



## DESIGN REVIEW BOARD RECOMMENDATION MEETING

2ND + STEWART | 1613 2ND AVENUE

DPD# 3016702 September 29, 2015

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# Project Vision

The development site at Second Avenue, from Stewart Street to Pine Street, is located adjacent to the Pike Place Market Historic District and just a few short blocks from the City’s shopping core. It is situated on one of the highest intersections in the downtown area and borders the edge where a shift in the City street grid occurs, forming a tapered, rectangular parcel adjacent to the 1908 era Broadacres Building. The project's location and site conditions are rich in potential and require a sound and thoughtful architectural response - a composition of necessities, grounded in the authenticity of the neighborhood.

Streetscapes offer variety of attitude and activity. Stewart Street is pedestrian-scaled and has a strong visual relationship to the Market and Elliott Bay beyond. Second Avenue is a broad, north to south, heavily-used city connector for vehicles, bicycles, and King County Metro and Sound Transit bus services. Each street is highly active and unique in character. The tower design responds with retail frontage lining both streets, including multiple entrances activating the pedestrian realm and featuring a tall retail space that anchors the corner. The Stewart Street facade is pedestrian-scaled. Residential Units and balconies wrap the street corner, while a tall bay window of a residential lounge highlights the Water/ Market connection. The Second Avenue frontage offers a slender tower face, featuring the primary residential building entrance.

Tower massing relates to the greater cityscape via a reflection of the shift in the street grid occurring at the intersection of Second Avenue and Stewart Street. The east facade cants away from Second Avenue and perpendicular to Stewart Street. This same angle is expressed on the southwest facade over the Broadacres Building. This allows some visual separation between the proposed structure and the tower at the south corner of second and pine. The upper-level tower form continues the shifted geometries of the street grid to form the roof level amenity rooms and terraces. At the roof, common recreation program, and elevator penthouse is organized to provide a unique and direct engagement of the structure and skyline. The interior rooms allow occupants to connect with dramatic urban and natural vistas.

### DEVELOPMENT OBJECTIVES -

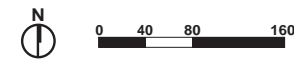
The proposal is a 400', 39-story, mixed-use structure with associated services and common recreation areas. Ground level retail with below and above grade parking are intended.

Approximate quantities are as follows:

Residential:	±177 apartment units
Affordable housing incentive:	Cash payment option proposed
Ground level retail:	±2800 sf
Parking:	±145 stalls; consisting of 7 levels below grade, 4 levels above grade

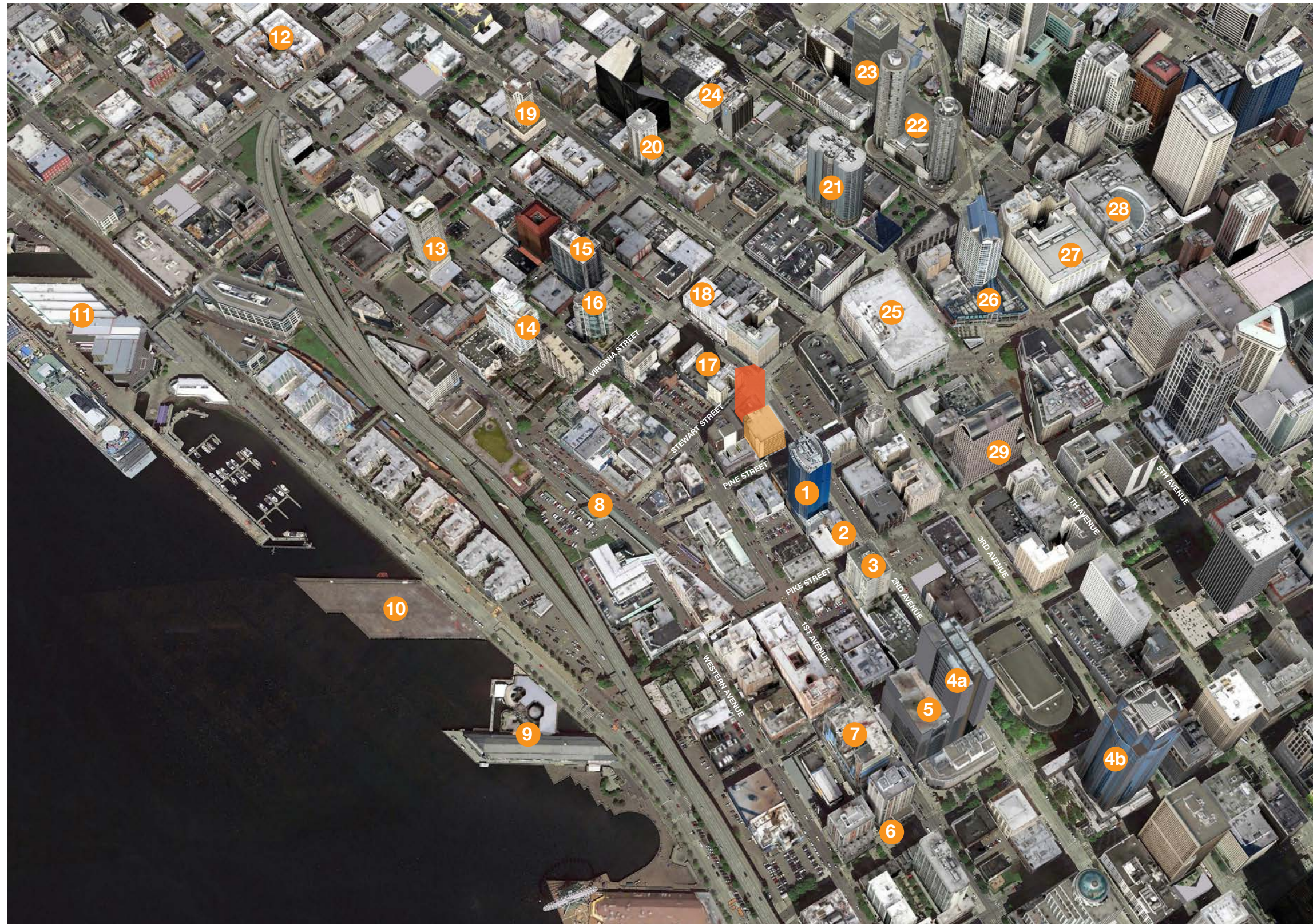
Potential departure requests:	Facade maximum setback limits
	Continuous overhead weather protection
	Parking aisle width





Stewart Street:  
Class I Pedestrian Street  
Principal Transit Street:  
No view corridors  
Second Avenue:  
Class I Pedestrian Street  
Principal Transit Street:  
No view corridors



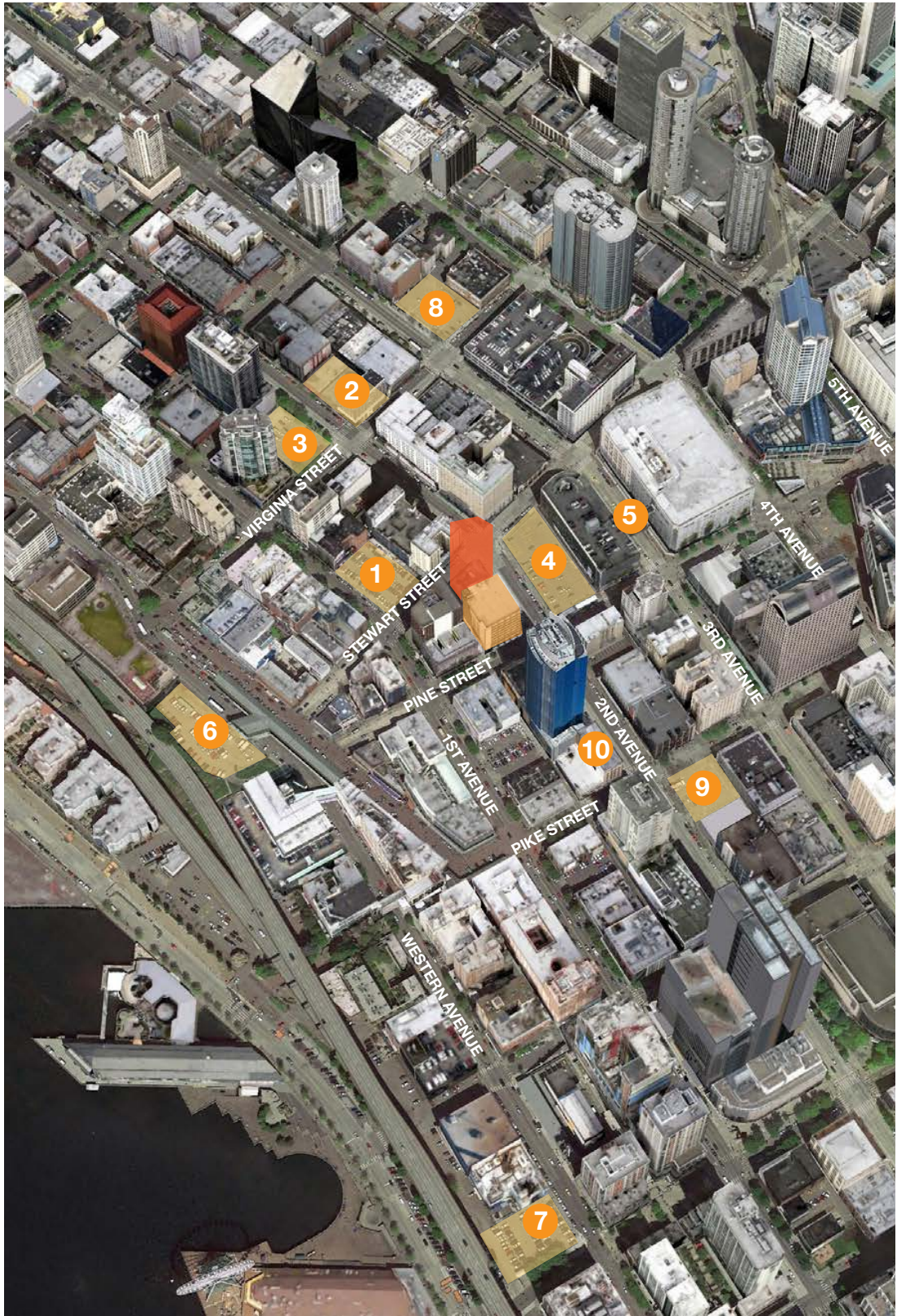


- 1 Fifteen Twenty-One 2nd Avenue
- 2 Eitel Building
- 3 Newmark Tower / Target
- 4a Russel Investment Center
- 4b 1201 3rd Avenue
- 5 Seattle Art Museum
- 6 Harbor Steps
- 7 Four Seasons Hotel / Residences
- 8 Pike Place Market
- 9 Seattle Aquarium
- 10 Pier 62 & 63
- 11 Bell Street Pier
- 12 Belltown Court
- 13 Continental Place Condominiums
- 14 Market Place Tower
- 15 Cristalla Residences
- 16 One Pacific Tower
- 17 Viktoria
- 18 Moore Theatre
- 19 The Grand View Condominiums
- 20 Royal Crest Condominiums
- 21 Escala
- 22 The Westin Seattle
- 23 The Westin Building
- 24 Cinerama
- 25 Macy's
- 26 Westlake Center
- 27 Nordstrom
- 28 Pacific Place
- 29 Century Square









- 1 1900 1st Avenue - Apartments / Hotel
- 2 2000 2nd Avenue - 9-story Hotel
- 3 2001 2nd Avenue
- 4 204 Pine Street
- 5 3rd Avenue Bus Corridor Improvements
- 6 PC1-North
- 7 Western & University
- 8 2000 3rd Avenue
- 9 1430 2nd Avenue
- 10 Eitel Building



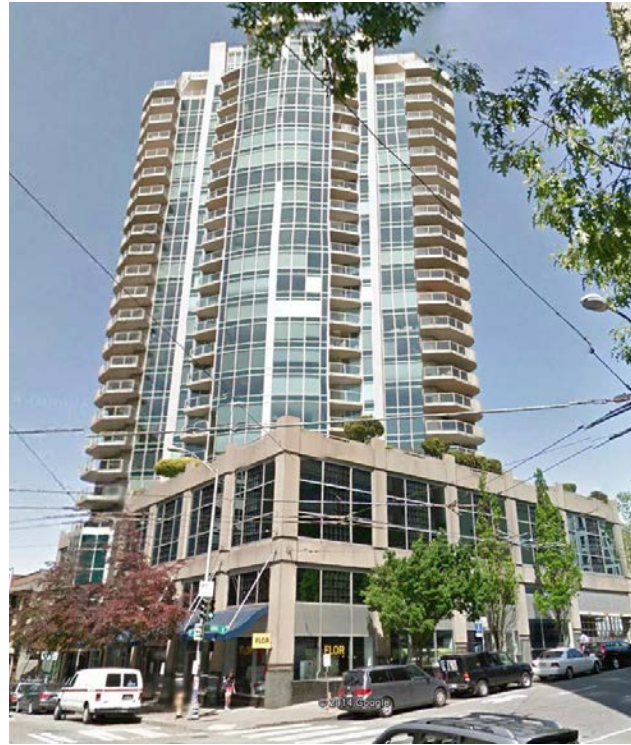


- Belltown
- Commercial Core
- Pike Place Market
- Towers
- Parks
- Transit
- View Corridor
- 1st Avenue
- || Sun Path

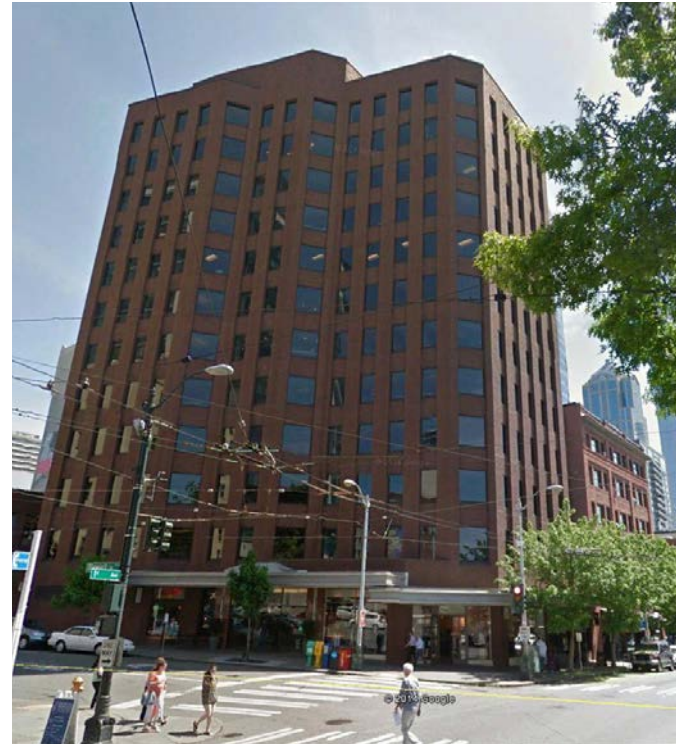




2nd & Pine (under construction)



1st & Virginia



101 Stewart



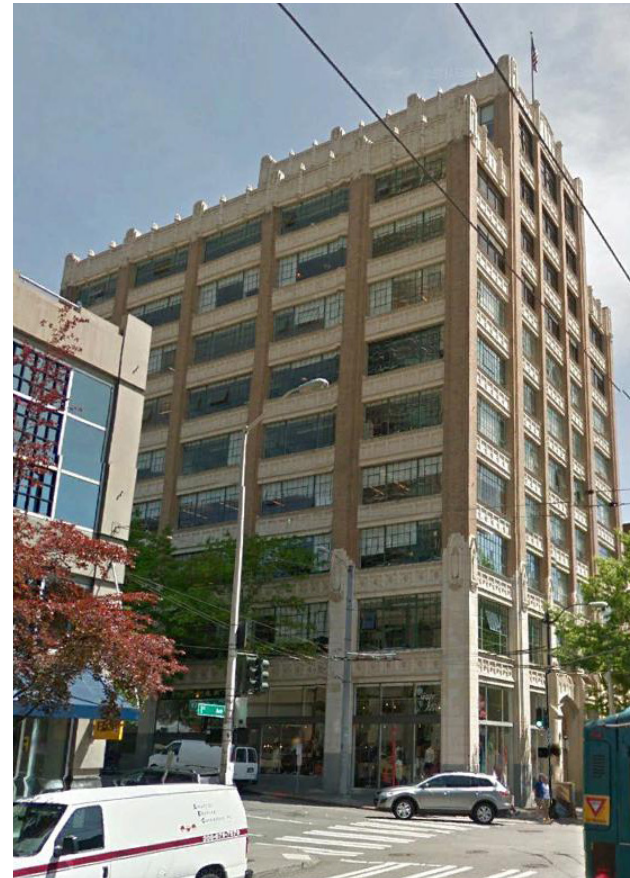
2nd & Pine - Doyle Building



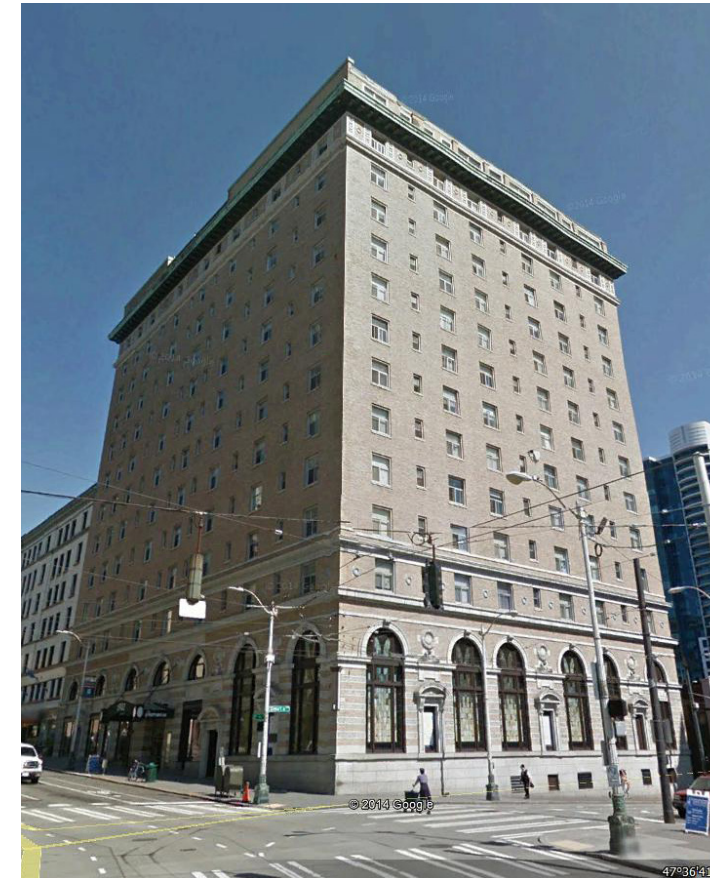
Hotel on 1st & Stewart (under construction)



Cristalla Building



Terminal Sales Building



Josephinum Building



Viktoria Building



## EDG#2 BOARD DIRECTION

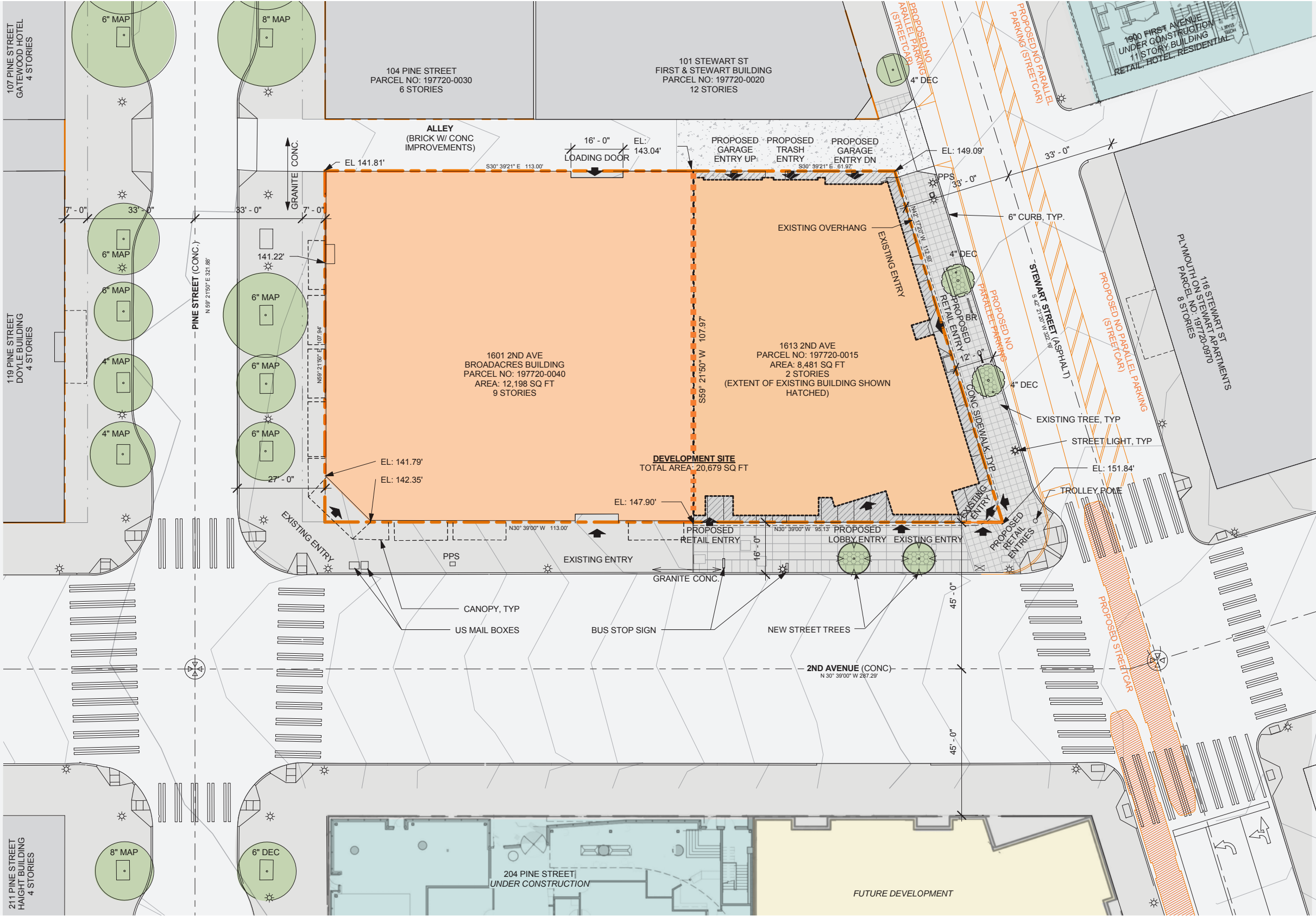
**MASSING** Explore the "shift and cant" of the street geometry

**ROOF CONFIGURATION** Define its relationship to the whole

**STREET LEVEL DEVELOPMENT** Activate the pedestrian experience

**ALLEY** Demonstrate vehicular access and unify the corner with the whole



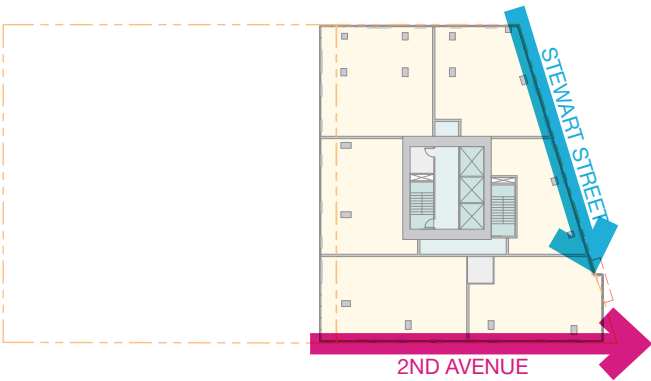




EDG#2



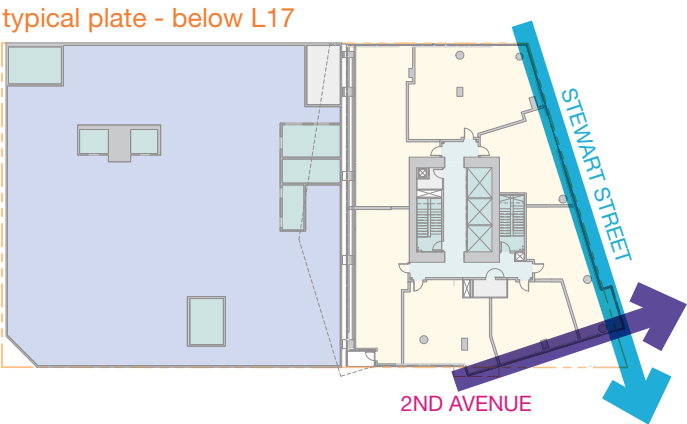
zf west on Stewart Street



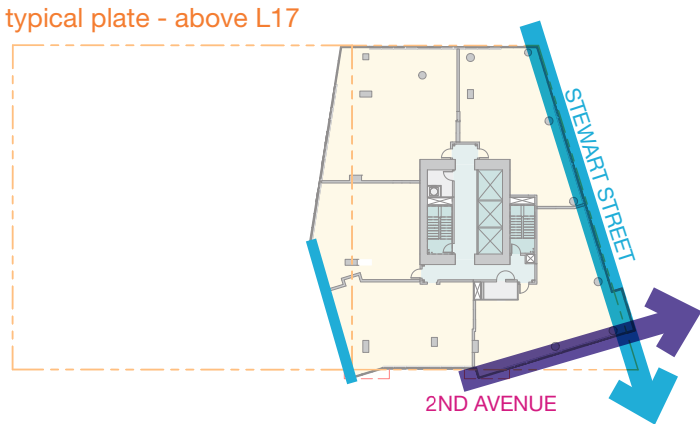
DRB



view west on Stewart Street



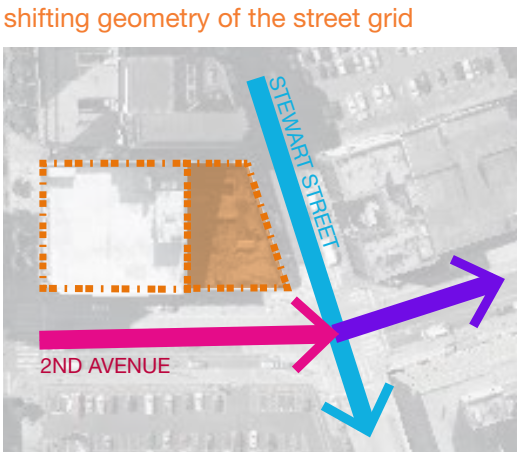
view from southwest



**Board's Deliberations:**  
“There was a good deal of discussion, however, and differences in feelings expressed, regarding preferences between the two new alternatives. While alternative two was thought to be a simpler, more perceptibly slender, calmer, and elegant design, one of the Board members responded more favorably to the shift and cant that occurred at the 17th floor of alternative 1. It was thought that alternative 1 deserved further study and exploration as a viable alternative, and the design team was encouraged to do that as the proposal progressed.”

