

HOWELL 8th & HOWELL

DESIGN RECOMMENDATION SUBMITTAL 1: 7.15.2014

PROJECT INFORMATION:

Property Address: 808 Howell Street
Seattle, WA 98101

DPD Project #: 3016917

Owner: R.C. Hedreen & Co.
217 Pine Street, Ste 200
Seattle, WA 98101
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1 DEVELOPMENT OBJECTIVES

The applicant has applied for a Master Use Permit utilizing the One Development Site approach for the site bound by 8th and 9th Avenue, Stewart and Howell Street in the Denny Triangle Urban Villag. This application details the proposed development for the ¼ block site north and east of the existing alley, which is to remain.

The development will consist of a hotel with 1,264 hotel guest rooms. 105,000 SF of meeting and ballroom space will be provided and will complement the hotel guest rooms. Ground level related retail and restaurants will activate the streetscape along 8th Avenue and Stewart Street, significantly enhancing the pedestrian experience within the neighborhood. Parking to serve the multiple uses in the structure will be provided below grade with 450 stalls.

The proposed building design will include an 8-story podium to support the hotel registration, retail, restaurant and meeting and ballroom functions. The 500 foot high hotel tower will share the site with the pair of 19,000 SF ballrooms. Six stories of below grade construction will support the hotel operation and parking.

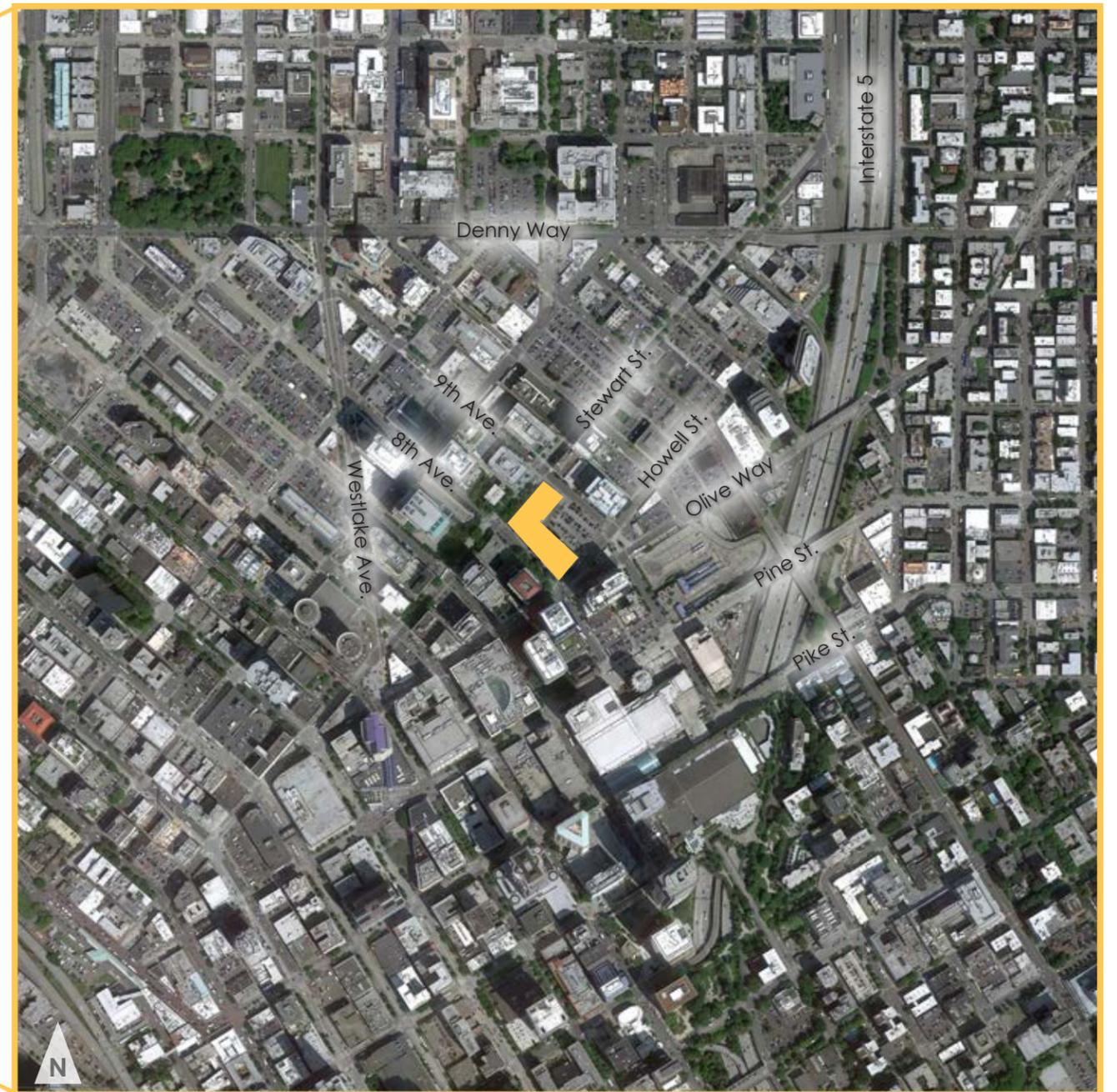
Summary:

- 1,264 hotel rooms
- 105,000 SF of meeting and ballroom space
- Ground-level retail and restaurants
- 6.5-levels of below-grade construction for approximately 450 parking stalls and support facilities for the hotel



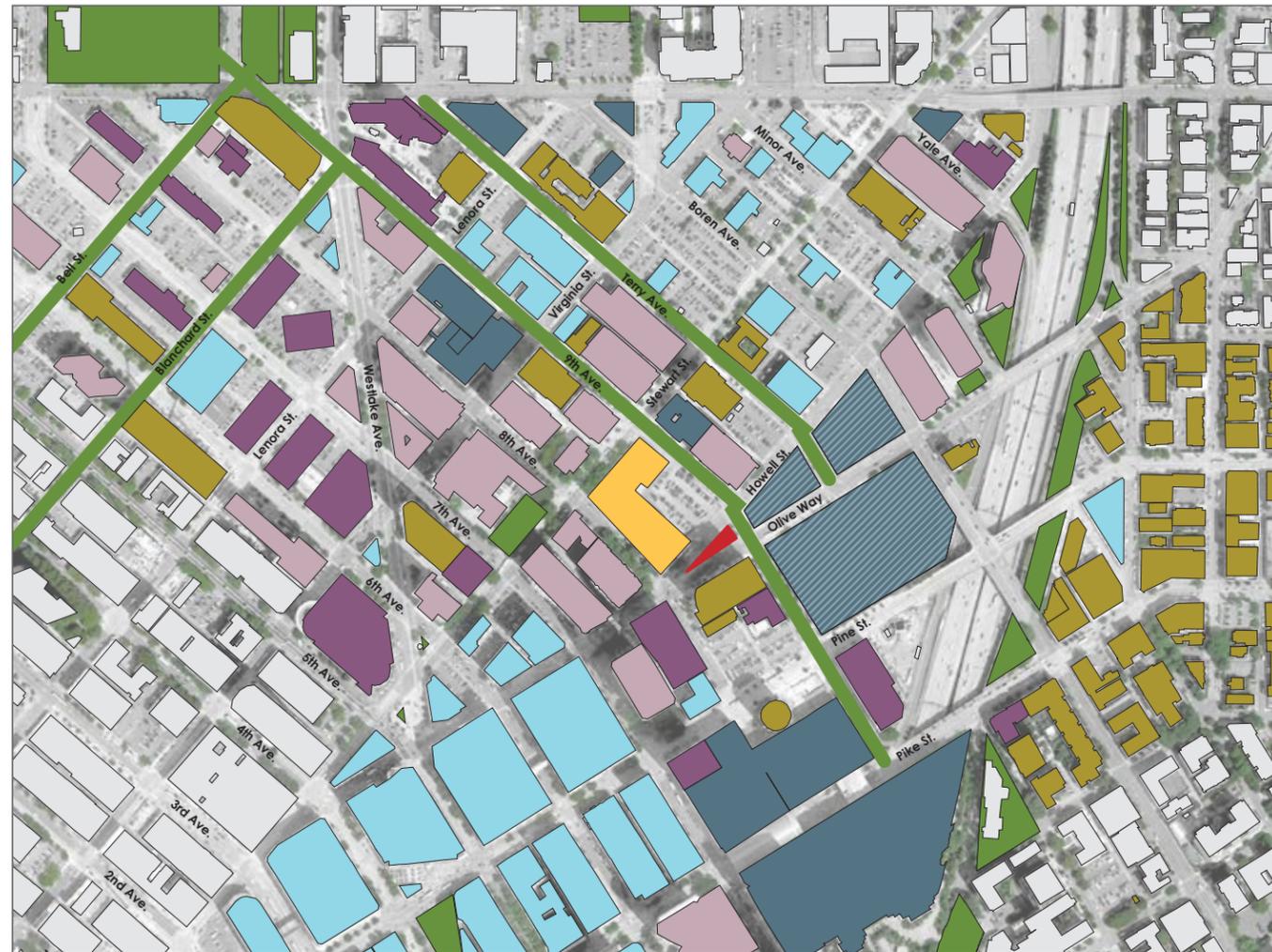
EXISTING PROJECT SITE: 807 Stewart Street

2 URBAN DESIGN ANALYSIS

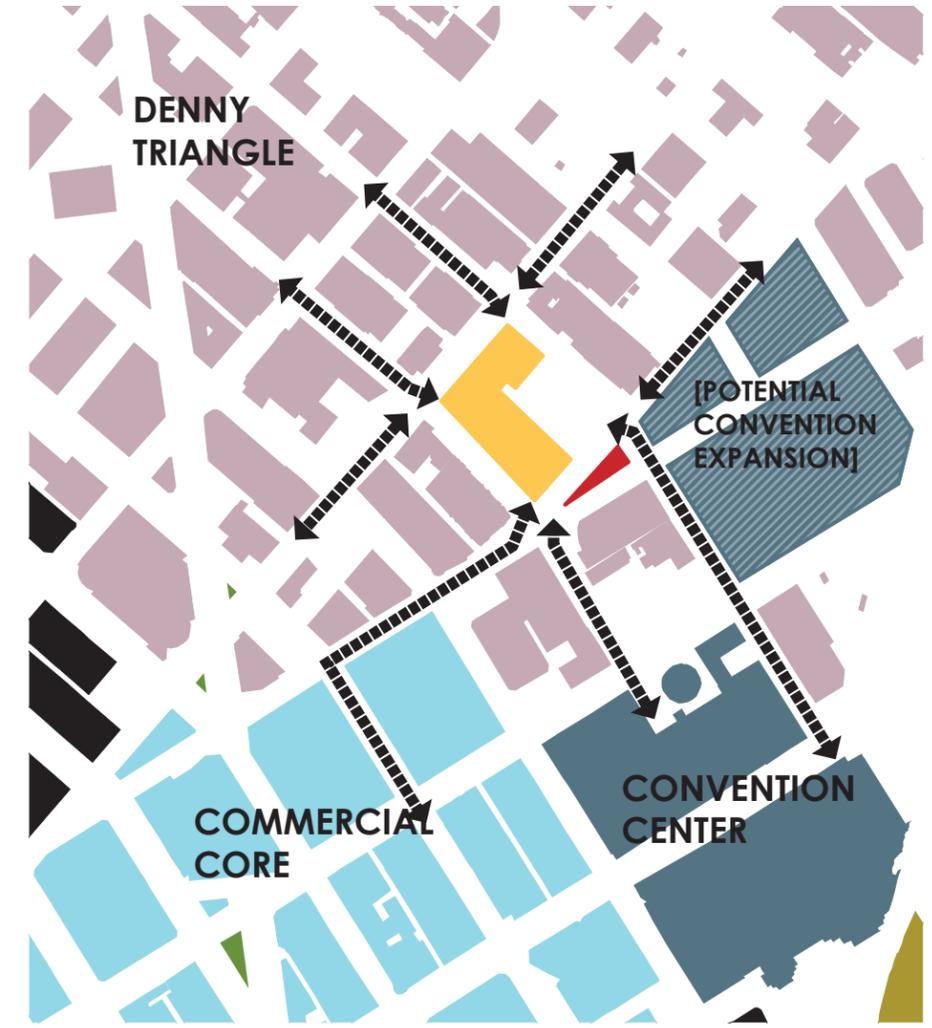


Vicinity Maps

SURROUNDING USES AND PROGRAMS



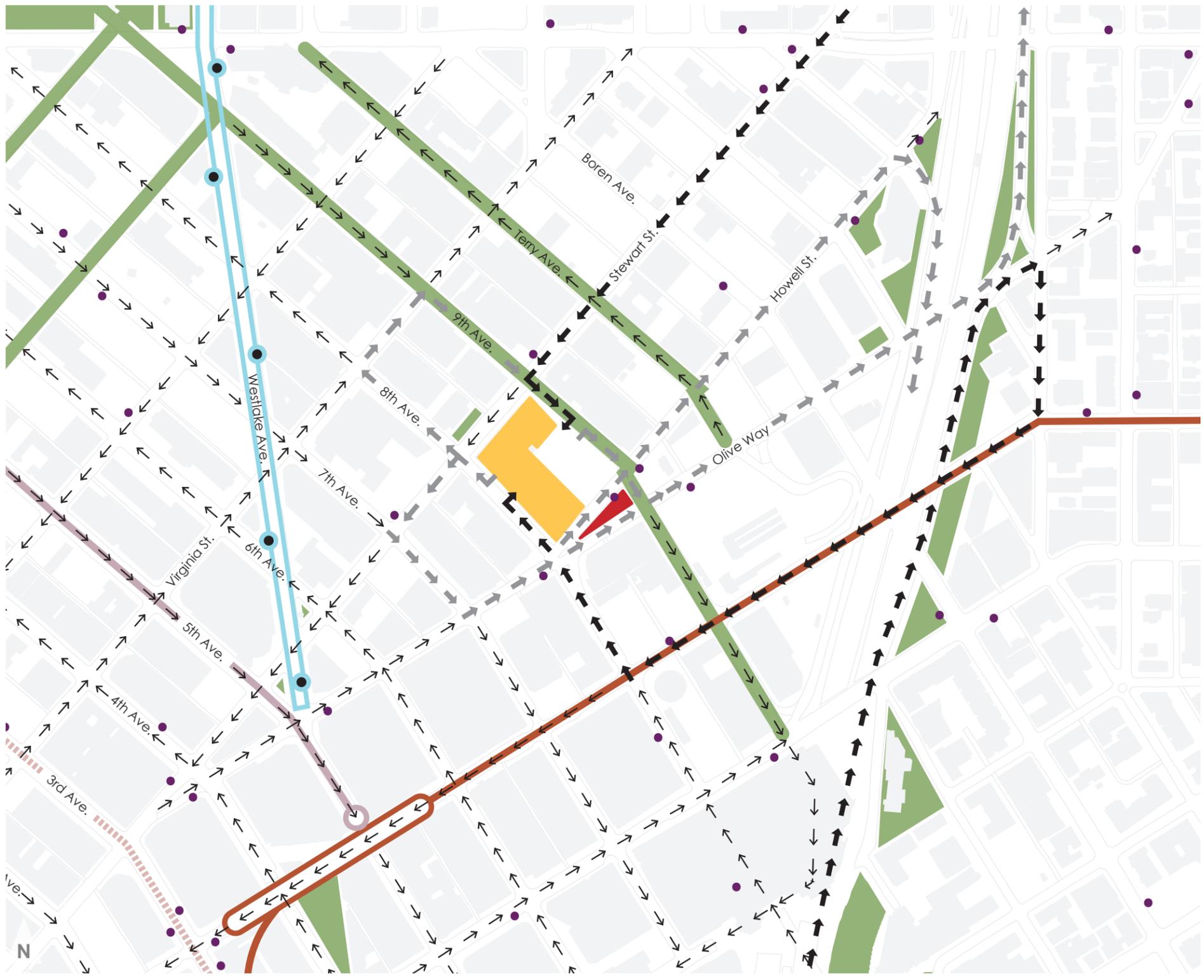
-  Site
-  Park
-  Residential
-  Hospitality
-  Commercial
-  Civic



SURROUNDING USES AND PROGRAMMING

MIXED-USE, COMMERCIAL AND CONVENTION PROGRAMS

The project site is uniquely positioned at the intersection of the city's primary commercial, convention and mixed use neighborhoods. The proposed program seeks to merge these uses into a significant urban and architectural collage.

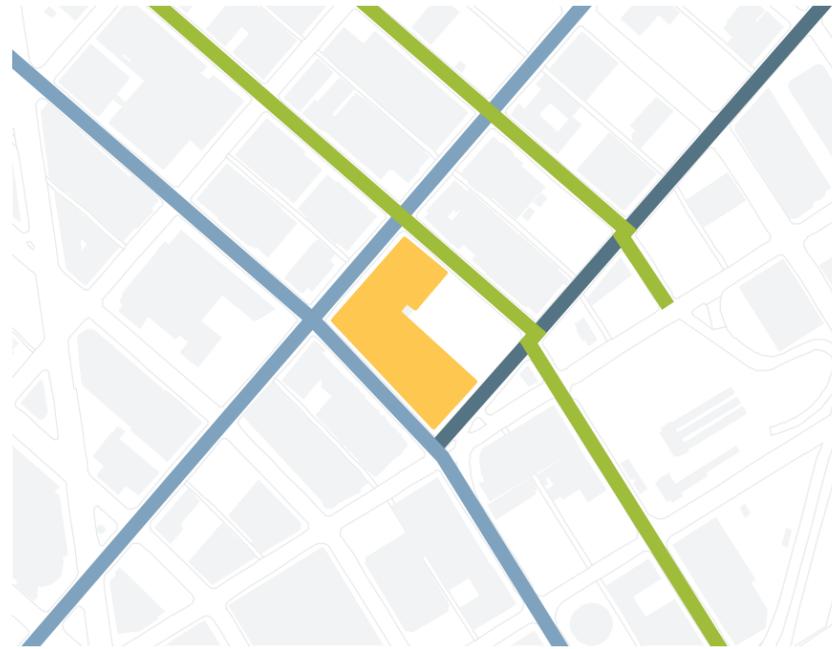


NEIGHBORHOOD TRAFFIC PATTERNS AND SITE ACCESS

The streets surrounding the site at Eighth and Howell serve a variety of uses, and are predominantly one-way. Approaches to the site from Interstate 5 lead from Stewart Street to the north and 8th Avenue to the South. Local access to and from the site is complicated by the immediately adjacent one way traffic patterns that prohibit around-the-block circulation. The heavy east-west circulation along Stewart and Howell streets makes direct vehicular access from these streets less desirable. As a result, the proposal aims to accommodate most vehicular access from 8th and 9th Avenues.

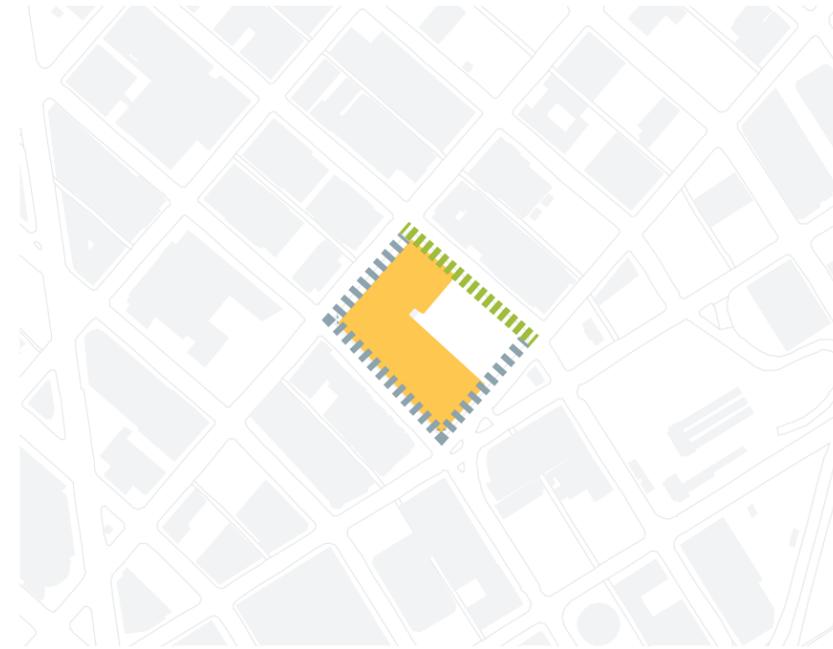
-  Site
-  Access from I-5
-  Access to I-5
-  Street Direction
-  Light Rail
-  Streetcar
-  Monorail
-  Restricted Bus Street
-  Bus Stop
-  Green Street / Open Space

PEDESTRIAN STREET CLASSIFICATIONS & SIDEWALK DESIGNATIONS



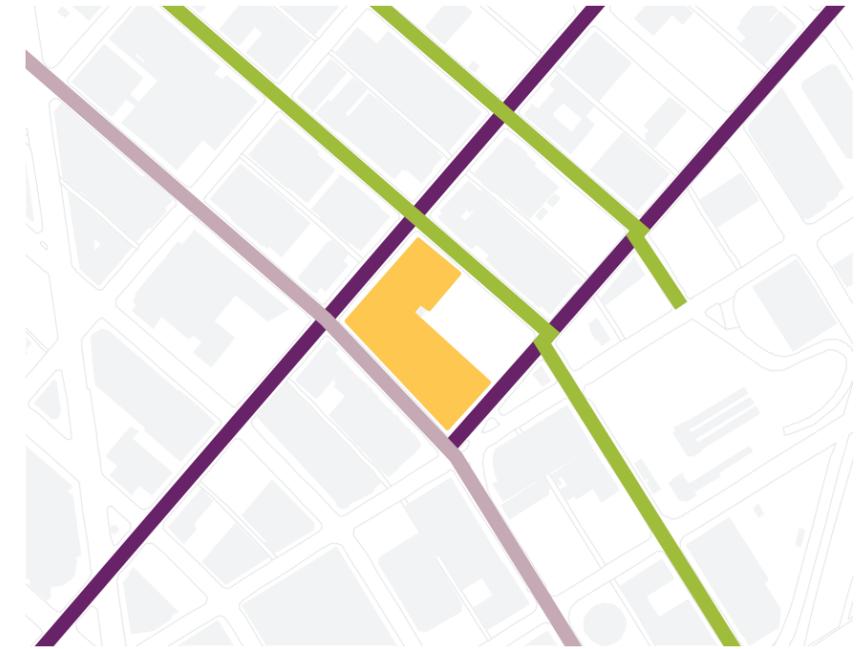
Pedestrian Street Classifications

- Green Street
- Class I Pedestrian Street
- Class II Pedestrian Street



Sidewalk Designations

- 15 feet required; sidewalk located on opposite side of bus stops
- Varied width due to Green Street Requirements



Vehicular Classifications

- Green Street
- Minor Arterial Street
- Primary Transit Street

TRANSPORTATION



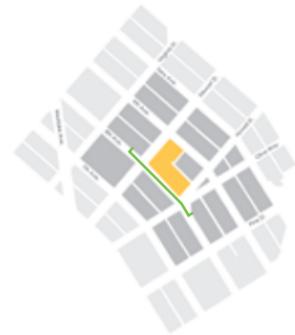
-  Bus stop
-  Sharrows
-  Designated Bicycle Lane
-  One-Way Traffic
-  Green Street Designation
-  Existing Street Trees

PUBLIC TRANSIT STOPS AND BIKE LANES

The site is appropriately located between two major bus arterials: Westbound bus traffic serves a primary station at 9th and Stewart Street while Eastbound bus traffic is accommodated at a series of stops along Olive and Howell Streets. Bicycle lanes surround the site on all sides except along the 9th Avenue Green Street.



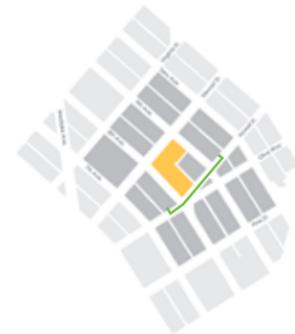
NEIGHBORHOOD ANALYSIS: CURRENT SITE CONDITION



A 8TH AVE LOOKING NORTHEAST



SITE



B HOWELL ST LOOKING NORTHWEST



SITE



C 9TH AVE LOOKING SOUTHWEST



SITE



D STEWART ST LOOKING SOUTHEAST



SITE

SITE



SITE



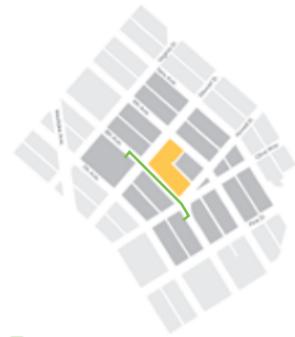
SITE



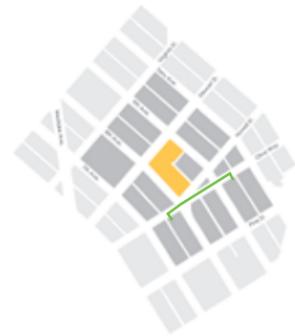
SITE



NEIGHBORHOOD ANALYSIS: ADJACENT STREETSAPES



E 8TH AVE LOOKING SOUTHWEST



F HOWELL ST LOOKING SOUTHEAST



G 9TH AVE LOOKING NORTHEAST



H STEWART ST LOOKING NORTHWEST

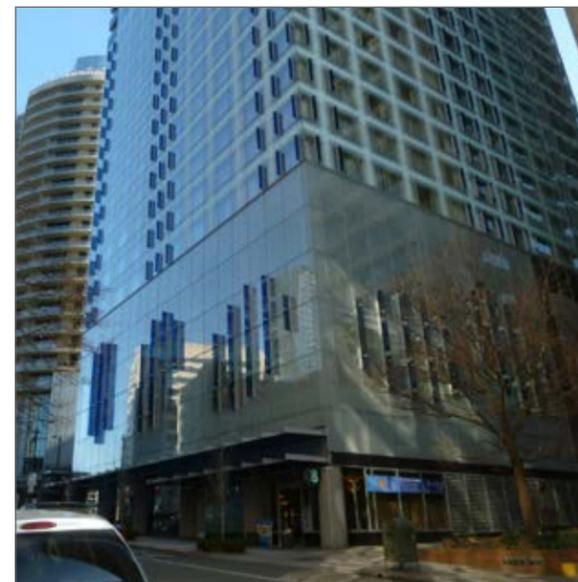




NEIGHBORHOOD BUILDING EXAMPLES



A 737 Olive Way
Olive 8, 39 story mixed-use with condominium residential and hotel uses. 2009.
Height: 459 feet



B 809 Olive Way
The Olivian, condominium residential.



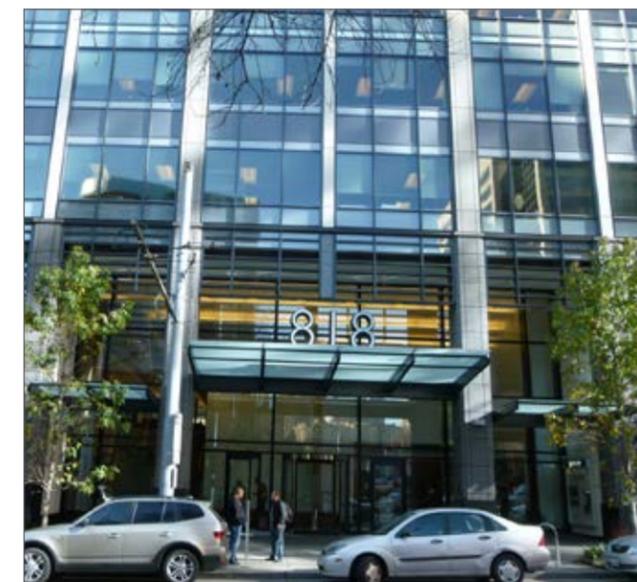
C 1823 Terry Avenue
Aspira Apartments, 37-story apartment building. LMN Architects, 2009
Height: 400 feet



D 1900 9th Avenue
Seattle Children's Research Institute



E 818 Stewart Street
Office tower.



NEIGHBORHOOD BUILDING EXAMPLES (CONT)



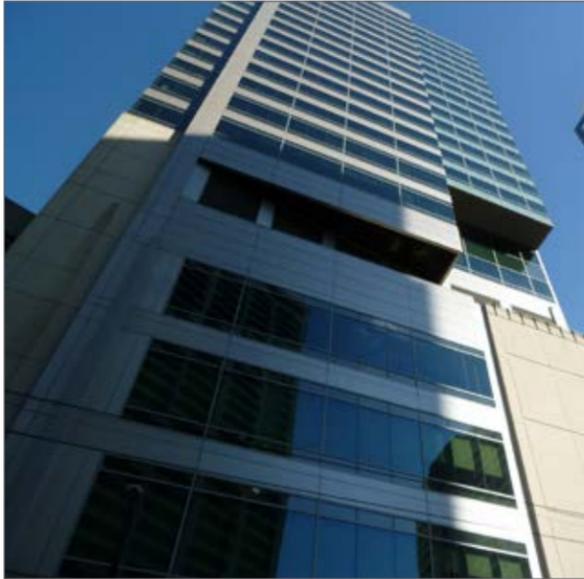
F 700 Stewart St
Federal Courthouse Building



G 1900 5th Avenue
Westin Hotel Seattle



H 1918 8th Avenue
Office tower.



J 800 Convention Place
Washington State Convention & Trade Center



K 911 Pine Street
The Paramount Theater

AREA ZONING MAP + MAXIMUM BUILDABLE ENVELOPE

Allowable Height

500 feet for commercial development
300 to 500* feet for residential development

Upper Level Setback

A continuous upper-level setback of 15' must be provided on the street frontage abutting 9th Avenue, a designated green street, above a height of 45'.

Minimum Alley Width:

The minimum alley width is 10 feet to the alley centerline. Underground and overhead portions of structure that would not interfere with the functioning of the alley may be allowed to project into the alley up to the original property line by the Director of the Department of Planning and Development after consulting with the Director of Transportation.

Upper-level Width Limit

On lots that exceed 200' in width and depth the maximum facade width parallel to the North-South Avenues (i.e. 8th Avenue) is 145' above 240' in height and the tower must be separated by 80' from any other tower above 240' on the same lot.

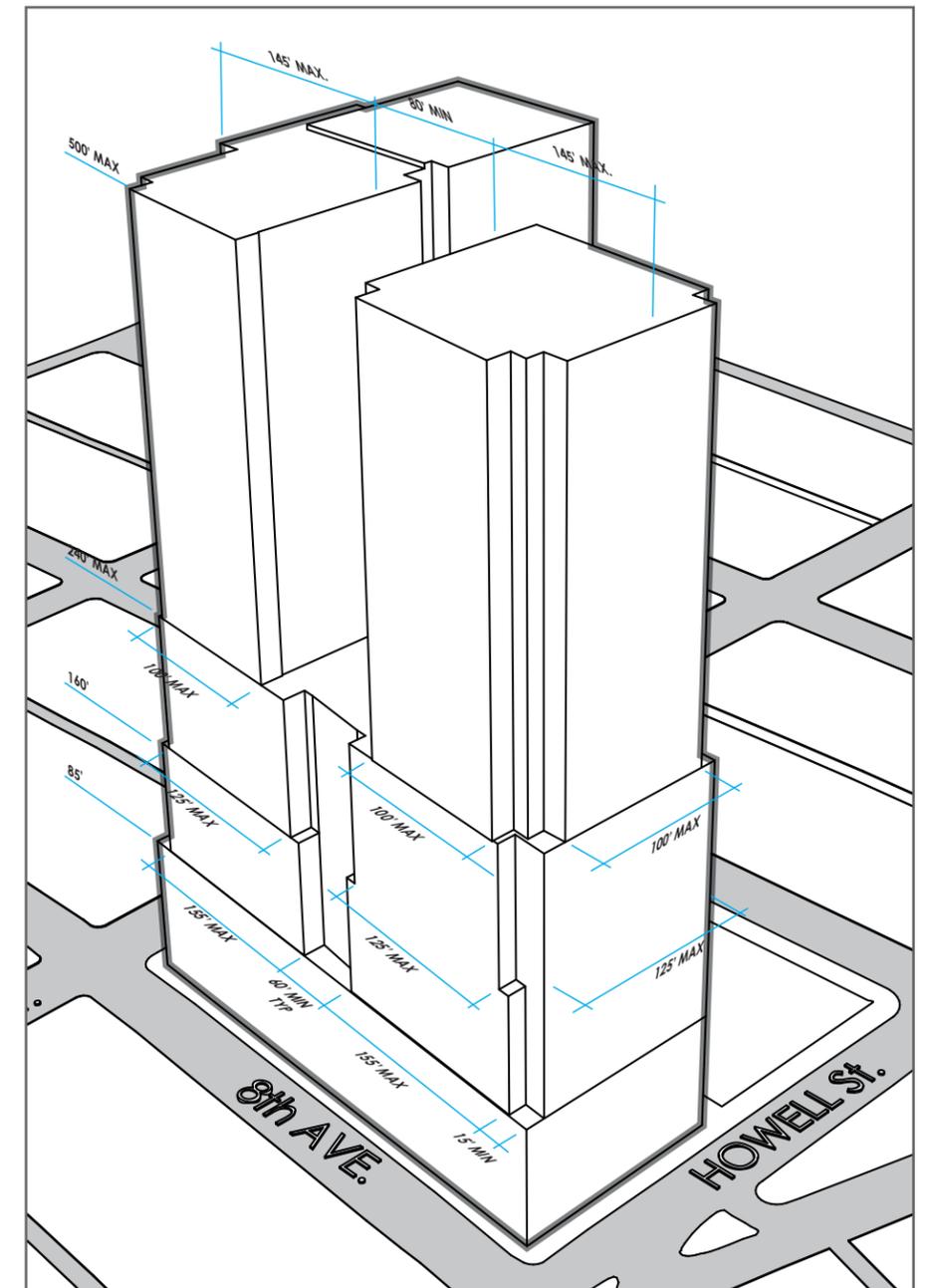
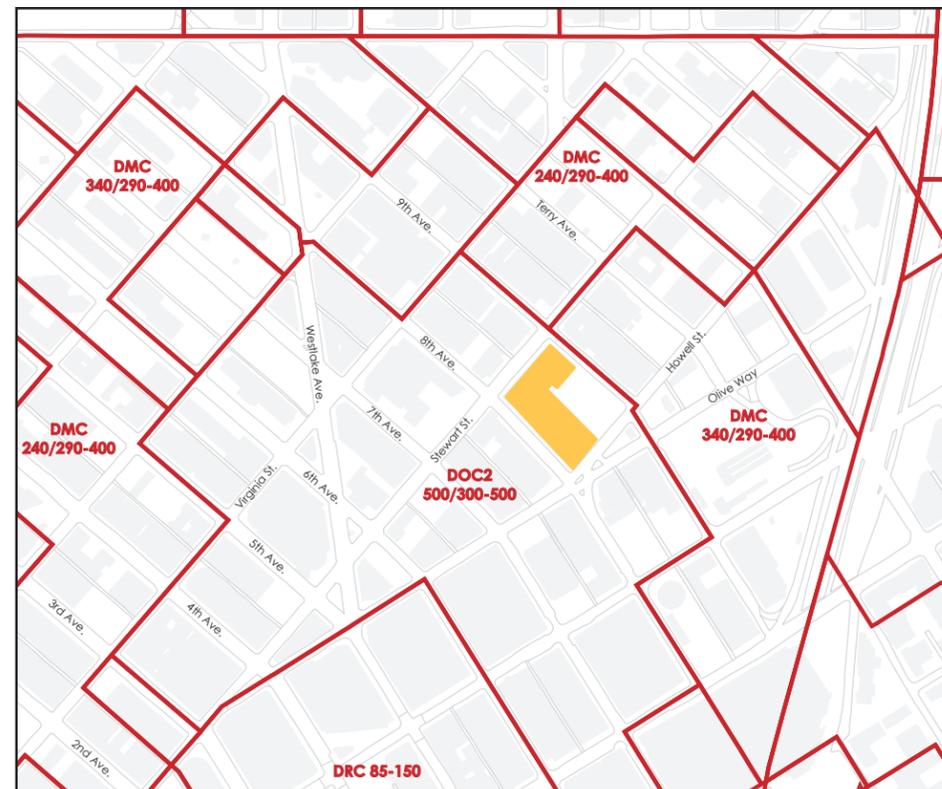
Access Opportunities and Constraints:

As shown in the urban design analysis the site is easily accessible by all modes of transportation, including buses, light rail, and streetcar. The current alley configuration directs traffic onto the site from Howell Street and 9th Avenue. The Seattle Municipal

Code prefers that access to the site occur off the alley with 8th Avenue as the next best alternative.

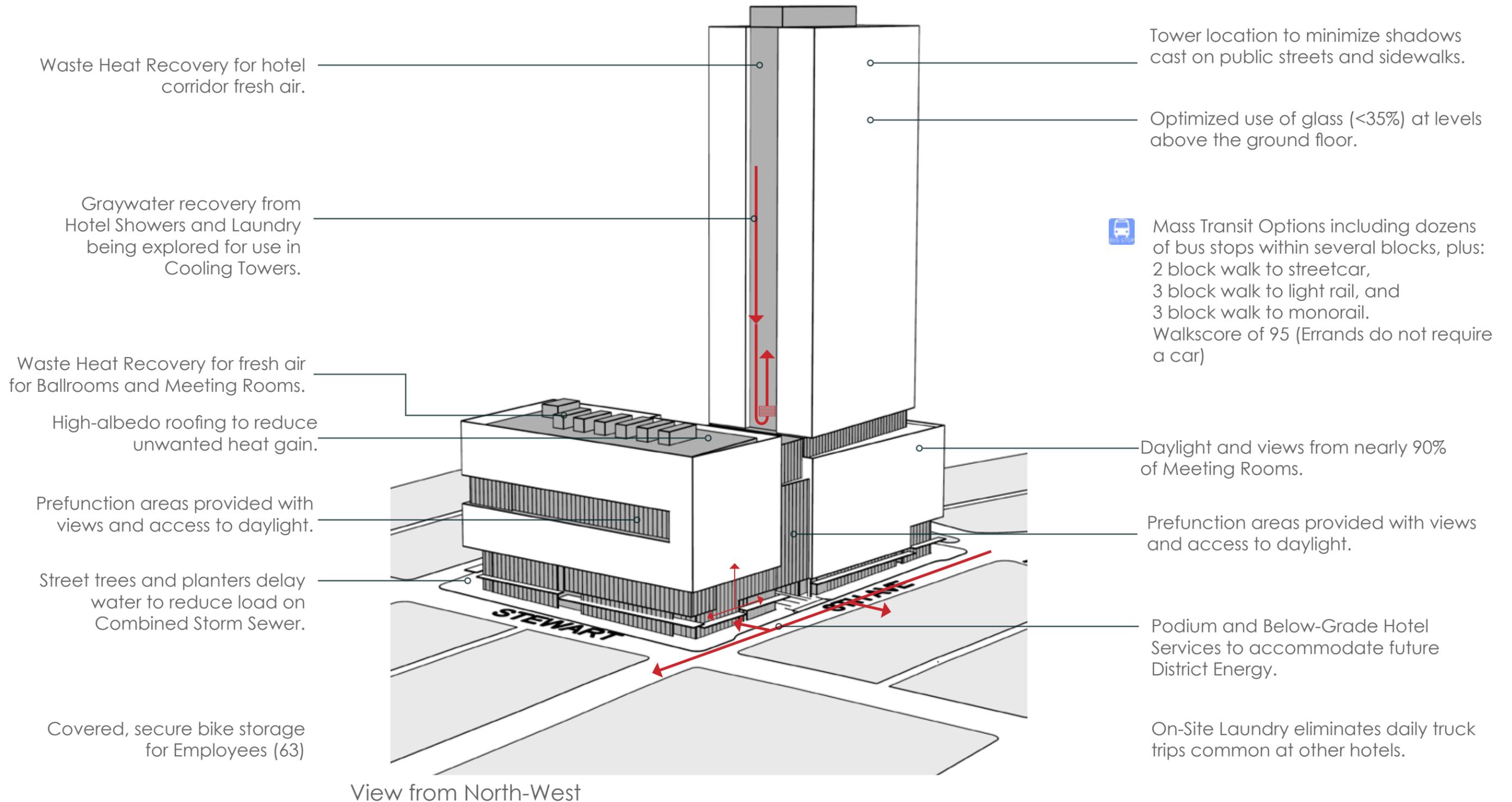
Facade Modulation

Facades must be modulated above a height of 85' or stepped back 15' for at least 60' in width. The maximum length of an unmodulated facade within 15' of the property line varies by height. Facades between 86' and 160' in elevation have a maximum width of 155'. Facades between 161' and 240' in elevation have a maximum width of 125'. Facades between 241' and 500' in elevation have a maximum width of 100'.



