

# The Natasha Project- Proposed New Residential Construction Project (DPD #3017563)

SEATTLE DESIGN REVIEW

July 29, 2014 | Early Design Guidance Meeting 1



## TABLE OF CONTENTS

1	Project Summary and Statistics
2	Zoning
3	Zoning
4	Neighborhood Context
5	Site Analysis
6	Site Analysis
7	Existing Site Conditions
8	Massing Options Comparison
9	Massing Option 1 (Code Compliant)
10	Massing Option 1 (Code Compliant) - Section and Plan
11	Massing Option 1 (Code Compliant) - Mezzanine and Typical Residential
12	Massing Option 2
13	Massing Option 2 - Section and Plan
14	Massing Option 2 - Mezzanine and Typical Residential
15	Massing Option 3 (Preferred)
16	Massing Option 3 (Preferred) - Section and Plan
17	Massing Option 3 (Preferred) - Mezzanine and Typical Residential
18	Massing Option 3 (Preferred) - P1/P2 and Alternate Access Plan
19	Design Guidelines
20	Design Guidelines
21	Departure Request 1 - Setbacks
22	Departure Request 2 - Area Coverage
23	DPD/SDOT Request - Western Ave. and Alley Access
24	Project Examples

## PROJECT SUMMARY

The proposed building located at 2134 Western Avenue in Downtown's DMR/C 85/65 zone within the Belltown Urban Center Village overlay. The proposed building will be 6 stories of residential (5 stories of type V and 1 story of type IA) over a (1) story retail and live/work (type IA) base. One half floor of parking will be accessed from the alley and two and a half from Western Avenue.

The lot fronts Western Avenue (a class II pedestrian street) to the west, Blanchard Street (a designated green street) to the north, a shared alley to the east and a shared property line to the south. The site area is 120'x180' and the site slopes down approximately 13' from east to west along Blanchard Street.



## PROJECT STATISTICS

<b>Number of Floors:</b>	
Retail, Live/Work, Residential Lobby	1
Residential	6
Below-grade parking	2
<b>Total floors above grade</b>	<b>7</b>
Project Height Allowed	85'
Project Height Proposed	82'-9"

Dwelling units	160
Parking quantity	130

<b>Project square footage</b>	
L7	18,716 SF
L6	18,716 SF
L5	18,716 SF
L4	18,716 SF
L3	18,716 SF
L2	18,356 SF
L1 Mezzanine	16,797 SF
L1	21,074 SF
P1	10,619 SF
P2	10,619 SF

Residential area for common recreation calculations	111,936 SF
<b>Required (5% * 111,936 SF)</b>	<b>5,597 SF</b>

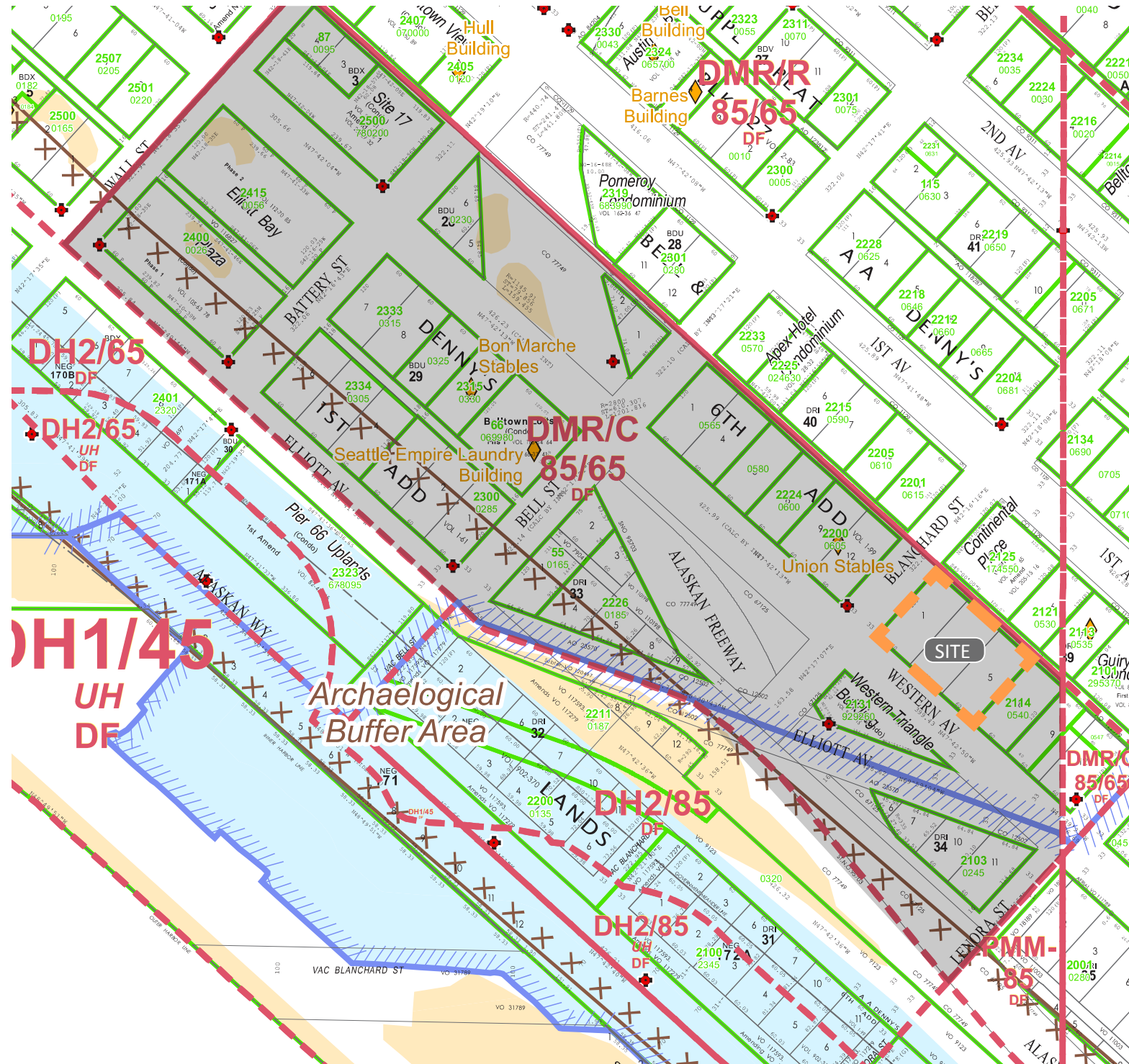
Provided (interior)	1,924 SF
Provided (exterior)	19,190 SF
<b>Total SF</b>	<b>21,114 SF</b>

### Code Departures:

1. Coverage and floor size limits above 65' (23.49.158)
2. Side and Green Street setbacks (23.49.166)

### DPD/SDOT Request:

1. Parking quantity, location and access requirements (23.49.019)



Address: 2134 Western Avenue  
 King County Parcel Number: 1977200505  
 Zone: DMR/C 85/65 (Downtown Mixed Residential/Commercial), Belltown Urban Village

Design Guidelines: Belltown Urban Center Village

Site Dimensions: 180' (fronts Western Ave) x 120' (Blanchard Street) x 120' (abuts adjacent existing building) x 180' (Alley).

Site Area: 180' (Western Ave) \* 120' (Blanchard St.) = 21,600 SF

**Seattle Zoning Analysis**

**Structure Height**

DMR/C 85/65: 85' Maximum Height (residential use), 65' Base height. (23.49.008.C.)  
 Structure height is 85' or less, therefore all extra residential floor area shall be housing bonus residential floor area pursuant to Section 23.58A.014. (23.58A.012)

**General Requirements for Residential Uses (23.49.010)**

Common Recreation Area: An area equivalent to 5% of the total gross floor area in residential use. (5% \* 111,936 SF = 5,597 SF)

For lots abutting designated green streets, up to fifty (50) percent of the common recreation area requirement may be met by contributing to the development of a green street.

**Floor Area Ratio (23.49.011 Table A)**

Base FAR: 1 (1 x 21,600 SF = 21,600 SF Base allowable floor area)  
 Max FAR: 4 (4 x 21,600 SF = 86,400 SF Maximum allowable floor area)  
 Exemptions/deductions from FAR: Residential use/Live-work units /Floor area below grade.

**Overhead Weather Protection and Lighting (23.49.018)**

Continuous overhead weather protection shall be required along the entire street frontage.

**Parking Quantity, Location and Access**

No parking is required for uses on lots in Downtown zones. (23.49.019.A.)  
 Bicycle Parking: 1 space required for every 2 dwelling units. (23.49.019.E.)  
 Curb cut Location (23.49.019.H.1.)  
 a. If a lot abuts an alley, alley access is required, unless the Director otherwise determines under subsection 23.49.019.H.1.c.  
 b. & c. Departure 1 for this proposal requests an additional access location off of Western Avenue, a Class II pedestrian street-Minor arterial (the #2 preference per 23.49.019.H.1.b.)

Curb cut Width and Number. The width and number of curbcuts shall comply with Section 23.54.030, Parking space standards. (23.49.019.H.2.)

**Minimum Sidewalk Widths**

Established by Map 1C (Blanchard Street: Variable, Western Avenue: 15') (23.49.022.A.)

**Coverage and Floor Size Limits**

Per Table A for 23.49.158 (Lot size 19,001-25,000 SF; 65 feet or less = 100% coverage permitted; greater than 65 feet up to 85 feet = 65%)

DEPARTURE REQUESTED

**Street Facade Requirements**

Street Designations per Map 1F (Blanchard Street: Designated Green Street, Western Avenue: Designated Class II Pedestrian Street) (23.49.162)

Minimum facade height (Western Avenue: 15', Blanchard Street: 25') (23.49.162.A.1)

Facade Setback Limits (23.49.162.B.1)

- a. 15' or less; 2' from street property line
- b. Greater than 15'
  - 1. No setback limits shall apply up to an elevation of 15'.
  - 2. Between elevations of 15' and 35' above sidewalk grade, the facade shall be located within 2' of the street property line.

Facade Transparency Requirements (23.49.162.C.)

- 1. Facade transparency requirements apply to the area of the facade between two (2) feet and eight (8) feet above the sidewalk.
- 4. Class II pedestrian streets (Western Avenue) and designated green streets : A minimum of thirty (30) percent of the street-level facade shall be transparent.

Blank Facade Limits (23.49.162.D.1.a)

Two (2) feet and eight (8) feet above the sidewalk at Western Ave (<7.5% grade)

Four (4) feet and eight (8) feet above the sidewalk at Blanchard (>7.5% grade)

Street trees are required on all streets that have a pedestrian classification and abut a lot. (both Blanchard Street & Western Avenue) (23.49.162.F.)

**Maximum Width and Depth**

Per Table A for 23.49.164 (Greater than 19,000 SF & Greater than 65 feet up to 125 feet = 120 feet maximum width and depth)

**Street Setback Requirements**

Per Table A for 23.49.166 (Greater than 120 feet up to 180 feet Frontage on Avenue = 20 foot Required Setback Above 65 Feet)

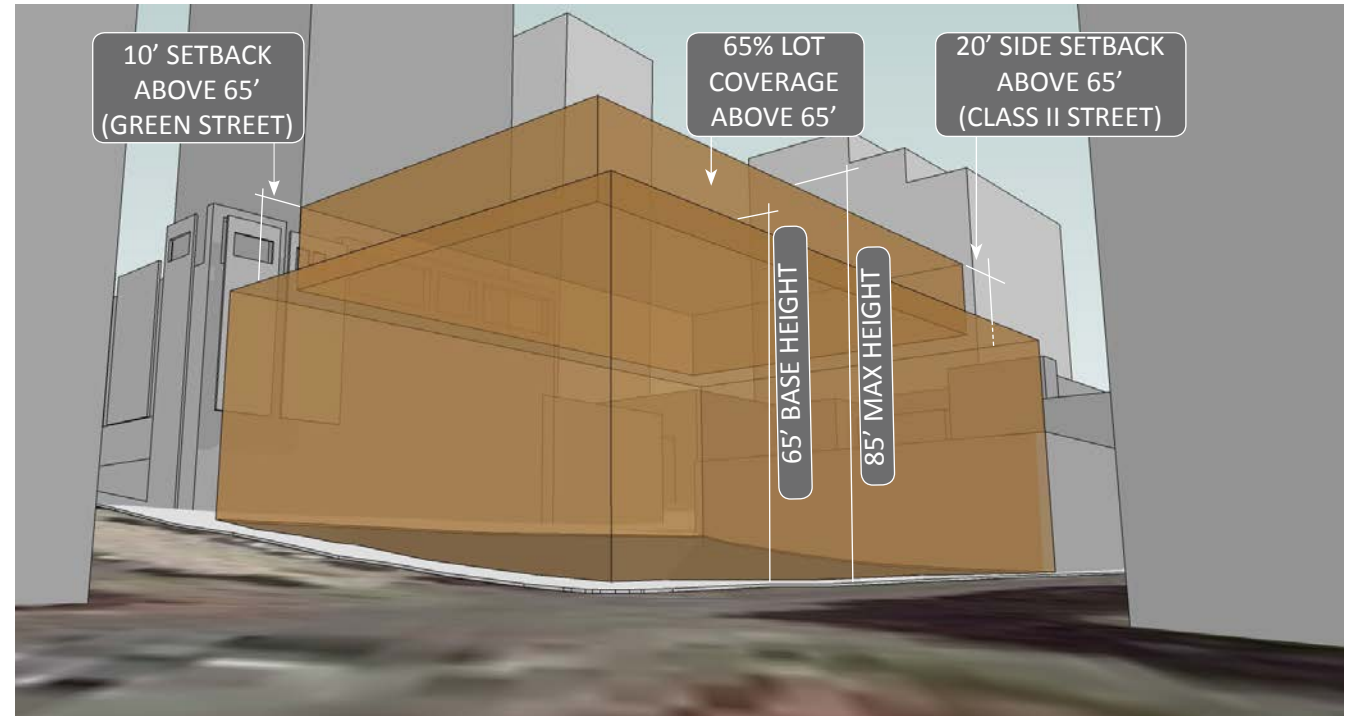
Green Street Setback at Blanchard Street is ten feet for portions of structures above 65 feet in height to a maximum of 85 feet (23.49.166.B.1.)

DEPARTURE REQUESTED

**Alley Improvements**

Minimum alley right of way width per Table C for Section 23.53.030 (All downtown zones = 20 feet)

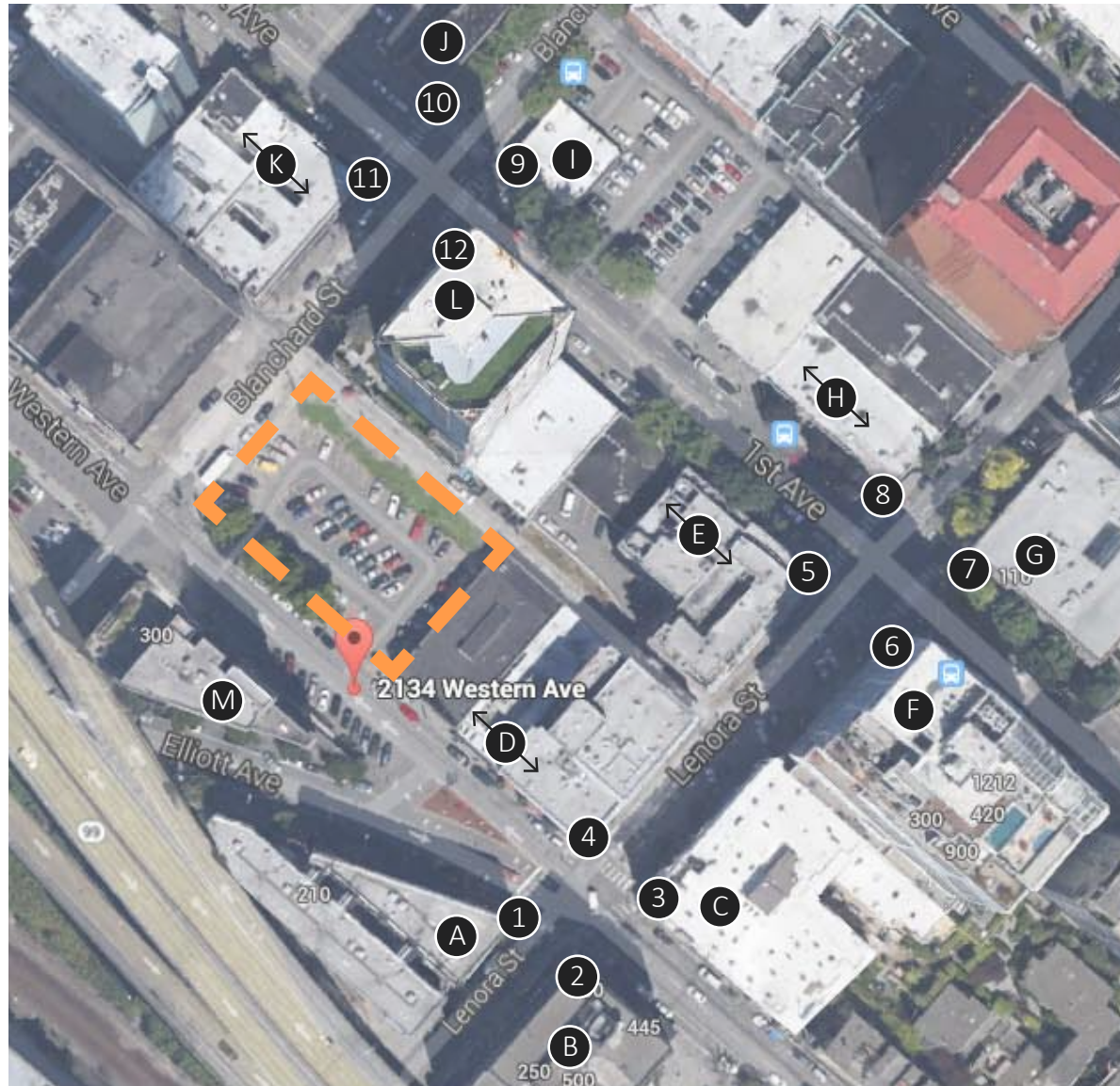
For existing alleys which do not meet the minimum width, a dedication equal to half the difference between the current alley right-of-way width and minimum right-of-way width established. (20' alley width required – 16' existing alley width = 4' (50%) = 2' alley dedication required) (23.53.030.F.1)



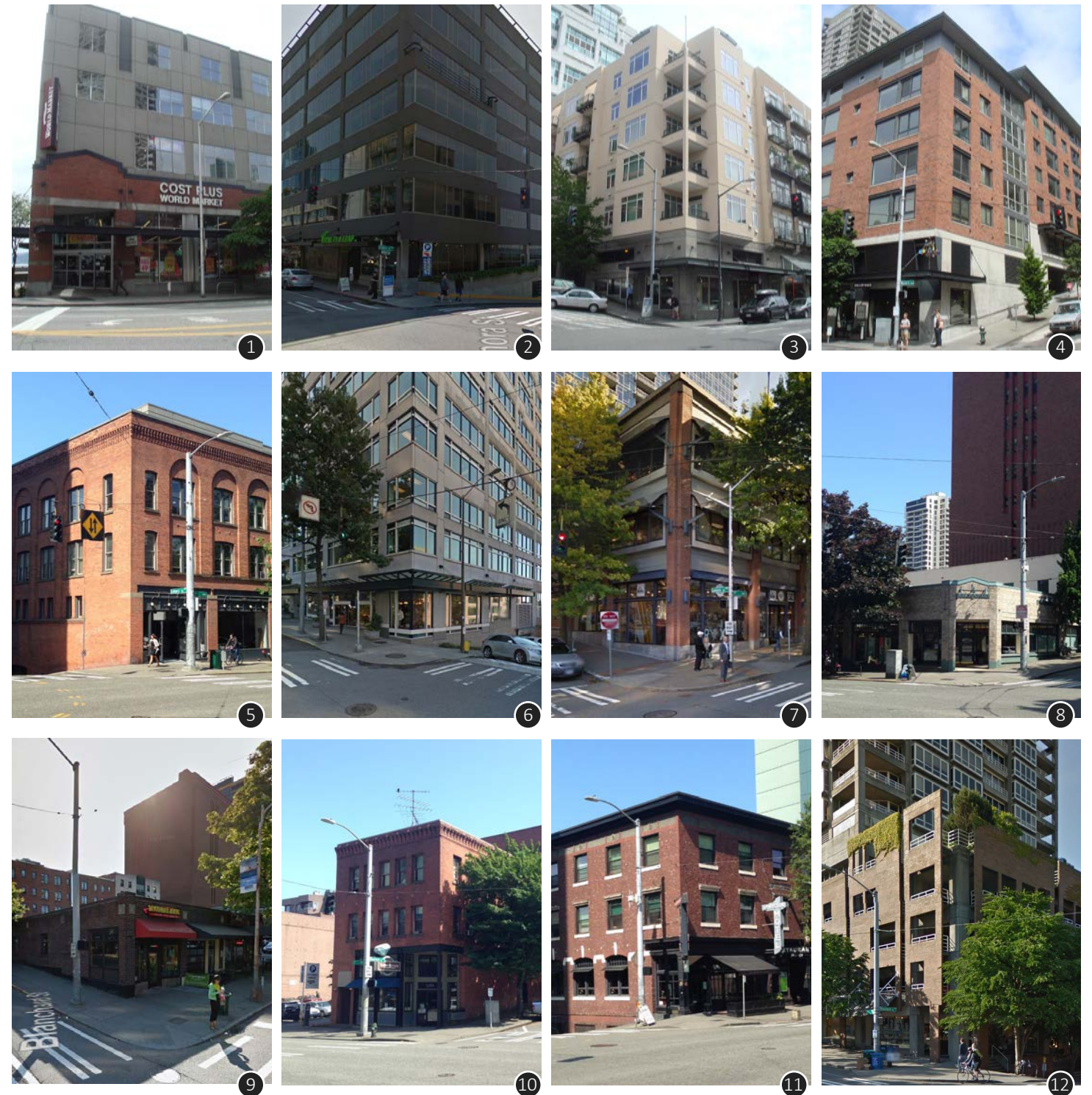
# NEIGHBORHOOD CONTEXT

This site on the corner of Western Avenue and Blanchard Street is unique in that it sits at the north edge of Pike Place Market and the start of the Belltown neighborhood with its restaurants and nightlife. Western Avenue has become pedestrian in nature and will continue to develop as the Alaska Way Waterfront Plan is implemented and the envisioned parklette at the northwest corner is completed.

