

**DRB RECOMMENDATION PACKAGE**

**DATE: JANUARY 14, 2015**

**MEETING TYPE: DRB RECOMMENDATION MEETING**

**MEETING DATE: JANUARY 27, 2015**

**DESIGN REVIEW BOARD: SOUTHEAST**



## PUBLIC STORAGE

1200 SOUTH DEARBORN STREET, SEATTLE, WA

**DPD PROJECT #:** 3017092

**OWNER:**

Public Storage  
1755 NE 48th St., Ste A1  
Renton, WA 98050  
206-972-5200

**ARCHITECT:**

BCRA // Joe Rydman  
414 Stewart Street, Suite 200  
Seattle, WA 98101  
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**DPD CONTACT:**

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Carly.guillory@seattle.gov

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<b>Client</b>	Public Storage 1755 NE 48th Street, Suite A1 Renton, WA 98056
<b>Project Facilitator</b>	Kathryn Jerkovich, kjerkovich@bcradesign.com
<b>Site Description</b>	A rectangular shaped parcel site bounded by parcels 8170100430 and 8170100455 and the unimproved right-of-way for South Lane Street to the north, parcel 0524049017 and 13th Ave South to the east, parcels 0609000080 and 0609000085 and the 12th Ave South Bridge to the west, and South Dearborn Street and Interstate 90 to the south. The project site is characterized with steep slopes previously created by the 1912 Dearborn Cut. The site has been previously developed and includes a two-story of approximately 9,673 sf located in the southeast corner, on-site parking, and a cell tower in the northwest corner.
<b>Parcel ID</b>	8170100520
<b>Address</b>	1200 South Dearborn Street, Seattle, WA
<b>Site Area</b>	58,262 sq. ft.
<b>Site Zoning</b>	DMC 85/65-150
<b>Overlay District</b>	Chinatown International Urban Center Village
<b>Project Description</b>	Demolition of existing 9,673 square foot building and on-site parking, site grading, and construction of a new multi-story self-storage building.
<b>Uses by Floor</b>	First Floor: Retail, Storage Second Floor: Storage Third Floor: Storage, Residential Fourth-Sixth Floor: Storage
<b>Construction Types</b>	Construction Type 1A for 1st level - Concrete columns and PT slab podium. Construction Type 2A for floors 2 through 6 - post and beam

## DEVELOPMENT OBJECTIVES

The proposal is to apply for a Master Use Permit for development on the lot bound by South Dearborn Street, 13th Ave South, 12th Ave South, and the unimproved Lane Street in the Chinatown International Urban Center Village.

The development will consist of a self storage facility of approximately 218,500 square feet, containing 1,899 storage units of various sizes, a retail space along South Dearborn Street, 23 short-term parking stalls, and one 1,100 square foot residential apartment.

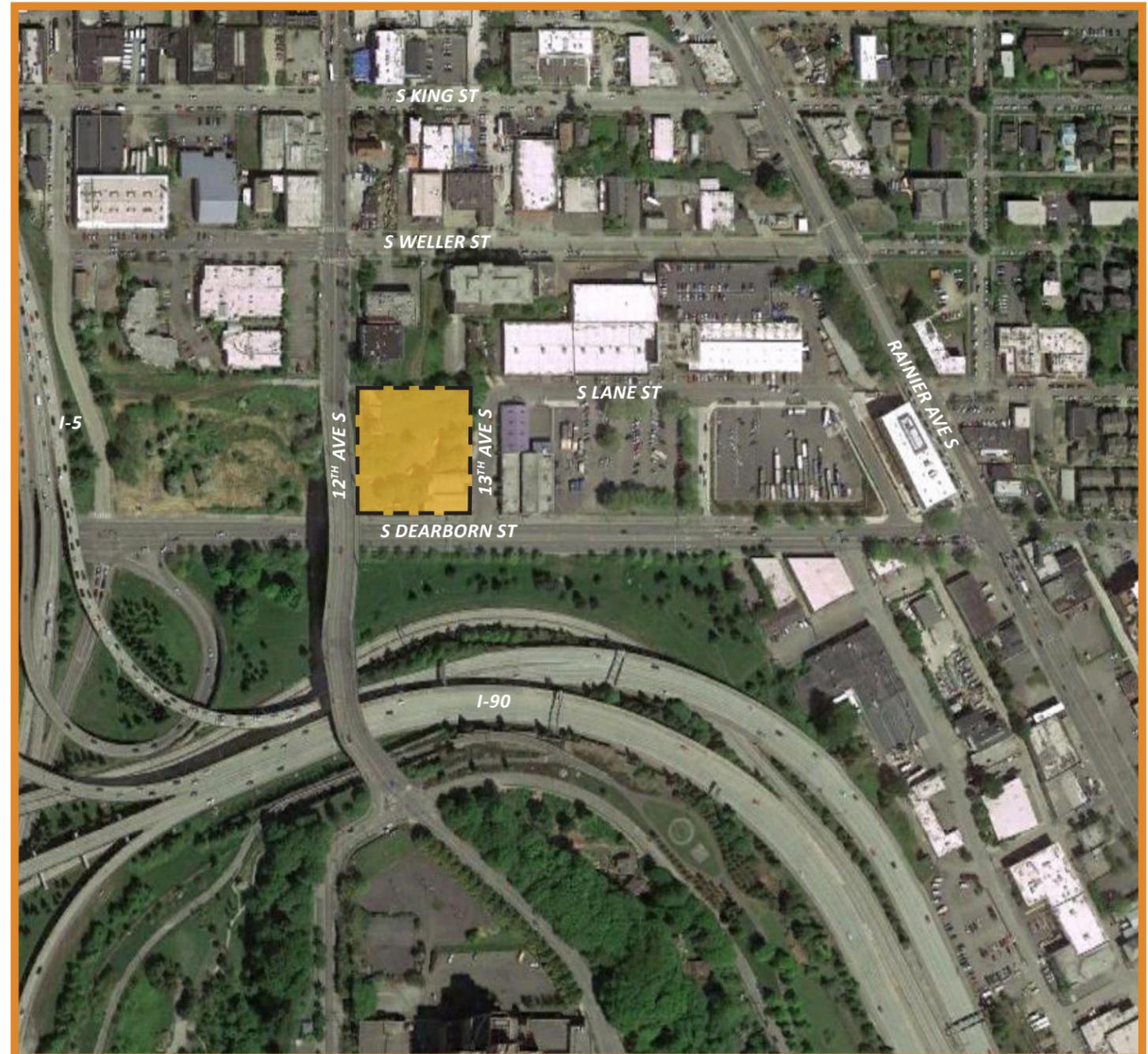
The proposed building design will include a six-story, 75'-5" foot high building that will include a podium at the first level containing the retail, short-term parking and storage units. Above the podium will be five levels of storage units and the one residential apartment. Access to the site is proposed on 13th Ave South and South Dearborn Street. Due to the steep slope on the site, access from 12th Ave South and the unimproved South Lane Street are not possible. Portions of the first four levels of the building are anticipated to be constructed into slope, supported by structural retaining walls.

## Summary:

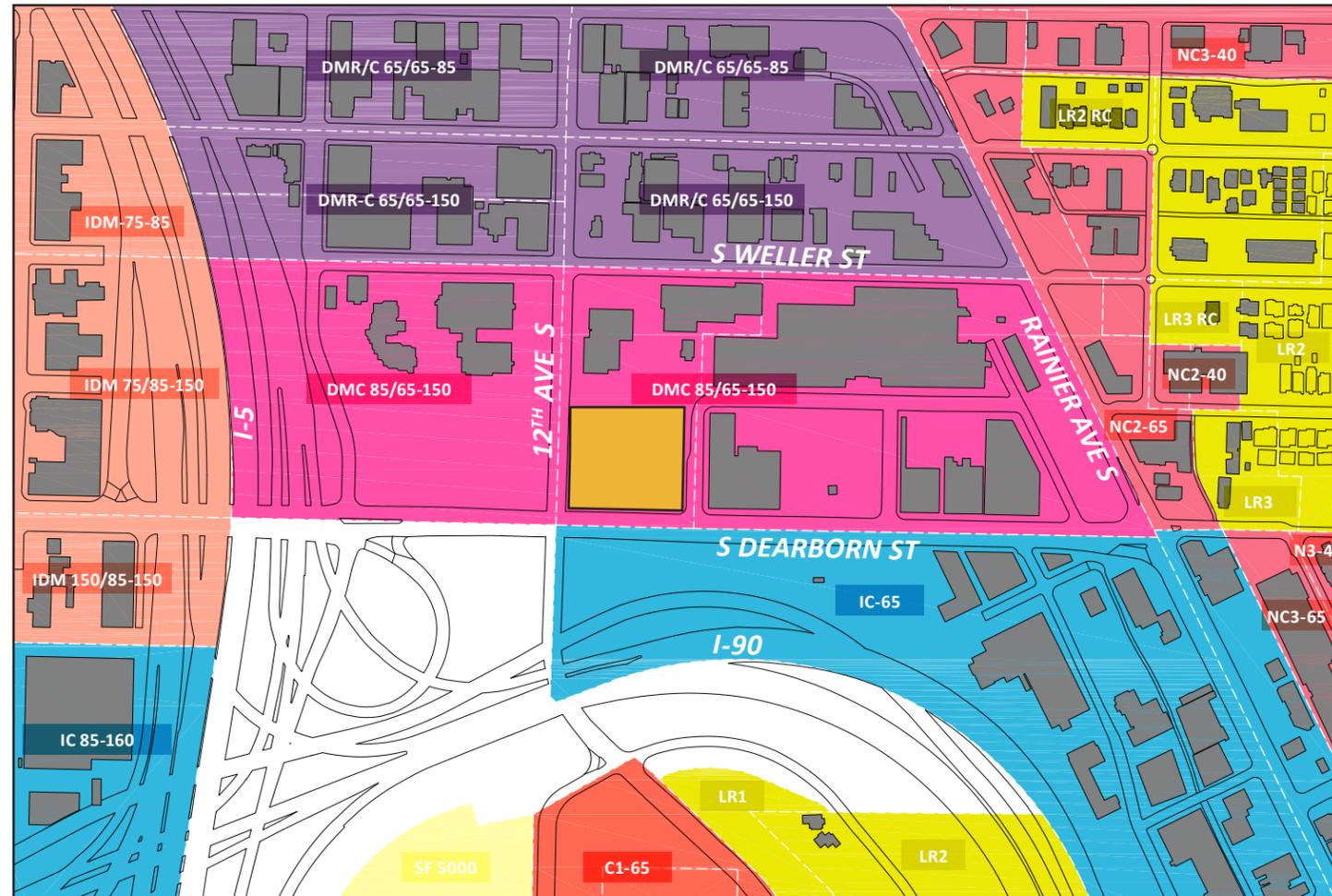
- 1,899 storage units
- 1,172 square feet of retail space on the first level
- One 1,100 square foot residential apartment
- Six levels with four levels partially below grade
- 23 Short-term parking stalls

## City Approvals Received To Date:

- Early Design Guidance
- Street Improvement Exception for South Lane Street
- ECA Waiver to Steep Slope Development Standards



## ZONING MAP



### LEGEND

- IDM - International District Mixed
- IC - Industrial Commercial
- DMC - Downtown Mixed Commercial
- DMR/C - Downtown Mixed Residential / Commercial
- Proposed Public Storage Building
- NC - Neighborhood Commercial
- LR - Lowrise
- C - Commercial
- SF - Single Family

## KEY ZONING AND LAND USE ISSUES

### SITE DATA:

**Area:**  
58,262 sf / 1.34 AC

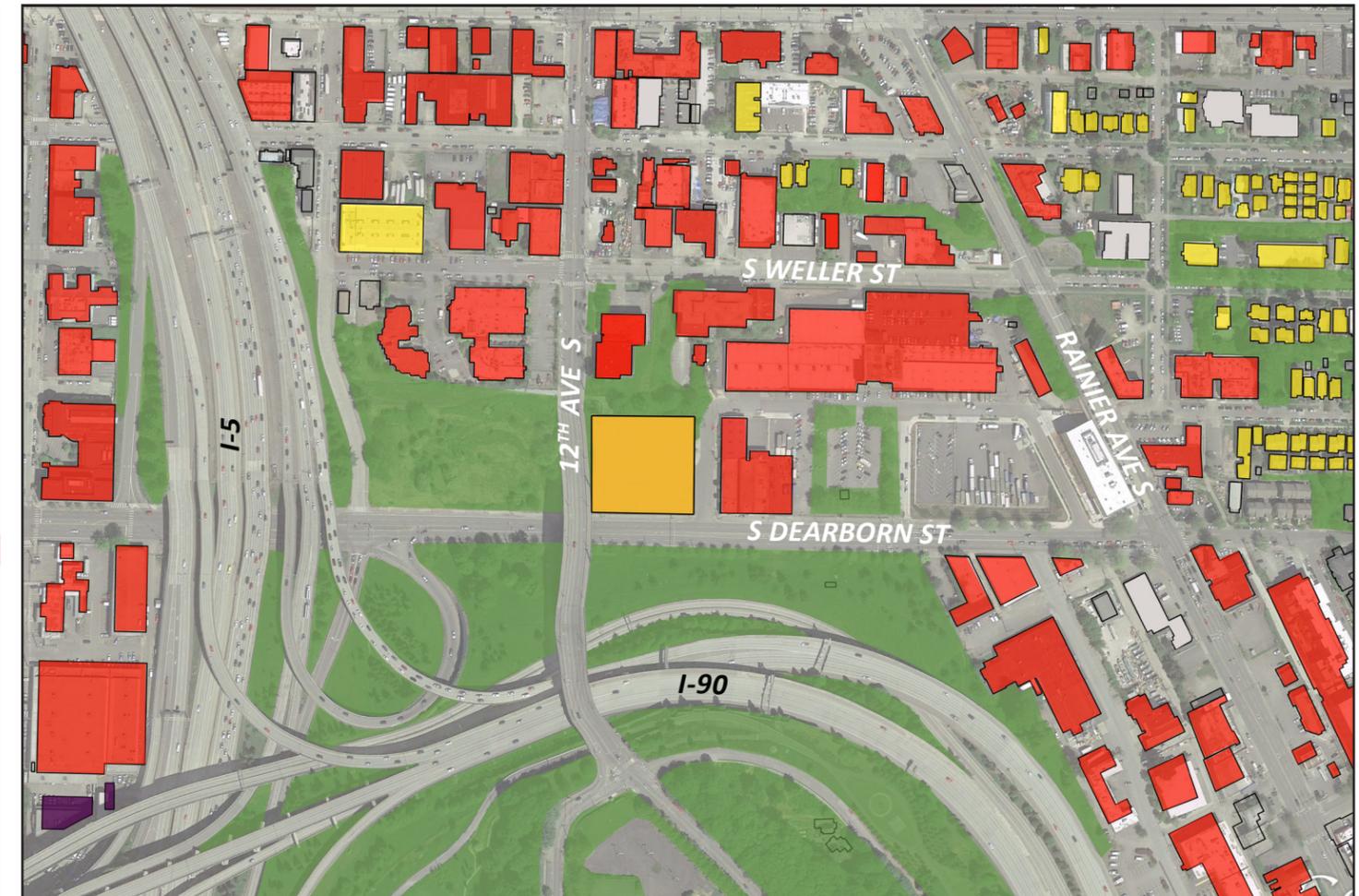
**Zoning**  
DMC 85/65-150

**Address**  
1200 S. Dearborn St.  
Seattle, WA

**Overlay Zone/District**  
Chinatown International Urban Village Center

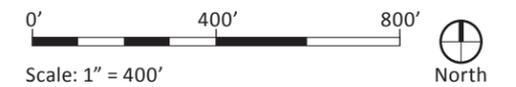
**Parcel ID #:**8170100520

## SURROUNDING USES AND STRUCTURES



### LEGEND

- Commercial
- Industrial
- Proposed Public Storage Building
- Vegetation/Greenbelts
- Civic
- Residential



# 4.0 Summary / Context / Analysis

## CONTEXT PHOTOS



KEY MAP

Not to Scale North



**A** GOODWILL JOB TRAINING AND EDUCATION CENTER



**B** COMMERCIAL BUILDING



**C** PEARL WARREN BUILDING



**D** KINGS ORIENTAL FOODS CO.



**E** GOODWILL CORPORATE HEADQUARTERS

# 4.0 Summary / Context / Analysis

## CONTEXT PHOTOS



KEY MAP

Not to Scale



F TEA GARDEN RESTAURANT



G WEST COAST PRINTING



H KUKURUZA GOURMET POPCORN / C-MARR AUTOMOTIVE



I PONTEDEIROS CONDOMINIUMS



J SEATTLE CAPOEIRA CENTER / HIAWATHA LOFTS

# 4.0 Summary / Context / Analysis

## AXONOMETRIC SITE PHOTO

### DESIGN CUES NOTES

Several design cues can be gleaned from the site's immediate context:

The adjacent 12th Avenue South Bridge is approximately 85 feet above South Dearborn Street. The mass of the bridge and scale of the bridge needs to be considered when designing the building to ensure the building isn't minimized and disappears into the background.