**Response:**  
This DRB proposal has progressed EDG #2's alternative 1 canted massing to reflect the change in the street grid as requested by the board. The preferred ground level and above grade parking co-mingled with residential units of the EDG #2 alternative 2 has been essentially maintained with refinement based upon the Board's direction described below.

**A-1** Respond to the Physical Environment



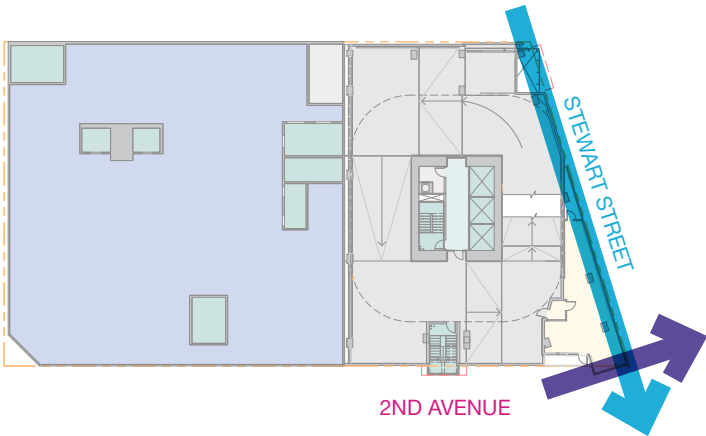
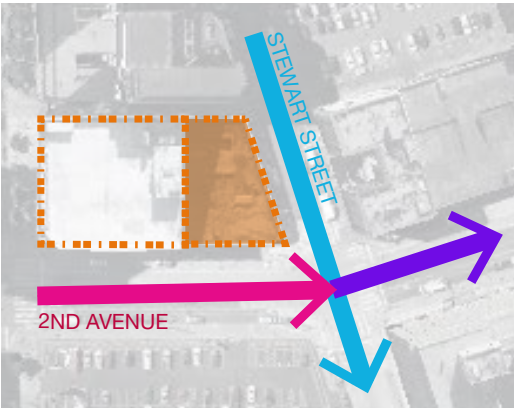


DRB .....



view from 2nd + Stewart

typical plate - above L17



**A-2** Enhance the Skyline

**C-6** Develop the Alley Facade

**A-1** Respond to the Physical Environment

**C-1** Promote Pedestrian Interaction



view of north (Stewart Street) elevation



view up Stewart Street



DRB



view looking northeast



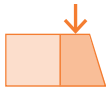
view looking southwest



A-2 Enhance the Skyline



view from the sound



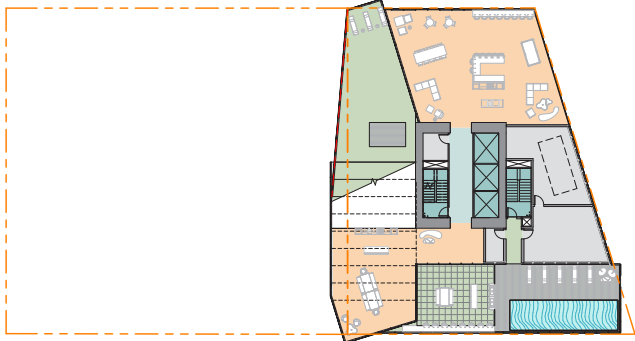
Board's Deliberations:

"There was a positive response to the unusual rooftop configuration, but the Board would like to see further details, and from a variety of perspectives, how it caps the building."

Response:

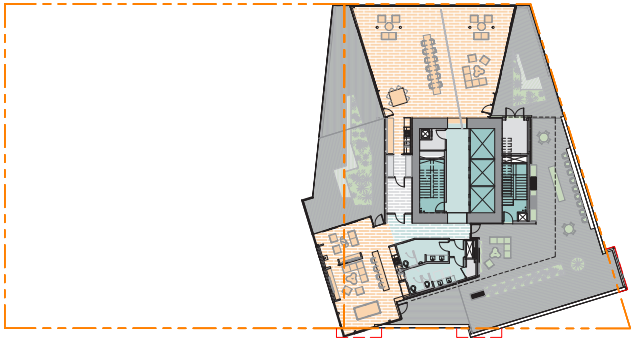
Two interior rooftop rooms and outdoor spaces provide outdoor common recreation area for the residents. The angular forms of the spaces are blended with the massing below to reflect the irregular street geometry.

EDG#2



aerial view from the southwest

DRB



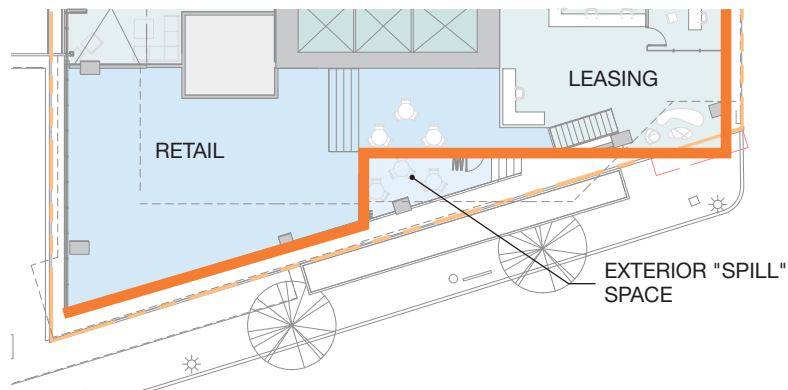
aerial view from the southwest



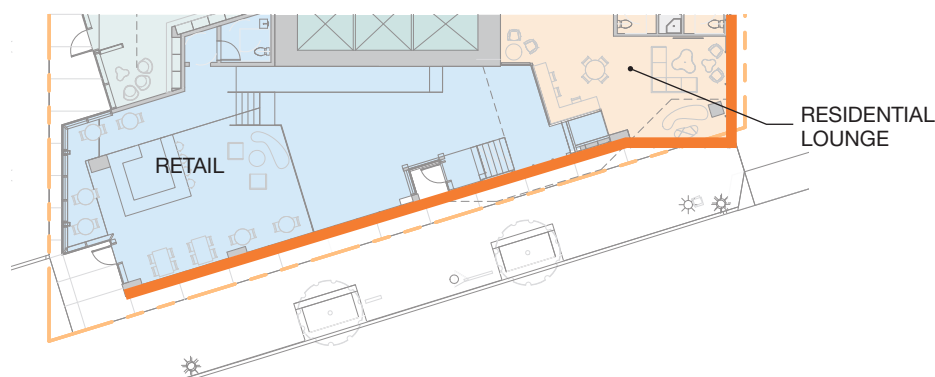


street level view | Stewart Street

## EDG#2



## DRB



street level view | Stewart Street + Alley

### Board's Deliberations:

"The raised outdoor platform area accessed by stairs from Stewart Street and providing accessory space for the adjacent retail was not universally acclaimed by the Board who desired to see further demonstration of actual views down the street toward the water and demonstration of how it would be protected from wind and weather and operate as a successful outdoor retail space. Provide vignettes of the relationship to the sidewalk and how the outdoor space would work."

### Response:

Please see L01 Floor Plan (page 20) and the 2nd Avenue street level perspective (page 36). Since the EDG #2 meeting the raised outdoor area has been revised to be included as interior space with a simpler storefront façade as suggested by the board. The retail space along Stewart is arranged as three stepped levels along the façade to better visually connect with the level 2 residential lounge area, creating an pedestrian friendly active façade for the length of Stewart Street.

**B-1** Respond to the Neighborhood context

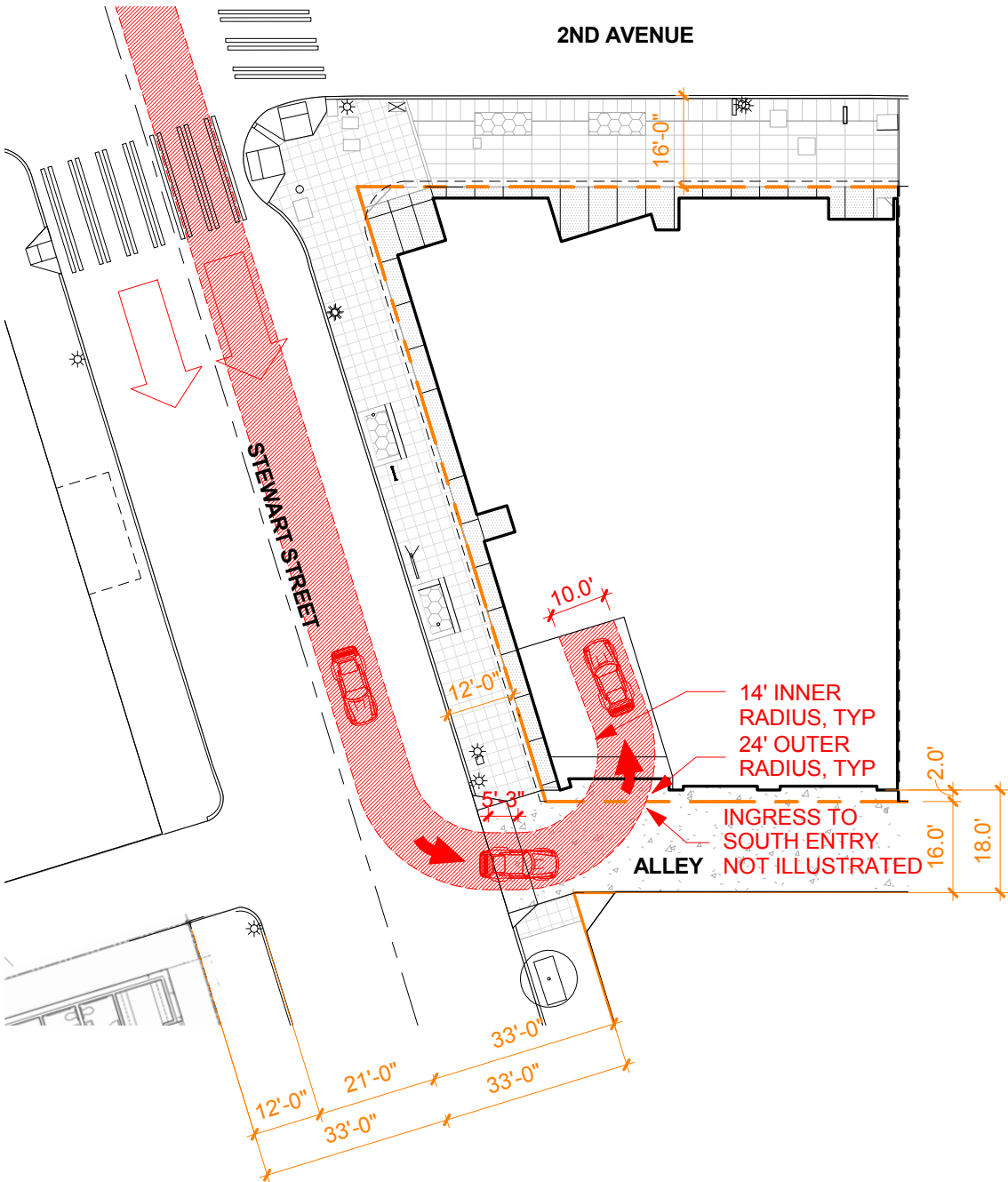
**C-6** Develop the Alley Facade

**E-2** Integrate Parking Facilities

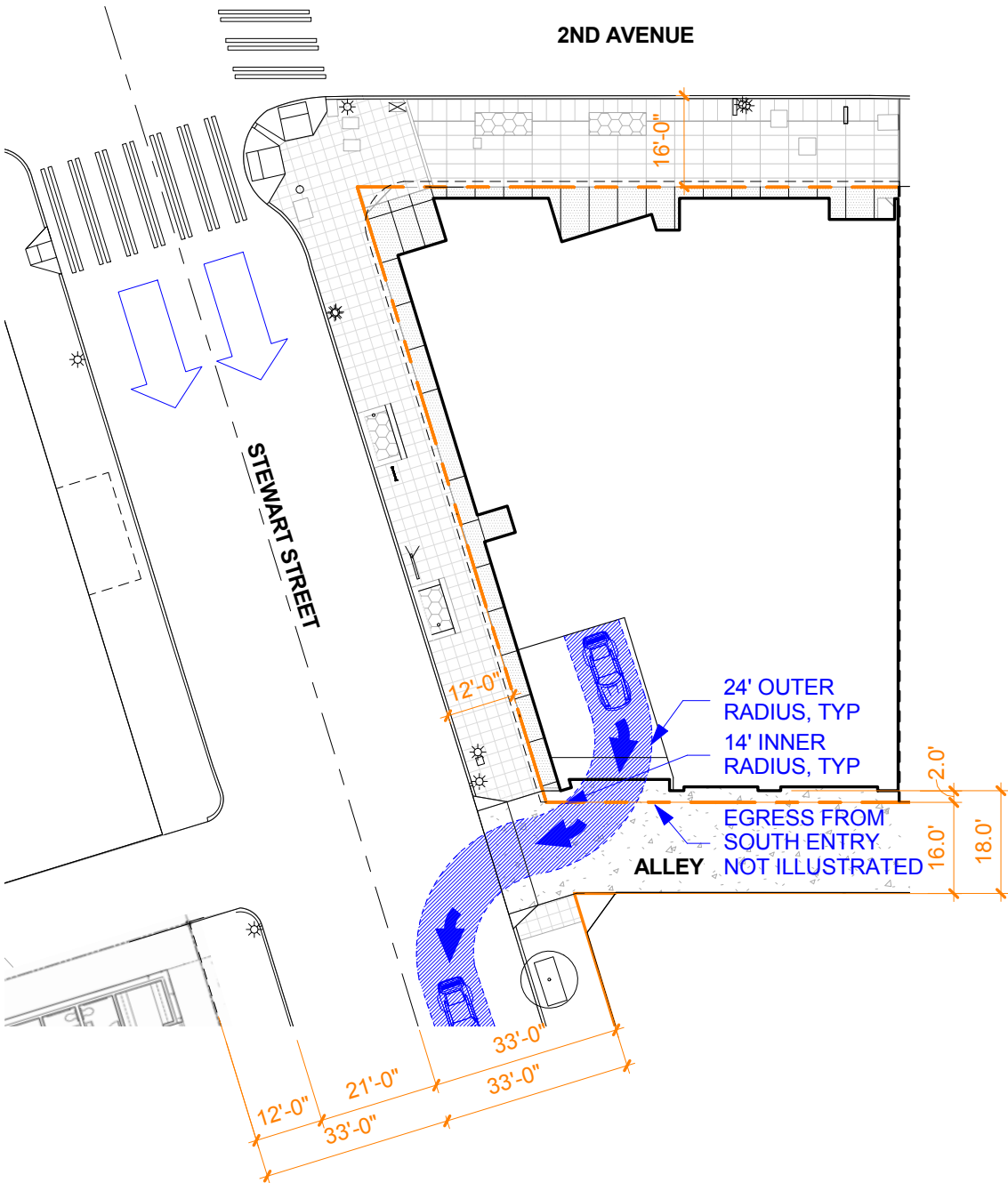


DRB | GARAGE ACCESS, NO STREETCAR .....

**Board's Deliberations:**  
"Demonstrate to the Board how the turning radii in and out of the parking openings would work safely and effectively."  
  
**Response:**  
Please see turning radius diagram on the following pages for entry and exit into the northern garage in both "no street car" and "future street car" configurations.



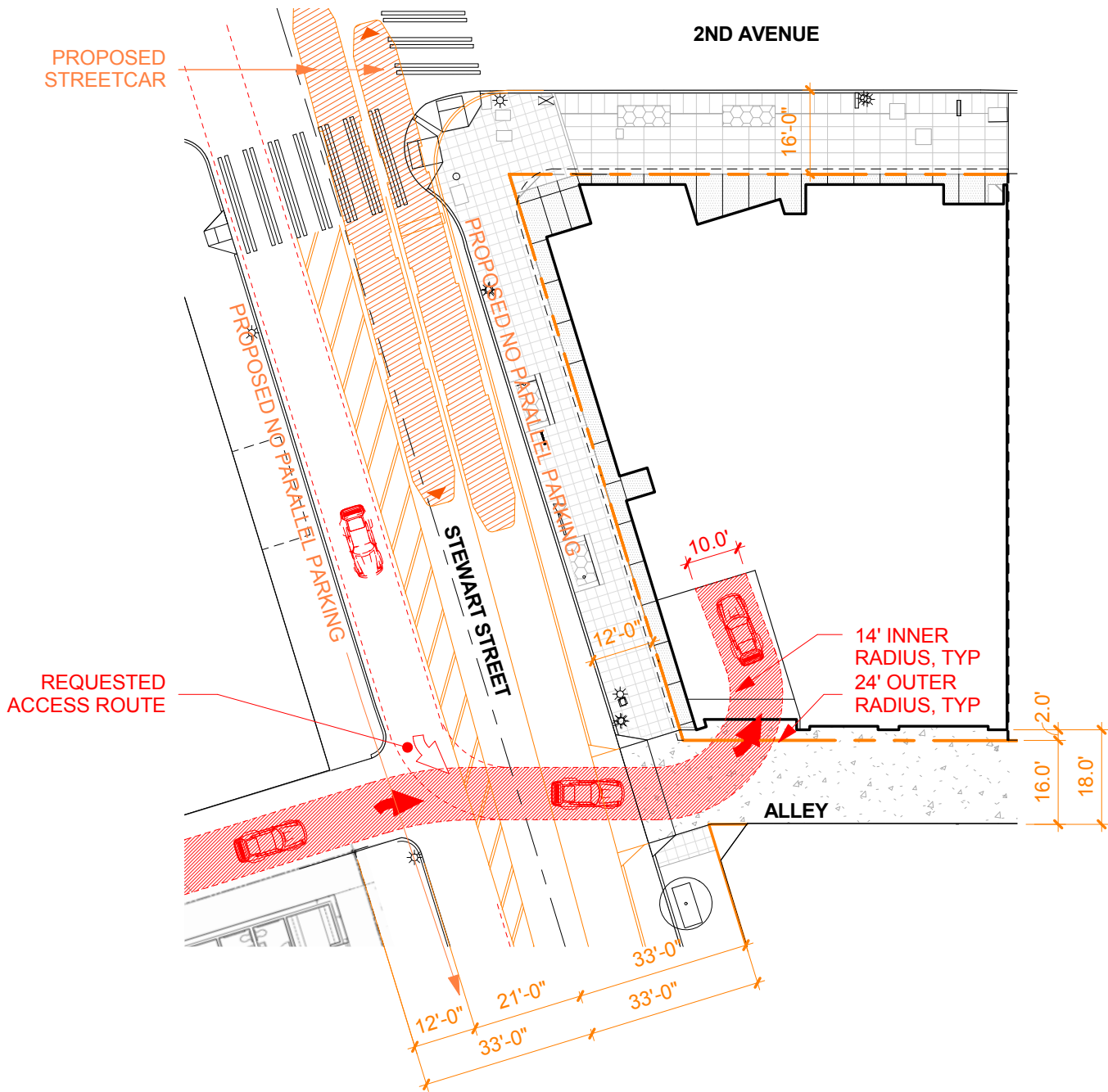
garage entry from the north | no streetcar



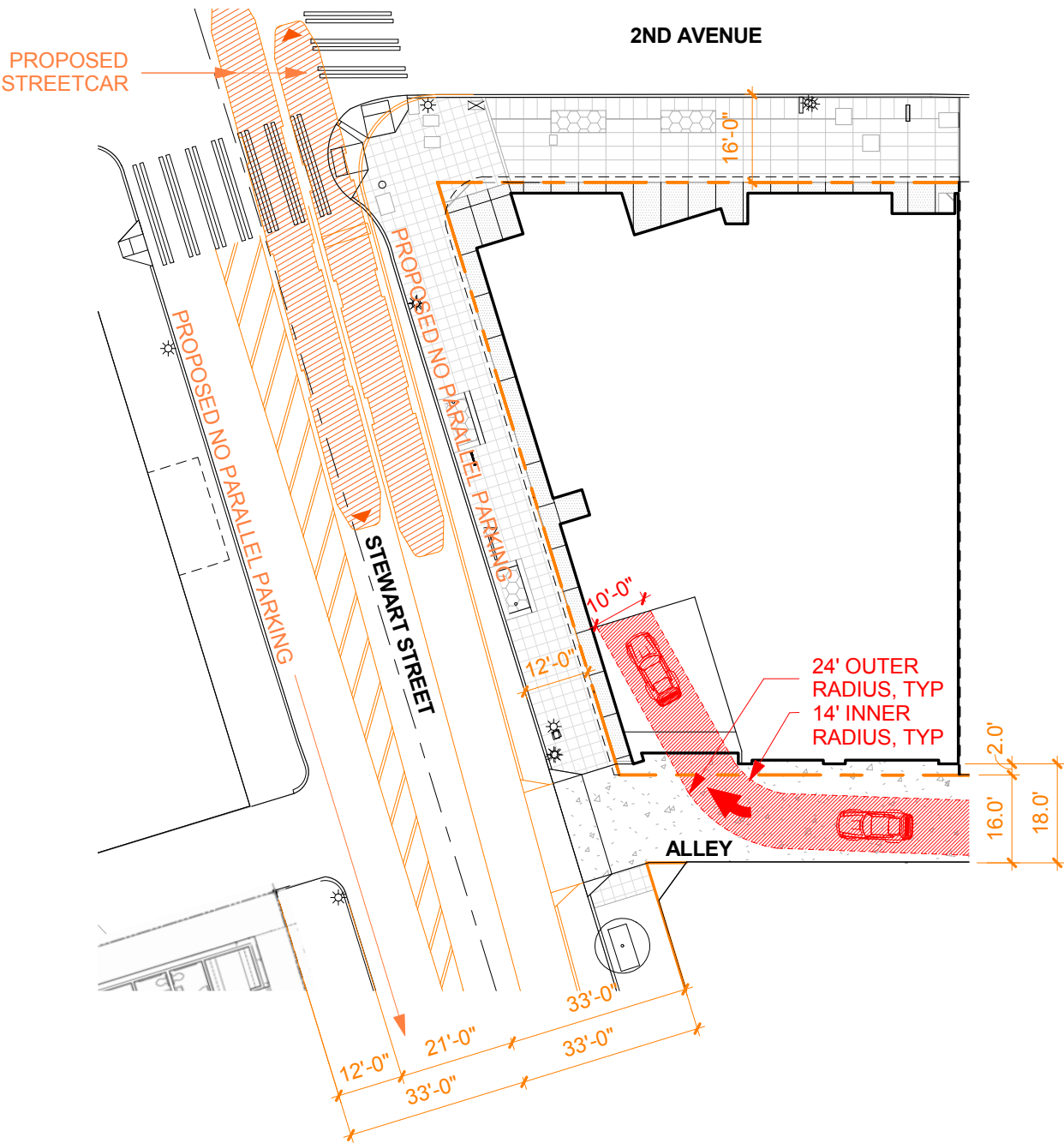
garage exit to the west | no streetcar



DRB | GARAGE ENTRY WITH FUTURE STREETCAR.....



