

# #3016656

6301 15th Ave NW **EARLY DESIGN GUIDANCE**

**S+HWorks** NORĒN  
ARCHITECTURE & DESIGN DEVELOPMENT

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## PROJECT DETAILS

ADDRESS	6301 15TH AVE NW
APN	2767600975
DPD#	3016656
APPLICANT	HUGH SCHAEFFER S+H WORKS LLC 1101 E PIKE ST STE 200 SEATTLE, WA 98122 P 206 329 1802 E HUGH@S-HW.COM
OWNER	NOREN DEVELOPMENT
ARCHITECT	S+H WORKS, LLC
SURVEYOR	EMERALD LAND SURVEYING

## DEVELOPMENT OBJECTIVES

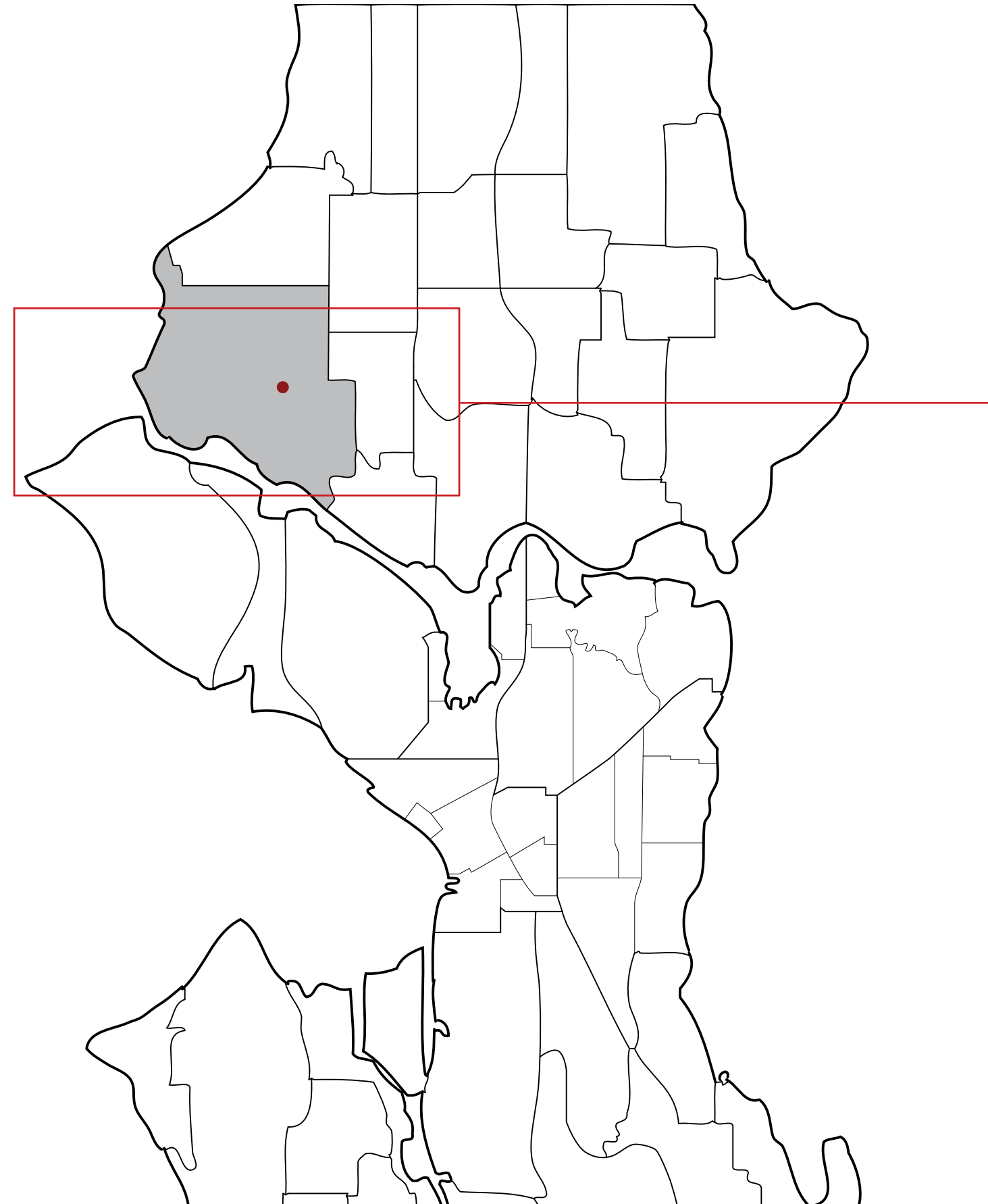
THE PROPOSED DEVELOPMENT IS A 4 STORY MIXED-USE APARTMENT BUILDING CONTAINING 3,000 SF OF COMMERCIAL, 2 LIVE-WORK UNITS AND 61 STUDIO APARTMENTS. NO PARKING IS REQUIRED OR PROPOSED.

## PROJECT GOALS

- DESIGN A PROJECT THAT RESPECTS BOTH THE COMMERCIAL AND RESIDENTIAL NATURE OF THE AREA.
- CREATE A STRONG DESIGN, ATTRACTIVE TO PEDESTRIANS AND VEHICULAR TRAFFIC.
- CREATE A PEDESTRIAN FRIENDLY PROJECT IN AN AREA DOMINATED BY AUTO-ORIENTED BUSINESSES.
- CREATE VIABLE GROUND FLOOR COMMERCIAL RATHER THAN LIVE-WORK UNITS.
- RESPOND APPROPRIATELY TO ADJACENT RESIDENTIAL USES.

## ZONING

THE PROPOSED PROJECT IS ZONED NC3-40 AND IS WITHIN THE BALLARD HUB URBAN VILLAGE AND THE FREQUENT TRANSIT OVERLAY.





### VICINITY INFORMATION

THE PROPOSED DEVELOPMENT IS LOCATED WITHIN THE BALLARD HUB URBAN VILLAGE. BALLARD IS DIVERSE IN ITS DEVELOPMENT PATTERNS AND HAS BEEN GOING THROUGH A PERIOD OF STEADY GROWTH. THE SITE IS LOCATED AWAY FROM POPULAR PEDESTRIAN DESTINATIONS, BUT ALONG A MAJOR ARTERIAL.

- BALLARD
- BALLARD HUB URBAN VILLAGE
- STUDY AREA
- SITE

## ADJACENCIES

THE SITE IS SITUATED AT THE CORNER OF THE MAJOR ARTERIAL 15TH AVE NW, AS WELL AS 63RD AVE NW, A FAR CALMER RESIDENTIAL STREET.

ON AN AVERAGE WEEKDAY 36,000 CARS A DAY PASS THE SITE ON 15TH AVE NW. THE 15TH AVE NW CORRIDOR IS DOMINATED BY VEHICULAR ORIENTED BUSINESSES. BETWEEN MARKET ST & 65TH THERE HAS BEEN LITTLE DEVELOPMENT IN THE PAST DECADES. WHILE THE CORRIDOR IS VEHICLE ORIENTED THERE ARE SUCCESSFUL PEDESTRIAN ORIENTED BUSINESSES FACING 15TH.

ALONG 63RD NW, THE STREET TRANSITIONS FROM THE BUSTLE OF 15TH NW TO A QUIETER RESIDENTIAL STREET.

TO THE NORTH, EAST & SOUTH THE LOT ABUTS OR IS ACROSS THE STREET FROM LOTS ALSO ZONED NC3-40. ABUTTING TO THE WEST IS A LOT ZONED LR1 RECENTLY DEVELOPED AS FOUR SINGLE FAMILY HOMES.

WALK SCORE 83  
BIKE SCORE 71  
(Source: walkscore.com)

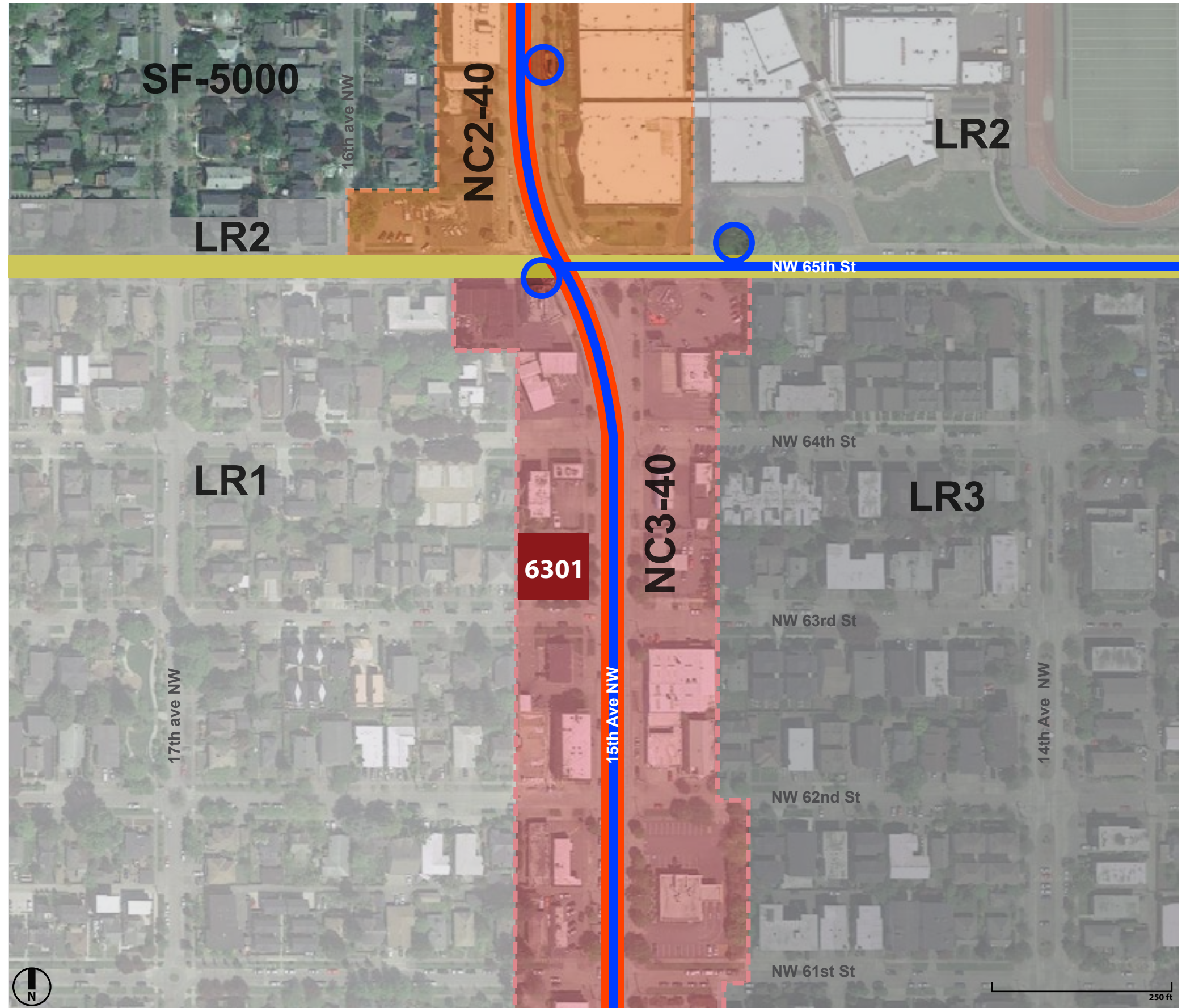
## ZONING

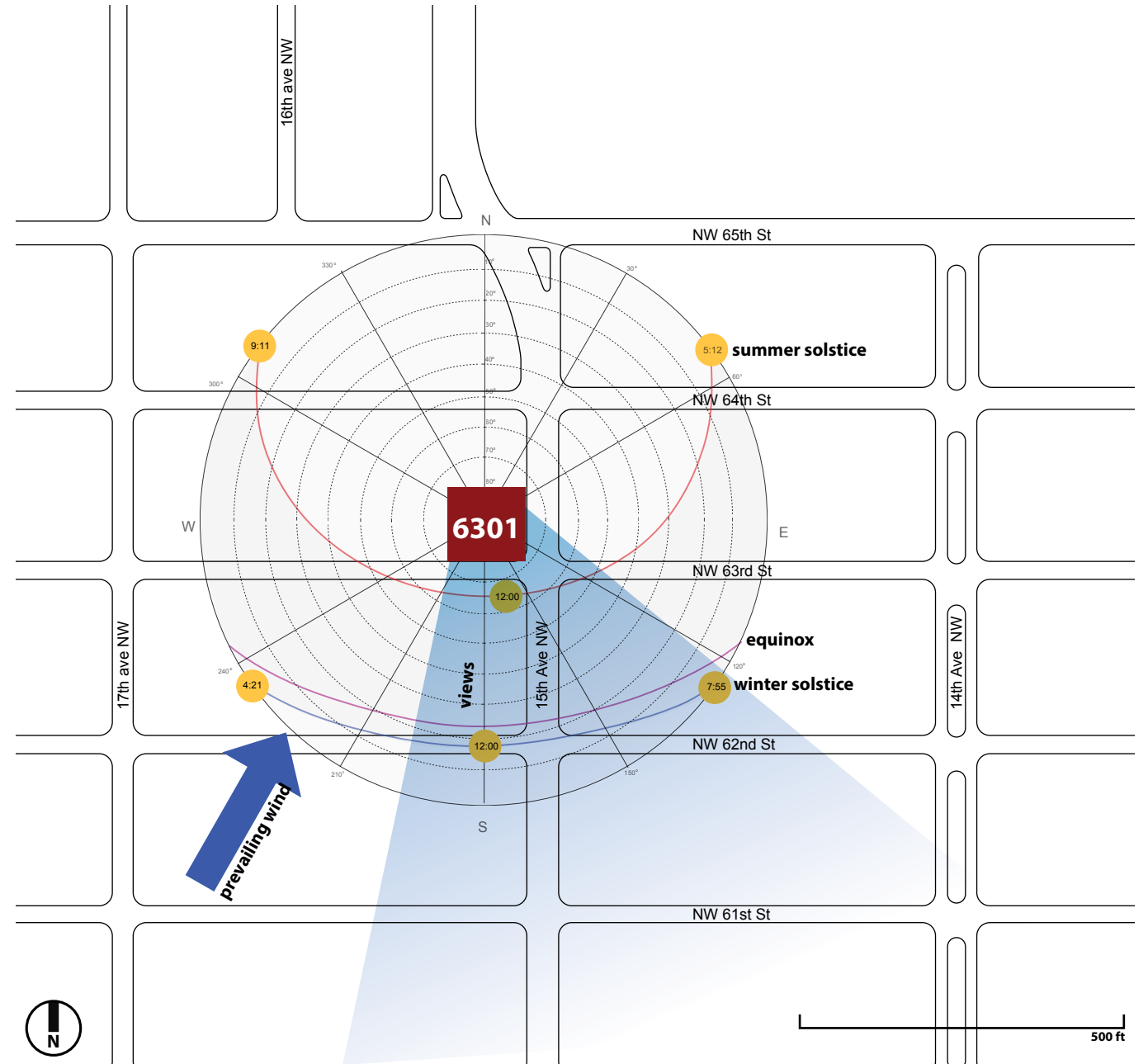
- SF-5000
- LR1
- LR2
- LR3
- NC2-40
- NC3-40

## STREETS

- MAJOR ARTERIAL
- MINOR ARTERIAL
- TRANSIT ROUTE
- TRANSIT STOP: ROUTE 15, D-LINE

# 4





### CURRENT LAND-USE

WEST OF 15TH AVE NW THERE HAS BEEN SUBSTANTIAL NEW TOWNHOME DEVELOPMENT (OR MULTIPLE SINGLE FAMILY HOUSES BUILT ON A SINGLE LOT). LITTLE NEW DEVELOPMENT HAS OCCURRED ALONG 15TH AVE NW, AND THE PRESENCE OF AUTO-ORIENTED SERVICES IS SIGNIFICANT.