The buildings surrounding the site range in height and architectural character, from the 33-story brick and concrete building to the east to the 2-story brick building to the south. Architectural concrete and mirrored glass are also found in the building across the street.



- A. Cost Plus World Market
- B. Vital Tea Leaf  
Tullys Coffee
- C. Steinbrueck Nature Gallery
- D. Elliot Bay Bicycles  
Ewing and Clark, INC  
Ventana Apartments  
Sushi Bella  
Gallery Mack
- E. Buffalo Deli  
Sell Your Sole  
Tune HIFI  
Cherry Street Coffee House  
Mud Bay  
Northwest Woodworkers  
Urban Hardwoods
- F. Gian DeCaro Sartoria Tailors
- G. DC/Quiksilver Clothing
- H. Patagonia  
Army Navy Store
- I. Cellars Restaurant  
Taco Del Mar
- J. Robbins Brothers  
K. O'hana  
Frontier Room  
Queen City Grill  
Belltown Billiards
- L. Gary Manuel Studio
- M. Has Offers





Looking Southwest at Site



Looking Northeast from Site



Looking Southeast at Site



Looking Northwest from Site



Looking Northeast at Site



Looking Southwest from Site



Looking Northwest at Site



Looking Southeast from Site

SITE



East Side of Western Avenue

SITE



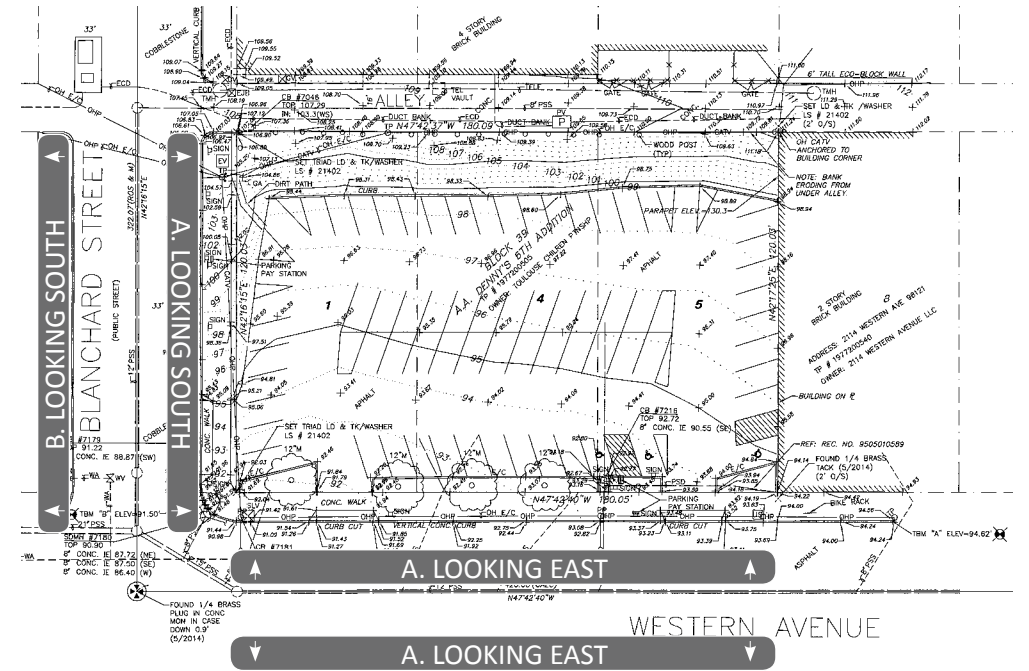
South Side of Blanchard Street



West Side of Western Avenue

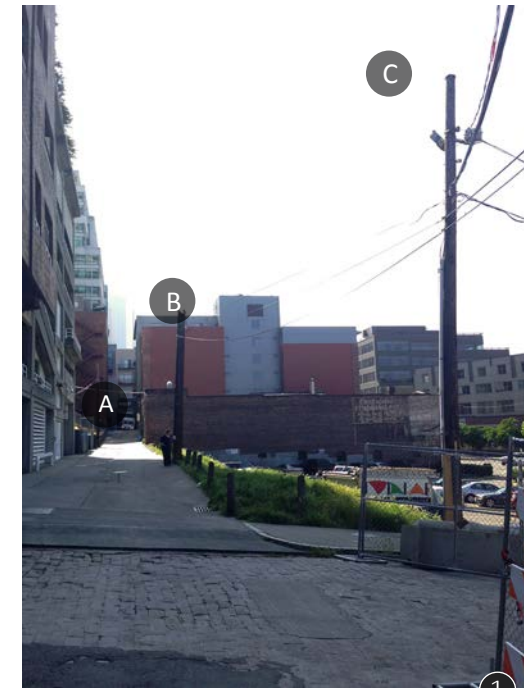
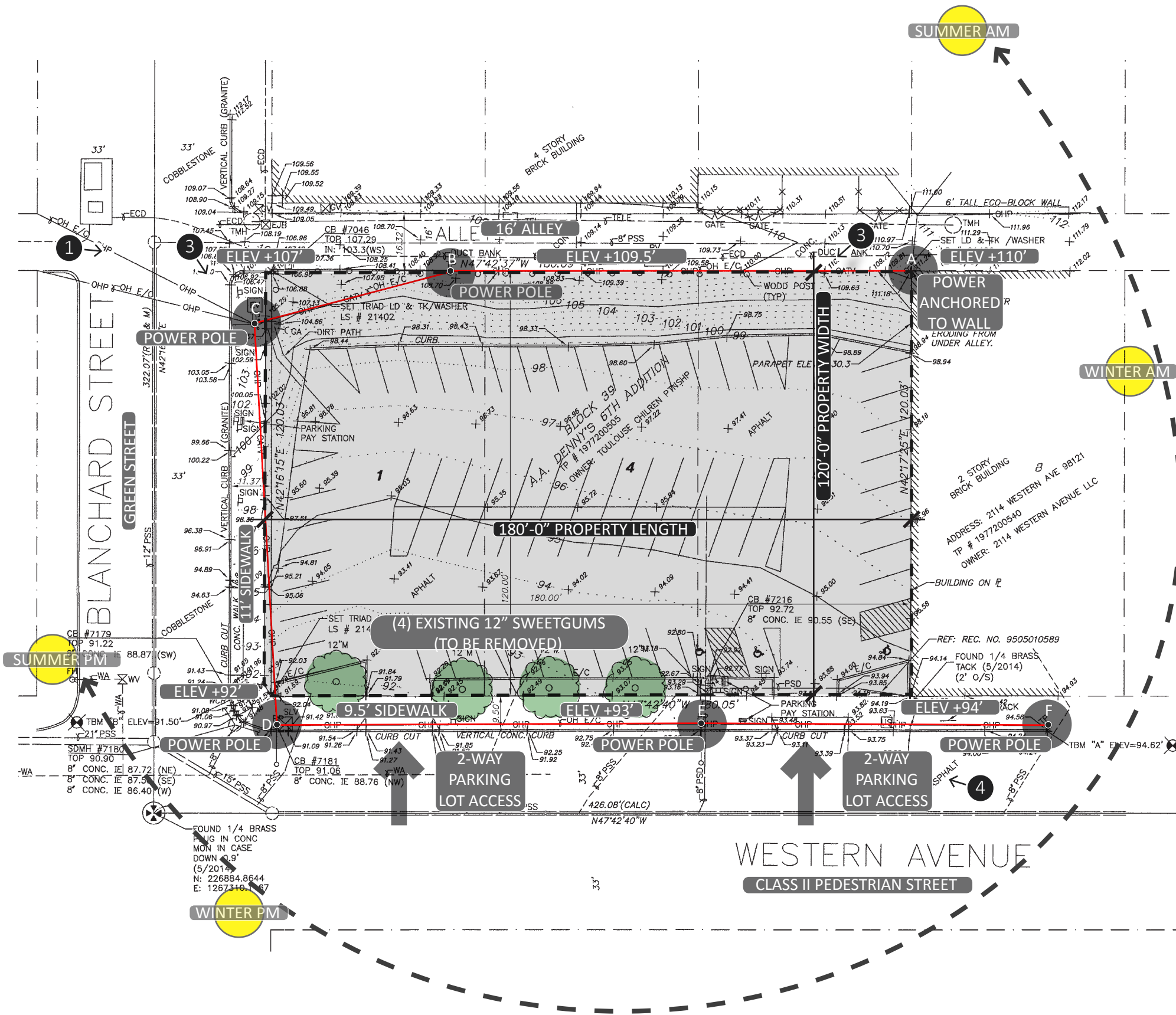


North Side of Blanchard Street

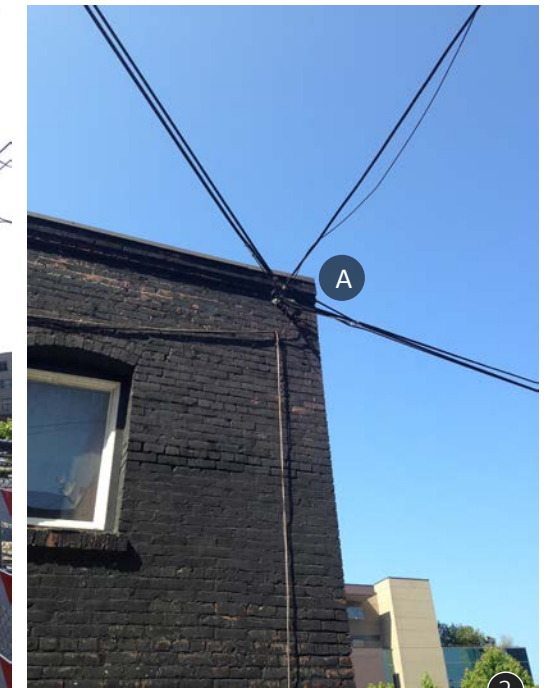


The Natasha Project  
 2134 Western Avenue  
 Early Design Guidance Meeting- DPD #3017563

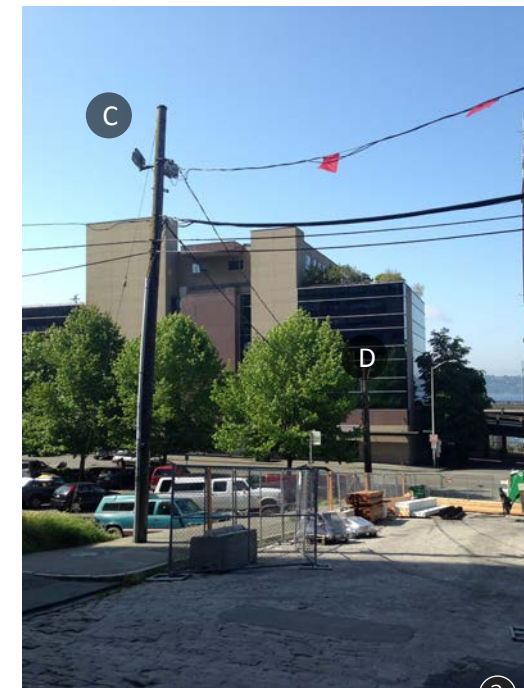
29 July 2014



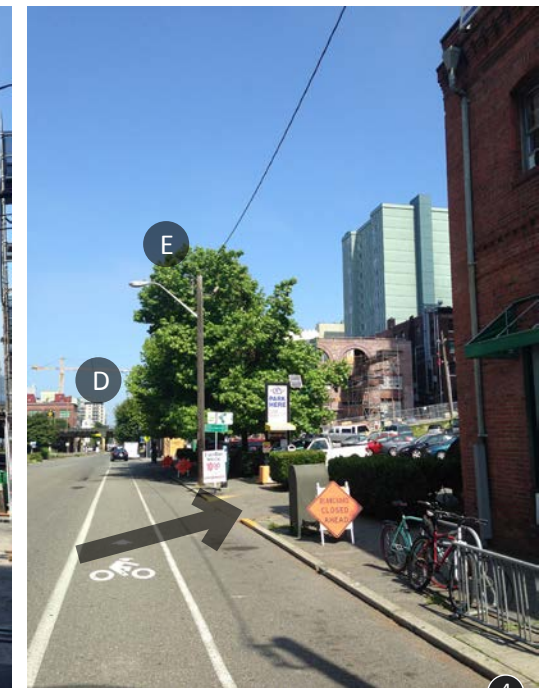
View South to Alley



Power Attached to Adjacent Building

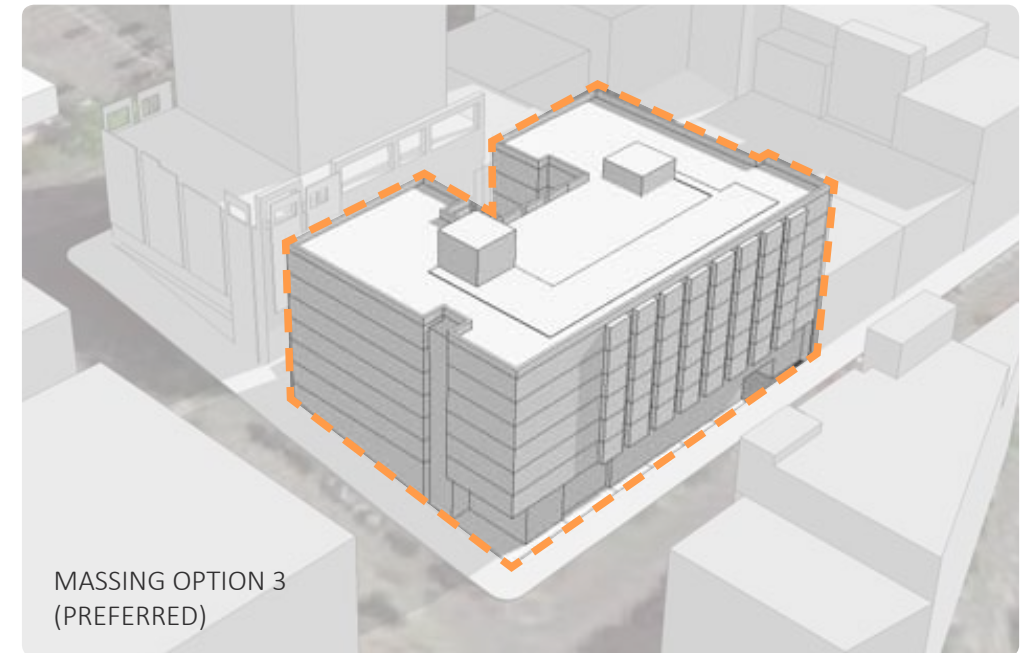
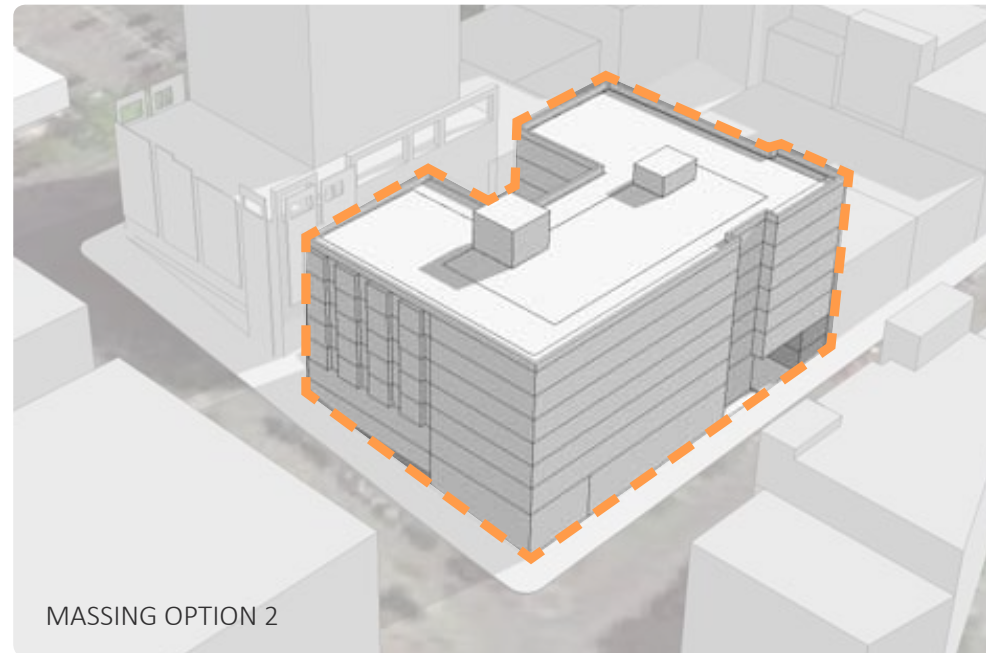
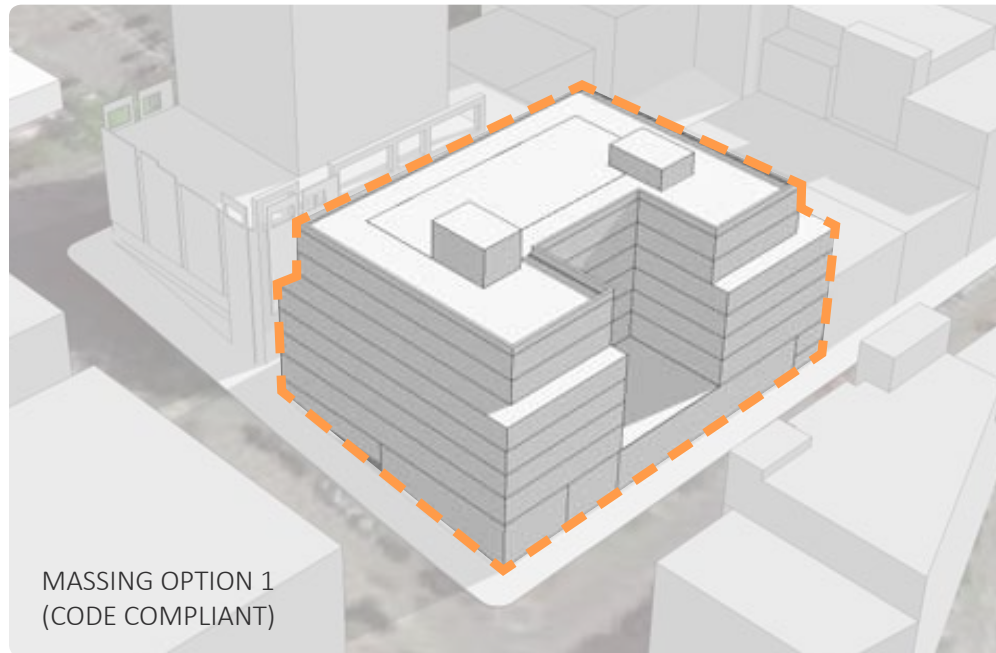


View West to Western Avenue



View North to Blanchard Street





PROS

- Required setbacks reduce the visual presence of the building in its context.
- The west oriented courtyard provides greater modulation of the mass above the retail level.
- All vehicular access to the parking garage is consolidated through the alley.