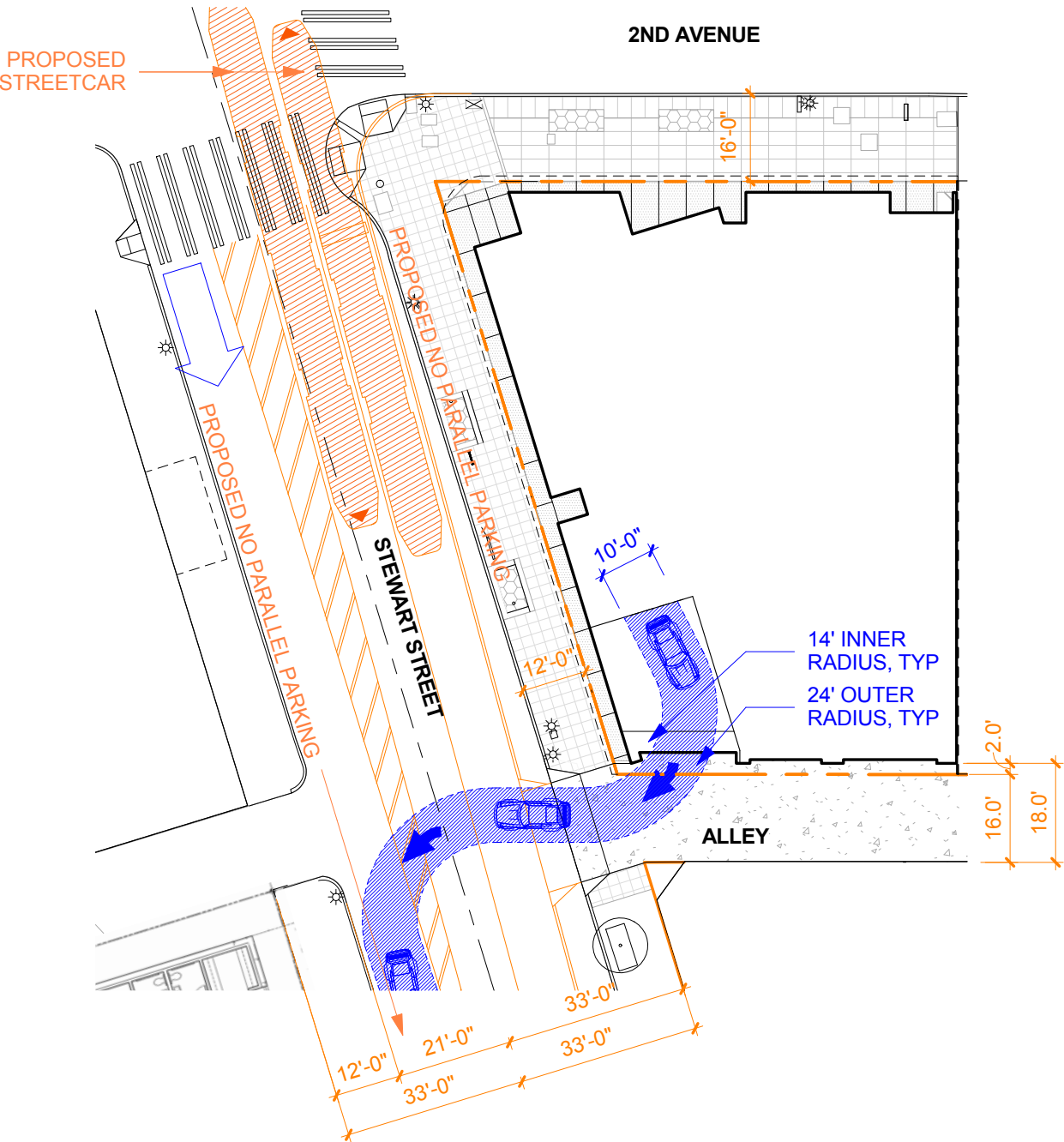
garage entrance from the north | future streetcar



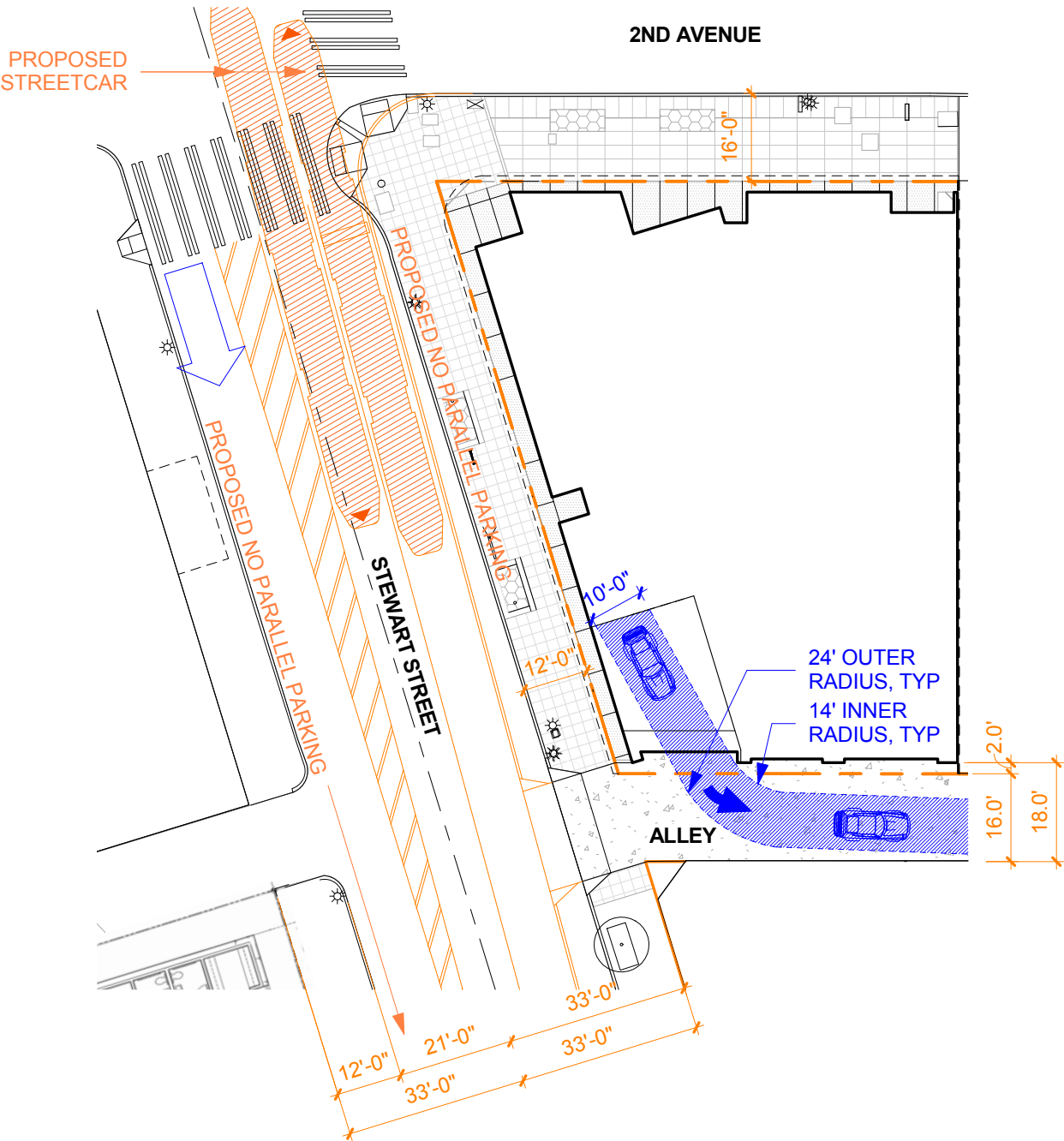
garage entrance from the south | future streetcar



DRB | GARAGE EXIT WITH FUTURE STREETCAR .....



garage exit to the north | future streetcar



garage exit to the south | future streetcar



EDG#2

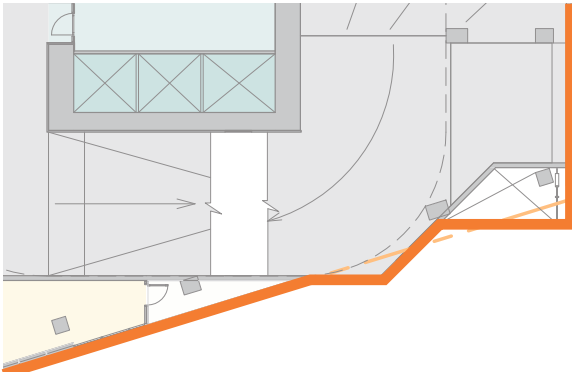
DRB

E-2 Integrate Parking Facilities

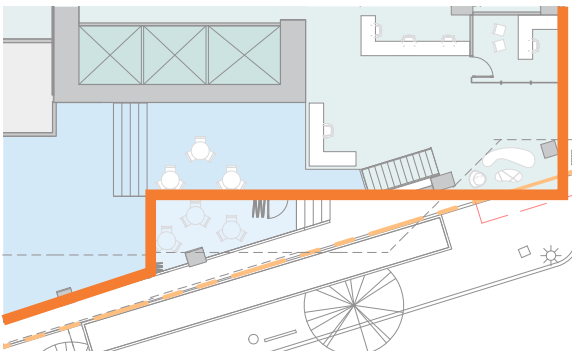
B-1 Respond to the Neighborhood context



street level view from hotel on Stewart Street



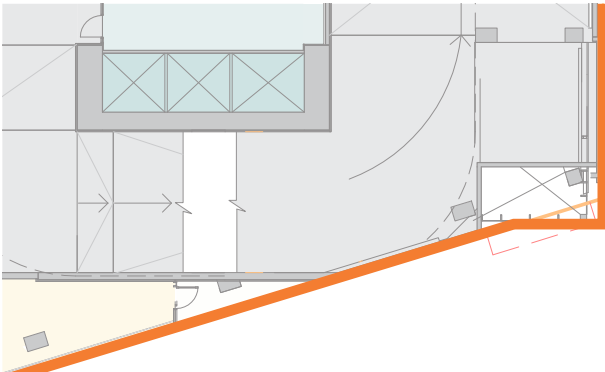
L03-04



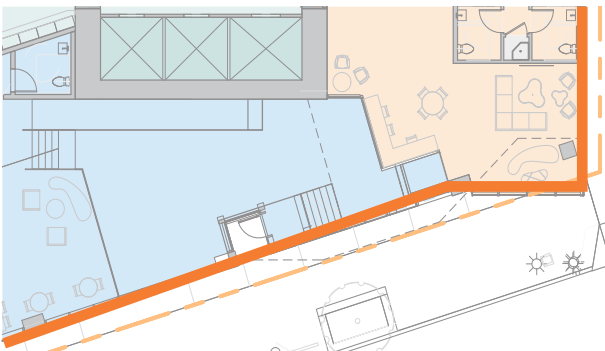
L01/L02



street level view from hotel on Stewart Street



L03-04

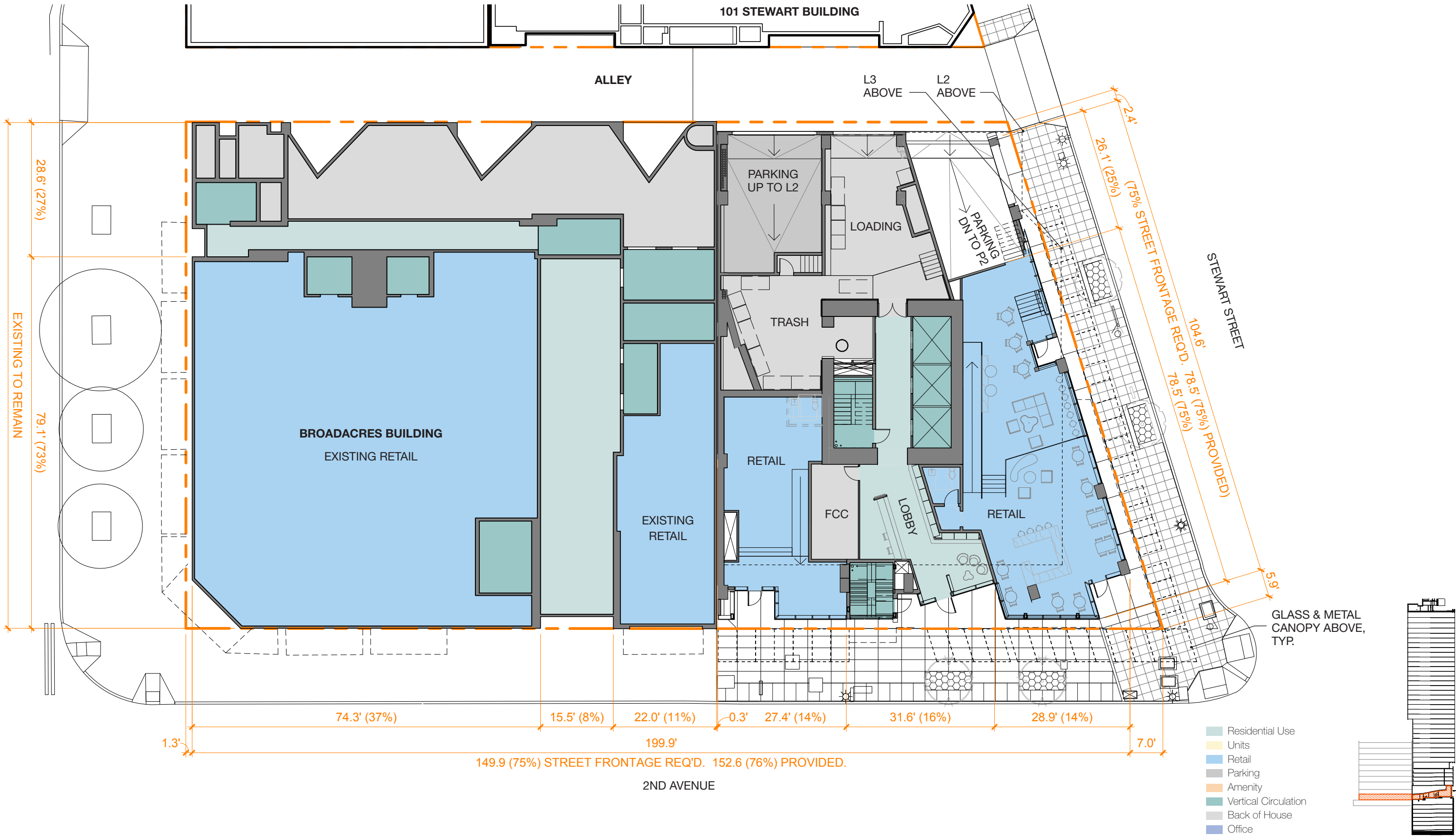


L01/L02

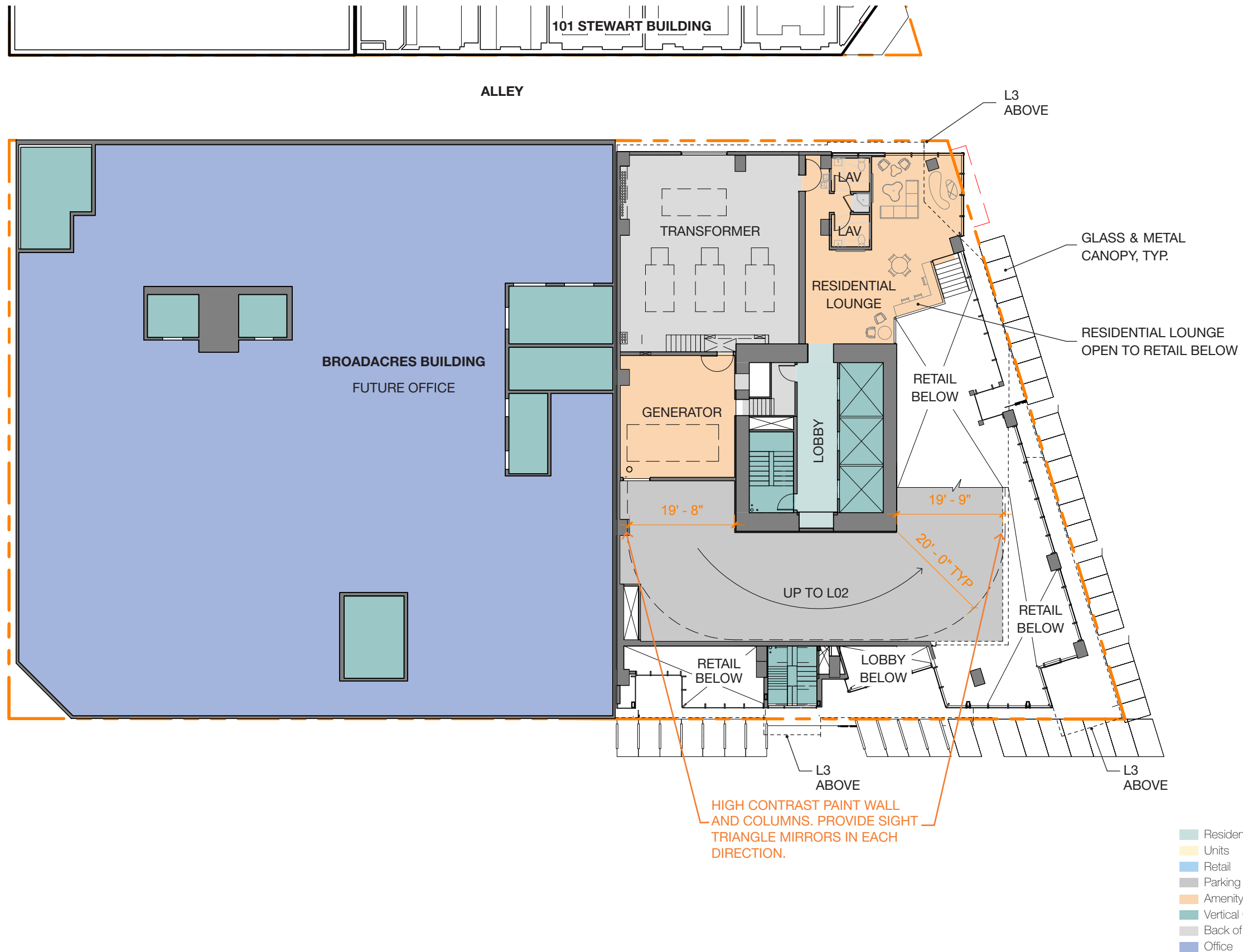
**Board's Deliberations:**  
Moving the leasing space up to a mezzanine level was acknowledged as a good move by the Board, but they also thought that the external expression of this and related spaces at the northwest lower corner, the junction of the alley and Stewart Street, as portrayed in both alternative one and two, needed to be revisited. It was suggested that the creation of a dynamic corner was a good idea, but perhaps the present rendering exhibited too many acute angles. As expressed by one of the Board members, the treatment of the lower corner should be as elegant as the treatment of the rest of the building.

**Response:**  
Since the EDG #2 meeting the massing at the corner has been simplified. A structural building overhang relating to the above grade parking level ramps has been removed.