- SINGLE FAMILY
- MULTI-FAMILY
- AUTO-ORIENTED SERVICE
- MIXED USE
- RETAIL / RESTAURANT
- PUBLIC PARK / SCHOOL
- NEW DEVELOPMENT
- SITE

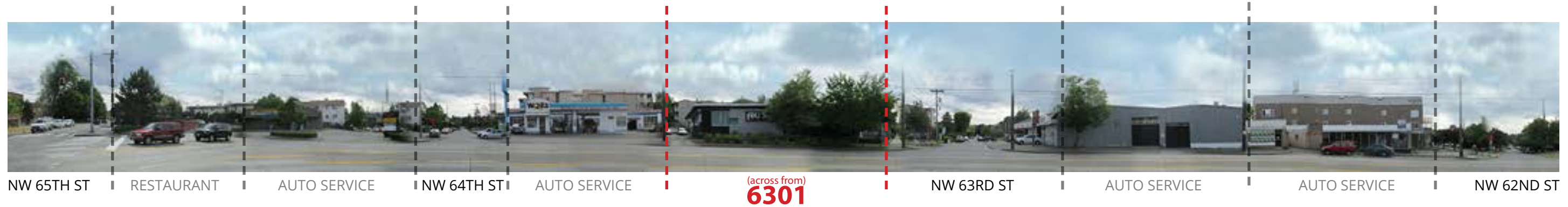
### ENVIRONMENTAL FACTORS

THE PRINCIPAL STREET-FACING ELEVATIONS FACE SOUTH AND EAST. NEIGHBORHOOD AND TERRITORIAL VIEWS ARE AVAILABLE TO THE EAST, WEST AND SOUTH.

**1** 15TH AVE LOOKING WEST



**2** 15TH AVE LOOKING EAST



**STREETSCAPE**

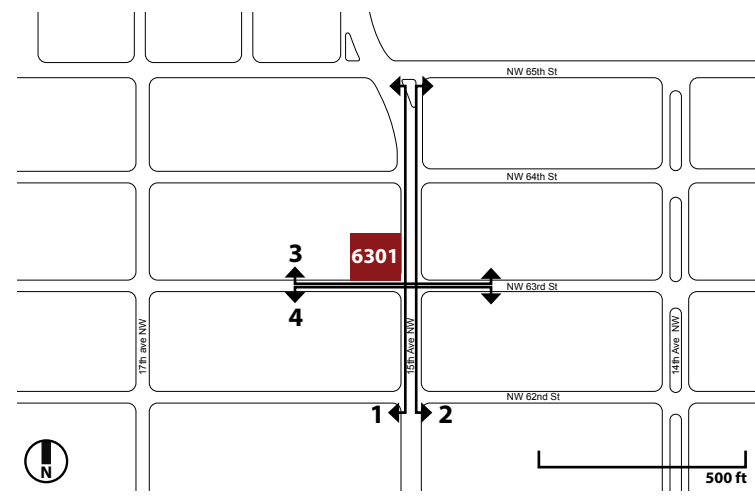
THE STREETSCAPES VARY IN CHARACTER IN KEEPING WITH THE USES. NEITHER STREET HAS A CONSISTENT SITING PATTERN. 15TH AVE NW CONTAINS A LARGE AMOUNT OF CURB CUTS, LIMITED STREET TREES AND MOSTLY SMALL SINGLE STORY AUTO-ORIENTED BUSINESSES.

63RD NW BEGINS WITH A SIMILAR PATTERN BUT TRANSITIONS TO A NEIGHBORHOOD SCALE WITH AN INTERMIX OF NEW TOWNHOMES AND EXISTING SINGLE FAMILY HOMES. 15TH AVE NW HAS SEEN LITTLE DEVELOPMENT WHILE THE MULTIFAMILY ZONES ARE BEING RAPIDLY DEVELOPED WITH TOWNHOMES AND SINGLE-FAMILY RESIDENCES.

**3** NW 63RD ST LOOKING NORTH




**4** NW 63RD ST LOOKING SOUTH



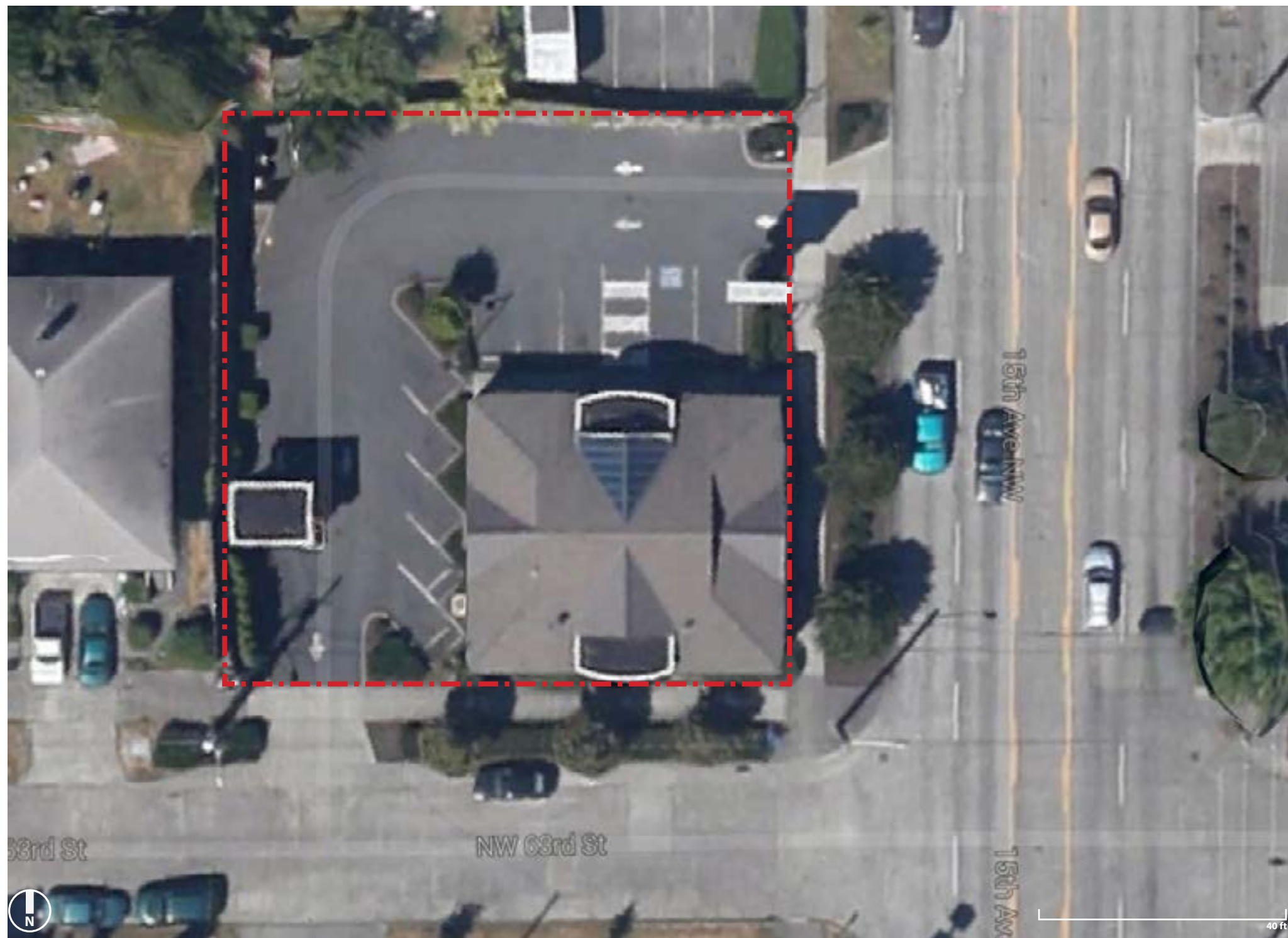


### RECENT DEVELOPMENTS

THE BLOCK TO THE WEST OF THE LOT HAS SEEN SIGNIFICANT DEVELOPMENT OF TOWNHOMES IN THE PAST SEVERAL YEARS. THE STYLE IS TYPICALLY MODERN WITH A MIXED MATERIAL PALETTE.

-  SITE
- A TOWNHOUSES  
1543 NW 63<sup>RD</sup> ST
- B TOWNHOUSES  
1533 NW 63<sup>RD</sup> ST
- C MULTIFAMILY  
1511 NW 64<sup>TH</sup> ST
- D SMALL SINGLE-FAMILY  
1510 NW 63<sup>RD</sup> ST
- E TOWNHOUSES  
1511 NW 63<sup>RD</sup> ST
- F LIVE-WORK + TOWNHOMES  
1506 NW 62<sup>ND</sup> ST





## EXISTING USE

THE EXISTING BRICK STRUCTURE ON THE SITE IS A SUBURBAN-TYPE BRICK BANK BUILDING BUILT IN THE 1980S. THE BUILDING HAS BEEN VACANT FOR YEARS. ASSOCIATED WITH THE AUTO-ORIENTED NATURE OF THE BANK IS SURFACE PARKING AND A DRIVE THROUGH KIOSK. ALL FEATURES RELATED TO THE EXISTING STRUCTURE WILL BE REMOVED.

# EXISTING CONDITIONS

## TOPOGRAPHY

FROM THE SOUTHEAST CORNER, THE SITE SLOPES UP APPROXIMATELY 4' ALONG BOTH STREET FRONTAGES. HAVING BEEN PREVIOUSLY LEVELED, THE SITE HAS RETAINING WALLS ALONG THE NORTH & WEST PROPERTY LINES. ADJACENT DEVELOPMENTS START 4' HIGHER THAN THE PROPOSED PROJECT. THE EXISTING PARKING LOT IS APPROXIMATELY 2' ABOVE THE ADJACENT SIDEWALK AT THE SE CORNER. THE PROPOSED DEVELOPMENT WILL FURTHER LEVEL THE SITE SO ALL COMMERCIAL USES ENTER DIRECTLY FROM SIDEWALK GRADE.

## ACCESS

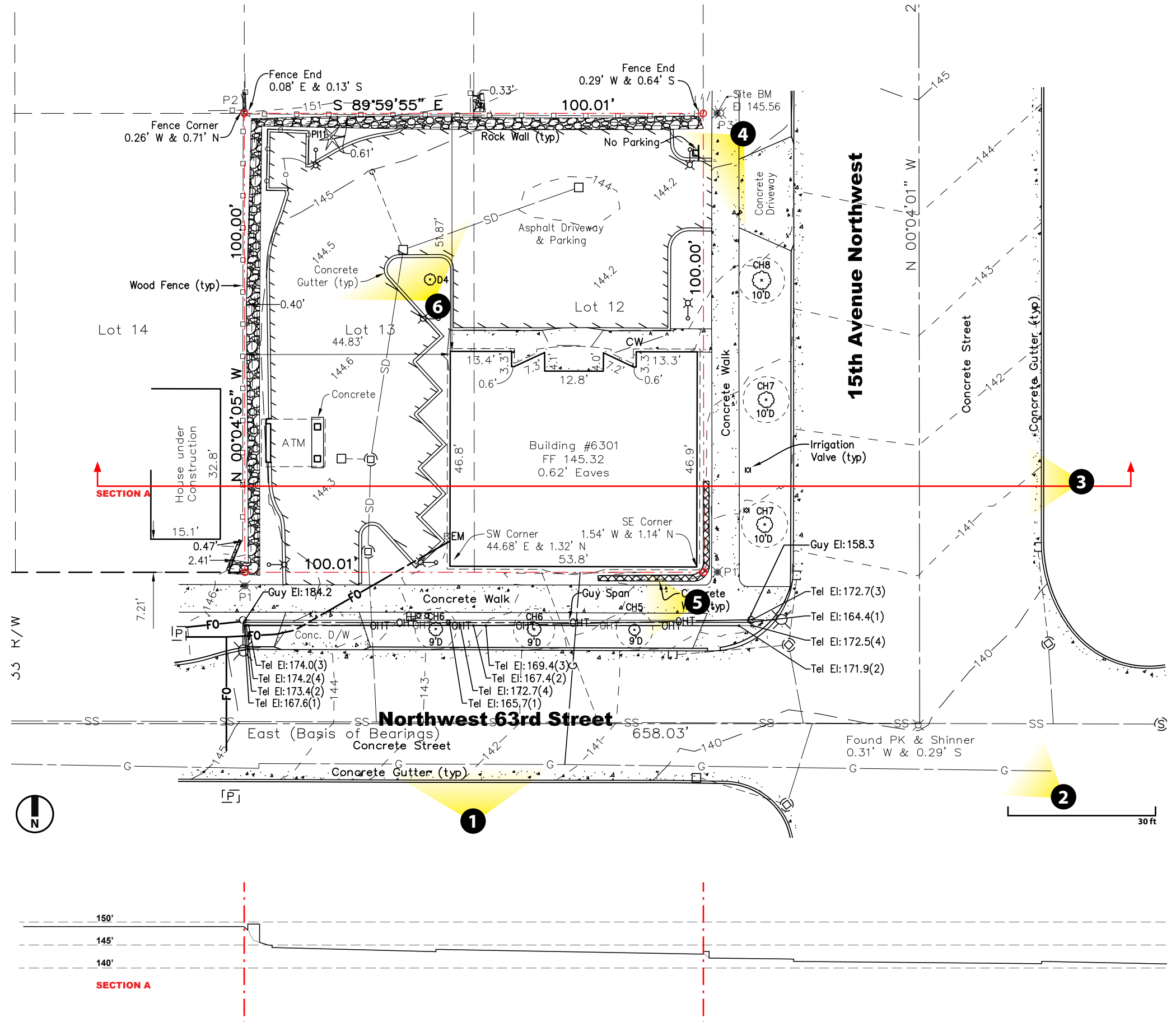
THE SITE IS CURRENTLY ACCESSED BY TWO CURB CUTS, ONE ALONG EACH STREET FRONTAGE. BOTH CURB CUTS WILL BE REMOVED AND REPLACED WITH PLANTING STRIPS AND SIDEWALKS TO MATCH THE EXISTING.