CONS

- Required setbacks reduce the potential area dedicated for the green roof.
- Vehicular access through the alley only requires more square footage dedicated to parking garage ramps and reduces the amount of square footage available for retail and live/work.
- The western facing courtyard reduces the number of residential units with enhanced views of the Puget Sound and Olympic Mountains.

PROS

- The residential lobby located away from the retail corner enlivens the entire Western Avenue frontage.
- Residential bay windows provide modulation to the Blanchard Street facade.
- A strong wall along Western Avenue responds to the building to the west.

CONS

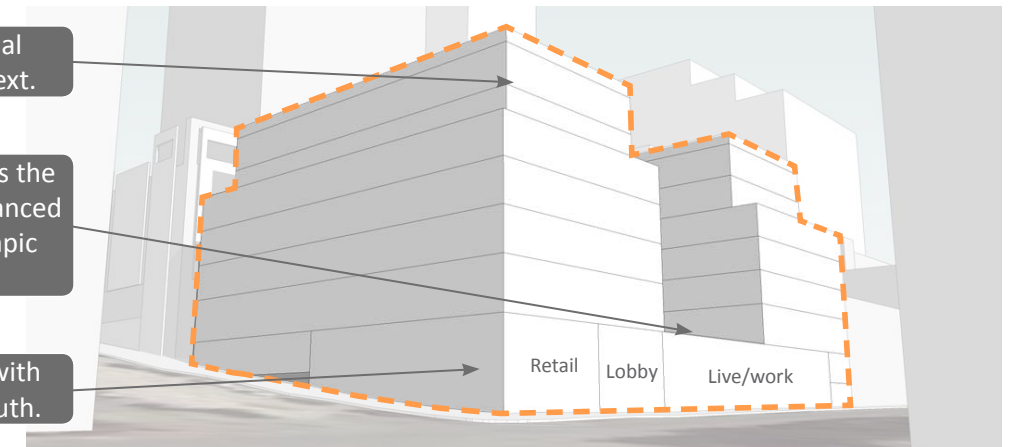
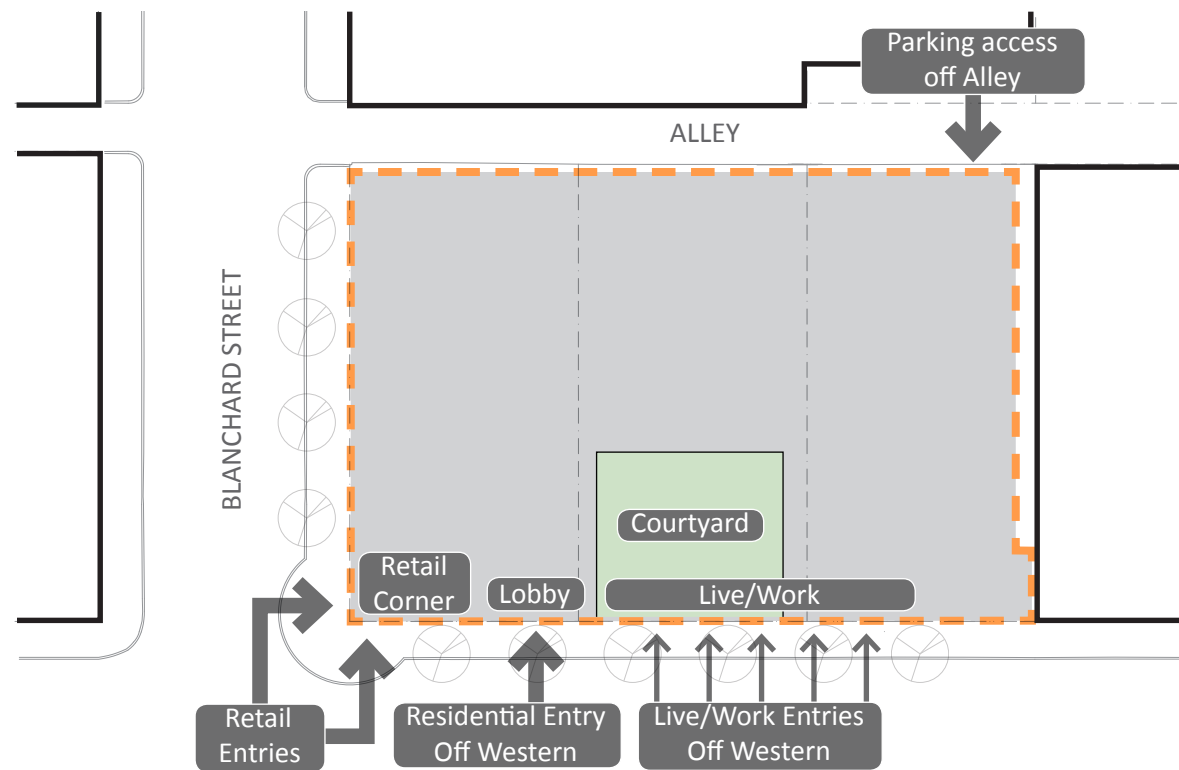
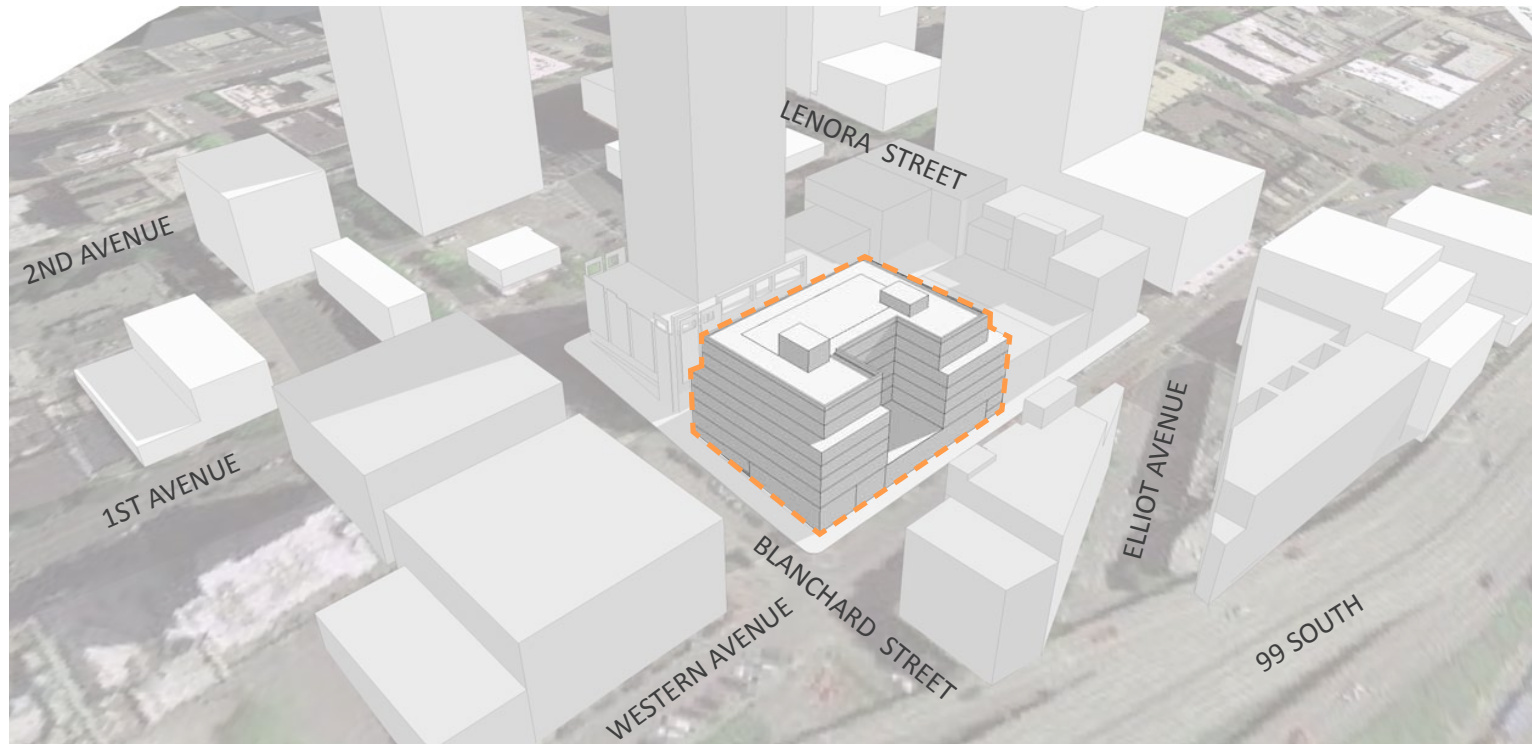
- By separating the residential lobby from the retail, the pedestrian activity level at the corner becomes weakened.
- By co-locating the residential lobby and the Western Avenue parking access, pedestrian and vehicle interaction becomes increased.
- A strong wall along Western Avenue offers less variation in form and scale.

PROS

- Residential bay windows add to the pedestrian nature of Western Avenue while still providing a strong street edge.
- A strong retail corner is consistent with the street level treatment to the south.
- The recessed facade actively expresses the location of the lobby entry to pedestrians.

CONS

- By co-locating the retail and residential lobby at the corner, the overall activity across the Western Avenue frontage becomes reduced.
- Western Avenue parking garage access increases the instances of pedestrian and vehicle interaction.
- The eastern facing courtyard overlooks the alley instead of the Puget Sound and Olympic Mountains.

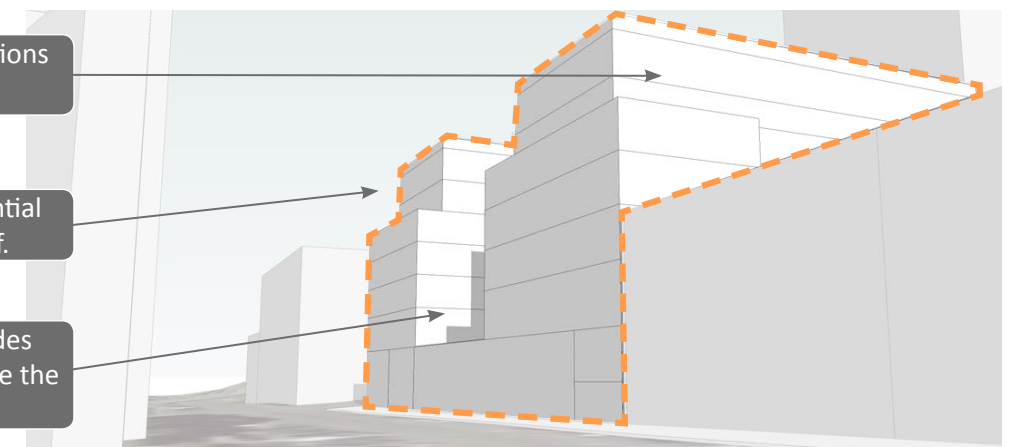


View at Blanchard Street and Western Avenue

Required setbacks reduce the visual presence of the building in its context.

The western facing courtyard reduces the number of residential units with enhanced views of the Puget Sound and Olympic Mountains.

A strong retail corner is consistent with the street level treatment to the south.

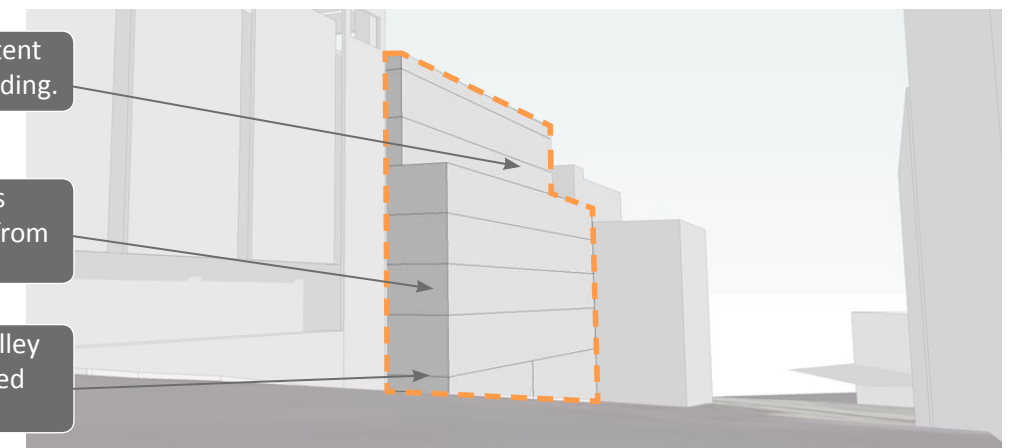


View Looking North Along Western Avenue

Setback allows for window fenestrations along the south property line.

Required setbacks reduce the potential area dedicated for the green roof.

The west oriented courtyard provides greater modulation of the mass above the retail level.

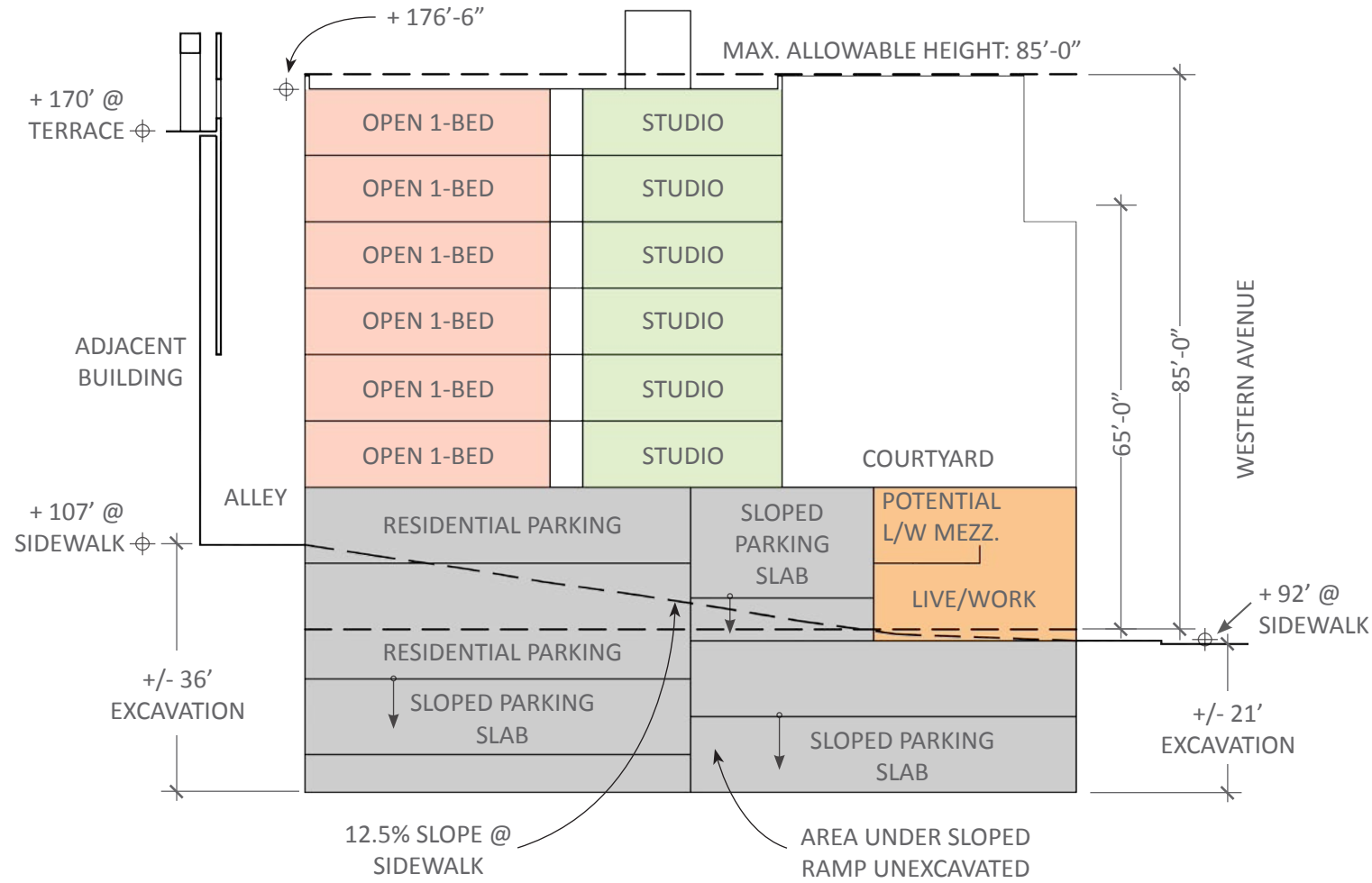


View Looking West Along Blanchard Street

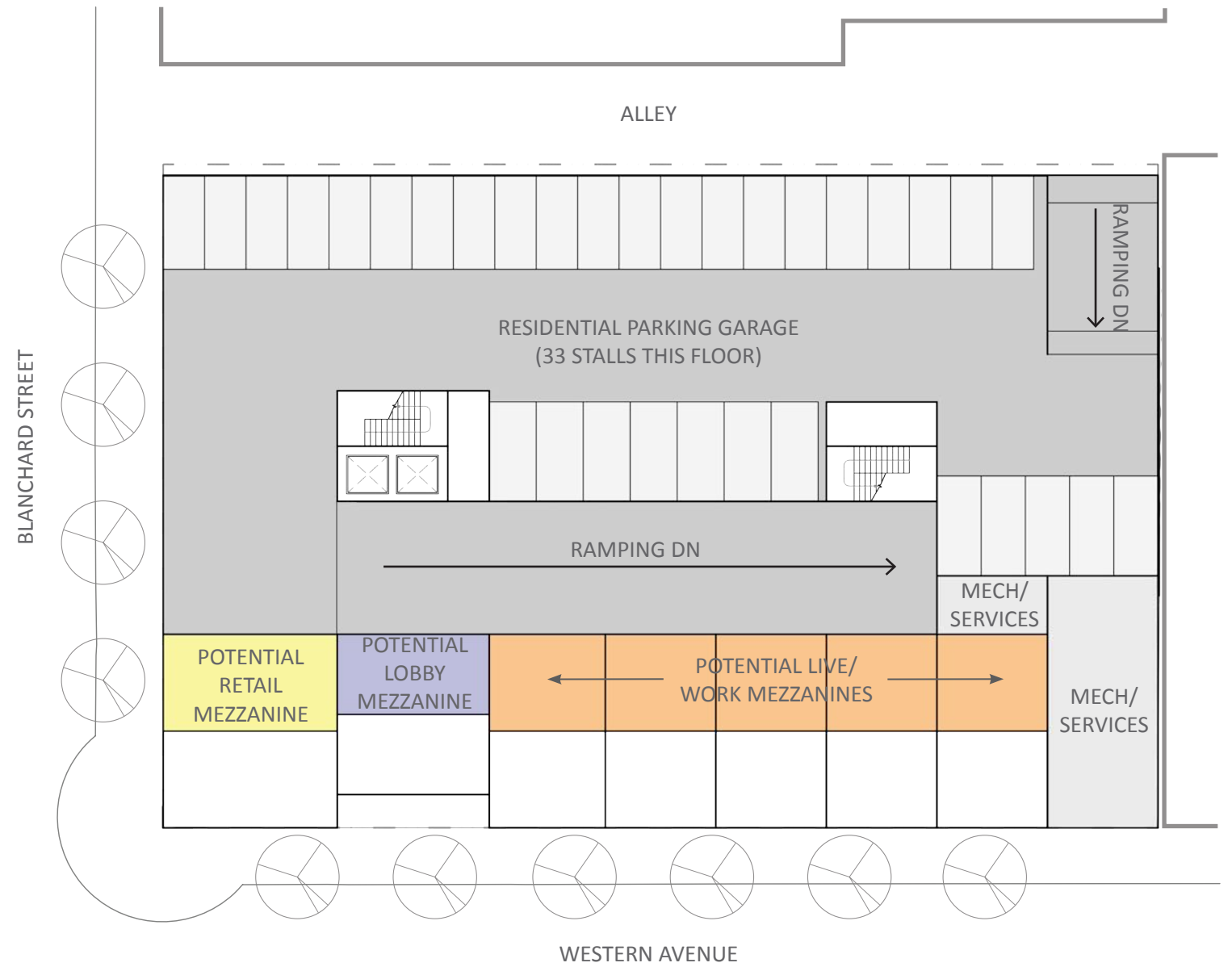
The setback above 65' is not consistent with the massing of the adjacent building.

