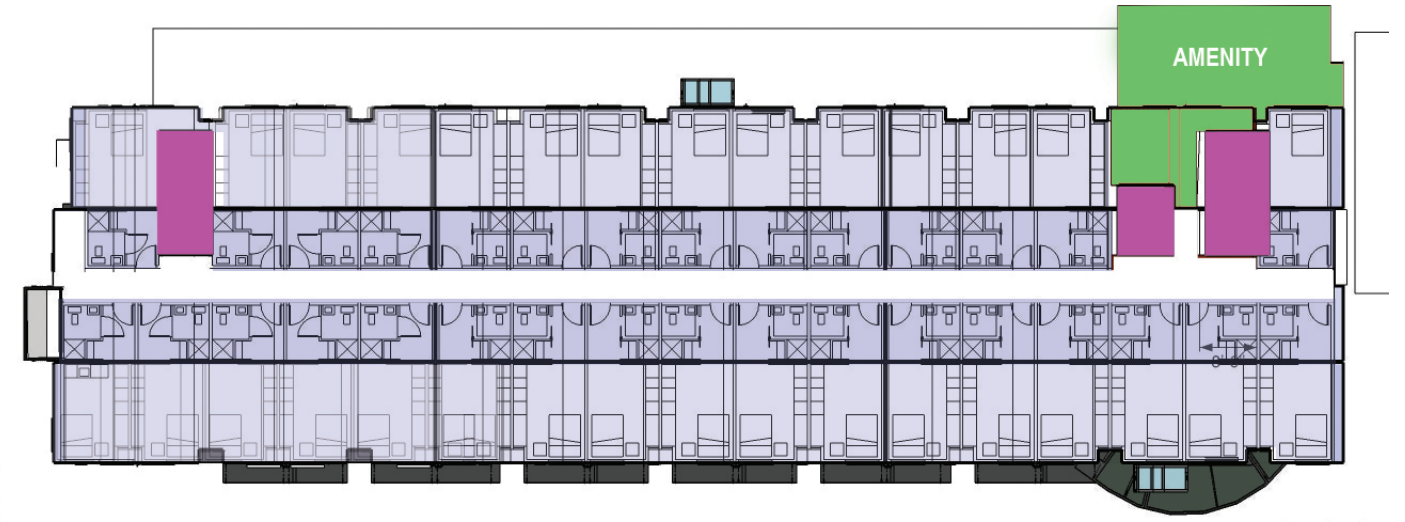
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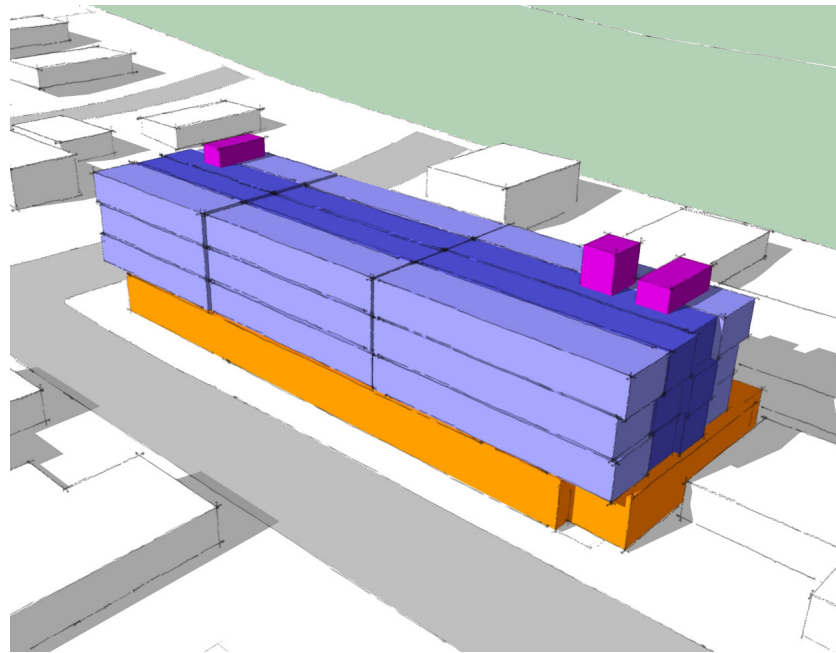