## VEGETATION

THE EXISTING STREET TREES ARE IN VERY GOOD CONDITION AND WILL BE RETAINED PER THE CITY ARBORIST. PRUNING WILL BE REQUIRED TO KEEP THE TREES WITHIN THE PLANTING STRIP AND OPEN UP THE SIDEWALK. EXISTING VEGETATION WITHIN THE PLANTING STRIP WILL BE REPLACED WITH MORE CONTEMPORARY PLANTINGS.

## POWER LINES

EXISTING POWER-LINES SOUTH OF THE SITE ARE LOW VOLTAGE AND DO NOT IMPACT THE PROPOSED DEVELOPMENT.

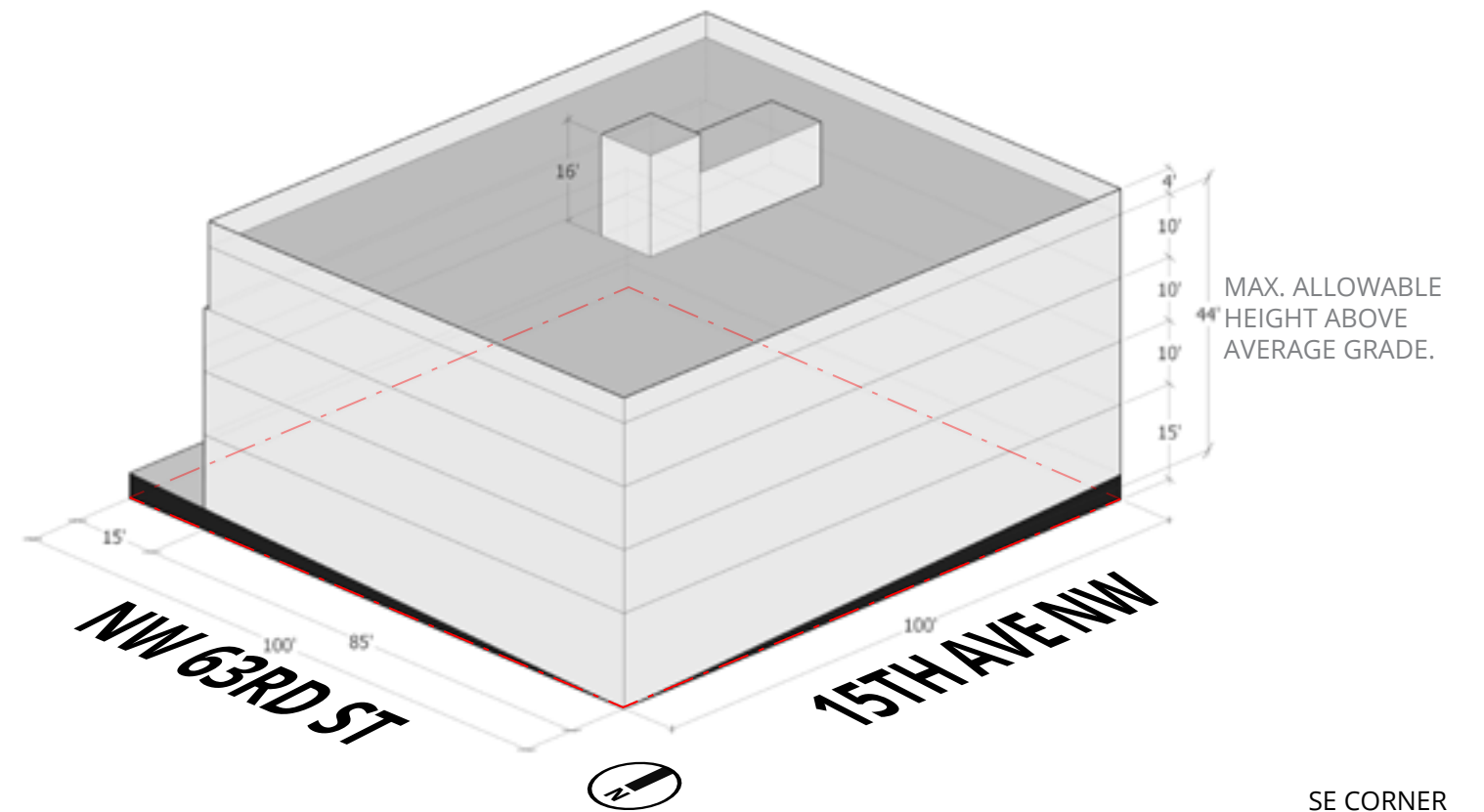
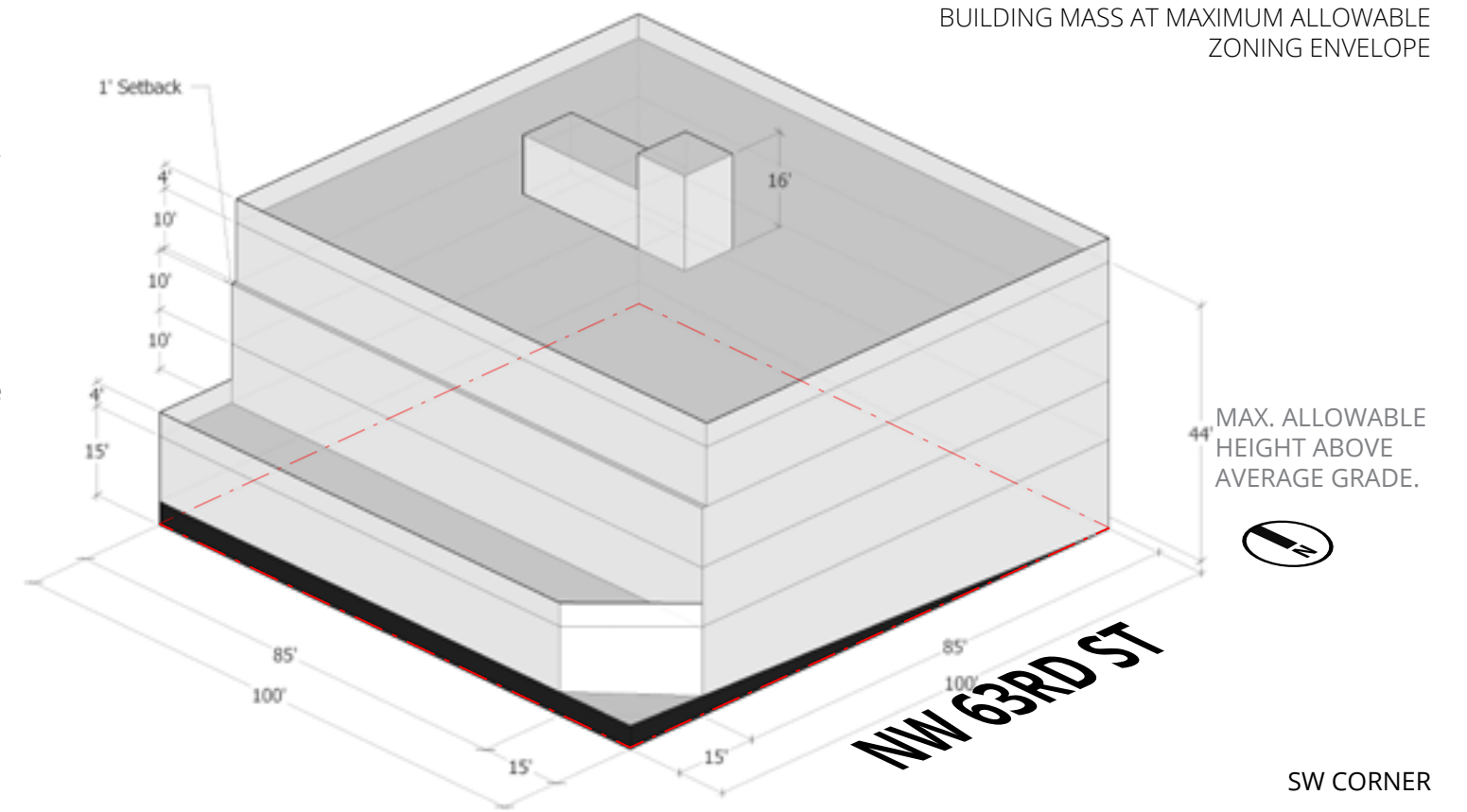




ZONING: NC3-40  
 OVERLAYS: BALLARD HUB URBAN VILLAGE, FREQUENT TRANSIT

**ZONING ENVELOPE**  
 BUILDING MASS AT MAXIMUM ALLOWABLE  
 ZONING ENVELOPE

SMC Code	Definition
SMC 23.47A.004	<b>PERMITTED USES</b> Uses permitted outright: eating drinking establishments, lodging, offices, sales & service, residential & live work. Live work is considered a commercial use.
SMC 23.47A.005	<b>STREET-LEVEL USES</b> Residential uses are limited to 20% of the ground floor street-level facade when facing an arterial.
SMC 23.47A.008	<b>STREET-LEVEL DEVELOPMENT STANDARDS</b> <ul style="list-style-type: none"> <li>- Blank facades may not exceed 20' in width or 40% of the facade within 2' and 8' above sidewalk grade.</li> <li>- 60% transparency required between 2' and 8' above sidewalk grade.</li> <li>- 15' min and 30' average commercial depth required.</li> <li>- Commercial at grade shall have a minimum 13' floor-to-floor height.</li> <li>- Street facing facades shall be within 10' of the lot line.</li> </ul>
SMC 23.47A.012	<b>HEIGHT</b> <ul style="list-style-type: none"> <li>- Base height limit of 40' above average grade.</li> <li>- Additional 4' of height for having a 13' minimum floor-to-floor height at ground level.</li> <li>- Parapets, railings etc. may extend 4' above the allowed height limit.</li> <li>- Stair and elevator penthouses may extend 16' above the allowed height limit.</li> </ul>
SMC 23.47A.013	<b>FLOOR AREA RATIO</b> 3.25 FAR allowed for mixed-use development
SMC 23.47A.014	<b>SETBACKS</b> <ul style="list-style-type: none"> <li>- 15'x15' triangular setback required where a structure with a residential use abuts the lot line of a lot within a residential zone.</li> <li>- 15' rear setback above 13' required where abutting a lot in a residential zone.</li> <li>- Additional setback of 2' horizontal for every 10' vertical above 40 is required .</li> </ul>
SMC 23.47A.016	<b>LANDSCAPING</b> - 0.30 Green Factor Required
SMC 23.47A.024	<b>AMENITY AREA</b> <ul style="list-style-type: none"> <li>- 5% of total floor area in residential use shall be provided as amenity area.</li> <li>- All residential shall have access to a common or private amenity area.</li> <li>- Common Amenity: minimum 250 sf, 10' min. dimension. Private decks &amp; balconies shall be 60 sf min., 6' min. dimension.</li> </ul>
SMC 23.54.015	<b>REQUIRED PARKING</b> <ul style="list-style-type: none"> <li>- No vehicular parking required (Urban Village + Frequent Transit)</li> <li>- Bicycle Parking:           <ul style="list-style-type: none"> <li>- Residential, Long Term: 1 space per 4 units</li> <li>- Commercial, Long Term 1 per 12,000 SF, Short Term, 1 per 4,000 sf.</li> </ul> </li> </ul>
SMC 23.54.040	<b>SOLID WASTER &amp; RECYCLING</b> <ul style="list-style-type: none"> <li>- Residential (51-100 units): 375sf + 4sf per unit above 50 units</li> <li>- Commercial (0-5,000 sf): 82sf.</li> <li>- Mixed-use building must meet the storage requirement for the residential portion of the structure + 50% of the commercial requirement.</li> <li>- 12' minimum horizontal dimension</li> </ul>



## PRIORITY GUIDELINES

### CS1 NATURAL SYSTEMS AND SITE FEATURES

#### C. TOPOGRAPHY

1. Land Form: The site was previously graded creating a relatively flat lot. At the north and west property lines the adjacent parcels are 4 feet higher than the proposed lot. This differential will be maintained, and the preferred alternative takes advantage of the land form by creating an entry that is private for the residents and creates a buffer between the zones.