A strong corner at the alley helps differentiate the proposed building from the building to the east.

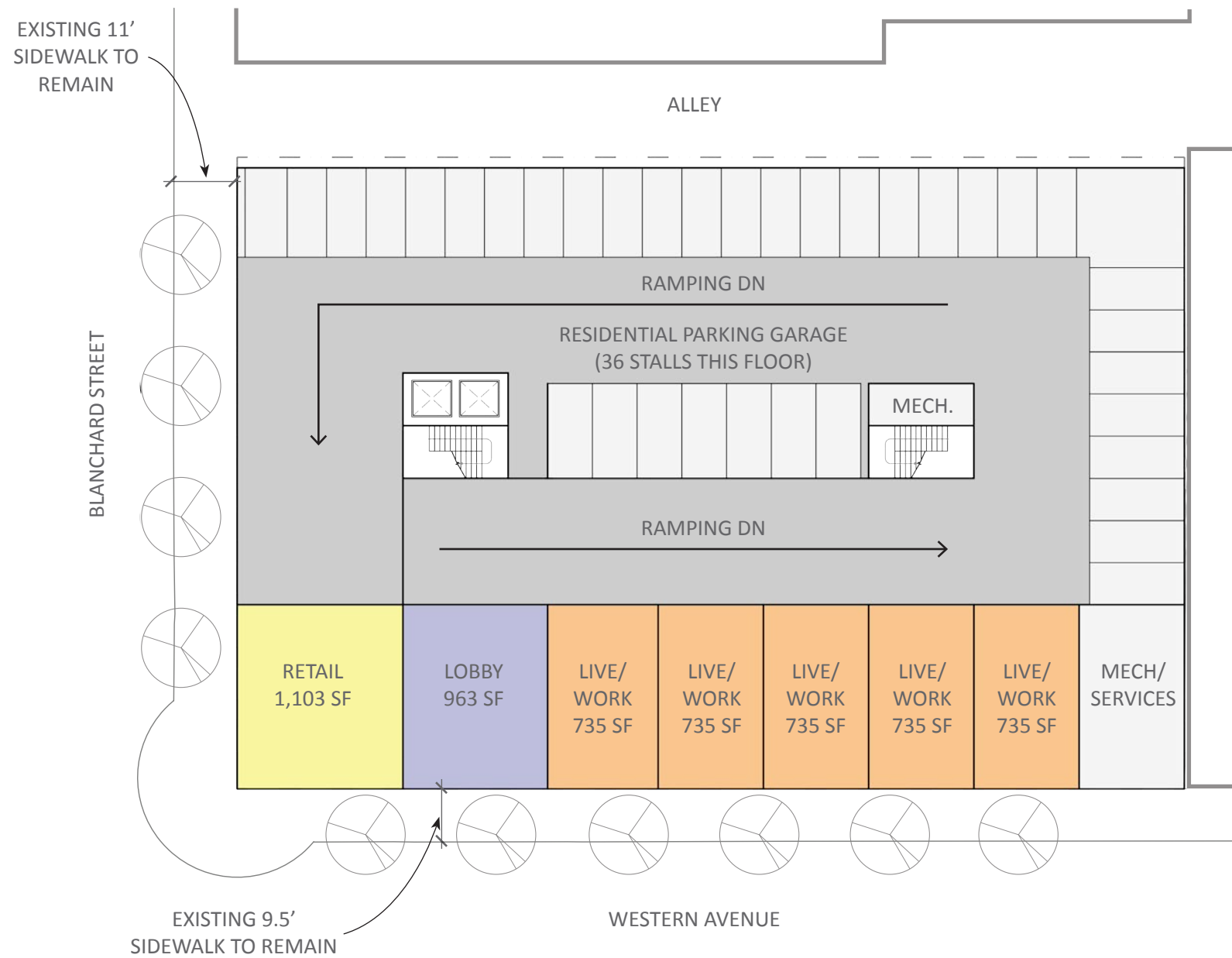
Vehicular access through only the alley requires more square footage dedicated to parking garage ramps.



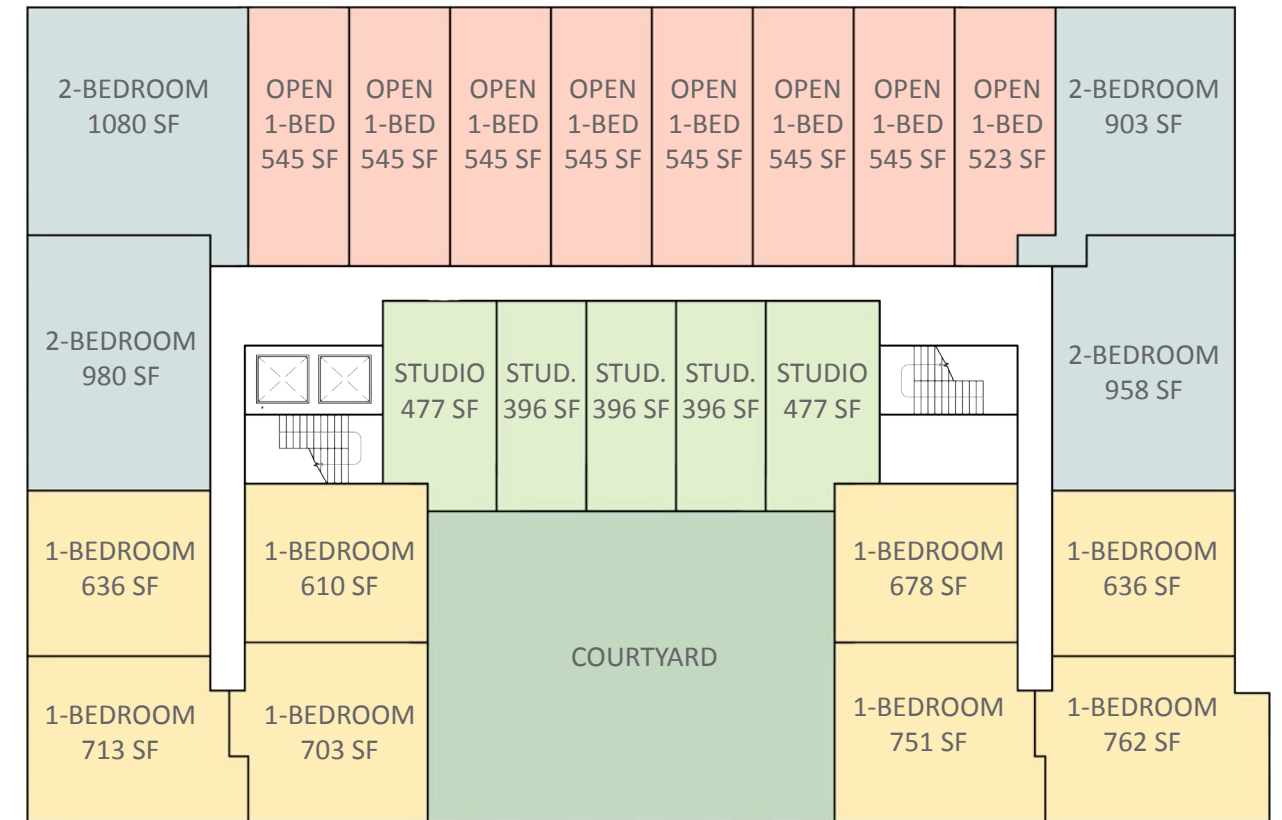
BUILDING SECTION



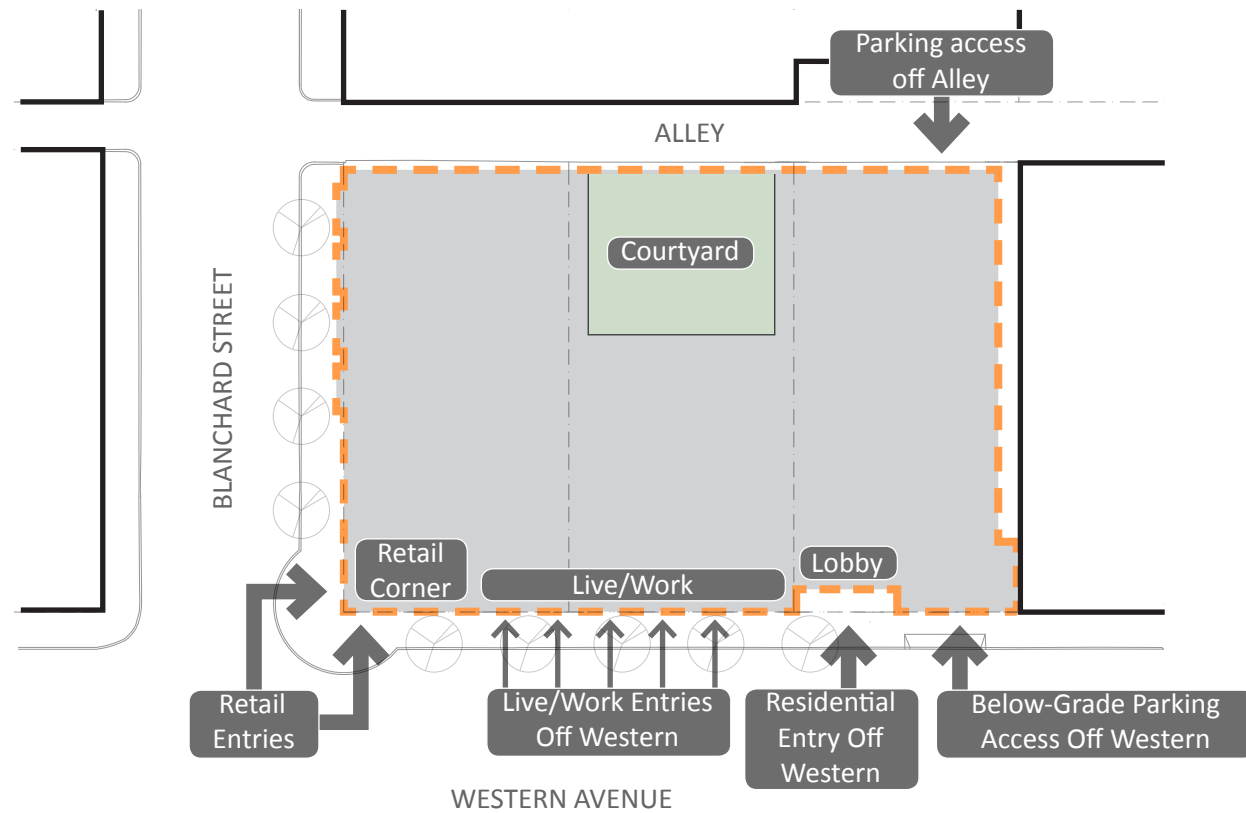
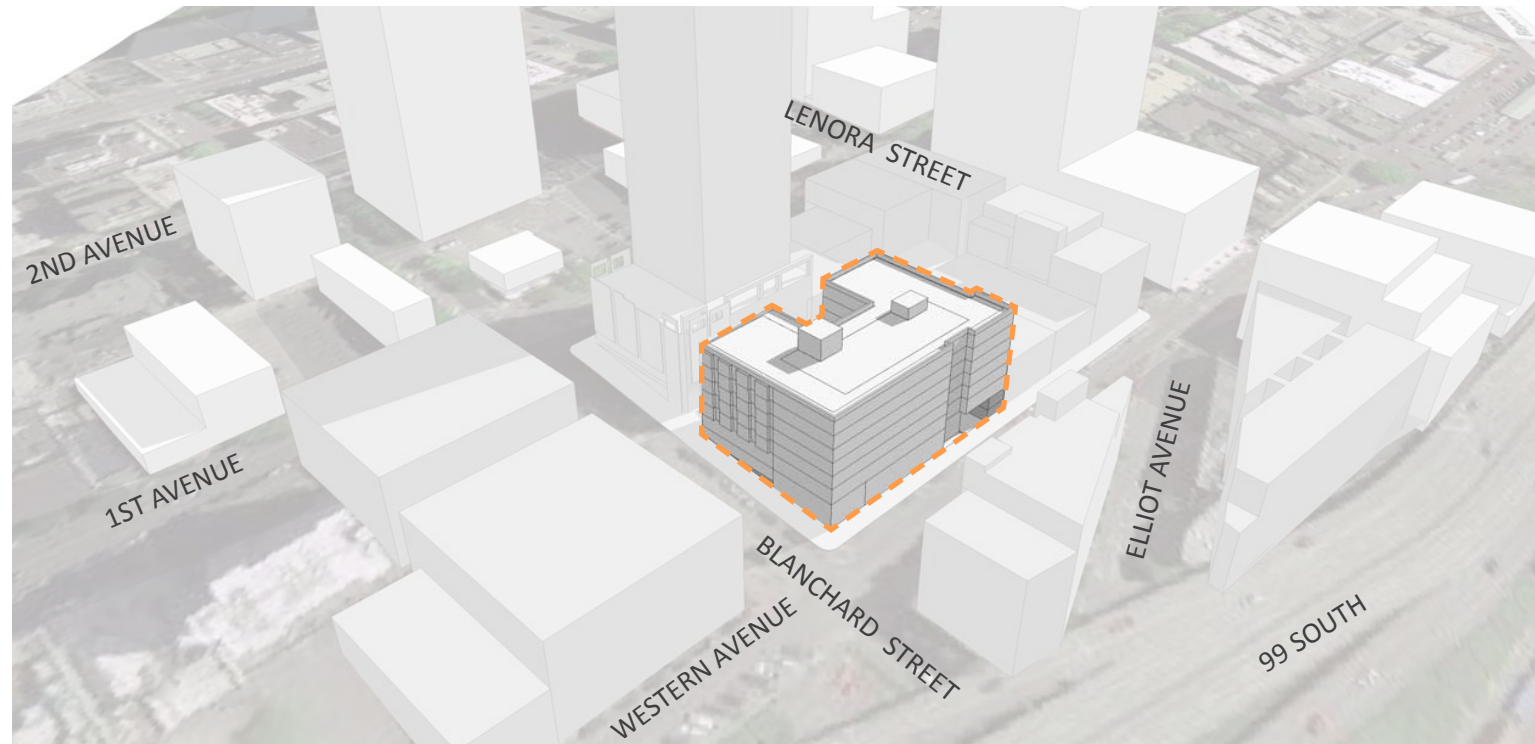
LEVEL 1 MEZZANINE PLAN



LEVEL 1 PLAN



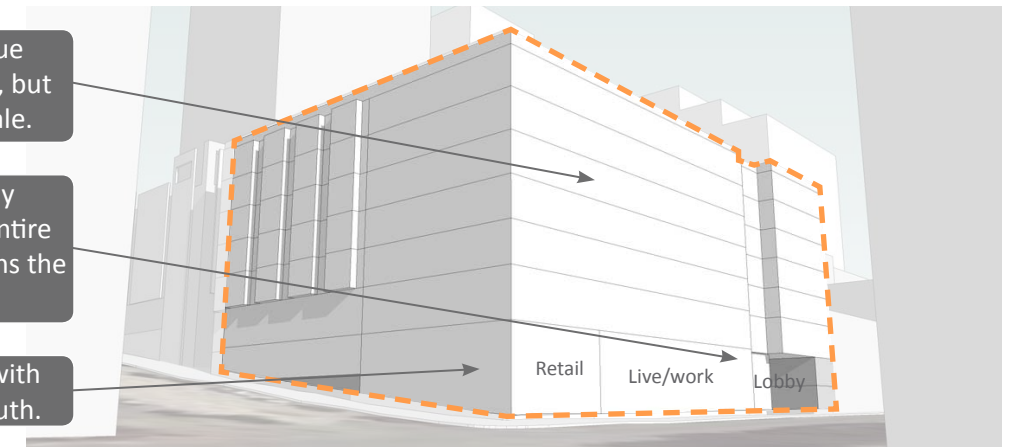
TYPICAL RESIDENTIAL PLAN



A strong wall along Western Avenue responds to the building to the west, but offers less variation in form and scale.

The residential lobby located away from the retail corner enlivens the entire Western Avenue frontage, but weakens the activity level at the corner.

A strong retail corner is consistent with the street level treatment to the south.



View at Blanchard Street and Western Avenue

Setback allows for window fenestrations along the south property line.

An eastern facing courtyard provides enhanced views for more residential units along Western Avenue.

By co-locating the residential lobby and the Western Avenue parking access, pedestrian and vehicle interaction becomes increased.

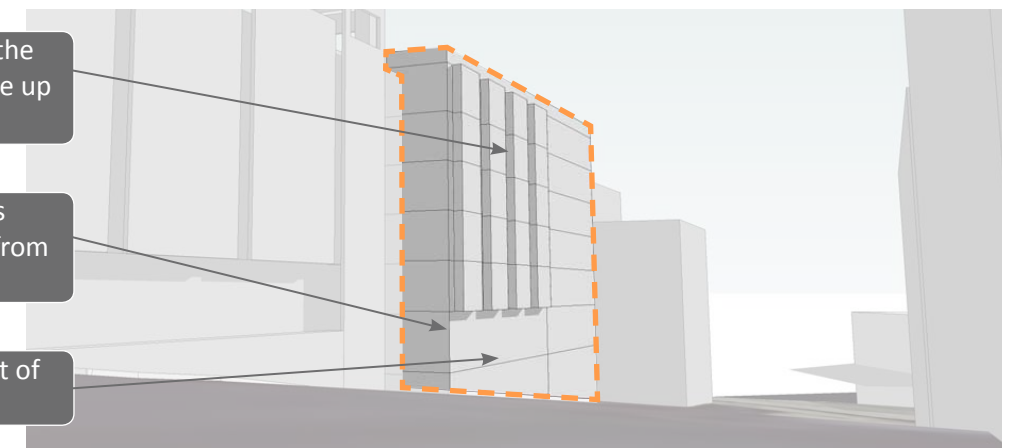


View Looking North Along Western Avenue

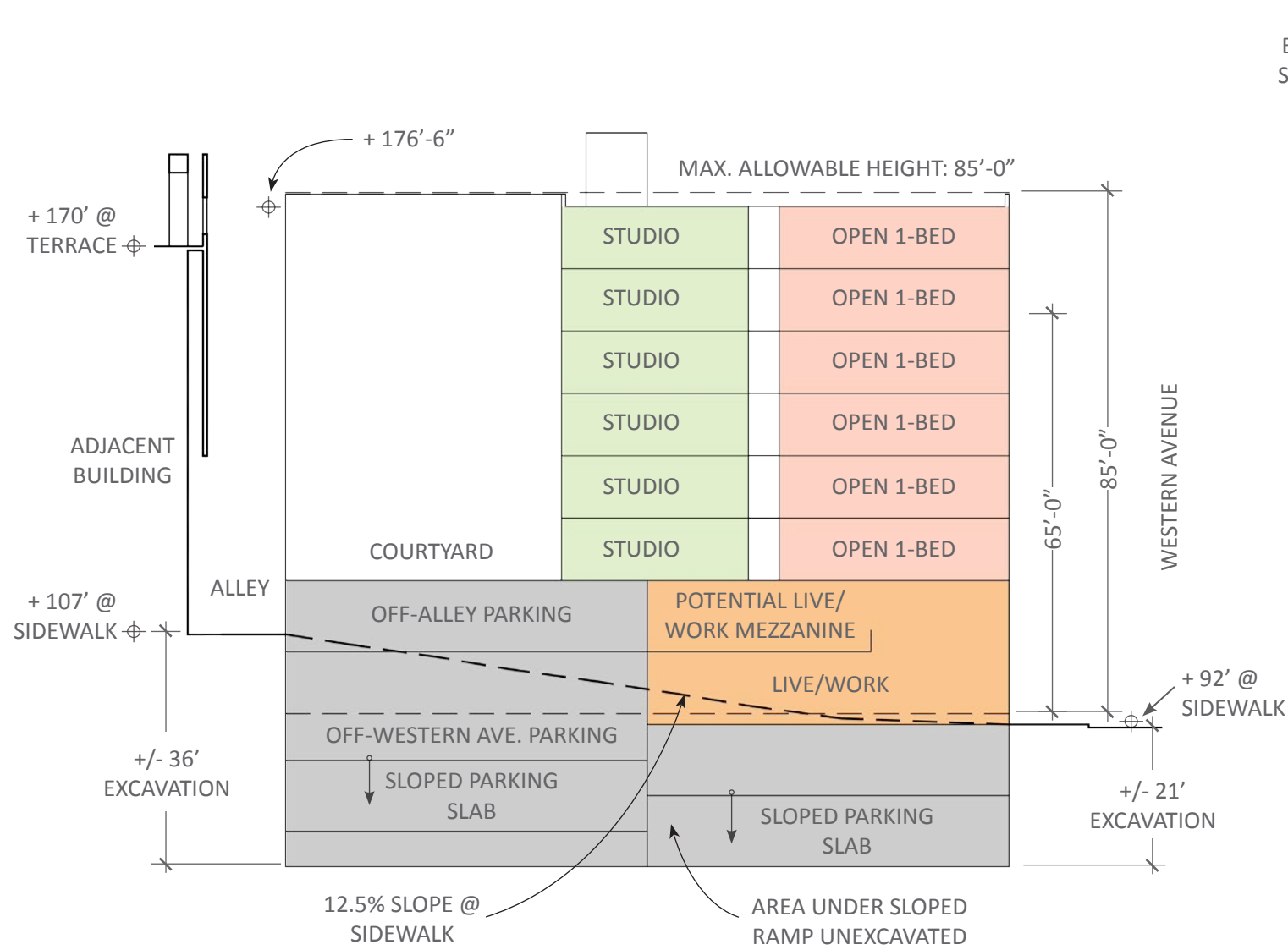
Residential bay windows continue the pedestrian nature of Western Avenue up Blanchard Street.