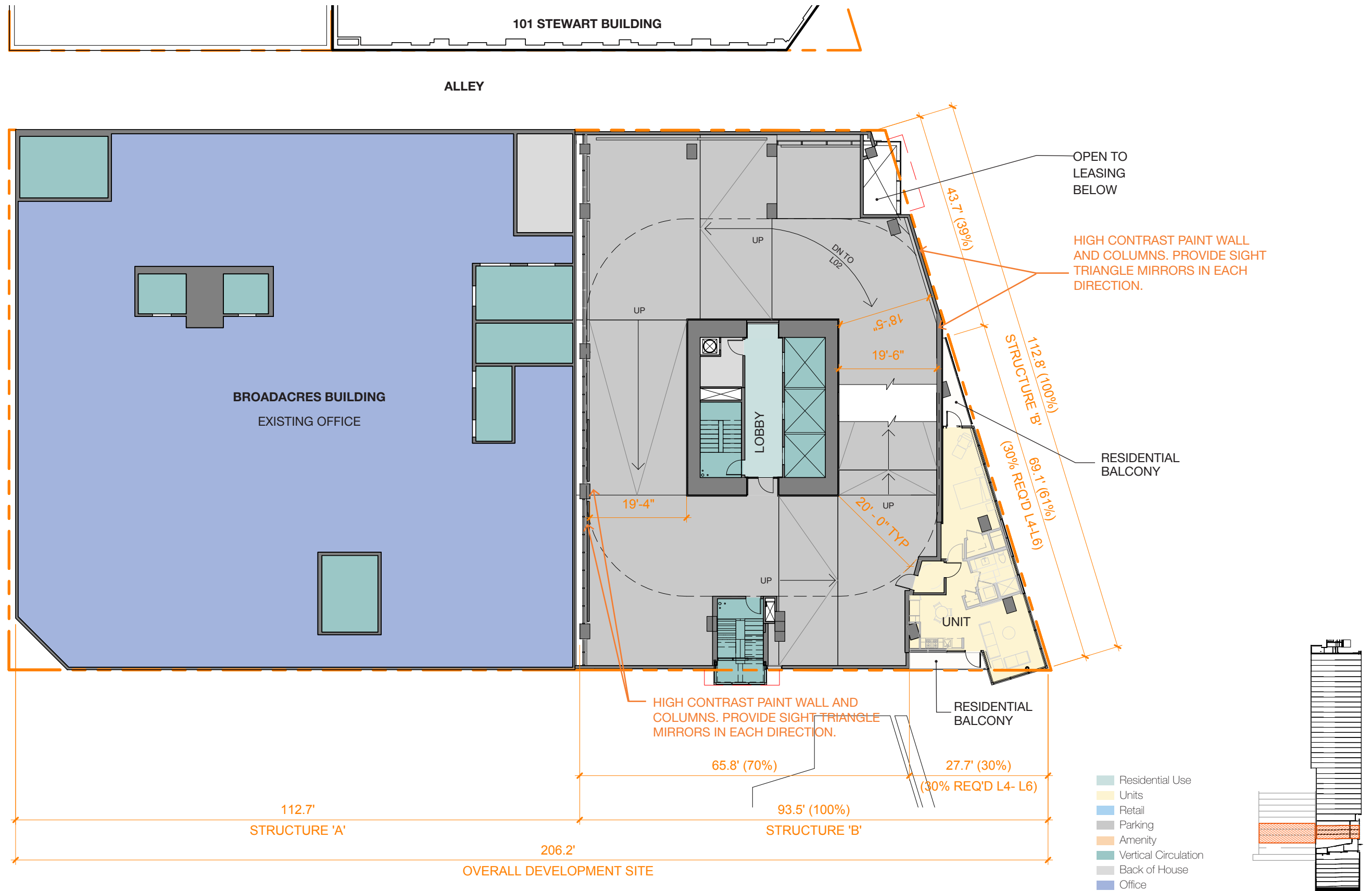








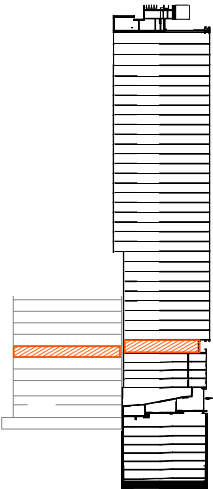
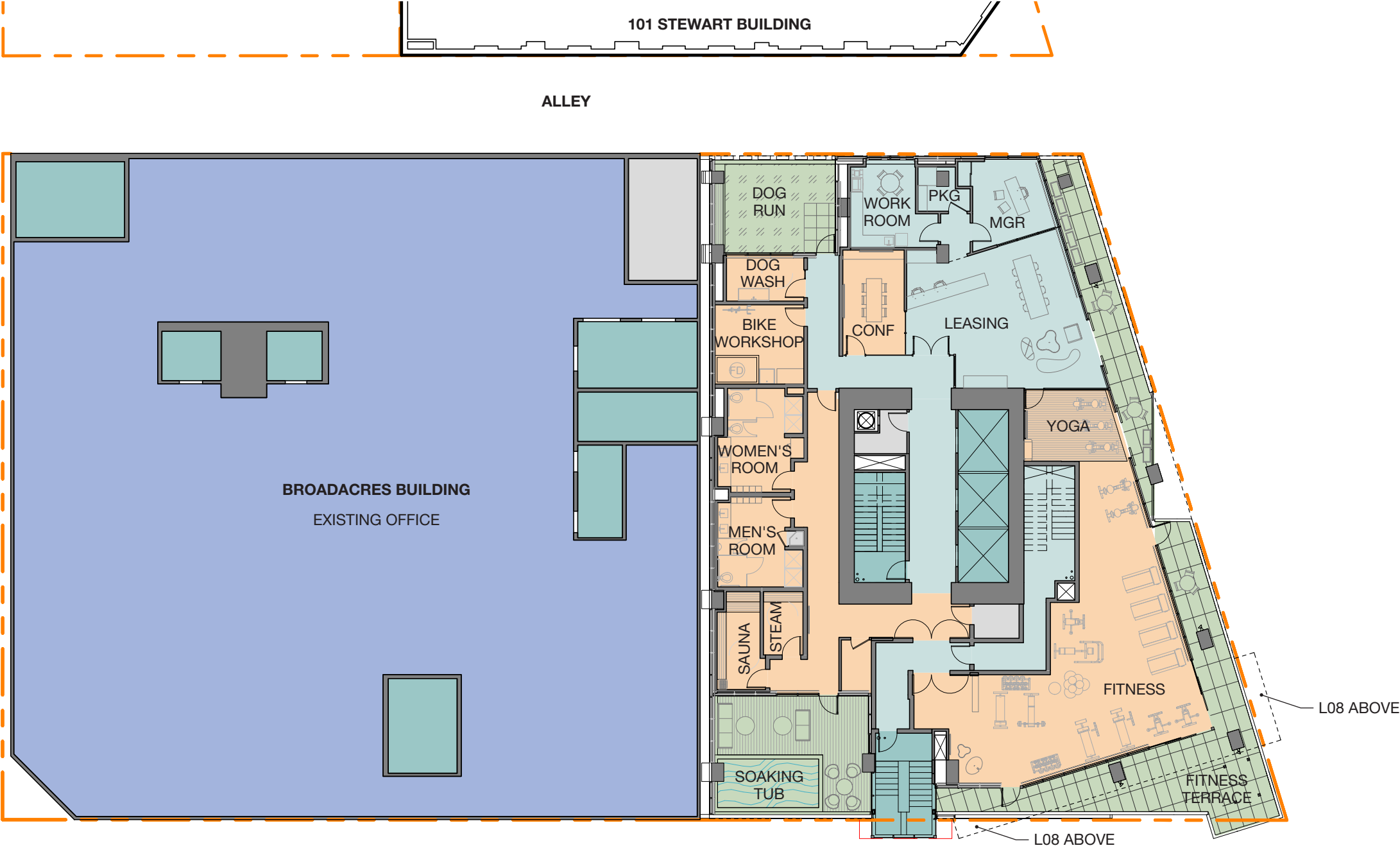




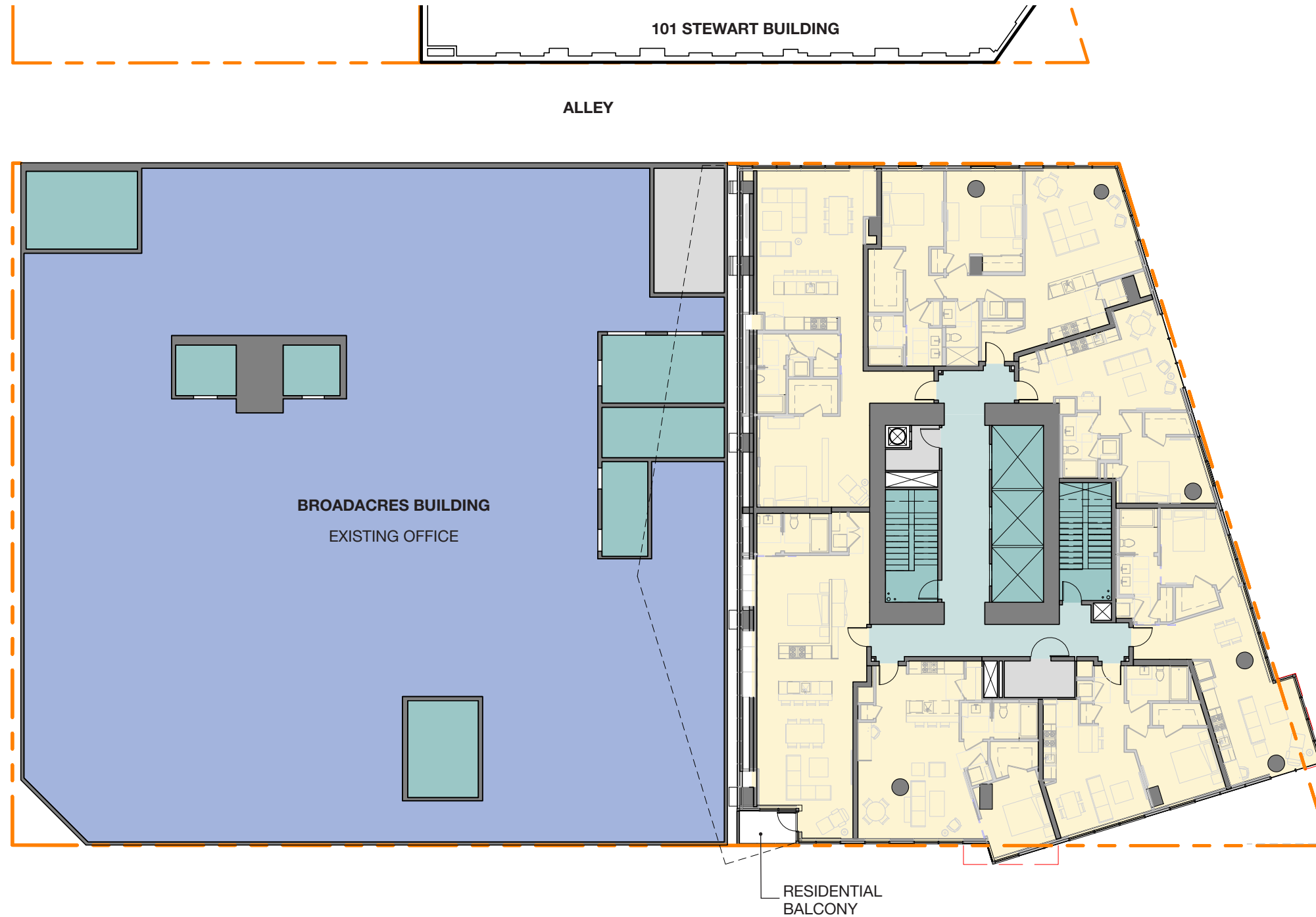




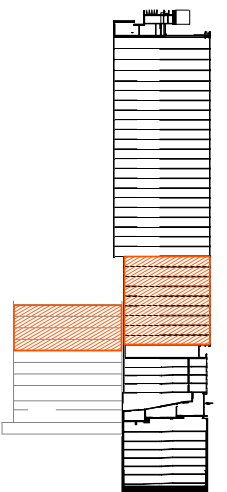




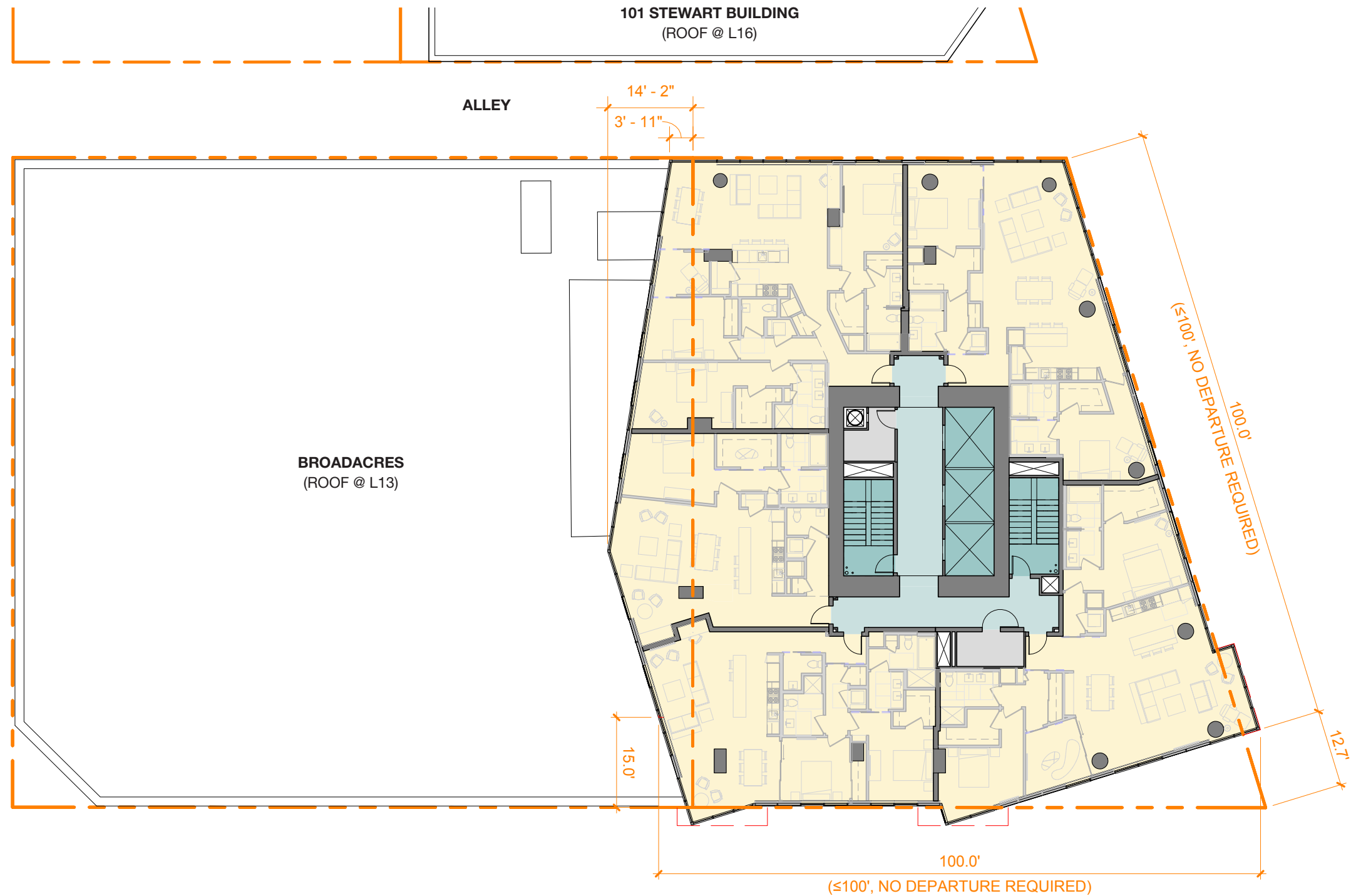




- Residential Use
- Units
- Retail
- Parking
- Amenity
- Vertical Circulation
- Back of House
- Office





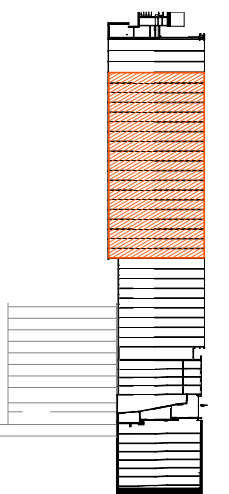


**BROADACRES**  
(ROOF @ L13)

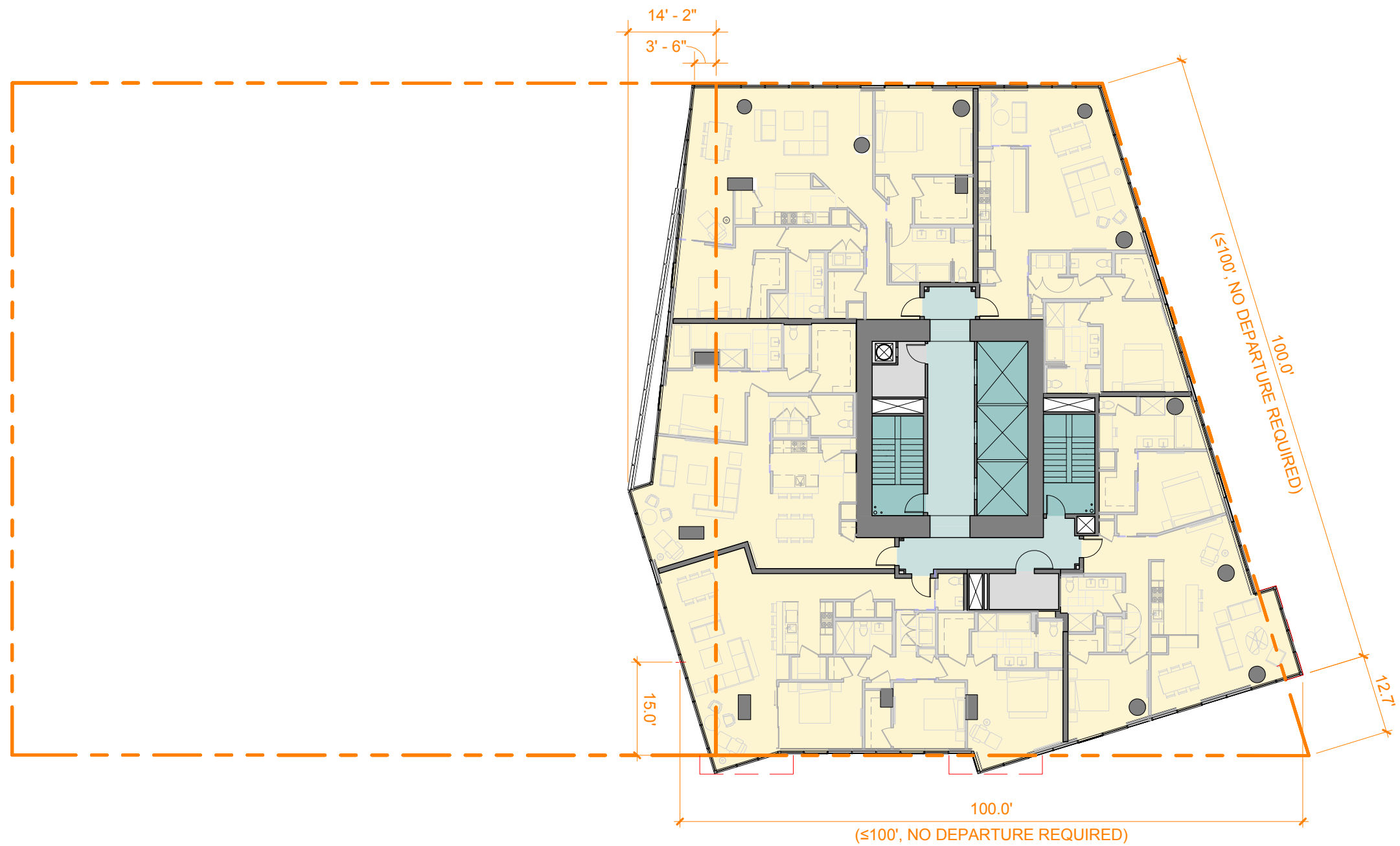
**101 STEWART BUILDING**  
(ROOF @ L16)

**ALLEY**

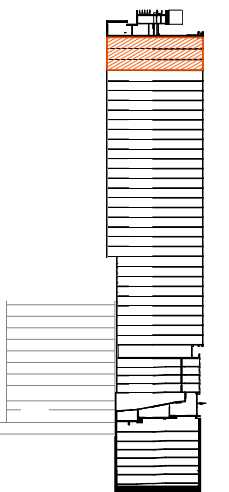
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- Units
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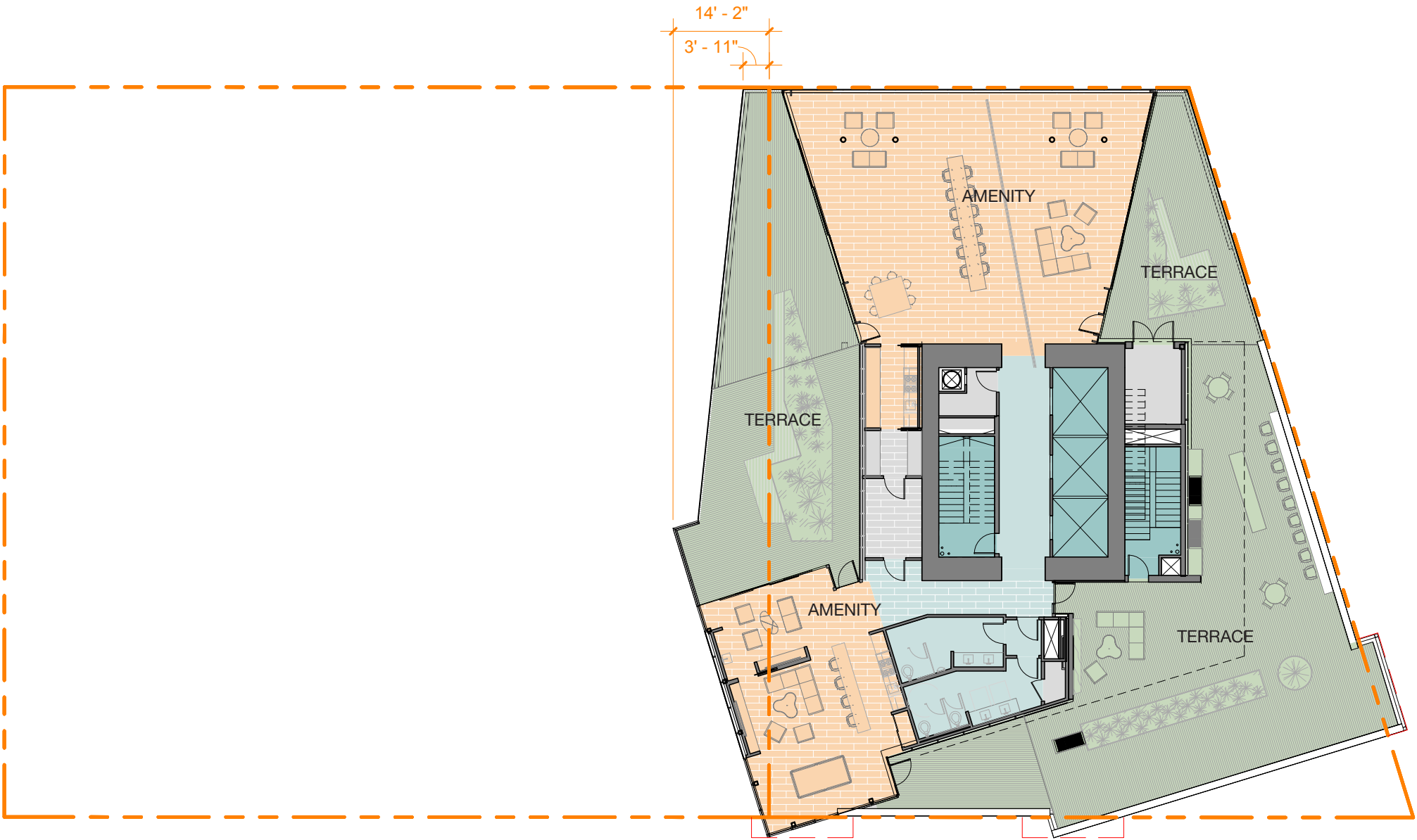