Perspective From Whitman Ave N



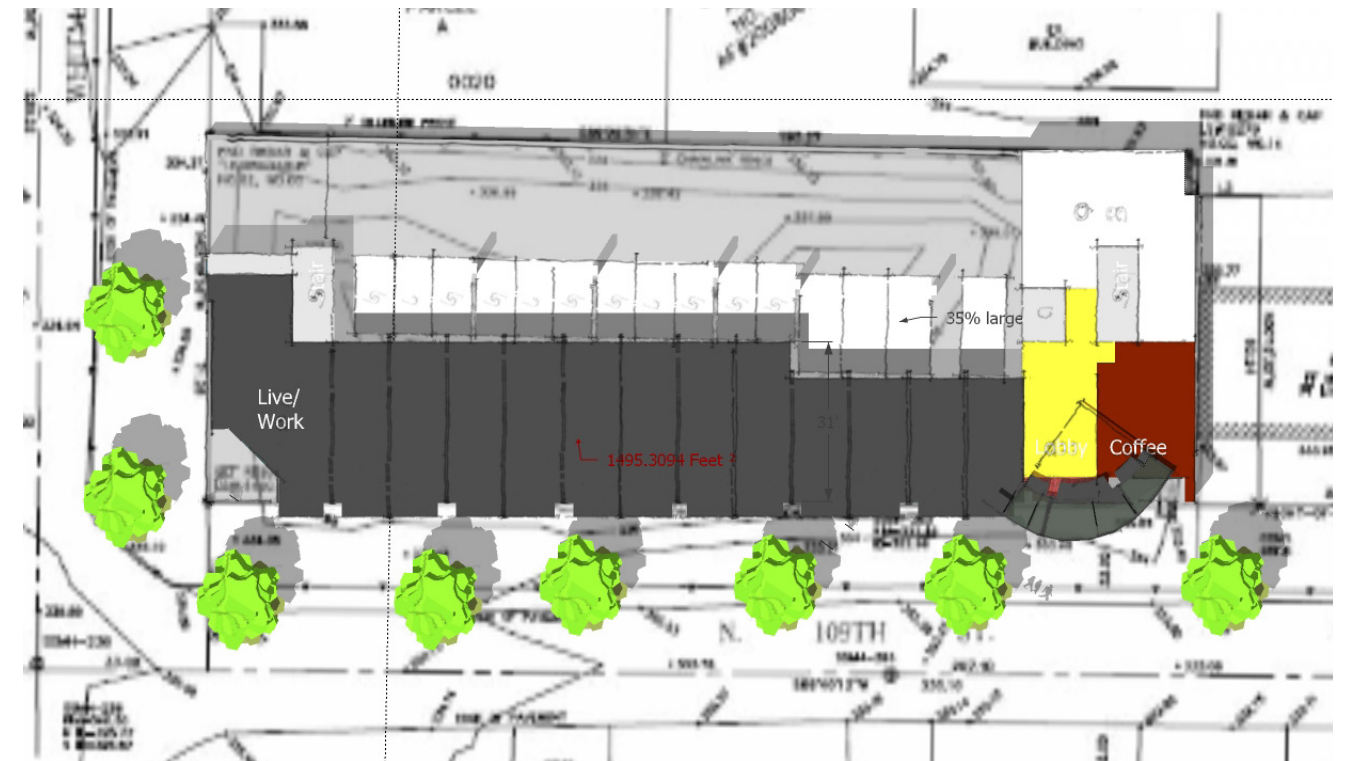
Second-Forth Floor Plan



Massing from Aurora Ave N

Parking	14
Total floor Area	38,991
FAR Proposed	2.94
FAR Allowed	3.25
Amenity Space Req.	1,430.85
Amenity Space Provided	1,464

- Stick Frame
- Modular
- Vertical Circulation



First Floor Plan

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**Architectural Concepts**  
**Option #1 - Modular with Parking**  
**(Preferred Option)**