3 SUSTAINABILITY

BUILDING STRATEGIES



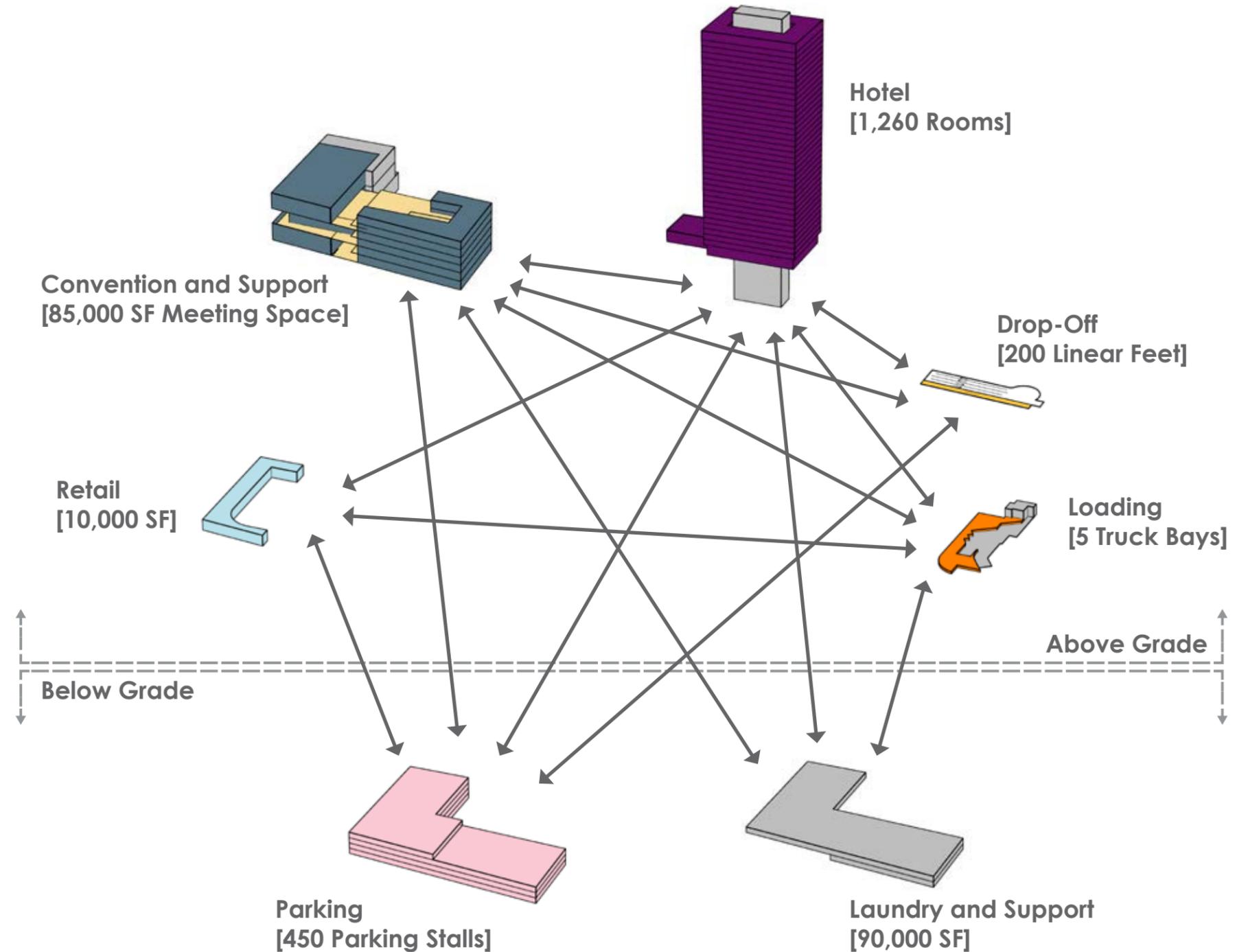
3 BUILDING PROGRAM AND FORMAL COMPOSITION

PRIMARY PROGRAM ELEMENTS

The proposed hotel, convention and ground-level retail uses for this site are well matched to the growing private and public development in the surrounding districts. While the proposed program elements complement the neighborhood, they each have related access and service requirements which historically have proven challenging to solve in a sensitive and pedestrian-friendly manner. This proposal actively seeks to minimize the adverse impacts of service and loading elements while maximizing the presence of active retail and hotel uses at grade.

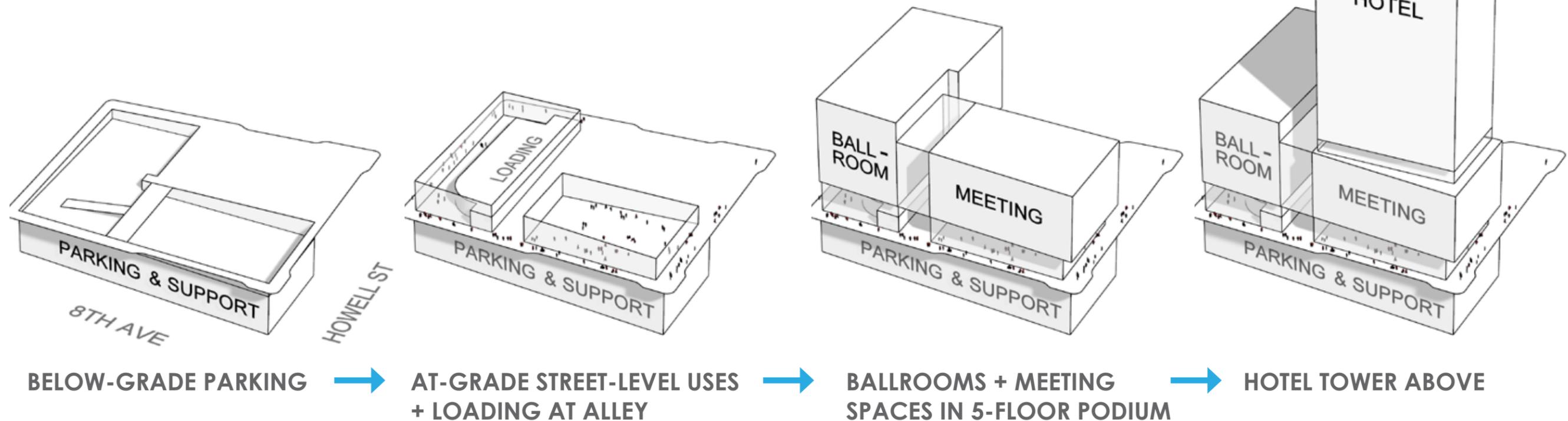
The proposed program components will create a variety of scales and activity and will form a vibrant urban collage.

The primary challenges in organizing the program and spatial requirements stem from accommodating the truck loading requirements and the conflicting hotel tower core. The L-shaped geometry and size of the site require that truck loading be accommodated at-grade. Due to the open area required for this loading dock, the hotel tower needs to be located away from the service areas. Each of the following alternatives show a different approach to the tower location, and the resulting configuration of the lower levels of the building.



FORMAL COMPOSITION

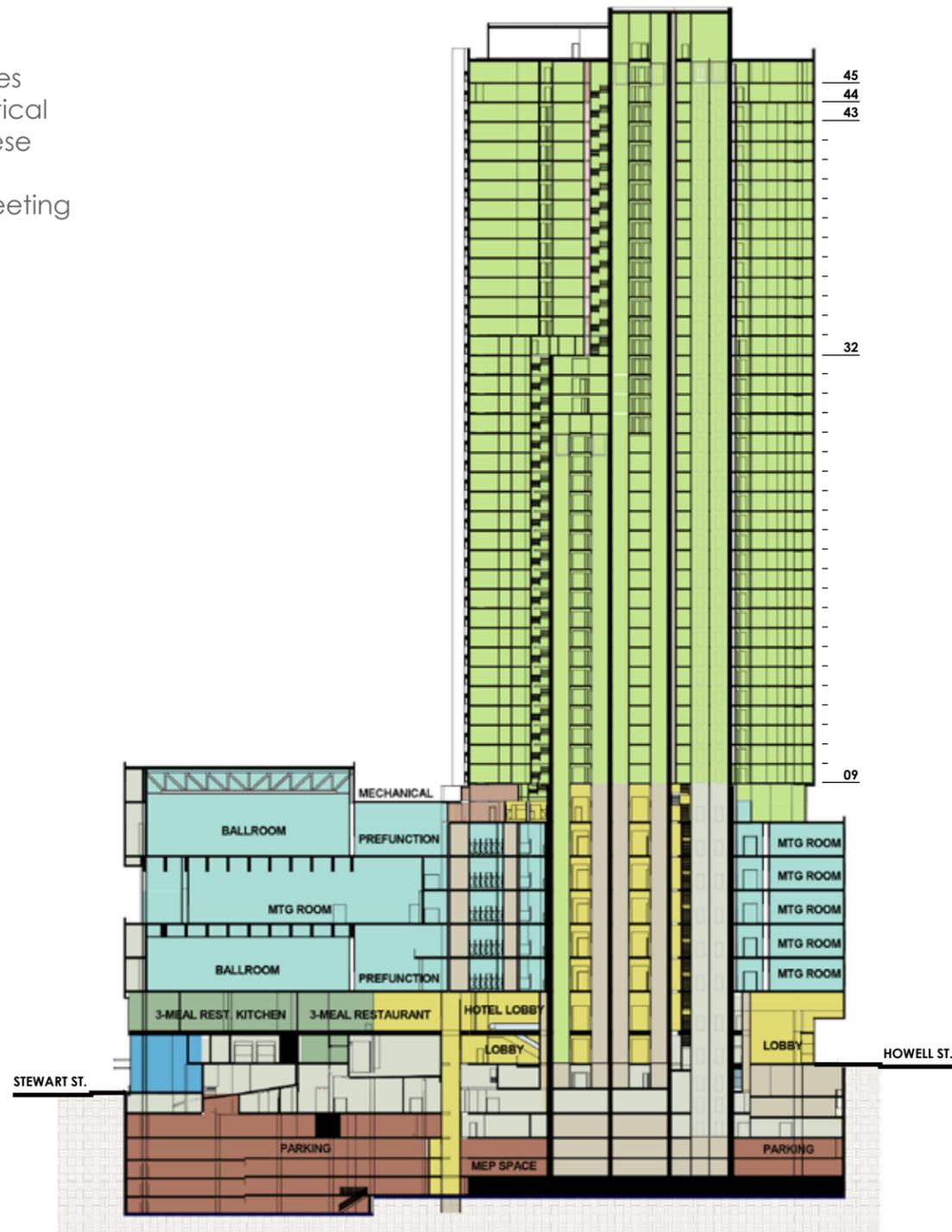
The complex interrelationships of both urban and building program elements are composed as a series of solid and transparent volumes. This design proposal expresses the internal public spaces as transparent voids between the more solid forms of the ballroom, meeting, and hotel function elements. At grade, the predominance of street-level retail and entry lobby spaces will be expressed as a continuous transparent and accessible series of frontages. The podium elements above will be expressed as two distinct entities, enclosing the ballroom elements and meeting spaces while exposing the prefunction as a void between the two. Above the podium, a level of lounges and support spaces will separate the hovering hotel tower above. The resulting building forms will express these elements as a unique composition of solids and voids, exposing the program within the building and the supporting public activities that engage them.



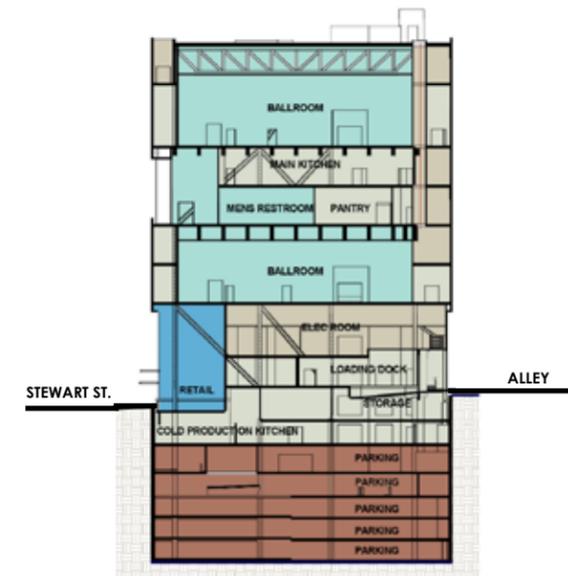
4 BUILDING PLANS AND SECTIONS

SECTIONS

These sections illustrate the volumetric and programmatic vertical relationships within the building. 6 levels of parking and support spaces will be located below-grade. A full-height vertical circulation and structural core will connect these lowest levels to the building elements above, allowing service to access the lobby levels, meeting spaces, and hotel rooms in the tower above.



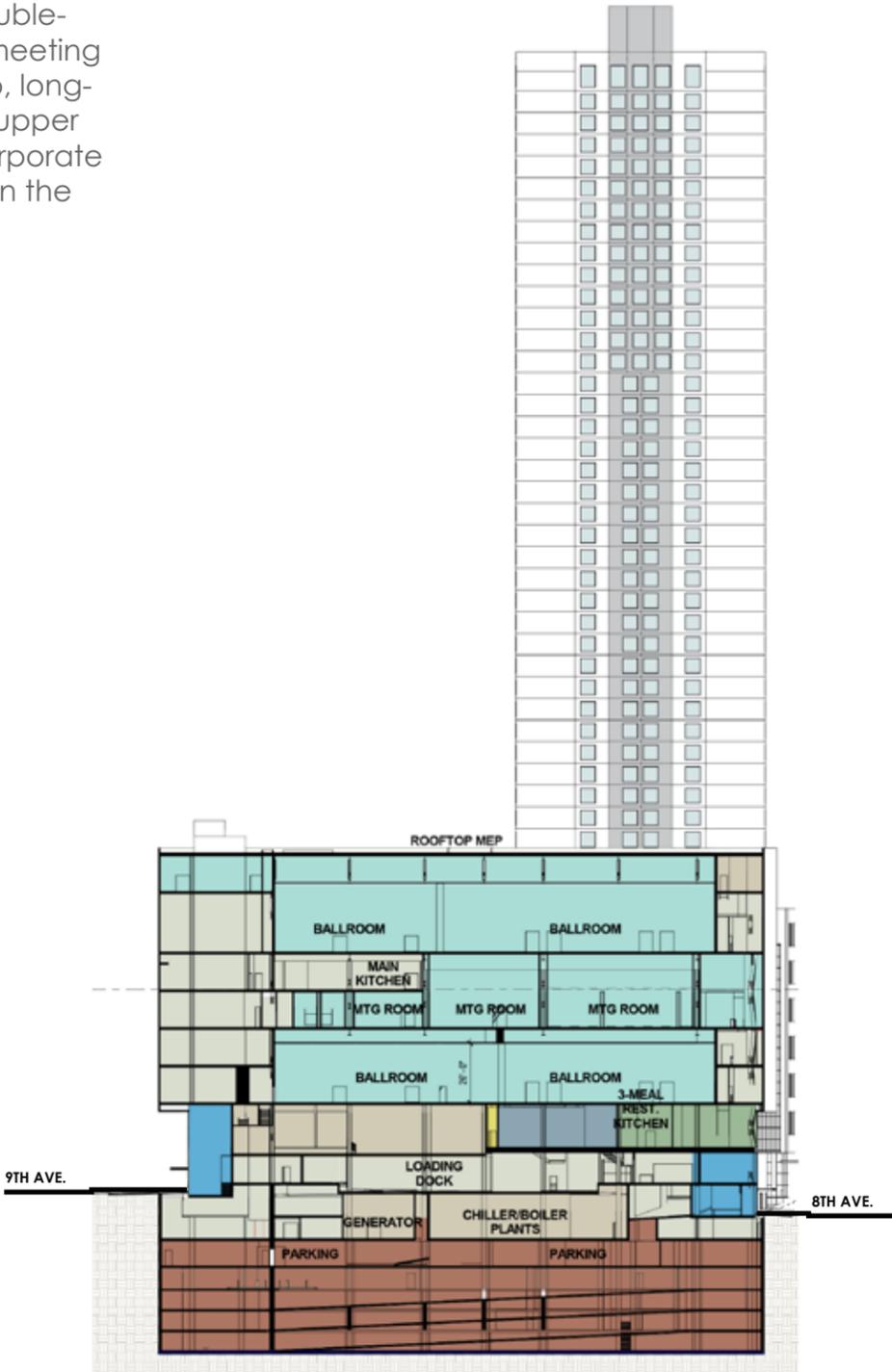
NORTH-SOUTH SECTION THROUGH HOTEL



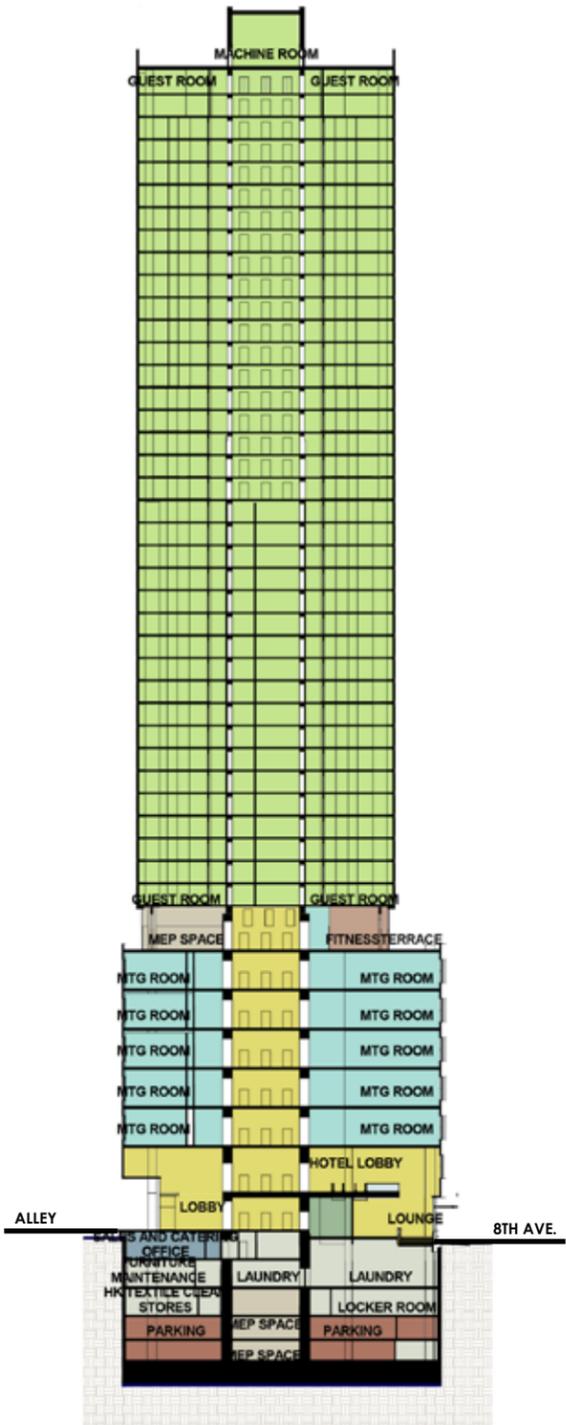
NORTH-SOUTH SECTION THROUGH BALLROOMS

PODIUM SECTIONS

The podium section is defined by the two double-height ballroom levels and their associated meeting levels. Between these ballroom levels a deep, long-span truss system will support the floor of the upper ballroom and affords the opportunity to incorporate a smaller ballroom and meeting spaces within the interstitial truss space.



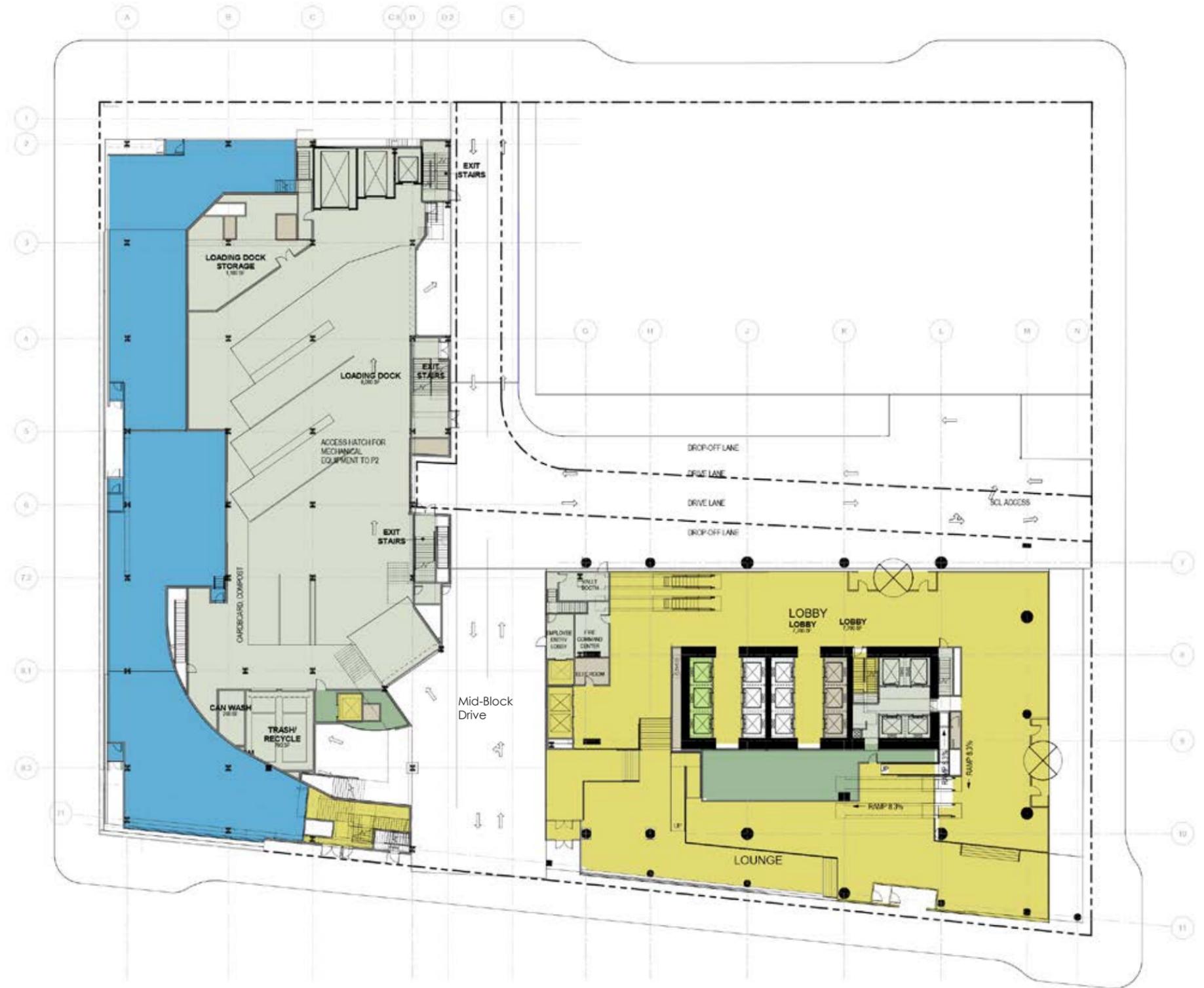
EAST-WEST SECTION THROUGH BALLROOMS



EAST-WEST SECTION THROUGH HOTEL

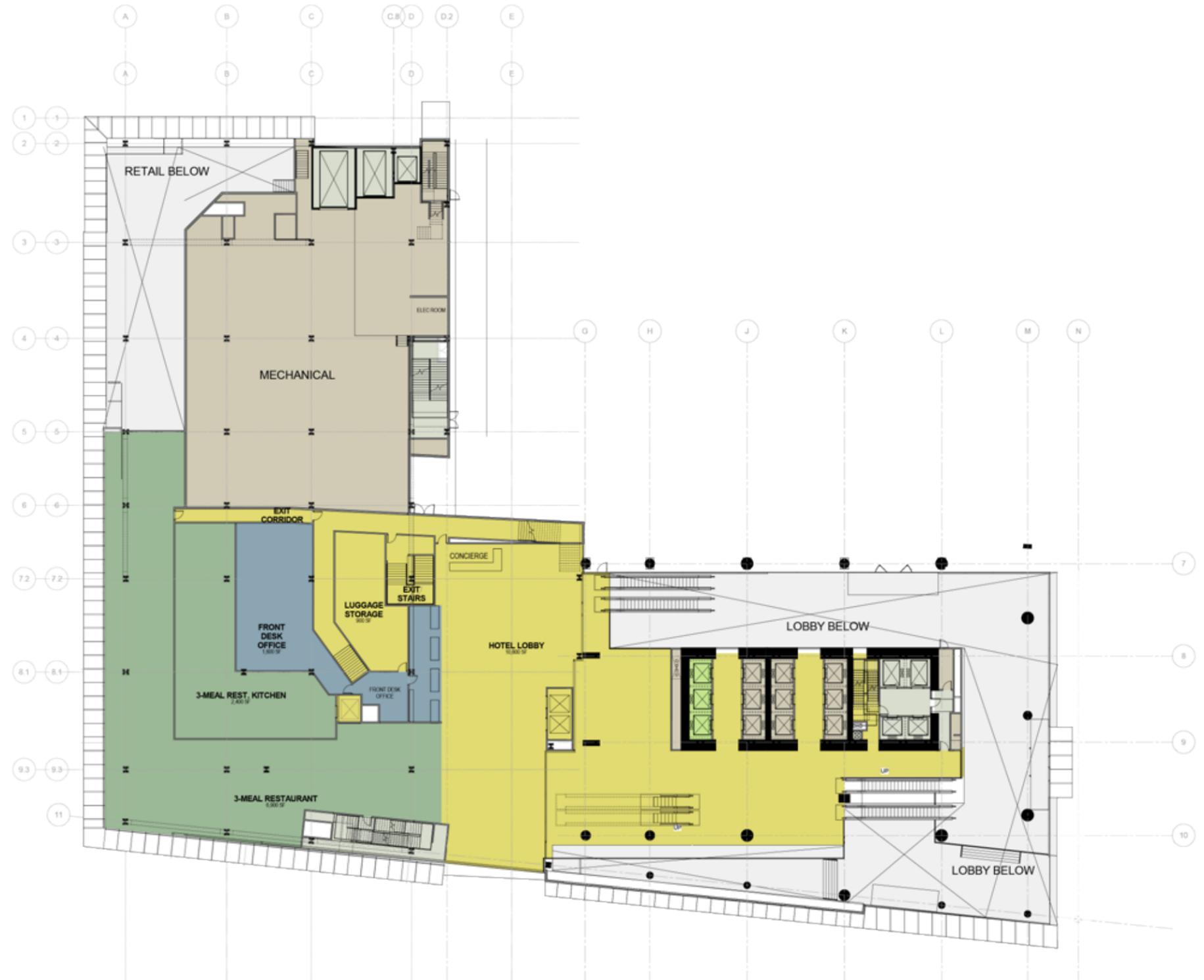
LEVEL 01 - GROUND-LEVEL CONVENTION LOBBY, LOUNGE, LOADING DOCK AND RETAIL

The grade level of the building houses the main hotel and convention entry lobby to the south, fronting on Howell Street, 8th Avenue, and the Alley. A vehicular and pedestrian cut-through separates the southern entry from the northern retail and loading areas. Truck loading will occur on the interior of the block, allowing the majority of the street frontages of 8th Avenue, Stewart, and 9th Avenue to be dedicated to retail uses. Exit stairs serving the below and above grade spaces are placed as discreetly as possible along the 8th Avenue, 9th Avenue, and alley building edges.



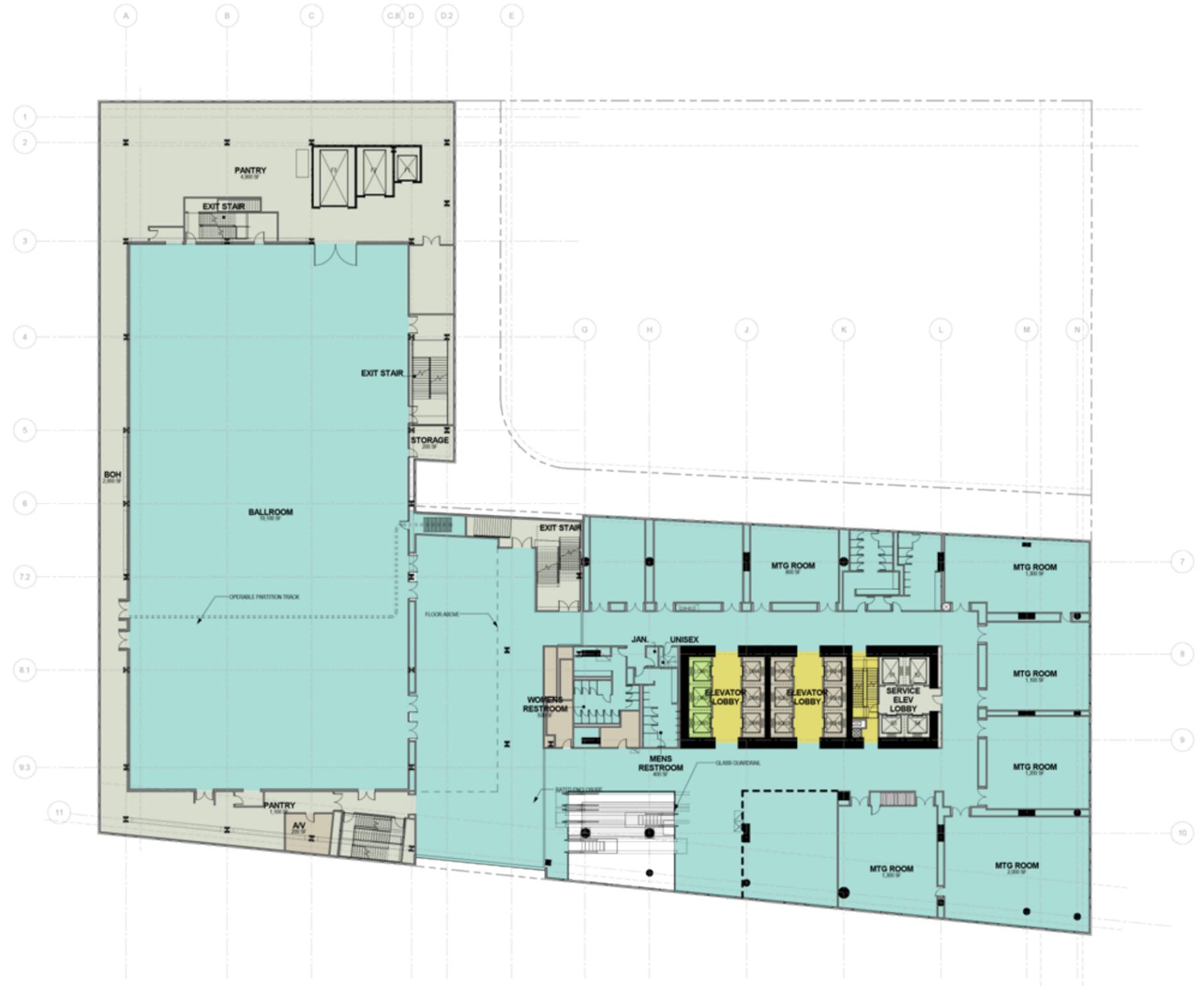
LEVEL 02 - HOTEL LOBBY AND RESTAURANT

The second level of the building houses the primary hotel check-in lobby, accessed from grade by escalators and elevators. To the north of this lobby space is the hotel restaurant, with a glass wall fronting on 8th Avenue and Stewart Street, allowing views out to the surrounding urban context. A mechanical space is located to the east above the loading dock.



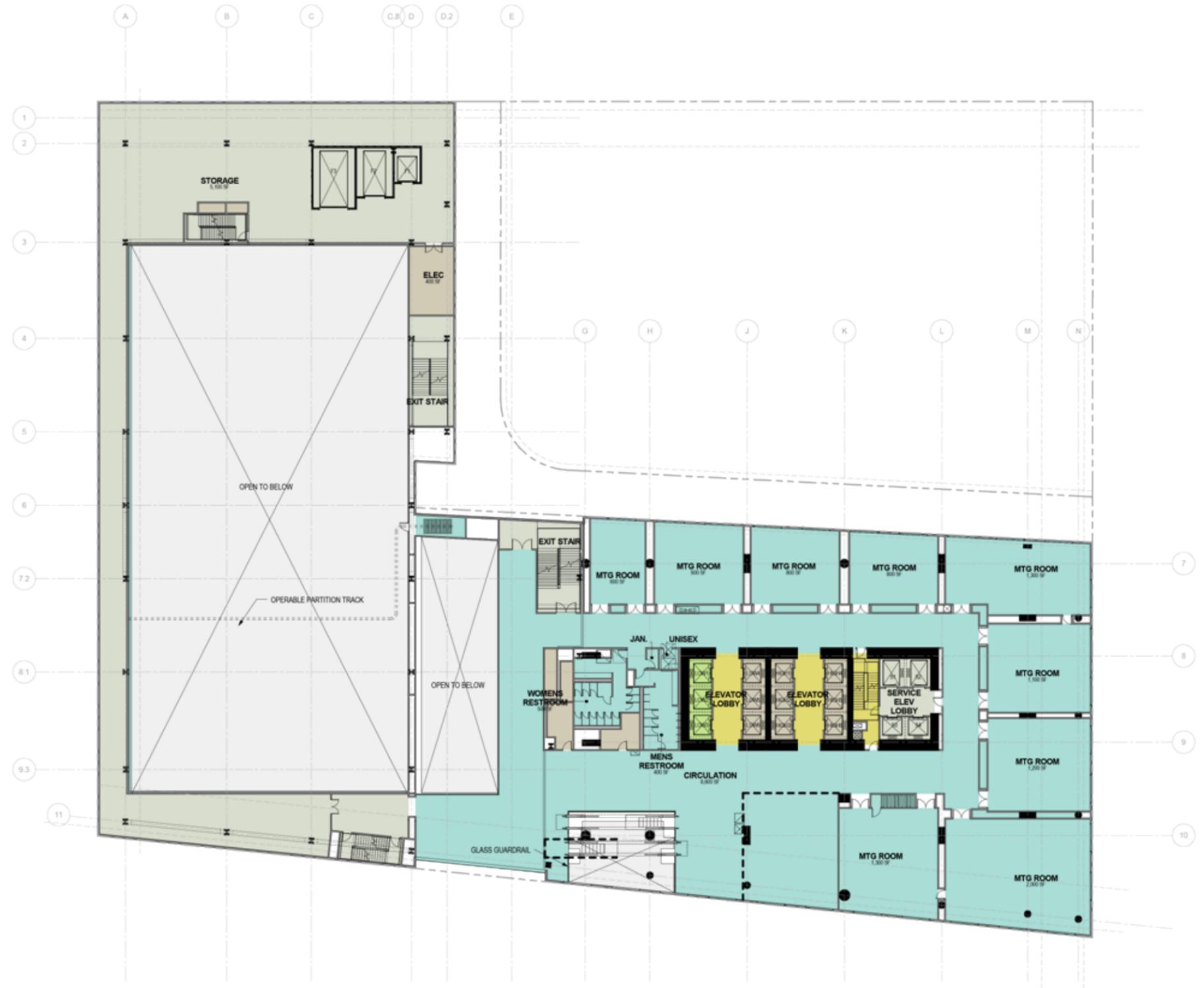
LEVEL 03 - BALLROOM AND MEETING

The third level is the first of two similar main ballroom levels, accessed by both a stack of escalators along 8th Avenue and the primary elevator core. On this level, a 19,000 square-foot ballroom is placed at the northern portion of the site and is surrounded by support and egress spaces. Meeting rooms surround the elevator core on the southern part of the floor. Between the ballroom and meeting rooms, a shared prefuction space with a full-height glass wall faces 8th Avenue, and exposes the activity within to the city beyond.



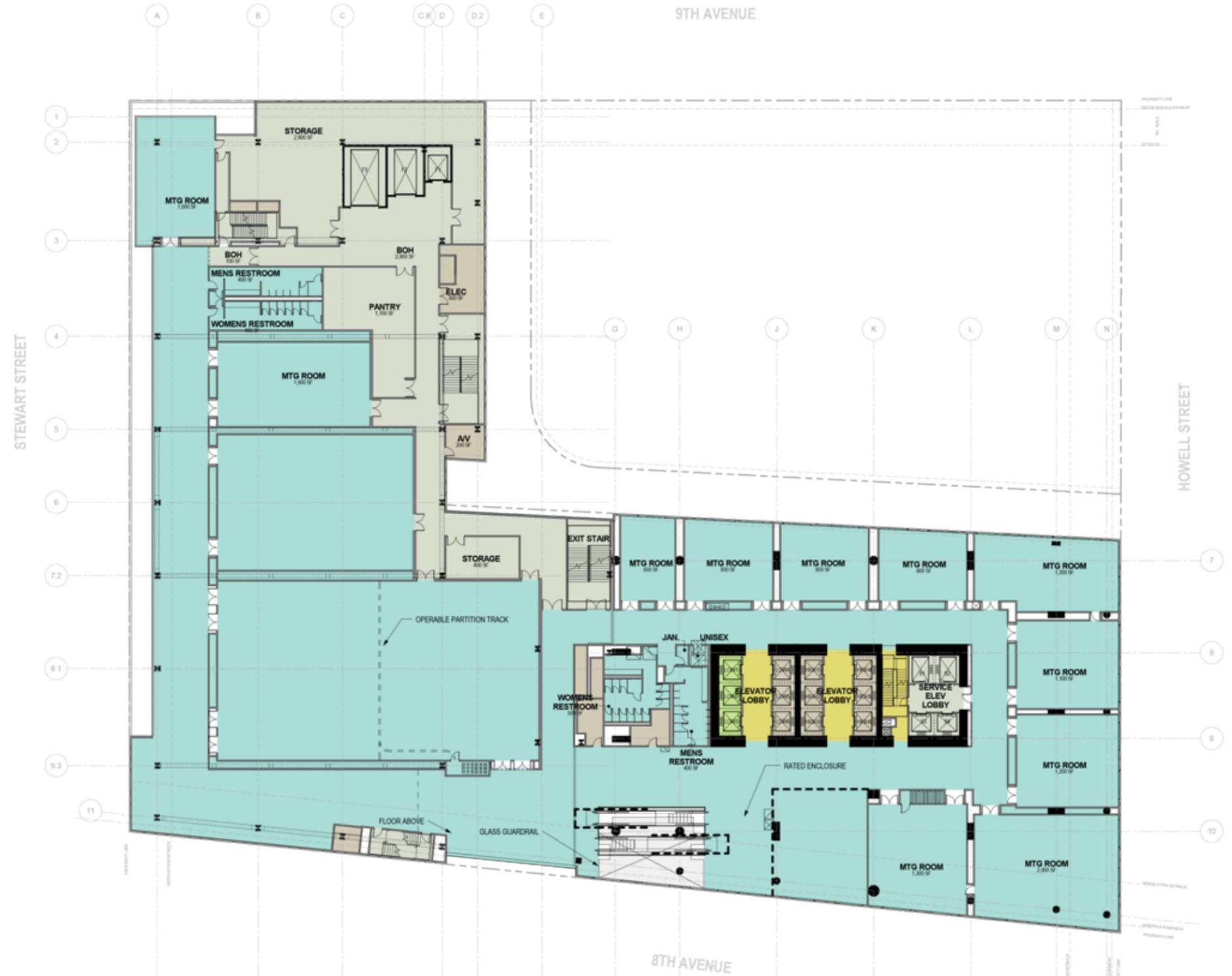
LEVEL 04 - MEETING

The fourth level is a partial level, housing meeting rooms around the primary core, similar to level 03. A prefunction balcony space overlooks the third level prefunction space. The remaining area on that level is dedicated to the double-height ballroom and its support spaces to the north.