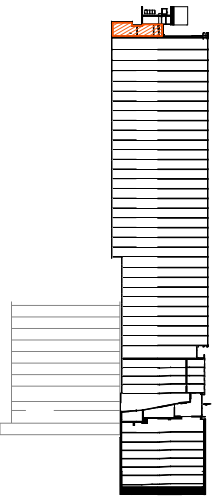
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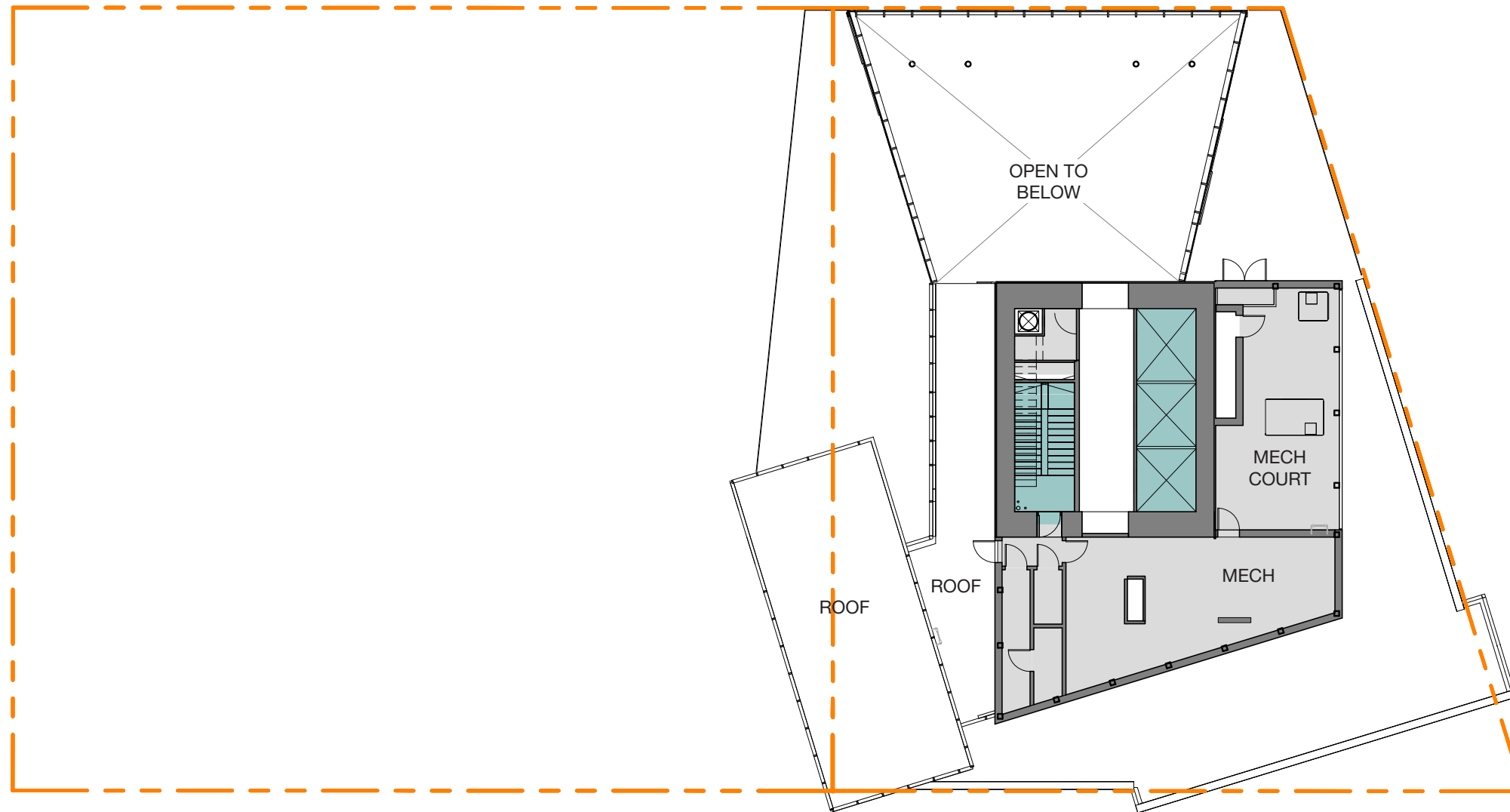




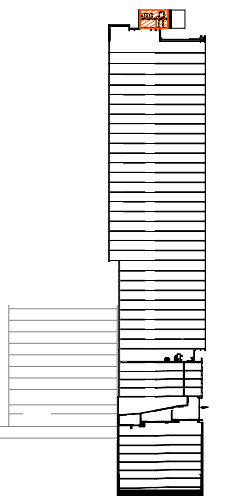
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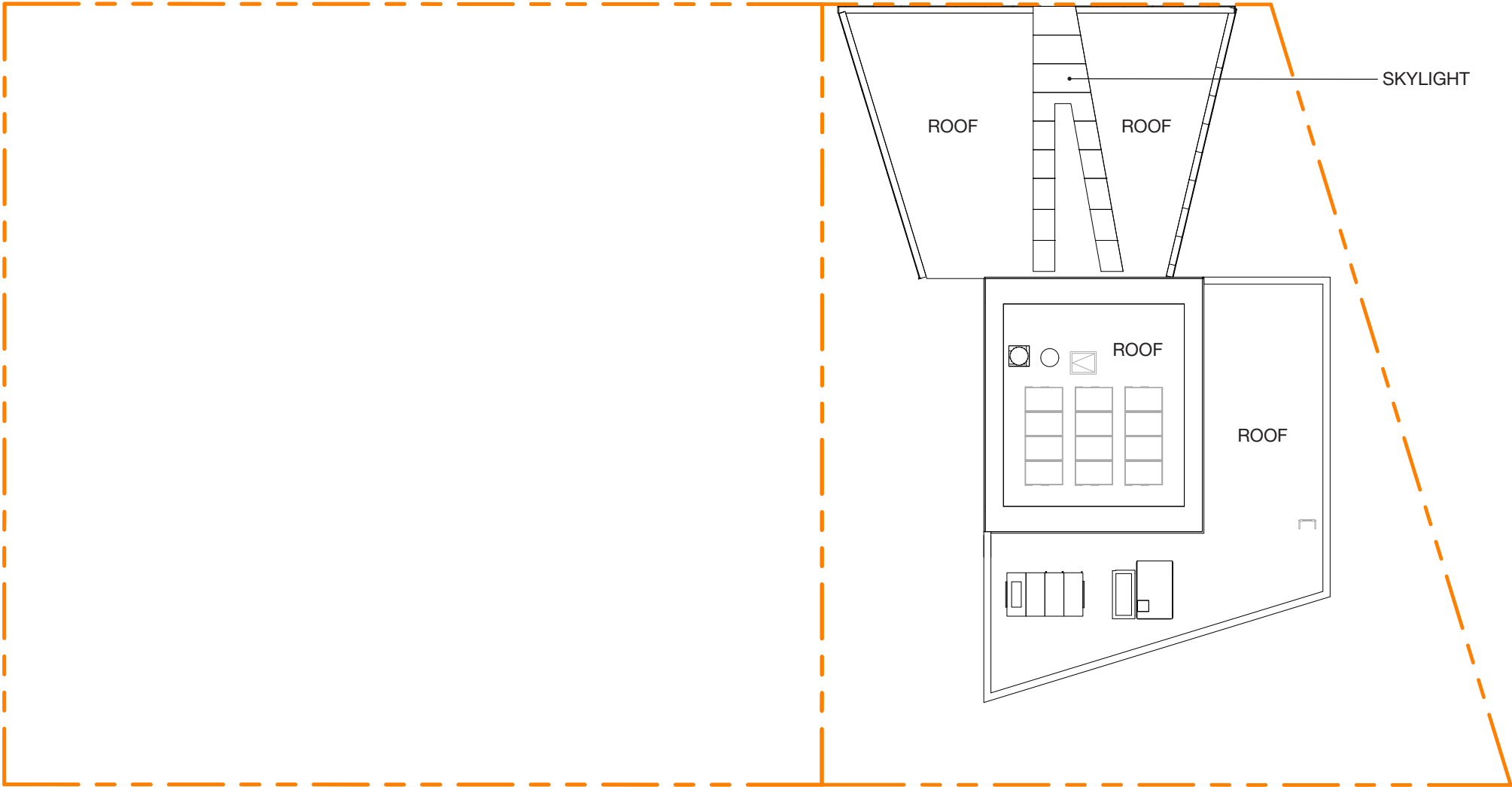




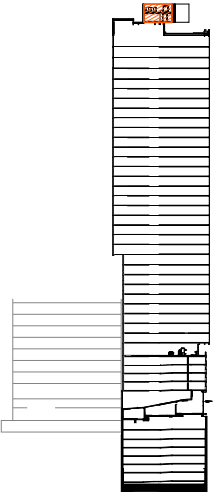
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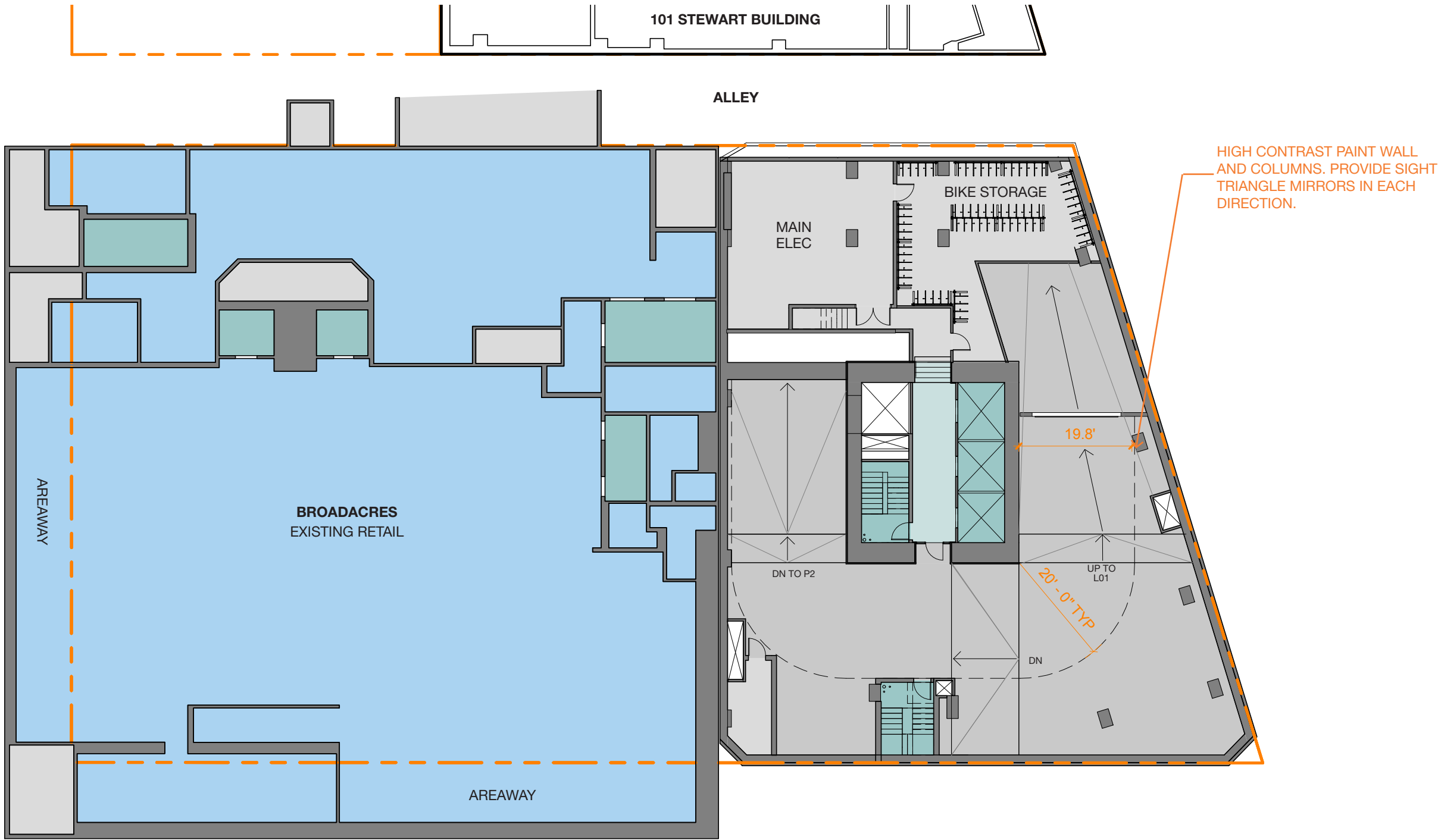




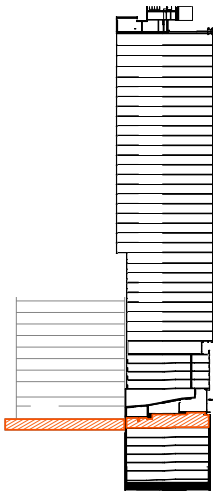
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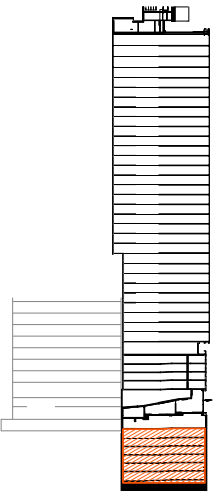
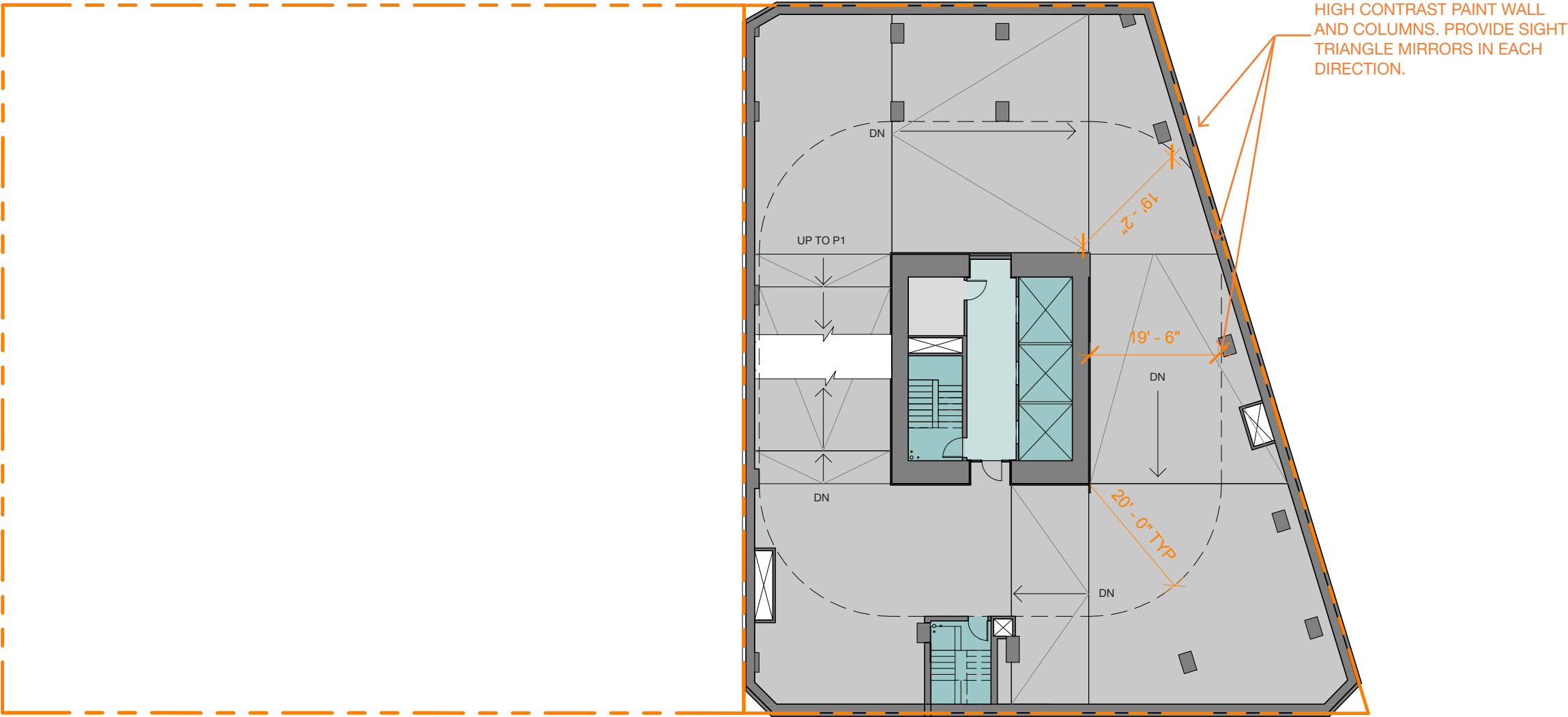


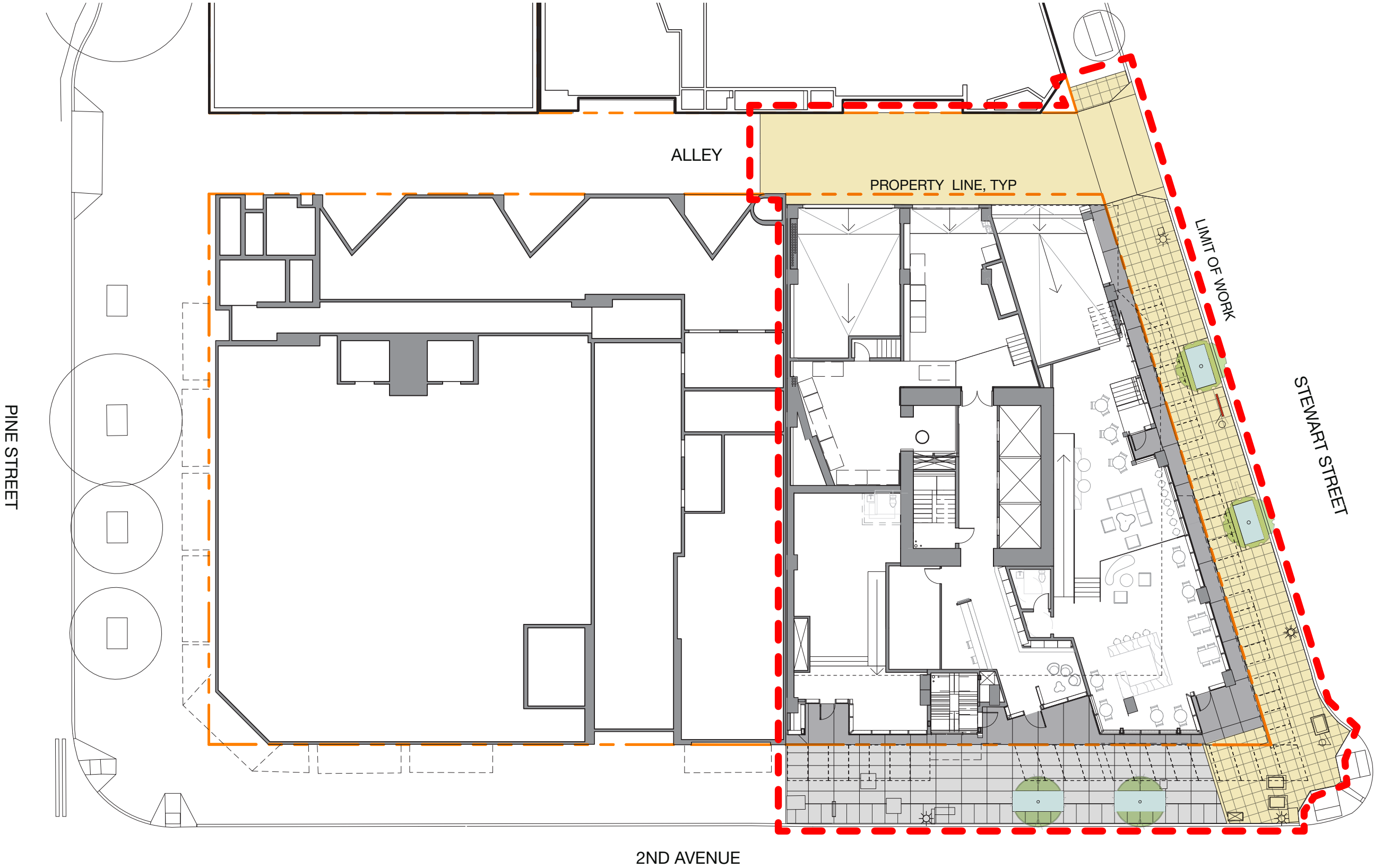


- Residential Use
- Units
- Retail
- Parking
- Amenity
- Vertical Circulation
- Back of House
- Office











MATERIAL PALETTE - STREET LEVEL



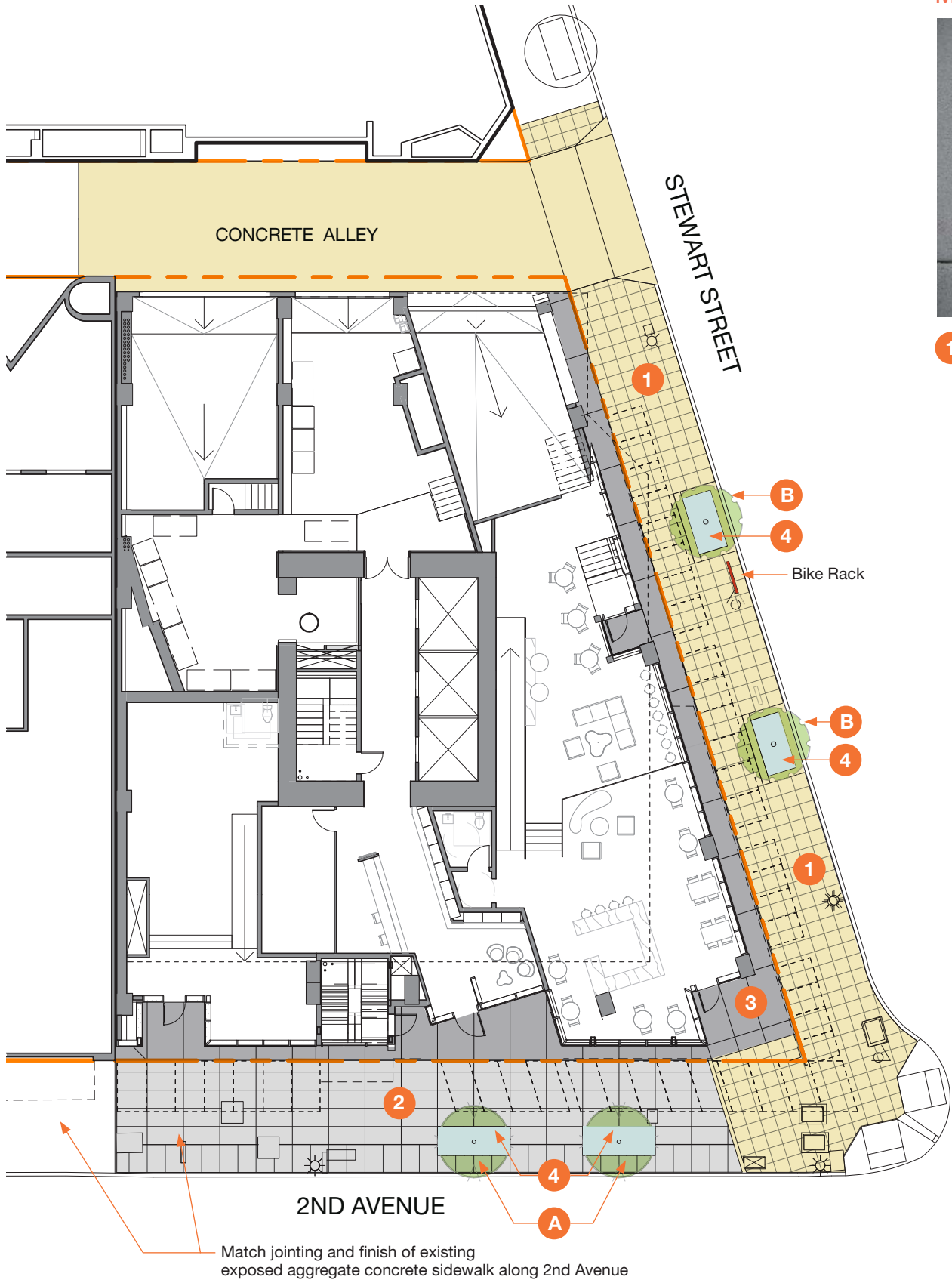
1 COS Sandblasted Concrete Paving with 2'x2' Score Pattern to match existing at Stewart Street



2 Top-seeded Aggregate Concrete Paving with Screed at Joints. Sidewalk to match existing at 2nd Avenue.



3 Granite Accent Pavers, Thermal Finish



4 Flexi-pave Surfacing (At tree pit locations)

PLANTING PALETTE | STREET LEVEL



A *Acer x Freemanii* 'Armstrong' Armstrong Maple



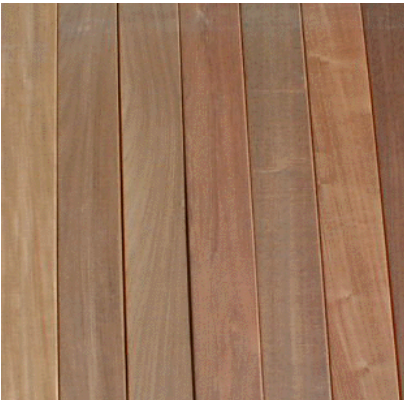
B *Acer saccharum subsp. nigrum* 'Green Column' - Green Column Black Maple (Matches existing at Stewart Street)