A strong corner at the alley helps differentiate the proposed building from the building to the east.

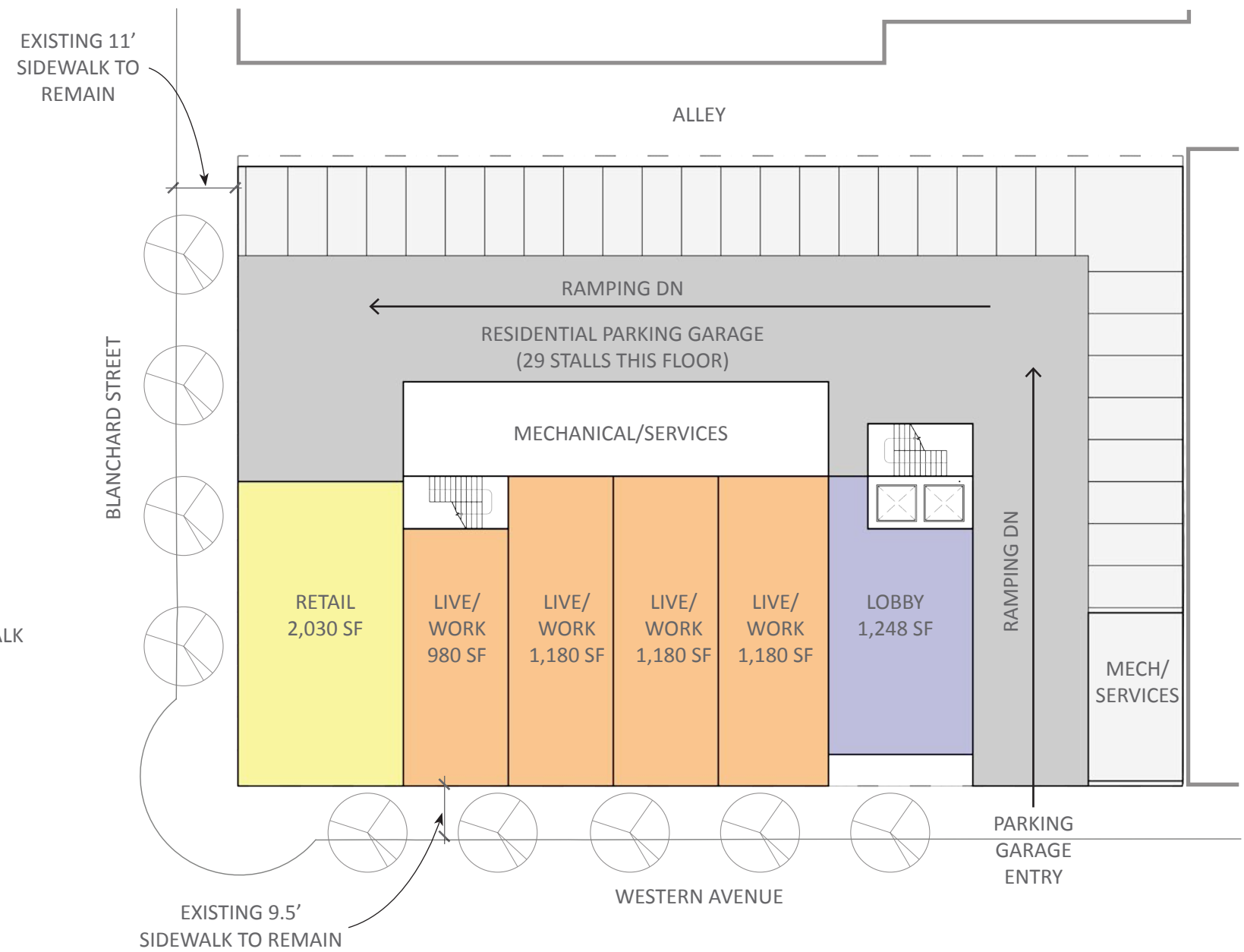
The building's massing matches that of the surrounding environment.



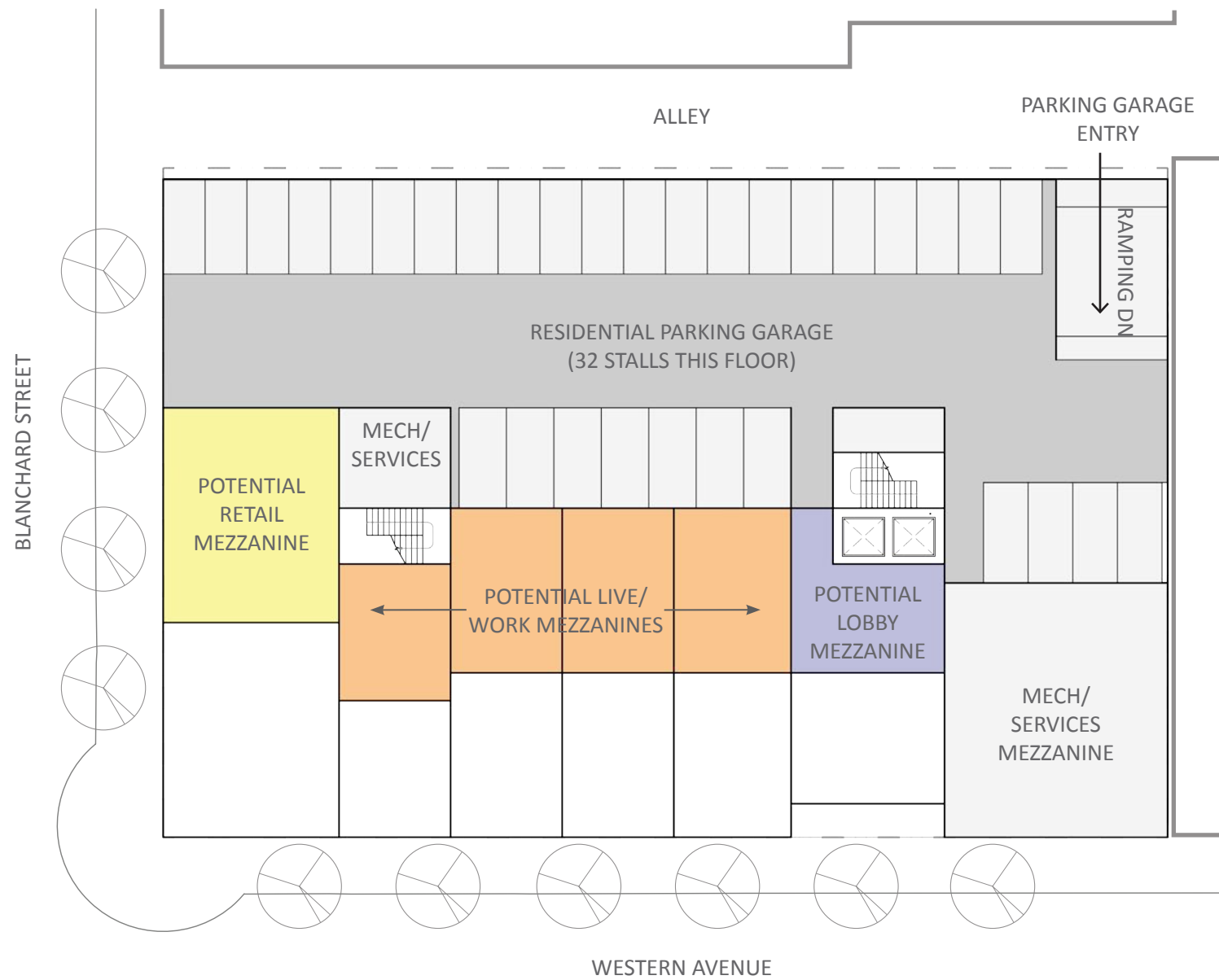
View Looking West Along Blanchard Street



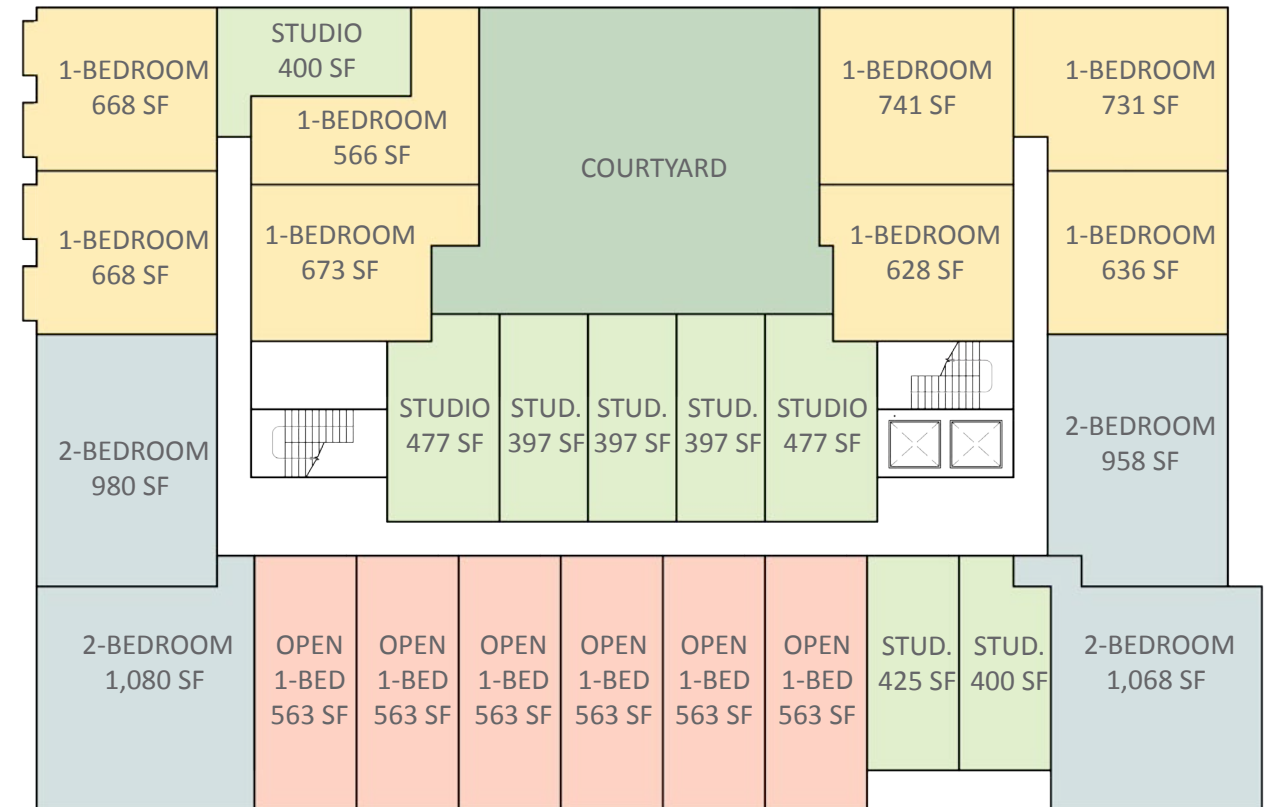
BUILDING SECTION



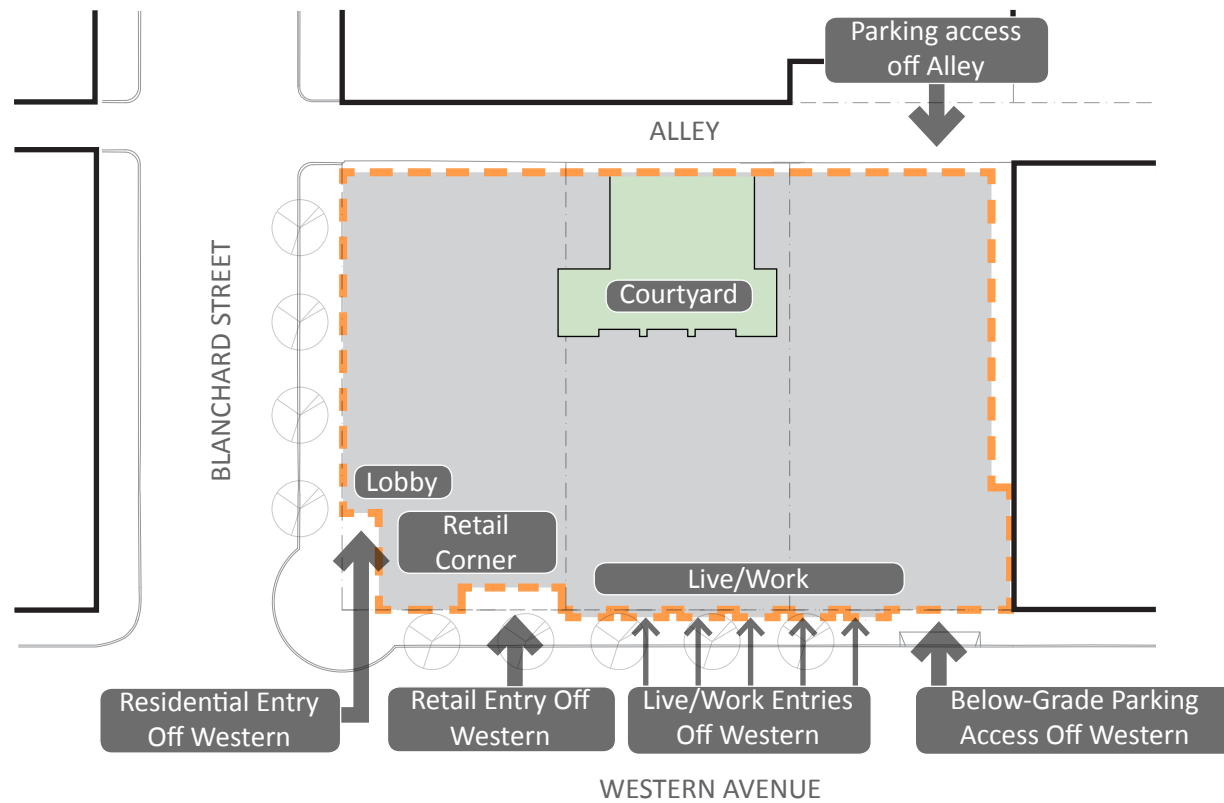
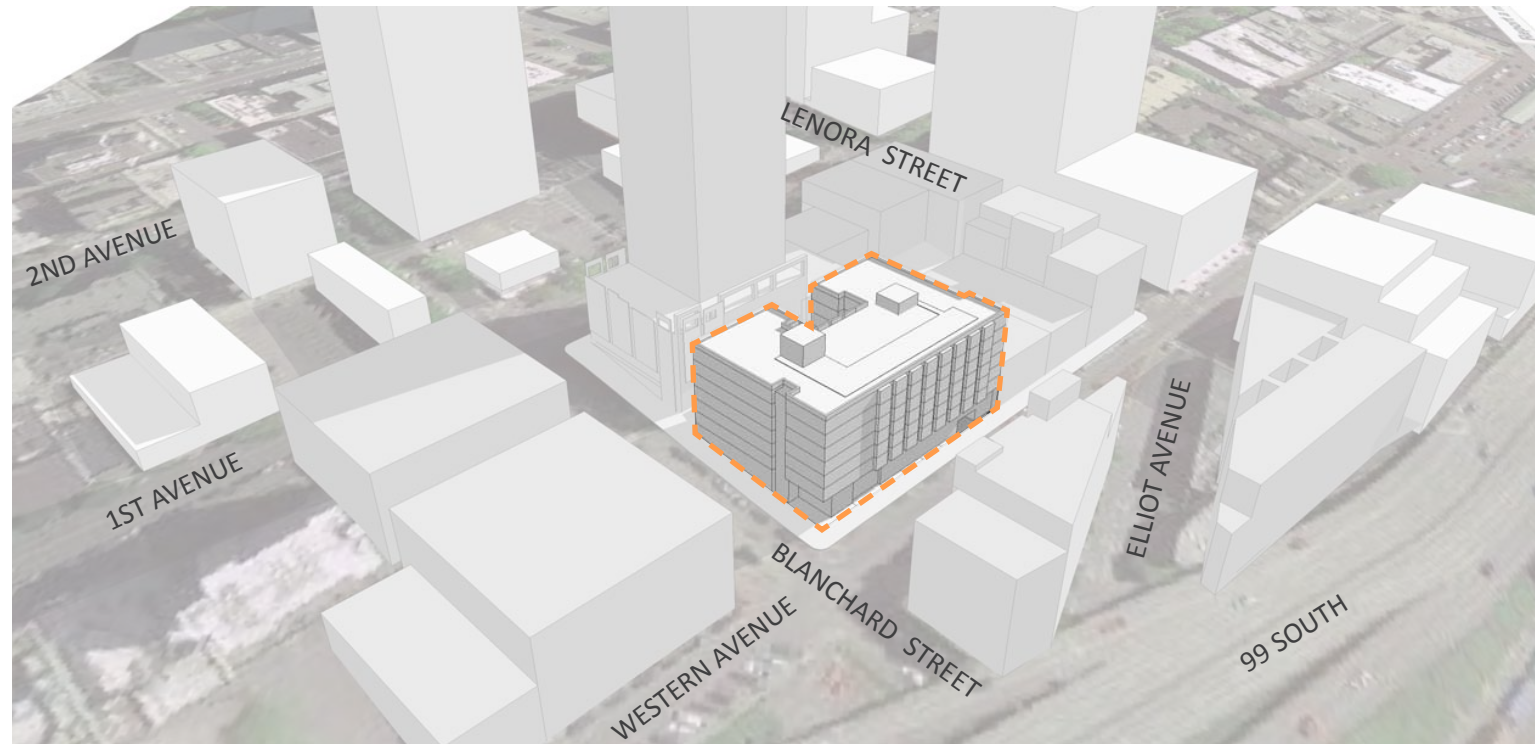
LEVEL 1 PLAN



LEVEL 1 MEZZANINE PLAN



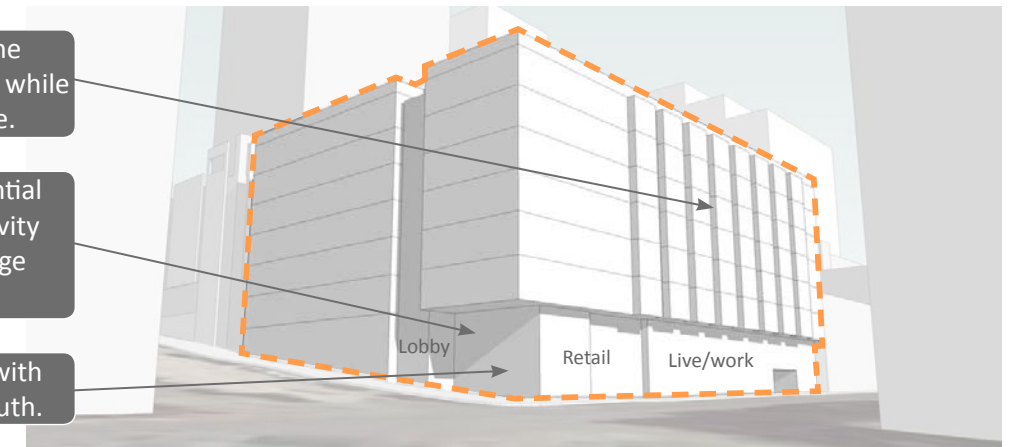
TYPICAL RESIDENTIAL PLAN



Residential bay windows add to the pedestrian nature of Western Avenue while still providing a strong street edge.