To maximize visibility opportunities, the main building facade is oriented toward the south facing South Dearborn Street and Interstate 90.

Newer development in the general vicinity of the project site include the Goodwill Corporate Headquarters, the Pontedeiros Condominiums, and the Seattle Capoeira Center. These developments have incorporated the use of contemporary materials such as pre-finished metal panels and refreshing color palettes of gray, red, blue, yellow and orange. Similar materials and colors have been incorporated into the building of the proposed development.

To take advantage of the project site's steep slopes, the proposed building has been stepped and recessed into the hillside.

Many of the buildings in the general vicinity are a mix of random size, scale and architecture that appear to have been developed over a period of many years. As an example, the building located directly east of the project site appears to have experienced a number of remodels resulting in a variety of styles, shapes and materials that have no relation to one another. The proposed building presents a cohesive design, similar to newer development in the area.

Newer buildings, such as the Goodwill Corporate Headquarters, are multi-story and consist of rectangular massings. The proposed building is a multi-story building similar in scale and massing.



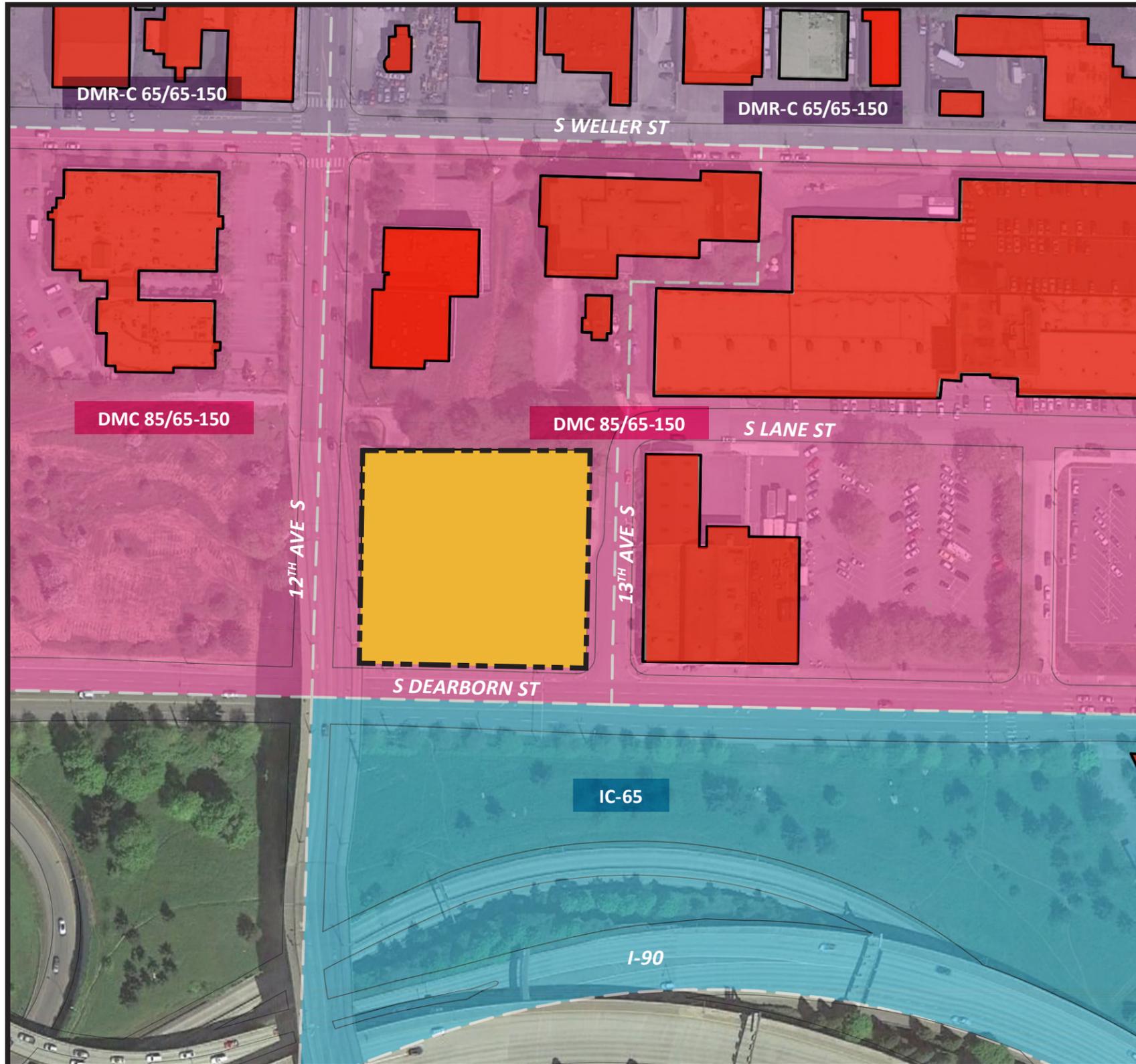
### CONTEXT IMAGE LEGEND

 Site Boundary

# 5.0 - Existing Site Conditions

ZONING / USES / STRUCTURES

## SITE CONDITIONS MAP



### KEY ZONING AND LAND USE ISSUES

#### SITE DATA:

**Area:**  
58,262 sf / 1.34 AC

**Address**  
1200 S. Dearborn St.  
Seattle, WA

**Parcel ID #:**8170100520

**Zoning**  
DMC 85/65-150

**Overlay Zone/District**  
Chinatown International Urban  
Village Center

#### LEGEND - ZONING

- IC - Industrial Commerical
- DMC - Downtown Mixed Commercial
- DMR/C - Downtown Mixed Residential / Commercial
- Proposed Public Storage Building

#### LEGEND - BUILDING USE

- Commercial
- Vegetation/Greenbelts



# 6.0 - Zoning Data

Land Use Code Section	Description	Achieved	Comments or Justification
23.49.044 23.49.046	All uses except those specifically prohibited by Section 23.49.044, those permitted only as conditional uses by Section 23.49.046.	Yes	Self-storage is an allowed in the DMC 85-65/150 zone. This criteria has been met.
SMC 23.49.008	A height limit of 85 feet applies to the portions of a structure that contain nonresidential or live work uses.	Yes	The height of the building has been design at 75'-5". This criteria has been met.
SMC 23.49.009	Street level uses are required on all lots abutting streets designated on Map 1G.	Yes	Per Map 1G in the Downtown zoning, street-level uses are not required along South Dearborn Street and 13th Ave South.
SMC 23.49.011	<b>Floor Area Ratios:</b> The FAR for the DMC 85/65-150 ranges from three (3) to five (5).	Yes	The proposed building will exceed the base FAR of 174,786 square feet (3.0) by approximately 4,937 square feet. The excess FAR of 4,937 square feet will be purchased through the provisions of SMC 23.49.011.A.1.h.
SMC 23.49.056	<b>Building Setbacks:</b> If the structure is greater than 15 feet in height, the setback limits apply to the facade between an elevation of 15 feet above the sidewalk grade and the minimum facade height established in subsection 23.49.056.A.	Yes	The minimum façade height for a Class II Pedestrian Street in the DMC zone is 15 feet. The façade height along South Dearborn Street are greater than the minimum required. As allowed by SMC 23.49.056B, no setback limits apply to a facade up to 15 feet above the sidewalk. The façade height along South Dearborn Street is setback approximately 2 feet from the property line. The façade above 15 feet is within the 2 foot setback with the exception of the tower-like element on the southeast corner of the building which is setback 2.8 feet from the property line from the sidewalk grade to the top of the building. Additionally, the stair tower located at the southwest corner of the building is setback the maximum allowable of 10 feet from the property line.

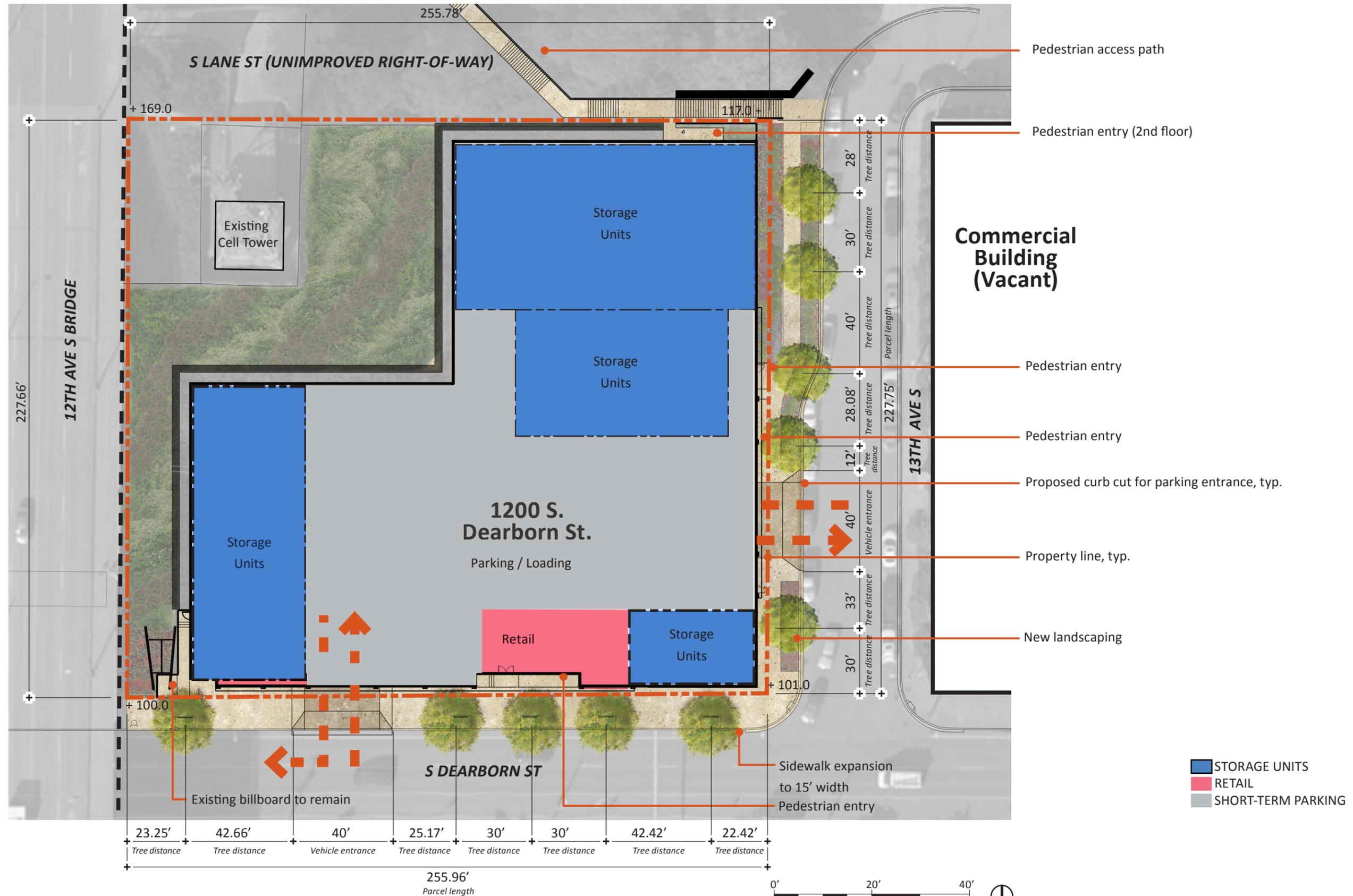
Land Use Code Section	Description	Achieved	Comments or Justification
			<p>The total façade area between 15 feet and 35 feet is 4,513. The portion of the building that is setback more than 2 feet from the property line is 1,180 or 26% of the total façade area.</p> <p>The area on the southeast corner along South Dearborn Street is approximately 50 feet in length, 30 feet longer than the allowed 20 feet. <b>A departure will be requested.</b></p> <p>13th Ave South does not require property line facades, according to Map 1H. The street-facing façade of up to 15 feet from the sidewalk is setback approximately 2 feet. The portion of the façade located at the intersection of South Dearborn Street and 13th Ave South is setback 2.8 feet from property line. Please refer to Sheet G1.02.</p> <p>The total façade area between 15 feet and 35 feet is 4,340 square feet. The portion of the building that is setback more than 2 feet from the property line is 1,215 or 28% of the total façade area.</p> <p>The area on the southeast corner along 13th Ave South is approximately 50 feet in length, 30 feet longer than the allowed 20 feet. <b>A departure will be requested.</b></p> <p>The portion of 12th Ave South along the west property line is the elevated portion of the bridge deck. Due to the bridge structure, high tension power lines, and the existing cell tower, the setback limitations cannot be met. SDOT has requested the project provide a minimum of 25 feet from of separation between the proposed structure and the bridge. <b>A departure from SMC 23.49.056 will be requested.</b></p>
SMC 23.49.019	<b>Parking Requirements:</b> No parking, either long-term or short-term is required for uses on lots in Downtown zones, except in the International District Mixed zones.	Yes	No minimum number of parking spaces are required. The project has been designed to include 19 short-term parking spaces within the street-level floor of the proposed building. This criteria has been met.

Land Use Code Section	Description	Achieved	Comments or Justification
SMC 23.49.019.E	<b>Bicycle Requirements:</b> Bicycle parking is required as follows: Office: 1:5,000 sf of GFA of office use; Retail over 10,000 sf: 1:5,000 sf of GFA of retail use.	Yes	The retail use within the proposed building is approximately 1172 sf. The retail space is below the 10,000 sf threshold, therefore bicycle parking is not required. This criteria has been met.
SMC 23.49.022	<b>Sidewalk Width Requirements:</b> Minimum sidewalk widths are established for certain streets by Map 1C. Map 1C indicates a 15' wide sidewalk along Dearborn and 13th.	Yes	The existing sidewalk along the South Dearborn Street frontage of the project site is currently 12' in width and may be increased to 15' as part of the construction. The sidewalk along the 13th Ave South frontage of the site does not currently extend to the north property line. A sidewalk extension on 13th Ave South will be included as part of the construction. This criteria has been met.
SMC 23.49.056	<b>Facade Transparency:</b> Class II pedestrian streets (Dearborn and 13th): A minimum of 30% of the street level street-facing facade shall be transparent		SMC 23.49.056C requires facade transparency of a minimum of 30 percent of the street-facing facades along Class II pedestrian streets. This code provision applies to the South Dearborn Street, 13th Ave South and 12th Ave South facades. The area between 2 and 8 feet of the South Dearborn Street facade is 1,356 square feet requiring a minimum of 407 square feet area of transparency at a rate of 30 percent. A total of 904 square feet of transparency has been provided. The area between 2 and 8 feet of the 13th Ave South facade is 1,338, requiring a minimum of 401 square feet. A total of 481 square feet of transparency has been provided. Please refer to Sheet G1.03.  The facade along 12th Ave South will be below the frontage and not visible to pedestrians and passersby using the roadway and transparency has not been provided. <b>A departure will be requested to the provisions of this code section.</b>  <b>The provisions of this code section are not applicable to the north side of the building due to the fact South Lane Street is not an improved street, therefore, no street frontage exists. A departure is not required.</b>
SMC 23.49.056	Blank Facade Limits - Class II Pedestrian Streets: Blank facade segments shall be no more than 30 feet wide, except for garage doors, which may exceed 30 feet. Any blank segments of a facade shall be separated by transparent areas at least 2 feet wide. The total of all blank facade segments, including garage doors, shall not exceed 70 percent of the street facing facade.		SMC 23.49.056D.3 limits blank facades to no more than 30 feet wide, with the exception of garage doors. The facade along South Dearborn Street between 2 and 8 feet has been broken up in 17 foot intervals, on average, and include transparency in the form of windows or display areas.