MATERIAL PALETTE | LEVEL 7 TERRACE



1 Concrete Pedestal Pavers



2 Wood Decking



3 Cedar Bark Nuggets (Dog Relief Area)



4 Decorative Fiberglass Planters

PLANTING PALETTE | LEVEL 7 TERRACE



*Phormium tenax* - New Zealand Flax



*Stipa tenuissima* - Mexican Feather Grass



*Crocasmia x crocosmiiflora*  
*'Emily McKenzie'* - Crocosmia



MATERIAL PALETTE | ROOF TERRACE



1 Wood Decking



2 Custom Blackened Steel Raised Landscape Planter



3 Custom Wood Seating Element

PLANTING PALETTE | ROOF TERRACE



*Elymus glaucus* - Blue Wild Rye Grass



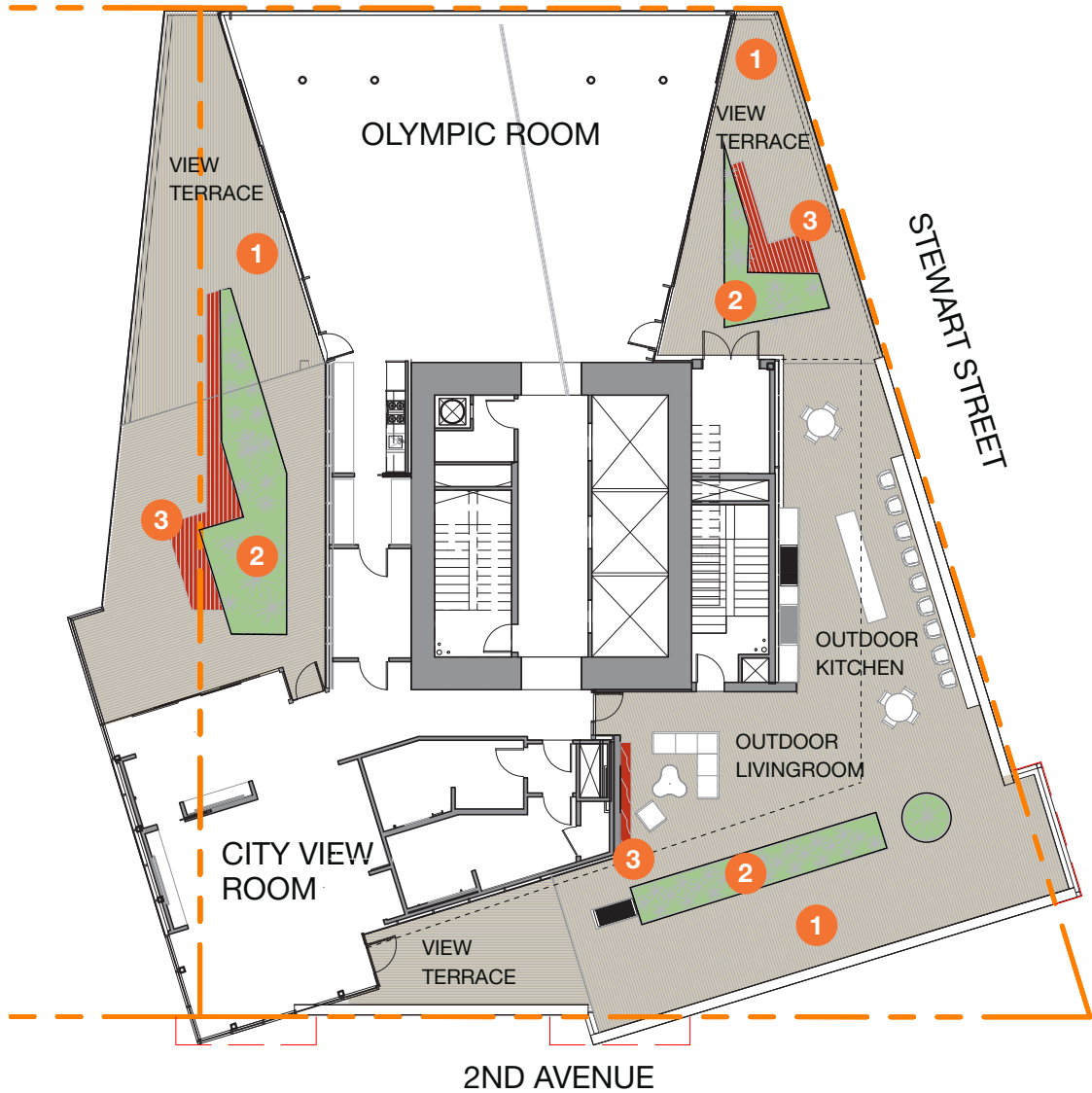
*Stipa tenuissima* - Mexican Feather Grass



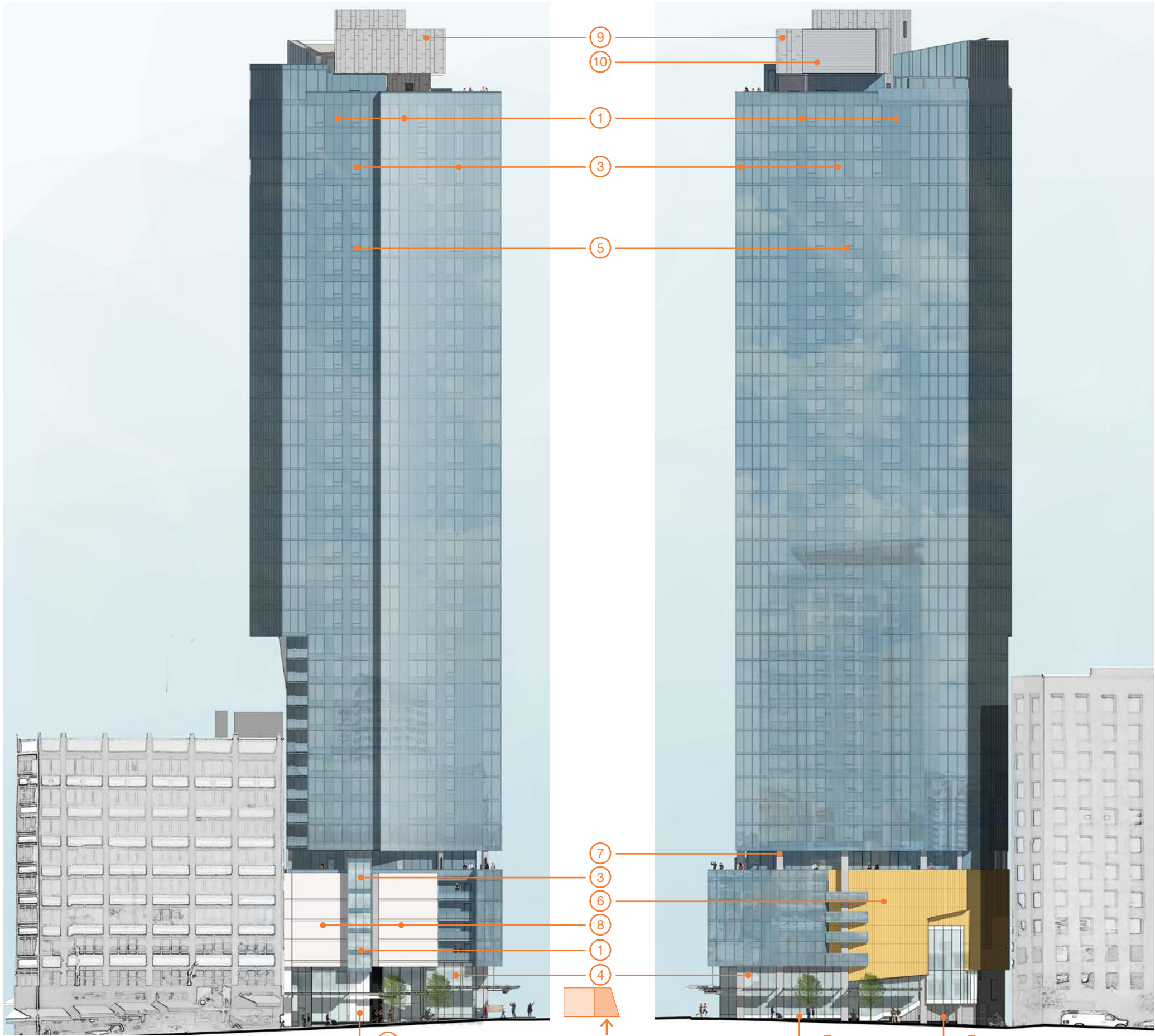
*Achillea millefolium* 'Walther Funcke' - Walter Funcke Yarrow



*Fragaria chiloensis* - Beach Strawberry

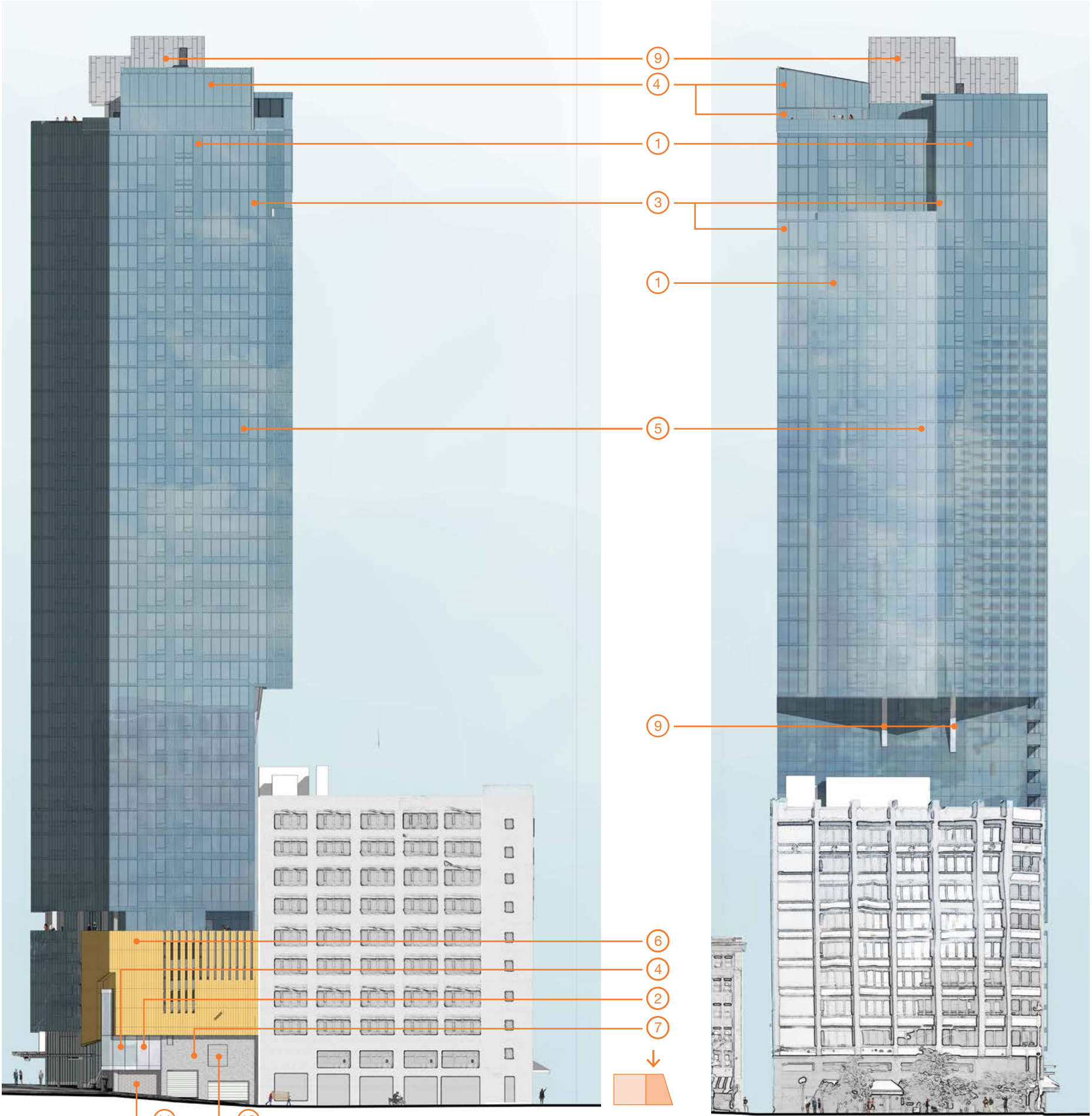






- ① Spandrel Glass
- ② Spandrel Glass: Curtain Wall
- ③ Vision Glass
- ④ Vision Glass: Curtain Wall
- ⑤ Operable Awning Windows
- \* Window Wall Mullion Exterior to match adjacent panels
- ⑥ Cement Composite Panels
- ⑦ Concrete
- ⑧ Precast Concrete Panel
- ⑨ Architectural Metal Panel
- ⑩ Louvers
- ⑪ Metal Mesh





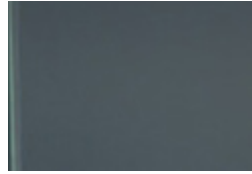



- ① Spandrel Glass
- ② Spandrel Glass: Curtain Wall
- ③ Vision Glass
- ④ Vision Glass: Curtain Wall
- ⑤ Operable Awning Windows
  - \* Window Wall Mullion Exterior to match adjacent panels
- ⑥ Cement Composite Panels
- ⑦ Concrete
- ⑧ Precast Concrete Panel
- ⑨ Architectural Metal Panel
- ⑩ Louvers
- ⑪ Metal Mesh

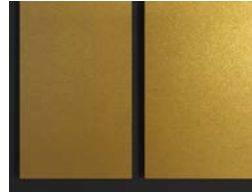



# MATERIALS PALETTE

- ①  Spandrel Glass
- ②  Spandrel Glass: Curtain Wall
- ③  Vision Glass
- ④  Vision Glass: Curtain Wall
- ⑤  Operable Awning Windows
- \* Window Wall Mullion Exterior to match adjacent panels
- ⑥  Cement Composite Panels
- ⑦  Concrete
- ⑧  Precast Concrete Panel
- ⑨  Architectural Metal Panel
- ⑩  Louvers
- ⑪  Metal Mesh




- ①  **spandrel glass**  
SW 6251 "outerspace" on #2 surface on clear float glass



- ⑥  **fiber cement panel system**  
swisspearl reflex "gold" - 12" wide x 8' H panels with exposed fasteners - fasteners patinted to match panels; alternating panel directions; typ joint thickness 5/16"



- ⑩  **horizontal aluminum louvers**  
horizontal blades; painted aluminium; color to match architectural metal panels



- ②  **Curtain wall spandrel glass**  
shadow box insulated spandrel unit; color: ici 1316 "swiss white" on #4 surface




- ⑦  **Exposed cast-in-place concrete**  
dark gray pigment; light sandblasted finish




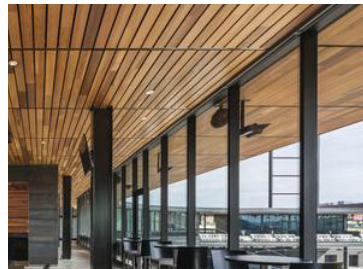
- ③  **vision glass**  
Guardian AG 50 - 30% reflectance outside 'light silver' color #2 surface on clear float glass



- ⑧  **precast concrete panel**  
concrete color - white; lightly sandblasted; coarse aggregate 1/4" to 1/2"



- ⑪  **perforated metal screening**  
12" wide vertical galvanized "perf-o-grip" planks; 38% open; round openings; powder coated - to match architectural metal panels



- ⑫  **wood slat soffit -**  
4" wide western red cedar; arch grade 'A clear;' with penetrating oil finish



- ④  **Curtain wall vision glass**  
Guardian Super Neutral 68; 10% outside reflectance



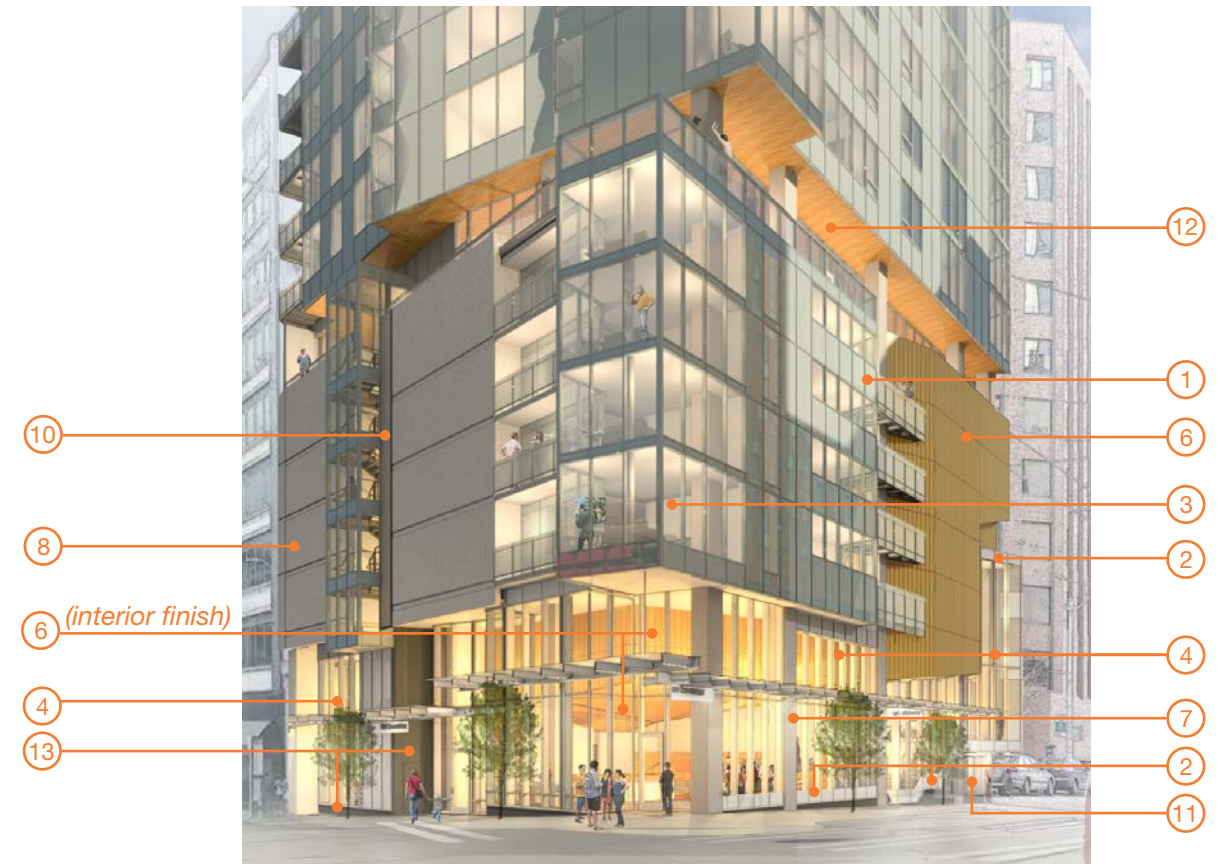
- ⑨  **Architectural composite metal panel**  
flat metal wall panels Phoenix flex system color - Reynobond Colorweld 500 "Silversmith"



- ⑬  **Stone base and wall panels -**  
polished black granite panels



- ⑤  **window wall mullion / operable awning window sash**  
factory painted 2-coat Duranar coating 'Silversmith'







view along Stewart Street looking west



view from Stewart Street looking south



view from 1st Avenue + Stewart Street looking east



view from Pine Street looking north





















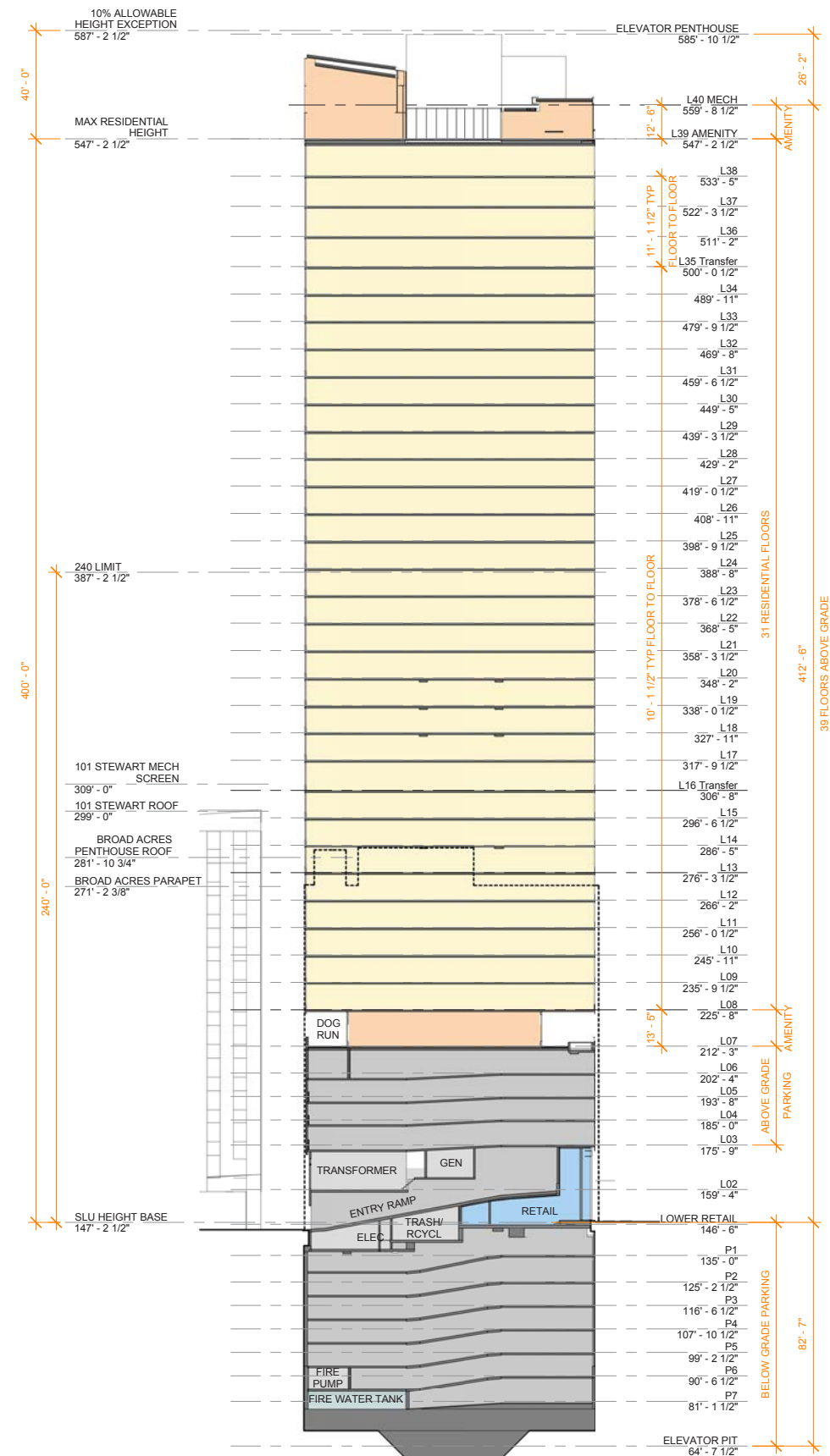
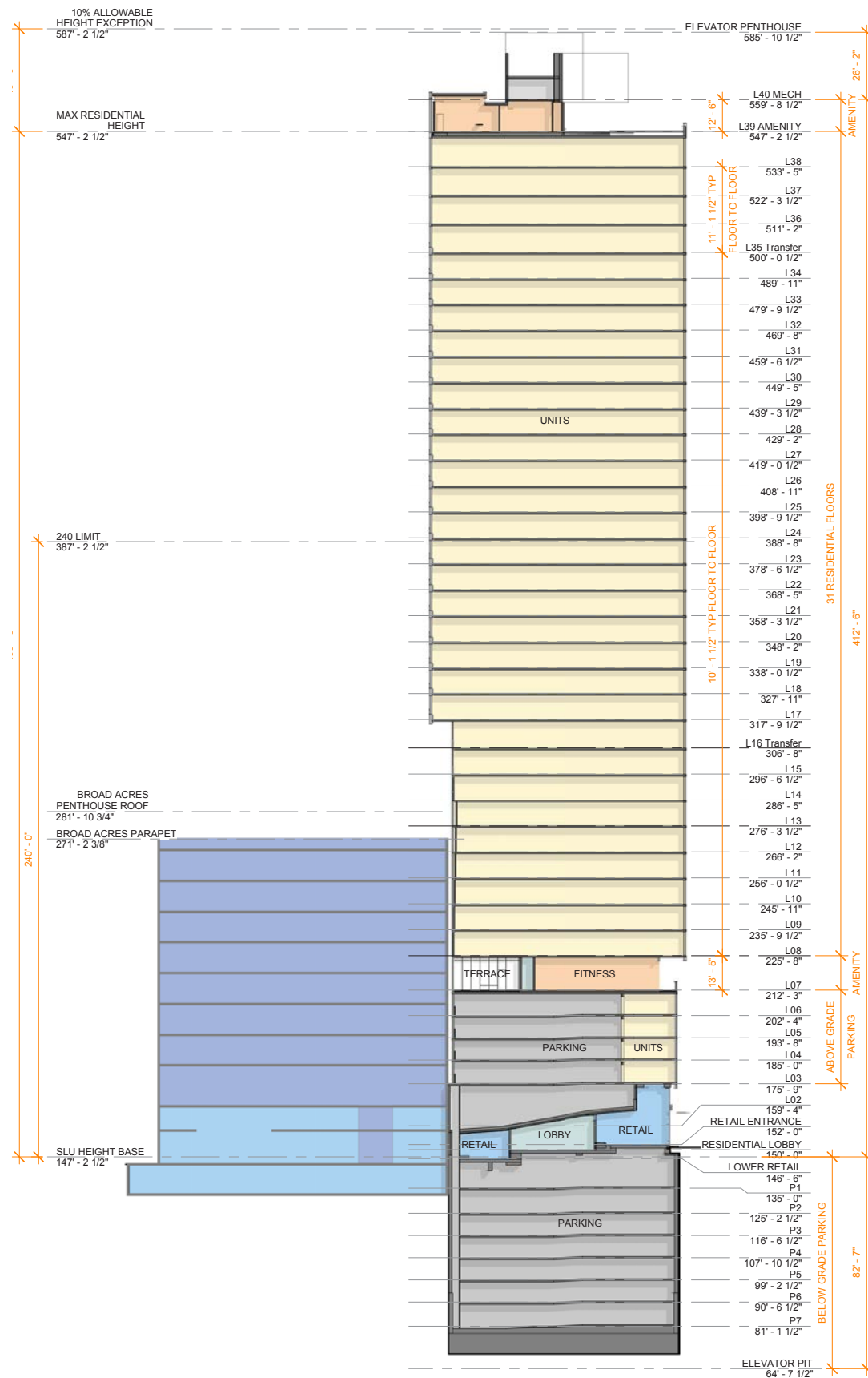
Stewart Street