### CS2 URBAN PATTERN AND FORM

#### A. LOCATION IN THE CITY AND NEIGHBORHOOD

2. Architectural Presence: The lot is highly visible from the 15th Ave NW corridor. An estimated 36,000 vehicles a day will pass the building. While there is currently limited pedestrian traffic, the area is developing and there are pedestrian friendly activities there will be more foot traffic. The design needs to reflect both the vehicular and potential pedestrian traffic.

#### B. ADJACENT SITES, STREETS, AND OPEN SPACES

2. Connection to the Street: The preferred alternative has extensive commercial facing the 15th Ave NW Arterial with multiple entry points. Existing street trees will remain, and the planting strip will be re-planted to provide a buffer between the traffic and the pedestrian uses. Two live-work units front 63rd to acknowledge and transition to the residential character of 63rd Ave.

#### C. RELATIONSHIP TO THE BLOCK

1. Corner Sites: The proposed massing provides a strong urban edge for the block. Units are oriented toward the south and west, allowing for fenestration and articulation at the corner. The existing street edge is currently not established because of the amount of on grade parking facing 15th Ave NW. The only established street edge is at the NW corner of 62nd where an existing brick building builds out to the corner.

#### D. HEIGHT, BULK, AND SCALE

1. Existing Development and Zoning: The existing 15th Ave NW corridor is under-developed. Both sides of 15th Ave NW are zoned NC and transition within 100 feet to residential zones. The adjacent residential zones are seeing a large amount of townhouse and single-family development with a typical height of 30 feet. Transitioning to the residential zones is one of the highest priorities.

2. Existing Site Features: The site was previously graded leaving it approximately 4 feet lower than adjacent developments. The preferred alternative takes advantage of the land-form by creating a buffer between the commercial and residential zones.

3. Zone Transitions: Because the site is already 4 feet lower than adjacent developments there is not a significant stepping issue. Schemes 1 & 2 place open areas very close to the residential zone; however the preferred Scheme 3 creates a larger buffer with the residential entry and an at grade courtyard. Two live-work units front onto 63rd NW, further easing the transition between commercial and residential.

4. Massing Choices: The massing choices follow guideline #D3 above. Schemes 1 & 2 place courtyards on an elevated deck that abuts the residential zone. Preferred Scheme 3 maintains a larger buffer for the residential zoning.

### CS3 ARCHITECTURAL CONTEXT AND CHARACTER

#### A. EMPHASIZING POSITIVE NEIGHBORHOOD ATTRIBUTES

2. Contemporary Design: New development in the neighborhood has been largely modern. Our proposed design will have a modern aesthetic.

4. Evolving Neighborhoods: The neighborhood is underdeveloped and the primary context is auto-oriented businesses. This is an opportunity to introduce both building form and character.

### PL1 CONNECTIVITY

#### C. OUTDOOR USES AND ACTIVITIES

1. Selecting Activity Areas: In the preferred alternative there are two primary activity areas. The first is the residential courtyard entry where tenants will see each other coming and going. The second is a large roof deck which will provide both semi-private and public areas. All activity areas are landscaped and designed to have a minimal impact on the adjoining neighbors.

### PL2 WALKABILITY

#### A. ACCESSIBILITY

1. Access for All: All entries and access points are at grade providing easy accessibility.

#### B. SAFETY AND SECURITY

1. Eyes on the Street: Large commercial areas and live-work units keep eyes on the sidewalk. The orientation of the upper level units provides for continual observation of the street.

3. Street-Level Transparency: The preferred alternative has the largest amount of commercial space creating a demand for more transparency. Storefront window systems will provide a significant amount of glazing.

#### C. WEATHER PROTECTION

1. Locations and Coverage: Overhead canopies will be provided at the commercial spaces and individual entries for the live-work spaces.

### PL3 STREET-LEVEL INTERACTION

#### A. ENTRIES

1. Design Objectives: The commercial entries are marked with wide alcoves which distinguish the entry. Signage will be added to mark the individuality of the tenants. Live-work units are scaled down and further differentiated using more residential-scale canopies and signage. The common residential entry is through a landscaped courtyard facing 63rd NW. The common courtyard will be marked by signage, landscaping and residential scale features.

#### B. RESIDENTIAL EDGES

3. Buildings with Live-Work Units: The preferred alternative has two live-work units front that 63rd NW which help the project to transition from the commercial corridor to a residential scale. The units are designed so that they could be converted to commercial in the future if desired.

#### C. RETAIL EDGES

1. Porous Edge: The preferred alternative has regularly spaced entries that establishes a rhythm of transparency. The regularity of the transparency creates a strong visual connection between the sidewalk and commercial activity.

### PL4 ACTIVE TRANSPORTATION

#### B. PLANNING AHEAD FOR BICYCLISTS

2. Bike Facilities: The preferred alternative will provide a mix of indoor bike storage for tenants and exterior bike parking for guests. We will work with SDOT to establish a bike parking area in the new planting strip area.

### DC1 PROJECT USES AND ACTIVITIES

#### A. ARRANGEMENT OF INTERIOR USES

4. Views and Connections: The majority of the units face south, east and west providing opportunities for views of the neighborhood and beyond.

#### C. PARKING AND SERVICE USES

4. Service Uses: The designated trash/recycling room is located on 63rd NW with the door facing away from the street. Because of the 15th Ave arterial, the service area must be located on 63rd and away from the intersection. The blank wall created by the room will be an opportunity to provide signage and lighting. The additional 2 foot buffer between the property and sidewalk allows for additional landscaping.

### DC2 ARCHITECTURAL CONCEPT

#### A. MASSING

2. Reducing Perceived Mass: The interior configuration gives natural places to break the massing into smaller portions. Additionally, the ground floor commercial and overhead canopies create a break between the lower and upper portions of the building. Compared to Scheme B, the preferred alternative reduces the perceived mass of the building.

#### B. ARCHITECTURAL AND FAÇADE COMPOSITION

2. Blank Walls: The preferred scheme substantially limits the amount of blank walls. Schemes A & B have more north facing blank walls. Scheme C breaks up the blank walls by orienting several units toward the north. At grade, blank walls are limited to the trash enclosure which will be treated with landscaping and signage.

### DC3 OPEN SPACE CONCEPT

#### B. OPEN SPACE USES AND ACTIVITIES

1. Meeting user Needs: The preferred alternative has multiple types of open spaces. At grade there is the entry courtyard providing for tenant interaction at the building entry. Also at grade are units that enter directly off of grade. Many units have private patios and all residents share a landscaped roof deck which is designed to provide both semi-private and shared open space.

### DC4 EXTERIOR ELEMENTS AND FINISHES

A. BUILDING MATERIALS: We envision a simple material palette including fiber cement panel and metal. The materials will be used to differentiate the massing and reduce the perceived mass of the building.

B. SIGNAGE & C. LIGHTING: Signage and lighting will be designed to be integrated into the street-front experience. Signage will be planned for at the commercial entries. Landscape lighting will be provided at the courtyard.

D. TREES, LANDSCAPE & HARDSCAPE MATERIALS: Landscape will be chosen using a modern palette that compliments the building and provides year round interest. Hardscape for the at-grade courtyard will be pervious.

# SCHEME A: CODE COMPLIANT

- 0 SF COMMERCIAL/RESTAURANT
- 7 LIVE-WORK UNITS
- 2 STUDIO APARTMENTS
- 51 EFFICIENCY STUDIOS

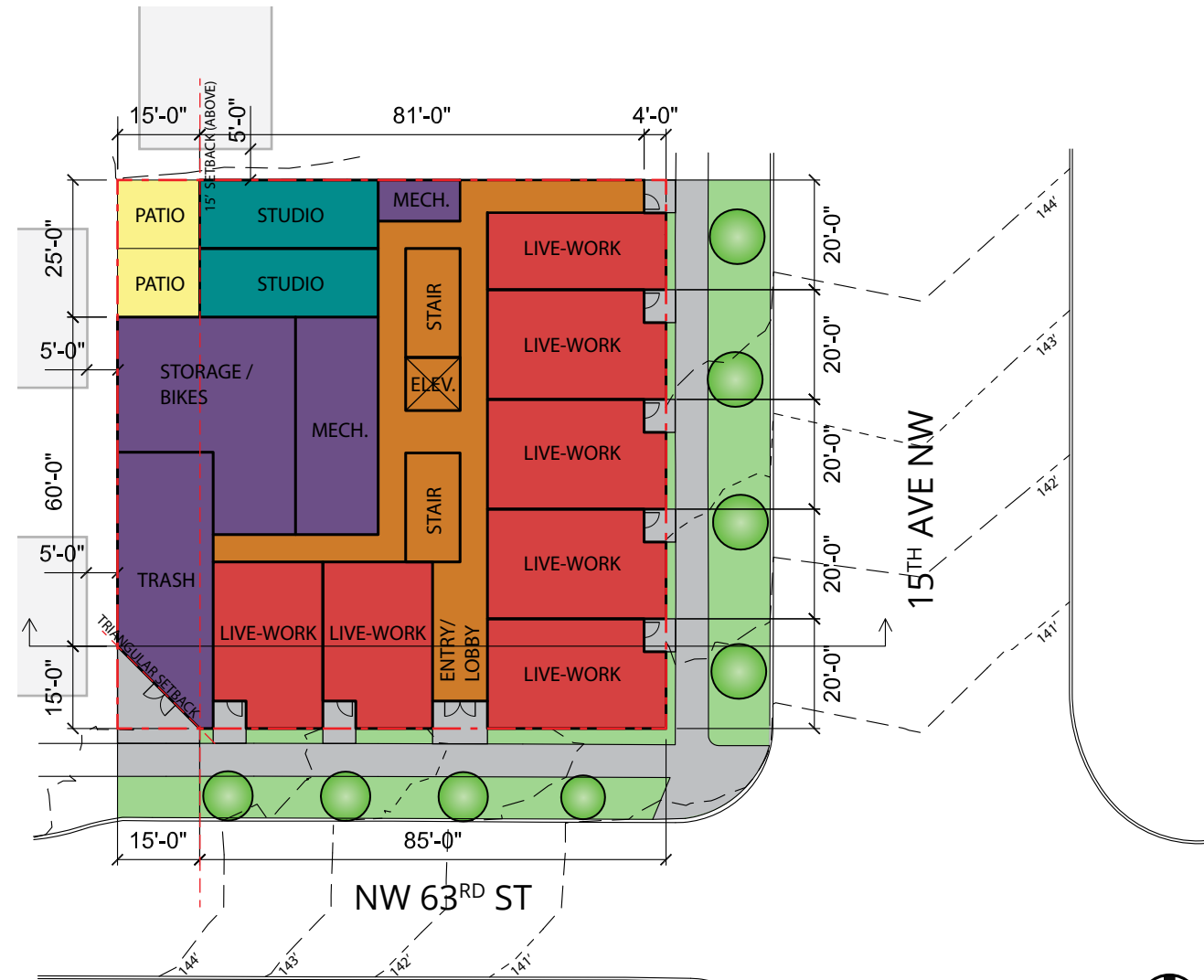
## POSITIVE

- MAXIMIZES BUILDABLE AREA
- SIMPLE BUT INEFFICIENT LAYOUT
- MODULATION ALONG 63RD @ L2-L4
- NO DEPARTURES REQUIRED
- ROOF DECK LOCATED AWAY FROM RESIDENTIAL ZONES

## NEGATIVE:

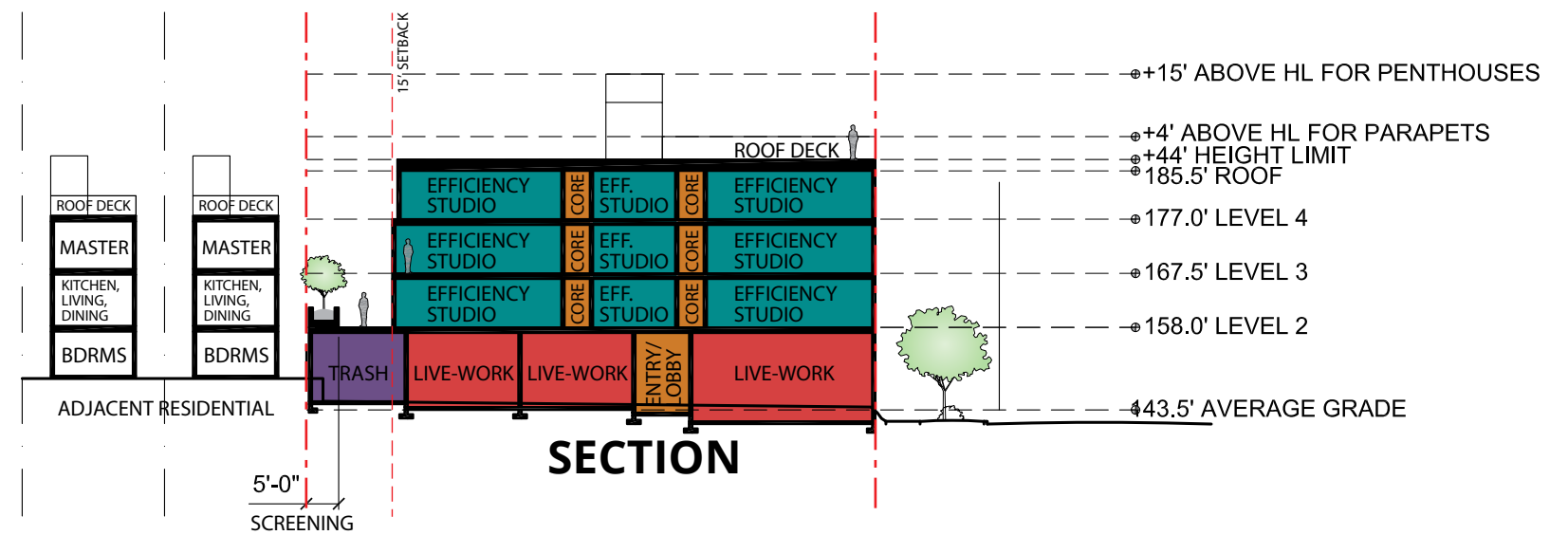
- NO GROUND FLOOR COMMERCIAL
- PRIVATE PATIO'S ABUTTING RESIDENTIAL ZONE
- UNITS NOT ORIENTED TOWARD VIEWS
- 8 UNITS PER FLOOR FACE RESIDENTIAL ZONE
- BLANK WALLS FACING ZERO-LOT LINE CONDITION TO THE NORTH