By co-locating the retail and residential lobby at the corner, the overall activity across the Western Avenue frontage becomes reduced.

A strong retail corner is consistent with the street level treatment to the south.

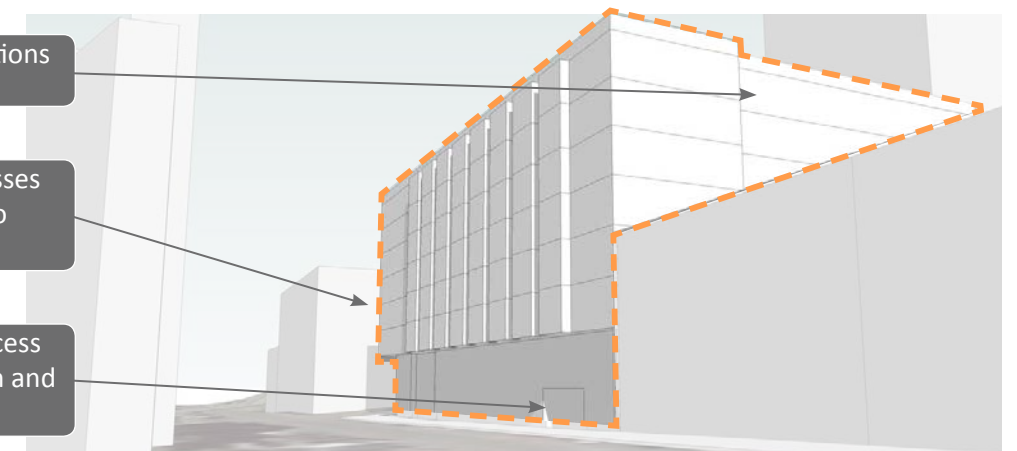


View at Blanchard Street and Western Avenue

Setback allows for window fenestrations along the south property line.

The recessed facade actively expresses the location of the lobby entry to pedestrians.

Western Avenue parking garage access increases the instances of pedestrian and vehicle interaction.

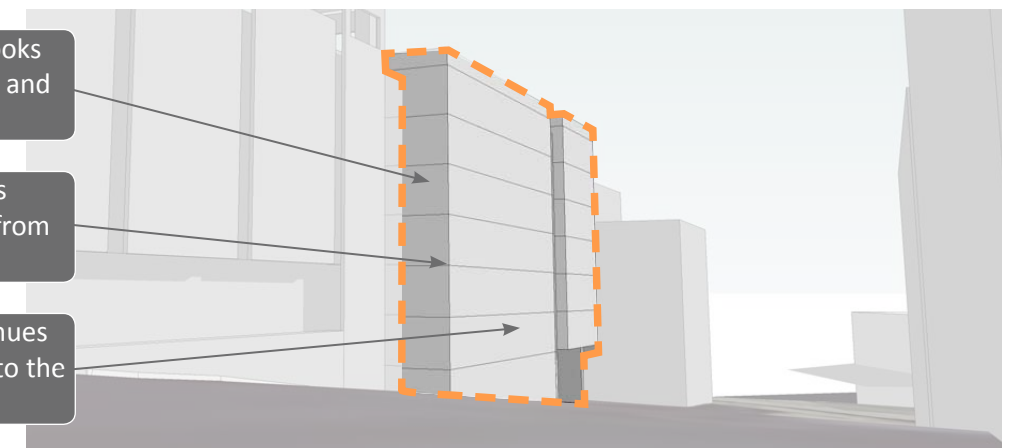


View Looking North Along Western Avenue

The eastern facing courtyard overlooks the alley instead of the Puget Sound and Olympic Mountains.

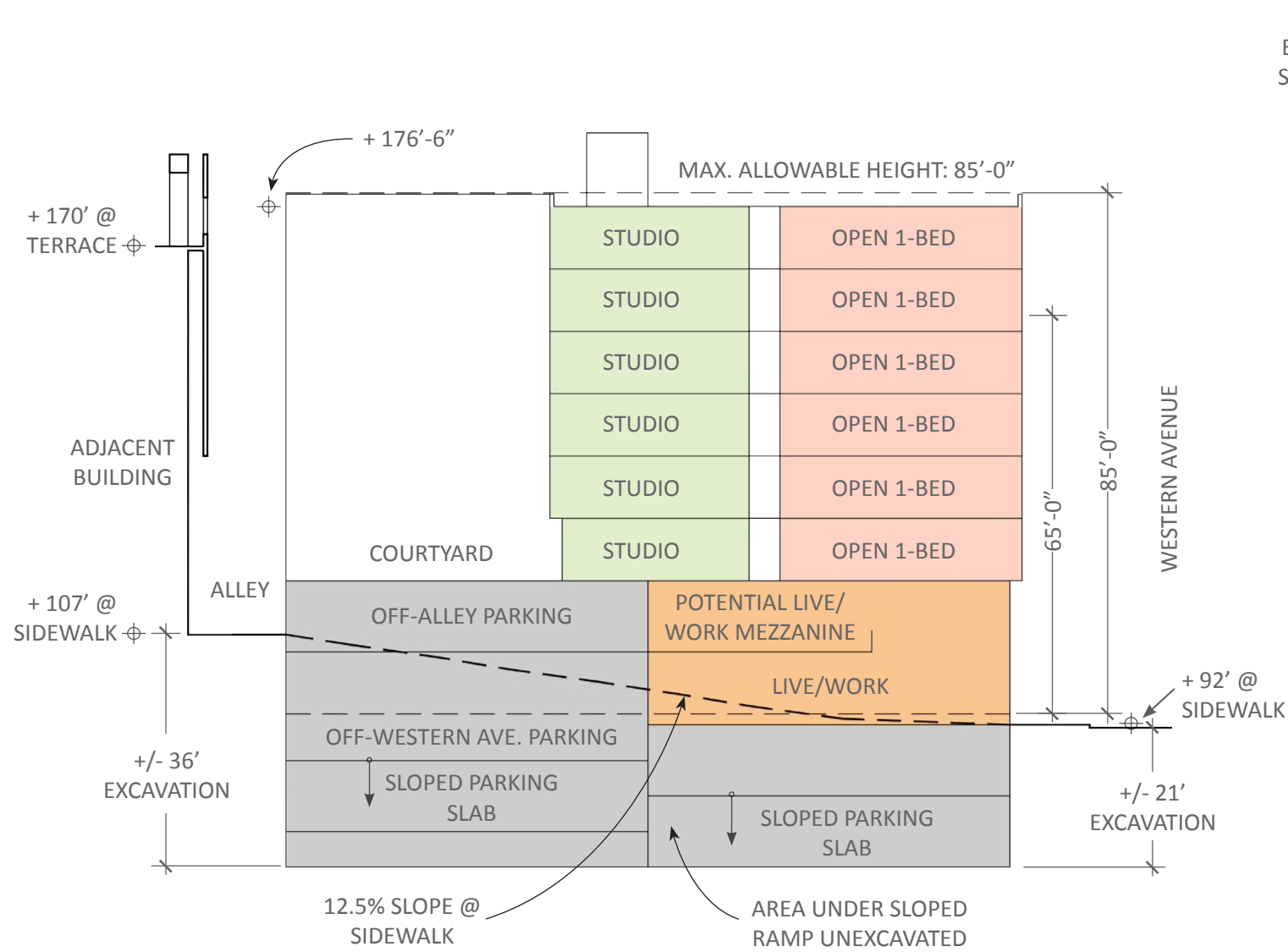
A strong corner at the alley helps differentiate the proposed building from the building to the east.

A strong wall along Blanchard continues the facade treatment of the building to the east.

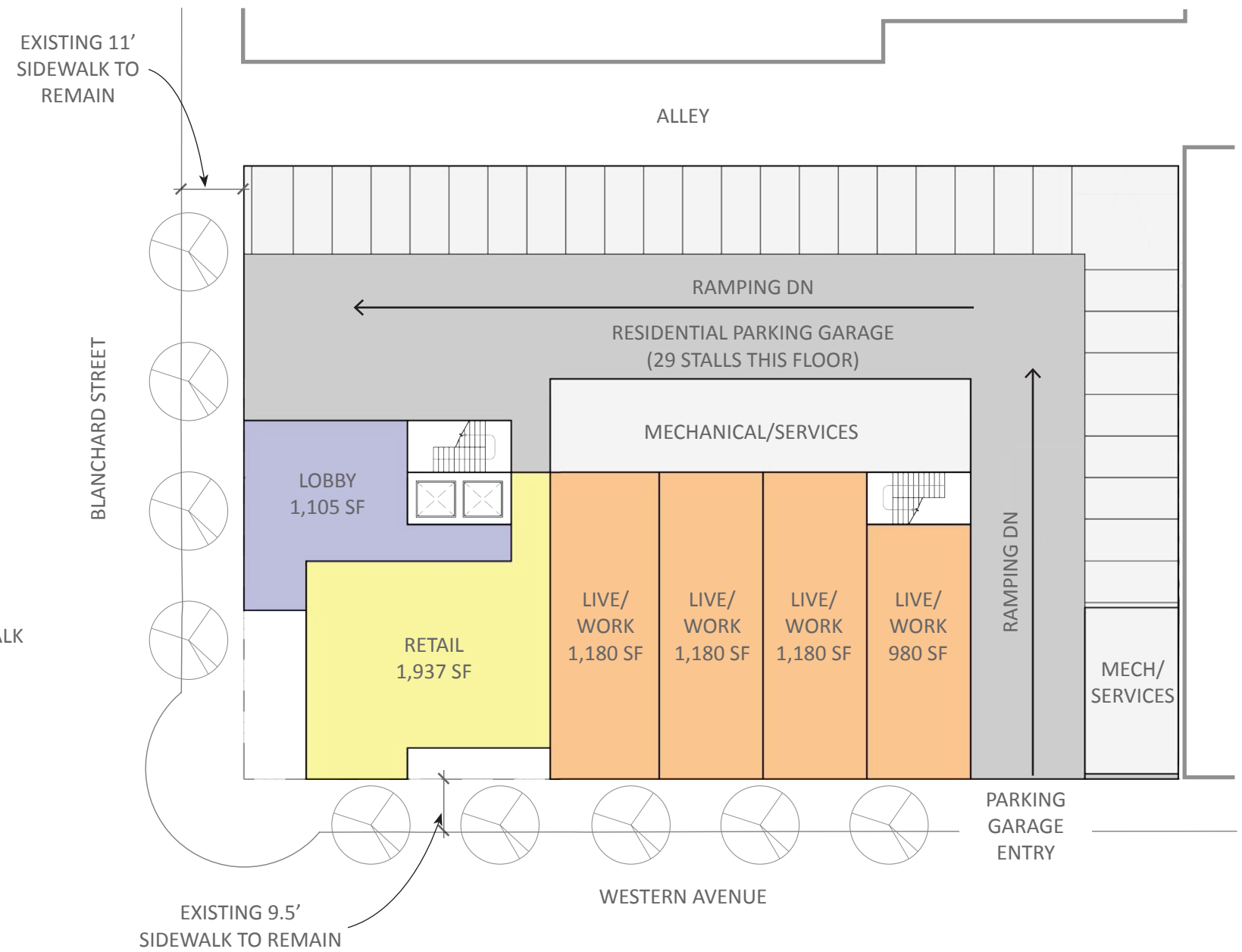


View Looking West Along Blanchard Street

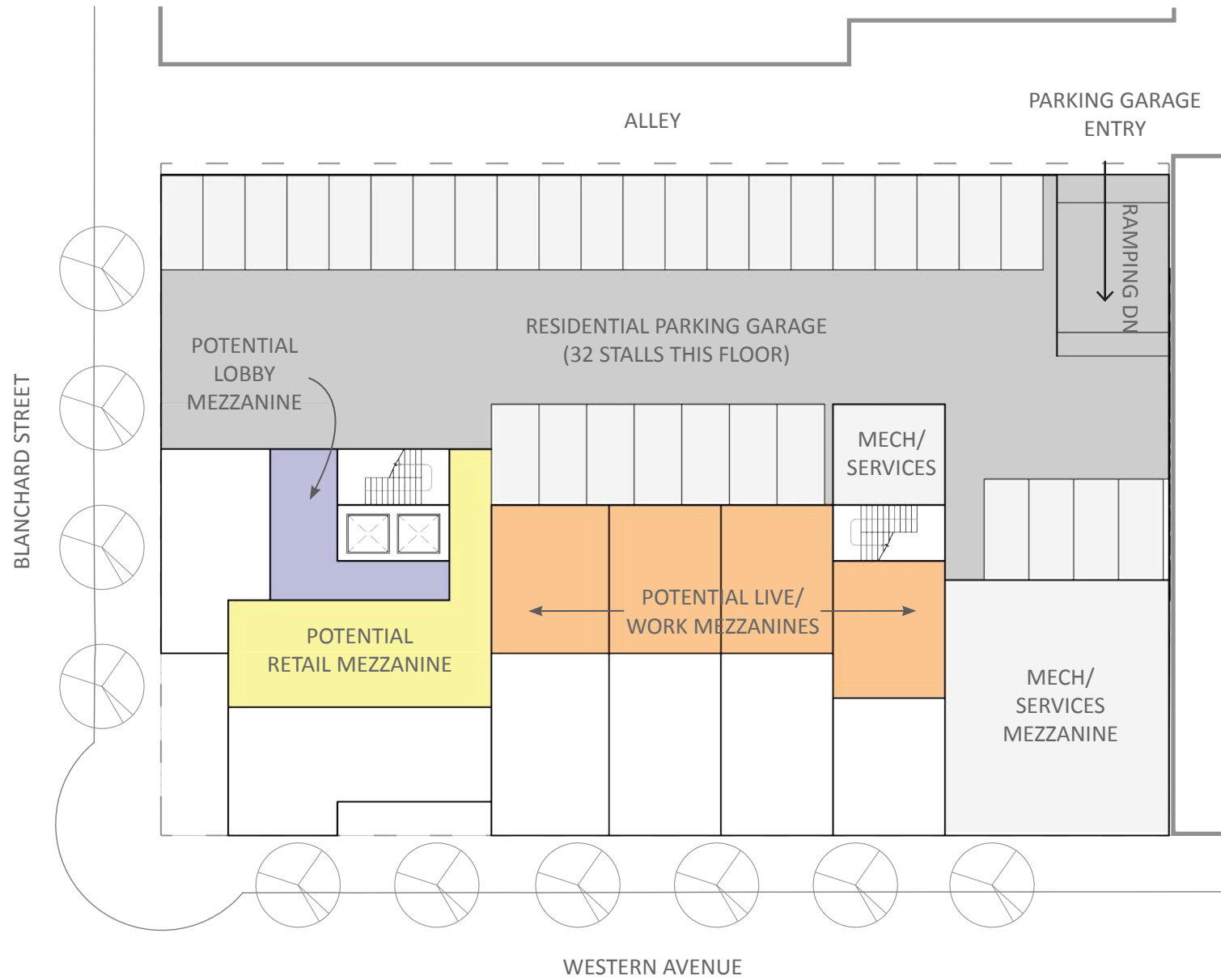




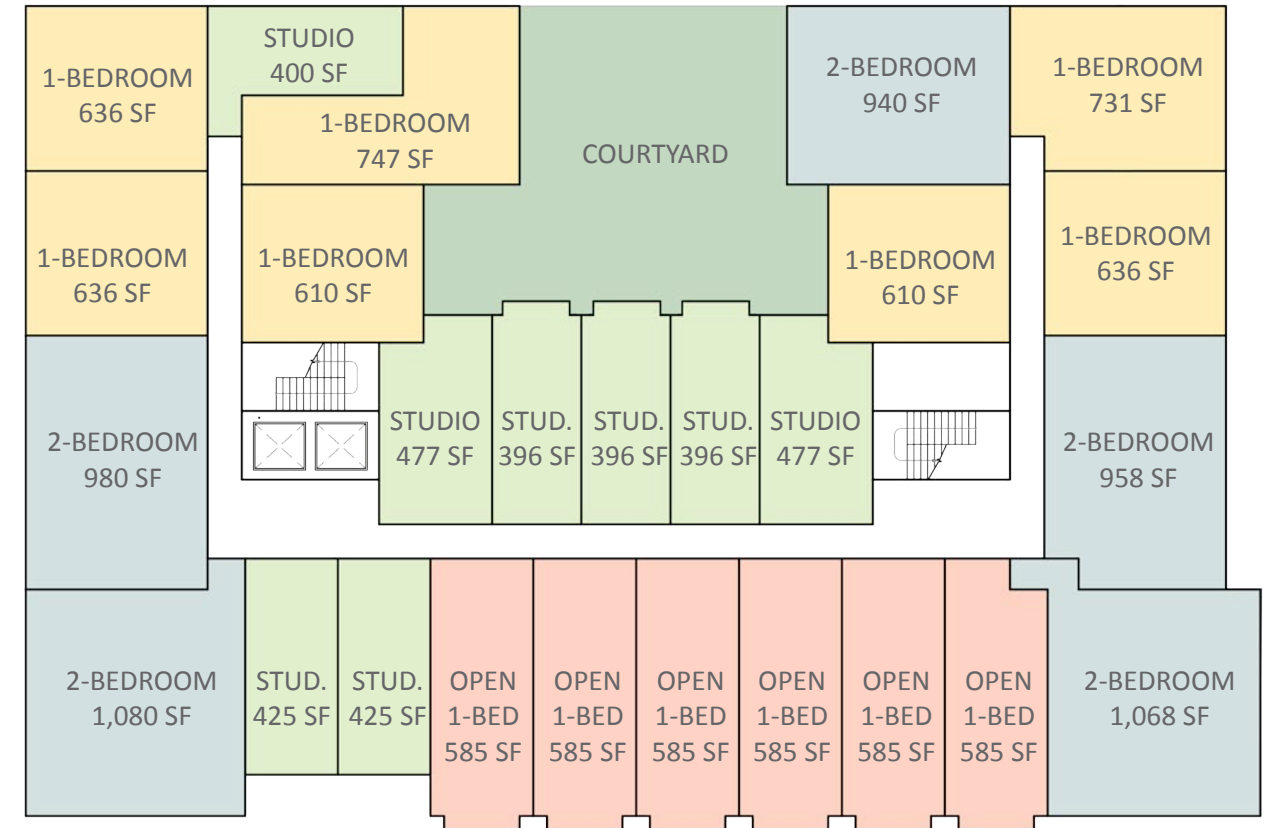
BUILDING SECTION



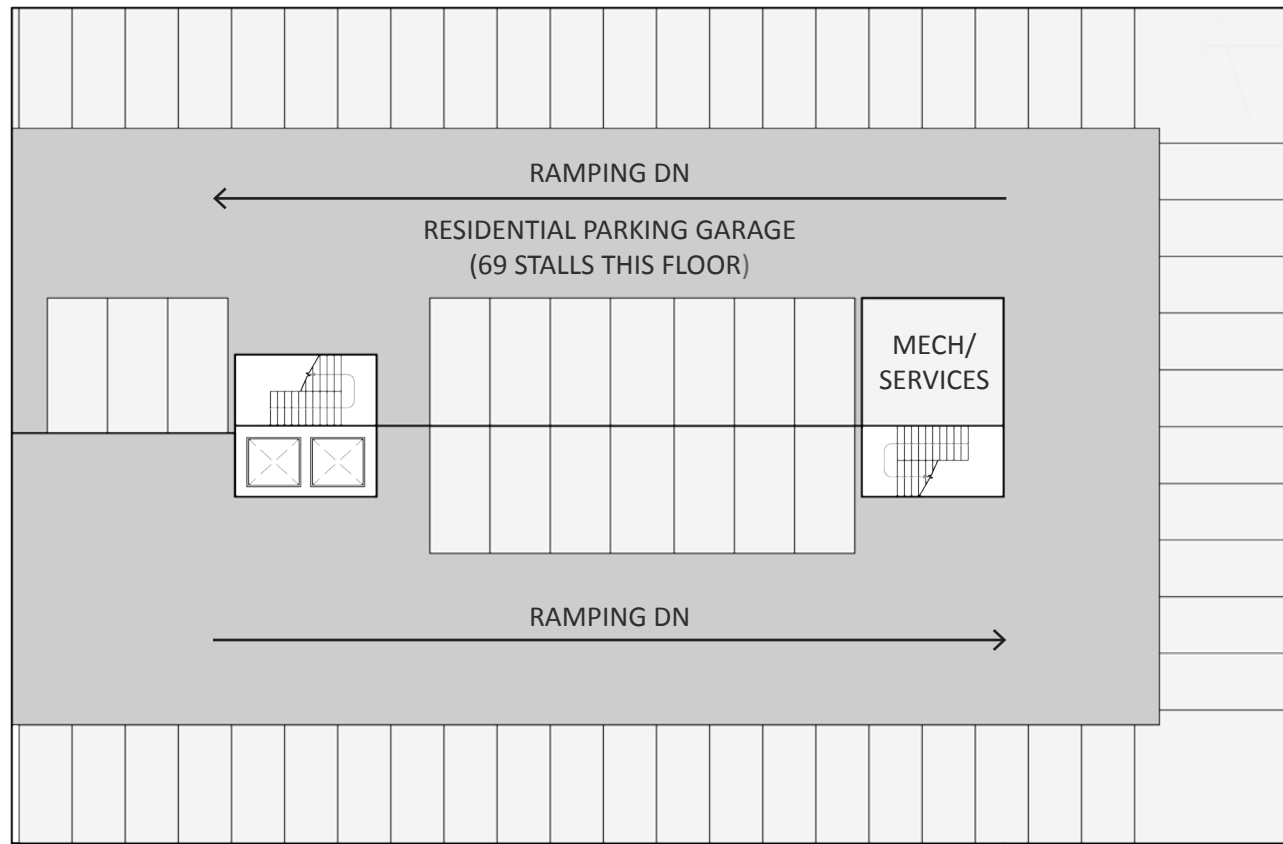
LEVEL 1 PLAN



LEVEL 1 MEZZANINE PLAN



TYPICAL RESIDENTIAL PLAN



LEVEL P1/P2

for DOWNTOWN DEVELOPMENT

**A. SITE PLANNING & MASSING**

- A-1 Responding to the Physical Environment
  - The massing of the proposed design mimics that of the buildings to the east and west.
  - There are potential views of Elliott Bay, Puget Sound and Mount Rainier.