2nd Avenue



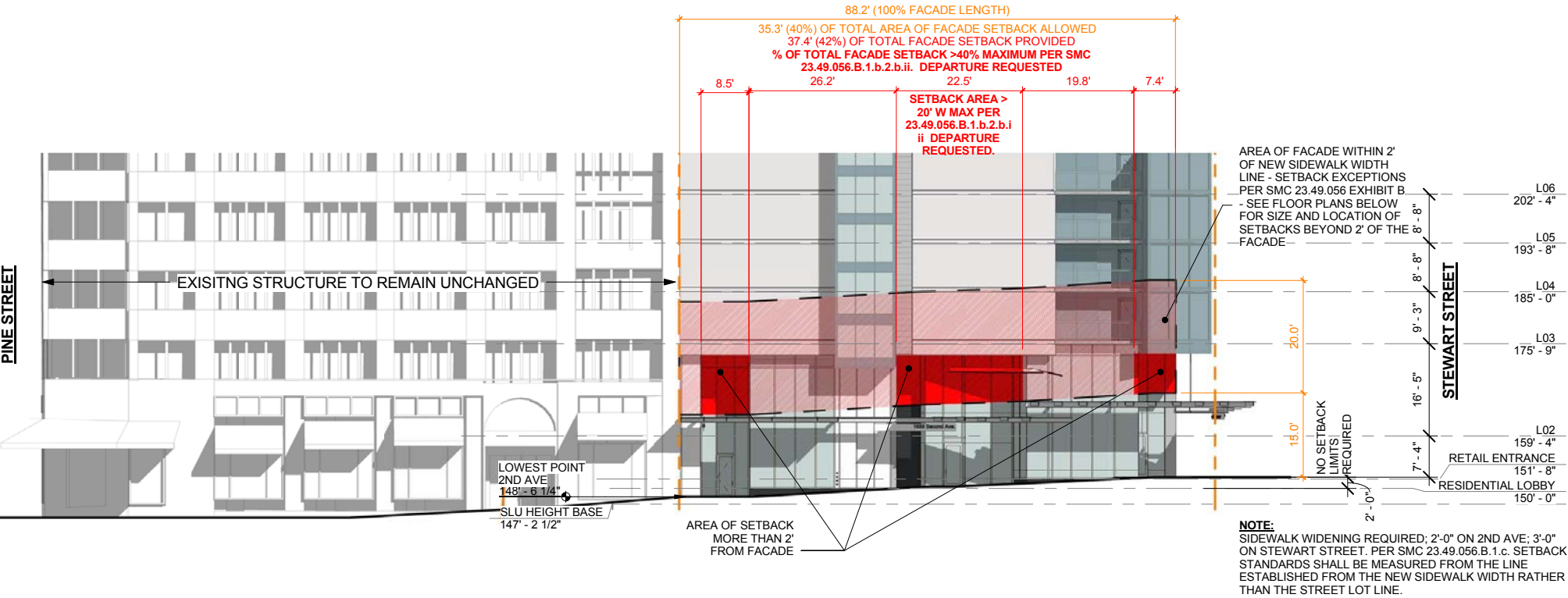
## OVERALL BUILDING SECTIONS



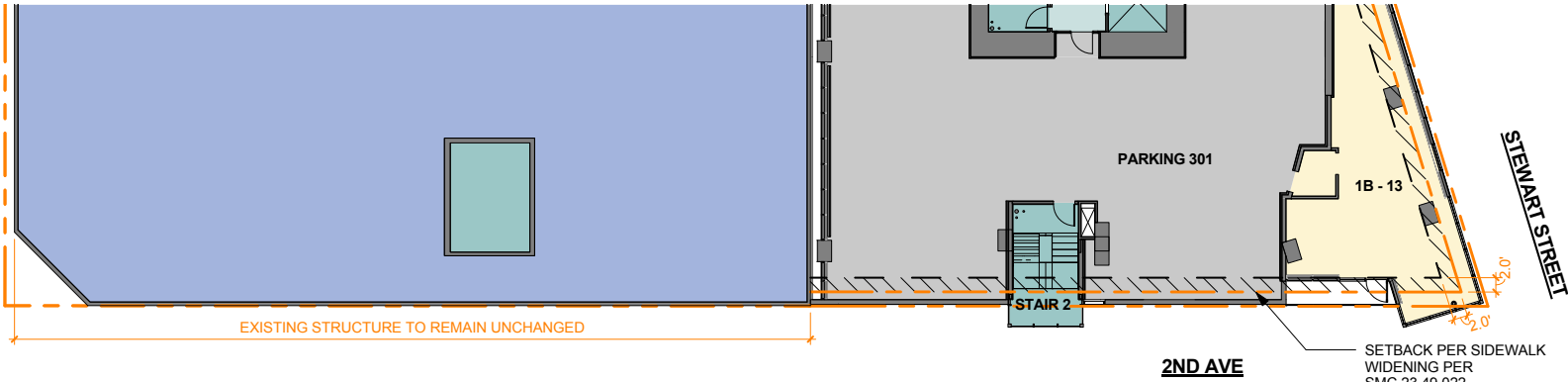
- Residential Use
- Units
- Retail
- Parking
- Amenity
- Vertical Circulation
- Back of House
- Office



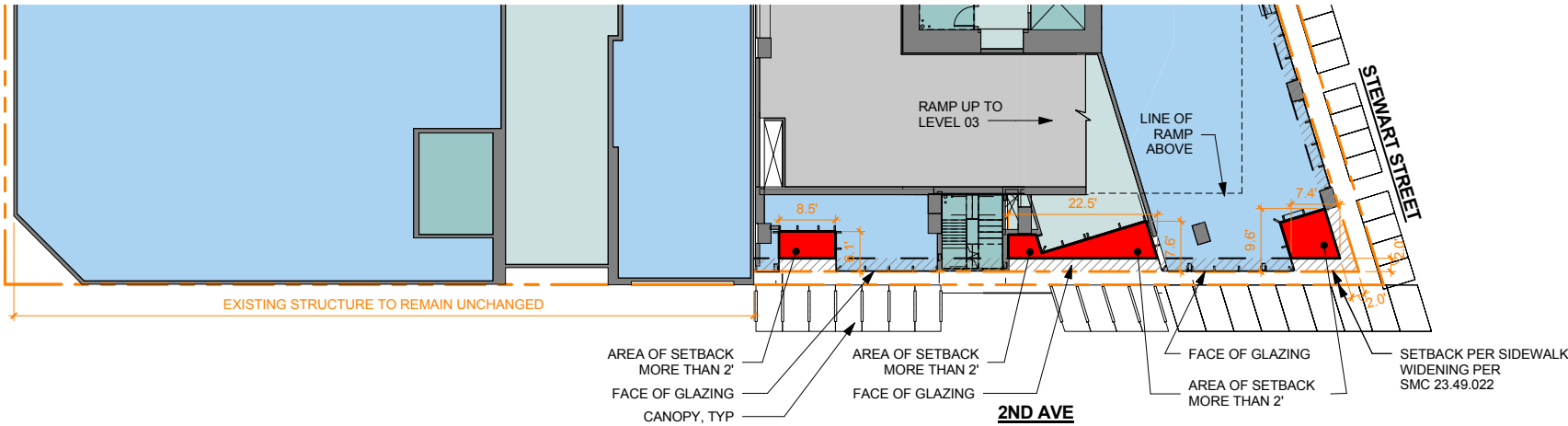
2nd Ave  
Elevation



level 3



level 2



ITEM #1

DEVELOPMENT STANDARD

SMC 23.49.056.B.1.b.2.b - Exemption to Maximum Setback Limits

REQUIREMENT

Facade setbacks shall be provided according to the requirements of Exhibit B for 23.49.056. The total area of a facade that is set back more than 2' from the street lot line shall not exceed 40% of the total facade area between the elevations of 15 and 35'. No setback deeper than 2' shall be wider than 20', measured parallel to the street lot line.

REQUEST

The applicant requests that the total area of a facade that is set back more than 2' from the street lot line shall exceed 40% by 2.1' (42% of total facade) and that a setback deeper than 2' shall be wider than the maximum 20' by 2.5' at the residential lobby.

JUSTIFICATION

- Setbacks on 2nd Ave and Stewart St are extensions in mass of the ground floor street edge, creating continuous transparent skin between L01 & L02. The setbacks at L01 are necessary to provide accessible door locations within the sidewalk dedications on 2nd Ave and Stewart St.

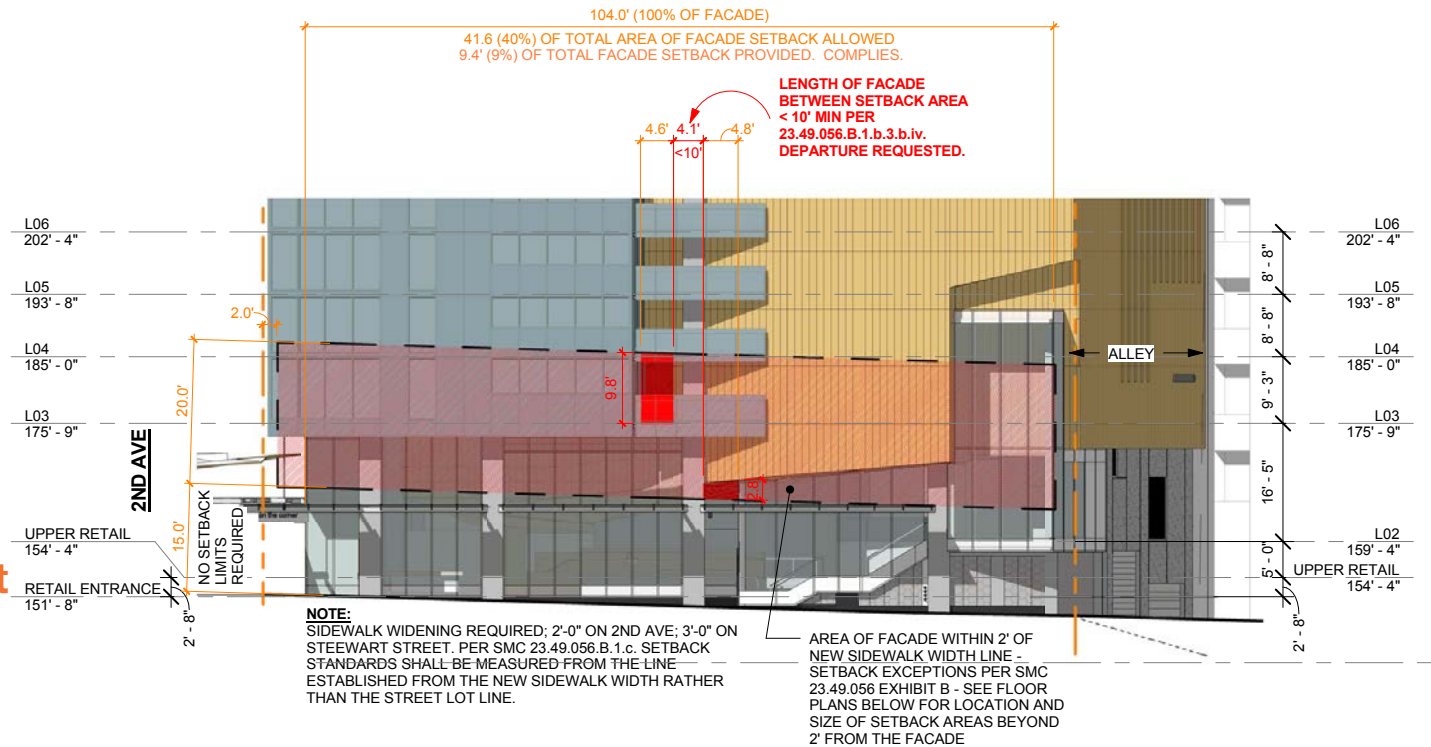
RELEVANT DESIGN GUIDELINES

The requested departure better meets and exceeds the design guidelines listed below:

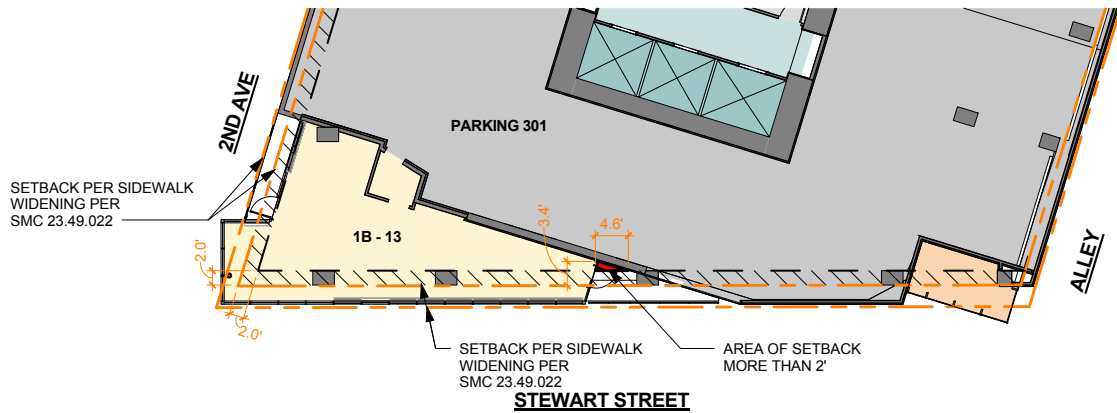
- B-2 Create a transition in bulk & scale.**
- B-4 Design a well-proportioned & unified building.**
- D-3 Provide elements that define the place.**
- E-2 Integrate parking facilities.**



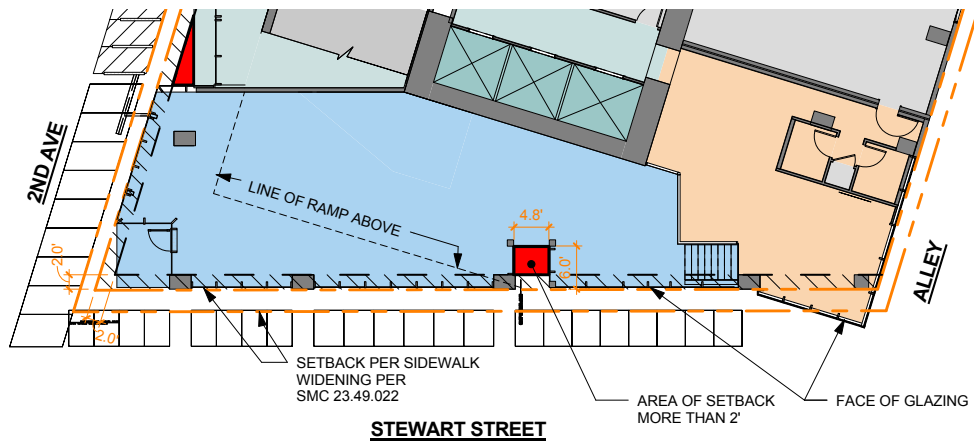
Stewart St Elevation



level 3



level 2



ITEM #1

DEVELOPMENT STANDARD

SMC 23.49.056.B.1.b.2.b - Exemption to Maximum Setback Limits

REQUIREMENT

Facade setbacks shall be provided according to the requirements of Exhibit B for 23.49.056. The facade of the structure shall return to within 2 feet of the street lot line between each setback area for a minimum of 10 feet. Balcony railings and other nonstructural features or walls are not considered the facade of the structure.

REQUEST

The applicant requests that the facade of the structure shall return to within 2 feet of the street lot line between each setback area for 4.1 feet on Stewart Street.

JUSTIFICATION

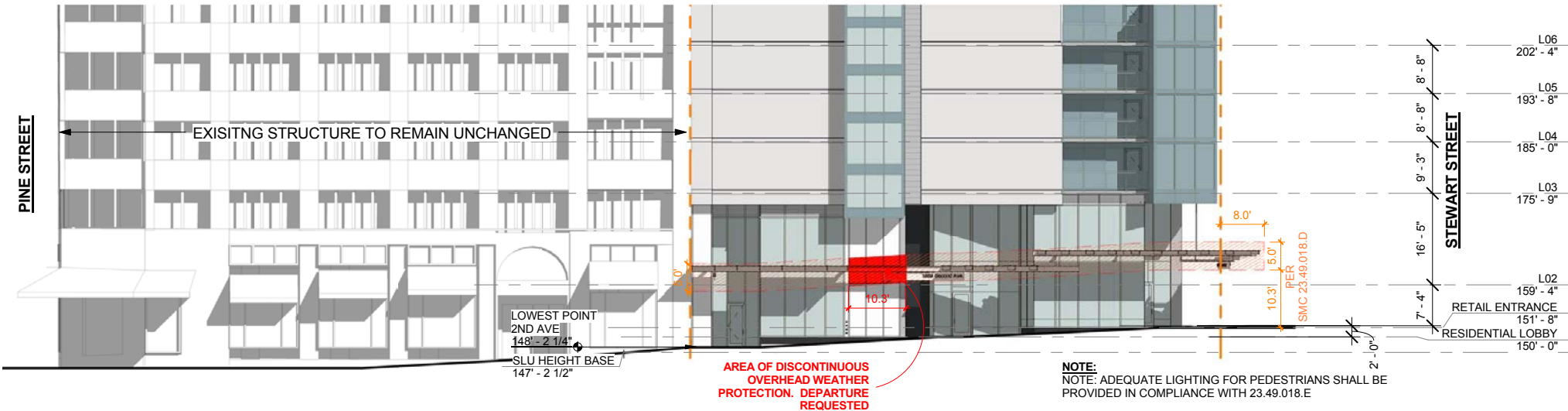
- Setbacks on 2nd Ave and Stewart St are extensions in mass of the ground floor street edge, creating continuous transparent skin between L01 & L02. The setbacks at L01 are necessary to provide accessible door locations within the sidewalk dedications on 2nd Ave and Stewart St.
- The residential balcony setback at Stewart St provides access to the residential balcony that architecturally integrates the upper level parking garage and the residential unit at L03.

RELEVANT DESIGN GUIDELINES

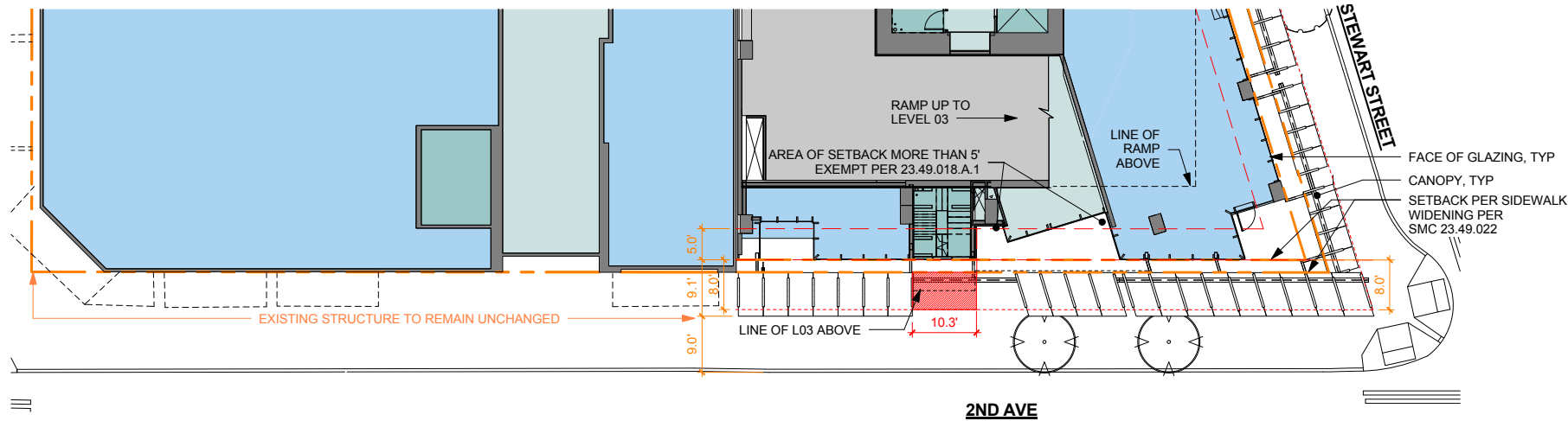
The requested departure better meets and exceeds the design guidelines listed below:  
**B-2 Create a transition in bulk & scale.**  
**B-4 Design a well-proportioned & unified building.**  
**D-3 Provide elements that define the place.**  
**E-2 Integrate parking facilities.**



2nd Ave  
Elevation



level 2



ITEM #2

DEVELOPMENT STANDARD

SMC 23.49.018 - Overhead Weather Protection

REQUIREMENT

Continuous overhead weather protection shall be required for new development along the entire street frontage of a lot except along those portions of the structure facade according to the requirements of 23.49.018

REQUEST

The applicant requests gaps in continuous overhead weather protection

JUSTIFICATION

- A gap in the continuous coverage emphasize the residential lobby entry canopy on 2nd Avenue. Weather protection is provided by the structural building overhang at L03.