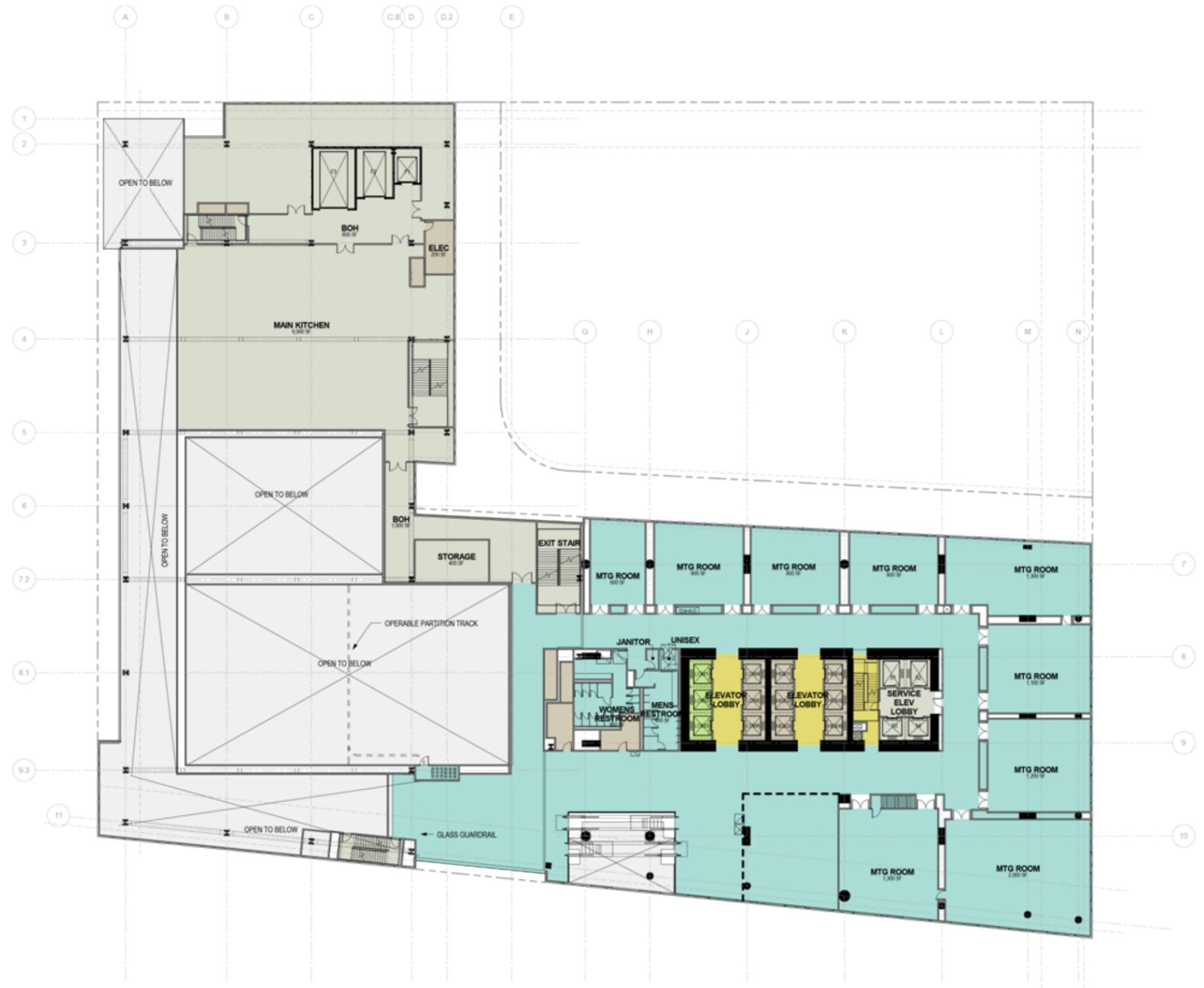
LEVEL 05 - JUNIOR BALLROOM AND MEETING

As in all of the podium levels, the fifth level contains meeting spaces surrounding the central core. Above the ballroom beneath, and housed within the large spaces between the long-span truss systems, a 7,000 square-foot junior-ballroom and two three smaller meeting spaces sit to the north. These are accessed by a prefunction space which engages the Stewart Street façade and features a double-height glass wall system.



LEVEL 06 - MEETING

The sixth level continues the theme of meeting spaces surrounding the central core. The double-height volume of the junior-ballroom, meeting and prefunction spaces separates the southern meeting space from the kitchen and support spaces to the north and east.



LEVEL 07 - BALLROOM AND MEETING

The seventh level is the highest level of the podium, and its program components and configuration are almost identical to those of level three.



LEVEL 08 - HOSPITALITY AND FITNESS

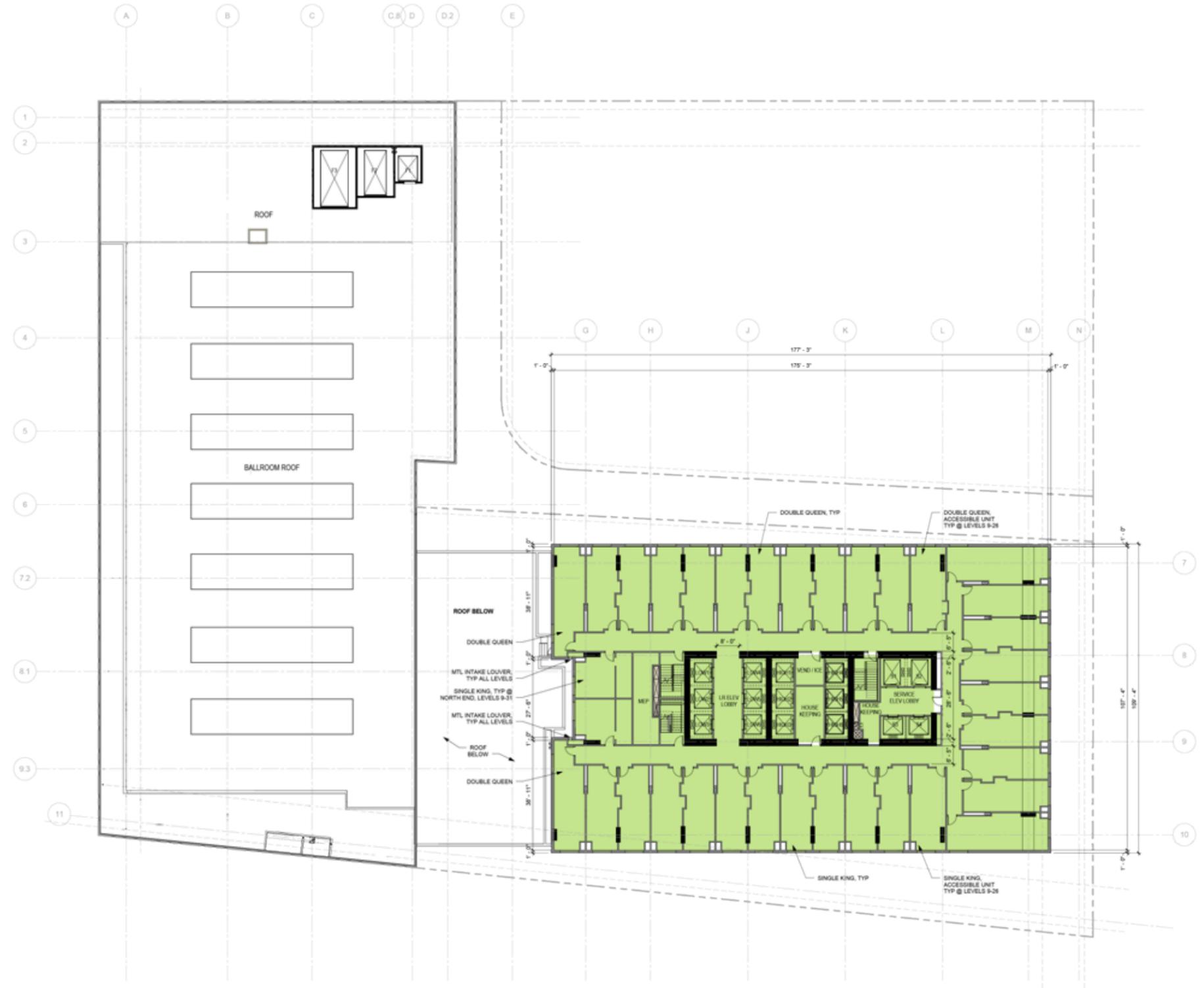
The eighth level is an intermediate space between the podium levels and the hotel tower, and as such contains uses and support elements for both components. The Howell Street and Eighth Avenue sides of the southern portion contain a fitness facility and a concierge lounge, and both of these uses will have access to an exterior terrace that is the roof of the podium beneath. The alley side of the southern portion is comprised primarily of mechanical spaces required to serve the tower and podium levels. To the north, the tall volume of the ballroom continues through this level.



LEVEL 09 - TYPICAL LOW-RISE HOTEL PLAN

The hotel tower consists of two primary zones. The low-rise zone occupies levels nine through thirty, while the high-rise zone occupies levels thirty-one through forty five.

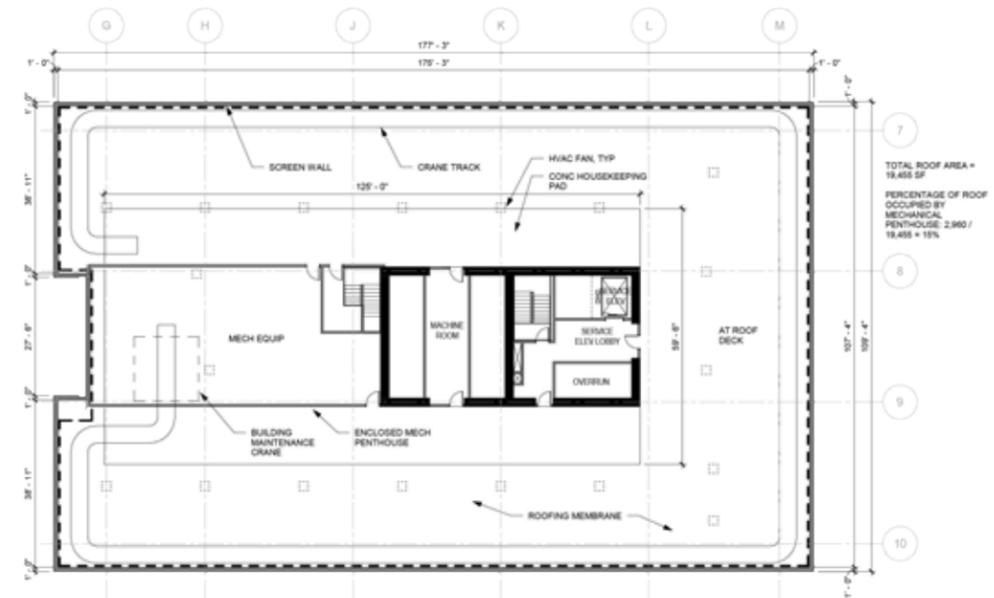
The low-rise levels contain a large central core created by the express elevators which serve the high-rise zone above. The northern edge of the tower accommodates this larger core by containing two wider and shallower rooms.



In the high-rise zone, a smaller central core allows longer rooms to the north. This difference between the plans of the low-rise and high-rise zones allows the tower form to remain uniform from bottom to top.

The top two levels of the tower contain larger suites, and the roof will be designed to facilitate window washing as well as house mechanical and elevator service elements.

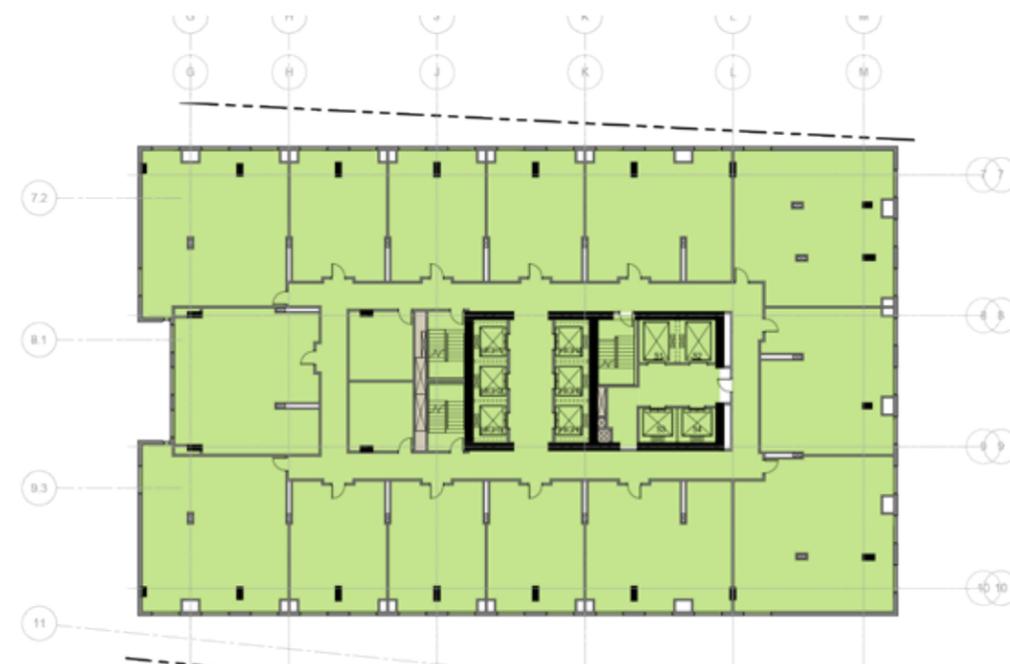
TOWER ROOF PLAN



LEVEL 32 - TYPICAL HOTEL HIGH-RISE PLAN



LEVEL 44 - TYPICAL HOTEL SUITES



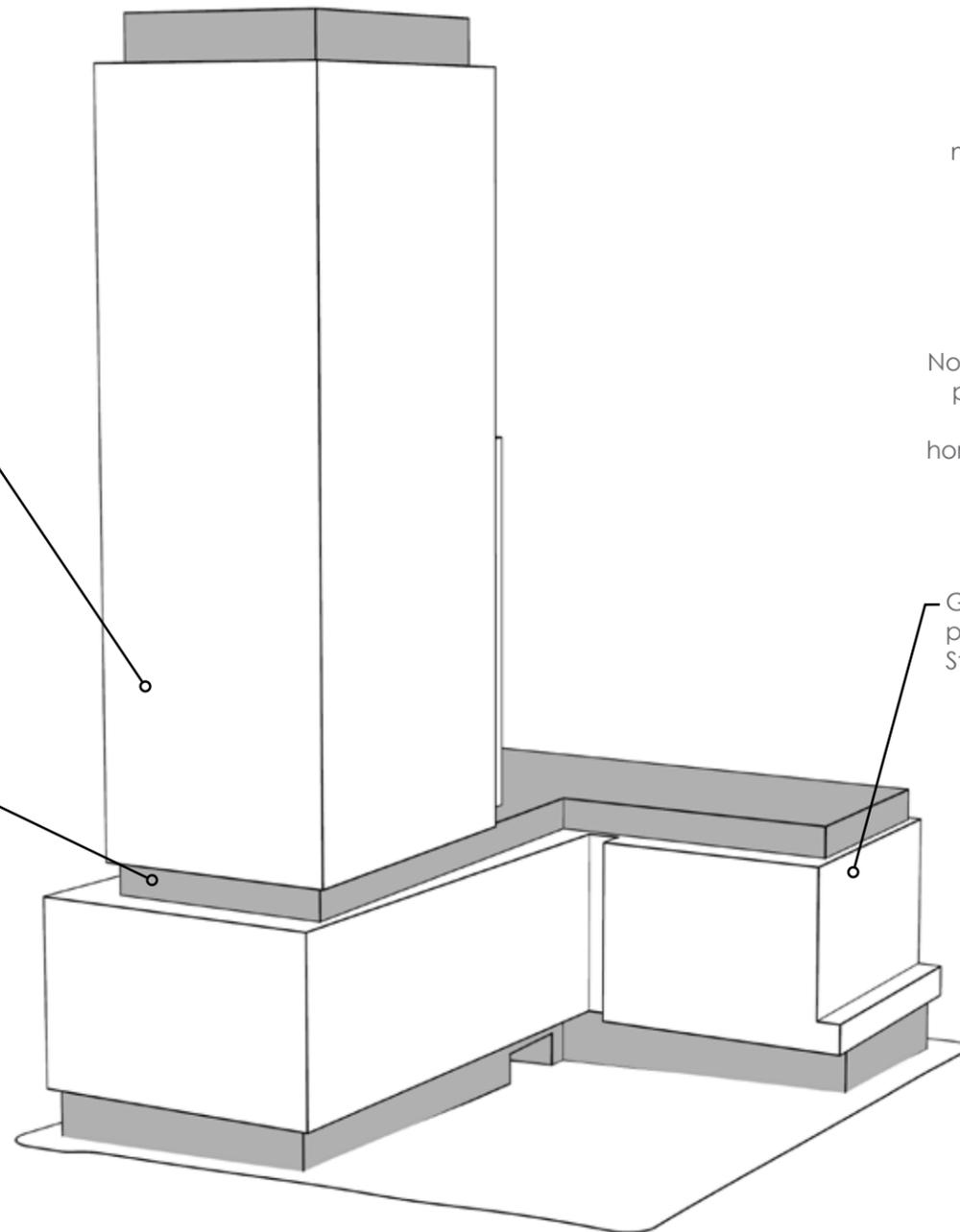
5 ENVELOPE DESIGN

PREVIOUS MASSING SCHEME

These two diagrams represent the previously reviewed building massing concept shown during the Early Design Guidance meeting on April 29, 2014. The board voiced support for the overall composition of the massing, particularly the placement of the tower to the south of the podium and the lower massing of the ballrooms to the north. It was suggested that the design team study a refined tower form, and that the appendage shown at the lower half of the tower was less desirable than a slightly wider but more singular tower form.

15-foot setback of tower from of southern property line breaks down scale of mass along Howell Street and emphasizes the unique tower form

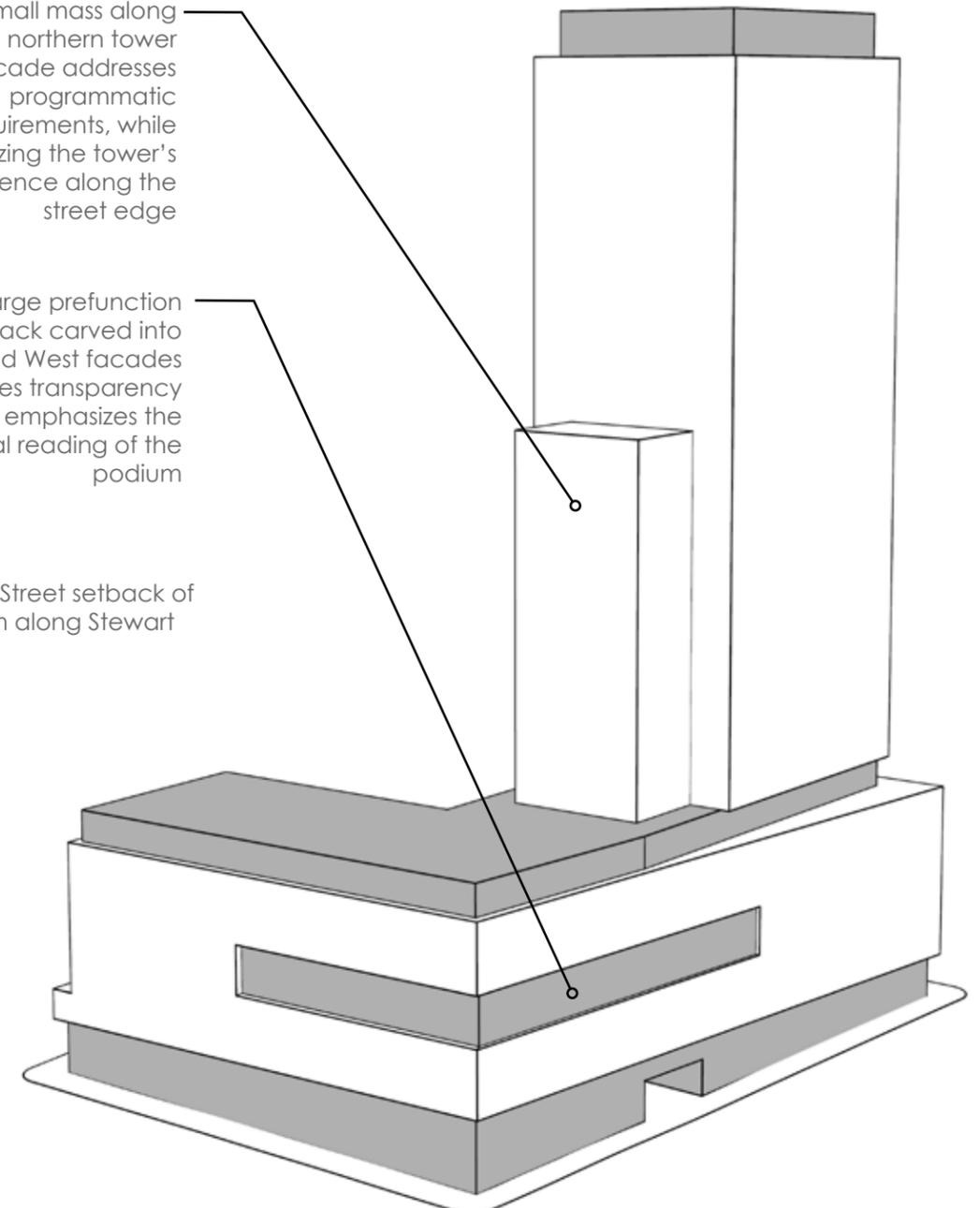
One-level material change, separates the tower and podium masses



Small mass along northern tower facade addresses programmatic requirements, while minimizing the tower's presence along the street edge

Large prefunction setback carved into North and West facades provides transparency and emphasizes the horizontal reading of the podium

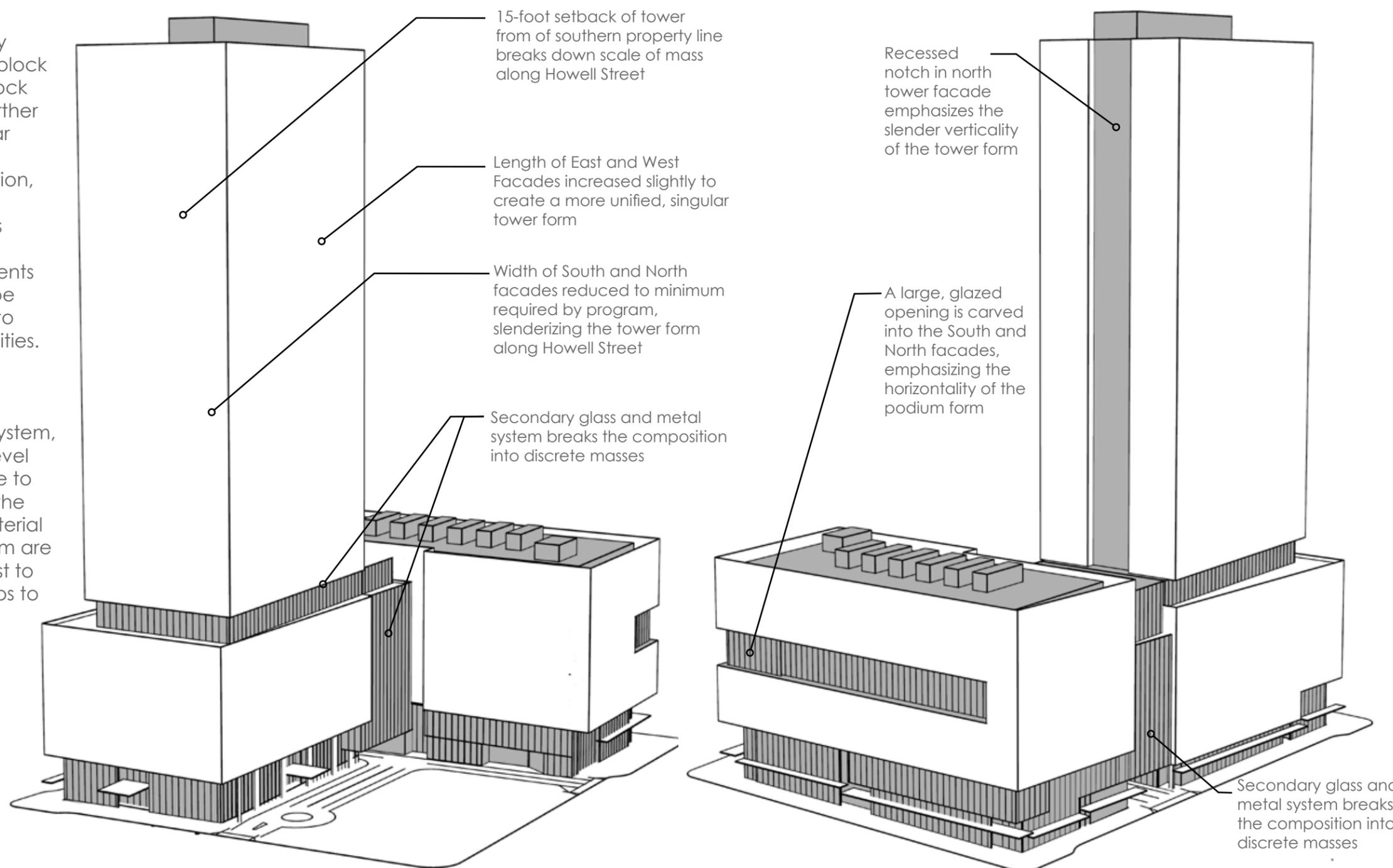
Green-Street setback of podium along Stewart Street



REVISED MASSING SCHEME

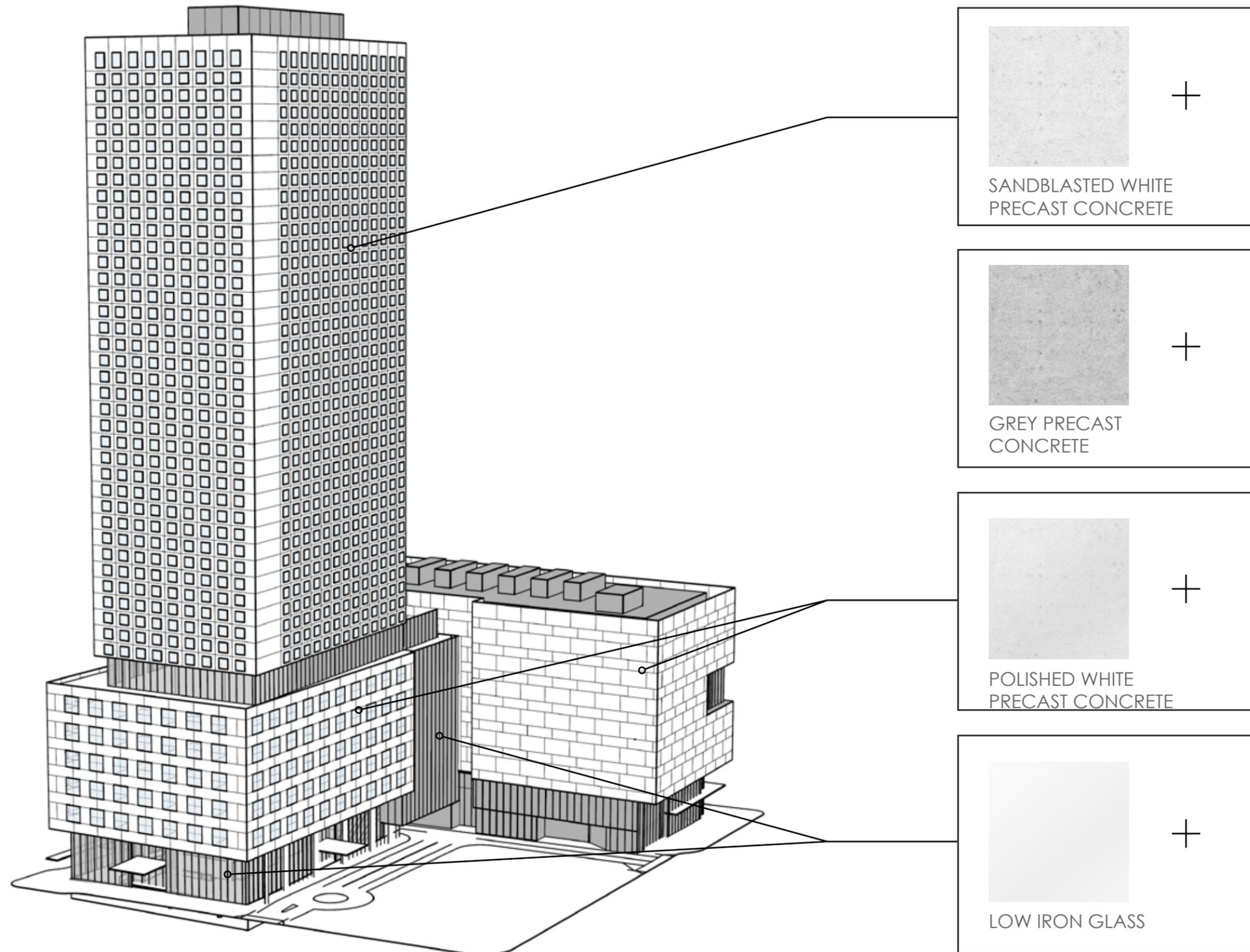
The revised massing approach is shown here in the accompanying diagrams. This refined composition further articulates the complexity of programmatic elements within the building. The podium is now divided vertically into two primary components: the meeting room block to the south and the ballroom block to the north. The tower form is further refined and simplified to a singular rectangular form, with a vertical modulation along its north elevation, integrating mechanical venting in this formal move which echoes the horizontal modulation along Stewart Street. These three elements are expressed as solids, and will be clad in a white concrete system to accentuate their monolithic qualities.

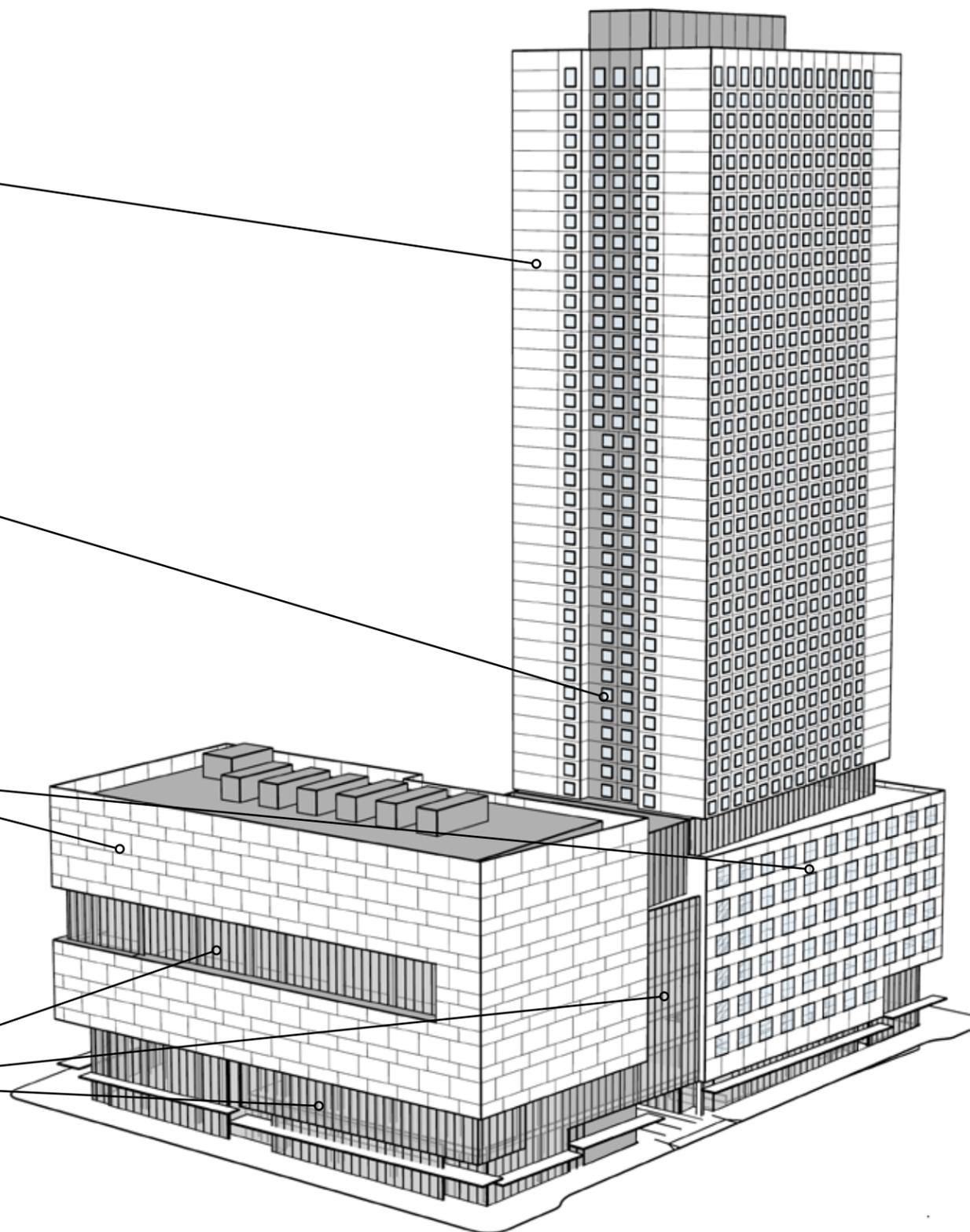
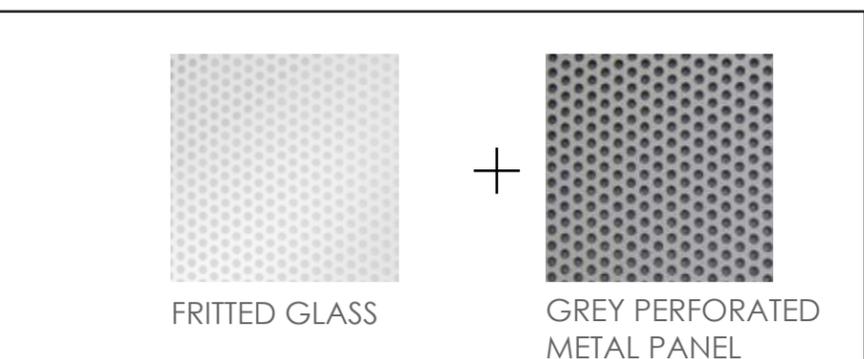
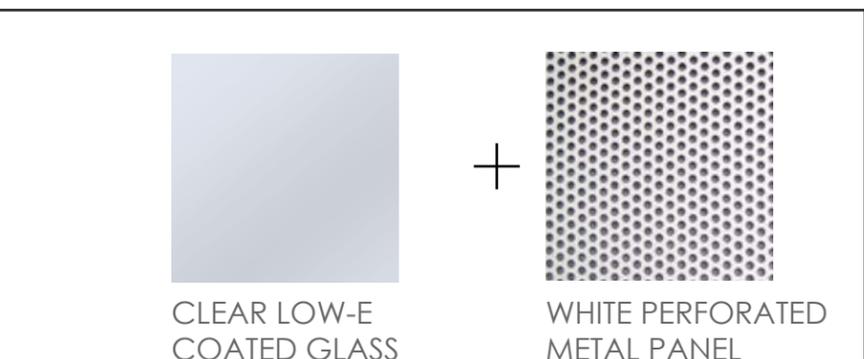
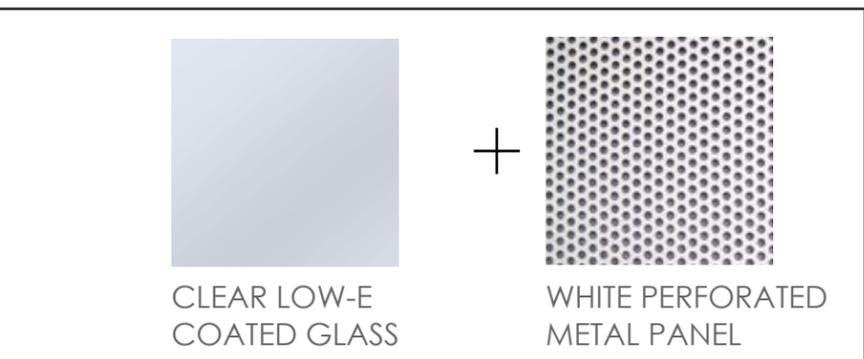
The three primary solid concrete elements are separated by a continuous transparent façade system, which migrates from the grade level up through this prefunction space to the intermediate zone between the podium and tower. Both the material quality and the scale of this system are intended to stand in stark contrast to the simple monolithic forms it helps to frame.



MATERIAL PALETTE

As described in the massing concept on the previous page, the material palette is intended to respond primarily to the building's formal composition. A series of clear glass elements at the base and around the podium of the building allow the monolithic solids of the program volumes to read as a strong, geometric composition. Moreover, it is intended that this predominantly white and grey building will become a new iconic landmark and contribute to the rich urban form of the surrounding neighborhood.





SOUTH ELEVATION ON HOWELL STREET

The Howell Street elevation is designed to create a strong presence of the hotel and convention components facing the expanding convention district to the south. The exceptionally tall ground floor space will be about 25 feet tall, and will be fully glazed in clear glass beneath the large cantilevered podium above, accentuating the height of the welcoming interior volumes within. The front door of the building is located in the center of the property edge along Howell Street.

Above the transparent grade level, the solid volume of the meeting spaces is expressed in a monolithic but minimal volume with square, flush glass windows that will be in-plane with the polished white concrete wall surface. The hotel tower face is set back fifteen feet from the Howell street property line, accentuating the formal juxtaposition of the tall tower above and the podium below.

The south facing elevation on the alley is intended to be a minimal but refined wall of polished white concrete, concealing the support and egress elements in the volume within. This façade uses a simple, yet defined, coursing pattern of panels that recall the proportions of the concrete and glass system of the remainder of the podium. The hovering solid is set above the articulated metal and glass elements of the ground level service entry, whose gated entry elements will be concealed from direct view down the length of the alley.