Land Use Code Section	Description	Achieved	Comments or Justification
			The facade along 13th Ave South between 2 and 8 feet has been broken up in 15'-5" foot intervals, on average and include transparency in the form of windows and display areas. However, due to the slope of the site along 13th Ave South, it is not possible to provide meaningful transparency along the north have of the facade. <b>A departure will be requested to the provisions of this code section.</b>  The facade along 12th Ave South will be below the frontage and not visible to pedestrians and passersby using the roadway. <b>A departure will be requested to the provisions of this code section.</b>  <b>The provisions of this code section are not applicable to the north side of the building due to the fact South Lane Street is not an improved street, therefore, no street frontage exists. A departure is not required.</b>

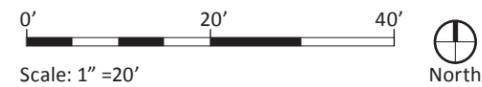
# 7.0 - Composite Site Plan

DESIGN



- STORAGE UNITS
- RETAIL
- SHORT-TERM PARKING

COMPOSITE SITE PLAN



## CS1 NATURAL SYSTEMS AND SITE FEATURES

### A. TOPOGRAPHY

1. Land Form: Use the natural topography and/or other desirable land forms or features to inform the project design.

**Response:** The proposed building will be stepped in order to minimize construction in the steep slope and take advantage of the existing site topography. This criteria has been met.

2. Elevation Changes: Use the existing site topography when locating structures and open spaces on the site. Consider “stepping up or down” hillsides to accommodate significant changes in elevation.

**Response:** The proposed building will be stepped in order to minimize construction in the steep slope and take advantage of the existing site topography. This criteria has been met.

### B. WATER

1. Adding Interest with Project Drainage: Use project drainage systems as opportunities to add interest to the site through water-related design elements. Features such as trees, rain gardens, bioswales, green roofs, fountains of recycled water, and/or water art installations can create movement and sound, air cooling, focal points for pedestrians, and habitats which may already be required to manage on-site Stormwater and allow reuse of potable water for irrigation.

**Response:** Due to the steep slopes, opportunities for water-related design elements will be limited. The applicant will work with DPD staff to explore viable options for site drainage options.

## PL2 WALKABILITY

### A. ACCESSIBILITY

1. Access for All: Provide access for people of all abilities in a manner that is fully integrated into the project design. Design entries and other primary access points such that all visitors can be greeted and welcomed through the front door. Refrain from creating separate “back door” entrances for persons with mobility limitation.

**Response:** Entries into the building will be from South Dearborn Street and 13th Ave South. Pedestrian access will be designed to meet the needs of people of all abilities. This criteria has been met.

## PL3 STREET-LEVEL INTERACTION

### A. ENTRIES

1. Design Objectives: Design primary entries to be obvious, identifiable, and distinctive with clear lines of sight and lobbies visually connected to the street. Scale and detail them to function well for their anticipated use and also to fit with the building of which they are a part, differentiating residential and commercial entries with design features and amenities specific to each.

a. Office/Commercial Lobbies should be visually connected to the street through the primary entry and sized to accommodate the range and volume of foot traffic anticipated;

b. Retail Entries should include adequate space for several patrons to enter and exit simultaneously, preferably under cover from weather.

c. Common Entries to Multi-Story Residential Buildings need to provide privacy and security for residents but also be welcoming and identifiable to visitors. Design features emphasizing the entry as a semi-private space are recommended and may be accomplished through signage, low walls and/or landscaping, a recessed entry area, and other detailing that signals a break from the public sidewalk.

d. Individual Entries to Ground-Related Housing should be scaled and detailed appropriately to provide for a more intimate type of entry. The design contribute to a sense of identity,

opportunity for personalization, offer privacy, and emphasize personal safety and security for building occupants.

**Response:** The building has been designed with the main pedestrian entrance on South Dearborn Street and vehicle access on both South Dearborn Street and 13th Ave South. The pedestrian access will be designed to include a storefront door and windows and canopy for weather protection. In addition, this area of the building façade will be recessed, differentiating it from the balance of the façade along South Dearborn Street. Vehicle access will be on South Dearborn Street and 13th Ave South and will include large openings with overhead doors, awnings and directional signage. All entries to the building will be highly visible to approaching customers. This criteria has been met.

## DC1 PROJECT USES AND ACTIVITIES

### B. VEHICULAR ACCESS AND CIRCULATION

1. Access Location and Design: Choose locations for vehicular access, service uses, and delivery areas that minimize conflict between vehicles and non-motorists wherever possible. Emphasize use of the sidewalk for pedestrians, and create safe and attractive conditions for pedestrians, bicyclists, and drivers by:

a. Using existing alleys for access or, where alley access is not feasible, choosing a location for street access that is the least visually dominant and/or which offers opportunity for shared driveway use;

b. Where driveways and curb cuts are unavoidable, minimize the number and width as much as possible; and/or

c. Employing a multi-sensory approach to areas of potential vehicle pedestrian conflict such as garage exits/entrances. Design features may include contrasting or textured pavement, warning lights and sounds, and similar safety devices.

**Response:** Due to the characteristics of the site, the proposed development is limited to access from South Dearborn Street and 13th Ave South. Existing site access is off of 13th Ave South. The proposed development would include a new access on South Dearborn Street, the removal and replacement of the existing access on 13th Ave South. This criteria has been met.

## DC2 ARCHITECTURAL CONCEPT

### A. MASSING

1. Site Characteristics and Uses: Arrange the mass of the building taking into consideration the characteristics of the site and the proposed uses of the building and its open space. In addition, special situations such as very large sites, unusually shaped sites, or sites with varied topography may require particular attention to where and how building massing is arranged as they can accentuate mass and height.

**Response:** The existing topography of the project site slopes from north to south with the highest elevation (170) at the northwest corner and the lowest elevation (100) at the southeast corner along S. Dearborn St. An existing cell phone tower is located near the northwest corner of the site. The proposed building has been designed in an “L” shape to avoid conflicting with the cell phone tower. To take advantage of the site’s existing topography and reduce the amount of excavation the first two floors of the building have been stepped. This criteria has been met.

### B. ARCHITECTURAL AND FAÇADE COMPOSITION

1. Façade Composition: Design all building facades—including alleys and visible roofs—considering the composition and architectural expression of the building as a whole. Ensure that all facades are attractive and well proportioned through the placement and detailing of all elements, including bays, fenestration, and materials, and any patterns created by their arrangement. On sites that abut an alley, design the alley façade and its connection to the street carefully. At a minimum, consider wrapping the treatment of the street-facing façade around the alley corner of

# 8.0 - Itemized Response to EDG

CITY OF SEATTLE

the building.

Response: The building has been designed using a variety of materials, colors, shapes and forms that work together to break-up the overall mass of the building and create a façade that is attractive and enhances surrounding development.

The street-level facades along South Dearborn Street and 13th Ave South will include fenestrations, awnings, directional signage, and texture, color and material changes. These design techniques work together to create a human-scale and interesting streetscape for pedestrians and passersby. This criteria has been met.

2. Blank Walls: Avoid large blank walls along visible façades wherever possible. Where expanses of blank walls, retaining walls, or garage facades are unavoidable, include uses or design treatments at the street level that have human scale and are designed for pedestrians. These may include:
- Newsstands, ticket booths and flower shops (even if small or narrow);
  - Green walls, landscaped areas or raised planters;
  - Wall setbacks or other indentations;
  - Display windows; trellises or other secondary elements;
  - Art as appropriate to area zoning and uses; and/or
  - Terraces and landscaping where retaining walls above eye level are unavoidable.

Response: The street-level facades of South Dearborn Street and 13th Ave South have been designed to include textures, materials and colors that break-up the façade and minimize blank walls. Additionally, these facades will include storefront windows, display windows, and doors to further articulate the walls at the street-level. Street trees will be added to the sidewalks along South Dearborn Street and 13th Ave South and a landscape strip will be included along portions of 13th Ave South. All of these design techniques work together to break-up the overall mass of the building and minimize blank walls at the street-level. This criteria has been met.

## C. SECONDARY ARCHITECTURAL FEATURES

1. Fit With Neighboring Buildings: Use design elements to achieve a successful fit between a building and its neighbors, such as:
- Considering aspects of neighboring buildings through architectural style, roof line, datum line detailing, fenestration, color or materials,
  - Using trees and landscaping to enhance the building design and fit with the surrounding context, and/or
  - Creating a well-proportioned base, middle and top to the building in locations where this might be appropriate. Consider how surrounding buildings have addressed base, middle, and top, and whether those solutions—or similar ones—might be a good fit for the project and its context.

Response: Many of the buildings in the area of the project site have been in existence for many years and are mainly one and two story. In recent years the area has experienced some redevelopment with the addition of the projects such as the Goodwill Headquarters, The Pontedeiros condominiums and the Seattle Capoeira Center. These new buildings are multi-story and include a mix of materials, textures, colors, shapes and forms similar to those proposed at the project site. The design of the proposed building will contribute to the redevelopment of this area. This criteria has been met.

## D. SCALE AND TEXTURE

1. Human Scale: Incorporate architectural features, elements, and details that are of human scale into the building facades, entries, retaining walls, courtyards, and exterior spaces in a manner that is consistent with the overall architectural concept. Pay special attention to the first three floors of the building in order to maximize opportunities to engage the pedestrian and enable an active and vibrant street front.

Response: The design of the building proposes features, elements and details along the street level that will enhance the experience of approaching pedestrians. The use of colors, materials, textures, landscaping and design details will create an exterior that is interesting and vibrant to those using the site as well as those walking or passing by. This criteria has been met.

2. Texture: Design the character of the building, as expressed in the form, scale, and materials, to strive for a fine-grained scale, or “texture,” particularly at the street level and other areas where pedestrians predominate.

Response: The character of the building will be expressed through the use of materials, colors, textures, and design elements. These characteristics will create a building that is bright, vibrant and attractive from a distance and the street-level. This criteria has been met.

## DC4 EXTERIOR ELEMENTS AND FINISHES

### A. BUILDING MATERIALS

1. Exterior Finish Materials: Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.

Response: The proposed building will include durable, high quality materials such as Concrete Masonry Units (CMU), pre-finished metal panels, and aluminum storefront windows and doors. In addition to the durability and quality, these materials will create a palette of textures and patterns that contribute to the overall design of the building. This criteria has been met.

EDG – Board Comment Responses  
DPD #3017092  
FIRST EDG MEETING: JUNE 10, 2014  
SECOND EDG MEETING: JULY 22, 2014

## **CS1 Natural Systems and Site Features: Use natural systems/features of the site and its surroundings as a starting point for project design.**

### **CS1-C TOPOGRAPHY**

CS1-C-2. Elevation Changes: Use the existing site topography when locating structures and open spaces on the site.

CS1-D Plants and Habitat

CS1-D-1. On-Site Features: Incorporate on-site natural habitats and landscape elements into project design and connect those features to existing networks of open spaces and natural habitats wherever possible. Consider relocating significant trees and vegetation if retention is not feasible.

**June 10, 2014: The Board requested additional information regarding the intention of treatment of the open space on site. The Board identified this area as an opportunity to incorporate on-site landscaping elements.**

*BCRA Response: The open space on the north and west portion of the project site will be landscaped with native restoration plantings such as those shown on page 32 of the updated EDG package. The open space will be used as part of the storm water management system.*

**July 22, 2014: The Board supported the conceptual landscape plan, and suggested exploration of adding a green roof element. Further refinement and additions of landscaping along South Dearborn Street was recommended. The Board requested a landscape plan be presented at the Recommendation Meeting.**

*BCRA Response: The concept of a green roof has been explored and was determined to not be a feasible option for this facility. The landscaping along the street frontages of South Dearborn Street and 13th Ave South has been thoughtfully designed with the pedestrian and the area in mind. The proposed landscape plan will enhance the pedestrian environment and improve the streetscape for passersby. A landscape plan has been included in this packet and can be found on page 23.*

## **CS2 Urban Pattern and Form: Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces in the surrounding area.**

### **CS2-A LOCATION IN THE CITY AND NEIGHBORHOOD**

CS2-A-1. Sense of Place: Emphasize attributes that give a distinctive sense of place.

Design the building and open spaces to enhance areas where a strong identity already exists, and create a sense of place where the physical context is less established.

CS2-A-2. Architectural Presence: Evaluate the degree of visibility or architectural presence that is appropriate or desired given the context, and design accordingly.

### **CS2-B ADJACENT SITES, STREETS, AND OPEN SPACES**

CS2-B-1. Site Characteristics: Allow characteristics of sites to inform the design, especially where the street grid and topography create unusually shaped lots that can add distinction to the building massing.

CS2-B-2. Connection to the Street: Identify opportunities for the project to make a strong connection to the street and public realm.

### **CS2-C RELATIONSHIP TO THE BLOCK**

CS2-C-3. Full Block Sites: Break up long facades of full-block buildings to avoid a monolithic presence. Provide detail and human scale at street-level, and include repeating elements to add variety and rhythm to the façade and overall building design.

**June 10, 2014: The Board recommended the use of material, modulation, and/or setbacks to break up the long façade along South Dearborn Street and better respond to the public realm.**

*BCRA Response: The massing options have been designed to include a variety of vertical and horizontal modulations that work together to break-up the overall mass of the building in conjunction with the use of cladding materials to be presented as part of the MUP submittal. Storefront windows and doors, display windows, opening and canopies have been thoughtfully placed to enhance the street-level experience for employees, pedestrians and passersby. All of these elements work together to create a cohesive design that will enhance this area and make it compatible with new and proposed developments in this area.*

**July 22, 2014: The Board supported the simple forms of preferred Concept E and the vertical relief and modulations of Concept D. In summary, the Board supported Concept E, and recommended adding vertical elements along South Dearborn Street and 13th Ave South. These vertical elements and modulations were encouraged to include glazing. The Board also requested all right-of-way improvement dimensions be included in the Recommendation Meeting packet.**

*BCRA Response: The design of Concept E has been further refined to include a vertical element in the form of a material/color change at the vehicle entrances on both South Dearborn Street and 13th Ave South. Awnings at the corner of the building and at the vehicle access point are larger and angled upward to further articulate the facades and provide some additional character to the design. These elements work together to create a break in the façade, highlight the location of the vehicle entrances and alert pedestrians due to the change in plane.*

The right-of-ways for both South Dearborn Street and 13th Ave South are shown on page 23.

### **CS2-D HEIGHT, BULK, AND SCALE**

CS2-D-1. Existing Development and Zoning: Review the height, bulk, and scale of neighboring buildings as well as the scale of development anticipated by zoning for the area to determine an appropriate complement and/or transition.

CS2-D-2. Existing Site Features: Use changes in topography, site shape, and vegetation or structures to help make a successful fit with adjacent properties.

**June 10, 2014: The Board noted that the adjacent 12th Avenue South Bridge to the west provides a prominent presence in the area, and recommended the project further respond to the height, bulk, and scale of adjacent development. The Board requested to see section drawing illustrating the relationship of the building to the 12th Avenue South Bridge at the Second Early Design Guidance Meeting.**

*BCRA Response: The proposed building will be approximately 10 feet below the 12th Ave. Bridge deck and is set back approximately 25 feet from the bridge structure, as shown in the site section on page 36*

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of the updated EDG package. As shown in the views looking east and west on South Dearborn Street on pages 26, 28, and 30 of the updated EDG package, the proposed building will not impact the view of the 12th Ave South Bridge. The height, bulk and scale of the proposed building is similar and complementary to the newer developments in the area such as the Goodwill Building, the Hiawatha Condominiums and Pontedera Apartments and the future development proposed on the adjacent Goodwill site.

**July 22, 2014: The Board reviewed the context analysis of the bridge and structure (page 31), and agreed that the size of the structure in keeping with the neighborhood context of existing structures and projected future development to the west. The Board requested that the following be presented at the Recommendation Meeting: a north-south drawing of the west wing of the structure, including the adjacent communication utility tower.**

BCRA Response: A building section through the west wing showing the existing communication tower is shown on page 36.

**CS3 Architectural Context and Character: Contribute to the architectural character of the neighborhood.**

## CS3-A EMPHASIZING POSITIVE NEIGHBORHOOD ATTRIBUTES

CS3-A-4 Evolving Neighborhoods: In neighborhoods where architectural character is evolving or otherwise in transition, explore ways for new development to establish a positive and desirable context for others to build upon in the future.

**June 10, 2014: The Board noted the evolving nature of the neighborhood, and felt the architectural relationship between the 12th Avenue South Bridge to the west and the Goodwill building to the east and the proposed building should be explored further.**

BCRA Response: The design of the proposed building incorporates simple forms and building materials to those on the Goodwill Job Training and Education Center, as well as other large scale developments in the area. The west building massing and modulation is intended to provide a clean and composed backdrop to celebrate the 12th Ave. South bridge so as not to compete with the articulation of the bridge structure. According to the MUP Design Review package for the Goodwill Job Training and Education Center (DPD 30112125, Goodwill has indicated future plans of mixed-use developments along the frontage of South Dearborn Street from 13th Ave South to the west side of their Job Training and Education Center at the corner of South Dearborn Street and Rainier Ave South. The scale proposed Public Storage building is compatible with both the existing Goodwill building and the future plans for the remaining block.