RELEVANT DESIGN GUIDELINES

The requested departure better meets and exceeds the design guidelines listed below:

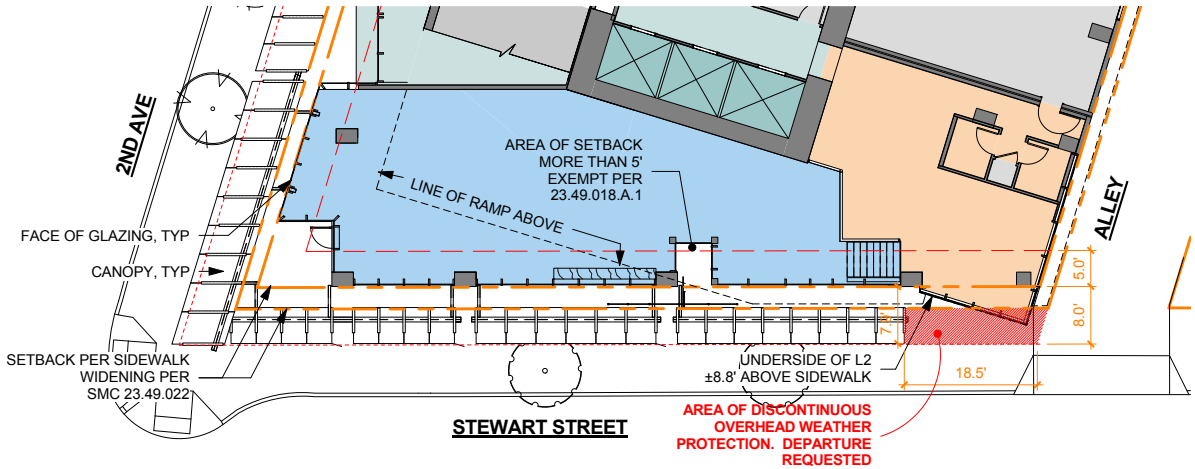
- A-1 Respond to the physical environment.
- D-3 Provide elements that define the place.



Stewart St  
Elevation



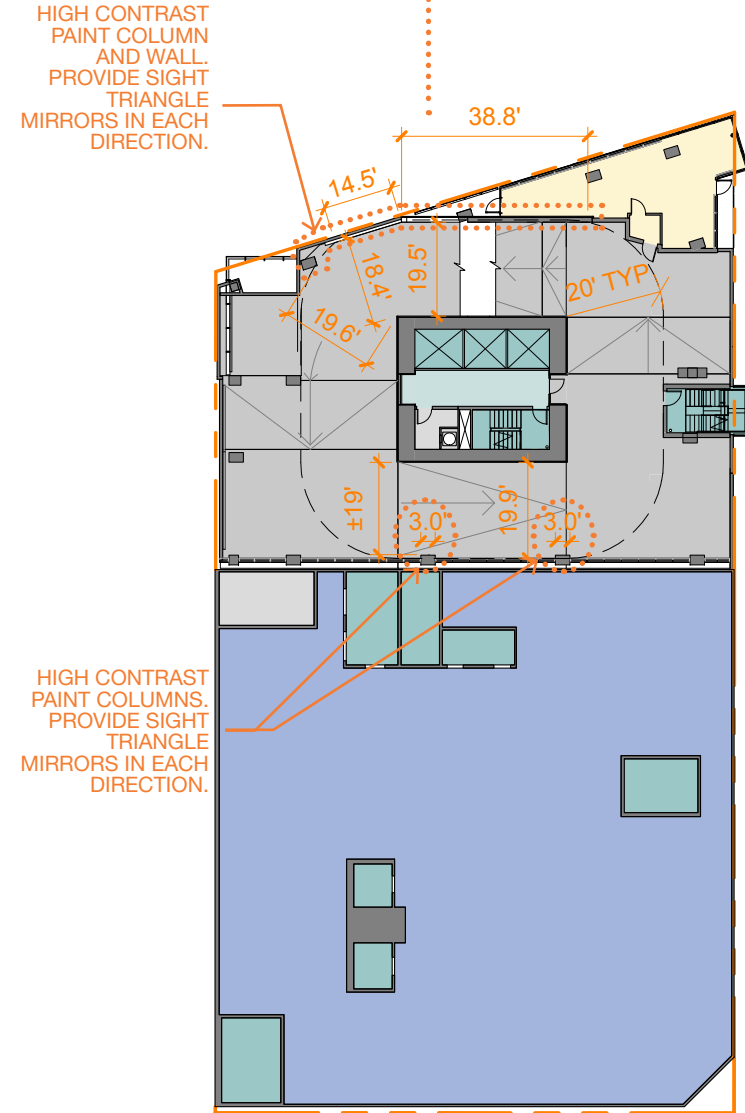
level 2



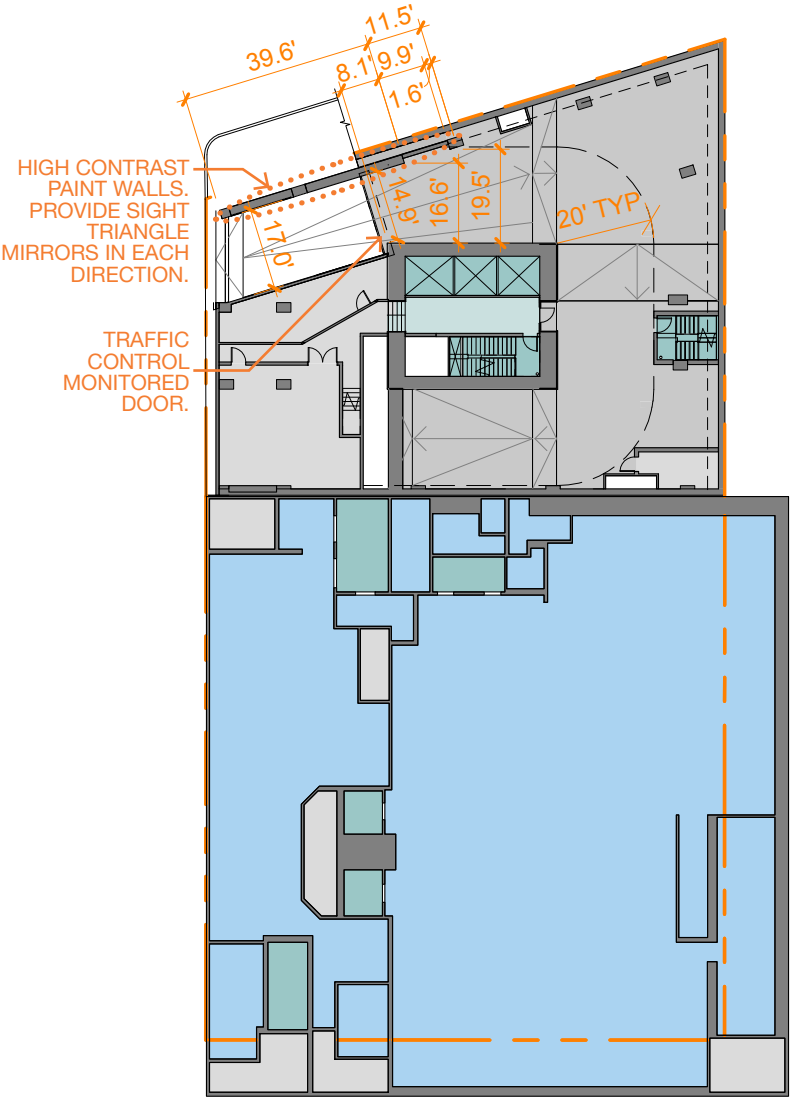
ITEM #2
DEVELOPMENT STANDARD
SMC 23.49.018 - Overhead Weather Protection
REQUIREMENT
Continuous overhead weather protection shall be required for new development along the entire street frontage of a lot except along those portions of the structure facade according to the requirements of 23.49.018
REQUEST
The applicant requests gaps in continuous overhead weather protection
JUSTIFICATION
<ul style="list-style-type: none"><li>Deletion of canopy at NW corner provides increased pedestrian and vehicular visual control of the Stewart St/alley juncture and provides architectural clarity to the residential 'lantern' that wraps Stewart St and the alley. Some overhead weather protection is offered by the structural building overhang at L02.</li></ul>
RELEVANT DESIGN GUIDELINES
The requested departure better meets and exceeds the design guidelines listed below:  <b>A-1 Respond to the physical environment.</b> <b>C-6 Develop the alley facade.</b> <b>D-3 Provide elements that define the place.</b> <b>E-2 Integrate parking facilities.</b>



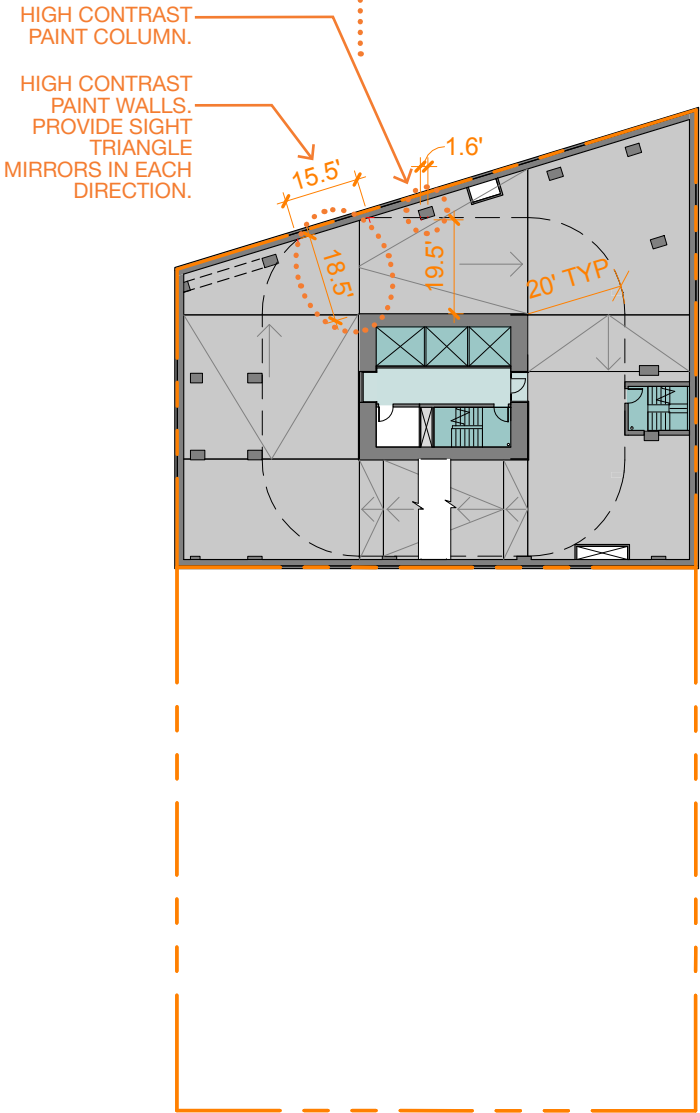
Examples of spot locations requesting varying widths less than 20'



above grade parking



street level/p1



below grade parking

ITEM #3

DEVELOPMENT STANDARD

SMC 23.49.030.E.1 - Parking Aisles

REQUIREMENT

Parking aisles shall be provided according to the requirements of Exhibit C for 23.54.030; footnote 1. Minimum Aisle width for two-way traffic shall be 20' or greater.

REQUEST

The applicant requests drive aisle widths of less than 20' for spot locations for drive aisles not associated with vehicle backing distances rather than 20' minimum required.

JUSTIFICATION

- No parking is required in downtown zones.
- No non-residential parking is being proposed. Garage to be used solely by residents familiar with the garage layout. The departure request does not impact vehicle backing distances from stalls or barrier free parking spaces.
- Low capacity garage (approx. 95 below-grade stalls and 50 above-grade stalls).
- 16' min dimension for two-way traffic is in the spirit of SMC.23.054.030.D.1.b. and City of Seattle alleys.
- Corner mirrors, traffic signaling, and high visibility marking of reduced areas to be implemented.
- Internal parking garage to reduce the impact of nearby street parking availability.

RELEVANT DESIGN GUIDELINES

The requested departure better meets and exceeds the design guidelines listed below:

- C-1 Promote pedestrian interaction.
- E-2 Integrate parking facilities.



# APPENDIX

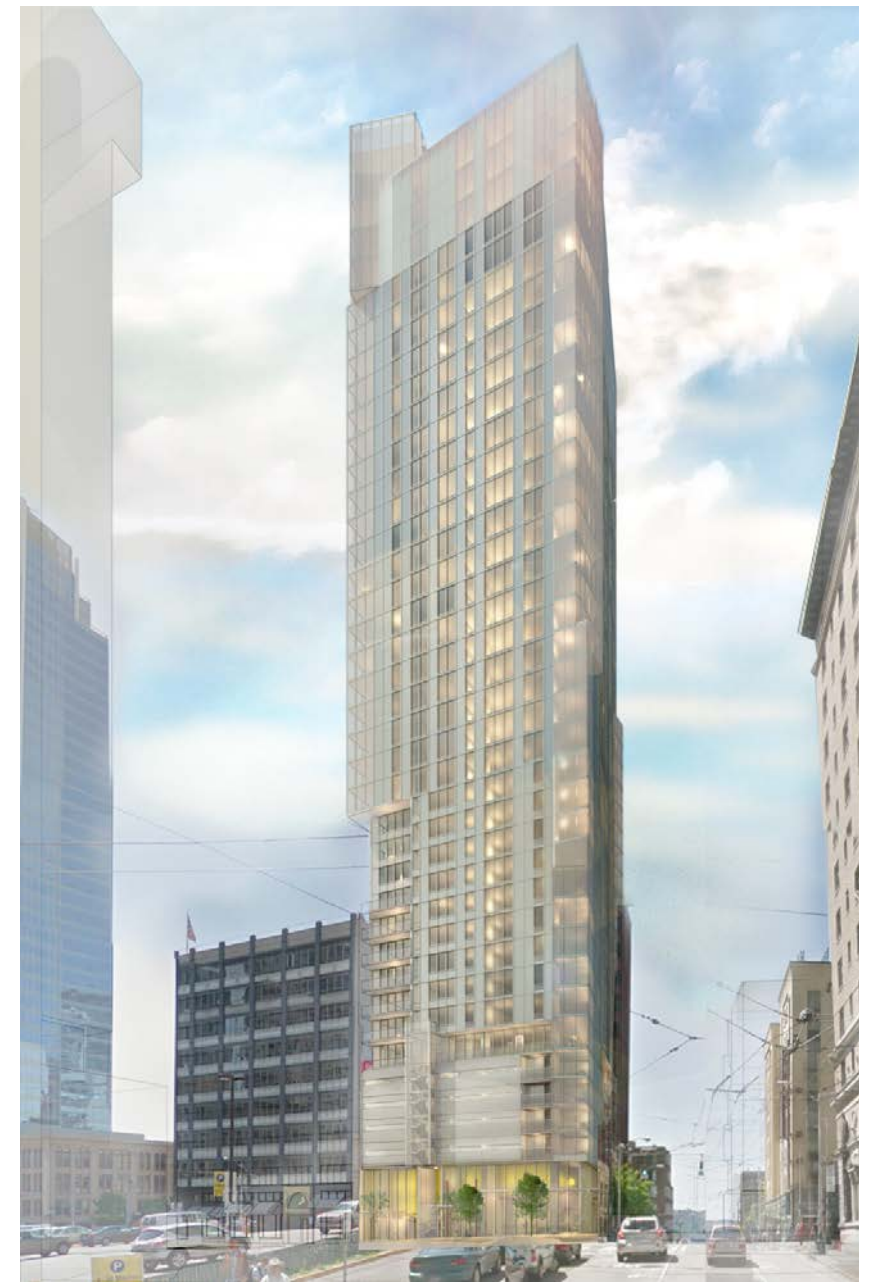




EDG #1 | **PREFERRED**



EDG #2 | **ALTERNATIVE 1**



EDG #2 | **ALTERNATIVE 2 PREFERRED**



**A-1 Respond to the physical environment.**

*Develop an architectural concept and compose the building's massing in response to geographic conditions and patterns of urban form found beyond the immediate context of the building site.*

**A-1 Response:**

The proposal site occurs at a change in the street grid alignment providing a non-standard lot shape. The massing of the building responds to the changing grid. Nearby buildings vary in height and scale. The proposal modulates its massing to respond to the different scales of its neighbors. The site occurs at the edge of a lower scale zone providing dramatic views of Elliot Bay, Pike Place Market and the downtown core.

**A-2 Enhance the skyline.**

*Design the upper portion of the building to promote visual interest and variety in the downtown skyline.*

**A-2 Response:**

The arrangement and expression of the residential rooftop amenity spaces provide a varied roof line. These spaces directly relate to the massing of the residential levels below.

**B-1 Respond to the neighborhood context.**

*Develop an architectural concept and compose the major building elements to reinforce desirable urban features existing in the surrounding neighborhood.*

**B-1 Response:**

The proposal's site near the northern edge of the Pike Place Market has the opportunity to contribute to the pedestrian experience on Stewart Street. The street level building facades are setback to create a two-story base. Retail spaces wrap the street level facades to activate the sidewalk. Two residential entries on each street provide address. The second floor residential lounge, open to retail, commands the corner created on Stewart Street and the alley providing a view to the Market as well as allowing the retail to wrap the corner offering a similar vantage point.

**B-2 Create a transition in bulk & scale.**

*Compose the massing of the building to create a transition to the height, bulk, and scale of development in neighboring or nearby less intensive zones.*

**B-2 response:**

The southeast facade, between the Broadacres Building and the mechanical penthouse of 101 Stewart, recesses its massing up to level 17, acknowledging the differing heights of its neighbors such as 1915 Second Ave.

**B-4 Design a well-proportioned & unified building.**

*Compose the massing and organize the publicly accessible interior and exterior spaces to create a well-proportioned building that exhibits a coherent architectural concept. Design the architectural elements and finish details to create a unified building, so that all components appear integral to the whole.*

**B-4 Response:**

The proposed area of new development on the site and irregular shape, combined with a 400' height achieved through the affordable housing incentive, creates a tall, slender appealing architectural form. Taut window wall cladding reinforces a distinctive form created by the unique site.

**C-1 Promote pedestrian interaction.**

*Spaces for street level uses should be designed to engage pedestrians with the activities occurring within them. Sidewalk-related spaces should be open to the general public and appear safe and welcoming.*

**C-1 Response:**

The expansive retail frontage enhances pedestrian interaction with double height transparent storefront facades at the street.





**C-6 Develop the alley facade.**

*To increase pedestrian safety, comfort, and interest, develop portions of the alley facade in response to the unique conditions of the site or project.*

**C-6 Response:**

Limited alley length for building services is mitigated with transparent facades at the intersection of the alley and Stewart Street. At the street level, a secondary day-to-day residential entry ascends via a stair to the second floor. This residential program is overlapped at the same intersection providing pedestrian activity, lighting for security for the alley and a vantage toward the Market and Elliot Bay.

**D- 3 Provide elements that define the place.**

*Provide special elements on the facades, within public open spaces, or on the sidewalk to create a distinct, attractive, and memorable “sense of place” associated with the building.*

**D-3 Response:**

The site's prominent location relative to the Pike Place Market, the downtown retail core, and Belltown neighborhood provides a unique opportunity. Retail spaces with double height glazing on Second Ave and Stewart Street wrap the north corner. A second floor residential lounge is positioned at Stewart Street and the alley. The angle of the alley relative to Stewart creates a highly visible corner from the market. The tall storefront glazed bay could provide a memorable marker viewed from the Market on Stewart.





23.49.008 Structure Height

The base structure height for residential use is 290'. The maximum height is 400’.

The proposed structure height is 400' and intends to meet the requirements of SMC 23.49.015 (voluntary agreements for low and moderate income housing.)

23.49.009 Street-Level Use Requirements

- A. Street-level uses are required per Map 1G; required street-level uses include Retail sales.
- B.1 A minimum of 75% of each street frontage must be occupied by uses listed in subsection A. The remaining 25% may contain other permitted uses and/or pedestrian or vehicular entrances
- B.3 Required street-level uses shall be located within ten (10) feet of the new sidewalk width.

The proposal complies.

23.49.011 Floor area ratio

- Table A: Base = 5; Max = 7
- B. Exemptions and deductions from FAR calculations.
- b. Street-level uses meeting the requirements of Section 23.49.009.

Chargeable FAR will not exceed the applicable base FAR; The proposal complies.

23.49.018 Overhead Weather Protection and Lighting.

- A. Continuous overhead weather protection shall be required for new development along the entire street frontage of a lot, except along those portions of the structure facade that:
  - 3. Are separated from the street property line or widened sidewalk on private property by a landscaped area at least two (2) feet in width.
- B. Overhead weather protection shall have a minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.

Please see departure request on pages 47-48.

23.49.019 Parking quantity, location and access requirements, and screening and landscaping of surface parking areas.

- B.1.a. Parking at street level. On Class I pedestrian streets, parking is not permitted at street level unless separated from the street by other uses, provided that garage doors need not be separated.
- 2. Except as provided in subsection B1 above for parking at street level, parking within structures shall be located below street level or separated from the street by other uses, except as follows:
  - a(1) One (1) story of parking shall be permitted above the first story of a structure for each story of parking provided below grade that is of at least equivalent capacity, up to a maximum of four (4) stories of parking above the first story.
  - a(2) Parking above the third story of a structure shall be separated from the street by another use for a minimum of thirty (30) percent of each street frontage of the structure. For structures on lots located at street intersections, the separation by another use shall be provided at the corner portion(s) of the structure.
  - a(3) The perimeter of each story of parking above the first story of the structure shall have an opaque screen at least three and one-half (3 1/2) feet high where the parking is not separated from the street by another use.

Seven (7) levels of below-grade parking and 4 levels of above-grade parking are proposed, per SMC 23.49.019.B.2.a(1). Proposal complies.

SMC 23.49.022 Minimum Sidewalk and Alley Width

MAP C; Stewart Street to be increased 3'; 2nd Avenue to be increased 2'; a 2' alley dedication is required. The proposal complies.

SMC 23.49.056 Street facade, landscaping, and street setback requirements

A. Minimum Facade Height. Table A, Class 1 pedestrian streets: 25’

The proposal complies.

B.1.b.2.b - Exemption to Maximum Setback Limits

Please see departure request on pages 45-46.

- C. Facade Transparency Requirements.
- 2. Facade transparency requirements do not apply to portions of structures in residential use.
- 4.a. Class I pedestrian streets: A minimum of 60 percent of the street level street-facing facade shall be transparent.

On the single defined lot, facades of the existing structure to remain, will remain unchanged; For portions of the proposed structure not in residential use, min 60% of street level street-facing facades will be transparent.

- D. Blank Facade Limits.
- c. Blank facade limits do not apply to portions of structures in residential use.
- 2.a. Blank Facade Limits for Class I Pedestrian Streets shall be no more than 15 feet wide.
- c. The total width of all blank facade segments, including garage doors, shall not exceed 40 percent of the street-facing facade of the structure on each street frontage, or 50 percent if the slope of the street frontage of the facade exceeds 7.5 percent.

Facades of the existing structure to remain, will remain unchanged; the proposed structure will comply.

E. Street Tree Requirements. Street trees are required on all streets that have a pedestrian classification and about a lot.

The proposal complies.

SMC 23.49.058.B Facade Modulation

Facade modulation required above 85' for portions of the structure within 15' of a street property line. Length of unmodulated facades decreases per Table 23.49.058A

The proposal complies.

23.49.058.D.1 Tower Floor Area Limits

Table 23.49.058D1: 10,700 sf average floor area for structures exceeding the base height; The maximum floor area of any story in a tower is 11,500 sf

The proposal complies.

23.49.058.D.2 Maximum Tower Width

Maximum facade width above 85' along 2nd Avenue. Limited to 80% of the street frontage or 120' whichever is less

The proposal's maximum tower width is 120'; The proposal complies.

SMC 23.49.058.E Tower Spacing

No separation required between structures on different blocks; on the same block but different zones and from existing structures allowed before the effective date of ordinance 122054.

The proposal complies.

SMC 23.54.030.E.1 Parking Aisles

Please see departure request on page 49.