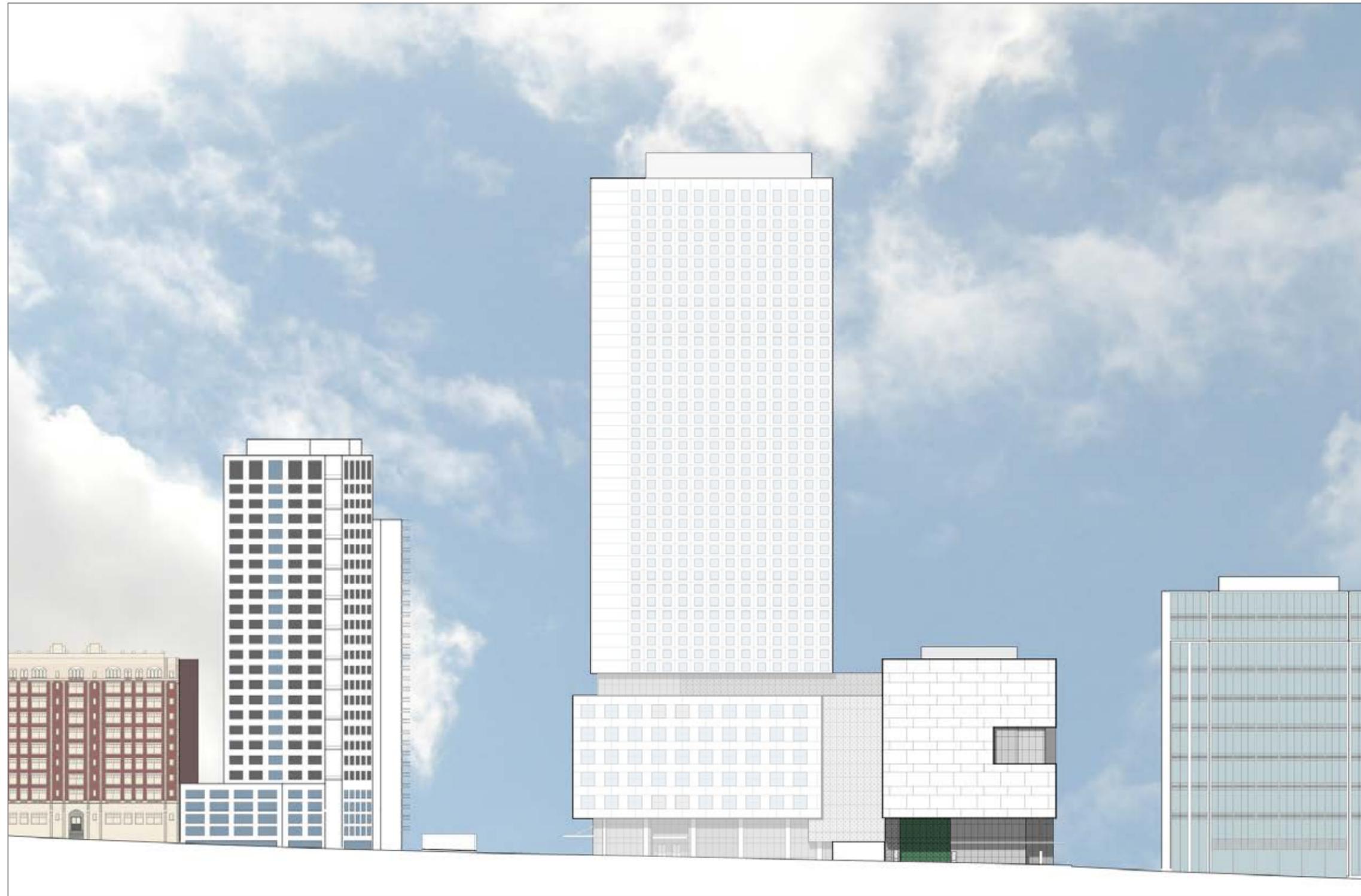




VIEW NORTHEAST FROM 8TH AVENUE

EAST ELEVATION ON 9TH AVENUE

The east elevation on 9th Avenue is a minimal composition which places a hovering carved solid above a setback and welcoming widened sidewalk space at the corner of 9th and Stewart. The grade level design will incorporate a landscape feature where internal support elements would preclude the use of transparent glass. Along the northern half of the street level, the façade will be completely glazed and accessible to host a corner retail space within. The upper elements of the 9th Avenue elevation respond to the predominantly support spaces for the ballroom elements within.



EAST ELEVATION ON ALLEY

The east elevation facing the alley continues the theme of the hotel tower/podium/ground floor established on Howell Street and 8th Avenue, bringing full-height clear glass at-grade along the full length of the alley. To facilitate a more spacious and functional drop off, this layer of glass is set back between 9 feet and 19 feet from the required 2 foot setback. Above, the polished white concrete and glass podium element cantilevers over a portion of the drop off, providing overhead weather protection as well as accentuating the formal massing composition of the building. The hotel tower above is distinguished by its juxtaposition of a field of square glazing with a solid band of sandblasted concrete to the north.

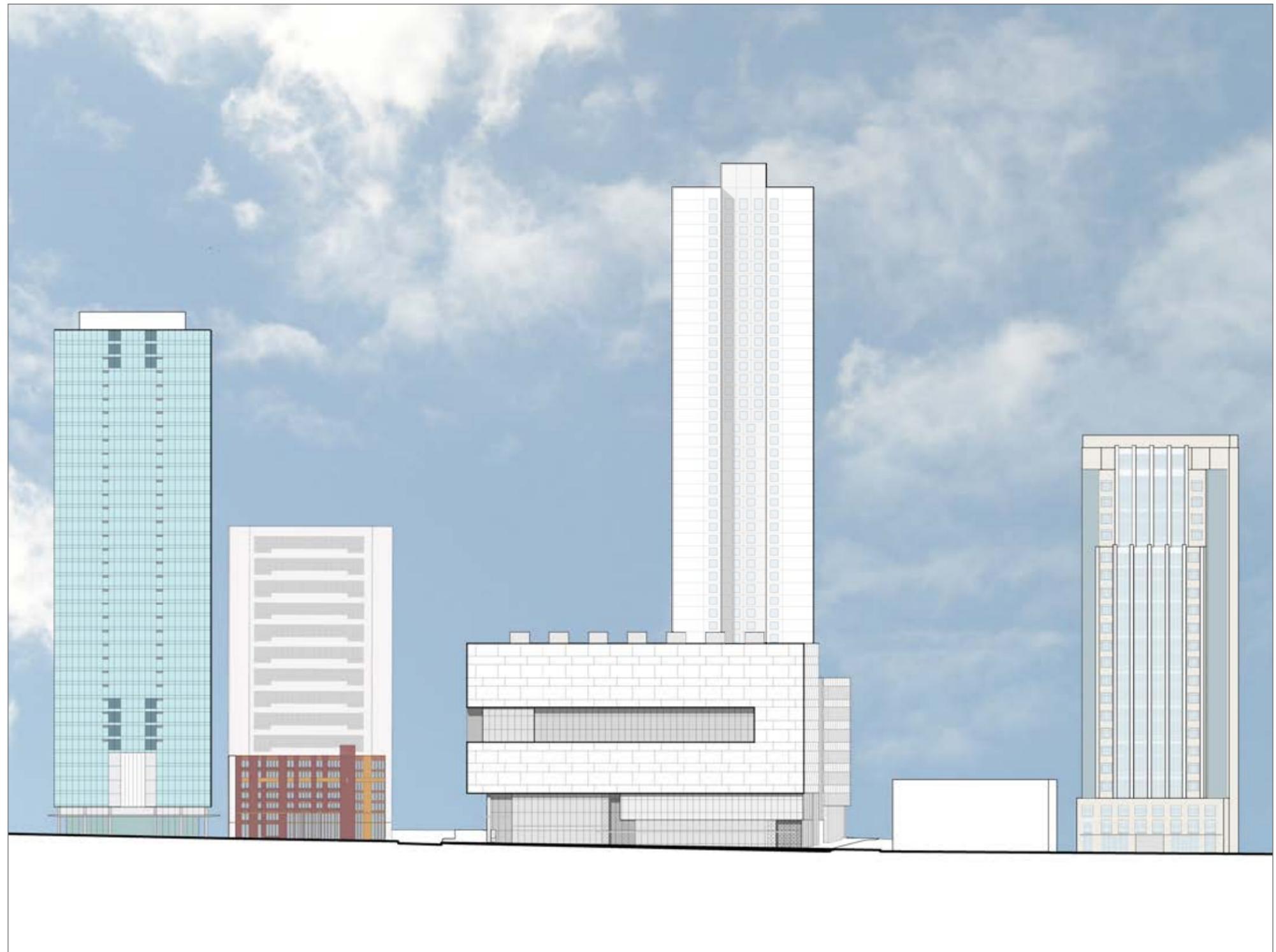


VIEW NORTHWEST FROM HOWELL STREET TO THE ALLEY DROP-OFF

NORTH ELEVATION ON STEWART STREET

The Stewart Street elevation is designed as a counterpoint to the Howell Street elevation, presenting a strong horizontal reading in contrast the bold vertical form of the tower and hovering podium found on Howell Street. The grade level design of the Stewart Street elevation continues the transparent and layered glass language found on the other primary street facades, exposing and activating the retail spaces within. The polished white precast concrete podium above uses a long, horizontal carve to reveal the meeting-level prefunction space on level five. At the corner of 9th and Stewart, a glazed board-room will punctuate the northern end of this long horizontal gesture.

The simplicity of the tower form gives way to a more articulated language on the north face of the tower. A seven foot setback will run the full height of the tower, and will be articulated by the use of a grey colored sandblasted concrete surface, and is intended to respond to the horizontal gesture of the Stewart Street podium. The top of this element will terminate in a tall parapet wall and mechanical penthouse.

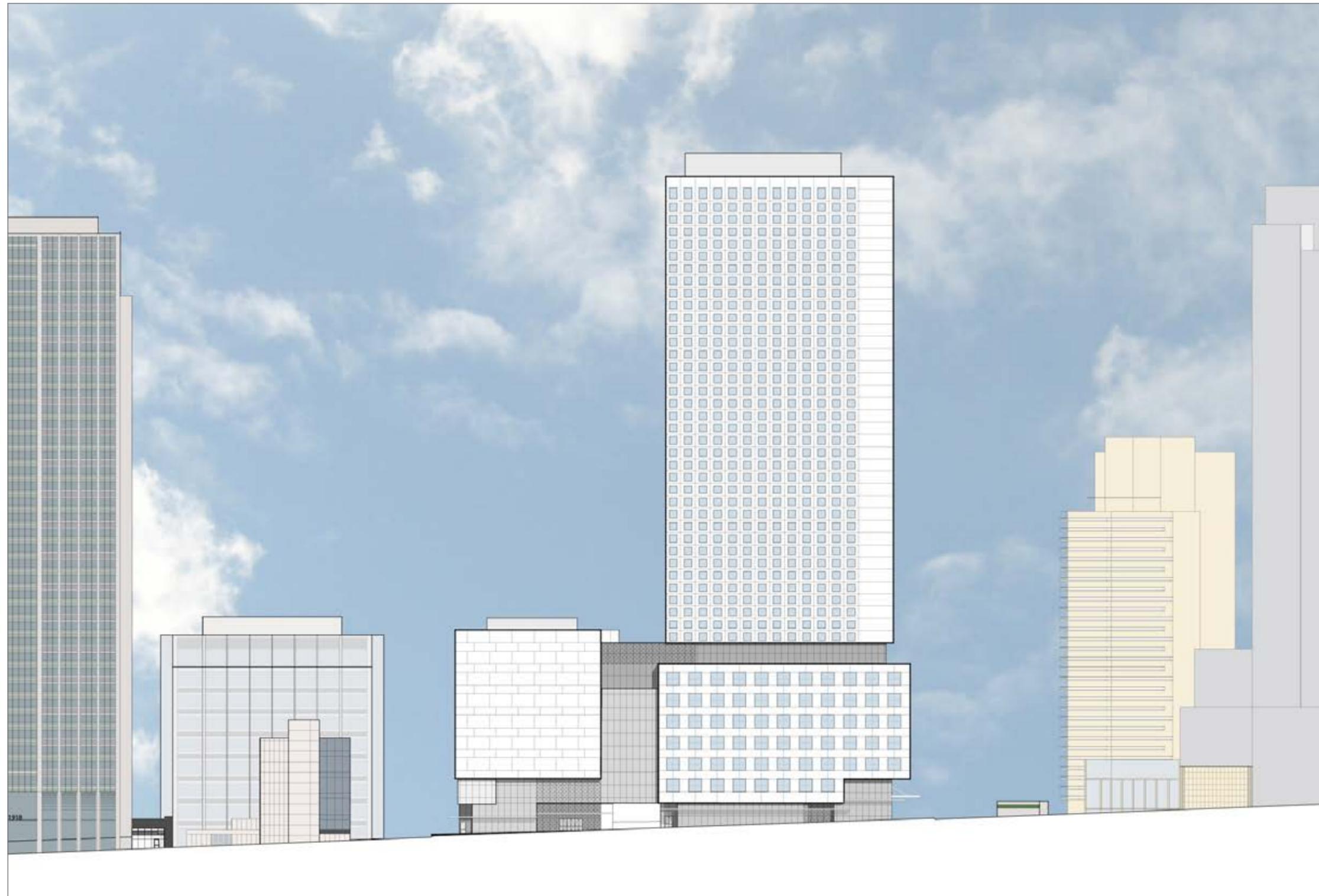




VIEW SOUTHWEST FROM 9TH AVENUE AND STEWART STREET

WEST ELEVATION ON 8TH AVENUE

The west elevation facing 8th Avenue offers a varied and holistic view of the building composition. From this vantage point, all three primary solid concrete forms are visible, and their languages present a different variant of a similar material theme. The northern form of the ballroom block will be a minimal rectangle clad in polished concrete, while the southern form of the meeting block will be a polished concrete solid carved by the Howell Street main entry to the south. Above, the tower form will set an ordered array of square glass openings with the glass flush to the sandblasted concrete surface, and a solid band of concrete that runs the full height along the northern edge.



WEST ELEVATION ON 8TH AVENUE (CONTINUED)

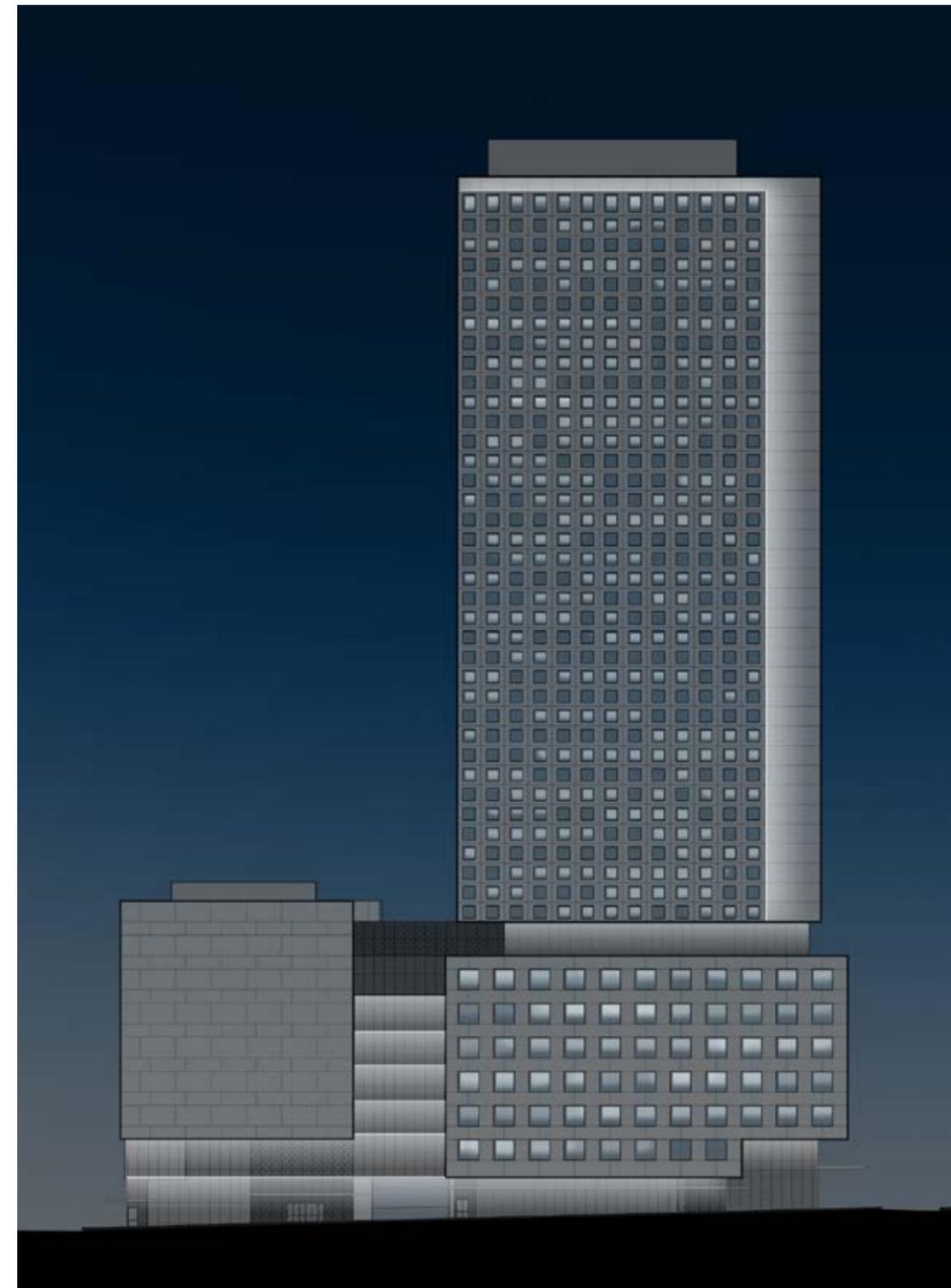
Between the solid masses, the secondary glass and metal system presents an articulated language that is a contrast from the solid concrete forms surrounding it. This glass language is comprised of a series of fractured planes, responding to the programmatic components within and creating a rich layering of surfaces that frame a variety of entries and internal public activities. The bold, vertical transparent element between the ballroom and meeting room components serves to modulate the full-height of the podium and presents the activities of the prefunction spaces within to the street level below.

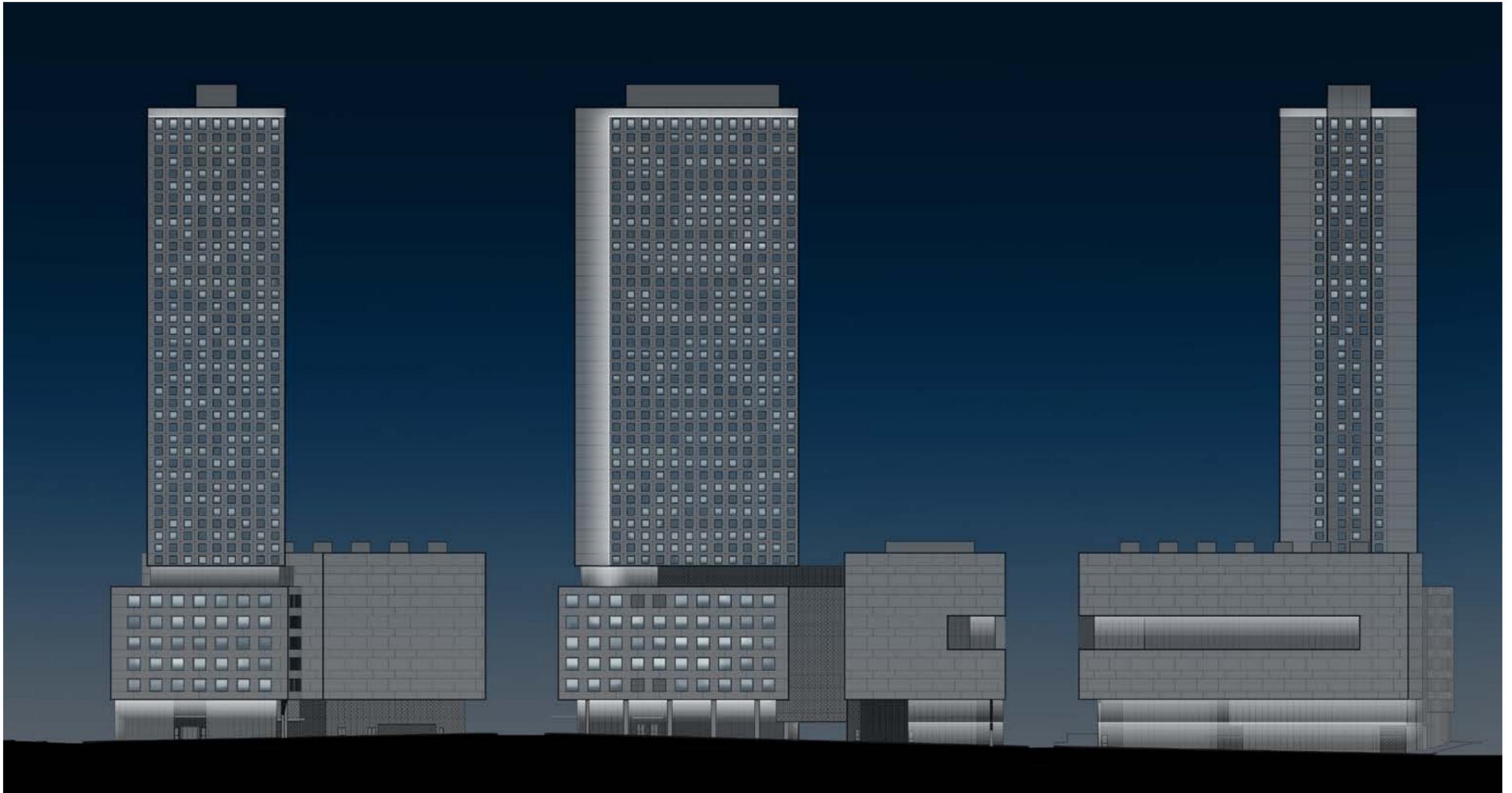


VIEW NORTHEAST FROM 8TH AVENUE AND STEWART STREET

EVENING ELEVATIONS PROPOSED FACADE LIGHTING

The proposed design seeks to use evening lighting to accentuate the unique tower form. Surface-mounted lights will wash the solid edges of the tower form as they rise from the top of the podium, and continue along the top of the tower façade parapet. Lighting from the heavily glazed spaces within will further accentuate the formal composition of the podium and tower elements, highlighting the joints between these solid components.





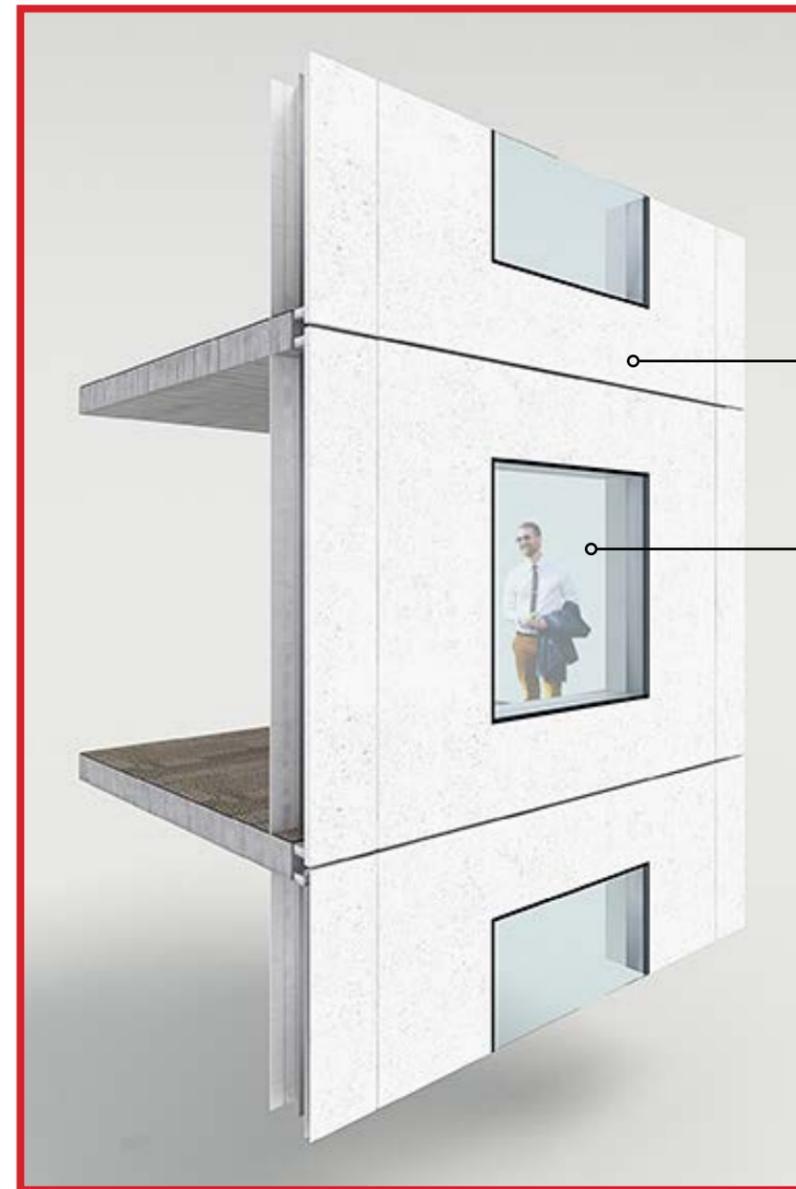
AERIAL VIEW LOOKING NORTHWEST

HOTEL FAÇADE MATERIAL SYSTEMS

The hotel tower will be clad in a solid system of sandblasted precast concrete and feature a regular grid array of punched-openings which will be deliberately minimal and planar. The intent of this system is to emphasize the tall solid form of the tower in contrast to the podium solid elements and intermediate glass.

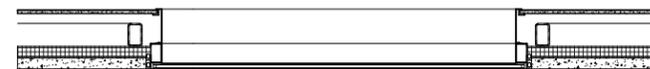
The glass located within the precast façade will be comprised of square openings which appear frameless from the exterior, and will be flush with the surrounding concrete surfaces. The precast concrete panel system will have an orthogonal language of expressed joints whose proportions are derived from the hotel and residential unit modules above. The sandblasted nature of the concrete will set this tall solid apart in a subtle textural manner from the podium solids beneath.

In the northern vertical recess of



WHITE
PRECAST
CONCRETE

CLEAR LOW-E
COATED
GLASS



PLAN DETAIL: TYPICAL WINDOW



VIEW OF THE SOUTH FACADE FROM 8TH AVE. AND HOWELL ST.



GREY PRECAST CONCRETE

TINTED LOW-E COATED GLASS



VIEW OF THE NORTH FACADE FROM 9TH AVE. AND STEWART ST.

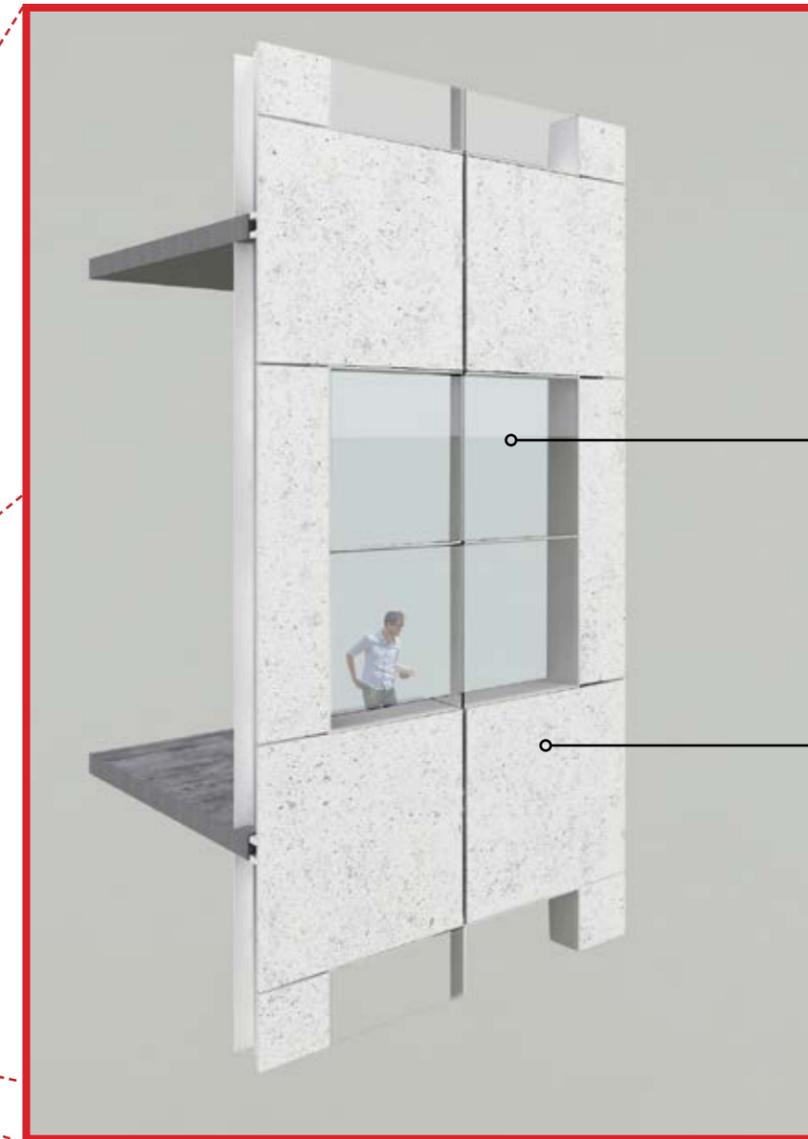
the tower, the precast concrete panels will be tinted grey to accentuate the massing elements, and the glass contained within will have a grey tint to allow the glazing and concrete to appear more uniform.

PODIUM: MEETING ROOM BLOCK MATERIAL SYSTEMS

The mass of the meeting room block will be clad in a polished white concrete. This material is similar to terrazzo in sheen and texture, and will lend a unique reflective quality to the podium volumes. The large square glass openings within the polished concrete façade will be arrayed in a uniform rectangular grid, and will appear flush to the polished concrete surface. The intent is to achieve a rich and varied appearance to the building. Throughout the day, the wall will at times appear almost uniform due to the planar reflectivity of both glass and concrete. At other times, particularly at night, the transparent punched openings will serve to dissolve the reading of the uniform surface, and animate the façade with the activity inside.



VIEW OF THE SOUTH FACADE FROM 8TH AVE.

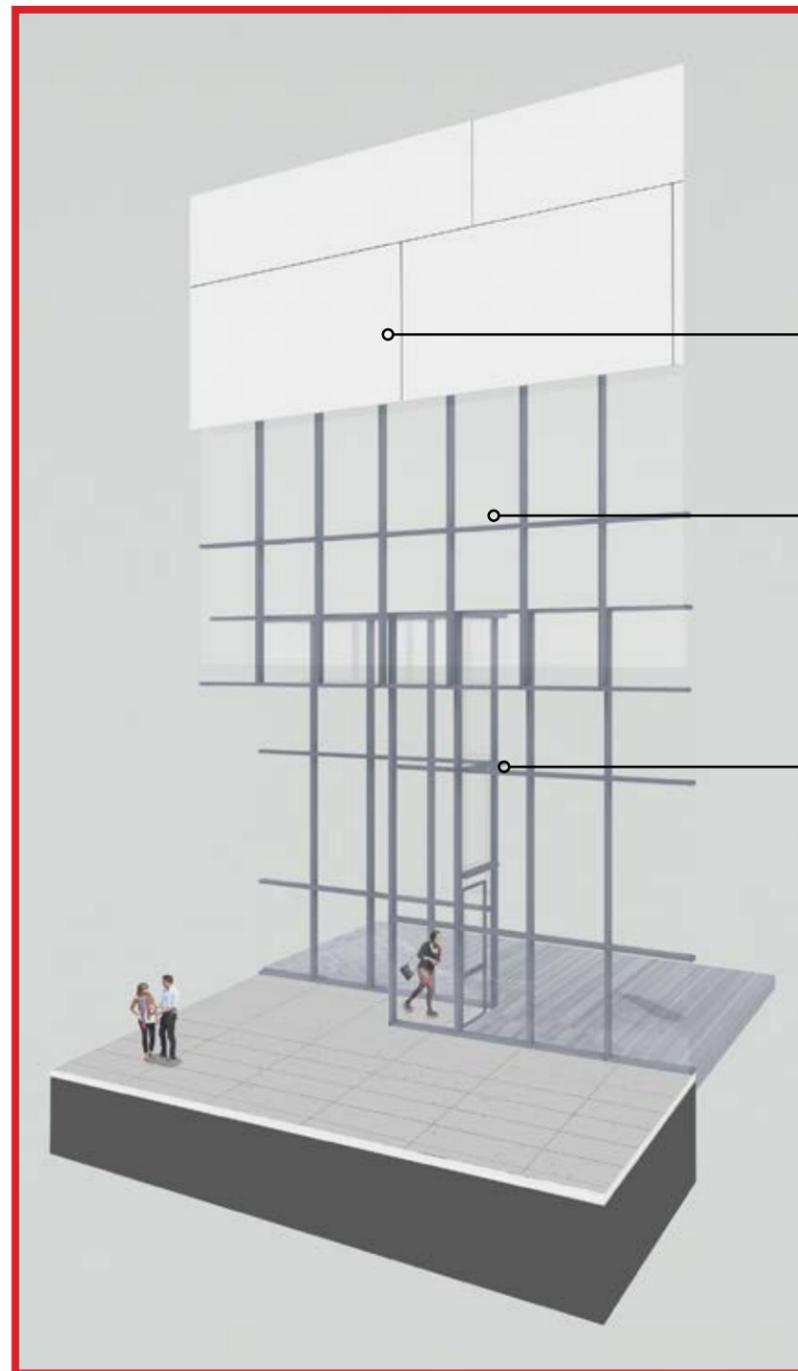


CLEAR
LOW-E
COATED
GLASS

WHITE
PRECAST
CONCRETE

GROUND FLOOR GLAZING

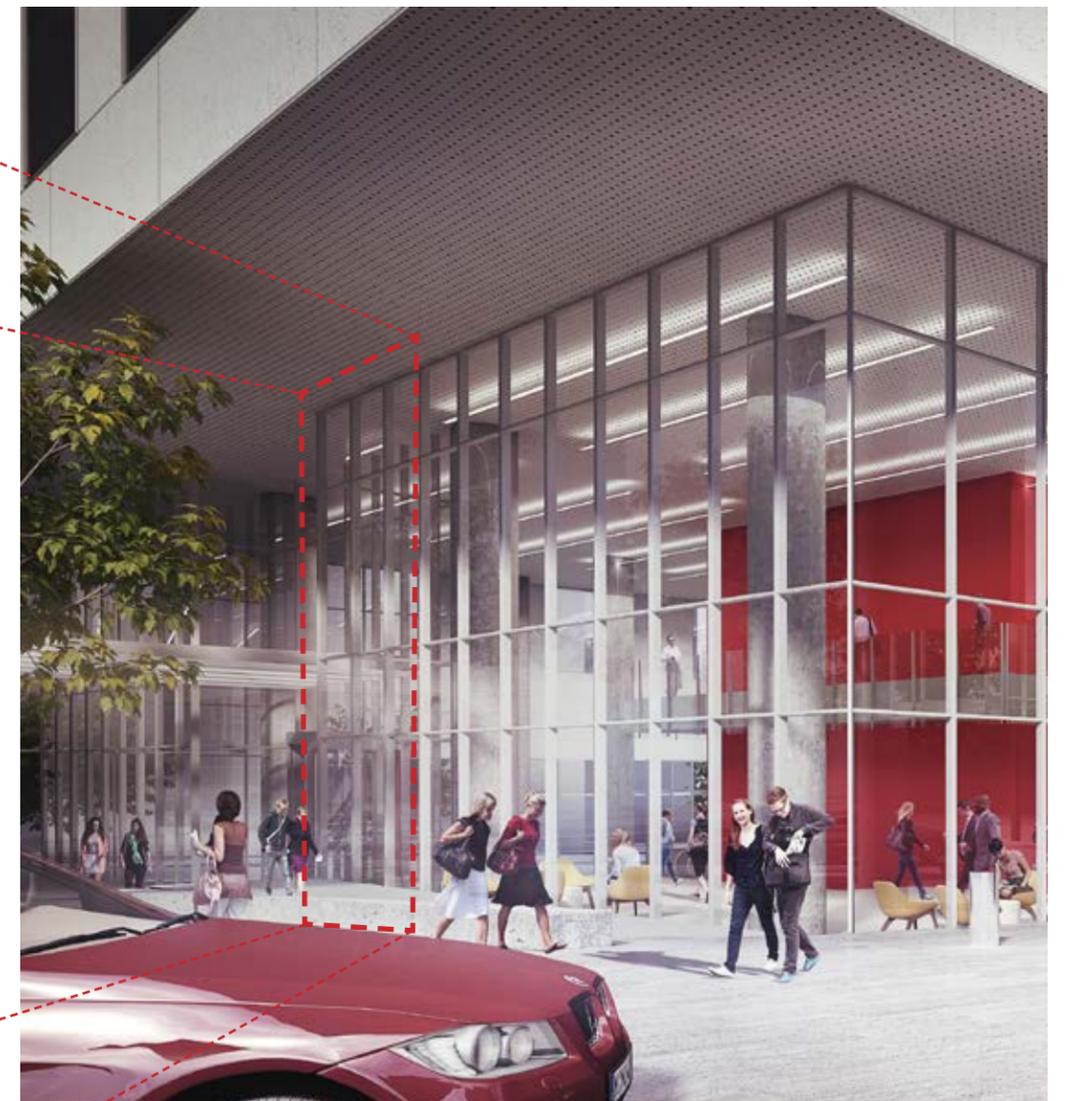
The material system of the ground floor and interstitial spaces is intended to stand in sharp contrast to the minimal volumes of the precast concrete systems above. By breaking up the glazing forms into smaller planar elements, and using low-iron transparent glass, the system is intended to read as a minimal transparent set of layers between the exterior public space and the interior public space. Anodized aluminum mullions will articulate framed low-iron glass to achieve this delicate and rich reading.



WHITE PRECAST CONCRETE

LOW IRON GLASS

CLEAR ANODIZED ALUMINUM MULLIONS



VIEW SOUTHEAST GLAZING FACADE FROM AND HOWELL ST.

NORTH PREFUNCTION GLAZING

The glazing system at the north prefunction space will use low-iron glass and clear anodized aluminum mullions to recall the thin and transparent reading expressed at the ground floor.



VIEW OF THE NORTH FACADE FROM 9TH AVE. AND STEWART ST.