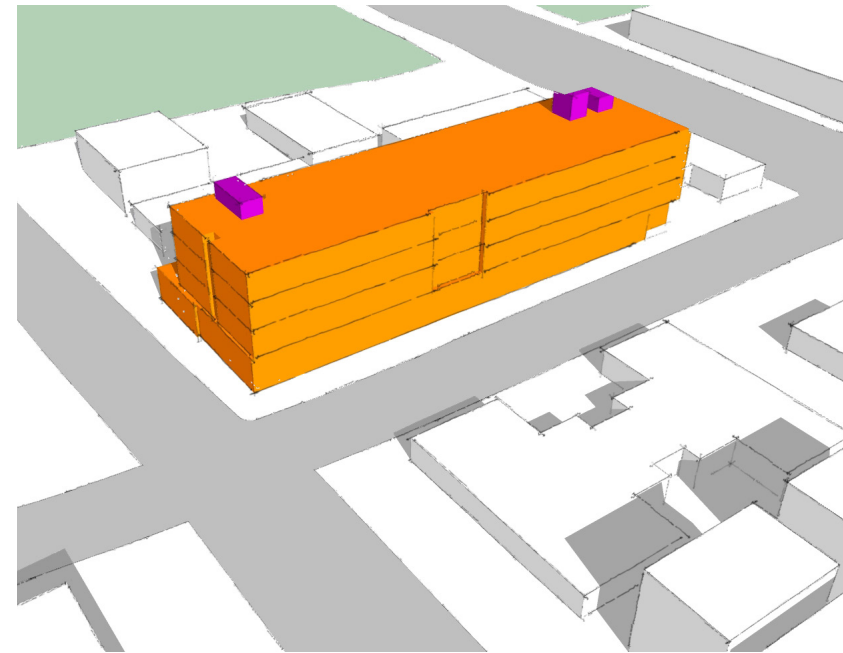
Elevation along Whitman Ave N



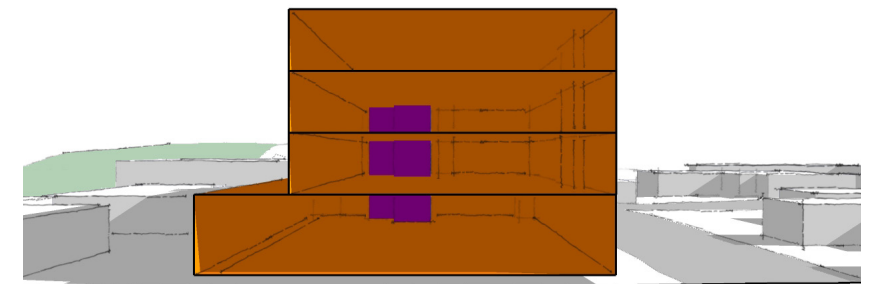
Elevation along 109th St



Perspective From Aurora Ave N



Massing from Whitman Ave N



Section through Massing

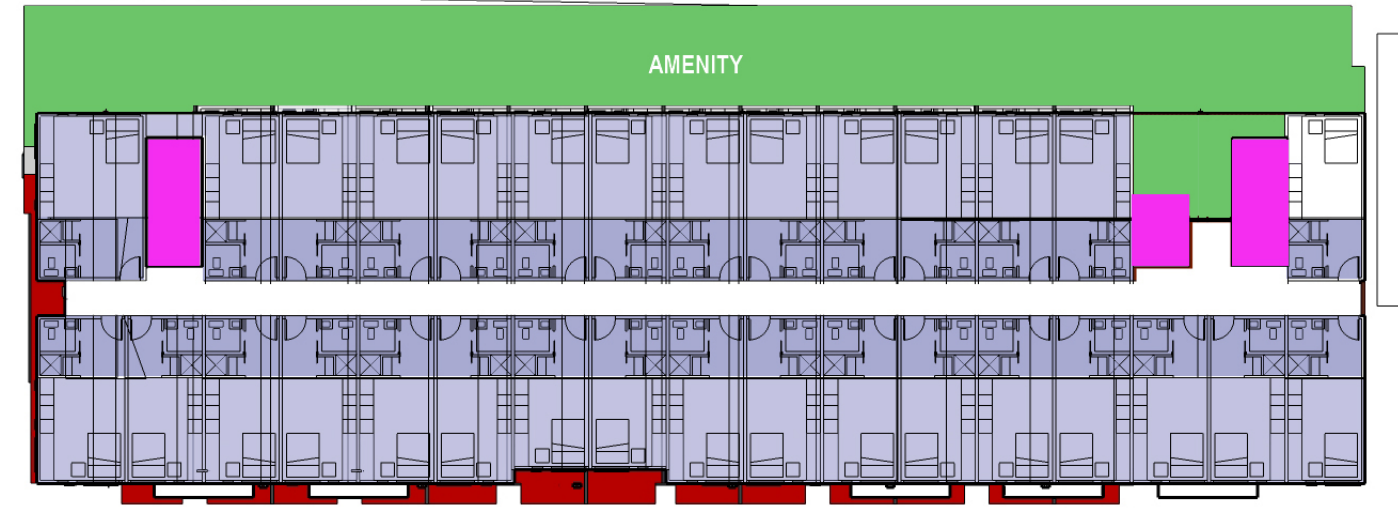
# 16 Option #2 - Site Built No Departures No Parking

## Early Design Guidance

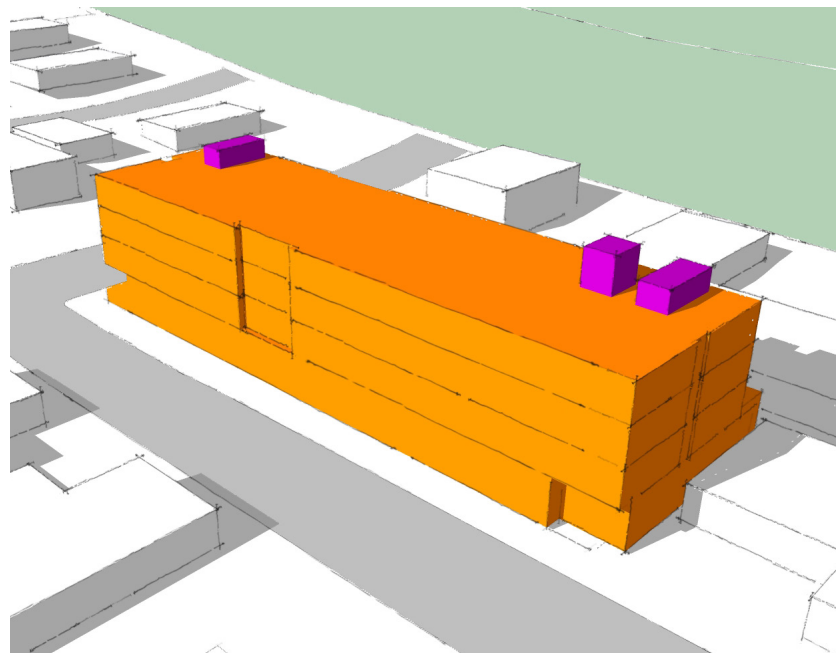
November 24th 2014



Perspective From Whitman Ave N



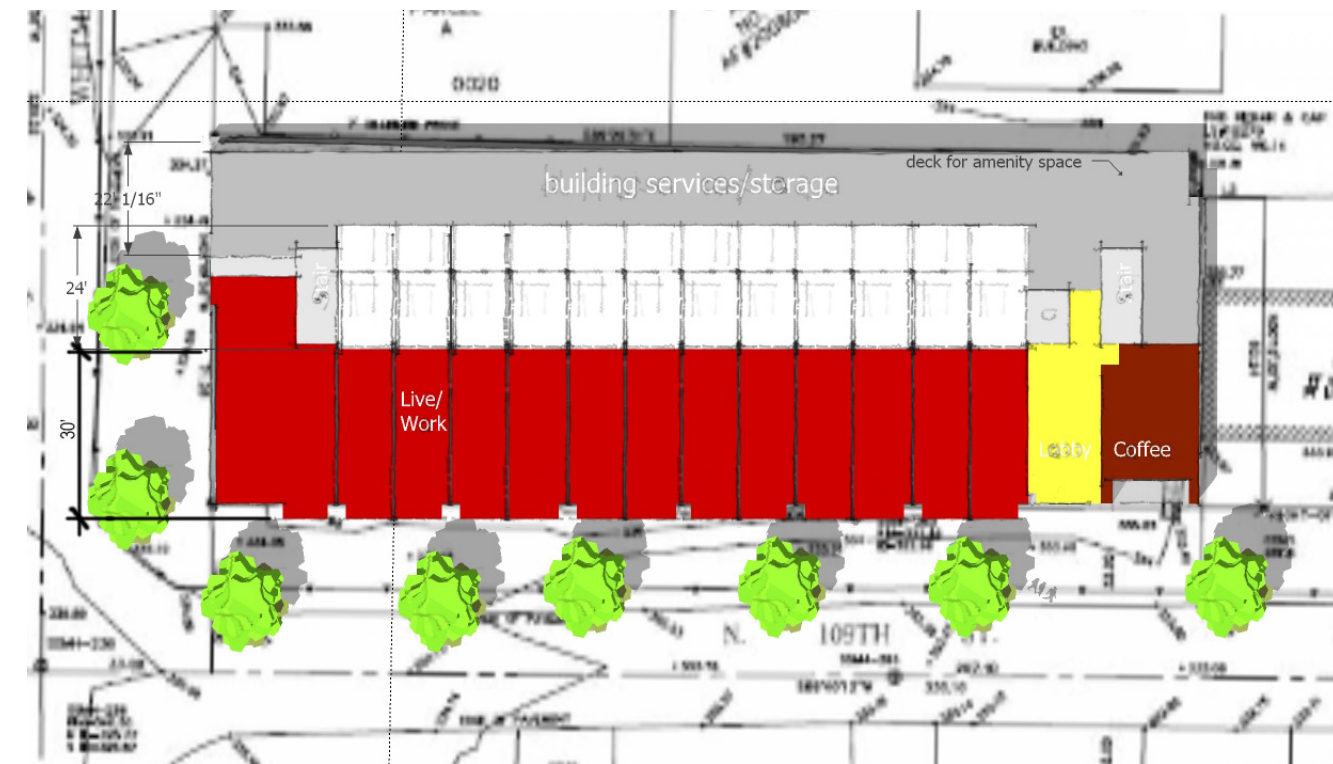
Second-Forth Floor Plan



Massing from Aurora Ave N

Parking	N/A
Total floor Area	41,531
FAR Proposed	3.13
FAR Allowed	3.25
Amenity Space Req.	1,430.85
Amenity Space Provided	3,834