**July 22, 2014: The Board discussed the evolving nature of the neighborhood, and agreed that potential future development to the west suggests structures and uses similar to preferred Concept E. The Board encouraged development of a design that is compatible with projected future development to the west.**

BCRA Response: Concept E has been designed to include forms, materials and colors similar to those found on newer developments in the area including the Goodwill Job Training and Education Center and the Pontedeiros Condominiums and Hiawatha Lofts. The proposed Public Storage facility will further enhance the design of future development and compliments redevelopment already occurring in this area.

**PL2 Walkability: Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features.**

otherwise in transition, explore ways for new development to establish a positive and desirable context for others to build upon in the future.

## PL2-A ACCESSIBILITY

PL2-A-1. Access for All: Provide access for people of all abilities in a manner that is fully integrated into the project design. Design entries and other primary access points such that all visitors can be greeted and welcomed through the front door.

## PL2-B SAFETY AND SECURITY

PL2-B-1. Eyes on the Street: Create a safe environment by providing lines of sight and encouraging natural surveillance.

**June 10, 2014: The Board agreed that ensuring eyes on the street will create a safe environment and natural surveillance. To achieve this, the Board recommended façade transparency on the ground level at the retail space.**

BCRA Response: Transparent storefront windows and doors are proposed at the retail space and will provide views onto the street and sidewalk. Additionally, the apartment located on 13th Ave. South will be occupied and the windows proposed at this location will provide views onto the street and sidewalk. The retail and residential spaces are located to optimize visibility to the street frontage on S. Dearborn St. and 13th Ave. S.

**July 22, 2014: The Board supported glazing at the ground level facade with the retail at the center along South Dearborn Street. The Board expects to see a lighting plan at the Recommendation Meeting.**

BCRA Response: A lighting plan has been included in this package and can be found on page 33.

## PL2-C WEATHER PROTECTION

PL2-C-1. Locations and Coverage: Overhead weather protection is encouraged and should be located at or near uses that generate pedestrian activity such as entries, retail uses, and transit stops.

**June 10, 2014: The Board recommended the addition of weather protection at ground level where possible.**

**July 22, 2014: The Board supported the concept of weather protection along South Dearborn Street and 13th Ave South. The Board requested the presentation of exterior materials at the Recommendation Meeting.**

BCRA Response: Colors and materials are shown on page 26 and a material/color board will be presented at the Recommendation Meeting.

## PL2-D WAYFINDING

PL2-D-1. Design as Wayfinding: Use design features as a means of wayfinding wherever possible.

**July 22, 2014: The Board discussed the importance of signage and wayfinding, and requested the presentation of a signage plan at the Recommendation Meeting.**

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BCRA Response: A signage plan has been included and can be found on page 34.

## **PL3 Street-Level Interaction: Encourage human interaction and activity at the street-level with clear connections to building entries and edges.**

### **PL3-A ENTRIES**

PL3-A-1. Design Objectives: Design primary entries to be obvious, identifiable, and distinctive with clear lines of sight and lobbies visually connected to the street.

**June 10, 2014: The Board agreed that Concept B provides the optimal location for the retail pedestrian entry and two towers. The Board found that the entry and tower creates an identifiable and distinctive entry and architectural feature.**

BCRA Response: Each massing concept proposes a distinct feature at the corner of the building on South Dearborn Street and 13th Ave South. The features will include transparency at the pedestrian level allowing transparency into either active spaces or display areas, overhead weather protection for pedestrians walking by or entering the site, a mix of materials and glazing to the roof deck. All of these features work together to create an identifiable and distinct element that engages the street corner, customers, employees and pedestrians using the site.

**July 22, 2014: The Board supported the location of the retail along South Dearborn Street and the vertical glazing element at the corner. Glazing along the street level facade is also encouraged. The Board recommended consideration of supplementing the South Dearborn Street facade with additional vertical elements such as glazing.**

BCRA Response: In addition to the vertical glazing element at the corner of South Dearborn Street and 13th Ave South, transparent windows have been added along the entire pedestrian level facade of South Dearborn Street. Transparent glazing has been added to the facade along 13th Ave South, however, these areas are limited due to the sloping of the site. All of the transparent glazing allows views in to either active spaces or display areas. The roll-up doors at the vehicle access points will be a grille and will allow views into the parking area of the building.

### **PL3-C RETAIL EDGES**

PL3-C-2. Visibility: Maximize visibility into the building interior and merchandise displays. Consider fully operational glazed wall-sized doors that can be completely opened to the street, increased height in lobbies, and/or special lighting for displays.

**June 10, 2014: The Board discussed placement of the retail component, and recommended the design maximize visibility into the building interior to enhance street-level interaction.**

BCRA Response: The pedestrian level facades of the building along South Dearborn Street and 13th Ave South will incorporate transparent storefront windows and doors that will provide visibility into active spaces or display areas.

**July 22, 2014: The Board discussed the street level transparency, location of the retail along South Dearborn Street, and the active display element at the corner. Active display was used to describe the vertical corner element: the glazing and transparency will allow a visual**

**connection to the interior of the space that will communicate forms and uses. This display area will be lit at night. The Board supported these elements, and recommended consideration of adding more active display elements along South Dearborn Street and/or 13th Avenue South. The Board requested floor plans to illustrate the uses and forms that will be translated at the exterior.**

BCRA Response: Display areas ranging in depth between 3' and 5' have been added along the South Dearborn Street facade. Floor plans have been included and can be found on pages 20 - 22.

## **DC1 Project Uses and Activities: Optimize the arrangement of uses and activities on site.**

### **DC1-A ARRANGEMENT OF INTERIOR USES**

DC1-A-1. Visibility: Locate uses and services frequently used by the public in visible or prominent areas, such as at entries or along the street front.

**June 10, 2014: The Board agreed that Concept B proposes the ideal location for the retail and pedestrian entry, and recommended it be visible and identifiable.**

BCRA Response: The retail space is located mid-block and will include wayfinding signage to highlight the buildings entrance on South Dearborn Street. The mid-block location provides an activation point along the long facade of S Dearborn St. and supports the internal operations of the building. Massing Concepts C and D include vertical glazing elements that further emphasize this entrance.

**July 22, 2014: The Board agreed that the location of the retail proposed in Concept E is ideal, and supported the transparency and glazing along the street front.**

BCRA Response: The retail portion of the Public Storage facility has been located along South Dearborn Street and has been enhanced with transparent glass and signage.

### **DC1-B VEHICULAR ACCESS AND CIRCULATION**

DC1-B-1. Access Location and Design: Choose locations for vehicular access, service uses, and delivery areas that minimize conflict between vehicles and non-motorists wherever possible. Emphasize use of the sidewalk for pedestrians, and create safe and attractive conditions for pedestrians, bicyclists, and drivers.

### **DC1-C PARKING AND SERVICE USES**

DC1-C-1. Below-Grade Parking: Locate parking below grade wherever possible. Where a surface parking lot is the only alternative, locate the parking in rear or side yards, or on lower or less visible portions of the site.

**June 10, 2014: The Board acknowledged that the programming of the structure compels the design to include of two vehicular access points, and recommended the incorporation of design elements to minimize conflict between vehicles and non-motorists.**

BCRA Response: The vehicle access points on both South Dearborn Street and 13th Ave South have been designed to include architectural elements that differentiate the access points. Elements such as textured pavement, signage, canopy locations, and building modulations will be used to minimize conflicts between vehicles and non-motorists.

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**July 22, 2014: The Board requested exterior materials be presented at the Recommendation Meeting.**

[BCRA Response: Colors and materials are shown on page 26 and a color/material board will be presented at the DRB Recommendation Meeting.](#)

**DC2 Architectural Concept: Develop an architectural concept that will result in a unified and functional design that fits well on the site and within its surroundings.**

## DC2-A MASSING

DC2-A-1. Site Characteristics and Uses: Arrange the mass of the building taking into consideration the characteristics of the site and the proposed uses of the building and its open space.

DC2-A-2. Reducing Perceived Mass: Use secondary architectural elements to reduce the perceived mass of larger projects.

**June 10, 2014: The Board requested additional massing study and options to better respond to the site characteristics and uses.**

[BCRA Response: The project site includes existing characteristics such as limited site access, grade changes of approximately 80 feet \(S. Dearborn St up to 12th Ave. S.\), and setbacks to the 12th Ave South Bridge and the existing cell phone tower. These characteristics along with city code requirements dictate the development options for the site. Based on these parameters, the proposed building footprint, location on the site and height of the building have been designed to create the best possible solution that allows the developer to optimize the utility of the site while minimizing construction into the steep slopes.](#)

**July 22, 2014: The Board discussed the use of modulation and projections to contribute to reduction of the perceived mass of the building. The Board liked the projections and relief proposed in Concept D, and recommended a similar consideration be applied to Concept E such that the corner element is further articulated. Suggestions offered include further projecting the horizontal floating elements, adding vertical elements within the floating element, and/or using accentuated awning forms at the corner.**

[BCRA Response: The design of Concept E has been further developed to include a vertical material/color change at the vehicle access. This change in material and color does a good job of breaking the plane of the metal insulated panels along the front of the building. The material change at this location also creates a defined edge and some modulation, as the materials are at different depths.](#)

[The canopies along the South Dearborn Street and 13th Ave South have been designed to provide weather protection for pedestrians as well as assist with breaking up the overall mass of the building. Awnings at the corner of the building and at the vehicle access point are larger and angled upward to further articulate the facades and provide some additional character to the design.](#)

## DC2-B ARCHITECTURAL AND FACADE COMPOSITION

DC2-B-1. Façade Composition: Design all building facades—including alleys and visible roofs—considering the composition and architectural expression of the building as a whole. Ensure that all facades are attractive and well-proportioned.

**June 10, 2014: The Board requested additional study and development of the rooftop elements as the roof will be visible from the 12th Avenue Bridge to the west. The Board requested a schematic view of the proposed structure from the 12th Avenue South Bridge.**

**July 22, 2014: The Board appreciated the additional information supplied by the applicant, and discussed the importance of the treatment of the roof. The Board suggested screening or other treatment, and requested further refinement of the roof. The Board asked that a roof plan be presented at the Recommendation Meeting. Perspectives from the 12th Avenue South Bridge, and access to/from the roof shall also be presented.**

[BCRA Response: Roof top units will be screened from view with enclosures constructed with similar materials as those found elsewhere on the building. A roof plan has been included and can be found on page 22. Perspectives from the 12th Ave South Bridge have been included and can be found on pages 30 and 31. Access to and from the roof will be via a roof hatch. Because the roof is a non-occupied space and the roof does not include elevator equipment extending, a stair tower is not required.](#)

DC2-B-2. Blank Walls: Avoid large blank walls along visible façades wherever possible. Where expanses of blank walls, retaining walls, or garage facades are unavoidable, include uses or design treatments at the street level that have human scale and are designed for pedestrians.

**June 10, 2014: The Board recommended articulation and other design solutions to provide attractive facades and avoid large blank walls.**

[BCRA Response: The massing concepts have been designed to include a variety of planes, setbacks, textures, transparency, materials, glazing and canopies that work in concert to break-up the overall mass of the building. The design intent is to create attractive facades that will complement the fabric of the neighborhood and enhance experience for pedestrians, passersby and customers using the site.](#)

**July 22, 2014: The Board supported Concept E's simple shapes, transparent corner element, horizontal glazing at the top, and floating element between floors two and five. The Board recommended consideration of including additional vertical elements (similar to Concept D) within the floating portion of the facade to provide further relief of the perceived mass and blank walls.**

[BCRA Response: The design of concept E has been further developed to include a vertical material/color change at the vehicle access. This change in material and color does a good job of breaking the plane of the metal insulated panels along the front of the building. The material change at this location also creates a defined edge and some modulation, as the materials are at different depths.](#)

## DC2-C SECONDARY ARCHITECTURAL FEATURES

DC2-C-1. Visual Depth and Interest: Add depth to facades where appropriate by incorporating balconies, canopies, awnings, decks, or other secondary elements into the façade design. Add detailing at the street level in order to create interest for the pedestrian and encourage active street life and window shopping (in retail areas).

DC2-C-3. Fit With Neighboring Buildings: Use design elements to achieve a successful fit between a building and its neighbors.

## DC2-D SCALE AND TEXTURE

DC2-D-1. Human Scale: Incorporate architectural features, elements, and details that are of human scale into the building facades, entries, retaining walls, courtyards, and exterior spaces in a manner that is consistent with the overall architectural concept.

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**June 10, 2014: The Board recommended the use of materials and texture to enhance the pedestrian experience along South Dearborn Street. The Board requested additional information regarding the relationship between the structure and the pedestrian realm on 13th Avenue South.**

BCRA Response: The pedestrian level facades will include materials and textures along with transparent storefront doors and windows and canopies for weather protection. All of these elements will enhance the experience for pedestrians along both South Dearborn Street and 13th Ave South.

BCRA Response: The façade along 13th Ave South will include similar pedestrian level elements as those proposed on the South Dearborn Street. Additionally, the sidewalk along 13th Ave South will be extended to the north property line and will include street trees. The addition of the pedestrian elements will improve the pedestrian realm along 13th Ave South and provide an amenity that does not currently exist on the west side of the street from Rainier Ave S (east), to the International District (west).

**July 22, 2014: The Board discussed the treatment of the street level facade along South Dearborn Street and recommended the use of glazing, awnings, weather protection, and/or texture to enhance the pedestrian experience and create human scale. The Board requested exterior materials be presented at the Recommendation Meeting.**

BCRA Response: The canopies along the South Dearborn Street and 13th Ave South have been designed to provide weather protection for pedestrians as well as assist with breaking up the overall mass of the building. Awnings at the corner of the building and at the vehicle access point are larger and angled upward to further articulate the facades and provide some additional character to the design.

Transparent glass has been included along the entire South Dearborn Street façade and for a portion of the 13th Ave South façade. These areas will allow views into active spaces or display areas.

Along the pedestrian level of the building, integrally color split-face concrete masonry units (CMU) has been proposed. This material will be durable, provide texture and will work well with the transparent glazing and awnings to enhance the pedestrian experience.

## DC2-E FORM AND FUNCTION

DC2-E-1. Legibility and Flexibility: Strive for a balance between building use legibility and flexibility. Design buildings such that their primary functions and uses can be readily determined from the exterior, making the building easy to access and understand. At the same time, design flexibility into the building so that it may remain useful over time even as specific programmatic needs evolve.

**June 10, 2014: The Board recommended the use of exterior material changes, articulation, or other design components translate the interior uses (retail, storage, and residential) to the exterior.**

BCRA Response: The primary use of the building will be self-storage. The accessory uses contained within the building will be articulated with materials, textures, and transparency on the exterior to provide additional interest.

**July 22, 2014: The Board discussed the residential unit and agreed that its use is accessory, and its translation to the exterior is inconsequential. The active display described for the corner element was supported, and the Board recommended consideration of adding other opportunities for active display along South Dearborn Street and/or 13th Avenue South. The Board requested floor plans be presented at the Recommendation Meeting to illustrate the relationship between the interior and the active display elements at the exterior.**

BCRA Response: Display areas ranging in depth between 3' and 5' have been added along the South Dearborn Street façade. Floor plans have been included and can be found on pages 20 - 22.

## DC4 EXTERIOR ELEMENTS AND FINISHES

Use appropriate and high quality elements and finishes for the building and its open spaces.

### DC4-A EXTERIOR ELEMENTS AND FINISHES

DC4-A-1. Exterior Finish Materials: Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.

DC4-A-2. Climate Appropriateness: Select durable and attractive materials that will age well in Seattle's climate, taking special care to detail corners, edges, and transitions.

**June 10, 2014: The Board requested the use of durable materials that enhance the pedestrian experience along the street frontages.**

BCRA Response: The base of the building along both South Dearborn Street and 13th Ave South will be constructed of durable material such as masonry or concrete at a more human scale to further enhance the pedestrian experience.

**July 22, 2014: The Board supported the use of durable materials to enhance the pedestrian experience along the street frontages. The Board requested that exterior materials be presented at the Recommendation Meeting.**

BCRA Response: Integrally colored CMU has been proposed along the pedestrian level facades of both South Dearborn Street and 13th Ave South. Building materials are shown on pages 26 and 27 and a color/material board will be presented at the Recommendation Meeting.

### DC4-B SIGNAGE

DC4-B-1. Scale and Character: Add interest to the streetscape with exterior signs and attachments that are appropriate in scale and character to the project and its environs.

DC4-B-2. Coordination with Project Design: Develop a signage plan within the context of architectural and open space concepts, and coordinate the details with façade design, lighting, and other project features to complement the project as a whole, in addition to the surrounding context.