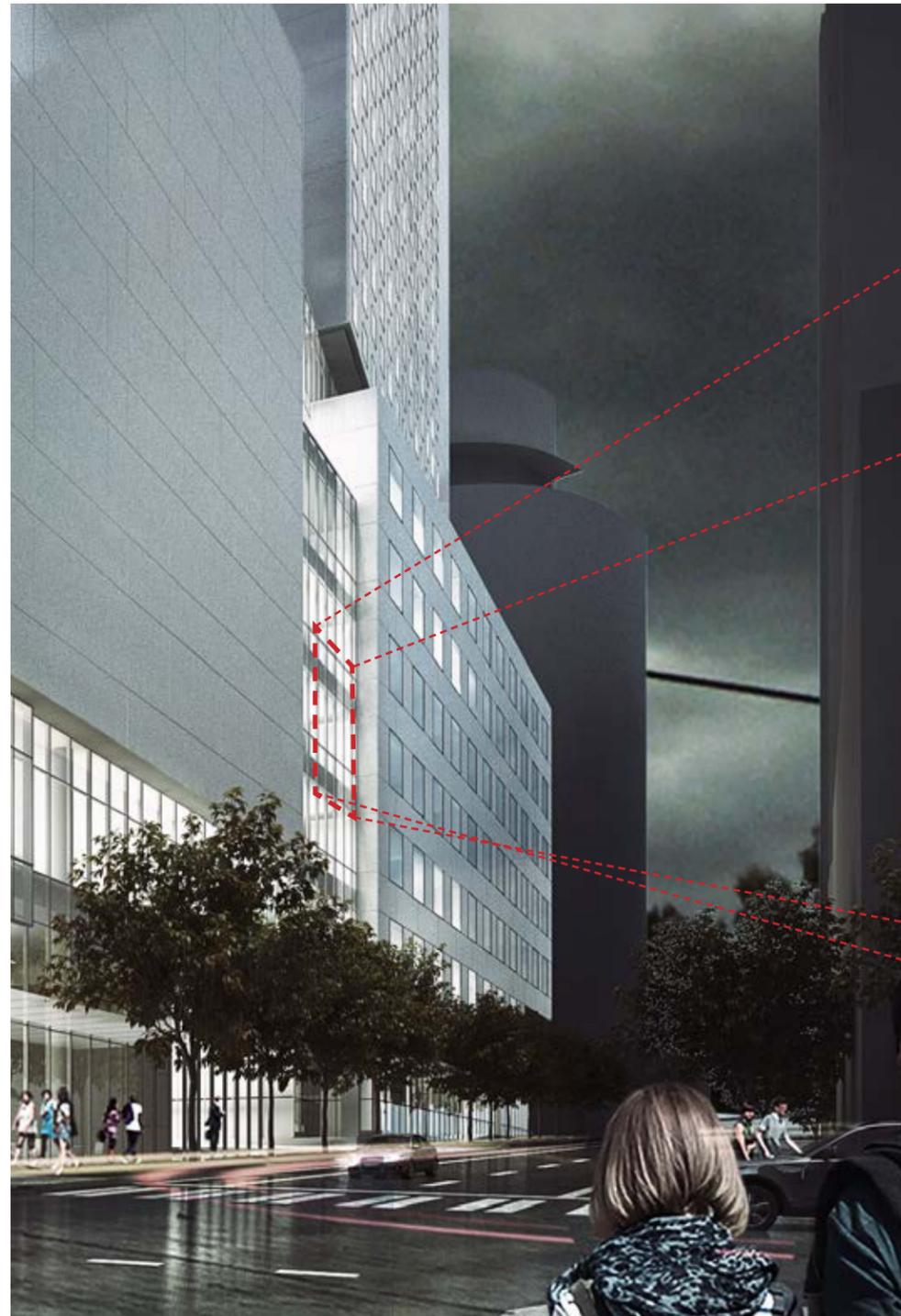
WHITE PRECAST CONCRETE

LOW-IRON GLASS WINDOW WALL SYSTEM



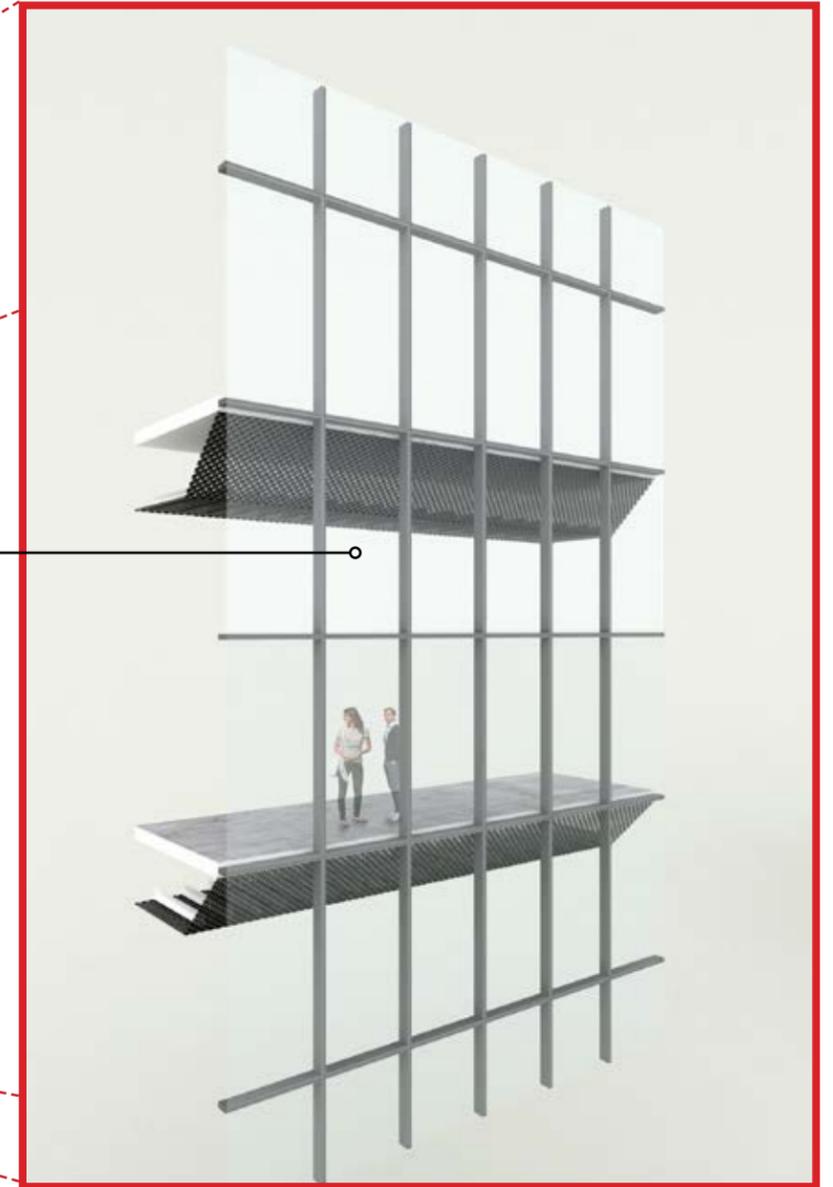
WEST PREFUNCTION GLAZING (VERTICAL CIRCULATION)

The glazing system at the west prefunction space will use low-iron glass and clear anodized aluminum mullions to recall the thin and transparent reading expressed at the ground floor, and the increased transparency at this location will help emphasize the break between the two primary podium concrete masses.



VIEW OF THE EAST FACADE FROM 9TH AVE. AND HOWELL ST.

LOW IRON GLASS



6 GRADE-LEVEL DESIGN

GRADE-LEVEL DESIGN

The revised grade-level design is inspired



Platanus acerifolius
Located on Howell Street and 8th Avenue



Tilia tomentosa
Located on Howell Street and 8th Avenue

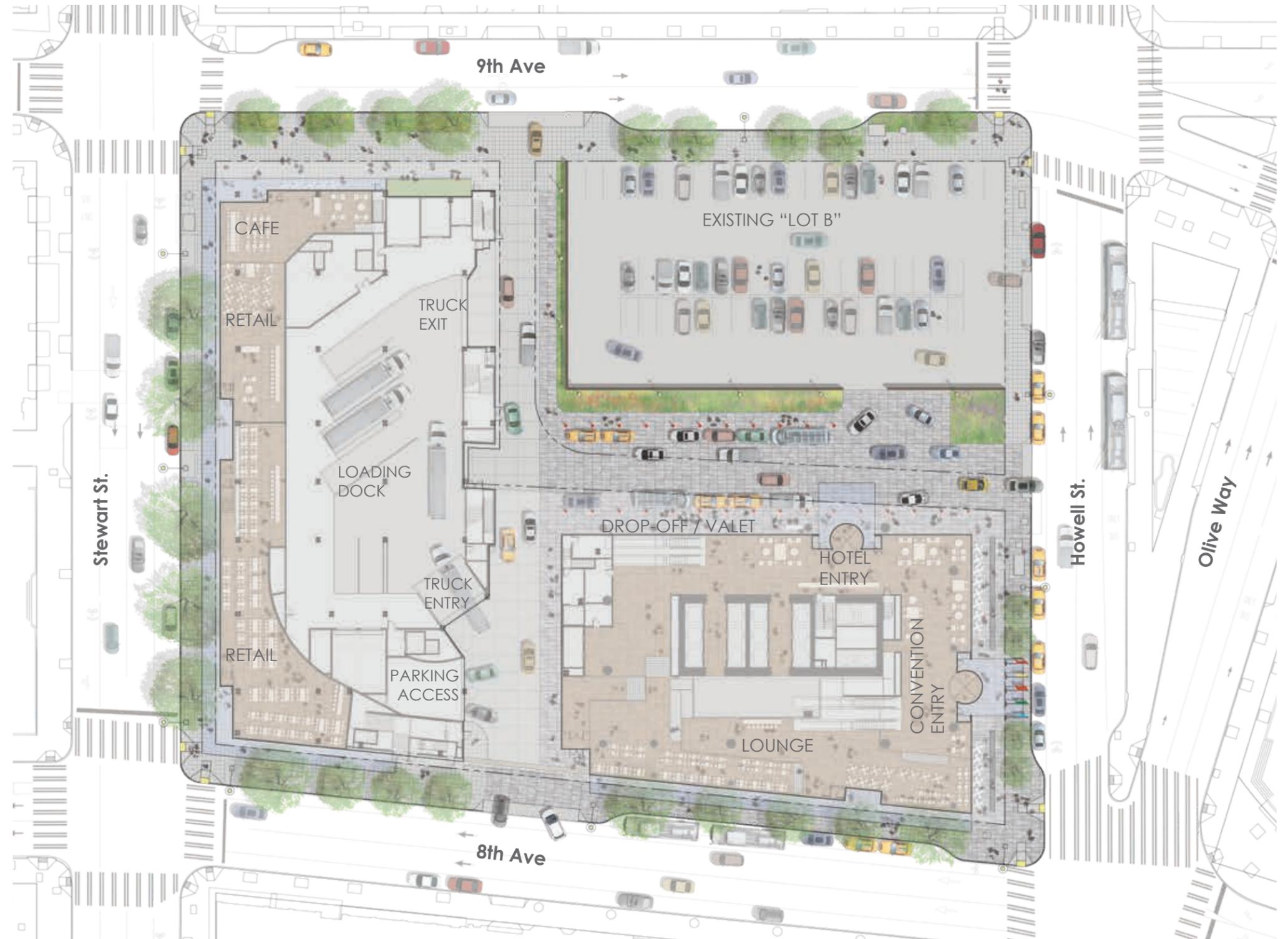


Carpinus betulus
Located on 9th Avenue



Zelkova serrata
Located on 9th Avenue

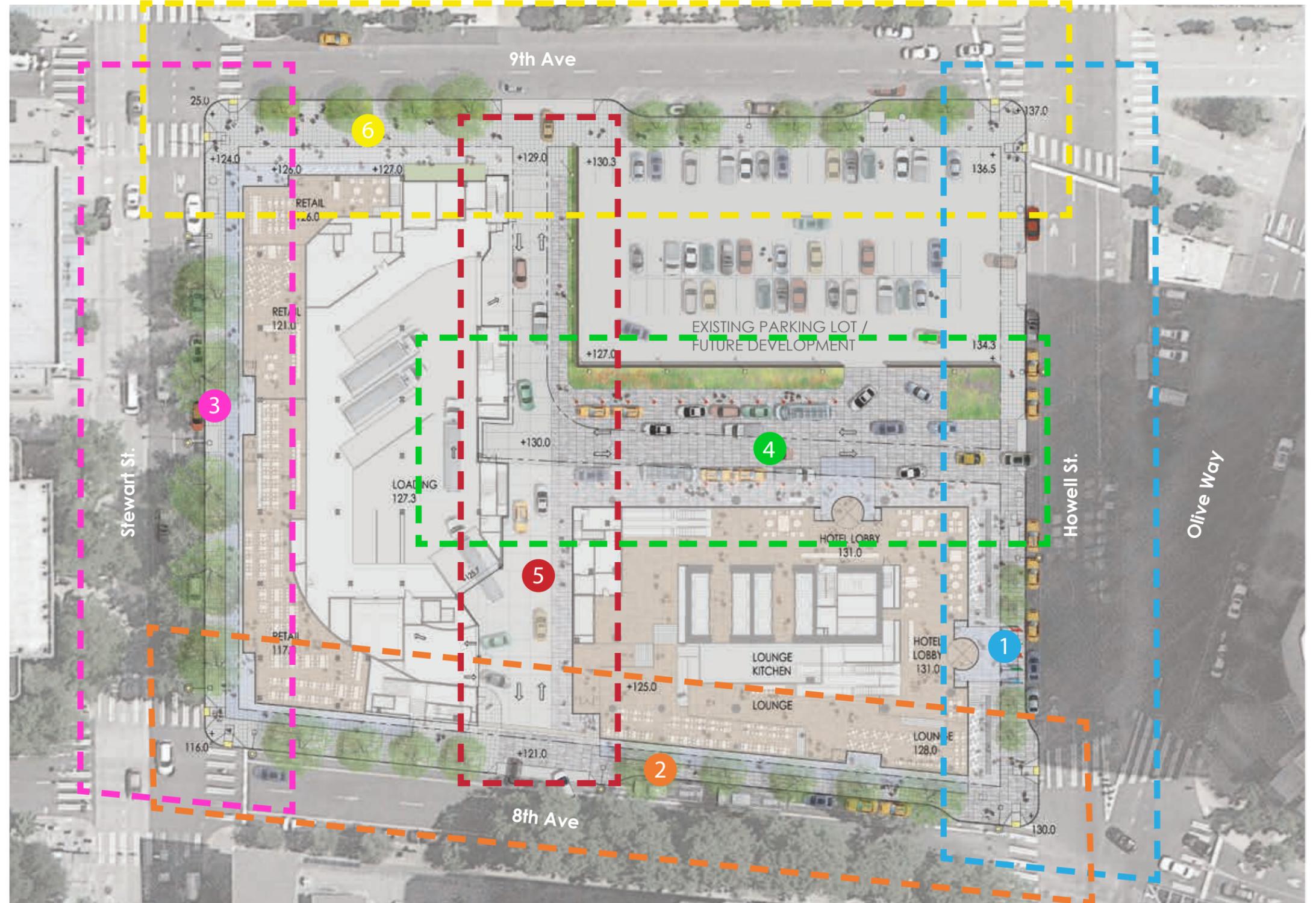
Existing street trees will be maintained on Stewart Street



SITE PLAN

LEGEND

- 1 Howell Street Setback / Streetscape
- 2 8th Ave Streetscape
- 3 Stewart Streetscape
- 4 Porte Cochere Alley
- 5 Service Drive
- 6 9th Ave Green Street



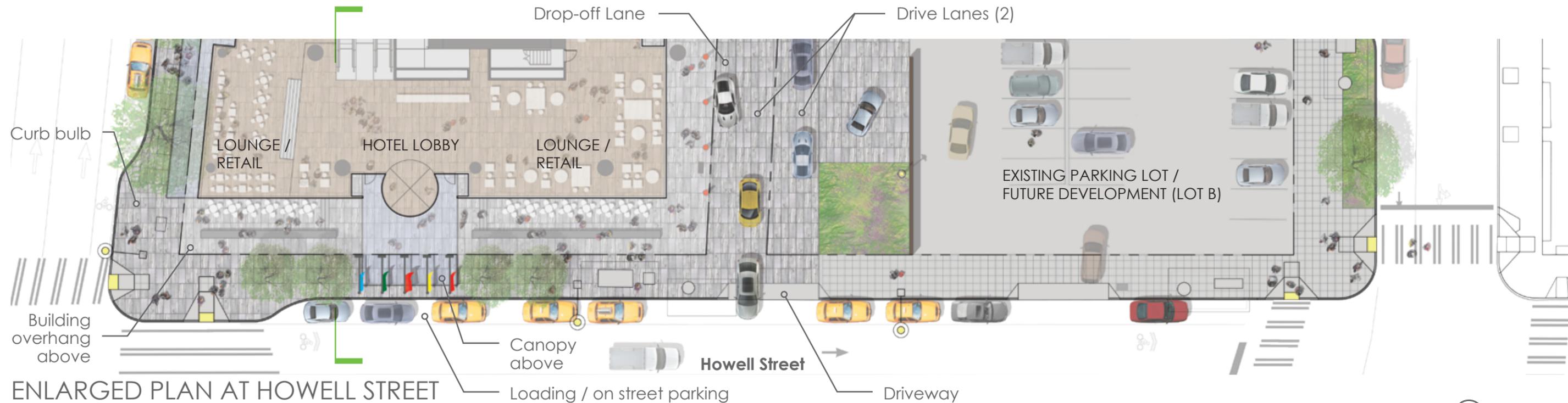
HOWELL STREET

CONCEPT

Howell Street is the public face and front door of the hotel. A deep building setback allows for generous outdoor retail areas, widened sidewalks, new street trees and landscape. Seatwalls are designed and located to accommodate the grade change between the sidewalk and retail terraces.

PROGRAM ELEMENTS

- Hotel entry
- Widened sidewalk areas
- New and improved street tree plantings
- Curb bulb-out at 8th Ave intersection
- Outdoor seating areas for adjacent retail uses





Linden street trees



Cut granite paving



Seatwall at retail terraces



Hotel entrance with canopy and flags

REFERENCE IMAGERY



HOWELL STREET ELEVATION

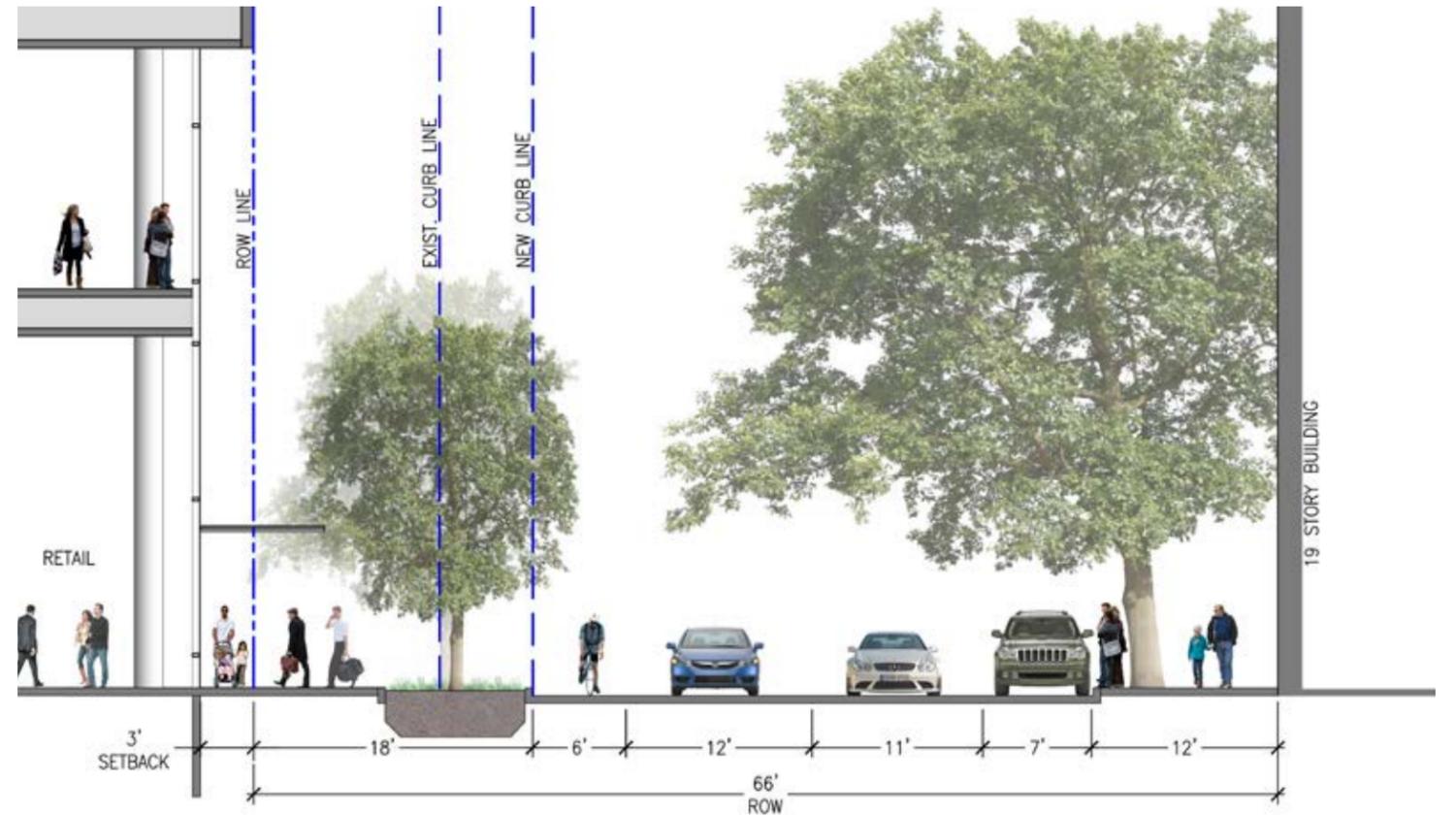
8TH AVENUE

CONCEPT

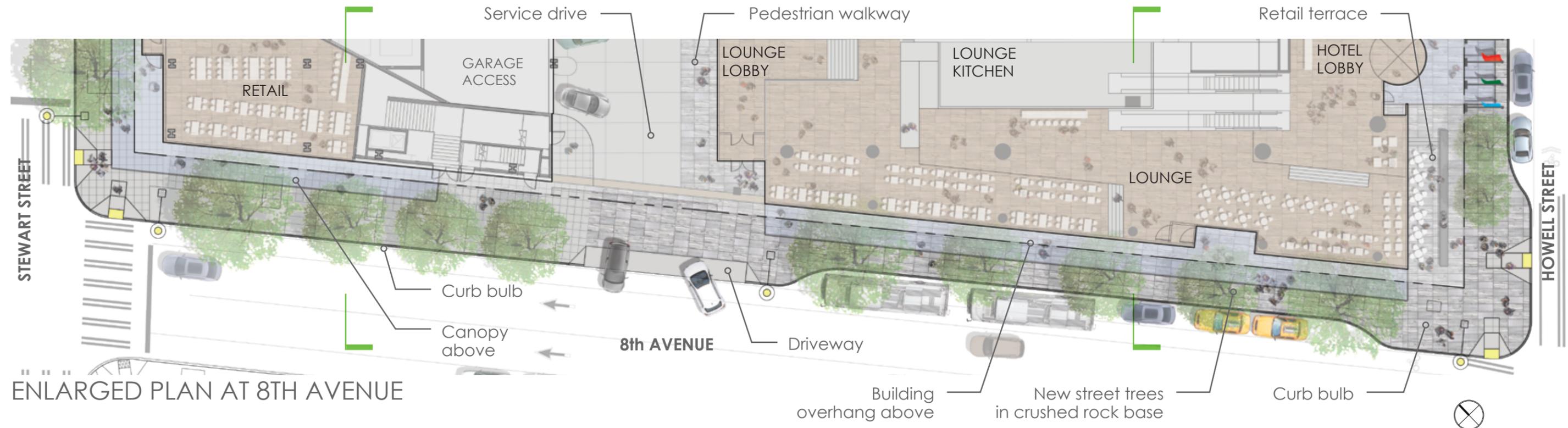
8th Ave serves as a secondary entrance to the hotel. The concept for 8th Ave is to compliment the existing mature, large canopy street trees across the street with new smaller scale street trees in improved and renovated planters. A widened sidewalk allows for loading and access to hotel and retail uses.

PROGRAM ELEMENTS

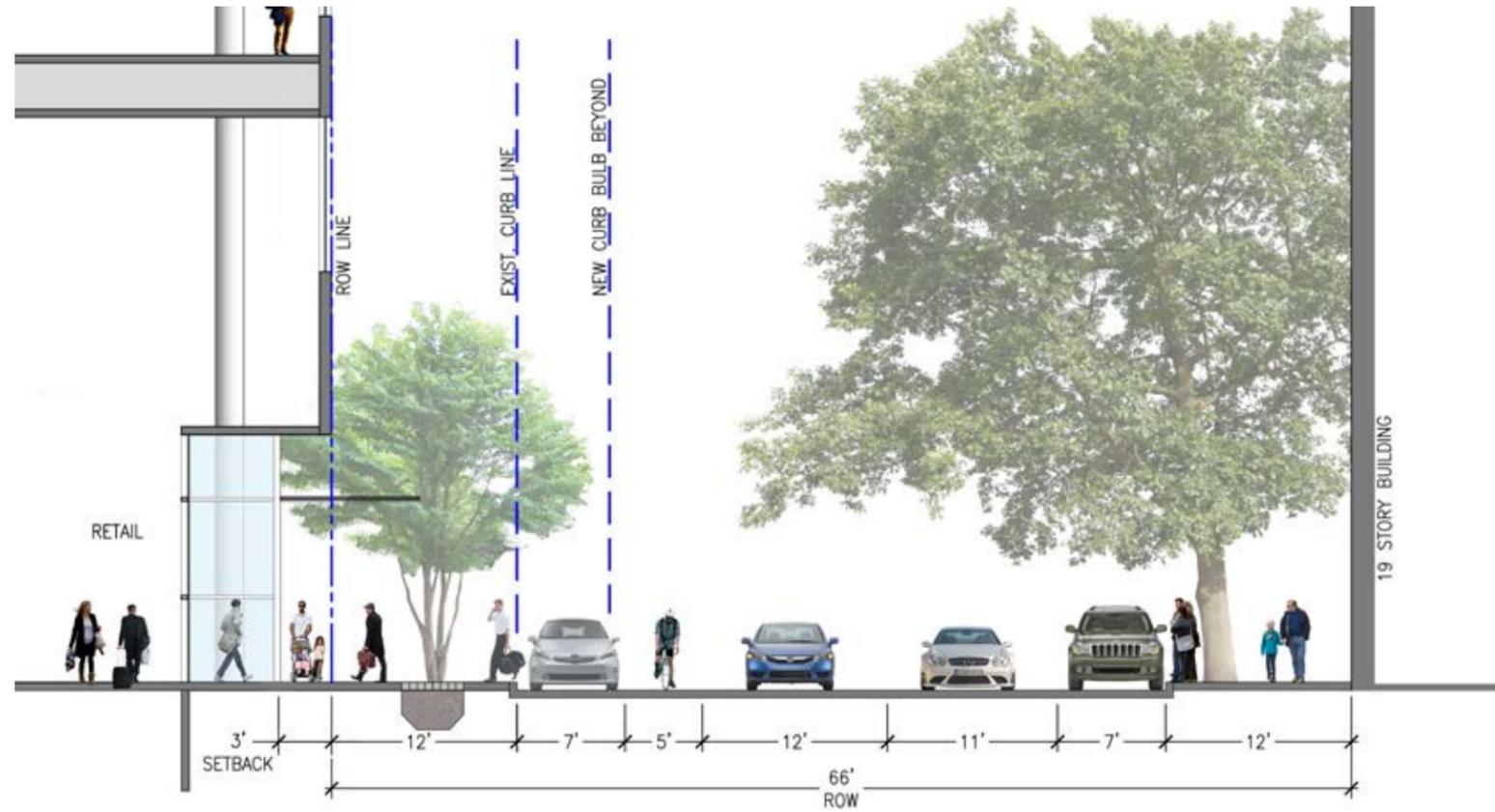
- Widened sidewalk areas
- New and improved street tree planting
- Curb bulb-outs at intersections
- Outdoor seating areas for adjacent retail uses



SECTION AT 8TH AVE NEAR STEWART



ENLARGED PLAN AT 8TH AVENUE



SECTION AT 8TH AVE NEAR HOWELL



New street trees in crushed rock base



Existing street trees on opposite side of 8th

REFERENCE IMAGERY



8TH AVE ELEVATION

STEWART STREET

CONCEPT

Stewart Street has mature, large canopy street trees that provide instant scale to the street. The concept is to improve the quality and width of sidewalk, improve and renovate existing street tree planters and create outdoor seating opportunities adjacent to new retail uses.

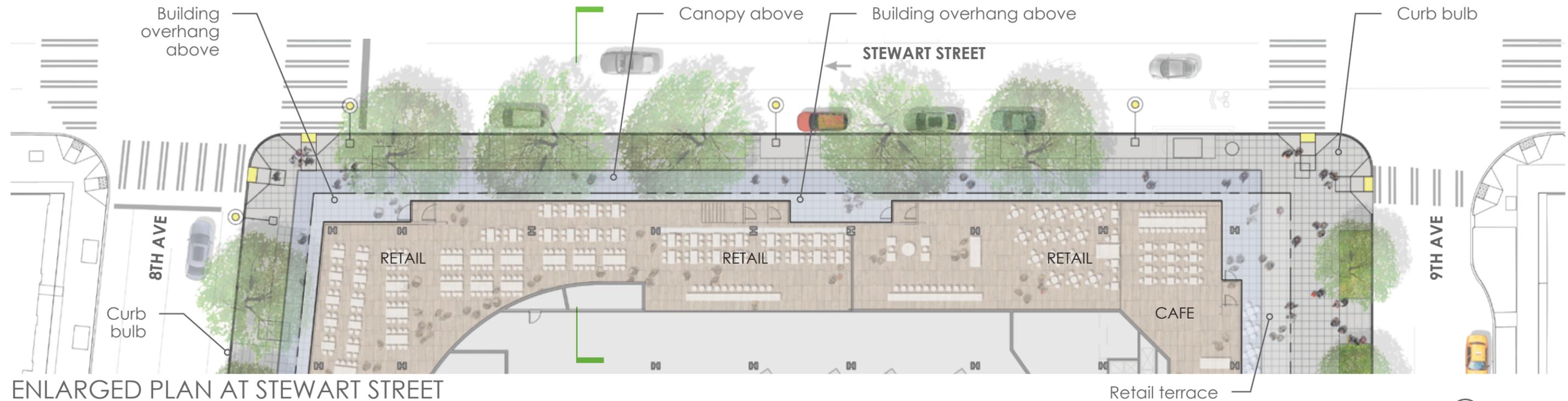
PROGRAM ELEMENTS

- Widened sidewalk areas
- Enhancement of existing street tree plantings
- Outdoor seating areas for adjacent retail uses



Existing street tree

REFERENCE IMAGERY



ENLARGED PLAN AT STEWART STREET



SECTION AT STEWART STREET



STEWART STREET ELEVATION

PORTE COCHERE ALLEY

CONCEPT

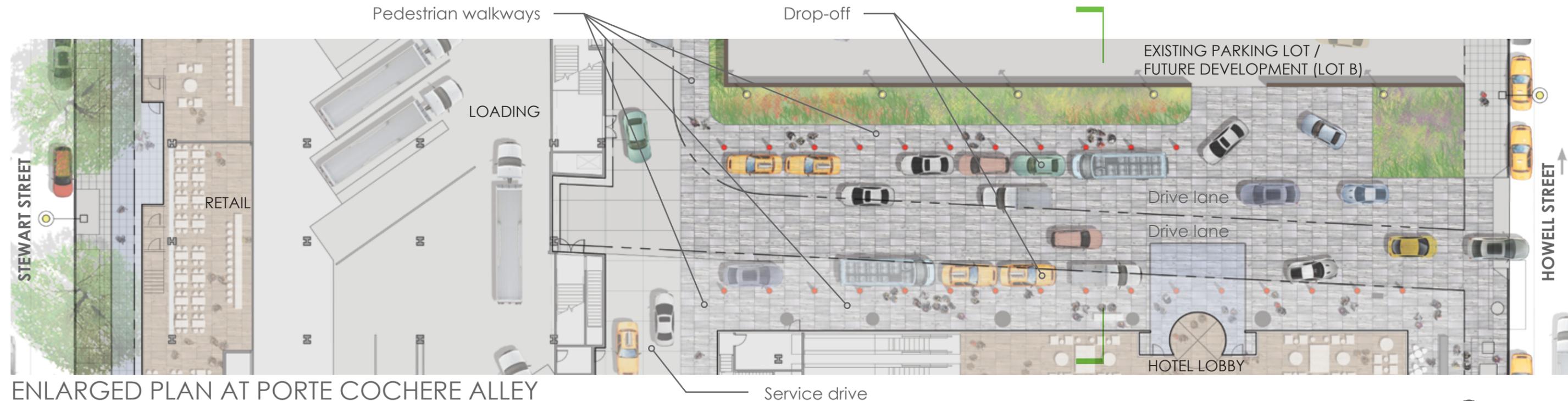
The existing alley will serve an important loading function removing cars, taxis, and van traffic from adjacent city streets. The alley will act as a secondary face to the hotel and will serve as a porte-cochere for hotel visitors. A refined pavement, furnishings such as benches, bollards and planters will create an inviting and safe shared use zone for pedestrians and vehicles.

PROGRAM ELEMENTS

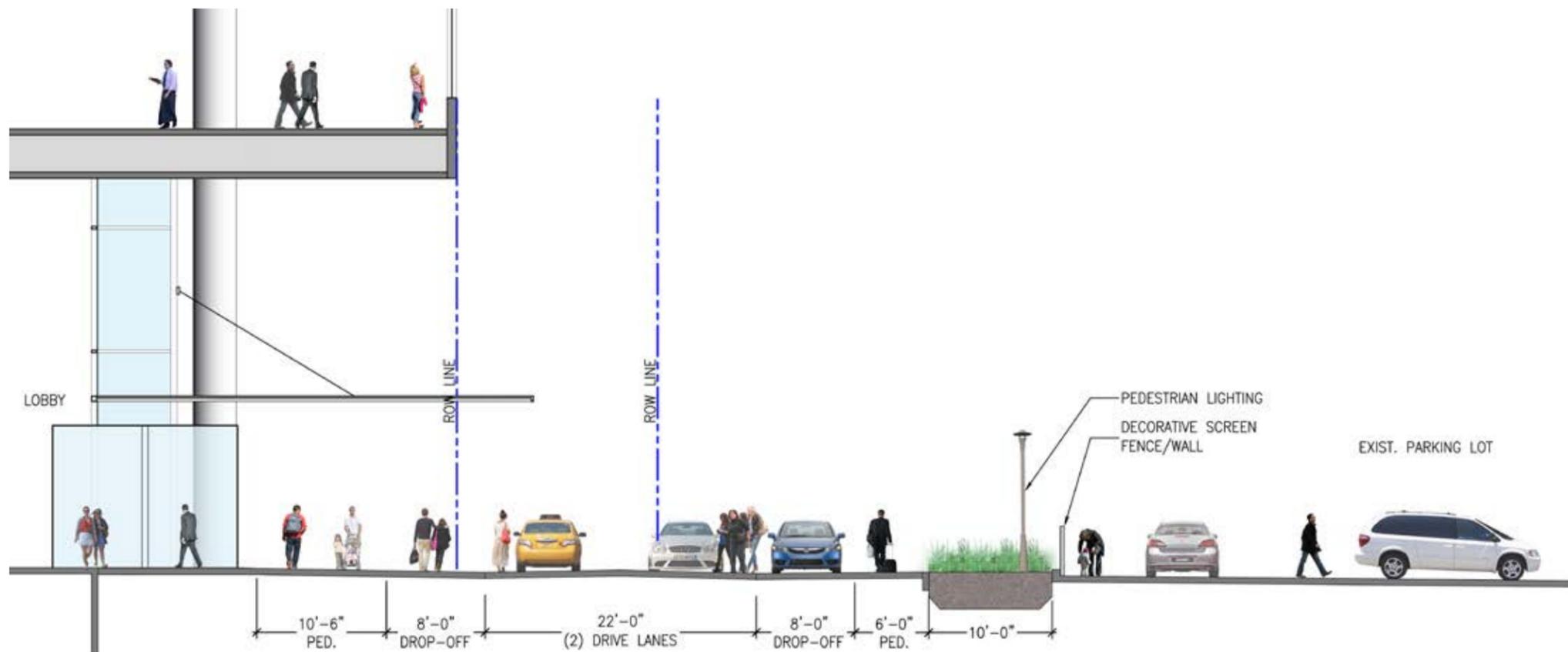
- Hotel loading area with valet operations
- Vehicle turn-around
- Pedestrian sidewalk areas protected by bollards, benches and planters
- Site lighting on pedestrian scale poles
- Refined paving
- Active lobby and retail uses with outdoor seating opportunities



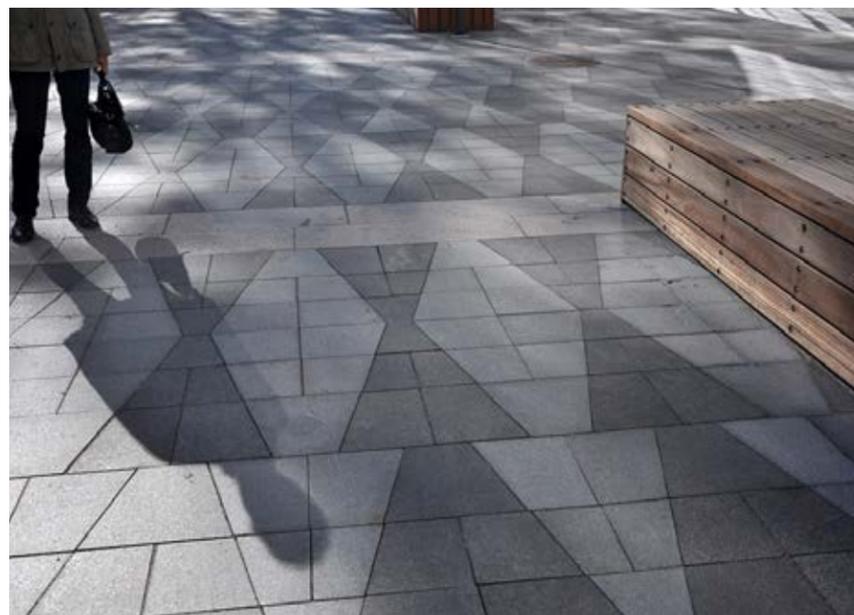
PORTE COCHERE ALLEY ELEVATION



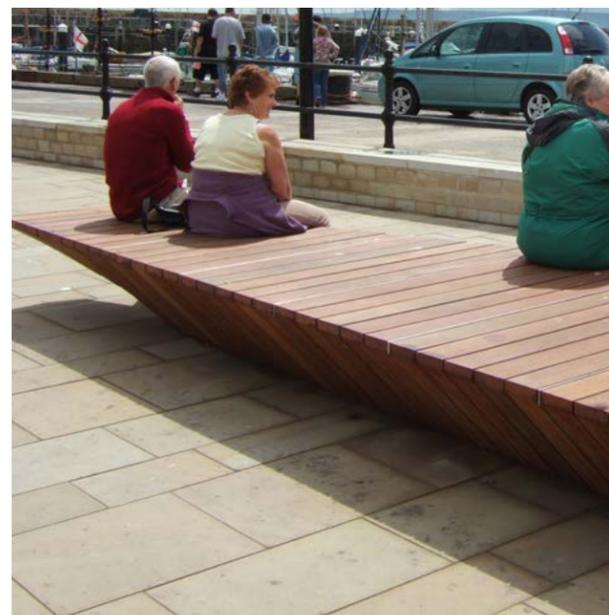
ENLARGED PLAN AT PORTE COCHERE ALLEY



SECTION AT PORTE COCHERE ALLEY



Cut granite paving
REFERENCE IMAGERY



Removable bollards, seating and art elements at hotel drop-off



SERVICE DRIVE

CONCEPT

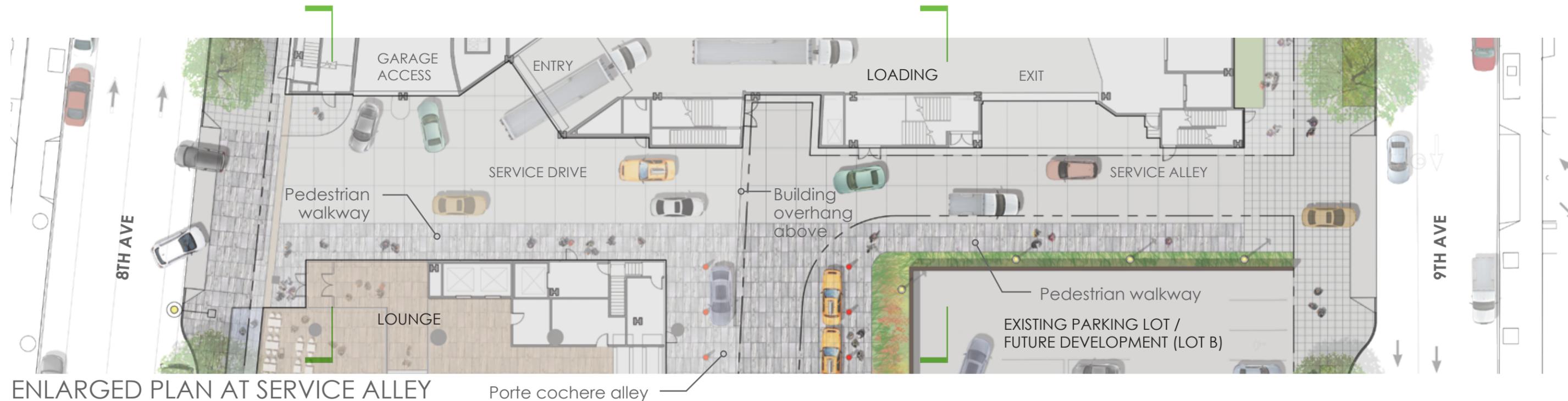
The service alley / drive performs an important function of the hotel allowing for safe passage of large service vehicles and access to hotel visitor parking. A portion of the existing alley and project property will be used to provide access to loading and parking areas. A continuous and protected pedestrian walk links 8th and 9th Aves.