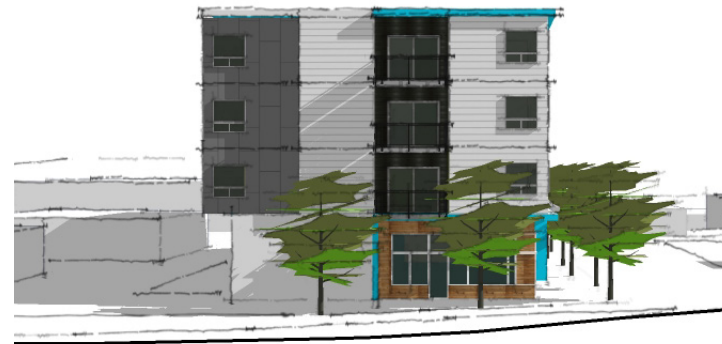
- Stick Frame
- Modular
- Vertical Circulation



First Floor Plan

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**Option #2 - Site Built No Departures No Parking 17**



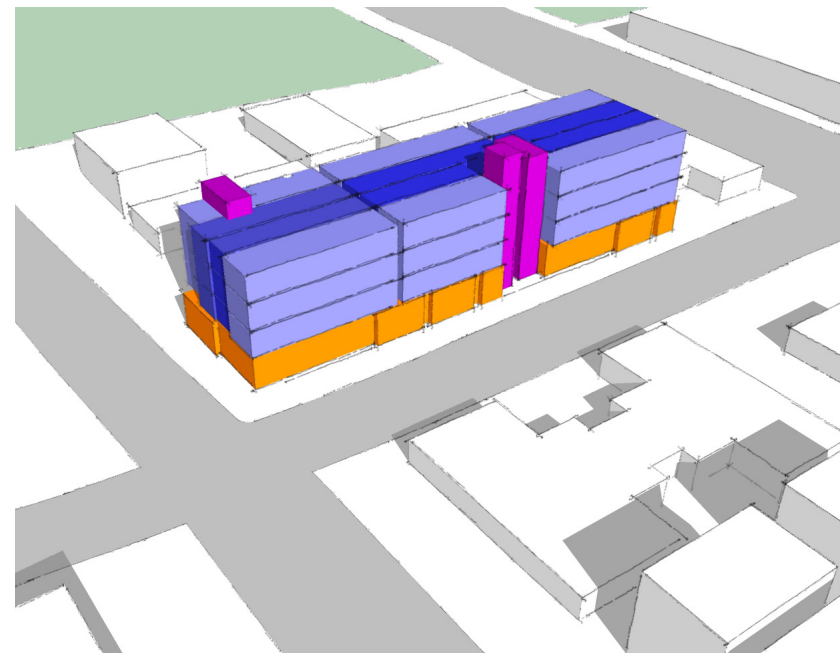
Elevation along Whitman Ave N



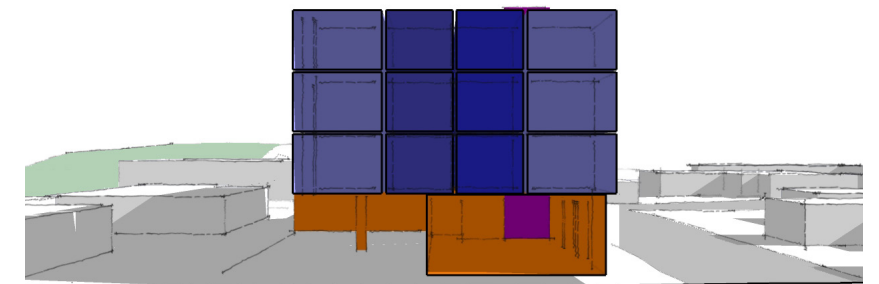
Elevation along 109th St



Perspective From Aurora Ave N



Massing from Whitman Ave N



Section through Massing

# 18 Option #3 - Modular Split Massing with Parking

## Early Design Guidance

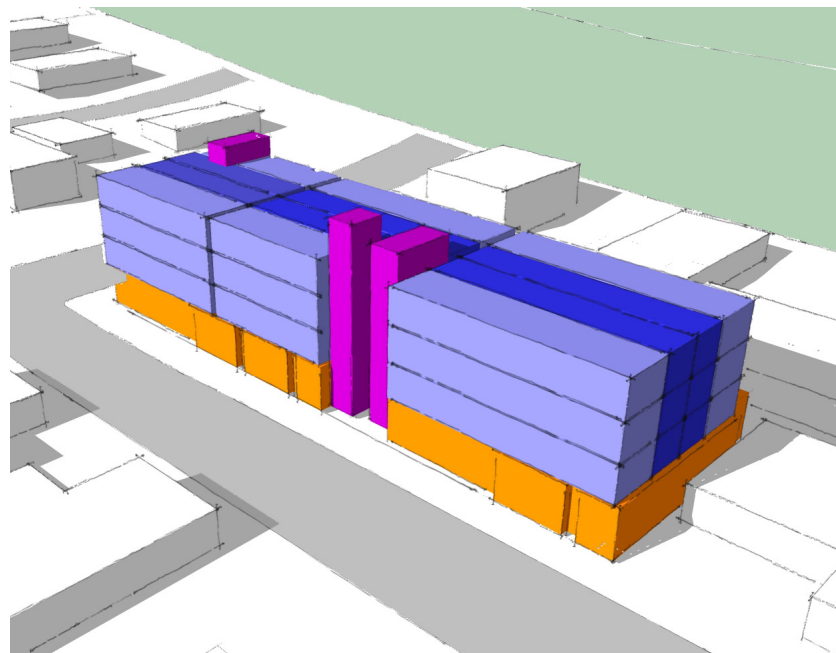
November 24th 2014



Perspective From Whitman Ave N



Second-Forth Floor Plan



Massing from Aurora Ave N

Parking	14
Total floor Area	38,991
FAR Proposed	2.94
FAR Allowed	3.25
Amenity Space Req.	1,430.85
Amenity Space Provided	1,464