- A-2 Enhance the Skyline
  - The mass of the proposed design enhances the skyline by providing deviation and finishing out the block in such a manner that is sympathetic to the surrounding context.

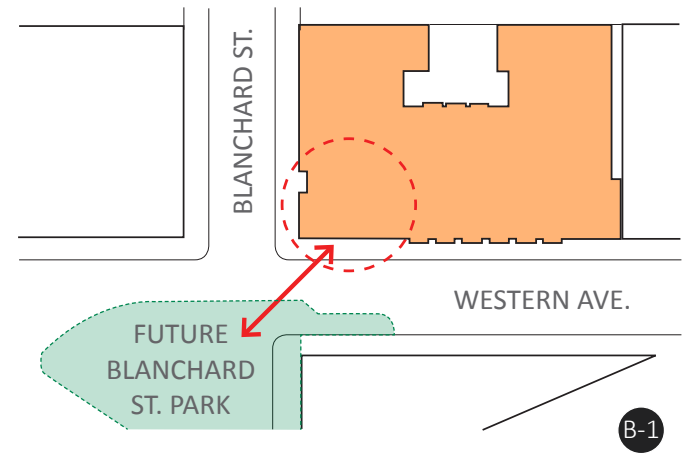
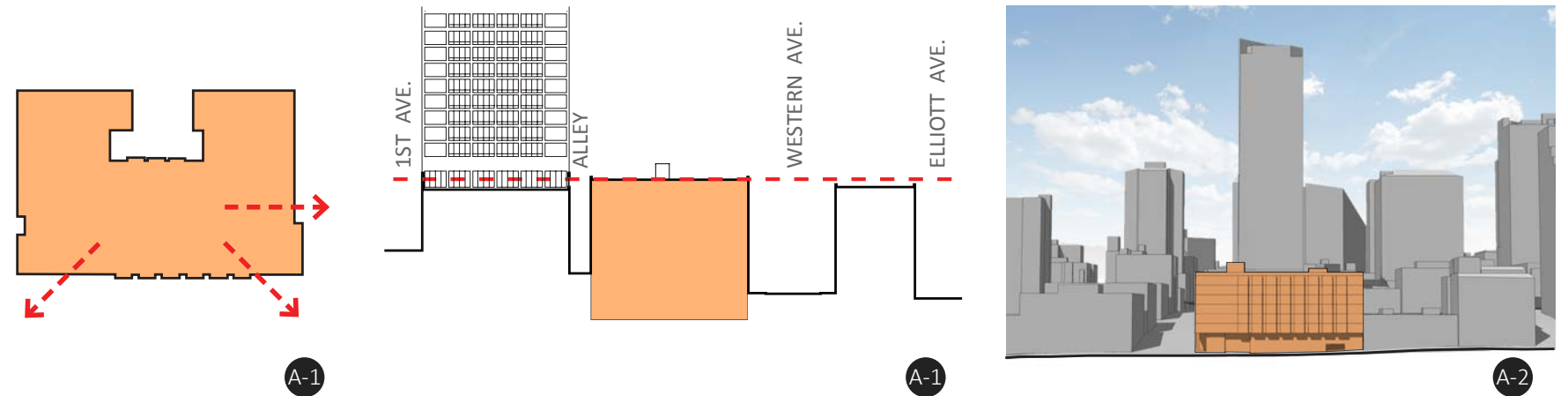
**B. ARCHITECTURAL EXPRESSION**

- B-1 Respond to the Neighborhood Context
  - Residential bay windows add to the pedestrian nature of Western Avenue while still providing a strong street edge.
  - The node of pedestrian activity at the retail and lobby spaces complements the Blanchard St. Park, which is part of the Waterfront Master Plan.

- B-2 Create a Transition in Bulk & Scale
  - Since the property does not fall at the edge of a downtown zone, the bulk and scale of the proposed design is compatible with that of the surrounding area. The requested setback departures further reinforce this continuity.

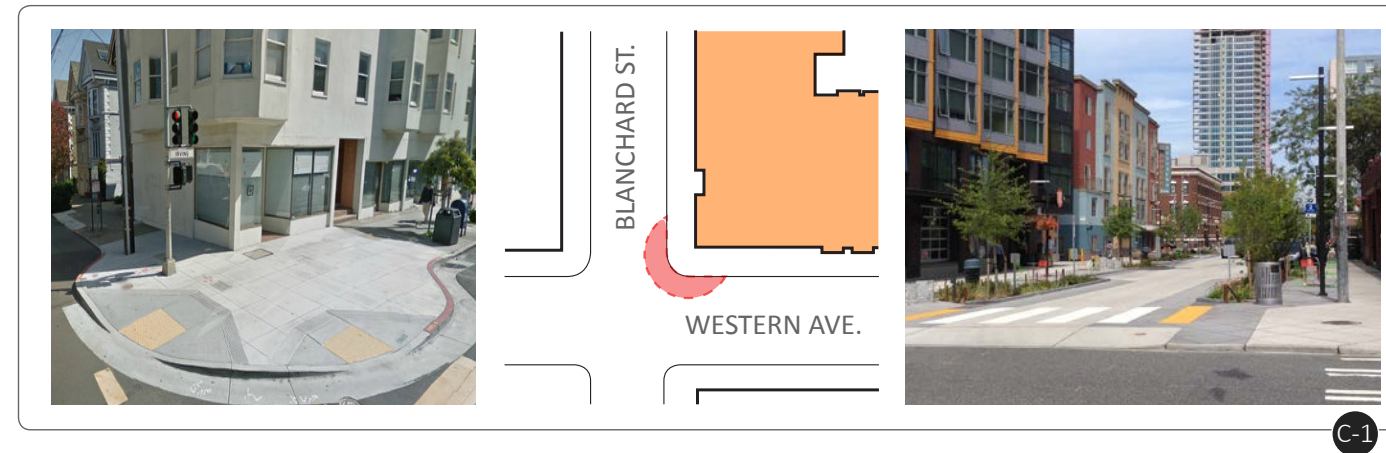
- B-3 Reinforce Positive Urban Form & Architectural Attributes
  - In response to the surrounding context, 0' setbacks at the street are incorporated into the proposed design.

- B-4 Design a Well Proportioned & Unified Building
  - The bay windows provide modulation in the facade along Western Ave.

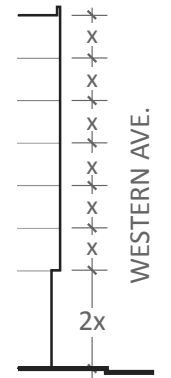


**C. THE STREETScape**

- C-1 Promote Pedestrian Interaction  
- A sidewalk bulb (currently part of the Waterfront Master Plan) creates a pedestrian-centric space.
- C-2 Design Facades of Many Scales  
- The double-height floor-to-floor at street level provides variation in facade scale and enhances the active facade by increasing visual interest for pedestrians.
- C-3 Provide Active - Not Blank - Facades  
- Glazing at the retail, lobby, and live/work spaces helps to engage pedestrians.
- C-4 Reinforce Building Entries  
- Adjacent entries for the retail and lobby spaces reinforces both an active corner and building security. The lobby entry location is visually reinforced by the recessed facade.



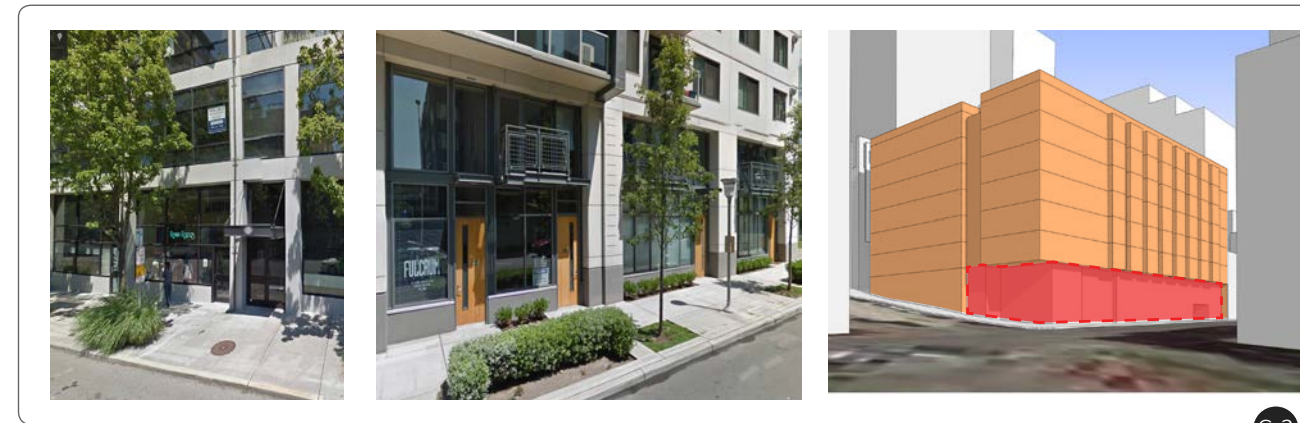
C-1



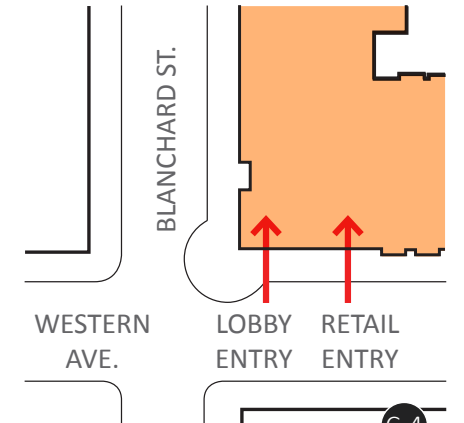
C-2

**D. PUBLIC AMENITIES**

- D-1 Provide Inviting & Usable Open Space  
- The proposed design includes a green roof, a roof-top terrace and a courtyard.
- D-3 Provide Elements That Define Place  
- Distinctive landscaping and possible public art at the sidewalk bulb help to define a sense of public place.



C-3



C-4

**E. VEHICULAR ACCESS & PARKING**

- E-1 Minimize Curb Cut Impacts  
- There are three existing curb cuts: two will be removed and one will remain at Western Ave. The second vehicular entry will be in the alley.
- E-2 Integrate Parking Facilities  
- The 130 parking spaces are visually minimized by being set below-grade.

for the BELLTOWN URBAN CENTER VILLAGE

**C. THE STREETScape**

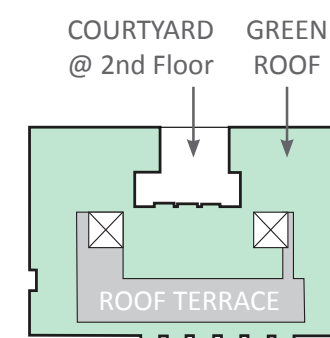
- C-5 Encourage Overhead Weather Protection  
- Overhead weather protection adds to a comfortable pedestrian environment.
- C-6 Develop the Alley Facade  
- The second parking garage entry provides a more dynamic and safe alley environment.

**D. PUBLIC AMENITIES**

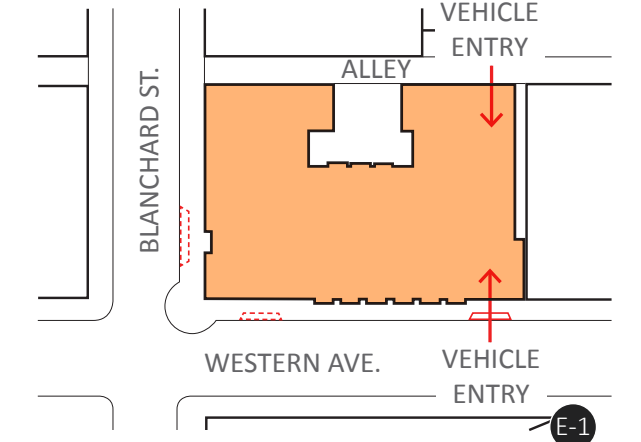
- D-2 Enhance the Building With Landscape  
- Landscaping along the street (specifically at the sidewalk bulb) and on the roof enhances a person's experience.



C-5

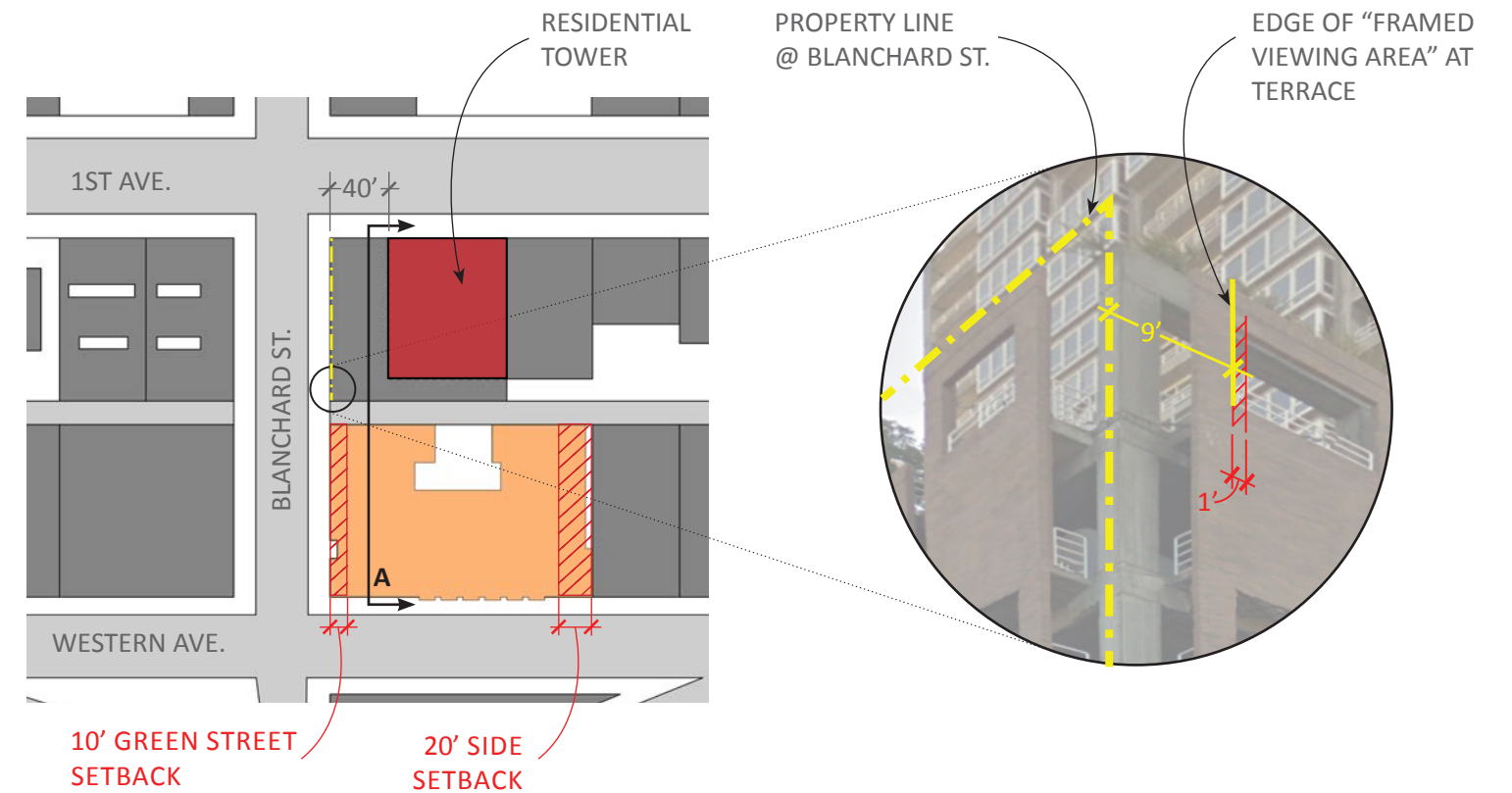
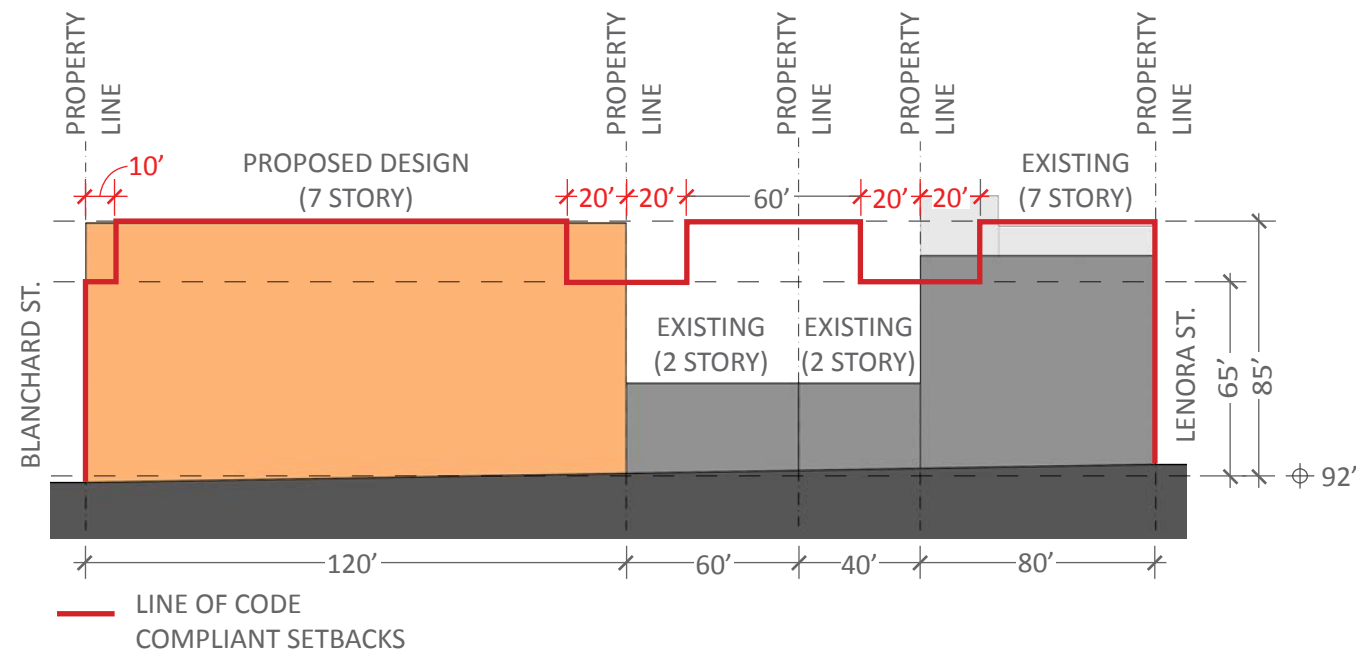