- COMMERCIAL
- LIVE-WORK
- BLDG. SERVICE
- RESIDENTIAL
- CIRCULATION / CORE
- PRIVATE BALCONY
- COMMON AMENITY

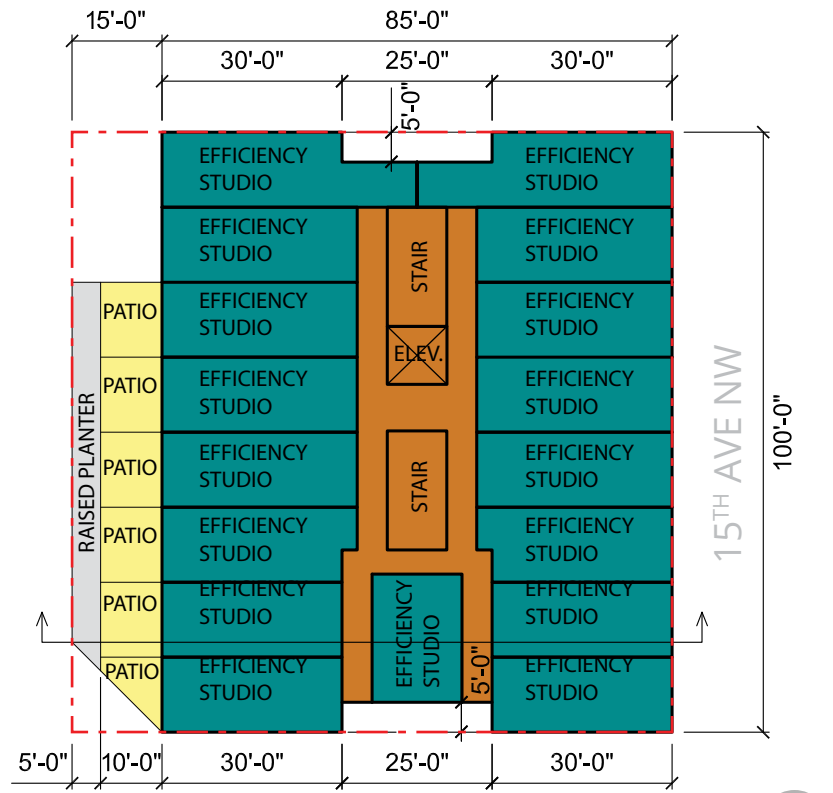


**SITE / LEVEL 1**

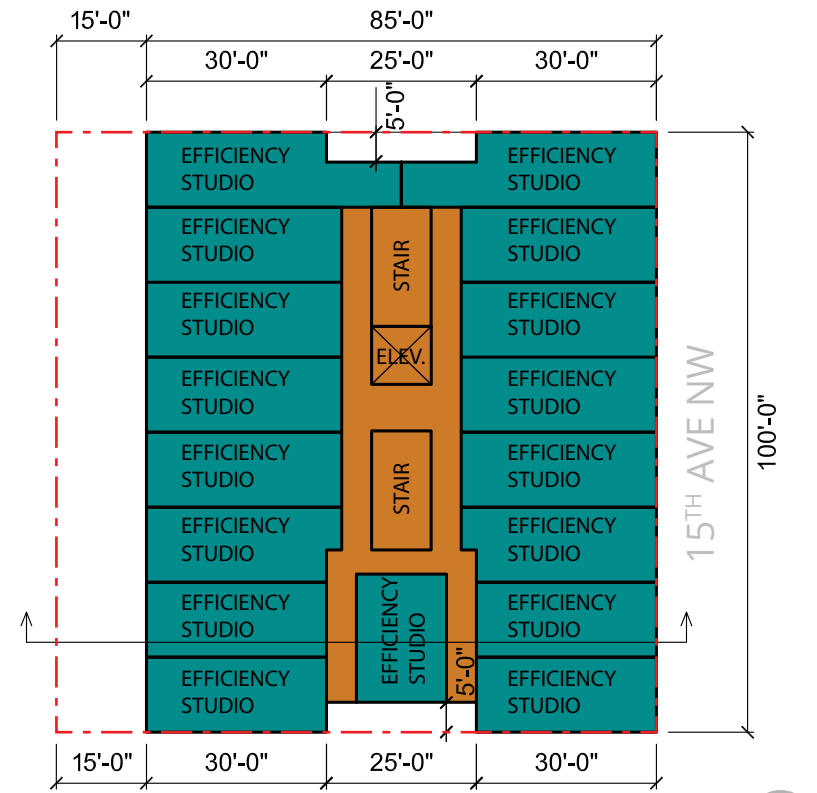
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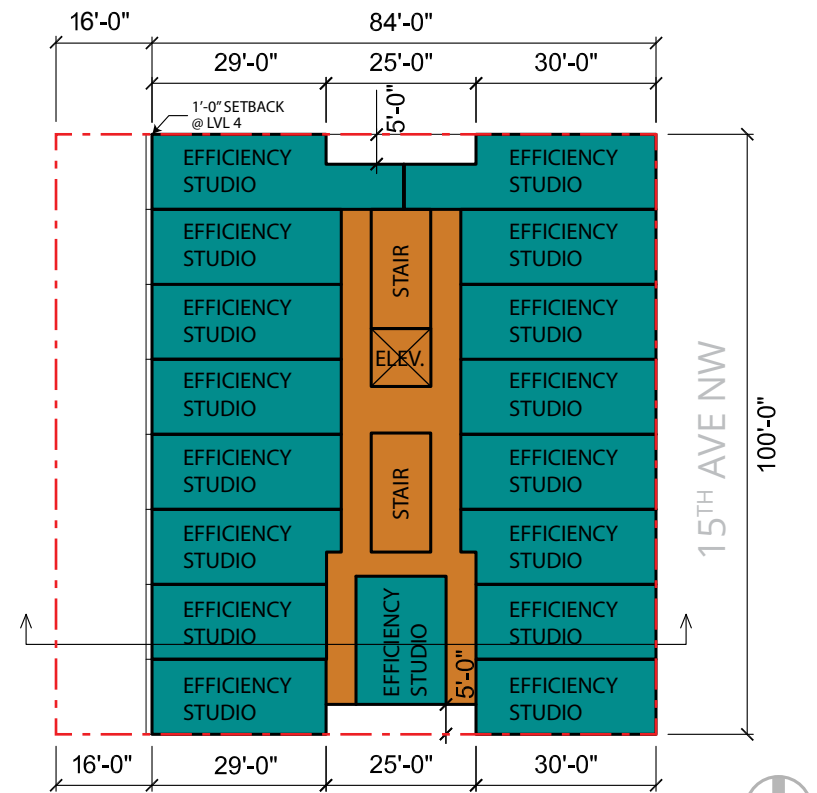
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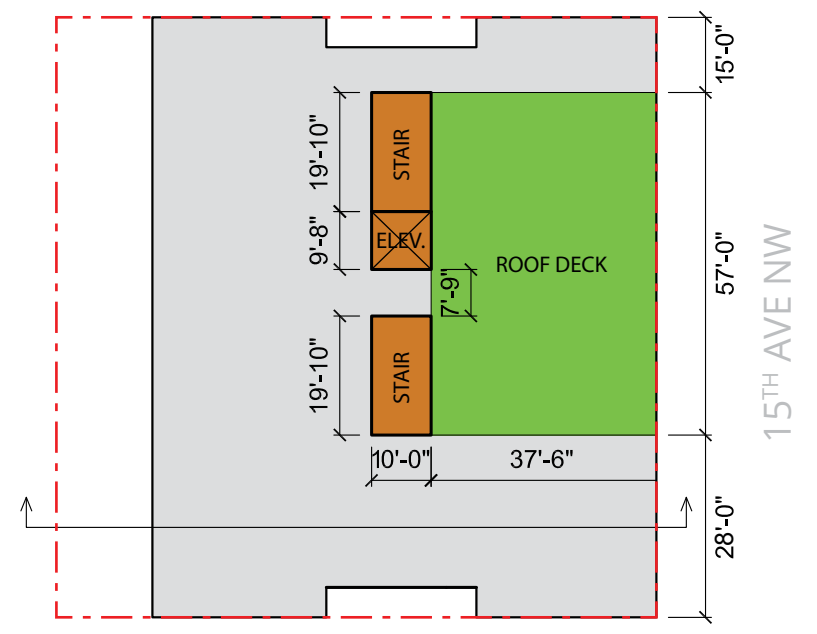
NW 63<sup>RD</sup> ST  
**LEVEL 2**