- Stick Frame
- Modular
- Vertical Circulation

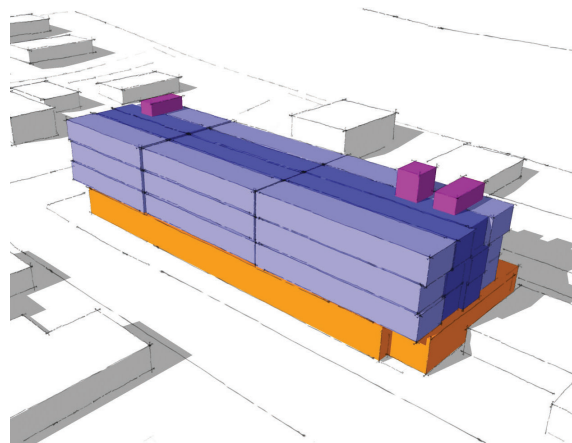


First Floor Plan

## Cubix Apartments

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## Option #3 - Modular Split Massing with Parking



Option #1 (Preferred Option)

**Details**

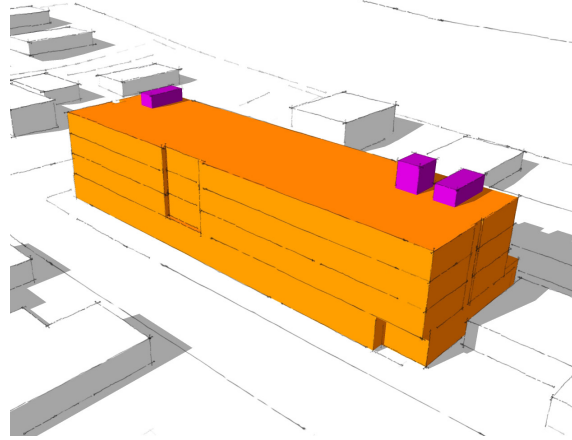
Parking:	13
Total Floor Area:	38,991
FAR Proposed:	2.94
FAR Allowed:	3.25
Amenity Space Req:	1,430.85
Amenity Space Provided:	1,464

**Pros**

Provides parking - Modular construction reduces waste and noise on site - does not max FAR while providing added density to neighborhood

**Cons**

Departure required - Continuous mass on street façade

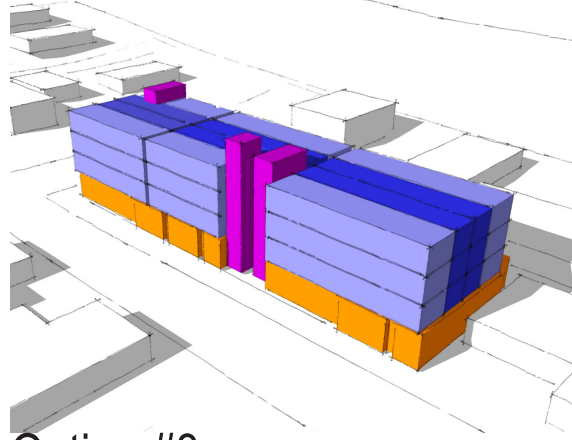


Option #2

Parking:	N/A
Total Floor Area:	41,531
FAR Proposed:	3.13
FAR Allowed:	3.25
Amenity Space Req:	1,430.85
Amenity Space Provided:	3,834

Provides max FAR - no departure required - traditional construction allows for increased modulation

No parking- Increased site noise and waste during longer construction time - less efficient use of ground floor space