PROGRAM ELEMENTS

- Hotel service vehicle access
- Hotel parking garage access
- Safe and secure pedestrian access
- Site lighting with pedestrian scale poles
- Refined paving at pedestrian sidewalks

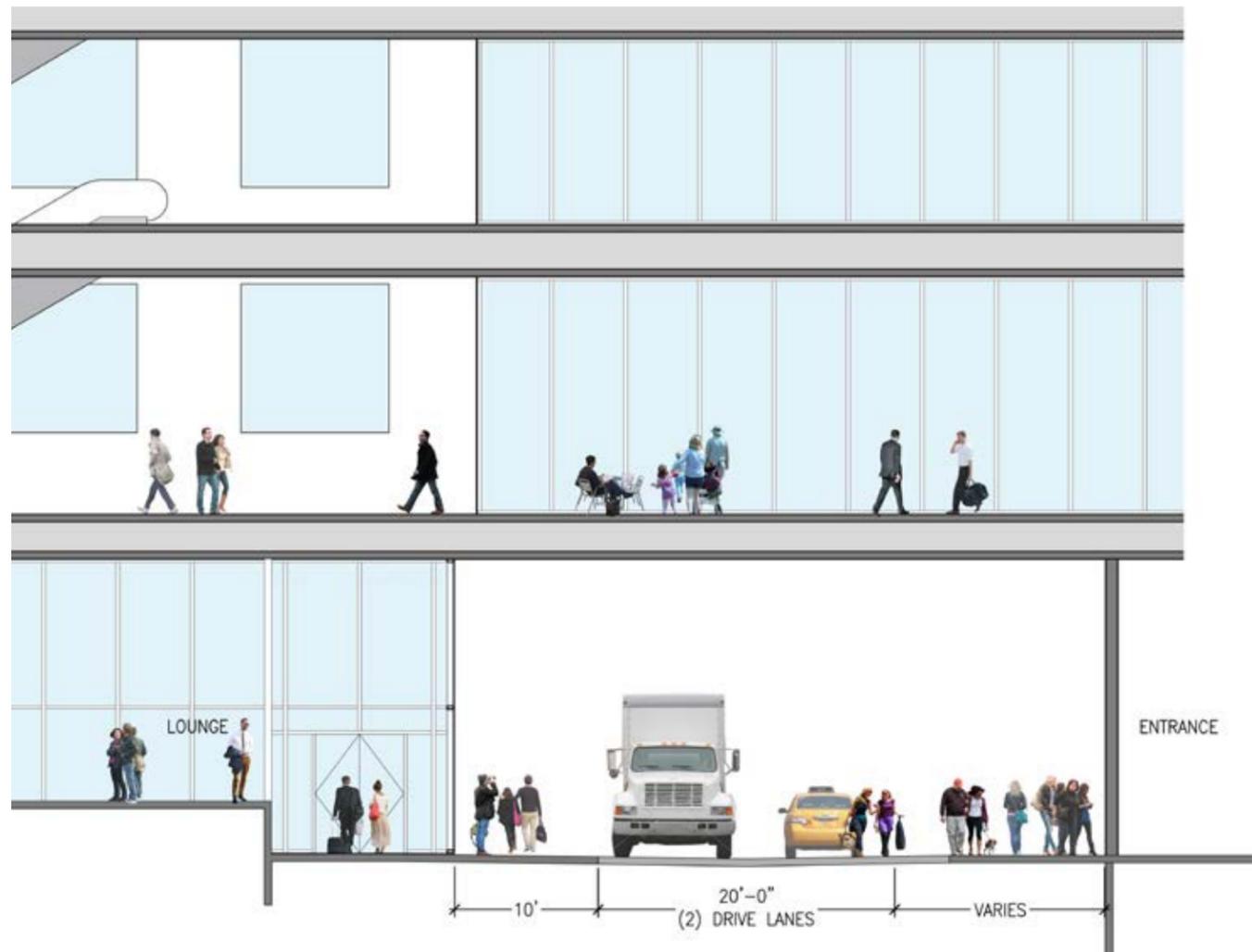


Screen wall with planting at parking lot edge
REFERENCE IMAGERY

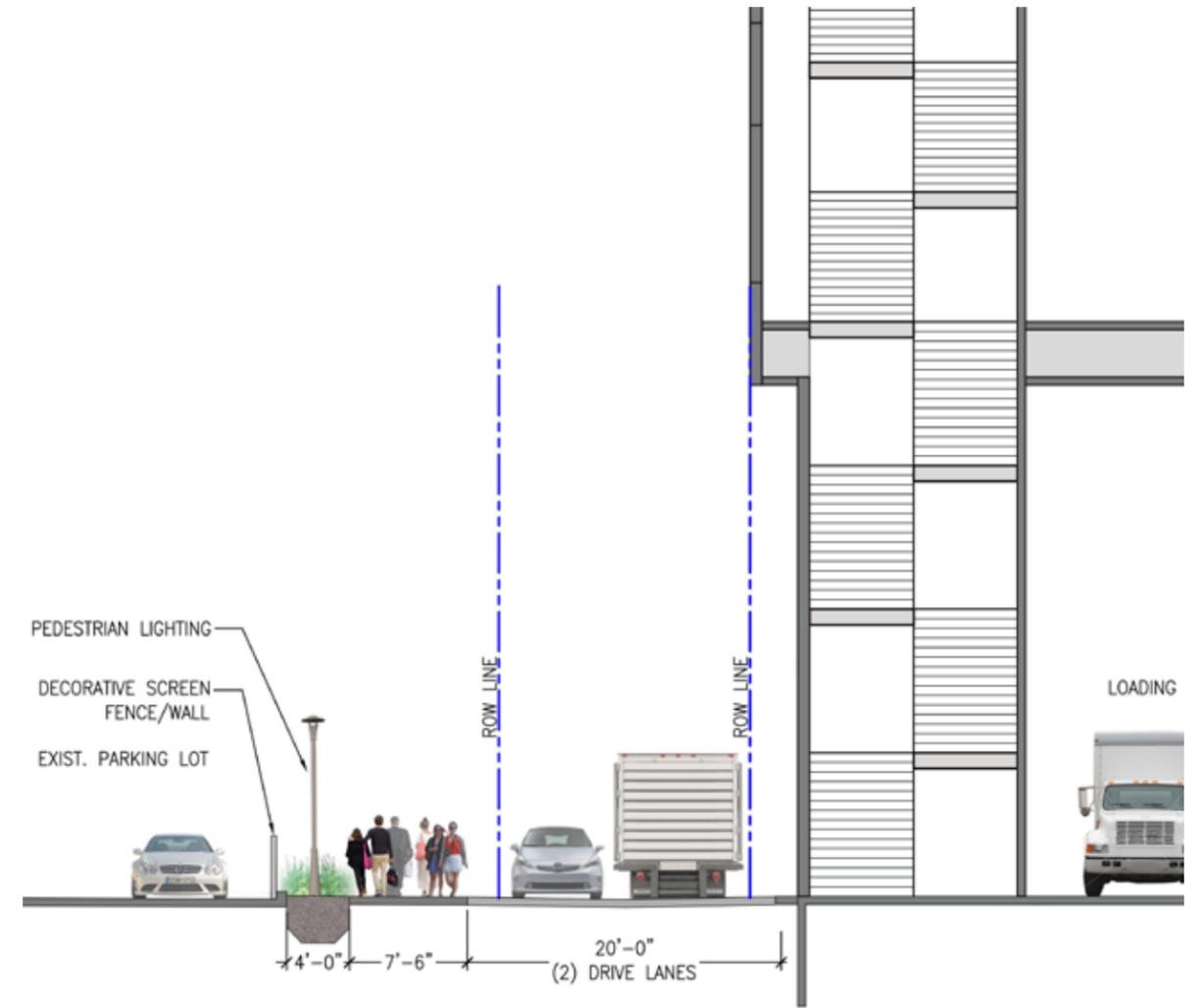


ENLARGED PLAN AT SERVICE ALLEY

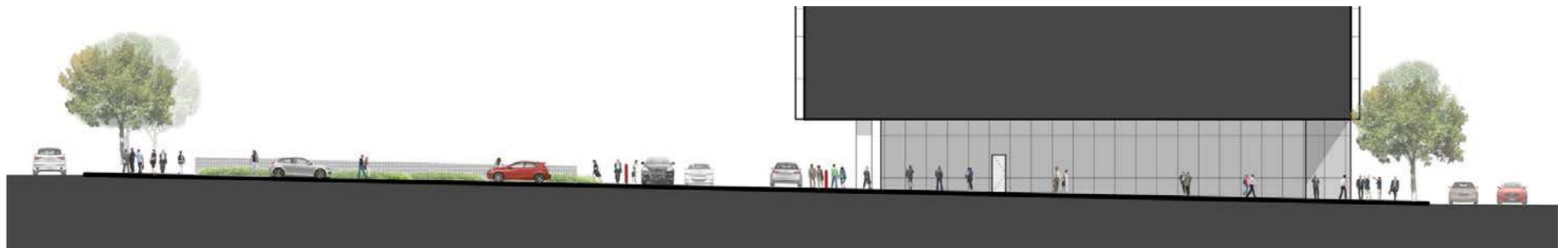
Porte cochere alley



SECTION AT SERVICE ALLEY NEAR 8TH AVE



SECTION AT SERVICE ALLEY NEAR 9TH AVE



ALLEY ELEVATION FACING HOTEL

9TH AVENUE GREEN STREET COMPARATIVE STUDY

MIXED CONDITIONS WITH ROOM FOR IMPROVEMENT

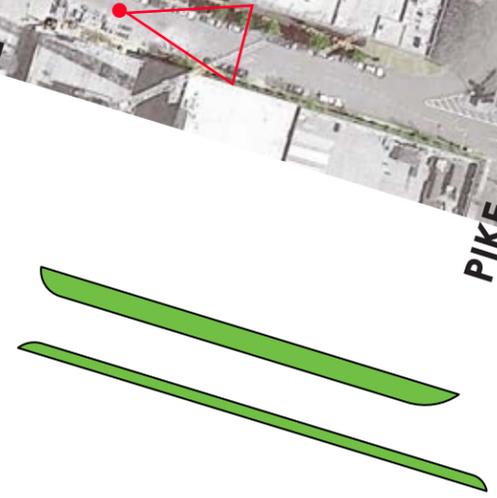
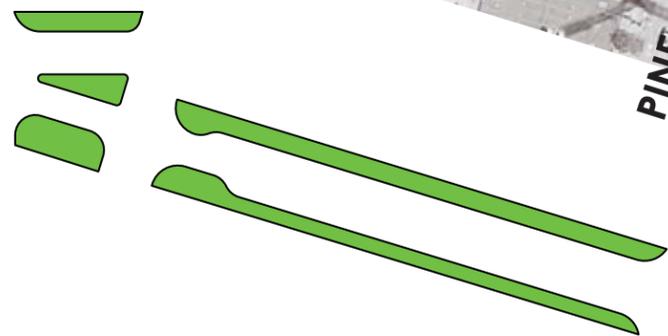
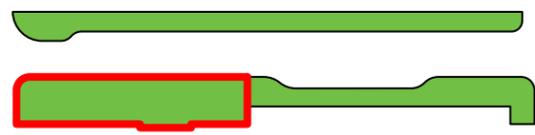
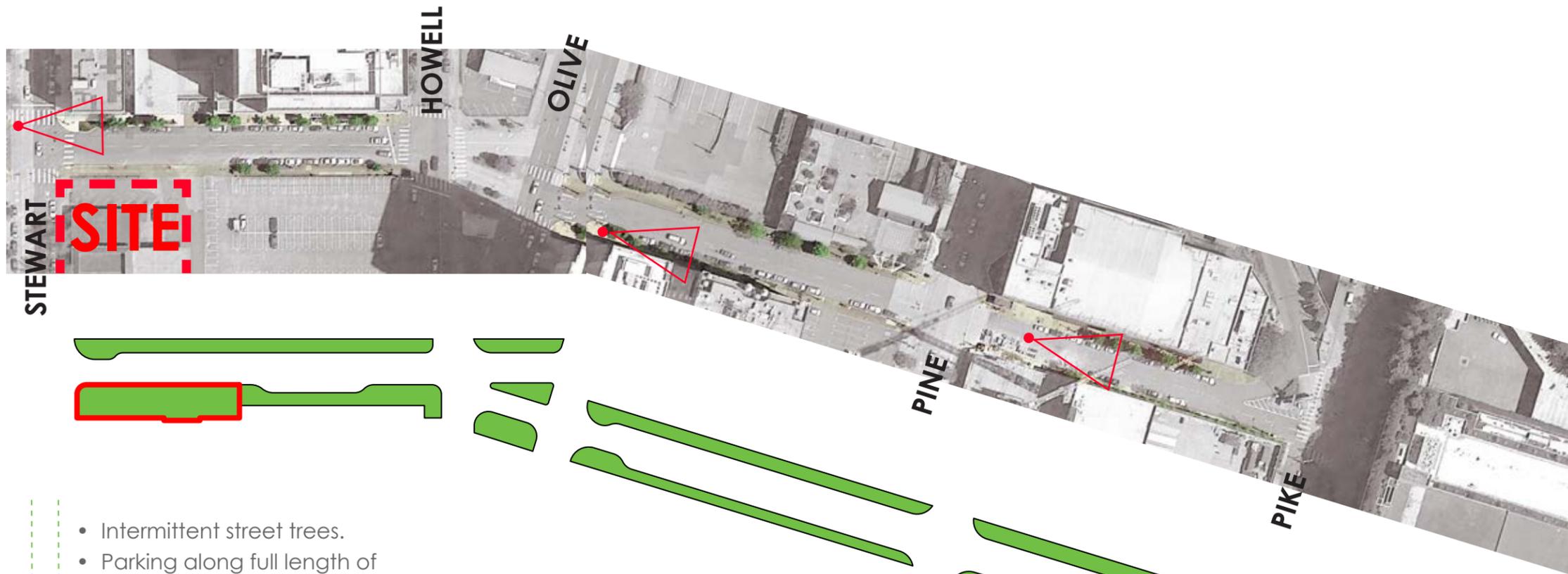
The 9th Avenue Green Street runs north-south beginning at Pike Street and ending At Westlake Avenue. The street reflects the general quality of the Denny Regrade Neighborhood with its eclectic mix of uses and building types from various periods. 9 out of the 19 buildings in this section were constructed before its green street designation, and 7 of its buildings are pre-war. The green street concept has begun to emerge in some areas but overall 9th Avenue functions to accommodate movement through tight sidewalks of varying quality.



- Continuous parallel parking on both sides.
- Narrow sidewalks (~6').
- Street trees are present on the east side. No plantings on the west.

- East sidewalk narrow (~6') and in poor condition.
- West side contains newer plantings and continuous 10' sidewalk.
- Sidewalk interrupted by several vehicular entries in southern portion of the block.

- Both sides of the street reconstructed in 2008.
- Continuous 9' to 11' sidewalk space with 8' planting zone against the street.
- Concrete bench seating and planter composition adjacent to 818 Stewart (SW corner).



- Intermittent street trees.
- Parking along full length of both sides. Minimal plantings.
- Varying sidewalk: 6' to 12'.

- East side abuts future Convention Center Expansion
- West side sidewalk interrupted by plantings and building edges, less than 6' feet width in some areas.

- Wide sidewalk without plantings next to Paramount.
- Street trees on a portion of the block.

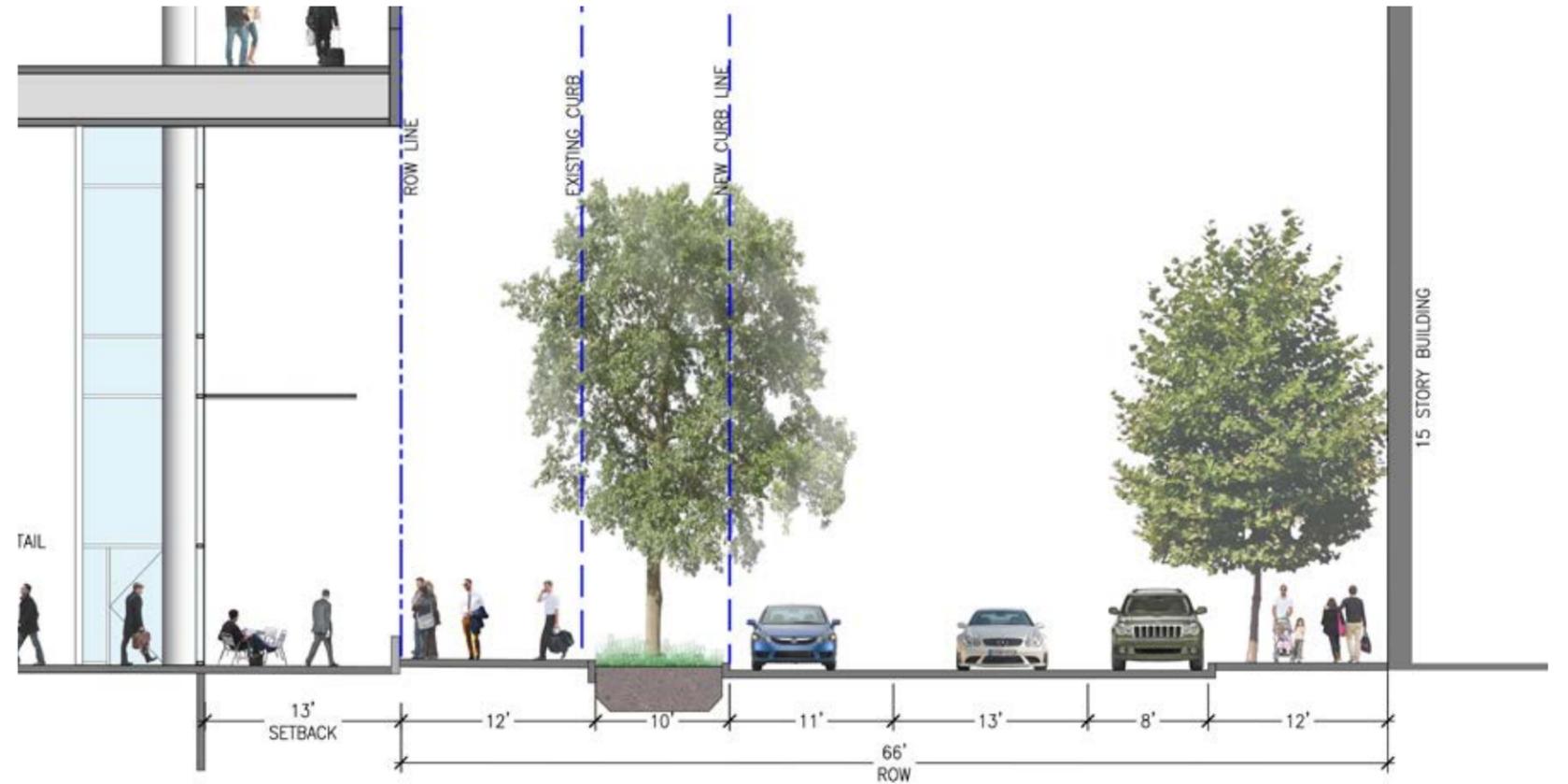
9TH AVENUE GREEN STREET

CONCEPT

The 9th Ave concept features a combination of landscape, seating, retail and a feature wall, creating a desirable pedestrian space to pause or pass through. The street is comprised of a deep building setback, allowing for a widened sidewalk area with seating, landscape that extends into an existing parking lane, outdoor retail seating and the feature wall.

PROGRAM ELEMENTS

- Widened sidewalk with seating and landscape
- Building setback with outdoor retail area
- Feature wall



SECTION AT 9TH AVE NEAR STEWART



ENLARGED PLAN AT 9TH AVENUE



SECTION AT 9TH AVE NEAR PARKING LOT



Featured Wall



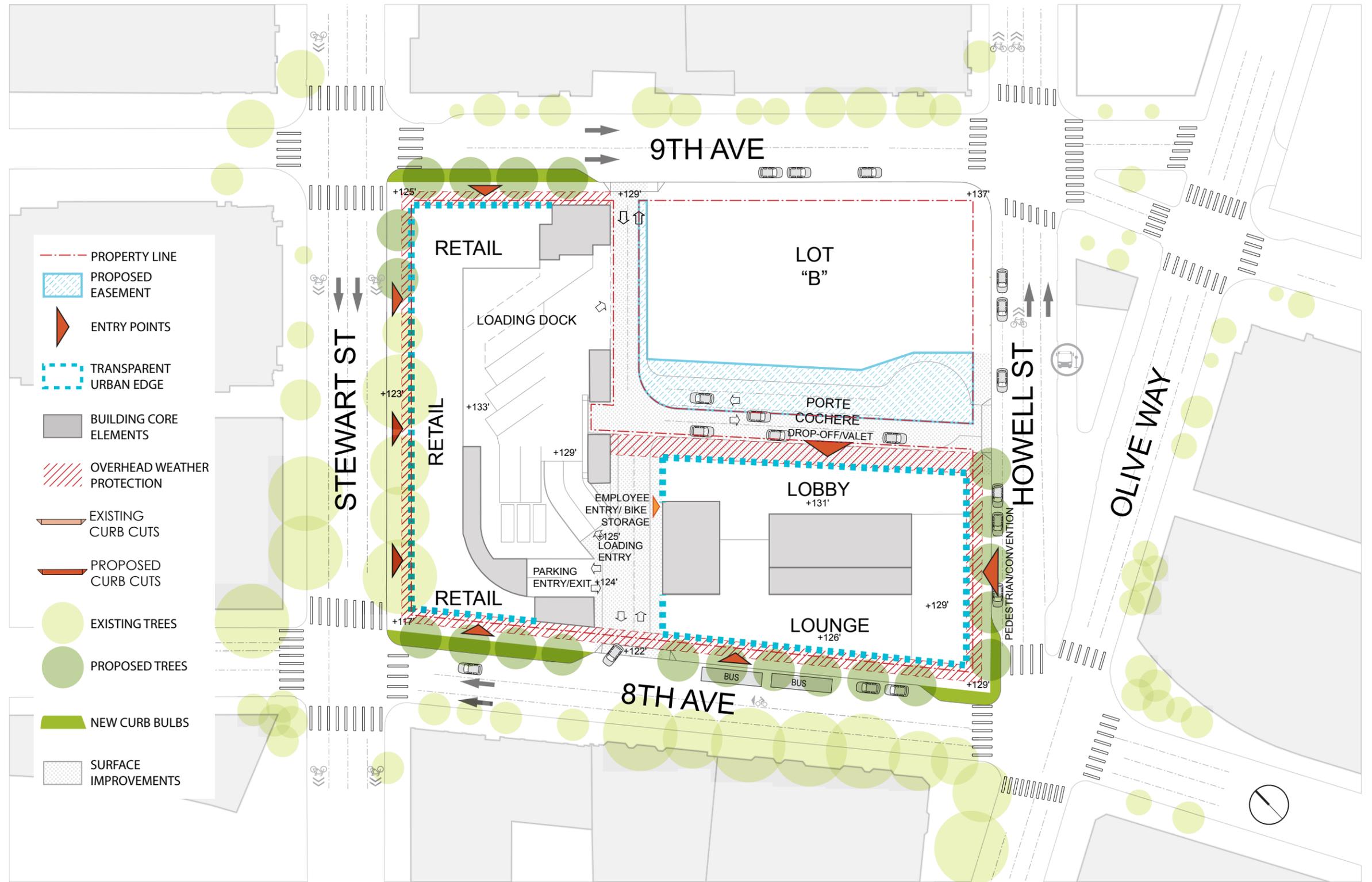
Retail terrace seating

REFERENCE IMAGERY

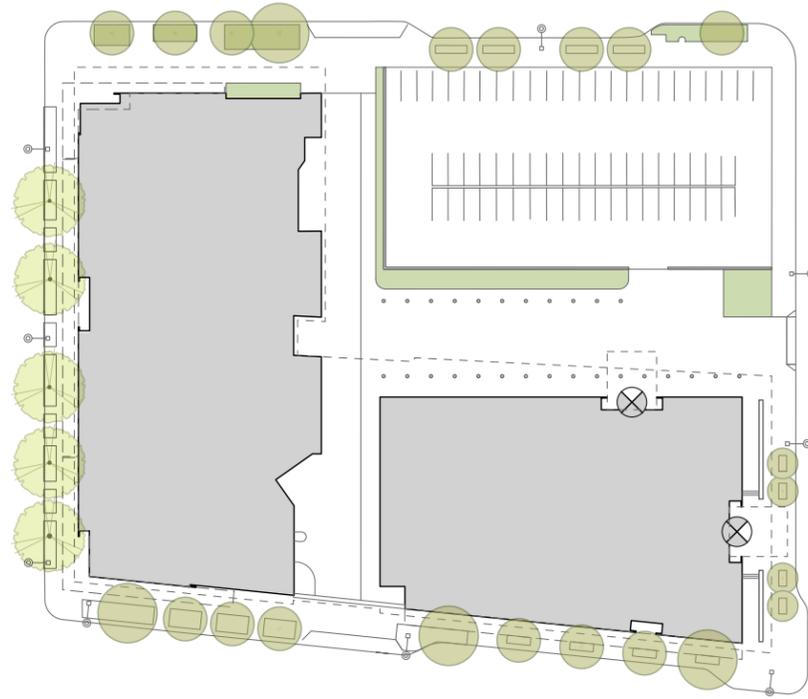


ELEVATION FACING 8TH AVENUE

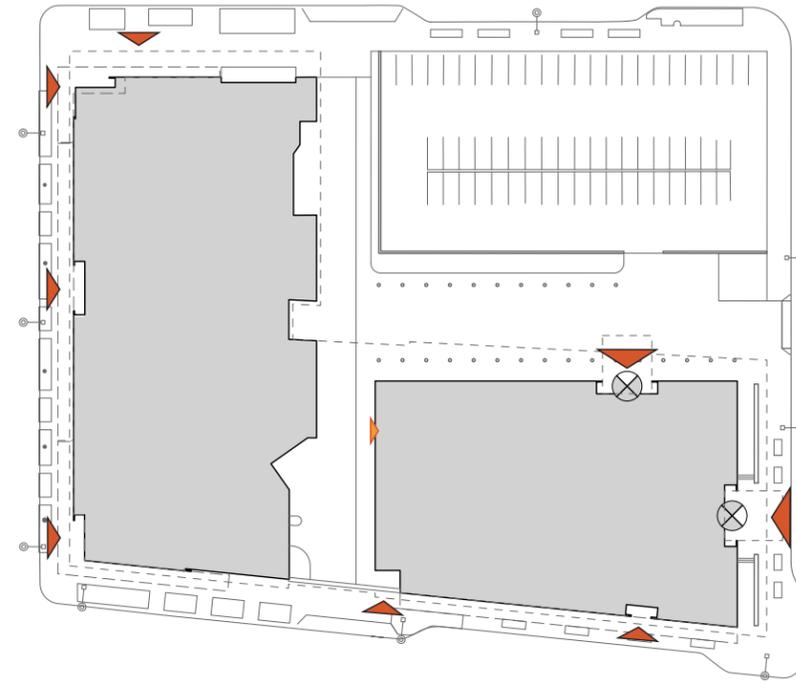
PROPOSED GRADE LEVEL FUNCTIONALITY



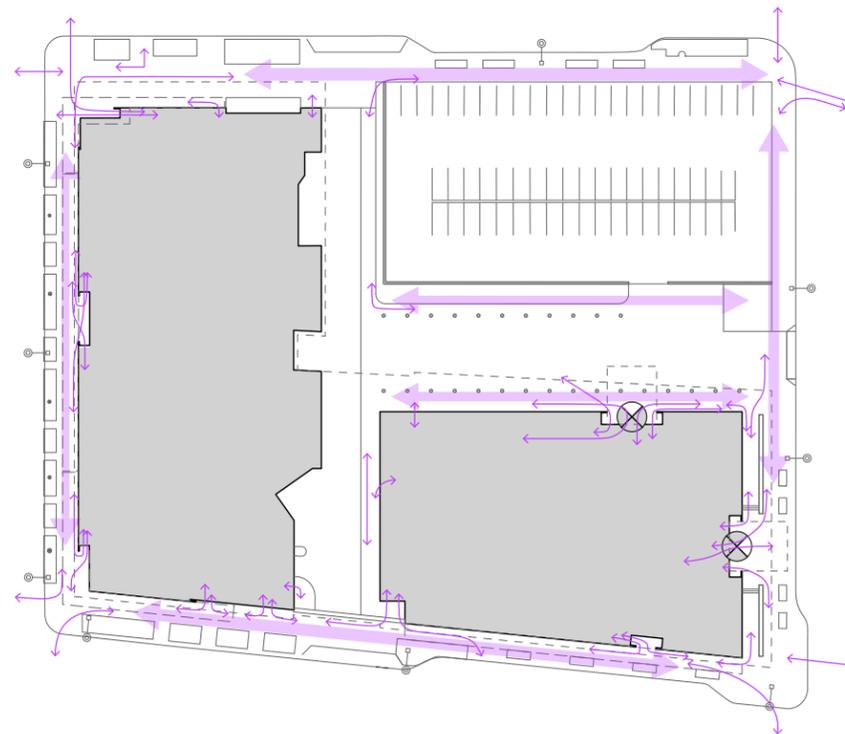
SITE ORGANIZATION DIAGRAMS



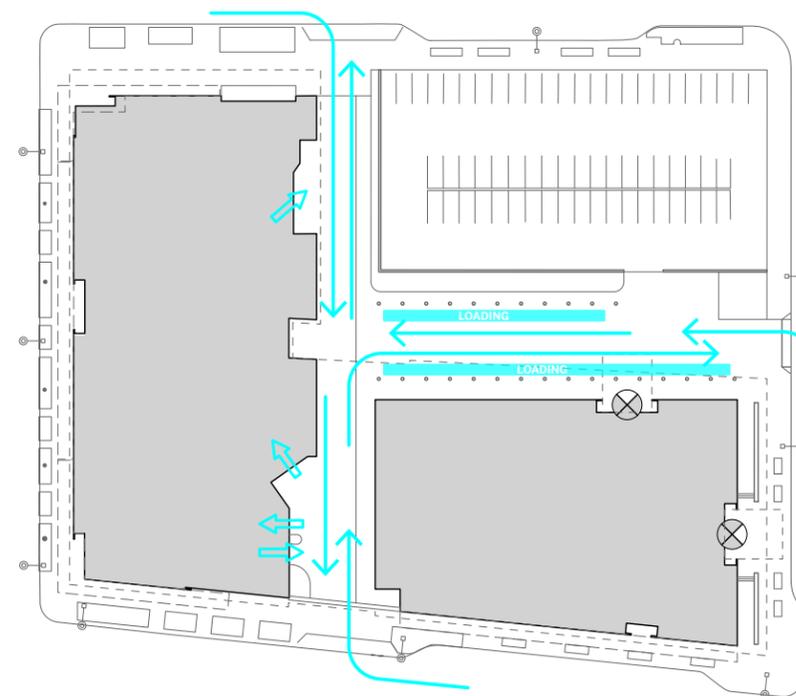
TREES & PLANTERS



BUILDING ENTRIES



PEDESTRIAN CIRCULATION

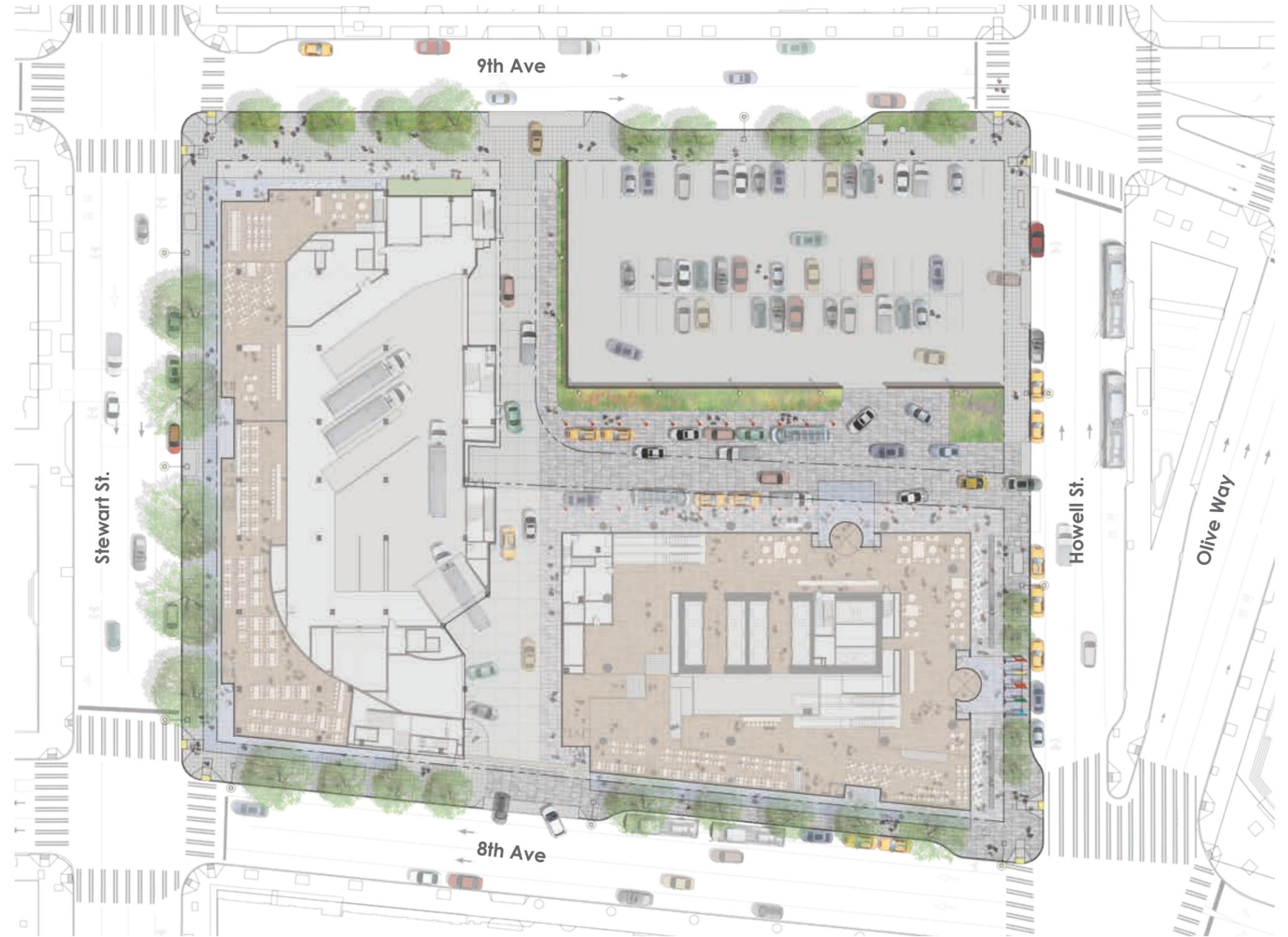


VEHICULAR CIRCULATION

MATERIALS

LEGEND

-  Granite Entry and Arrival Paving
-  Standard Sidewalk Paving
-  10' Square Alley Concrete Paving
-  Seatwall / Bench at Howell Street
-  Removable Bollards
-  Lamp Post



MATERIAL PALETTE



Pedestrian lighting



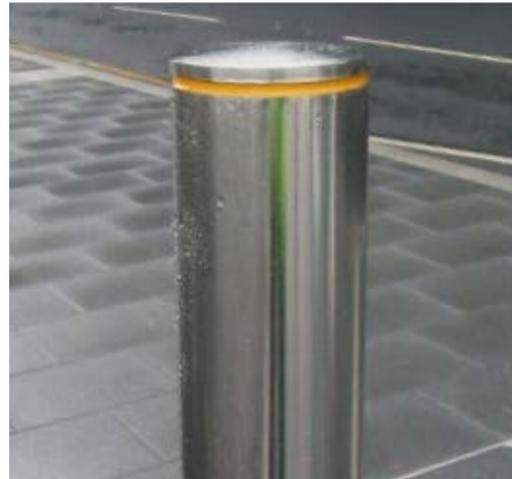
Standard City Wayfinding Elements



Decorative fence w/ planting



Feature wall



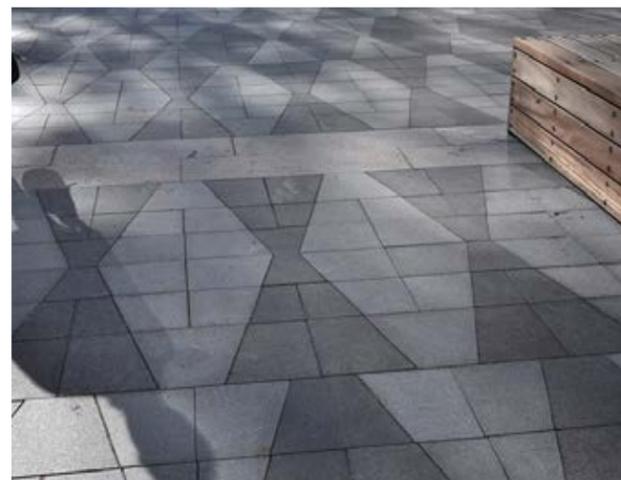
Removable Bollards or Art Elements at Drop-Off



Seatwalls at Howell St.