NW 63<sup>RD</sup> ST  
**LEVEL 3**



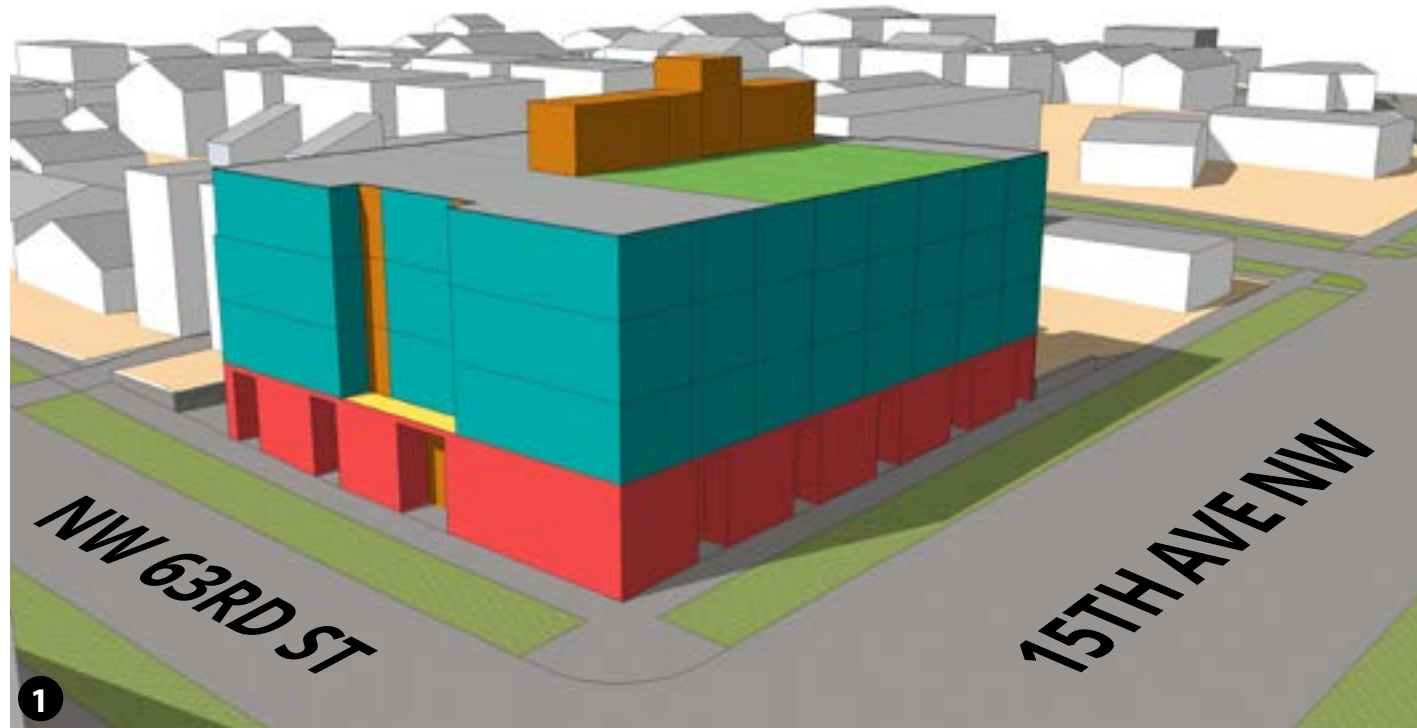
NW 63<sup>RD</sup> ST  
**LEVEL 4**



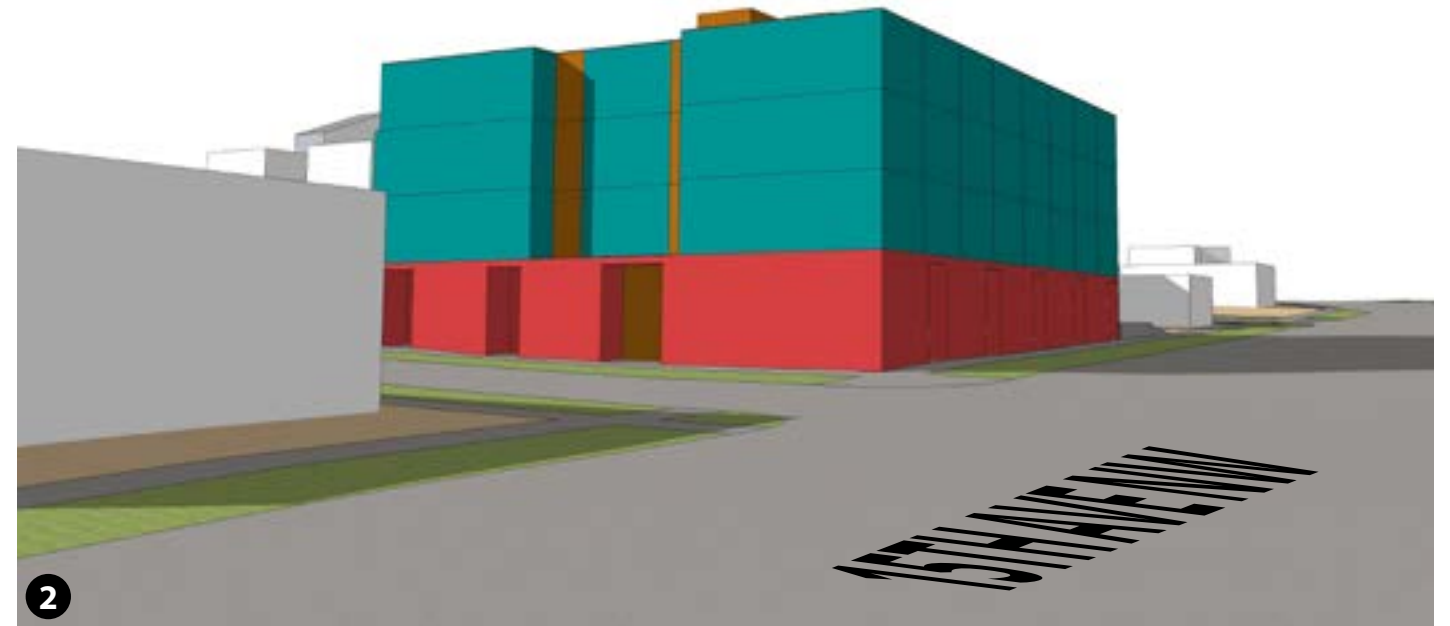
NW 63<sup>RD</sup> ST  
**ROOF**



**SCHEME A: CODE COMPLIANT**



1 BIRDSEYE PERSPECTIVE ABOVE CORNER OF NW 63RD ST & 15TH AVE NW, LOOKING NW



2 15TH AVE NW, LOOKING NW



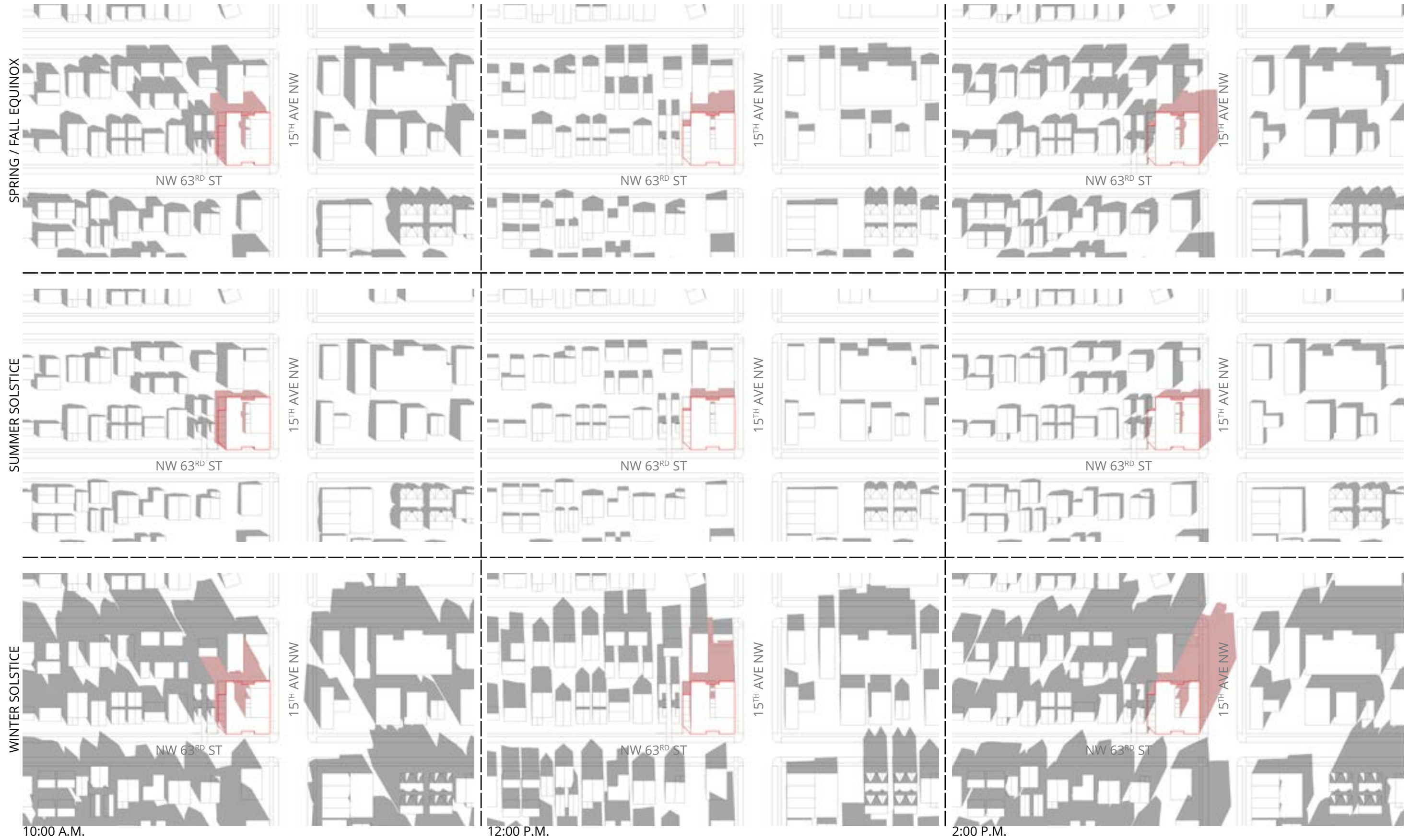
3 BIRDSEYE PERSPECTIVE ABOVE NW 63RD ST, LOOKING NE



4 BIRDSEYE PERSPECTIVE ABOVE CORNER OF NW 64TH ST & 15TH AVE NW, LOOKING SW



# SUN PATH / SHADOW STUDY: SCHEME A



# SCHEME B: L-SHAPED

1,682 SF COMMERCIAL/RESTAURANT

4 LIVE-WORK UNITS

11 STUDIO APARTMENTS

43 EFFICIENCY STUDIOS

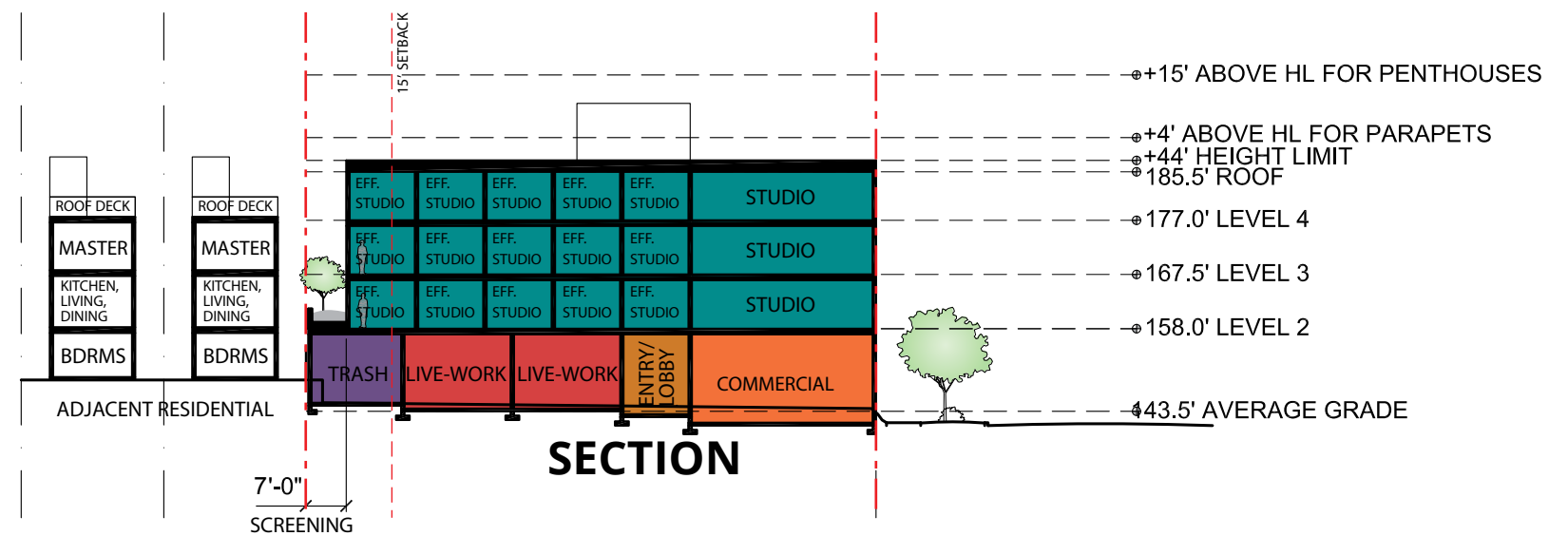
## POSITIVE

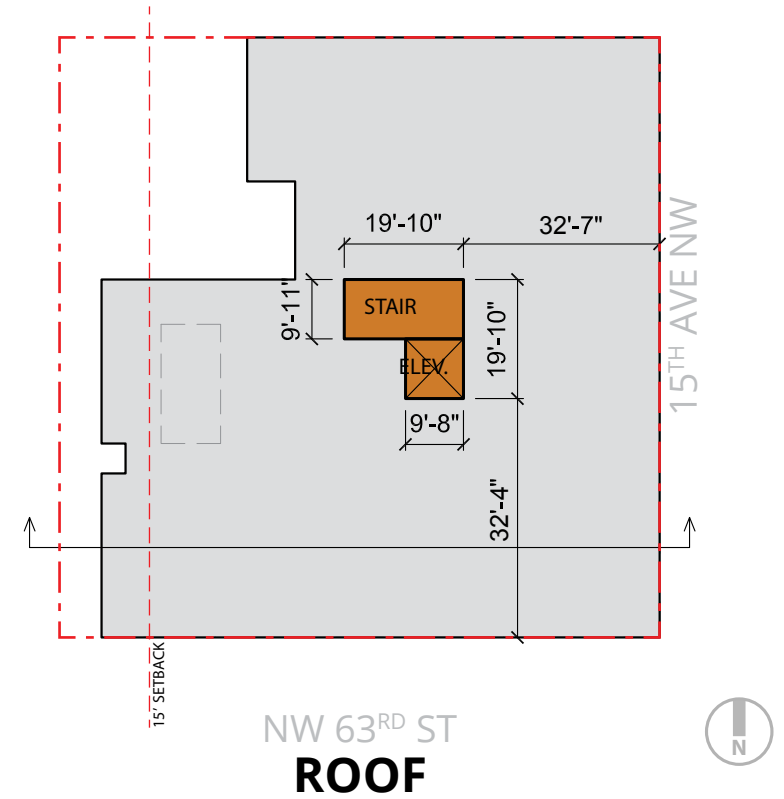
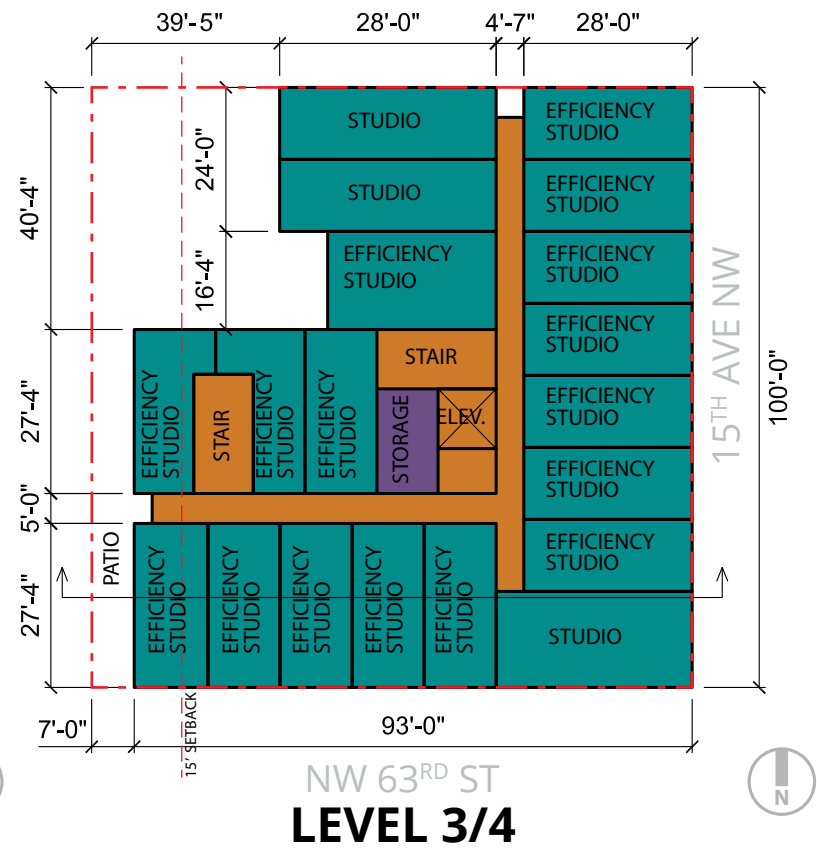
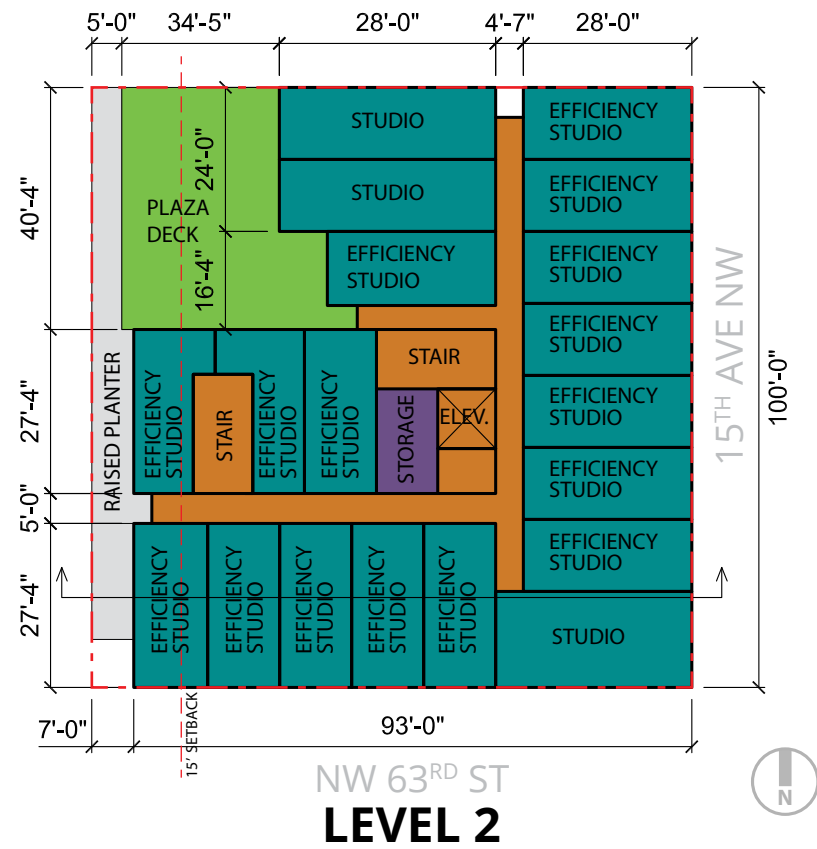
- MAXIMIZES BUILDABLE AREA
- SUBSTANTIAL GROUND FLOOR COMMERCIAL
- MAJORITY OF UNITS ORIENTED TOWARD VIEWS
- ONLY 5 UNITS PER FLOOR FACE RESIDENTIAL ZONE (INCLUDING 2 INDIRECTLY)