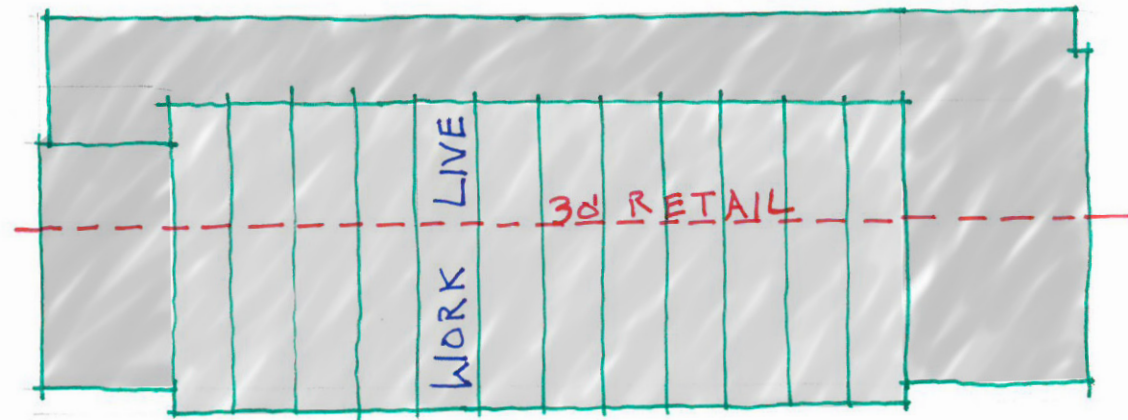


Option #3

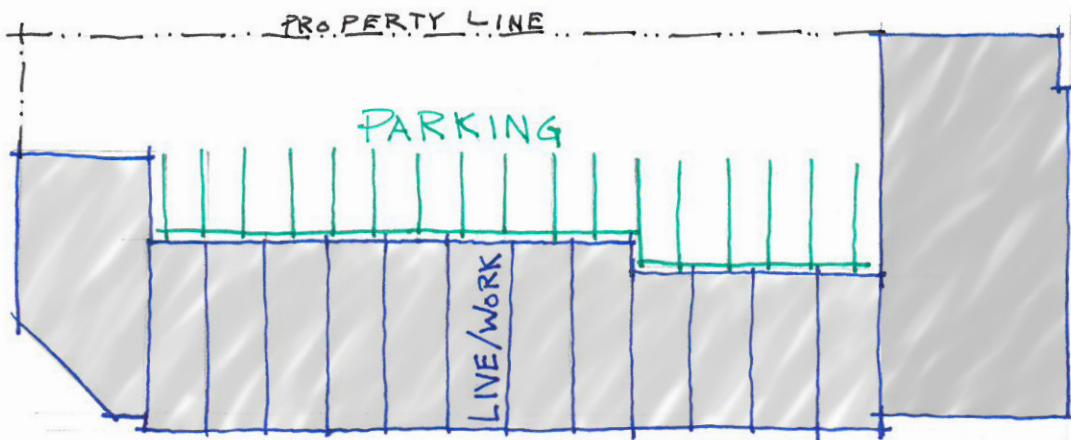
Parking:	13
Total Floor Area:	38,991
FAR Proposed:	2.94
FAR Allowed:	3.25
Amenity Space Req:	1,430.85
Amenity Space Provided:	1,464

Parking provided - Split mass on long block - clear pedestrian entry

Departure required minimal private outdoor space for residents- less units



Code Compliant



Departure

The only departure requested for this project applies to the first floor in option 1 and 3. The owner would like to have some parking on the site for residents. The urban village overlay requires no parking, it is at the discretion of the owner to reduce the FAR and add parking. The current code requires a 30' deep commercial use for Live / Work Units on the ground floor. Following the code would eliminate any possibility of parking on the site.

- We request for director approval to reduce the 30' retail depth requirement to allow the residential and commercial uses to coexist in the space provided, and allow on site parking.

**23.47A.008 Street-level development standard**

3. Height and depth provisions for new structures or new additions to existing structures. Non-residential uses shall extend an average depth of at least 30 feet and a minimum depth of 15 feet from the street-level street-facing facade. If the combination of the requirements of Sections 23.47A.005 or 23.47A.008 and this depth requirement would result in a requirement that an area greater than 50 percent of the structure's footprint be dedicated to non-residential use, the Director may modify the street-facing facade or depth requirements, or both, so that no more than 50 percent of the structure's footprint is required to be non-residential. Non-residential uses at street level shall have a floor-to-floor height of at least 13 feet.