**The Board requested additional information regarding proposed signage, and made note of the scale of signage shown.**

BCRA Response: The building signage shown on the massing models is conceptual to illustrate general intent and integration with the building massing and articulation. The signage will be designed in conformance to the City Sign Code.

**July 22, 2014: The Board discussed the influence of signage, and requested that a signage plan be presented at the Recommendation Meeting.**

BCRA Response: A signage plan has been included and can be found on page 34.

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## DC4-C LIGHTING

DC4-C-1. Functions: Use lighting both to increase site safety in all locations used by pedestrians and to highlight architectural or landscape details and features such as entries, signs, canopies, plantings, and art.

DC4-C-2. Avoiding Glare: Design project lighting based upon the uses on and off site, taking care to provide illumination to serve building needs while avoiding off-site night glare and light pollution.

**June 10, 2014: The Board requested a lighting study be provided at the Recommendation Meeting.**

[BCRA Response: A lighting study will be provided at the DRB Recommendation Meeting.](#)

**July 22, 2014: The Board requested a lighting study be provided at the Recommendation Meeting.**

[BCRA Response: A lighting study has been provided. Please refer to the exterior lighting plan on page 33.](#)

## DC4-D TREES, LANDSCAPE, AND HARDSCAPE MATERIALS

DC4-D-1. Choice of Plant Materials: Reinforce the overall architectural and open space design concepts through the selection of landscape materials.

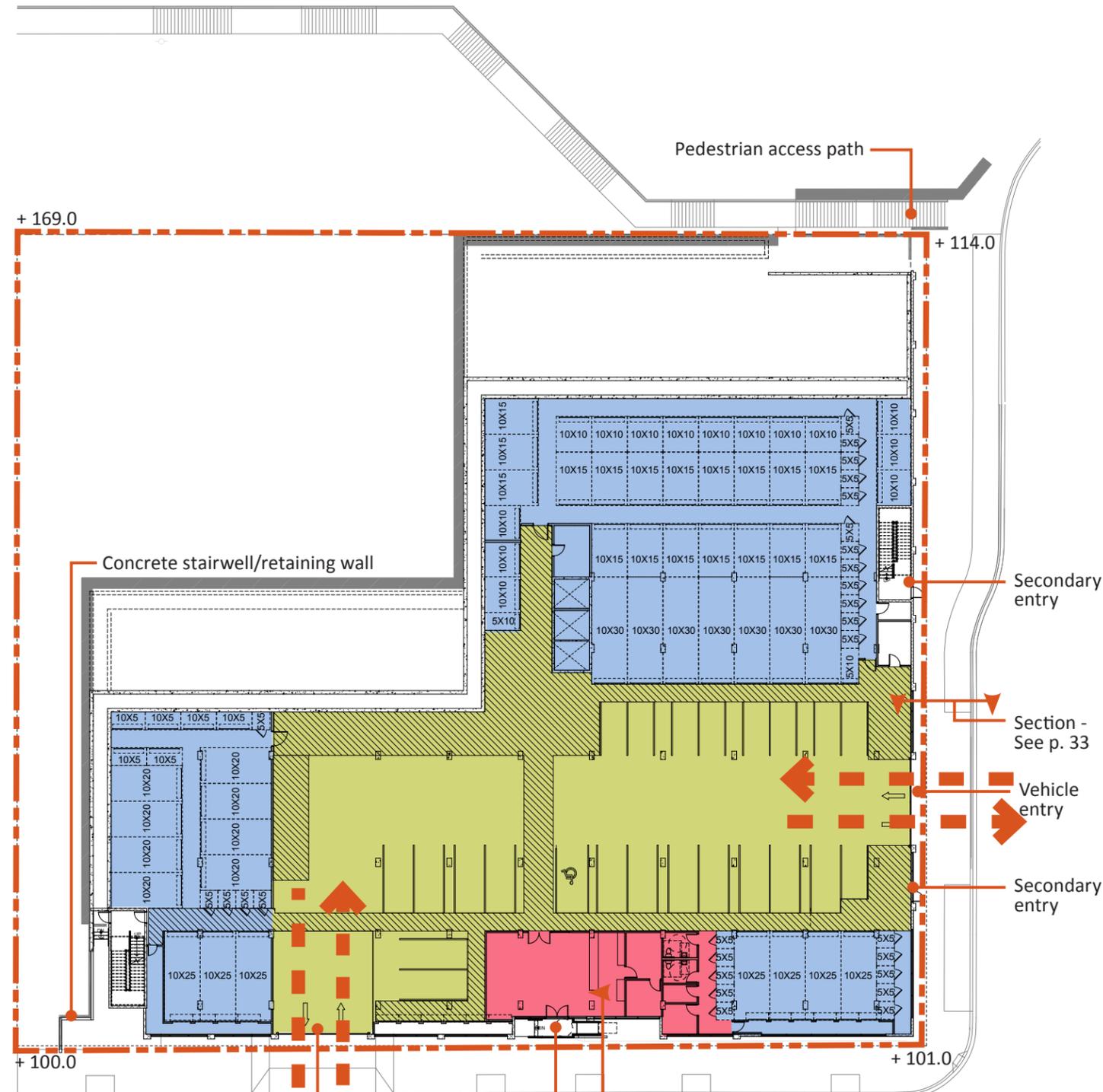
**June 10, 2014: The Board recommended the use of landscaping on-site to provide visual depth and interest which will enhance the open space. The Board asked for a conceptual landscape plan at the Second Early Design Guidance Meeting.**

[BCRA Response: The open space on the site will be landscaped with native plantings such as those shown on the Conceptual Landscape Palette on page 32 of the updated EDG package. Street trees will line the sidewalk along South Dearborn Street and 13th Ave South. The open space will be used as part of the storm water management system and as a visual buffer.](#)

**July 22, 2014: The Board discussed the conceptual landscape plan and street trees. The Board agreed that there may be opportunity along South Dearborn Street to include landscaping in addition to the street trees, and recommended further consideration and refinement. The Board requested a landscape plan be presented at the Recommendation Meeting. This landscape plan shall include all required right-of-way dimensions for South Dearborn Street and 13th Avenue South. Additionally, the Board discussed the importance of the treatment of the roof, and suggested consideration of a green roof element.**

[BCRA Response: The concept of a green roof has been explored and was determined to not be a feasible option for this facility. The landscaping along the street frontages of South Dearborn Street and 13th Ave South has been thoughtfully designed with the pedestrian and the area in mind. The proposed landscape plan will enhance the pedestrian environment and improve the streetscape for passersby. A landscape plan has been included in this packet and can be found on page 23.](#)

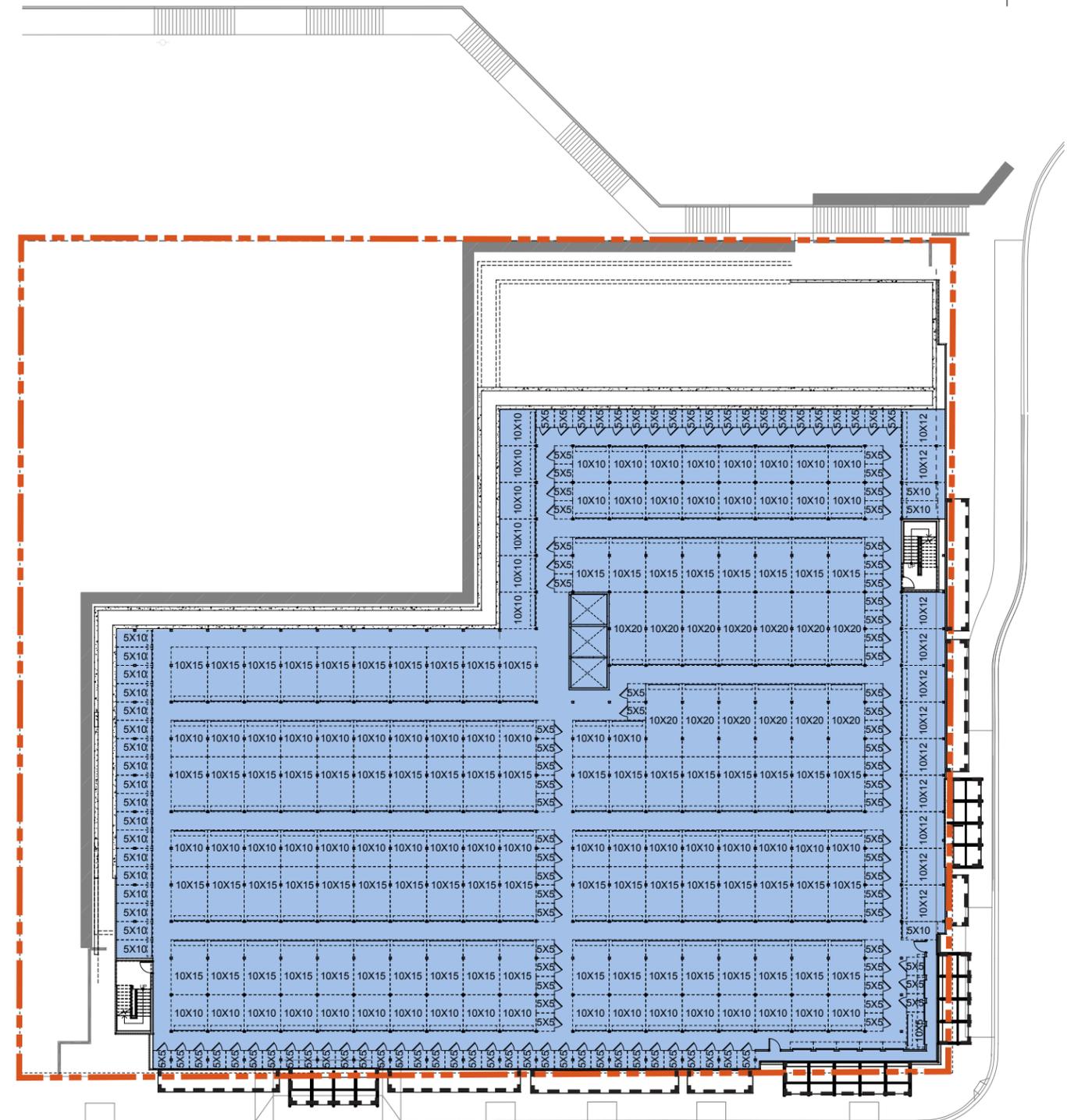
[The dimensions of the rights-of-ways of South Dearborn Street and 13th Ave South have been added to the landscape plan.](#)



FLOOR 1

**FLOOR PLAN LEGEND**

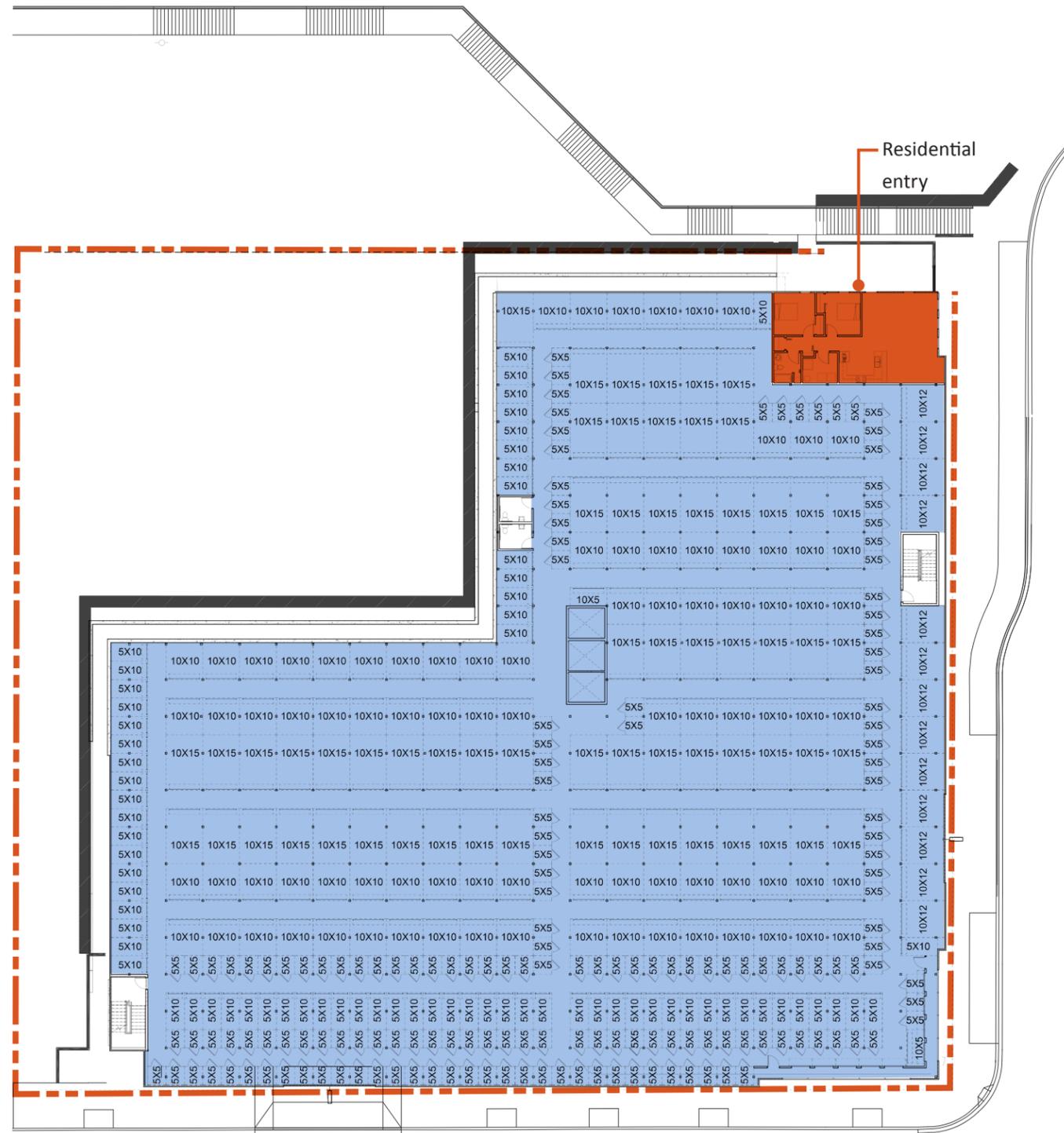
- Parking / Loading
  - Storage
  - Retail
- Vehicle entry
- Primary pedestrian entry (commercial)