## NEGATIVE

- COMMON PLAZA DECK @ L2 ABUTTING RESIDENTIAL ZONE
- NO ROOF DECK
- LARGE DEPARTURES REQUIRED FOR REAR SETBACK ABUTTING RESIDENTIAL ZONE
- BLANK WALLS FACING ZERO-LOT LINE CONDITION TO THE NORTH

- COMMERCIAL
- LIVE-WORK
- BLDG. SERVICE
- RESIDENTIAL
- CIRCULATION / CORE
- PRIVATE BALCONY
- COMMON AMENITY





**DEPARTURES**

1) SMC 23.47A.014.B.1: SETBACK FOR LOTS ABUTTING RESIDENTIAL ZONES

**REDUCE THE 15'X15' REAR CORNER SETBACK AT LEVEL 1.**

- **CS2/D.3 ZONE TRANSITIONS:** THE REDUCED SETBACK CREATES A RECESSED ALCOVE WHICH BUFFERS AND MITIGATES THE TRANSITION TO THE RESIDENTIAL ZONE BY MATCHING THE FRONT SETBACK OF THE ADJACENT DEVELOPMENT.
- **DC1/C.4 PARKING AND SERVICE USES:** THE SETBACK REDUCTION ALLOWS THE BUILDING EDGE TO BE PARALLEL TO THE SIDEWALK, KEEPING THE TRASH ROOM DOORS AWAY FROM THE ADJACENT RESIDENTIAL USE AND PULLED BACK FROM THE PROPERTY LINE.
- THE BUILDING FACADE IS ALREADY RECESSED 7' FROM THE PROPERTY LINE, REDUCING THE MAGNITUDE OF THE DEPARTURE REQUEST.

2) SMC 23.47A.014.B.3: REAR SETBACK FOR LOTS ABUTTING RESIDENTIAL ZONES.

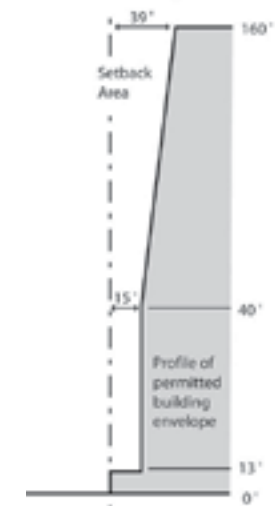
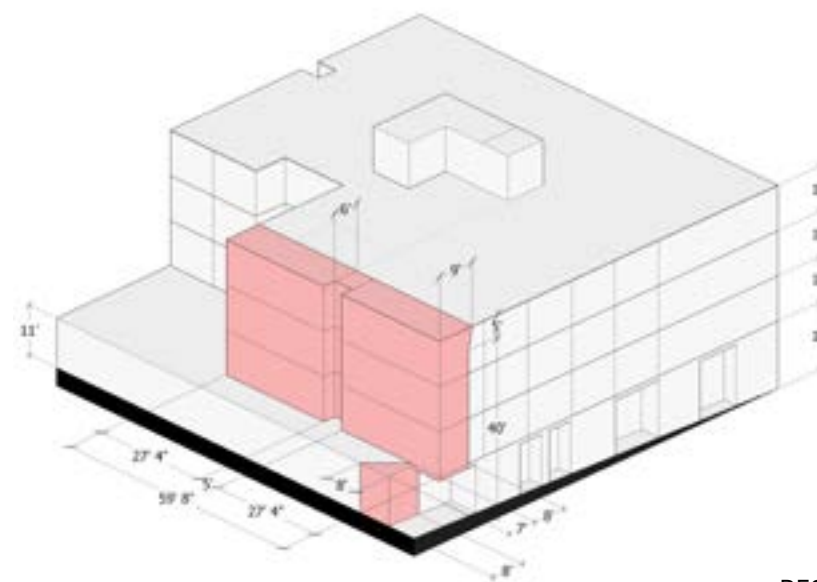
**REDUCE THE REAR SETBACK AT LEVELS 2-4 FOR A 59'-8" SECTION OF THE BUILDING FROM 15' TO A 7' MINIMUM.**

- **CS2/B.2 ADJACENT SITES, STREETS, AND OPEN SPACES:** THE REDUCED REAR SETBACK ESTABLISHES A STRONG STREET EDGE AS THE PROJECT TURNS THE CORNER FROM 15TH NW.
- **CS2/D.3 ZONE TRANSITIONS:** THE RESULTING 7' SETBACK ESTABLISHES A SIDE YARD PATTERN THAT IS SIMILAR TO OTHER LOW RISE ZONE TRANSITIONS.
- THE REQUESTED 7' SETBACK OCCURS FOR A FACADE LENGTH OF 59'-8". THE REMAINING PORTION OF THE BUILDING IS SETBACK 39'-5" MINIMUM, PROVIDING AN AVERAGE SETBACK OF 17'-6".

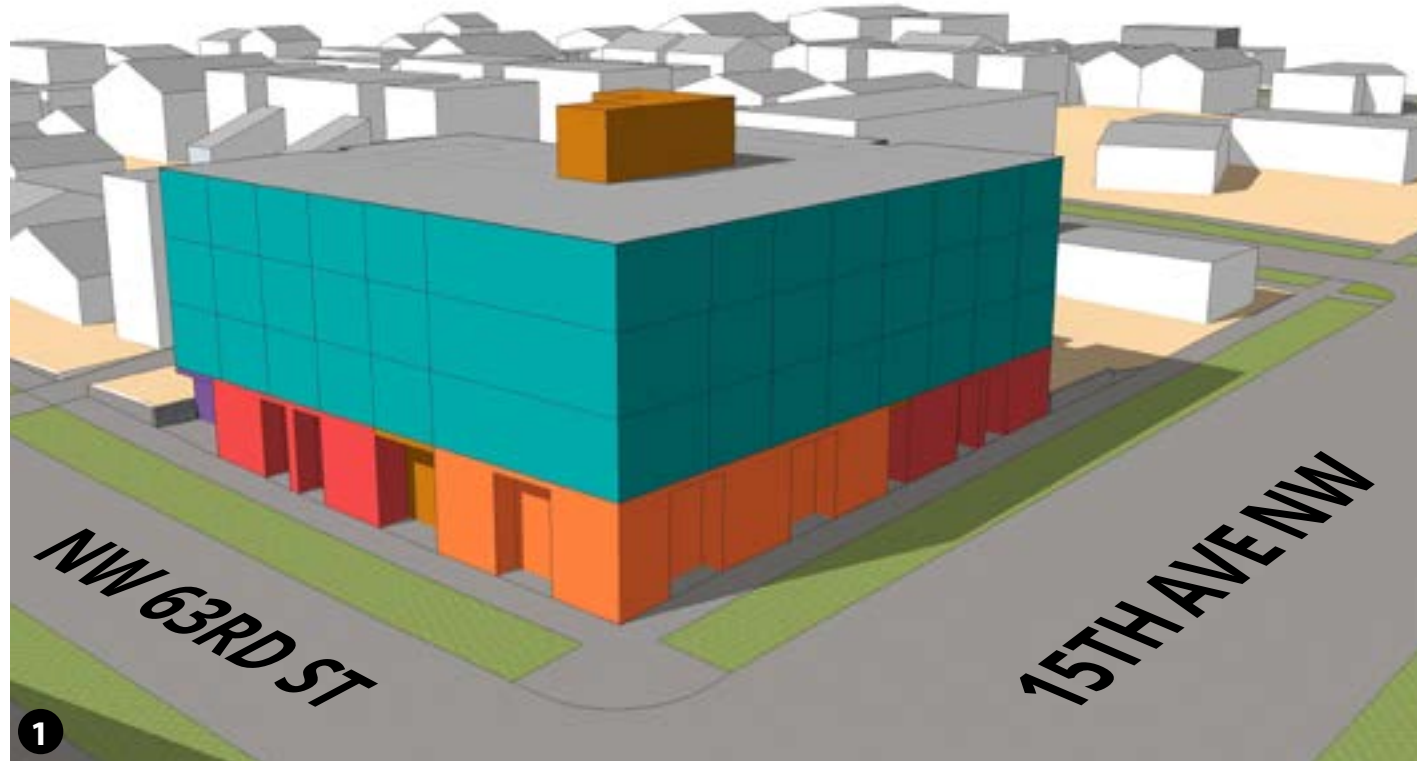
**DEPARTURE DIAGRAM**

Building envelope encroaching required setback

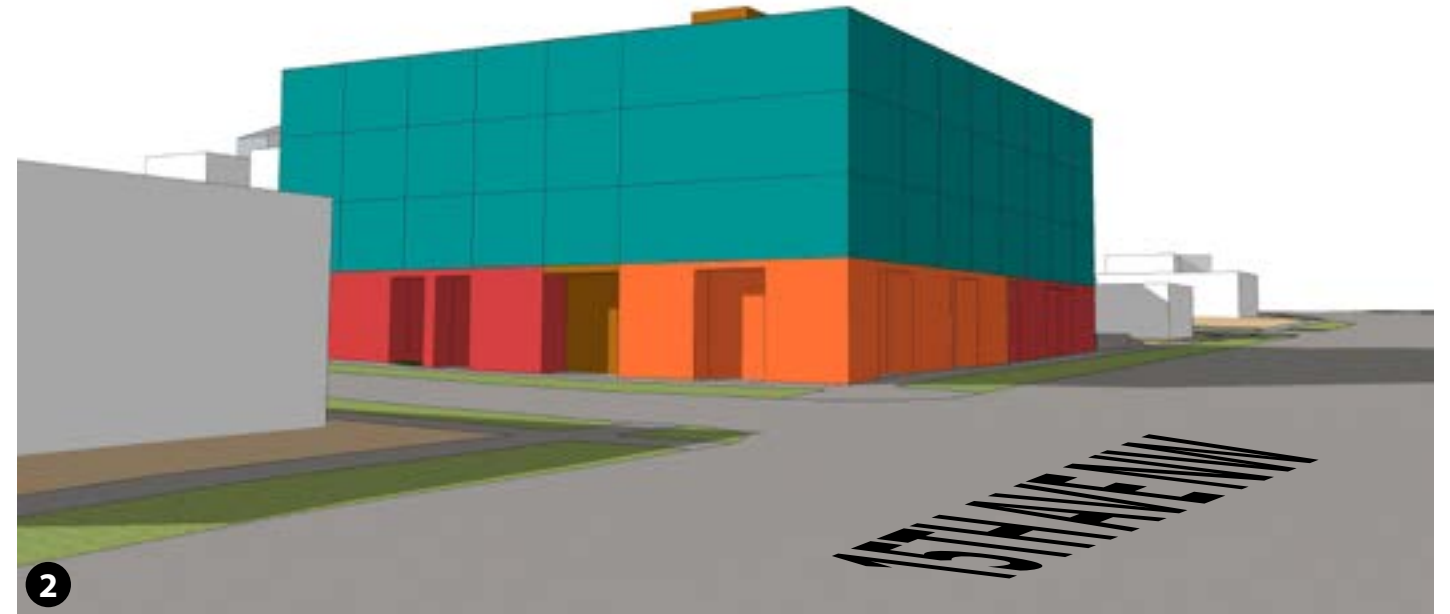
23.47A.014, EXHIBIT C  
Setbacks for structures with more than one residential unit along a rear or side lot line abutting a residential-zoned lot