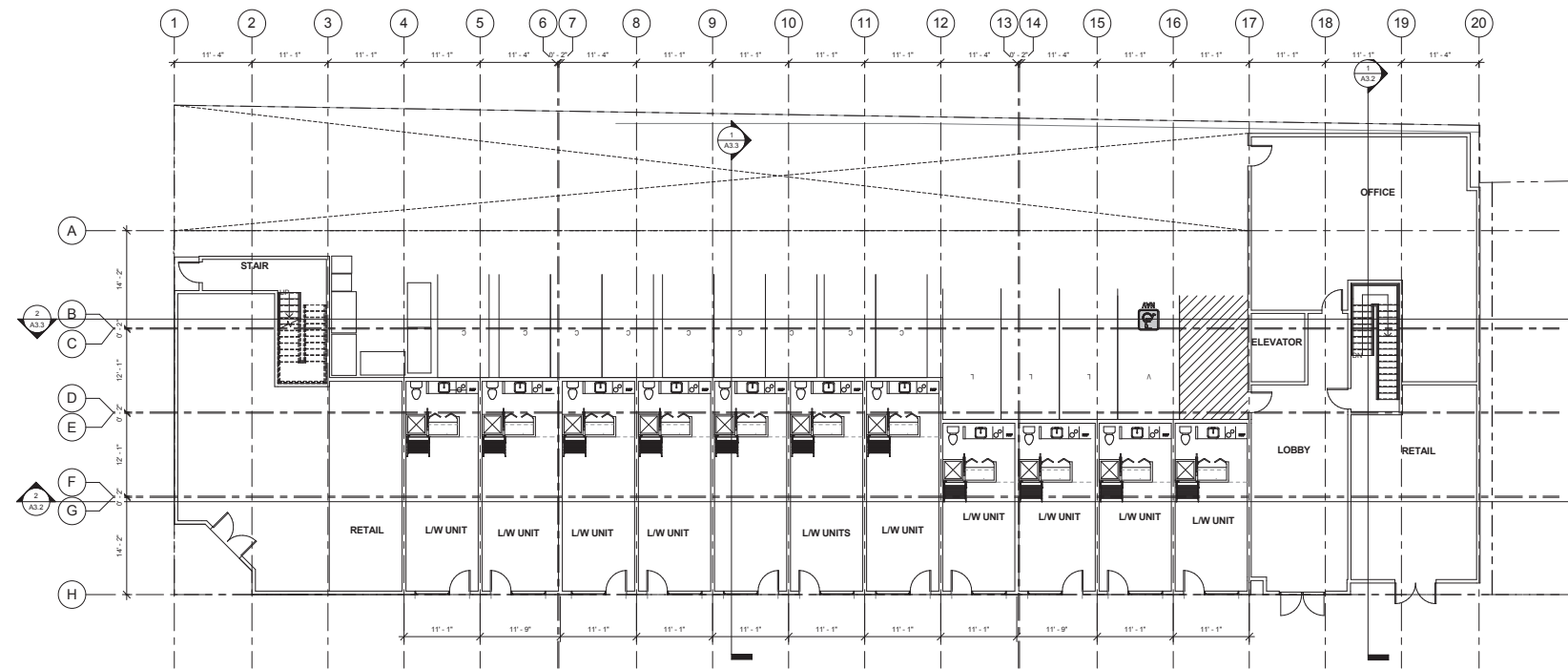
Table B for 23.54.015

**Parking for residential uses**

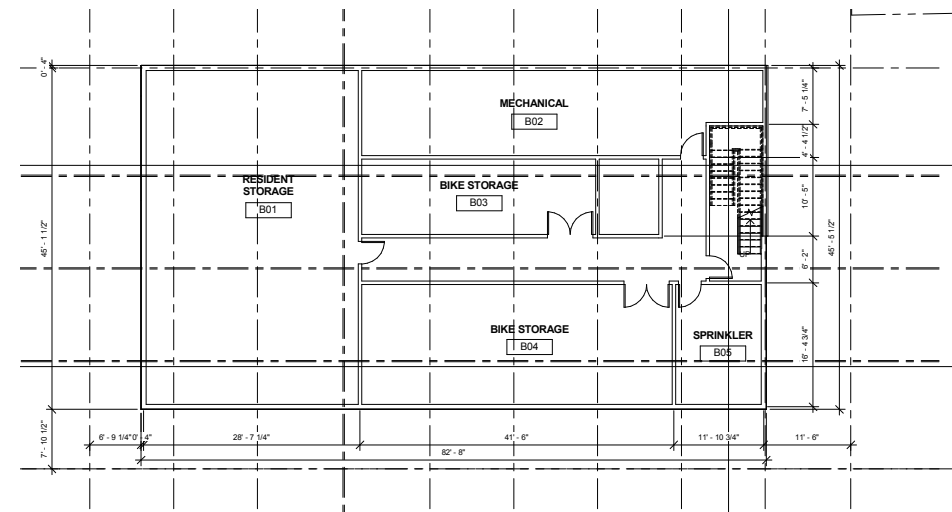
II. Residential Use Requirements For Specific Areas

M. All residential uses in commercial and multifamily zones within urban villages that are not within urban center or the Station Area Overlay District, if the residential use is located within 1,320 feet of a street with frequent transit service, measured as the walking distance from the nearest transit stop to the lot line of the lot containing the residential use.(1)