FLOOR 2

**FLOOR PLAN LEGEND**

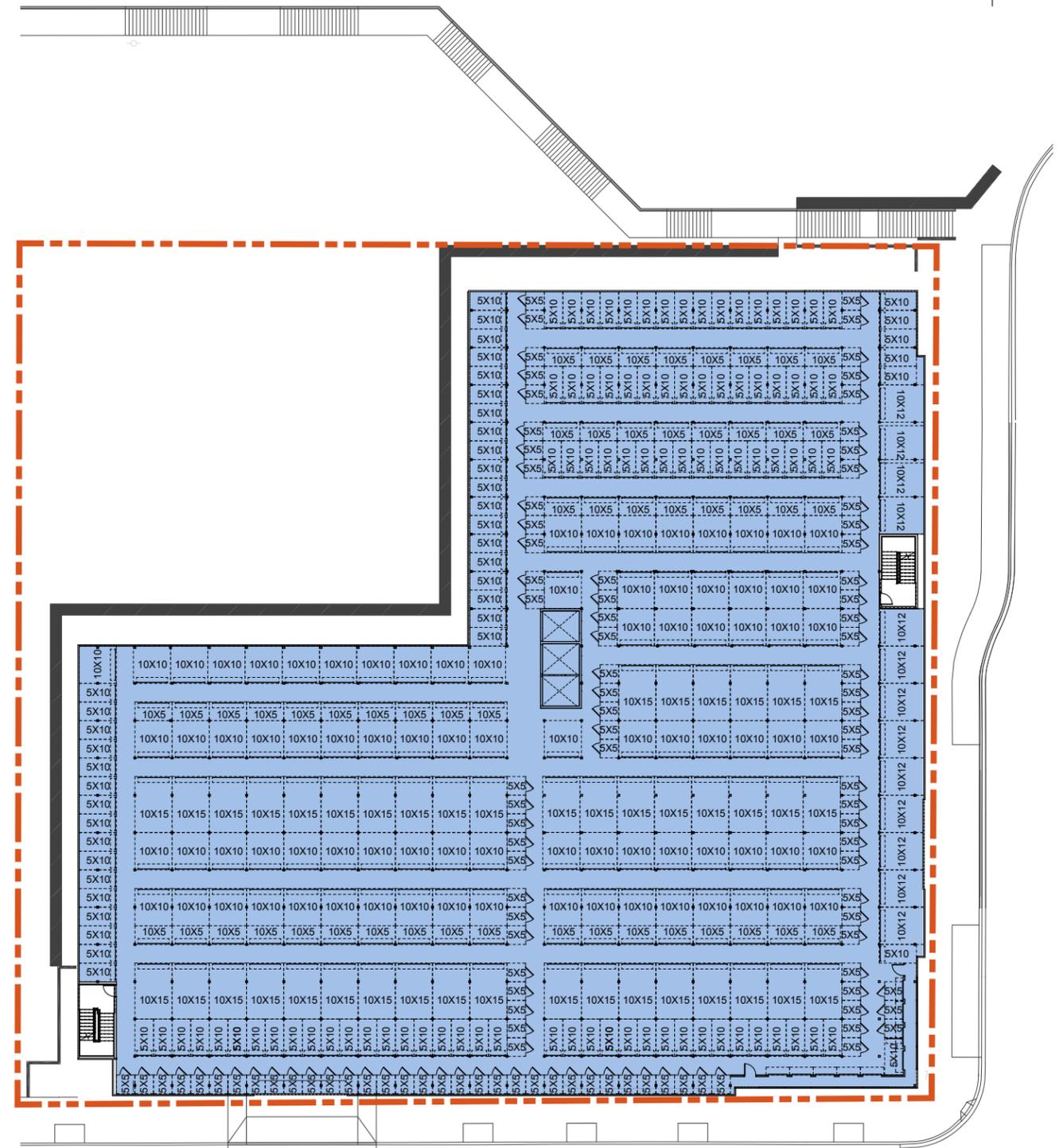
- Parking / Loading
- Storage
- Retail



FLOOR 3

**FLOOR PLAN LEGEND**

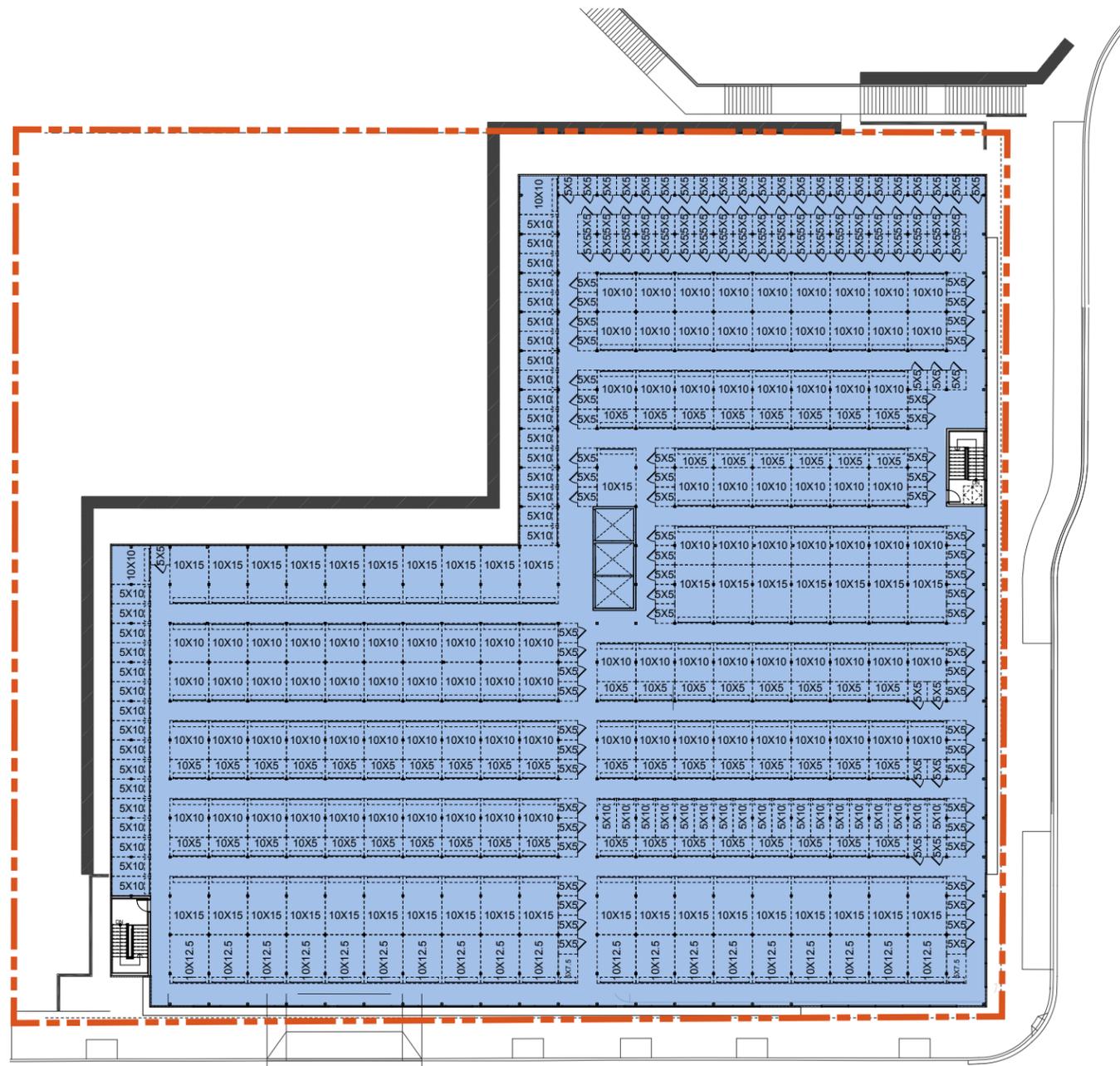
-  Parking / Loading
-  Storage
-  Retail
-  Residential



FLOOR 4-5

**FLOOR PLAN LEGEND**

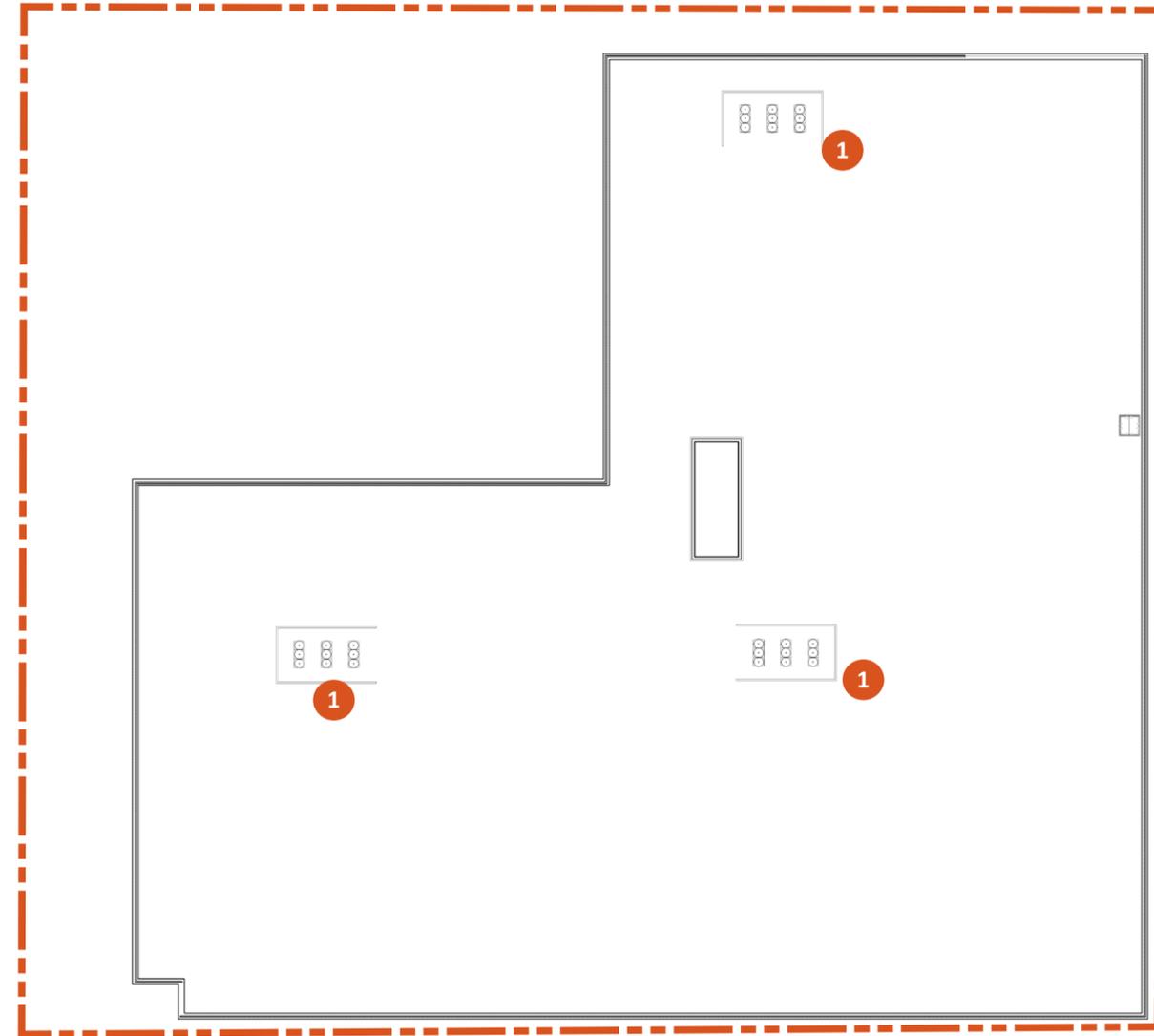
-  Parking / Loading
-  Storage
-  Retail



FLOOR 6

FLOOR PLAN LEGEND

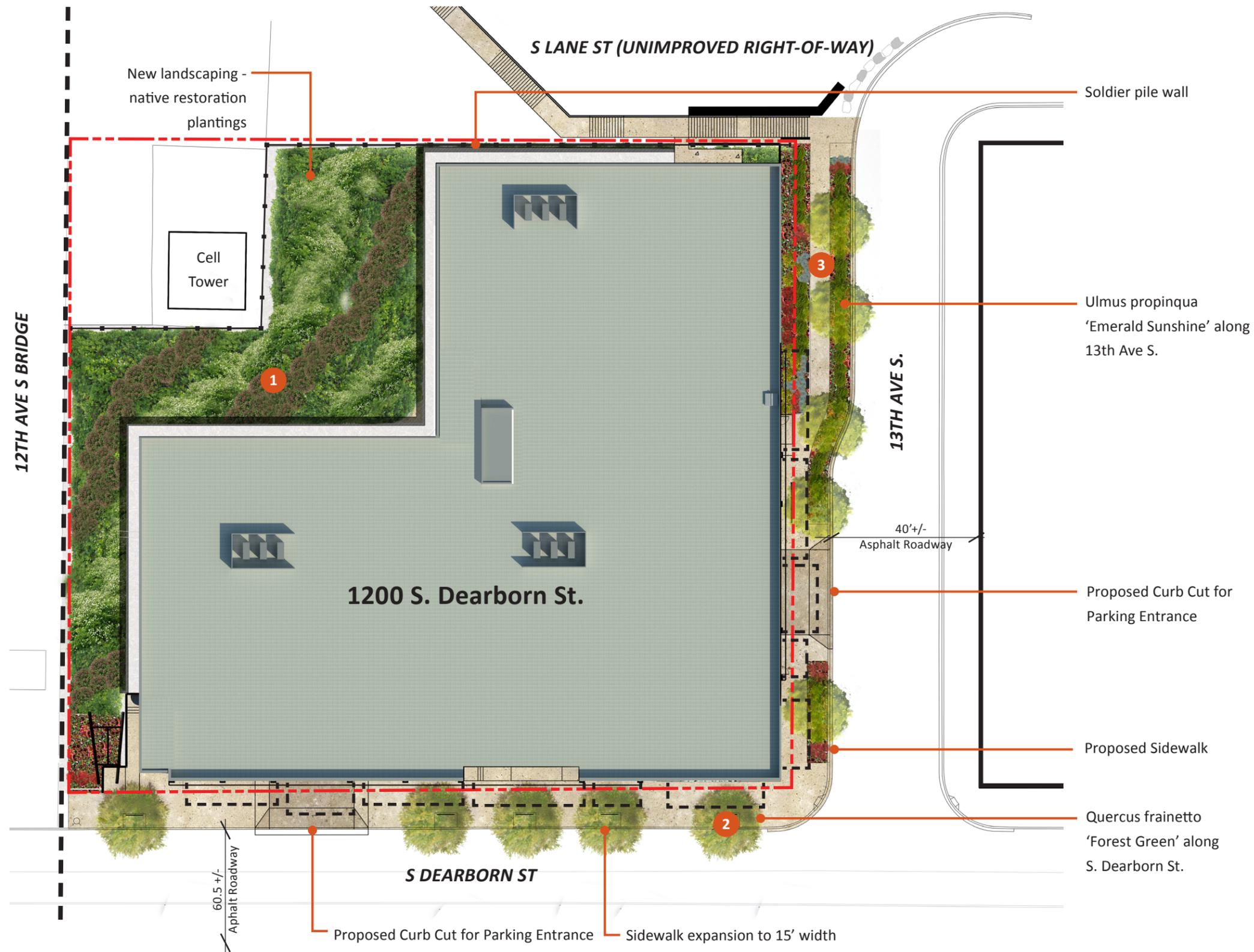
-  Parking / Loading
-  Storage
-  Retail



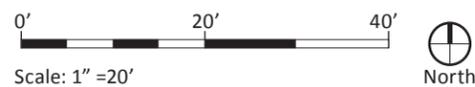
ROOF PLAN



-  Corrugated Metal Panel Roof Top Unit Screening Material



COMPOSITE LANDSCAPE / HARDSCAPE PLAN



### 1 RESTORATION PLANTINGS



Rosa rugosa/Rugosa Rose (defensibe / thorns)  
Size: 1 gallon; Spacing: 36" o.c.



Vaccinum Ovatum/Evergreen Huckleberry  
Size: 1 gallon; Spacing: 36" o.c.



Gaultheria Shallon/Salal  
Size: 1 gallon; Spacing: 36" o.c.



Ribes Sanguineum/Red Flowering Currant  
Size: 1 gallon; Spacing: 36" o.c.

### 2 STREET TREES



Ulmus propinqua 'Emerald Sunshine'/Emerald Sunshine Elm  
Size: 2" caliper



Quercus frainetto 'Forest Green'/ Forest Green Italian Oak  
Size: 2" caliper



Mahonia aquifolium / Oregon Grape (defensibile / prickly leaves)  
Size: 1 gallon; Spacing: 36" o.c.



Cornus Sericea/Red Twig Dogwood  
Size: 1 gallon; Spacing: 36" o.c.

### 3 STREETScape PLANTINGS



*Arctostaphylos uva-ursi*/Kinnickinnick  
Size: 1 gallon; Spacing: 18" o.c.



*Berberis thunbergii* 'Atropurpurea' / Red-leaf Barberry  
(defensible / thorns)  
Size: 5 gallon



*Heliotrichon sempervirens*/Blue Oat Grass  
Size: 5 gallon



*Berberis thunbergii* 'Crimson Pygmy'/Crimson Pygmy Barberry  
(defensible / thorns)  
Size: 2 gallon



*Hydrangea quercifolia* 'Snow Queen'/'Snow Queen' Hydrangea  
Size: 5 gallon



*Heuchera* x 'Plum Pudding'/Coral Bells  
Size: 5 gallon; Spacing: 18" o.c.