Cut granite paving



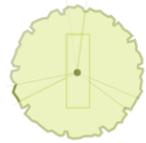
Linear drain at porte cochere alley



Standard sidewalk paving

PLANTING

LEGEND



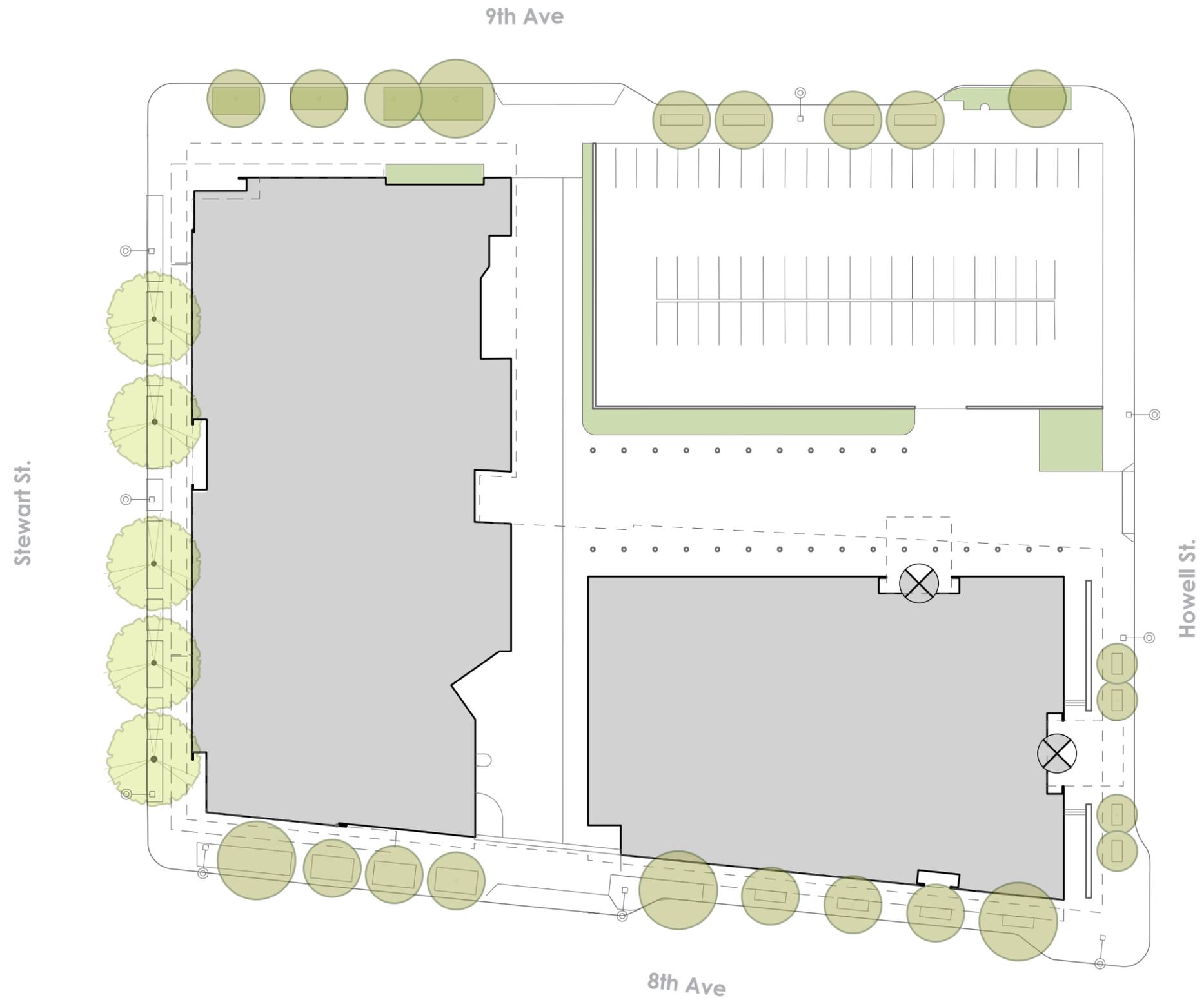
Existing tree



Proposed tree



Under-story planting



PLANT PALETTE



Platanus acerifolius



Tilia tomentosa



Zelkova serrata



Carpinus betulus



Lonicera pileata



Daphne tangutica



Pachysandra axillaris 'Windcliff'



Polystichum neolobatum



Pittosporum tenuifolium 'Golf Ball'

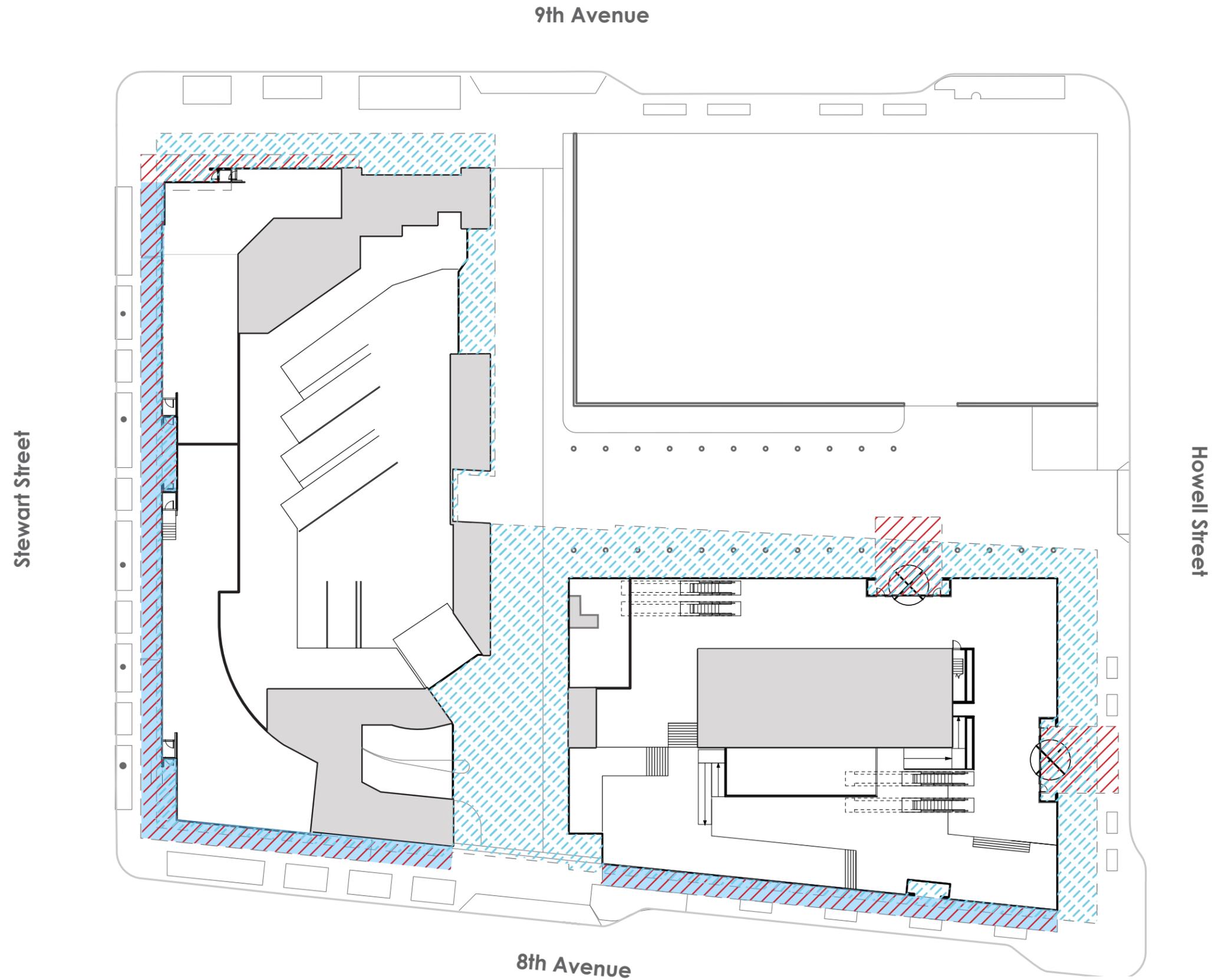


Sarcococca hookeriana var. digyna

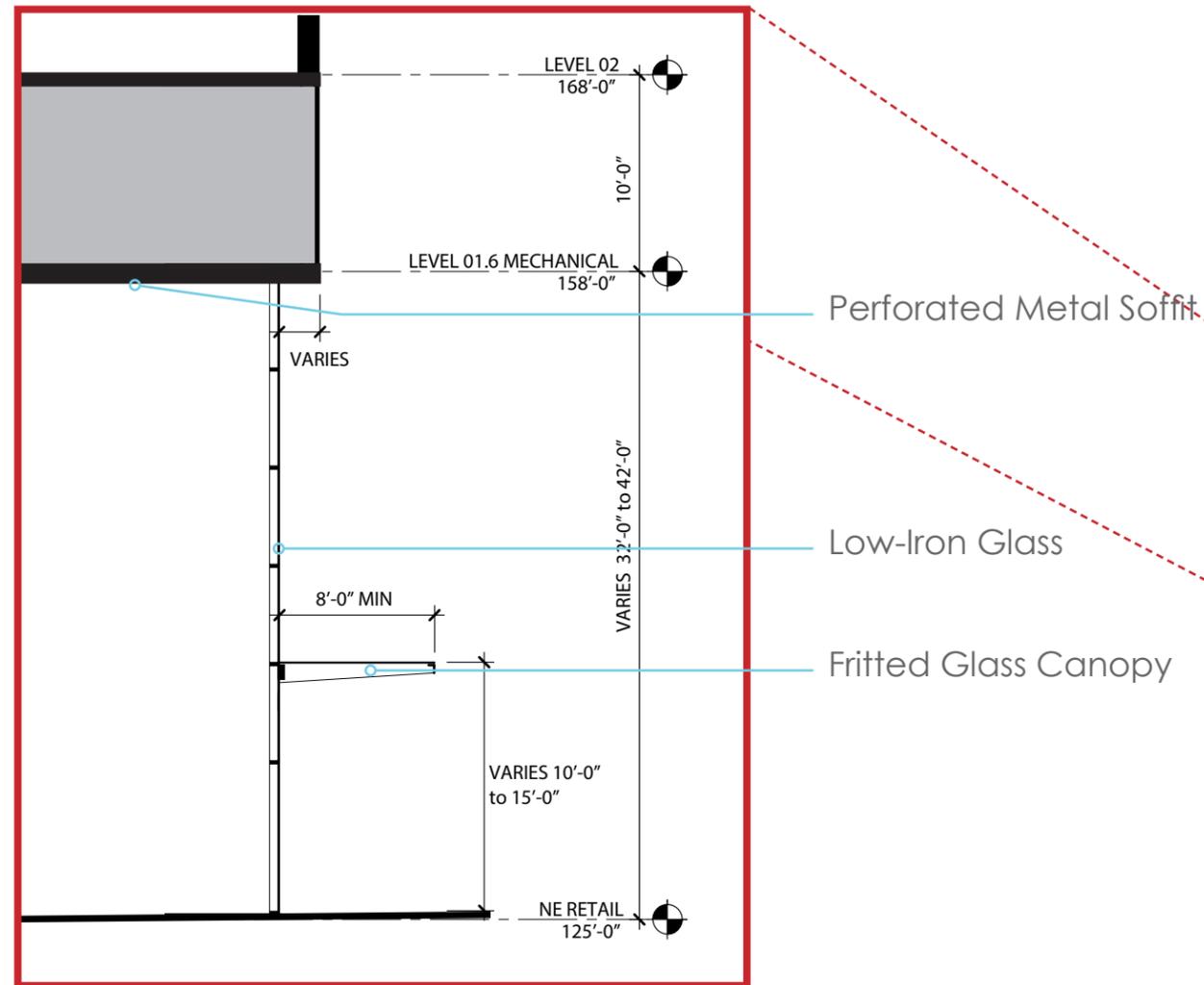
BUILDING CANOPIES

This diagram indicates the areas of required overhead coverage around the building perimeter. Due to the voluntary setbacks on both 9th Avenue and Howell Street, overhead coverage will not be required. However it will be accommodated over a vast majority of the building frontages and will far exceed the required coverage. Finally, the 70' wide through block connection will provide a new and fully covered urban through-block connection.

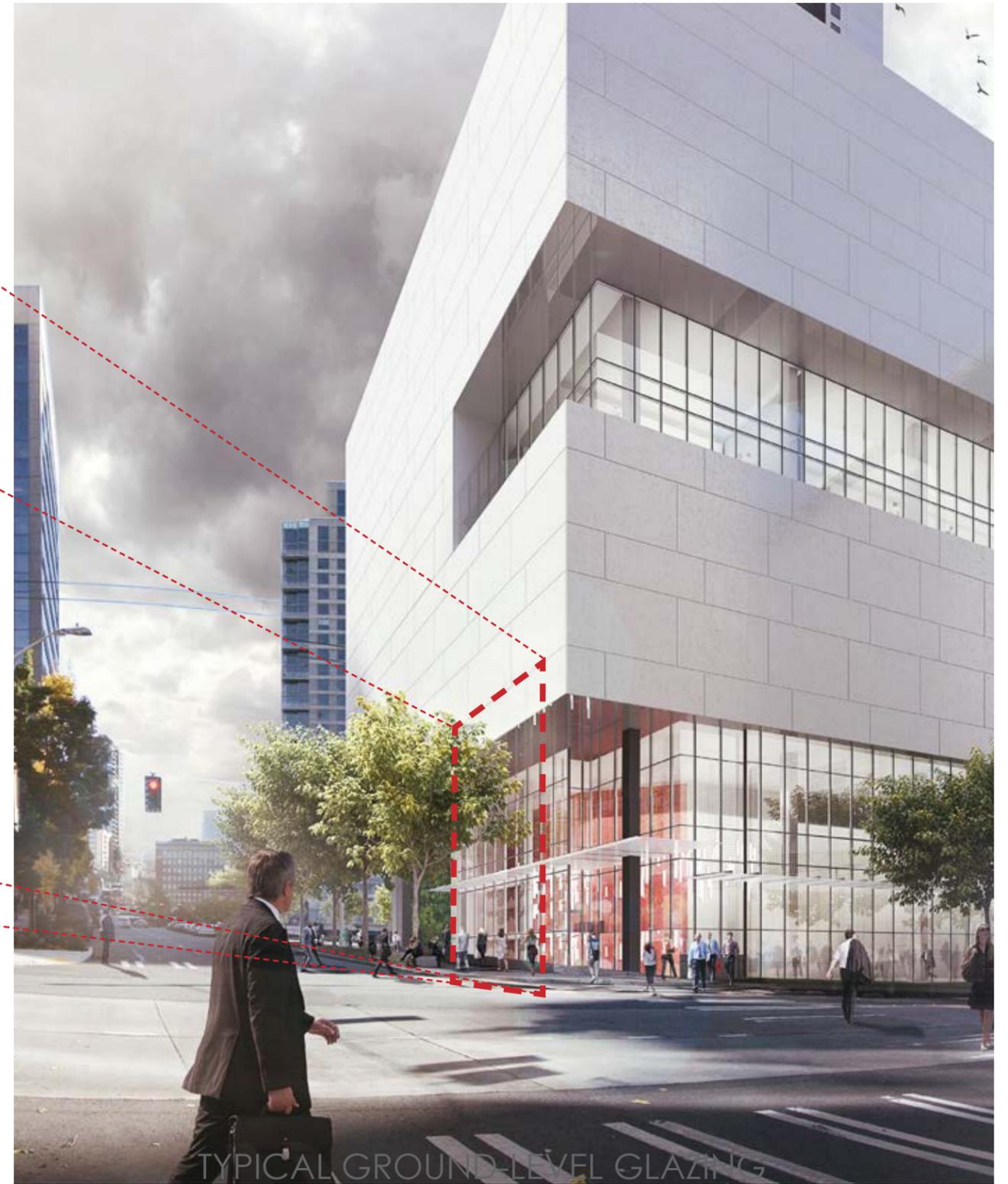
-  Provided Overhead Cover: Soffit
-  Provided Overhead Cover: Canopy
-  Required Overhead Cover



GROUND LEVEL GLAZING AND CANOPY SECTION



This section shows the typical dimensional relationships of the varying material systems on the ground level. The predominant language for this uniquely tall lowest level will be that of high-transparency low-iron glass, with incorporated canopies as indicated on page 6. Above this glass wall system will be the continuous perforated metal panel ceiling system indicated on page 7. This perforated soffit will then wrap vertically on the outboard edge and become a perforated screen fascia system, concealing critical mechanical intake and exhaust components.



LIGHTING DESIGN CONCEPT

The site lighting approach aims to integrate lighting systems into the landscape components at grade level. The lighting of the sculptures within the new Sculpture Garden will be of particular significance, emphasizing their material and formal qualities. Lighting integrated into the seating benches will both create a safe pedestrian experience and emphasize the unique quality of the surface finishes within.

In addition to site lighting shown on the plan the extent and effect of the ceiling lighting shown on pages 14-15 and the transparency and light provided by the street-facing ground level uses will provide a strong sense of light and openness. The continuous perforated ceiling system above the ground plane will contain both lighting required for the functionality of the space as well as contain a dynamic and engaging public lighting art element within the through-block connection.



8 RESPONSE TO DESIGN GUIDELINES

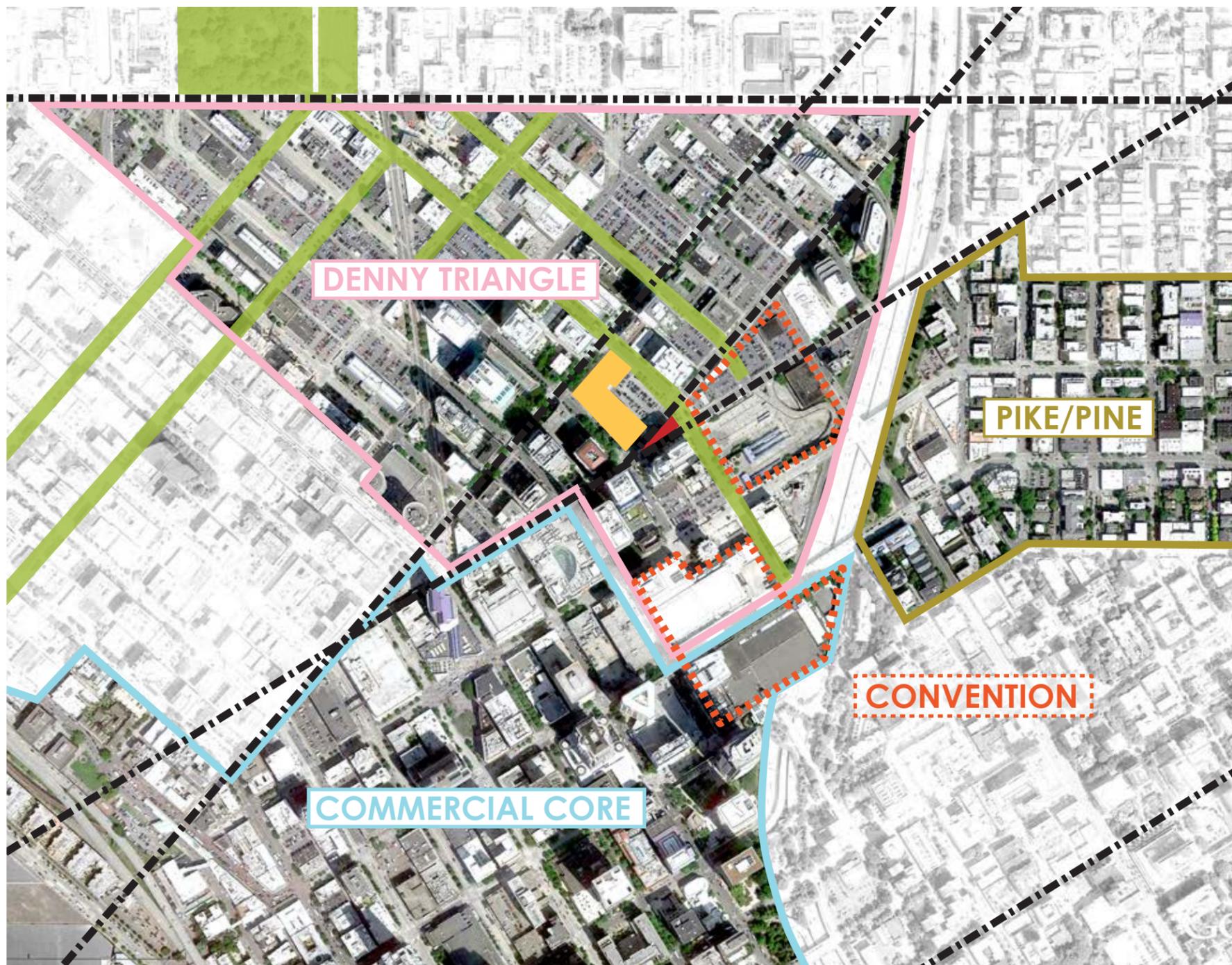
The Design Team finds the following guidelines from the *Design Guidelines for Downtown Development Document* relevant to the design of this project.

A-1 Respond to the physical environment

Develop an architectural concept and compose the building's massing in response to geographic conditions and patterns of urban form found beyond the immediate context of the building site.

The primary volumes of the proposed scheme are situated to respond directly to the surrounding neighborhoods and urban uses. The Hotel high rise tower is placed along the Howell Street edge, where it will be adjacent to the downtown core and convention areas. In this location the tower directly responds to opening in street grid at Olive and Howell Triangle, where the tall form will serve to anchor that significant void in the urban form.

Along the northern edge of the site adjacent to Stewart Street, the proposal locates the lower height podium portions adjacent to the vibrant and Denny Triangle neighborhood, relating more to the mixed scales of buildings within that neighborhood.



Neighborhood Adjacencies

A-2 Enhance the skyline.

Design the upper portion of the building to promote visual interest and variety in the downtown skyline.

In the proposed scheme, the tower location and geometry is aligned to Howell Street, helping to define the south eastern edge of the shifted orthogonal grid of the Denny Triangle neighborhood. The tower's simple rectangular form is a direct representation of the hotel room layouts within, and the clean, minimal form will stand in contrast to the modulated and highly-articulated adjacent tower geometries. From the north, the slenderized tower form will present a unique composition of forms to the surrounding neighborhood. These minimal planar surfaces will be animated by articulating transparent and solid components, configured to interact with Seattle's unique and varied daylight conditions.



View from Capitol Hill



View from South Lake Union

B-1 Respond to the neighborhood context

Develop an architectural concept and compose the major building elements to reinforce desirable urban features existing in the surrounding neighborhood.

The bulk and scale of the proposed scheme directly addresses the growing convention center and adjacent retail core by locating the hotel lobby and drop off elements to the southern portion of the site.

Along the northern edges of the site, retail spaces at grade will become a critical programmatic asset for the surrounding neighborhoods.

In locating the valet, drop-off, parking entry and truck loading spaces in and adjacent to the existing alley, the project internalizes the spaces which would otherwise compromise the building's perimeter street conditions.



B-2 Create a transition in bulk and scale.

Compose the massing of the building to create a transition to the height, bulk and scale of development in nearby less intensive zones.

The bulk of the proposed scheme responds directly to the surrounding context by placing the tower closest to the high-rise district surrounding the convention center, minimizing the impact of shadows cast on the adjacent streets.

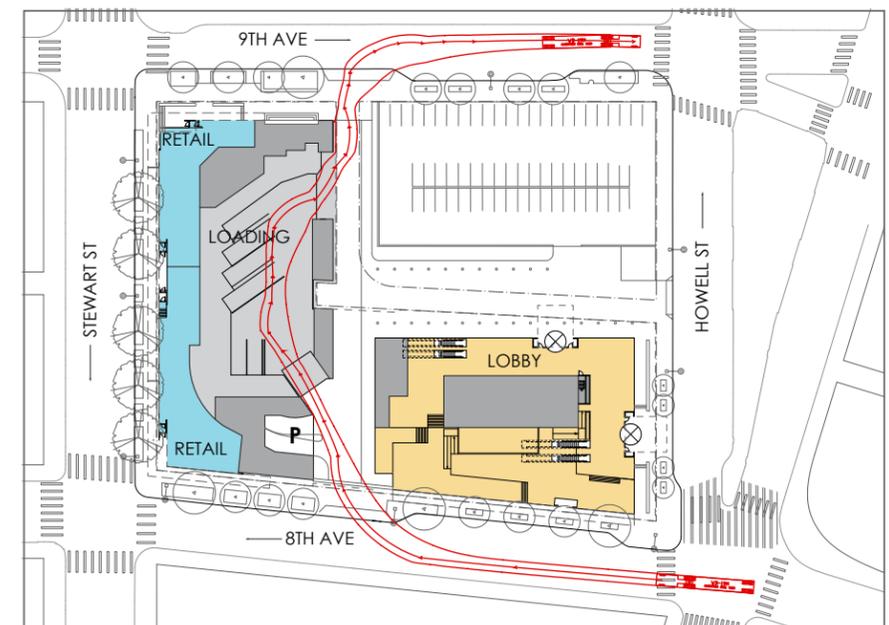
Further articulation of the podium massing adds variety and richness to the neighboring context with its bold formal composition.



B-3 Reinforce the positive urban form & architectural attributes of the immediate area.

Consider the predominant attributes of the immediate neighborhood and reinforce desirable siting patterns, massing arrangements, and streetscape characteristics of nearby development

By internalizing the most disruptive access and service components of the building, the proposed scheme prioritizes an active streetscape and creates vibrant urban edges.



B-4 Design a well-proportioned & unified building

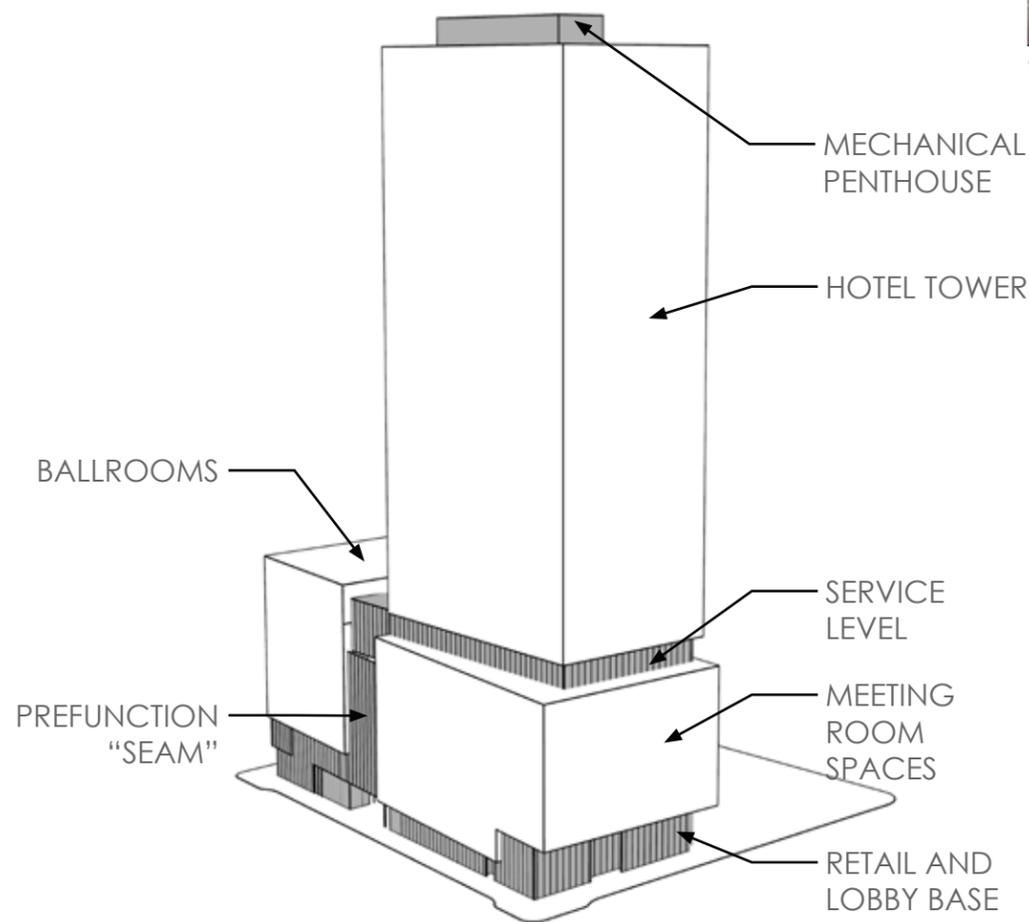
Compose the massing and organize the publicly accessible interior and exterior spaces to create a well-proportioned building that exhibits a coherent architectural concept. Design the architectural elements and finish details to create a unified building, so that all components appear integral to the whole.

There are three primary design goals of the approach:

Express the programmatic functions within: The proposed scheme seeks to express the unique programmatic composition of the building within an honest and cohesive composition. The grade level is left as transparent and open, activating the street scape to the greatest extent possible. The podium above has a geometry which extends to the property line extents, setting the podium apart as two clear volumetric elements containing the ballrooms to the north and meeting rooms to the south. Between the podium and tower, a service level will allow for the efficient placement of mechanical and service related elements for the building. This layer along with the vertical seam of the podium will be expressed as an inset that negotiates between the differing forms of the convention podium and the hotel tower above. These gestures seek to create a unified architectural composition with the various interdependent program elements.

Open the Public Realm: The concept form seeks to accentuate the ground-plane by integrating active street-level uses in a transparent sequence of urban edges. In using the alley for primary valet drop-off, the alley space will become a more safe and inviting presence in the urban fabric of the district.

Minimize the scale of the base: Through the articulation of the programmatic elements, as well as materials and formal articulation, the proposed scheme minimizes the scale of the base and its impact on the surrounding streetscapes.



View southwest from 9th Avenue and Stewart Street

C-1 Promote pedestrian interaction.

Spaces for street level uses should be designed to engage pedestrians with the activities occurring within them. Sidewalk-related spaces should be open to the general public and appear safe and welcoming.

The proposed scheme features an open and transparent ground level, designed for maximum visibility and interaction with the retail and hotel uses within. The predominant use of clear glass at grade will increase visual connectivity into the active ground floor uses along all street frontages.

C-2 Design facades of many scales.

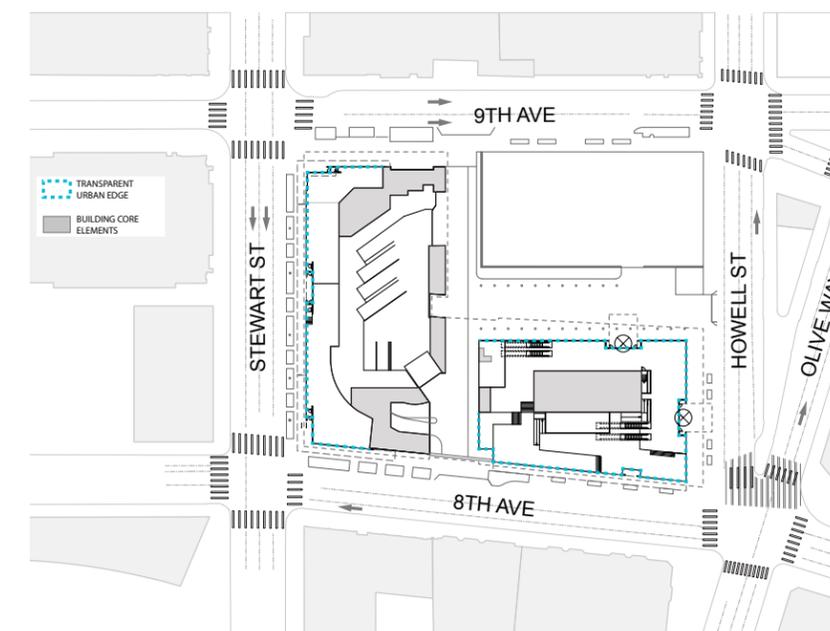
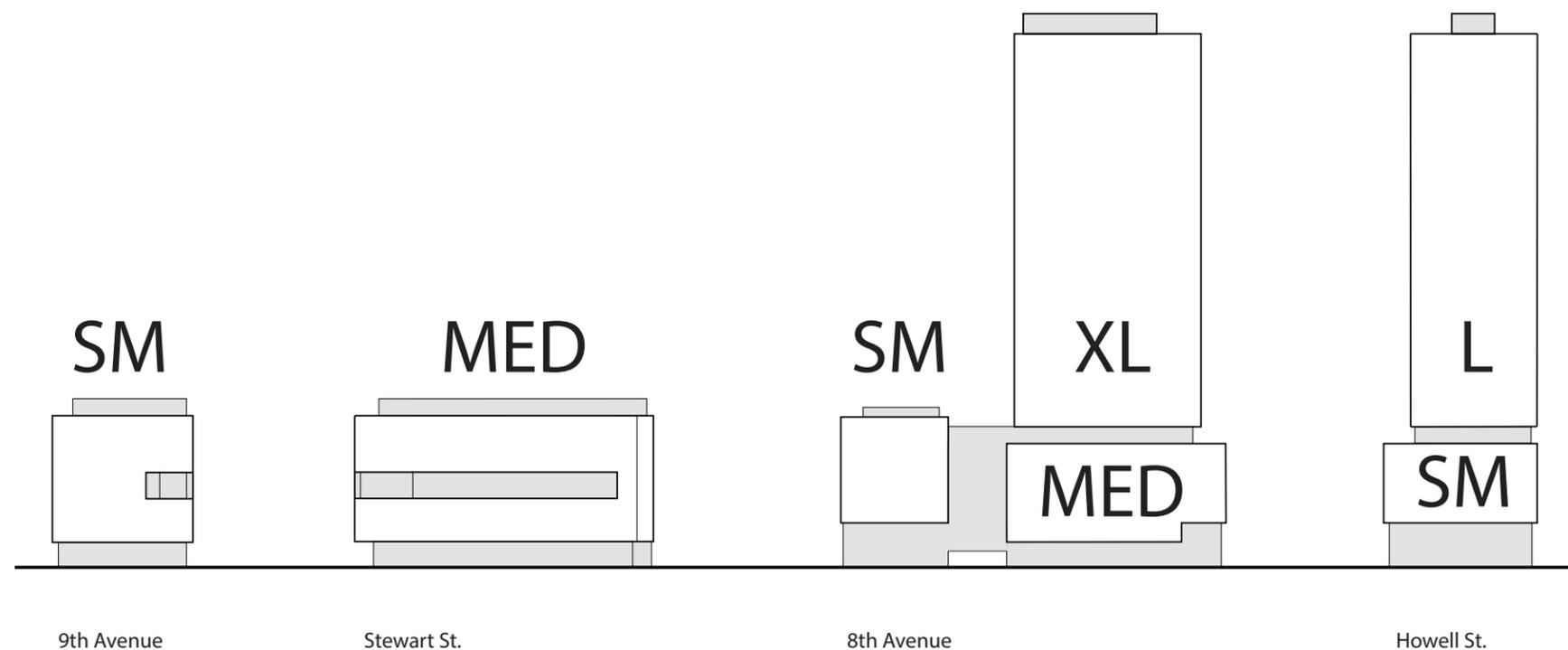
Spaces for street level uses should be designed to engage pedestrians with the activities occurring within them. Sidewalk-related spaces should be open to the general public and appear safe and welcoming.

The proposed scheme creates a simple composition of massing elements that presets a sequence of varied elevations and scales around the various street frontages. This variety of experience not only conveys the internal program elements within but relates

C-3 Provide active-not blank- facades.

Buildings should not have large blank walls facing the street especially near sidewalks.

85% of the street-frontage and pedestrian oriented facades will contain transparent, active, and accessible ground-level uses.



C-4 Reinforce building entries

To promote pedestrian comfort, safety, and orientation, reinforce the building's entry.

A hierarchy of entries will surround the urban edges of the proposed scheme. The hotel will have a significant street presence along the southern most edges of the site. A pedestrian and convention-focused entrance will be along the Howell Street elevation. A taxi and bus drop-off entrance will be located on the southern half of 8th Avenue. Finally, a valet and parking drop-off entry will be located within the alley.

A mid-block driveway will link the alley to 8th Avenue, and will provide a vehicular and pedestrian access point which will serve valet, self-serve parking, the truck loading dock, as well as staff entry.

The northern edges of the site will be lined almost entirely by retail occupancies, and will have appropriate entries to service those components.

C-5 Encourage overhead weather protection.

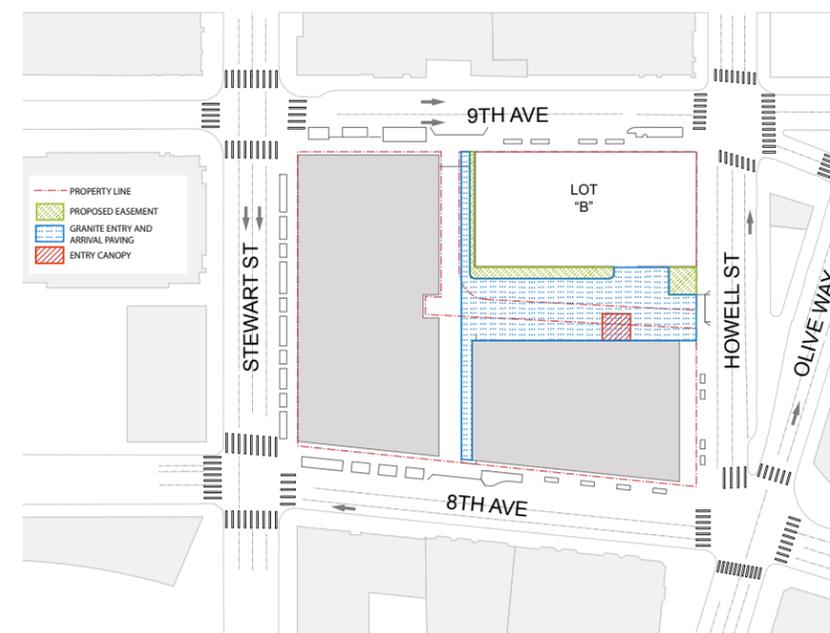
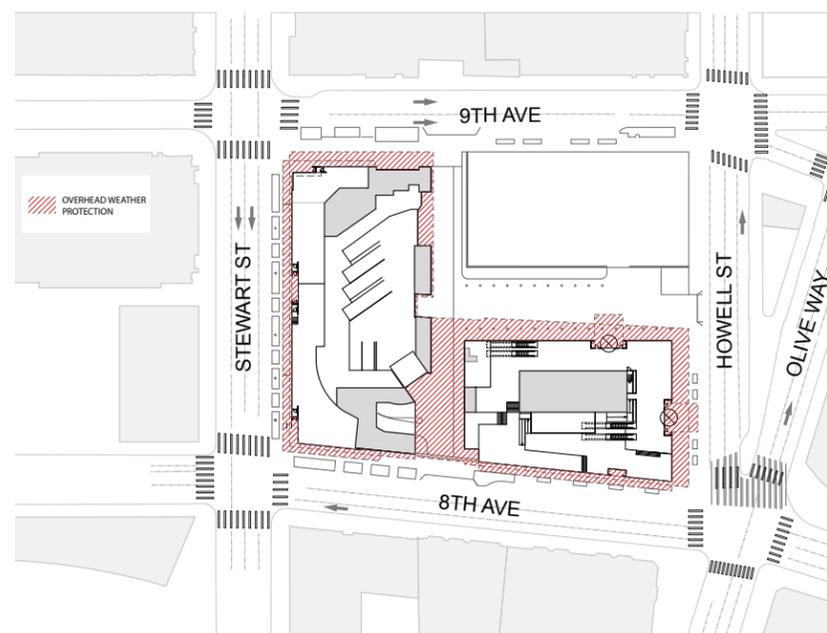
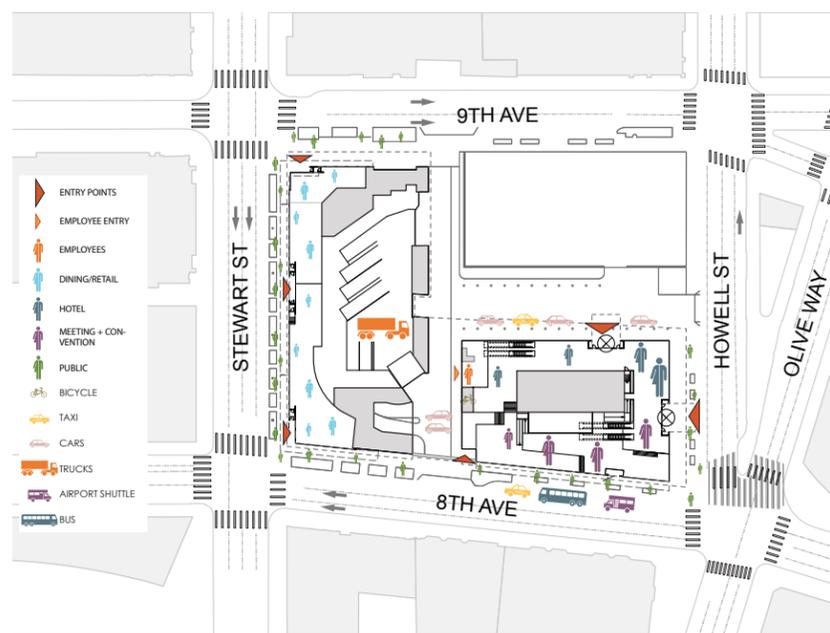
Encourage project applicants to provide continuous, well-lit, overhead weather protection to improve pedestrian comfort and safety along major pedestrian routes.

The proposed scheme will accommodate overhead weather protection along the entire façade lengths of Howell Street, 8th Avenue, and Stewart Street. The driveway crossing under the building is also protected from the weather.

C-6 Develop the Alley Facade.

To increase pedestrian safety, comfort and interest, develop portions of the alley facade in response to the unique conditions of the site or project.

The proposed scheme will provide a highly finished alley both in the building facade and ground plane as appropriate for a hotel main entry and drop-off. The ground plane will include high-quality paving treatments, pedestrian lighting and landscape buffer from the adjacent existing surface parking lot. Although the alley contains access to building support services such as loading, trash and garage access, the alley facades are composed of the same high-quality materials as the street elevations.



D-1 Provide inviting & usable open space.

Design public open spaces to promote a visually pleasing, safe, and active environment for workers, residents, and visitors. Views and solar access from the principal area of the open space should be especially emphasized.

The proposed scheme seeks to widen the pedestrian areas of the surrounding sidewalks as appropriate, providing curb-bulbs to minimize the distance of the crossings at intersections. The ground level of the building will be populated by very active uses, and the building will remain a vibrant neighborhood presence throughout the day and night.

The proposed scheme also places the primary valet and parking drop-off access within the alley, eliminating the traditional porte-cochere from the surrounding sidewalks. Furthermore, the alley will feature an enhanced paving surface to make it a more inviting and safe space, appropriate to a main entry of the proposed hotel.

In locating the tower along the southern edge of the site, the impact of shadows on the adjacent streets has been minimized.

A significant Green Street setback is proposed at the ground level. Coupled with the 9th Avenue curb bulb, a generous and inviting open space is provided with widened sidewalk, added landscape, seating and a feature “green wall”.

D-2 Enhance the building with landscaping.

Enhance the building and the site with generous landscaping which includes special pavements, trellises, screen walls, planters, and site furniture, as well as living plant material.

The proposed scheme seeks to preserve the large existing trees along Stewart Street and enhance the sidewalks of Howell Street and 8th and 9th Avenues with new tree plantings along the sidewalk edge. Appropriate site furnishings may also be provided in front of the Hotel entries on Howell Street and 8th Avenue.

D-3 Provide elements that define the place.