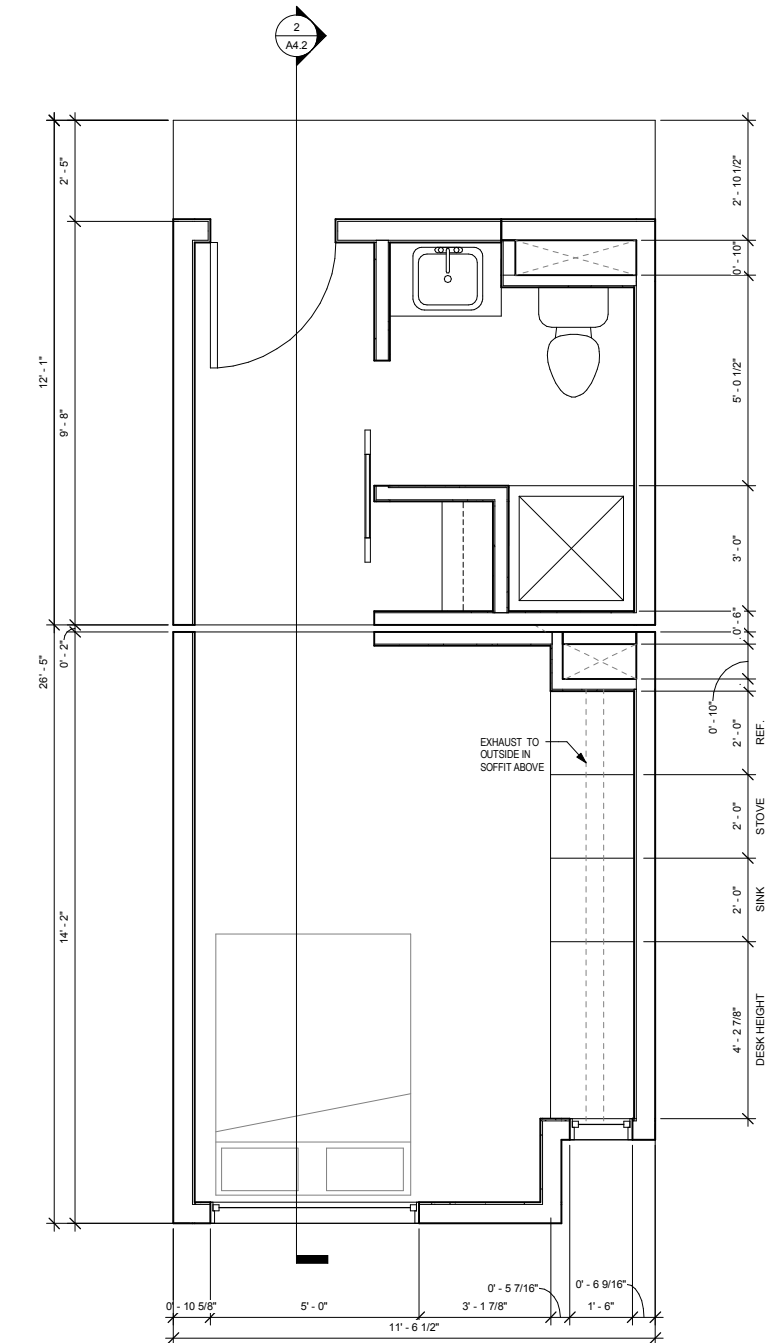
No minimum requirement



Ground Floor

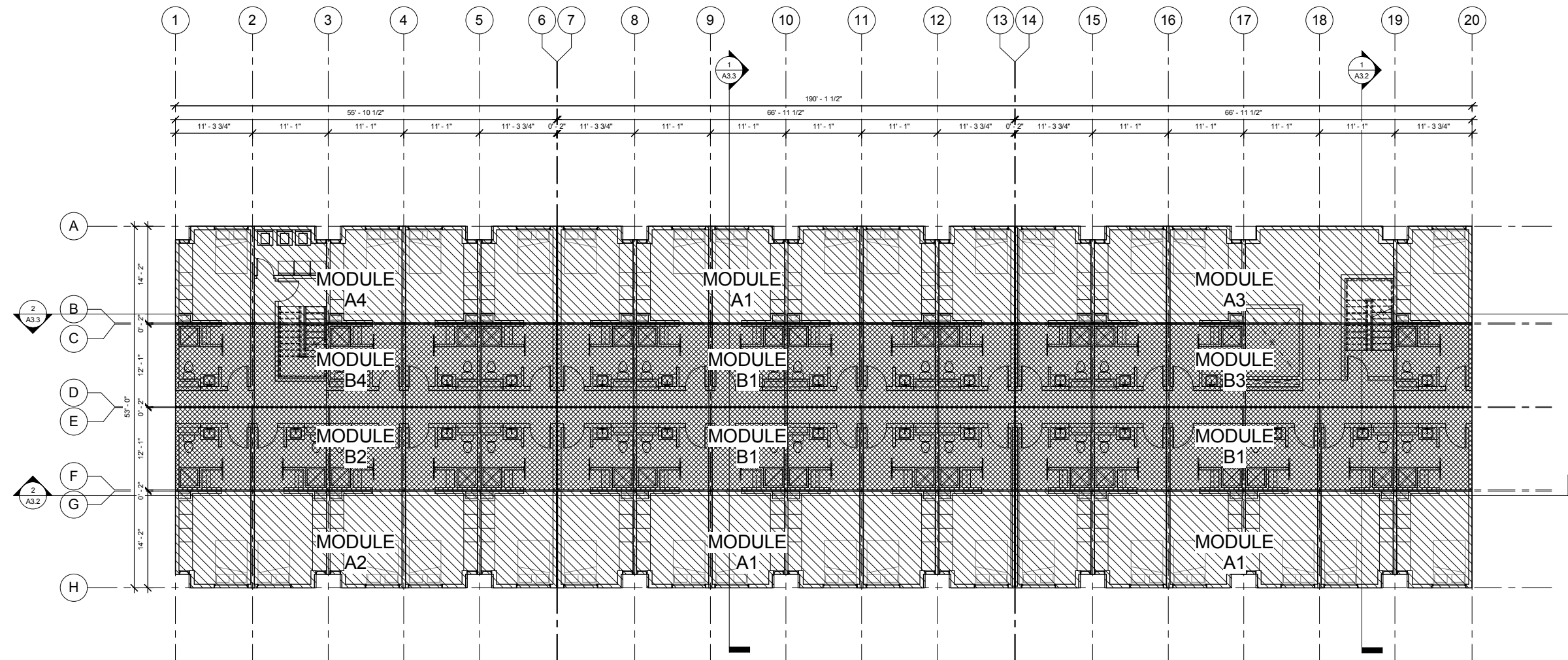


Basement

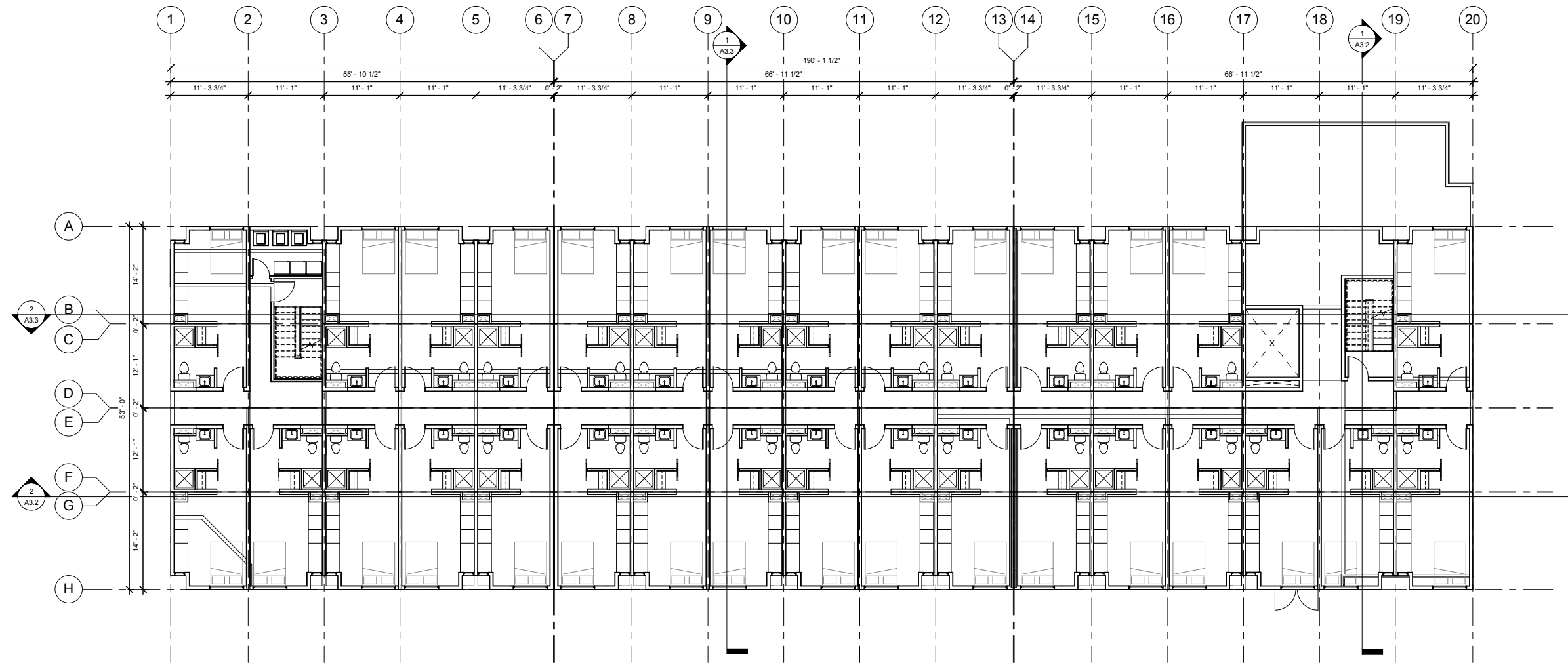


Typical Unit Plan





Modular Layout



Modular construction typical floor