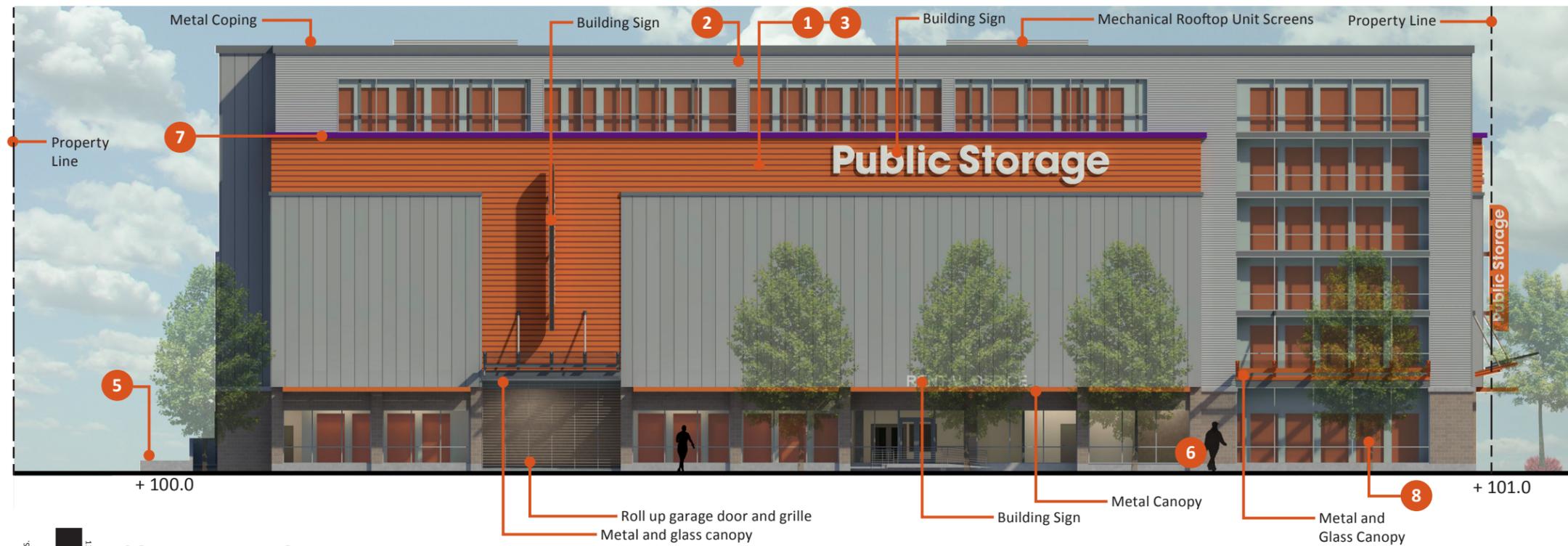
*Skimmia japonica* 'Rubella' / Skimmia  
Size: 3 gallon



*Viburnum davidii*/David Viburnum  
Size: 3 gallon

# 11.0 Building Elevations

DESIGN

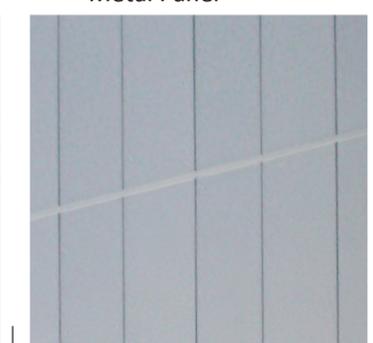
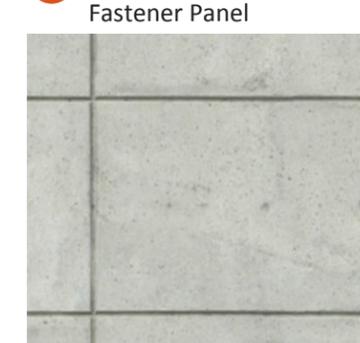
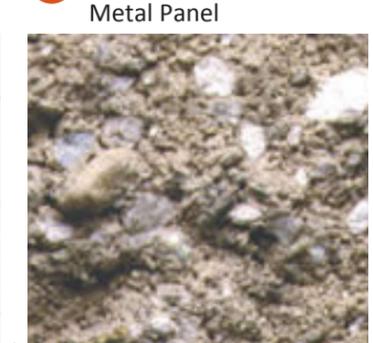


**SOUTH ELEVATION  
ELEVATION DESCRIPTION AND DIRECTION FACING**



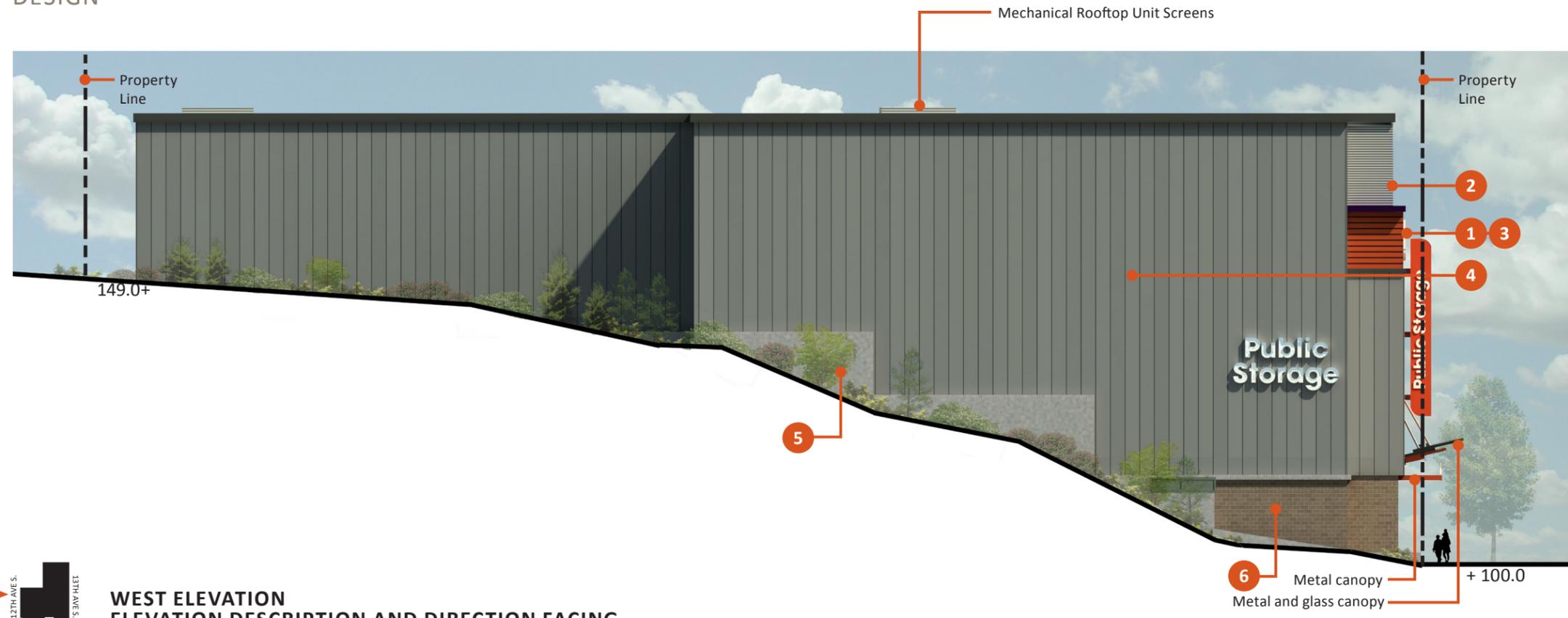
**EAST ELEVATION  
ELEVATION DESCRIPTION AND DIRECTION FACING**

## COLOR AND MATERIAL PALETTE

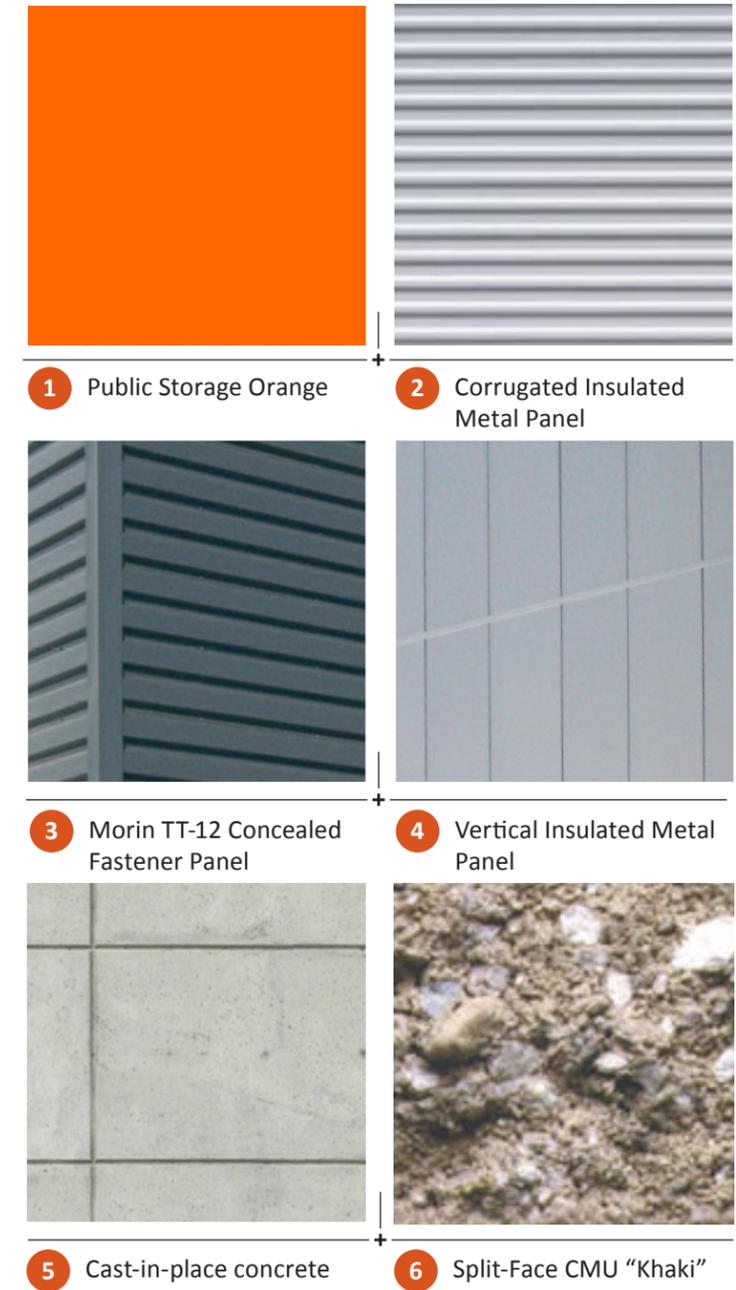
	
<b>1</b> Public Storage Orange	<b>2</b> H-Wave Insulated Metal Panel
	
<b>3</b> Morin TT-12 Concealed Fastener Panel	<b>4</b> Vertical Insulated Metal Panel
	
<b>5</b> Cast-in-place concrete	<b>6</b> Architectural Masonry
	
<b>7</b> Public Storage Plum	<b>8</b> Clear Anodized Aluminum Storefront

# 11.0 Building Elevations

DESIGN



## COLOR AND MATERIAL PALETTE



**WEST ELEVATION  
ELEVATION DESCRIPTION AND DIRECTION FACING**  
Note: Grades not shown for clarity



**NORTH ELEVATION  
ELEVATION DESCRIPTION AND DIRECTION FACING**  
Note: Grades not shown for clarity



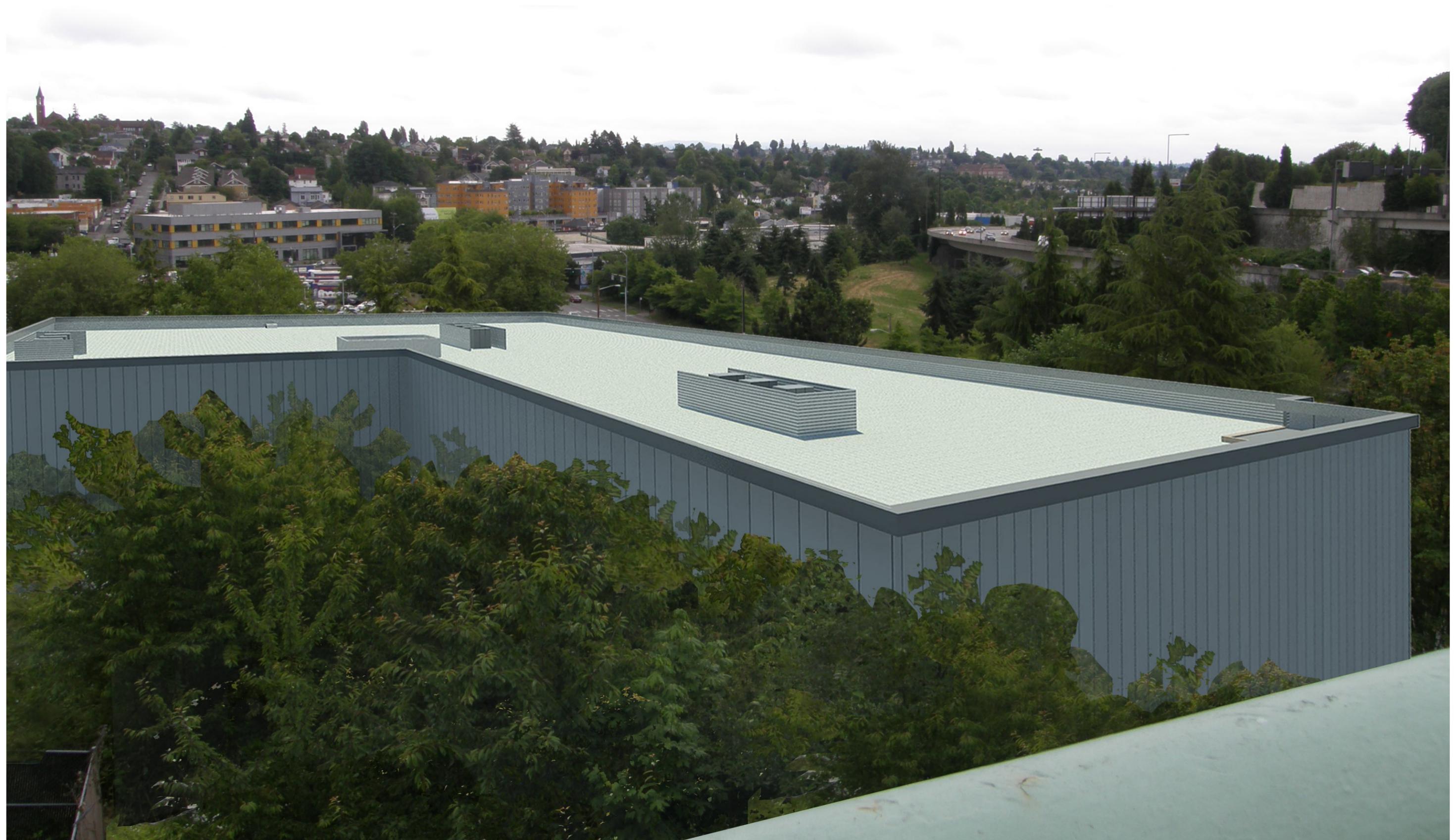
VIEW FROM S. DEARBORN ST. LOOKING NW



VIEW FROM S. DEARBORN ST. LOOKING NE



VIEW FROM SOUTH END OF 12TH AVE BRIDGE LOOKING NE



VIEW FROM NORTH SIDE OF 12TH AVE BRIDGE LOOKING SE



VIEW FROM SIDEWALK ALONG SOUTH DEARBORN STREET LOOKING WEST

# 14.0 - Exterior Lighting Plan

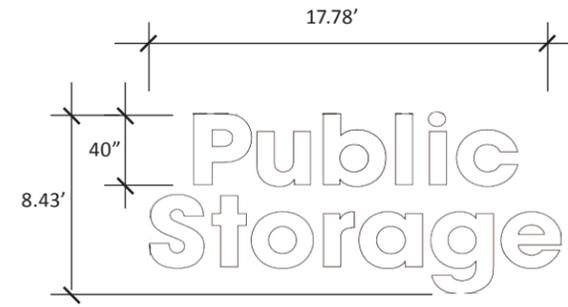


1 Luminaire LED - Blockade 58 Wall / Ceiling Mount  
Vandal Resistant CFL, typ.



SIGNAGE CONCEPT - SOUTH ELEVATION

4 5

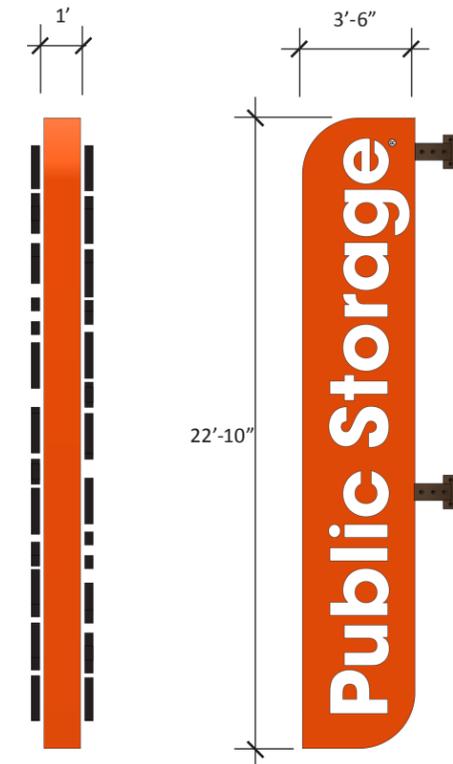


1 STACKED LAYOUT

- 5" deep channel letters, black returns & trim cap
- Letter faces - 3/16" white acrylic with orange vinyl overlay