D-1

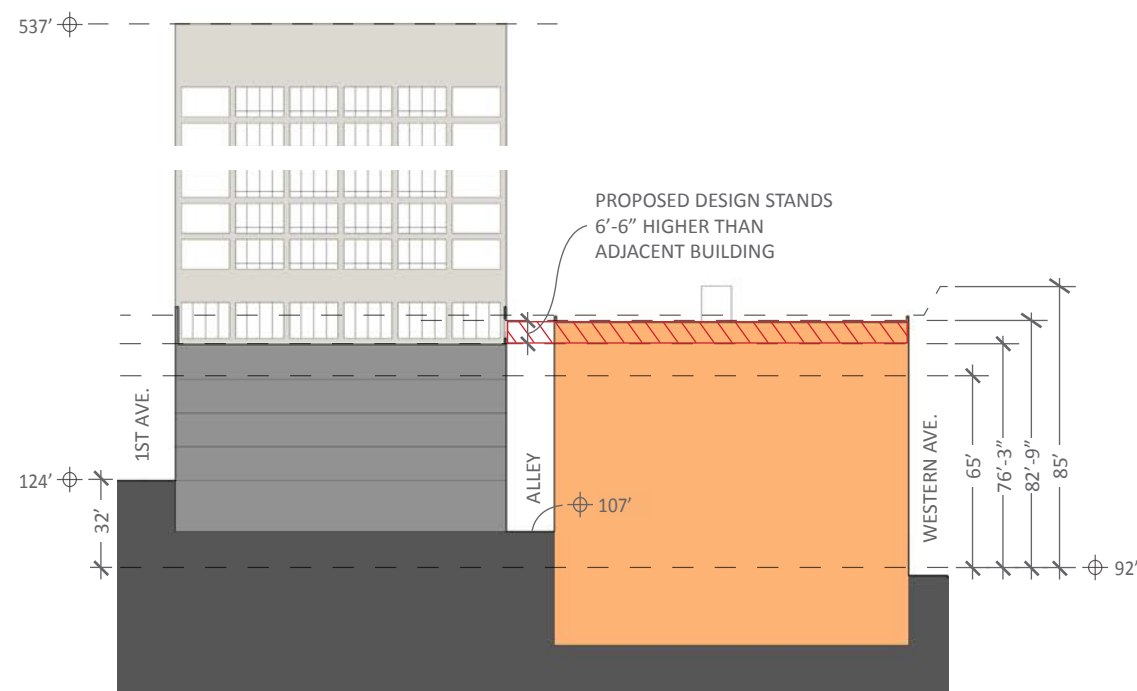


E-1

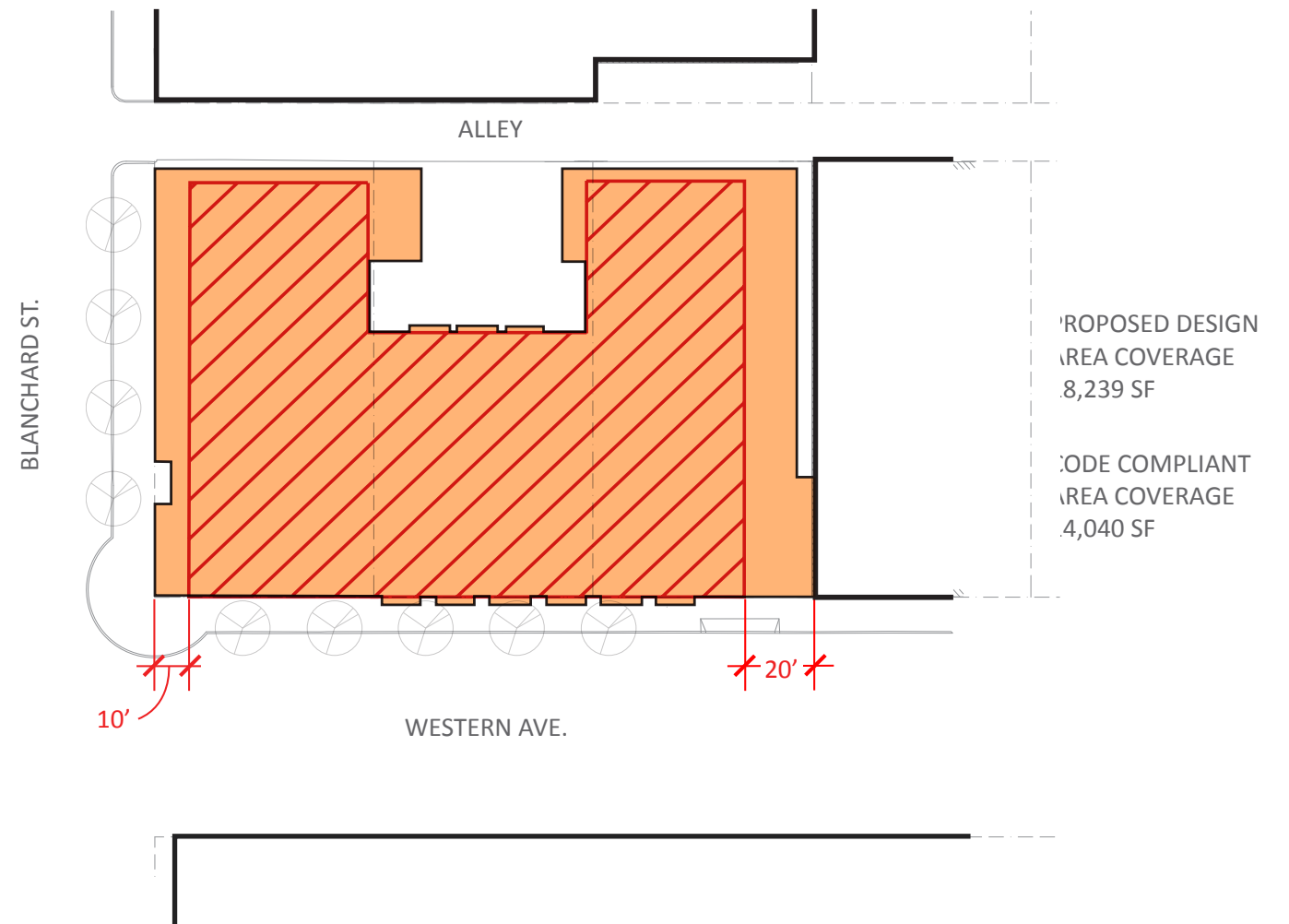
ITEM	DEVELOPMENT STANDARDS	REQUIREMENT	PROPOSED	DIFFERENCE	CONSIDERATIONS
1A	23.49.166 Downtown Mixed Residential, street facade requirements	Table A Required Side Setbacks Above 65 Feet, DMR Zones Outside South Downtown Except DMR/R 85/65 Zones Frontage on Avenue --> Greater than 120 feet up to 180 feet Req. Setback Above 65' --> 20 feet	We request a reduction of the 20 feet Side Setback above 65 feet to 0 feet.	20 feet of Side Setback.	With regard to the Side Setback Departure request, the 2 adjacent properties to the south are both 2-story structures and are only a combined width of 100 feet. The building to the south of those properties is a 6-story building that is built to the property line without a setback. Imposing the setback on the development site would set precedent for the parcels to the south leaving a width of only 60' above 65 feet for structure and would be in conflict with the existing conditions on the block. This request supports Design Guideline A1 (Respond to the Physical Environment) by reinforcing the building's relationship with the existing and future surrounding environment.
1B	23.49.166 Downtown Mixed Residential, street facade requirements	B. Green Street Setbacks. In DMR zones outside South Downtown, except in DMR/R 85/65 zones, a setback is required from the street lot line abutting a green street designated on Map 1B. The setback shall be as follows: 1. Ten feet for portions of structures above 65 feet in height to a maximum of 85 feet	We request a reduction of the 10 feet Green Street Setback above 65 feet to 0 feet.	10 feet of Green Street Setback.	Due to the steep slope along Blanchard Street, the base of the adjacent building to the east is approximately 32 feet higher than the base of the proposed building. This makes the 65 feet height lower than that of the adjacent building. The residential tower portion of the adjacent building, which starts at 169' (terrace height), is set back from the Blanchard St. property line by 40'. The view to the south from the "framed viewing area" on the adjacent building's terrace begins at 9' inward from the property line, so the 10' Green Street setback departure would potentially hinder only 1 foot of a person's view from the terrace. This request would allow the proposed design to comfortably continue the massing of the adjacent building along Blanchard St. in response to Design Guideline B3 (Reinforce the positive urban form & architectural attributes of the immediate area).



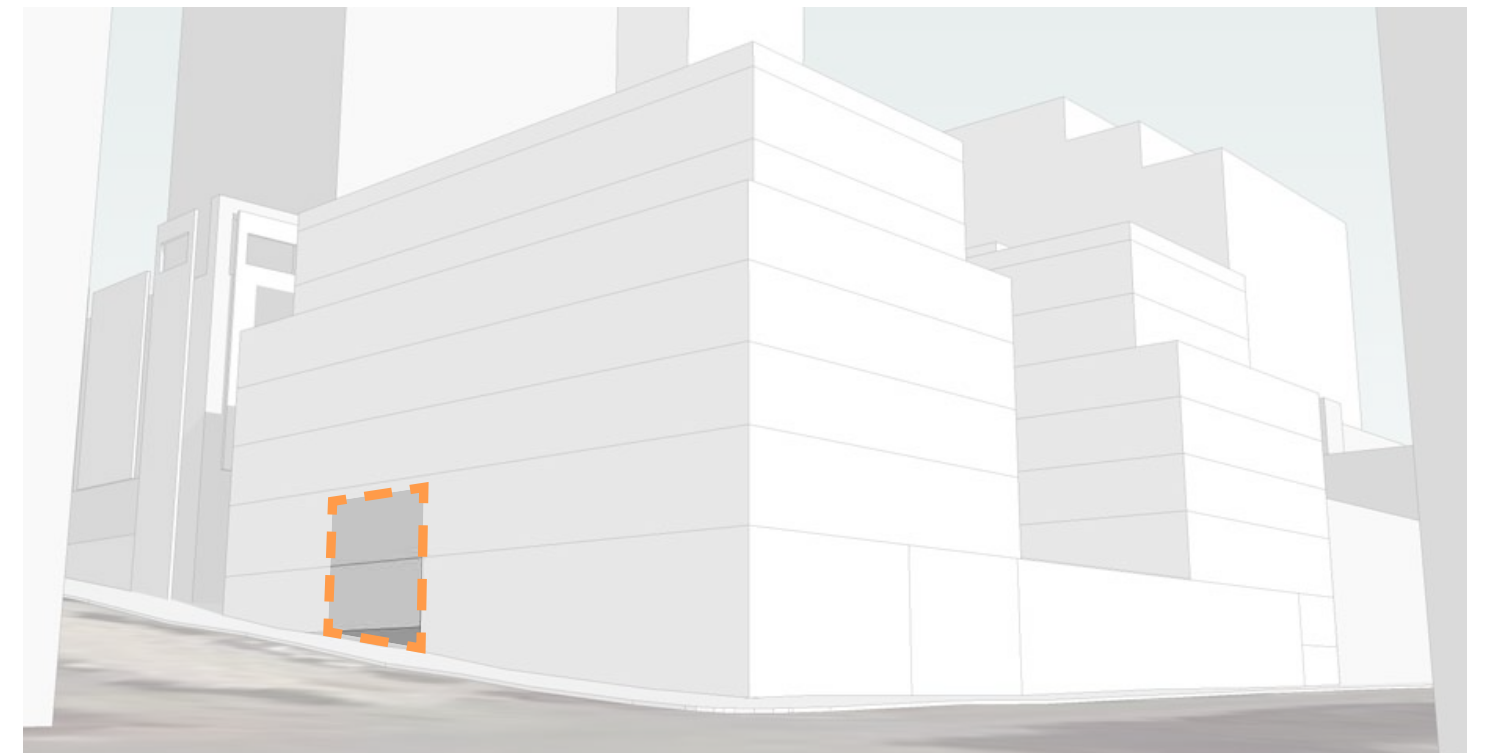
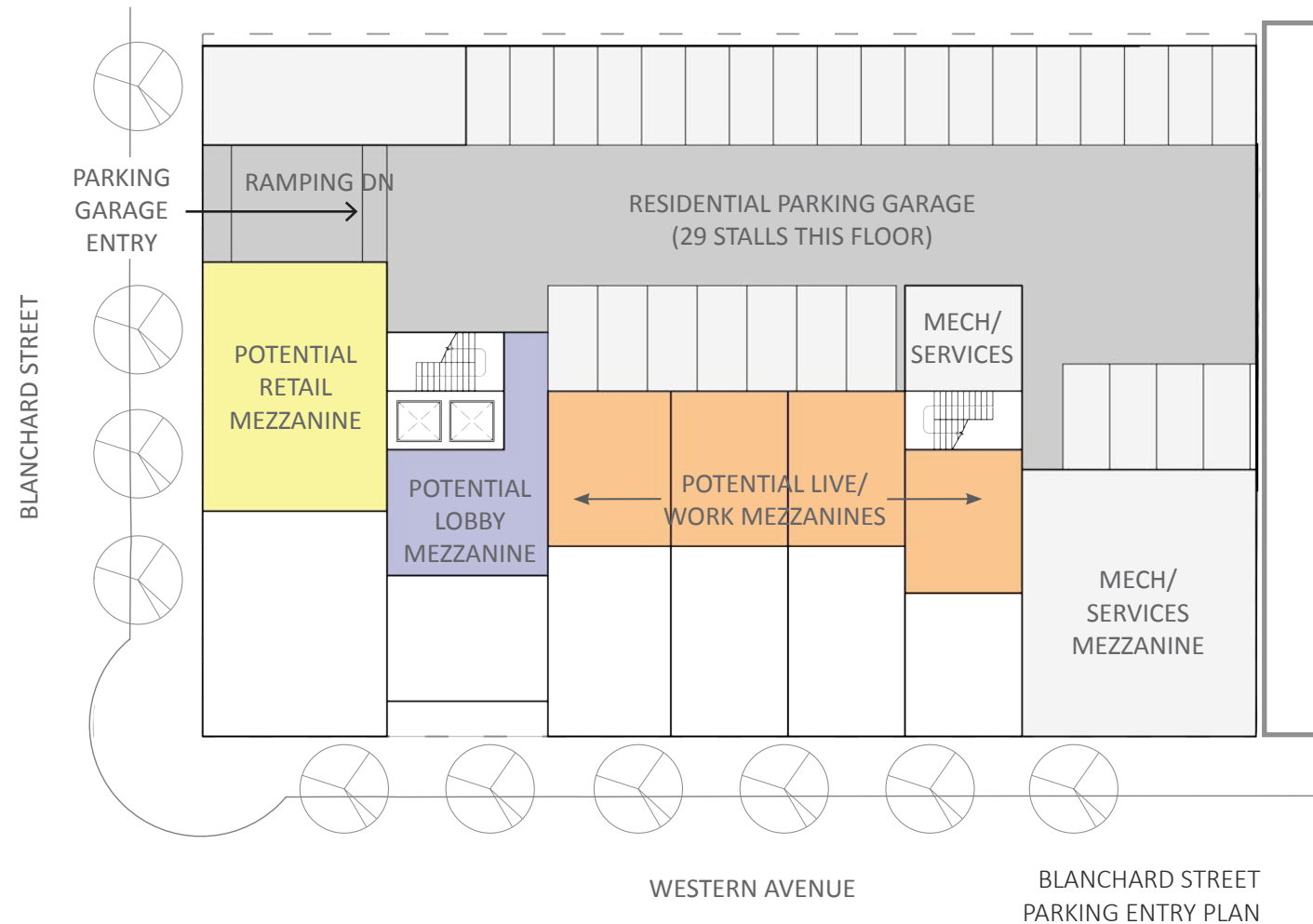
ITEM	DEVELOPMENT STANDARDS	REQUIREMENT	PROPOSED	DIFFERENCE	CONSIDERATIONS
2	23.49.158 Downtown Mixed Residential, coverage and floor size limits	Table A Percent Coverage Permitted by Lot Size Lot size = 180' x 120' = 21,600 SF (19,001 - 25,000 SF) Elevation of Portion of Structure ≤ 65' --> 100% Greater than 65 feet up to 85 feet --> 65% 65% x 21,600 = <b>14,040 SF</b>	We request an increase in the required 65% area coverage above 65' (14,040 SF) to 85% (18,239 SF) area coverage.	4,199 SF of additional coverage at floors 6 and 7 each.	The property to the east is zoned DMR/R and is not subject to the same coverage restriction. The added area coverage requested (in conjunction with Departure #2) would not adversely affect views and would help reinforce the positive urban form already set by buildings in the immediate vicinity.



Section showing the relative height of the adjacent building to the east



DEVELOPMENT STANDARD	REQUIREMENT	PROPOSED	DIFFERENCE	CONSIDERATIONS
23.49.019 Parking quantity, location and access requirements, and screening and landscaping of surface parking areas.	23.49.019.H.1c The Director may allow or require access from a right-of-way other than one indicated by subsection 23.49.019.H.1.a or 23.49.019.H.1.b if, after consulting with the Director of Transportation on whether and to what extent alternative locations of access would enhance pedestrian safety and comfort, facilitate transit operations, facilitate the movement of vehicles, minimize the on-street queuing of vehicles, enhance vehicular safety, or minimize hazards, the Director finds that an exception to the general policy is warranted. Curb cut controls on designated green streets shall be evaluated on a case-by-case basis, but generally access from green streets is not allowed if access from any other right-of-way is possible.	We request one parking access to be permitted to the proposed structure at the location of an existing parking access curb cut along Western Avenue. Additionally, one parking access is proposed from the abutting alley in conformance with 23.49.019.H1.a for a total of two parking access points.	1 additional parking access.	The existing site topography and related building geometry requires this departure in order to provide adequate access to provide below-grade parking facilities. Two parking access points work to reduce parking traffic quantities at the alley and curb cut. Per SMC 23.49.019.H.1c, right-of-way access is not permitted from Green Streets. Blanchard Street is a designated Green Street, and therefore not a viable option for parking access. We believe that the re-use of the existing Western Avenue parking curb cut access enhances pedestrian safety and comfort, facilitates the movement of vehicles, minimizes the on-street queuing of vehicles, enhances vehicular safety and minimizes potential hazards. In response to Design Guideline E1 (Minimize Curb Cut Impacts), this request removes one of the two existing curb cuts off of Western Ave. and replaces it with the access point through the alley.



BLANCHARD STREET PARKING ENTRY PERSPECTIVE





Staybridge Hotel on Aurora  
(Under Construction)



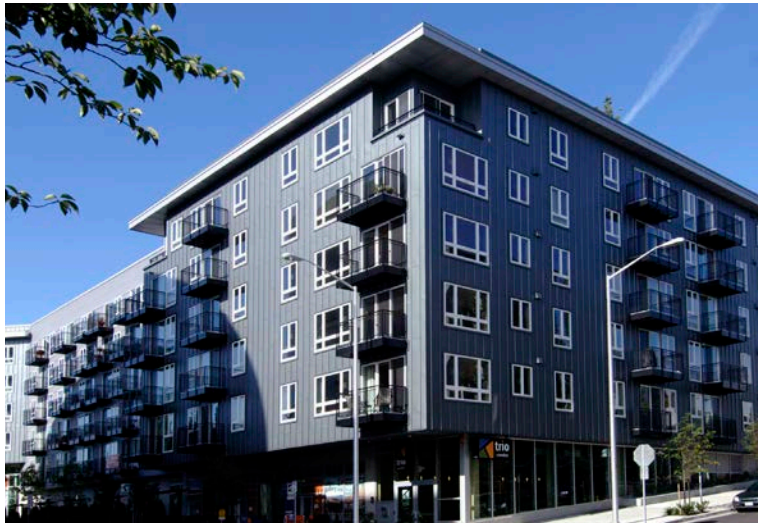
80 South Main Street  
(In Design)



Publix  
(In Design)



Downtowner II  
(In Design)



TRIO Condominiums  
(Complete 2007)



Ballard Blocks  
(Complete 2009)



Alaska Building - Hotel Renovation/Addition  
(Complete 2009)



Belay Apartments  
(Complete 2013)

The Natasha Project  
2134 Western Avenue  
Early Design Guidance Meeting- DPD #3017563

29 July 2014