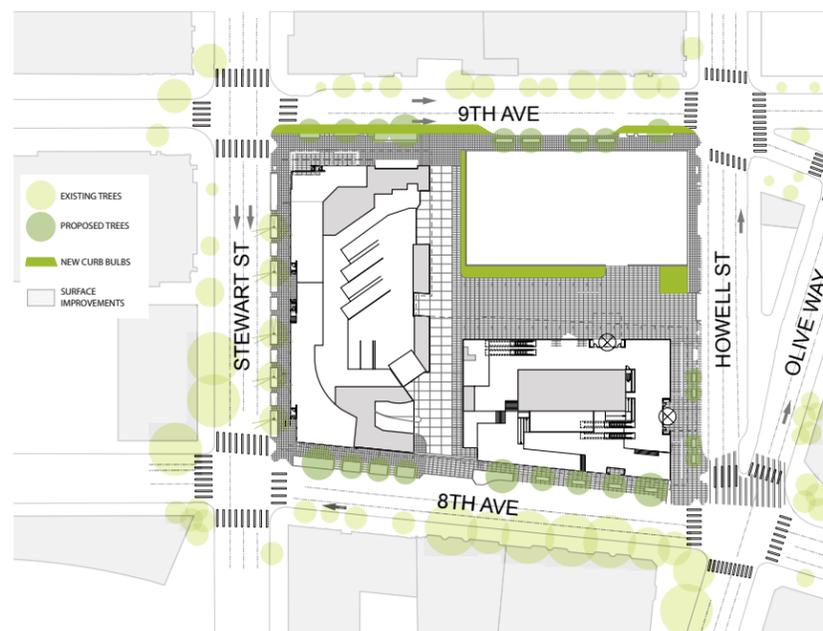
Provide special elements on the facades, within public open spaces, or on the sidewalk to create a distinct, attractive, and memorable “sense of place” associated with the building.

The proposed scheme enhances the urban form of Seattle in three primary ways:

Celebrating the Olive and Howell Triangle: By placing both the primary entry to the hotel and orienting the hotel tower form towards the Olive and Howell triangle, the project will help define this significant shift in the urban grid as a major spatial moment in the urban form of Seattle.

Enhancing the alley: By internalizing the valet and parking drop-off, as well as the truck loading and service, the existing alley will be vastly improved and feature the continuous, 24 hour presence of the hotel lobby.

Activating the perimeter: With an internalized service and loading zone, the proposed scheme provides the maximum amount of active street level uses around the perimeter, creating a vibrant street life within the neighborhood.



D-4 Provide Appropriate Signage.

Design signage appropriate for the scale and character of the project and immediate neighborhood. All signs should be oriented to pedestrians and/or persons in vehicles on streets within the immediate neighborhood.

Exterior signage will be appropriately placed scaled to the surrounding context. The transparent ground-level of the building will provide opportunities for commercial uses to integrate signage and identity within their space.

D-5 Provide Adequate Lighting.

To promote a sense of security for people downtown during nighttime hours, provide appropriate levels of lighting on the building facade, on the underside of overhead weather protection, on and around street furniture, in merchandising display windows, and on signage.

A well integrated lighting approach will be further developed in the project design including pedestrian-level lighting of the alley and mid-block driveway, as well as façade and penthouse lighting. Refer to pages 74 and 42 for schematic lighting plan and elevations.

D-6 Design for Personal Safety & Security.

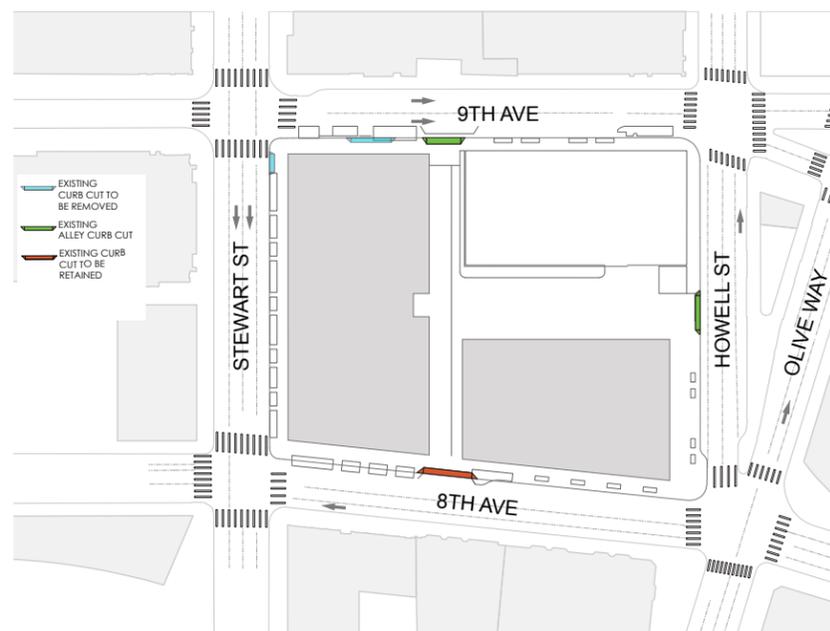
Design the building and site to enhance the real and perceived feeling of personal safety and security in the immediate area.

The visual transparency and around-the-clock occupancy of a majority of the ground floor will help create a safe pedestrian environment throughout the day.

E-1 Minimize curb cut impacts.

Minimize adverse impacts of curb cuts on the safety and comfort of pedestrians.

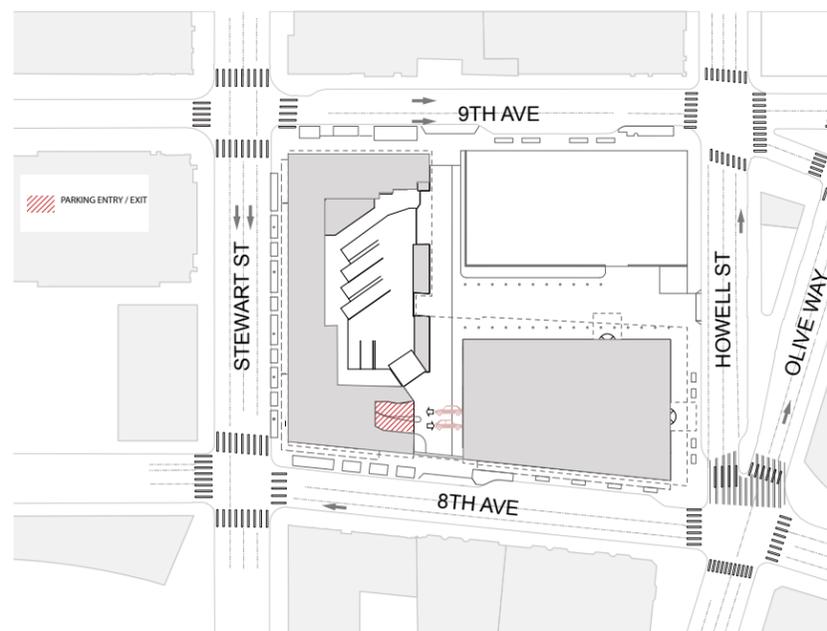
There are currently five curb cuts adjacent to the site, including the two alley curb cuts, and the proposed scheme would require only three curb cuts.



E-2 Integrate parking facilities.

Minimize the visual impact of parking by integrating parking facilities with surrounding development. Incorporate architectural treatments of suitable landscaping to provide for the safety and comfort of people using the facility as well as those walking by.

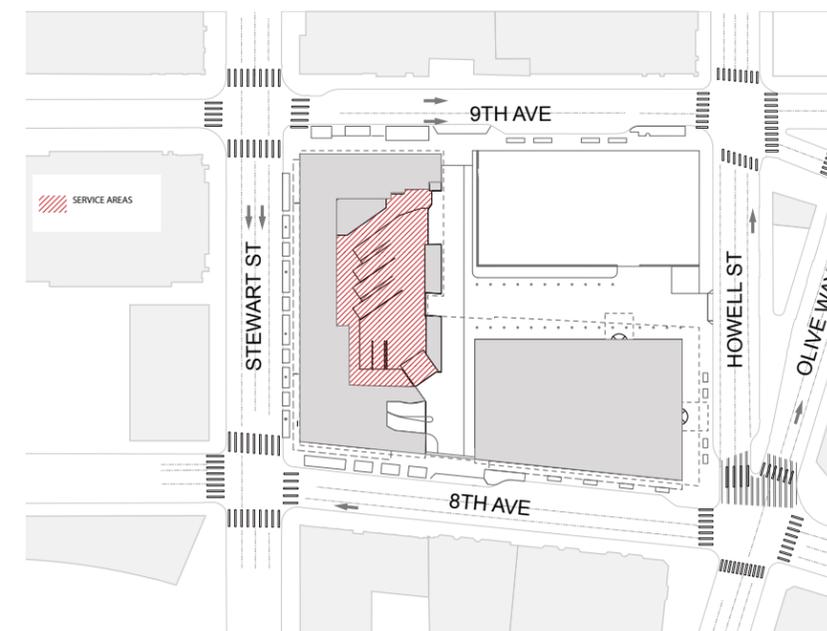
The proposed scheme places all parking facilities underground and places the primary parking entry within the block. Exiting parking traffic will benefit from three means of egress from the site, along 8th and 9th Avenues as well as Howell Street.



E-3 Minimize the presence of service areas.

Locate service areas for trash dumpsters, loading docks, mechanical equipment, and the like away from the street front where possible. Screen from view those elements which for programmatic reasons cannot be located away from the street front.

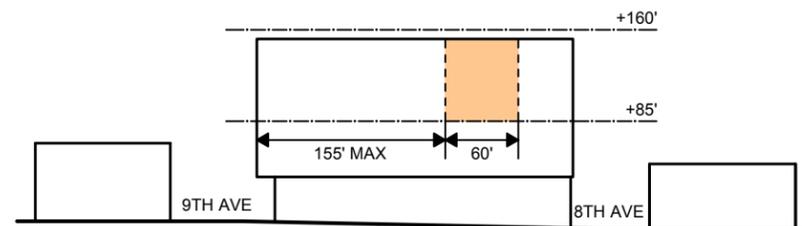
The proposed scheme minimizes the presence of service areas and places all loading and service for the project within the alley and away from public view.



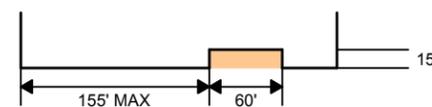
9 DEPARTURE REQUESTS

Stewart Street Façade Modulation

ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	RATIONALE	DOWNTOWN DESIGN GUIDELINES REINFORCED
1	23.49.058.B.1 Façade Modulation	<p>Façade modulation is required above a height of eighty-five feet above the sidewalk for any portion of a structure located within 15 feet of a property line. No modulation is required for portions of a façade set back fifteen feet or more from a street property line.</p>	<p>The proposed scheme introduces a horizontally-oriented modulation in lieu of the prescribed vertical modulation.</p> <p>The north facing elevation along Stewart Street expresses the functionality of the programs within the building. At grade, a transparent and active retail façade will line the full length of Stewart Street. In the podium above, a similar theme will be echoed in the exposure of the prefunction spaces of the meeting levels. A large, 26.5-foot tall by 235-foot wide glazed aperture will allow the prefunction gathering activity of the spaces within to animate and modulate the façade. This element will also be set-back 8 feet, further enhancing the horizontal modulation proposed.</p> <p>The prescribed modulation would create a conflicting recess in the floor plan, compromising the functionality of the large ballroom and service spaces within.</p>	<p>A-1 Respond to the physical environment</p> <p>B-1 Respond to the neighborhood context</p> <p>B-2 Create a transition in bulk and scale</p> <p>B-3 Reinforce the positive urban form & architectural attributes of the immediate area</p> <p>B-4 Design a well-proportioned and unified building</p> <p>C-2 Design Facades of many scales</p> <p>D-1 Provide inviting and usable open space</p> <p>D-3 Provide elements that define the place</p>

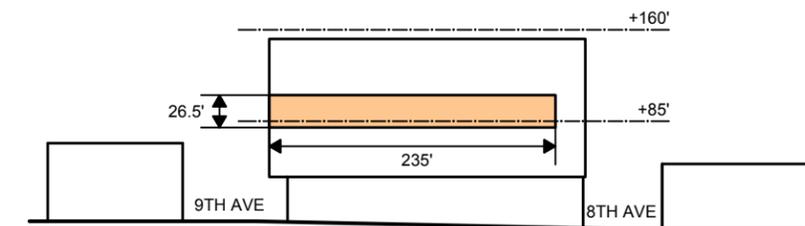


NORTH ELEVATION

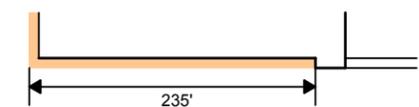


PLAN

PRESCRIBED MODULATION



NORTH ELEVATION

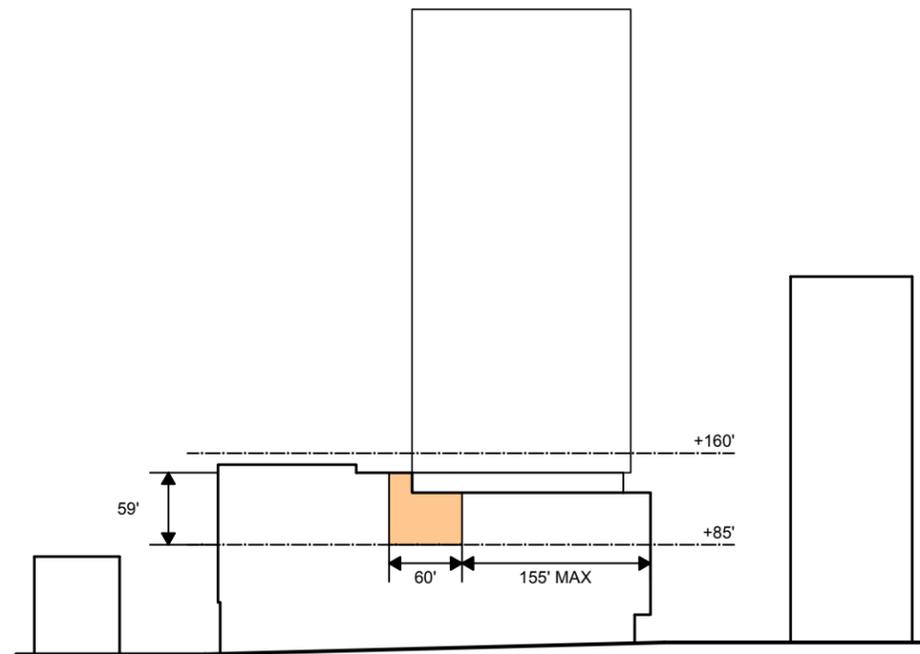


PLAN

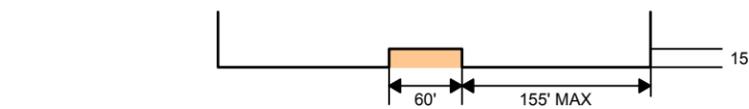
PROPOSED DEPARTURE

8th Avenue Façade Modulation

ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	RATIONALE	DOWNTOWN DESIGN GUIDELINES REINFORCED
2	23.49.058.B.1 Façade Modulation	<p>Façade modulation is required above a height of eighty-five feet above the sidewalk for any portion of a structure located within 15 feet of a property line. No modulation is required for portions of a façade set back fifteen feet or more from a street property line.</p>	<p>The proposed scheme introduces a vertically-oriented modulation similar to the prescribed vertical modulation.</p> <p>Similar to the departure request along Stewart Street, the west elevation facing 8th Avenue proposes a vertical band of glazing, 126-feet tall and 46-feet wide. This vertical seam will be recessed 3 feet and provides a distinctive break between the ballroom massing to the north and the meeting room massing to the south, further emphasizing the modulation of the façade planes and materials.</p> <p>The prescribed modulation would require a conflicting recess in the floor plan near the center of the façade's elevation, compromising the functionality of the meeting and hotel levels above.</p>	<p>A-1 Respond to the physical environment</p> <p>B-1 Respond to the neighborhood context</p> <p>B-2 Create a transition in bulk and scale</p> <p>B-3 Reinforce the positive urban form & architectural attributes of the immediate area</p> <p>B-4 Design a well-proportioned and unified building</p> <p>C-2 Design façades of many scales</p> <p>D-1 Provide inviting and usable open space</p> <p>D-3 Provide elements that define the place</p>

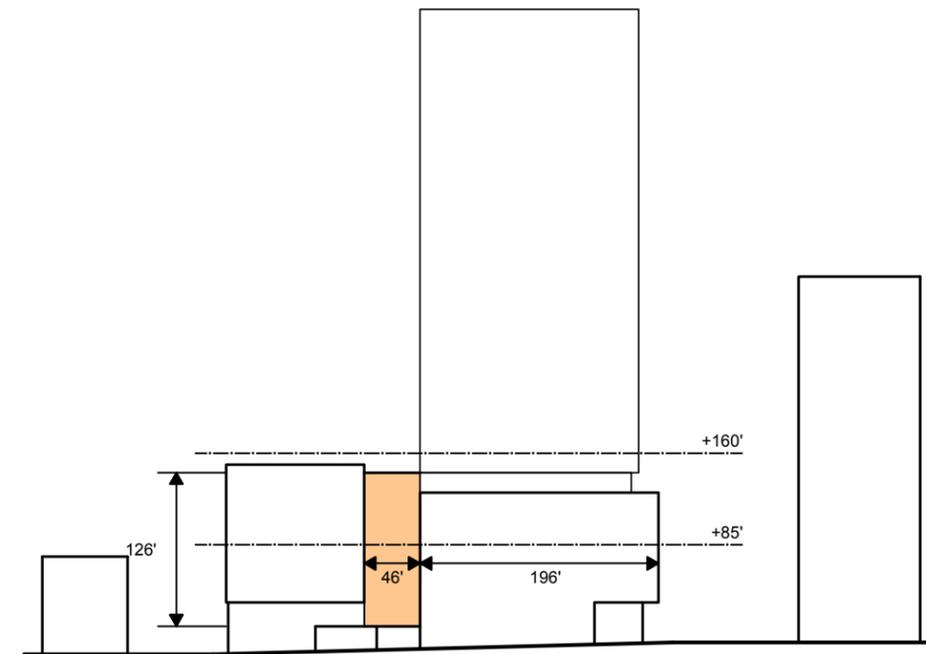


WEST ELEVATION



PLAN

PRESCRIBED MODULATION



WEST ELEVATION

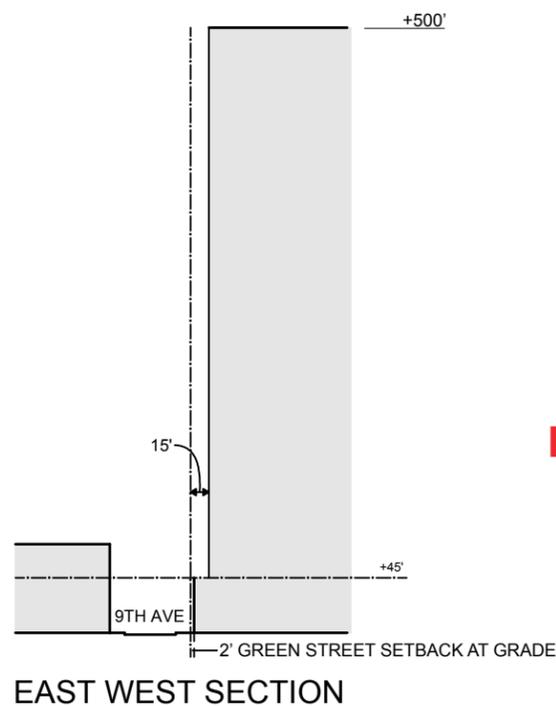


PLAN

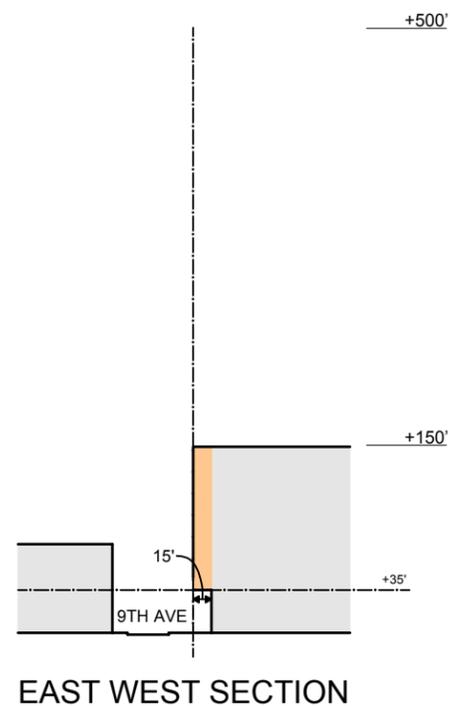
PROPOSED DEPARTURE

Green Street Setback

ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	RATIONALE	DOWNTOWN DESIGN GUIDELINES REINFORCED
3	23.49.058.F.2 Upper Level Setback at Green Street	An upper level setback is required at a Green Street above a height of forty-five feet above the sidewalk for any portion of a structure located within 15 feet of a property line. No modulation is required for portions of a facade set back fifteen feet or more from a street property line.	<p>The proposed scheme takes into consideration the existing streetscape, sidewalk width and view corridor condition for the full length of the 9th Avenue Green Street. The proposal introduces a recess at the ground floor to create a generous pedestrian experience at a significant intersection in the full Green Street expanse at Stewart Street. The recess is a 15 foot ground floor setback that is 35 feet high and provides a widened sidewalk, seating, landscape, an animated outdoor retail area and a special 'green wall' landscape feature. The building would return to the property line above 35' up to the roof level of 150 feet, well below the available building height allowing less shadow and more direct daylight into the public right-of-way. The east side of the building is also composed with an upper level set back 26 feet high by 48 feet wide to provide additional relief to the Green Street façade.</p> <p>The prescribed upper level setback would create a conflicting recess in the floor plan of multiple ballroom levels compromising the functionality of the large ballroom and service spaces within.</p>	<p>A-1 Respond to the physical environment</p> <p>B-1 Respond to the neighborhood context</p> <p>B-2 Create a transition in bulk and scale</p> <p>B-3 Reinforce the positive urban form & architectural attributes of the immediate area</p> <p>B-4 Design a well-proportioned and unified building</p> <p>C-2 Design Facades of many scales</p> <p>D-1 Provide inviting and usable open space</p> <p>D-3 Provide elements that define the place</p>



PRESCRIBED SETBACK



PROPOSED SETBACK



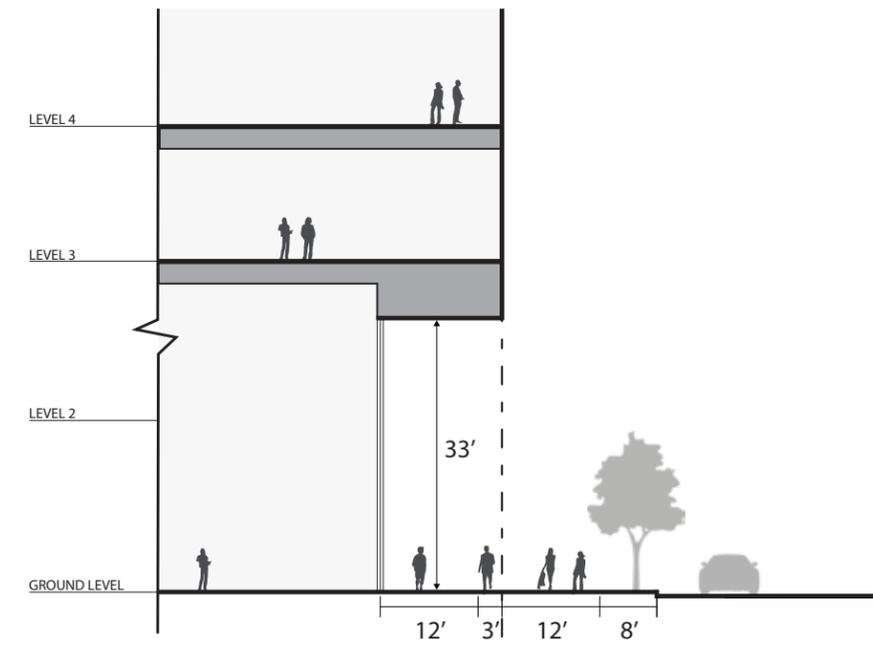
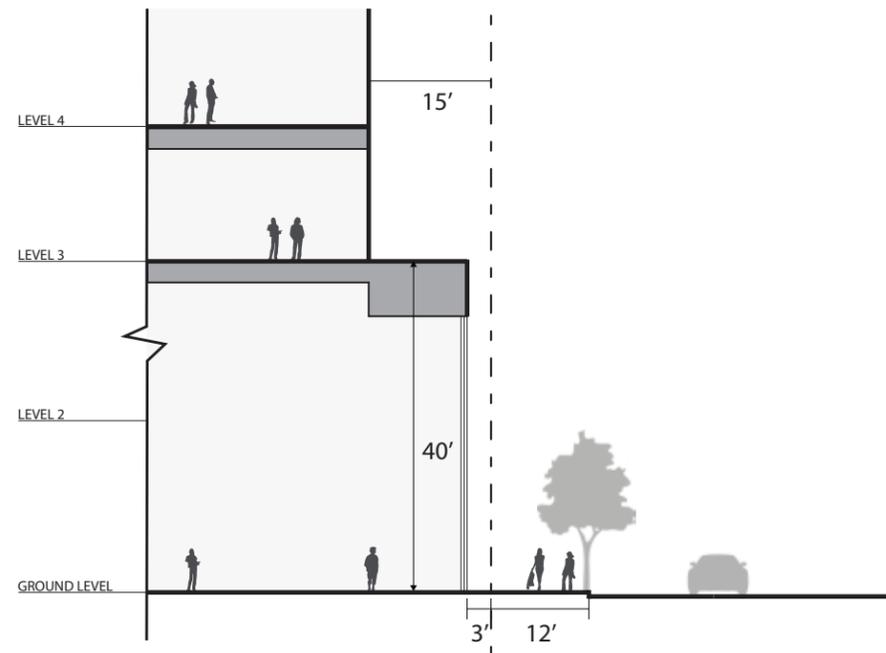
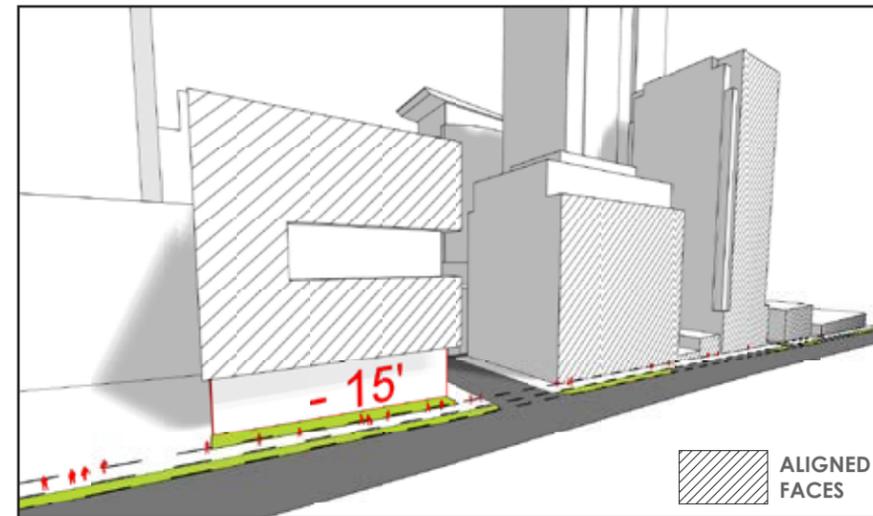
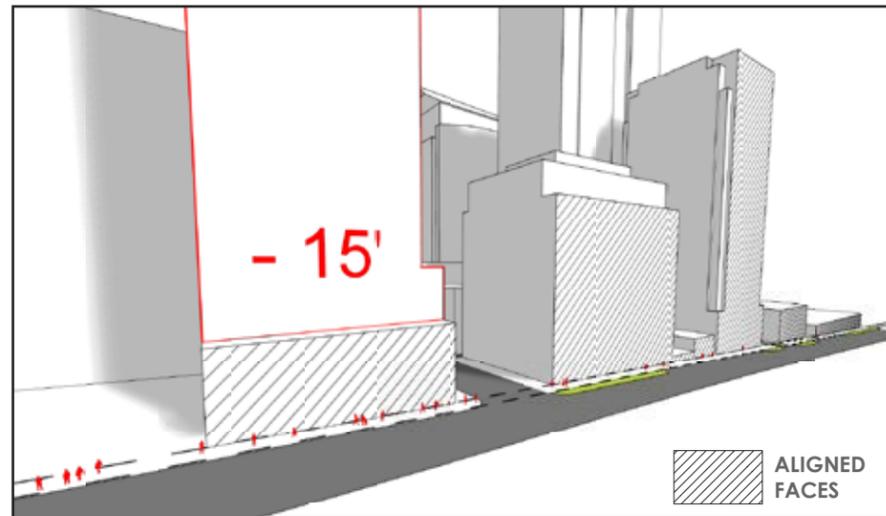
PRESCRIBED SETBACK



PROPOSED SETBACK

Green Street Setback (Continued)

Alignments and Open Space, Schematic Sections



AS PRESCRIBED BY CODE

- Upper tower setback 15' relative to neighboring buildings.
- Lower portion (first 45' above sidewalk) aligned with neighboring buildings, creates typical sidewalk condition.

PROPOSED DESIGN

- Podium aligned with neighboring buildings.
- Grade level retail (up to 35' above grade) set back 15', creates occupiable pocket space within the green street.
- Grade level retail (up to 35' above grade) set back 15', creates a generous occupiable space at-grade along the green street.

Green Street Setback (Continued)

Massing Comparison Looking South and North on 9th Ave



Looking South



Looking North



Looking South



Looking North

AS PRESCRIBED BY CODE

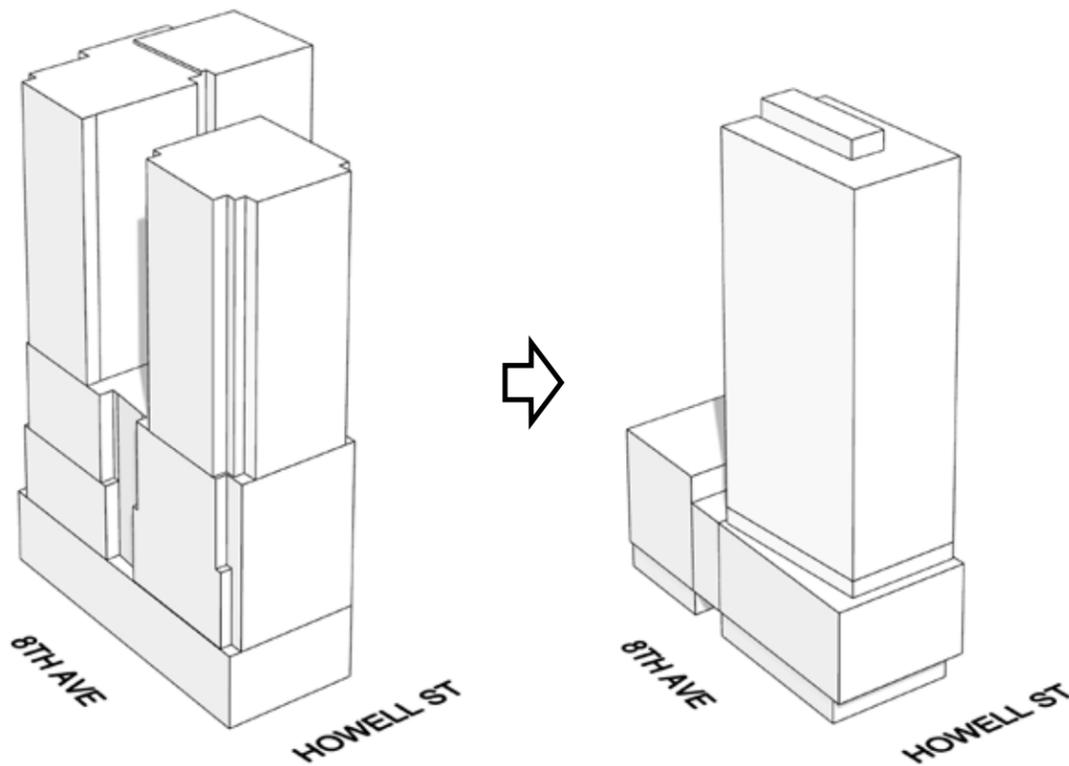
- Upper tower setback 15' relative to neighboring buildings.
- Lower portion (first 45' above sidewalk) aligned with neighboring buildings, creates typical sidewalk condition.

PROPOSED DESIGN

- Podium aligned with neighboring buildings.
- Grade level retail (up to 35' above grade) set back 15', creates a generous occupiable space at-grade along the green street.

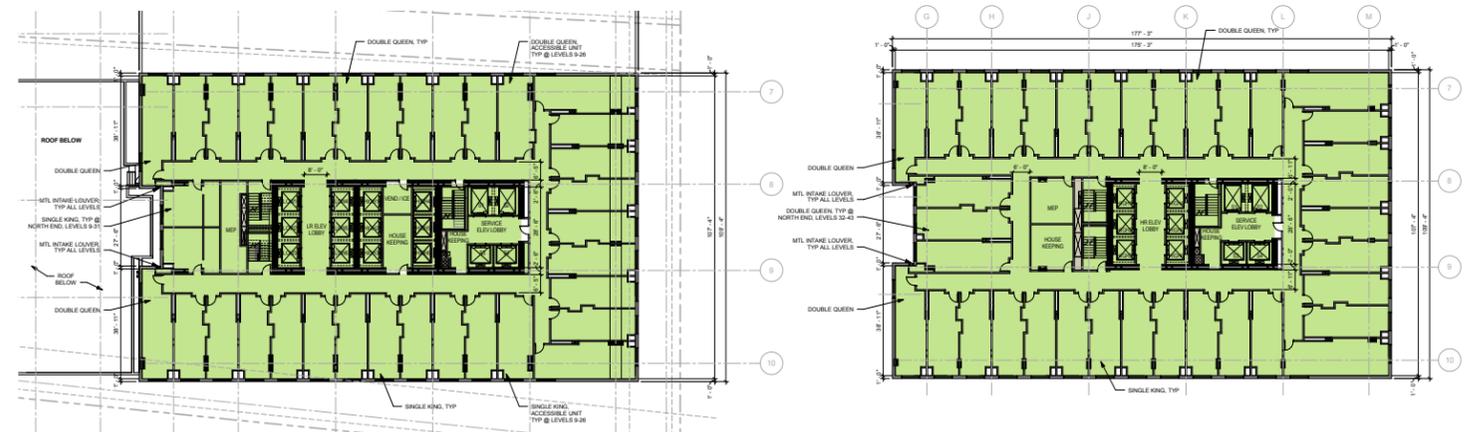
8th Avenue Tower Width

ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	RATIONALE	DOWNTOWN DESIGN GUIDELINES REINFORCED
3	23.49.058.C Upper-Level Width Limit	On lots where the width and depth of the lot each exceed two hundred (200) feet, the maximum facade width for any portion of a building above two hundred forty (240) feet shall be one hundred forty-five (145) feet along the general north/south axis of a site (parallel to the Avenues), and this portion of the structure shall be separated horizontally from any other portion of a structure on the lot above two hundred forty (240) feet by at least eighty (80) feet at all points.	The proposed scheme seeks to minimize the impact of the tower massing on the street while creating the most efficient use of floor area within the tower itself. To achieve the desired program quantity and urban design objectives, a departure for the Upper Level Width Limit is requested. The most efficient tower footprint that achieves the specific programmatic requirements of the central elevator and service core creates a slightly wider tower footprint in the upper levels, and in the lower tower levels requires a set-back extension of the floor-plate to accommodate the wider core element. The increased width of the lower portion will be set back 23 feet from the tower face. The proposed tower form will have a minimal impact on the streetscape below, emphasizing the tall vertical form of the tower rather than the alternative bulk extending the podium massing to the height limit of 240-feet.	A-1 Respond to the physical environment B-1 Respond to the neighborhood context B-2 Create a transition in bulk and scale B-3 Reinforce the positive urban form & architectural attributes of the immediate area



ALLOWABLE MASSING PER CODE

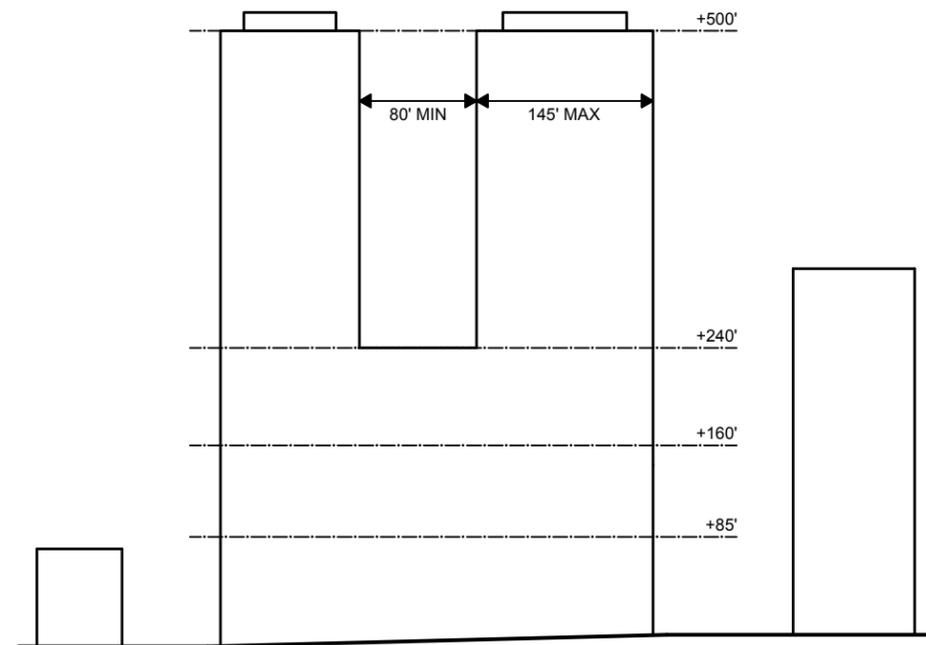
PROPOSED MASSING



LOWER TOWER PLAN

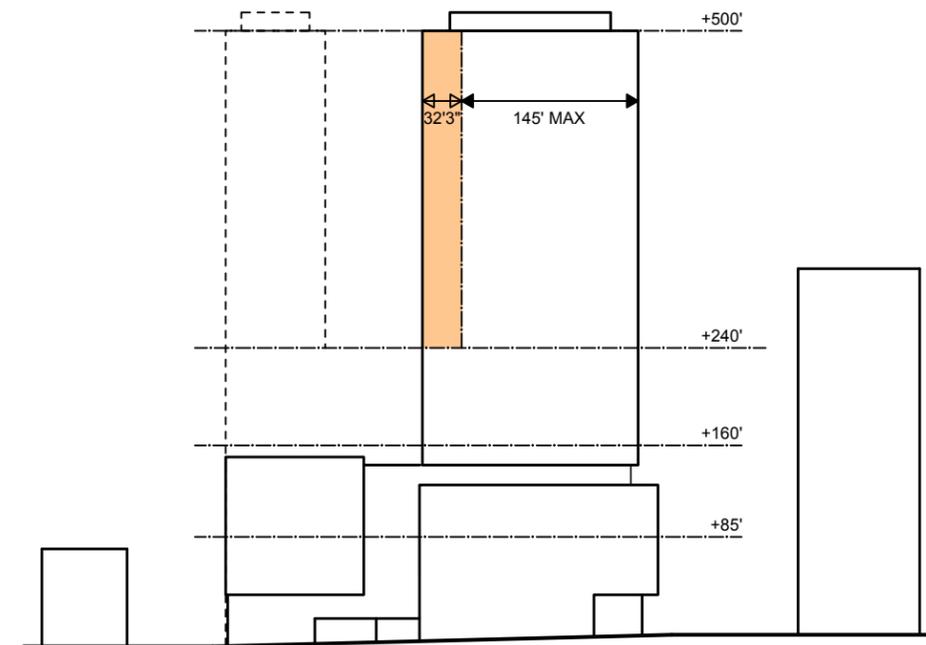
UPPER TOWER PLAN

8th Avenue Tower Width (Diagrams Continued)



WEST ELEVATION

PRESCRIBED MODULATION AND TOWER WIDTH



WEST ELEVATION

PROPOSED DEPARTURE