SIGNAGE CONCEPT - EAST ELEVATION



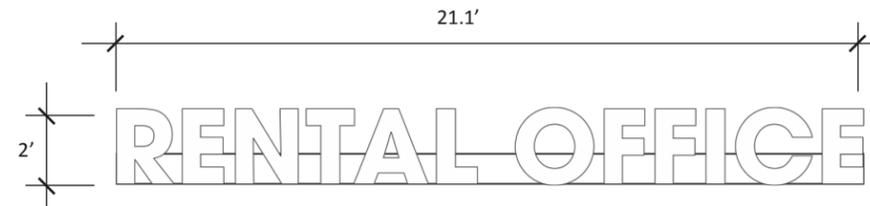
2 D/F BLADE MARQUE

- LED channel letters
- dual-lit illumination



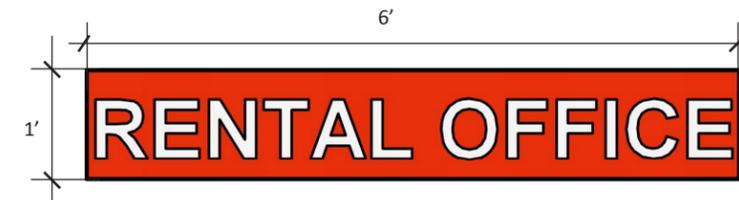
3 SIDE BY SIDE LAYOUT

- LED channel letters
- halo illumination



4 RENTAL OFFICE

- LED illuminated channel letters
- on w/w - remote

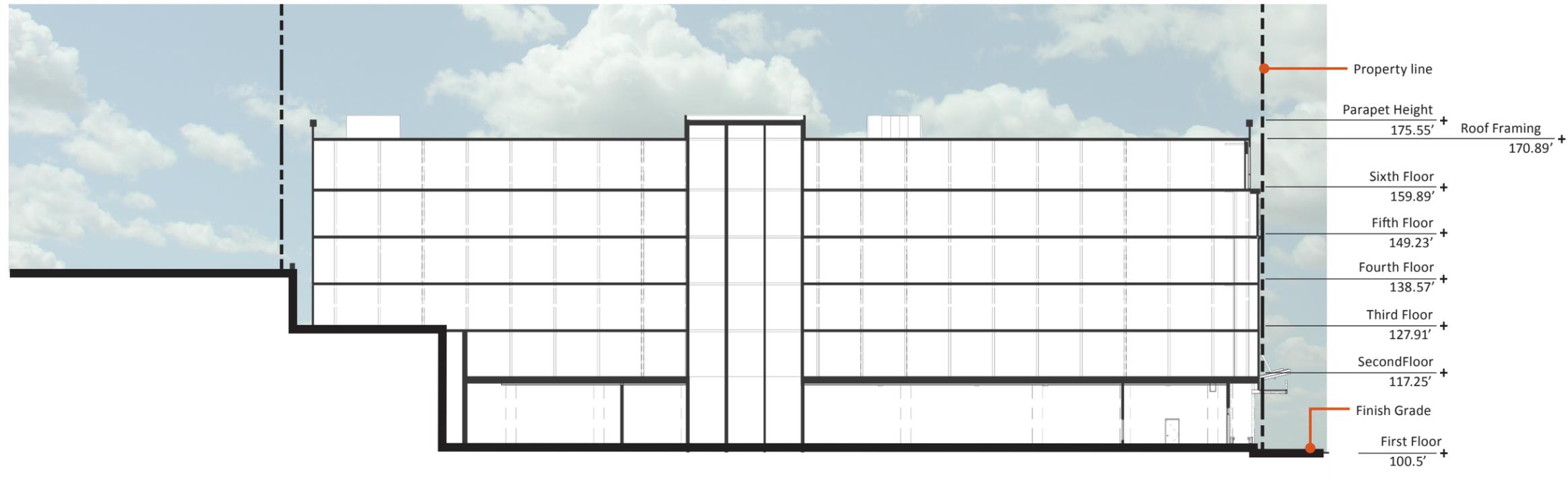


5 RENTAL OFFICE

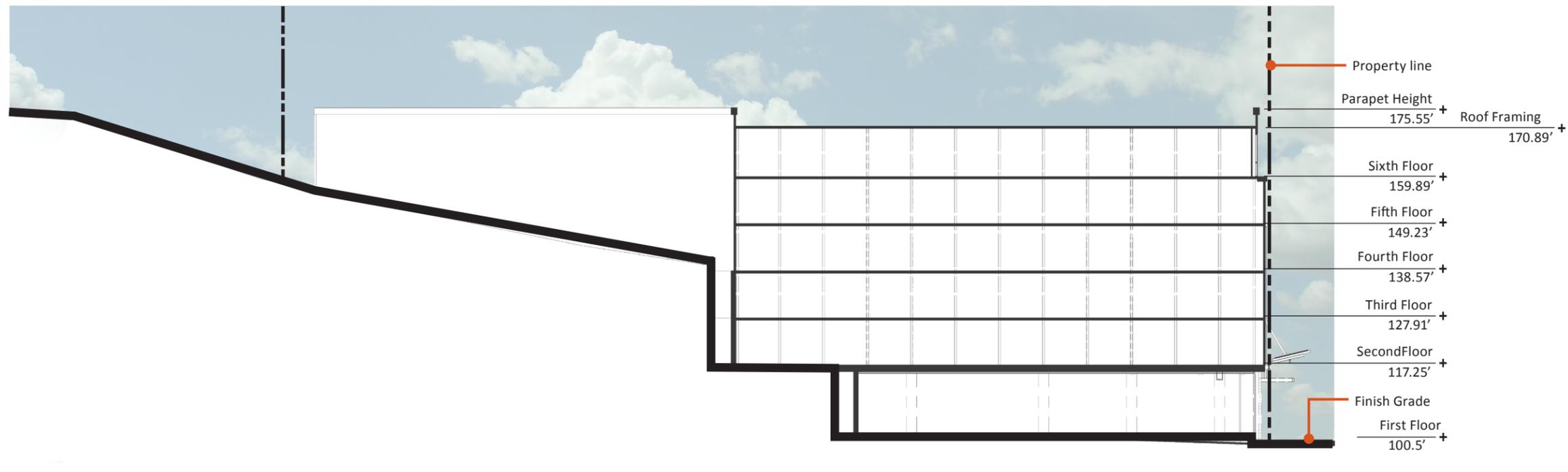
- BLADE SIGN UNDER CANOPY

# 16.0 - Building Sections

NORTH-SOUTH



SECTION AT PROPOSED BUILDING-FACING EAST



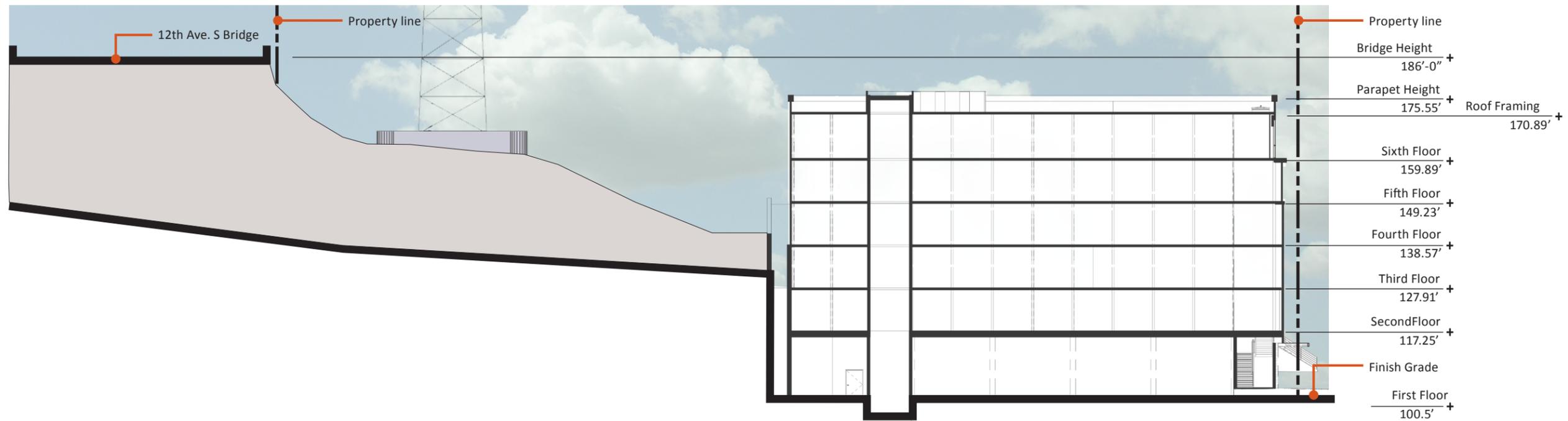
SECTION AT BRIDGE PROPOSED BUILDING-FACING EAST

Not to Scale



SECTION AT BRIDGE AND PROPOSED BUILDING-FACING NORTH

Not to Scale



SECTION AT BRIDGE AND PROPOSED BUILDING-FACING NORTH

Not to Scale

# 16.0 - Building Sections

NORTH-SOUTH



**SECTION AT PROPOSED BUILDING - S. DEARBORN STREET FACING WEST**  
Not to Scale



**SECTION AT PROPOSED BUILDING- 13TH AVE S. FACING NORTH**  
Not to Scale



Land Use Code Section	Description	Comments or Justification
SMC 23.49.018	<b>Overhead Weather Protection and Lighting:</b> A. Continuous overhead weather protection shall be required for new development along the entire street frontage of a lot except along those portions of the structure facade that: 4. are driveways into structures or loading docks.	The overhead weather protection has been broken in various locations along both South Dearborn Street and 13th Ave South to provide elements of modulation and articulation along the facades and the use of a different style of overhead protection at the vehicle entrances.
SMC 23.49.056	<b>B. Facade Setback Limits:</b> Structures greater than 15 feet in height are governed by the following criteria: 1. No setback limits apply up to an elevation of 15 feet above the sidewalk grade. 2. Between the elevations of 15 and 35 feet above sidewalk grade, the facade shall be located within 2 feet of street lot line.	<b>South Dearborn Street:</b> The façade height up to 15 feet along South Dearborn Street is setback approximately 2.8 feet from the property line. The façade above 15 feet is within the 2 foot setback requirements with the exception of the tower-like element on the southeast corner of the building which is setback 2.8 feet from the property line from the sidewalk grade to the top of the building. Additionally, the stair tower located at the southwest corner of the building is setback the maximum allowable of 10 feet from the property line. The total façade area along South Dearborn Street between 15 feet and 35 feet is 4,513. The portion of the building that is setback more than 2 feet from the property line is 1,180 or 26% of the total façade area. The area on the southeast corner along South Dearborn Street is approximately 50 feet in length, 30 feet longer than the allowed 20 feet. A departure is requested. <b>13th Ave South:</b> 13th Ave South does not require property line facades, according to Map 1H. The street-facing façade of up to 15 feet from the sidewalk is setback approximately 2 feet. The portion of the façade located at the intersection of South Dearborn Street and 13th Ave South is setback 2.8 feet from property line. Please refer to Sheet G1.02.  The total façade area between 15 feet and 35 feet is 4,340 square feet. The portion of the building that is setback more than 2 feet from the property line is 1,215 or 28% of the total façade area.  The area on the southeast corner along 13th Ave South is approximately 50 feet in length, 30 feet longer than the allowed 20 feet. A departure is requested.

Land Use Code Section	Description	Comments or Justification
SMC 23.49.056 (cont.)	<b>B. Facade Setback Limits (cont.):</b>	<b>12th Ave South:</b> The portion of 12th Ave South along the west property line is the elevated portion of the bridge deck. Due to the bridge structure, high tension power lines, and the existing cell tower, the setback limitations cannot be met. SDOT has requested the project provide a minimum of 25 feet from of separation between the proposed structure and the bridge. A departure is requested.
SMC 23.49.056	<b>C. Facade Transparency Requirements:</b> <b>Class II pedestrian streets require a minimum of 30 percent of the street level street-facing facade shall be transparent.</b>	<b>12th Ave South:</b> The façade along 12th Ave South will be below the frontage and not visible or accessible to pedestrians and passersby. The facade will also be blocked by the structure of the bridge along the west property line. A departure will be requested to the provisions of this code section.
SMC 23.49.056	<b>D. Blank Wall Limits:</b> 3. limits blank façades to no more than 30 feet wide, with the exception of garage doors. The façade along South Dearborn Street between 2 and 8 feet has been broken up in 17 foot intervals, on average, and include transparency in the form of windows or display areas.	<b>13th Ave South:</b> The façade along 13th Ave South between 2 and 8 feet has been broken up in 15'-5" foot intervals, on average and include transparency in the form of windows and display areas. However, due to the slope of the site along 13th Ave South, it is not possible to provide meaningful transparency along the north have of the façade. A departure is requested. <b>12th Ave South:</b> The façade along 12th Ave South will be below the frontage and not visible to pedestrians and passersby using the roadway. The facade will also be blocked by the structure of the bridge along the west property line and not accessible to pedestrians. A departure will be requested to the provisions of this code section.
	Departures from SMC 23.49.056 for facade transparency, blank wall limits, and setbacks are not requested for the north facade due to the fact South Lane Street is not an improved street. A street improvement exception has been granted by the City.	

# Departures

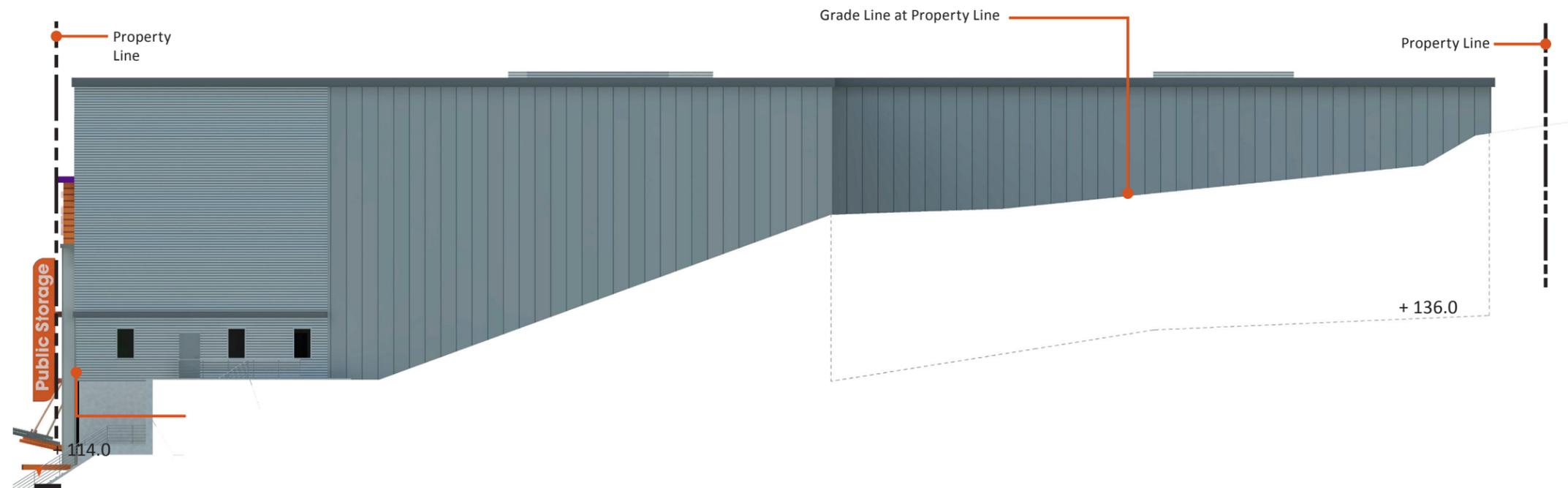
WA13217 DEARBORN (SEATTLE)



**12th Ave South:**  
The façade along 12th Ave South will be below the frontage and not visible to pedestrians and passersby using the roadway. The façade will also be blocked by the structure of the bridge along the west property line and not accessible to pedestrians. A departure will be requested to the provisions of this code section.

**12th Ave South:**  
The façade along 12th Ave South will be below the frontage and not visible or accessible to pedestrians and passersby. The façade will also be blocked by the structure of the bridge along the west property line. A departure will be requested to the provisions of this code section.

**WEST ELEVATION  
ELEVATION DESCRIPTION AND DIRECTION FACING**



**NORTH ELEVATION  
ELEVATION DESCRIPTION AND DIRECTION FACING**