**SCHEME B: L-SHAPED**



BIRDSEYE PERSPECTIVE ABOVE CORNER OF NW 63RD ST & 15TH AVE NW, LOOKING NW



15TH AVE NW, LOOKING NW

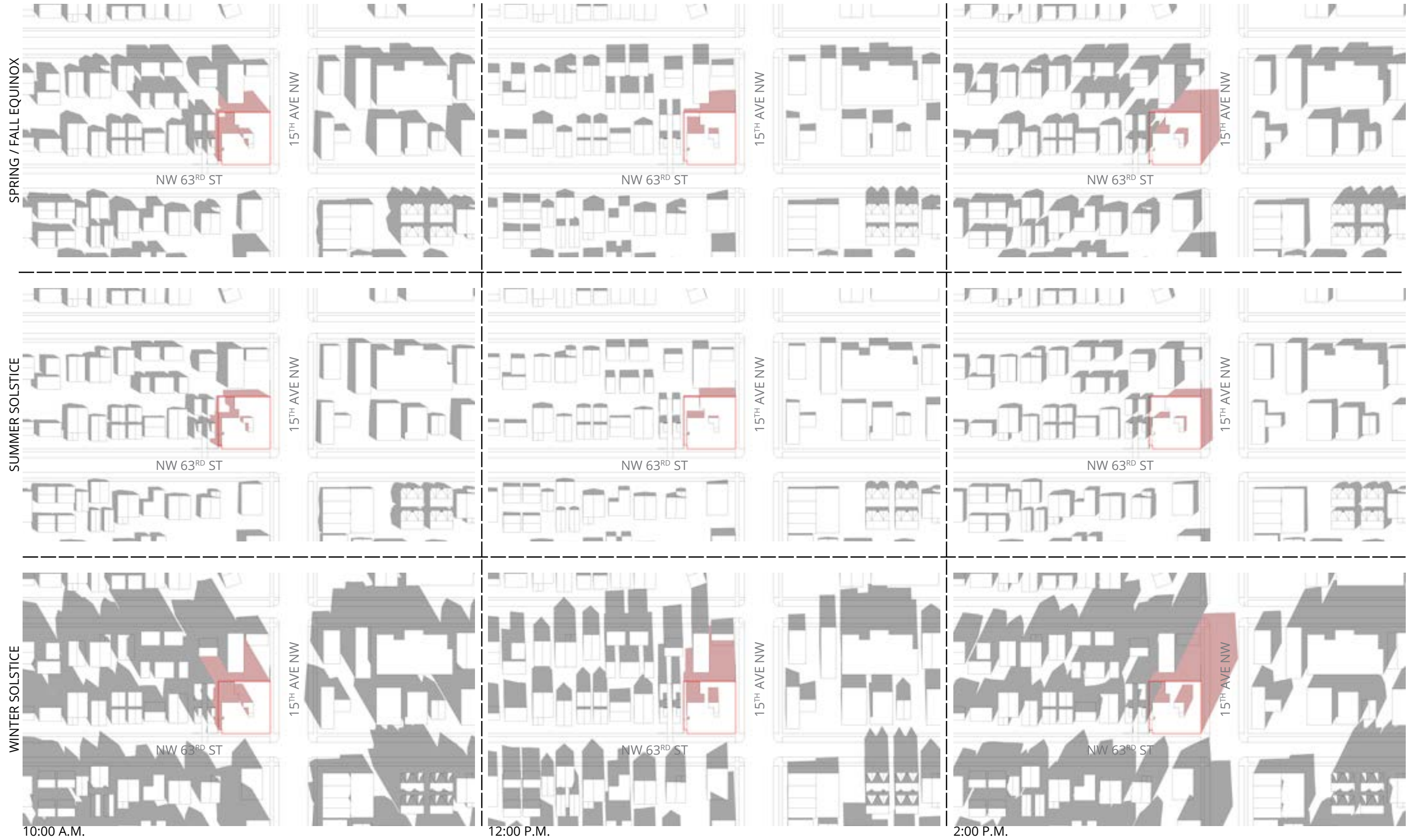


BIRDSEYE PERSPECTIVE ABOVE NW 63RD ST, LOOKING NE



BIRDSEYE PERSPECTIVE ABOVE CORNER OF NW 64TH ST & 15TH AVE NW, LOOKING SW

# SUN PATH / SHADOW STUDY: SCHEME B



# SCHEME C: PREFERRED

3,011 SF COMMERCIAL/RESTAURANT

(2 OR 3 POTENTIAL SPACES)

2 LIVE-WORK UNITS

8 STUDIO APARTMENTS

45 EFFICIENCY STUDIOS

## POSITIVE

- MAXIMIZES BUILDABLE AREA
- TWO SUBSTANTIAL GROUND FLOOR COMMERCIAL SPACES, DIVISIBLE TO THREE SPACES
- 2 LIVE WORK UNITS HELP THE TRANSITION TO THE RESIDENTIAL ZONE
- MAJORITY OF UNITS ORIENTED TOWARD VIEWS
- ONLY 6 UNITS PER FLOOR FACE RESIDENTIAL ZONE (INCLUDING 1 INDIRECTLY)
- 15' REAR SETBACK IS CARRIED DOWN TO THE GROUND RATHER THAN BEGINNING AT 13'
- RESIDENTIAL SCALE COURTYARD ENTRY ALONG WEST PROPERTY LINE ELIMINATES DECKS 5' FROM NEIGHBORING STRUCTURES
- ROOF DECK LOCATED AWAY FROM RESIDENTIAL ZONES

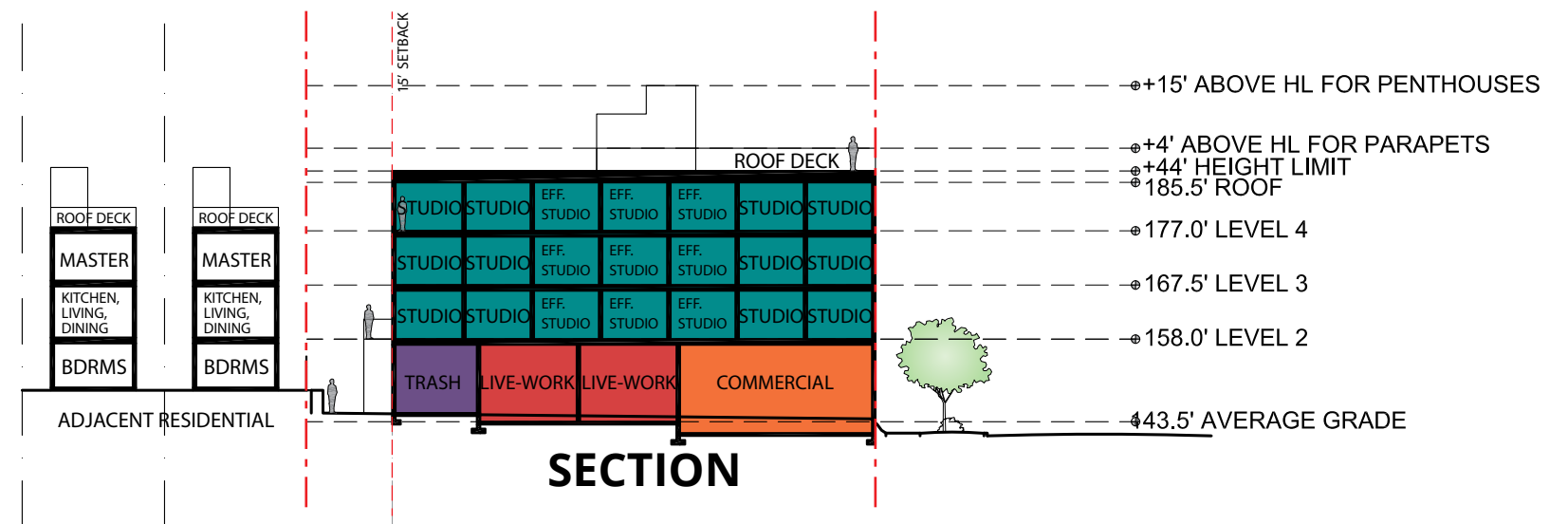
## NEGATIVE

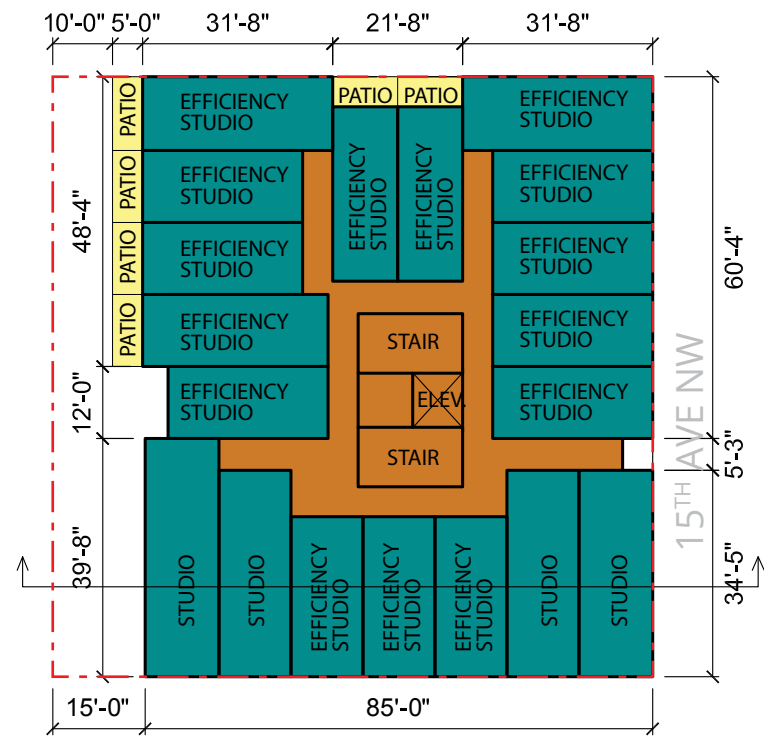
- DEPARTURE REQUIRED FOR UPPER LEVEL
- BLANK WALLS FACING ZERO-LOT LINE CONDITION TO THE NORTH (LEAST OF THE THREE OPTIONS)

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<span style="display:inline-block; width:15px; height:15px; background-color:red; border:1px solid black;"></span> LIVE-WORK
<span style="display:inline-block; width:15px; height:15px; background-color:purple; border:1px solid black;"></span> BLDG. SERVICE
<span style="display:inline-block; width:15px; height:15px; background-color:teal; border:1px solid black;"></span> RESIDENTIAL
<span style="display:inline-block; width:15px; height:15px; background-color:orange; border:1px solid black;"></span> CIRCULATION / CORE
<span style="display:inline-block; width:15px; height:15px; background-color:yellow; border:1px solid black;"></span> PRIVATE BALCONY
<span style="display:inline-block; width:15px; height:15px; background-color:green; border:1px solid black;"></span> COMMON AMENITY

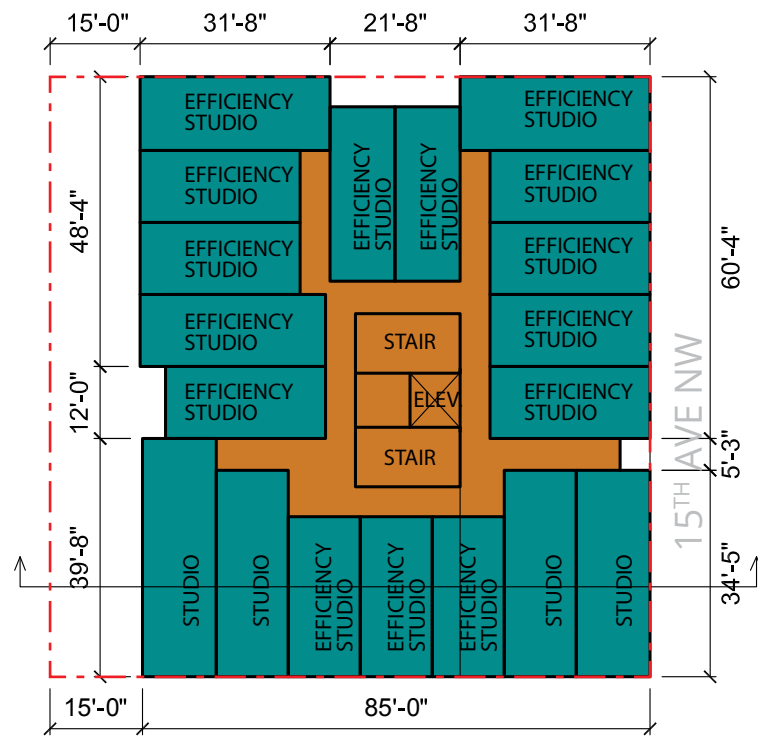


SCALE: 1/32" = 1'-0"

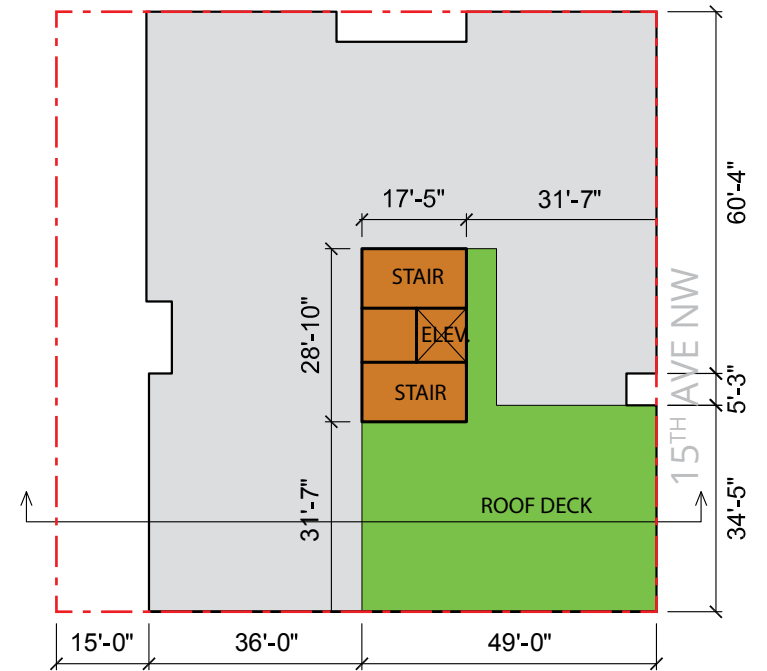




NW 63<sup>RD</sup> ST  
**LEVEL 2**



NW 63<sup>RD</sup> ST  
**LEVEL 3 / 4**



NW 63<sup>RD</sup> ST  
**ROOF**

## DEPARTURES

1) SMC 23.47A.014.B.3: REAR SETBACK ABUTTING RESIDENTIAL ZONES

**PROVIDE AN OVERALL 15' SETBACK TO AVOID THE 12" SETBACK AT LEVEL 4.**

- **CS2/D.3 ZONE TRANSITIONS:** THE REDUCED SETBACK ABOVE 40' ALLOWS THE MASSING TO LOGICALLY TERMINATE AT THE TOP OF THE BUILDING. BECAUSE THE 15' SETBACK IS MAINTAINED AT LEVEL 1, A STRONGER BUFFER TRANSITION IS ESTABLISHED FOR THE RESIDENTIAL ZONE.

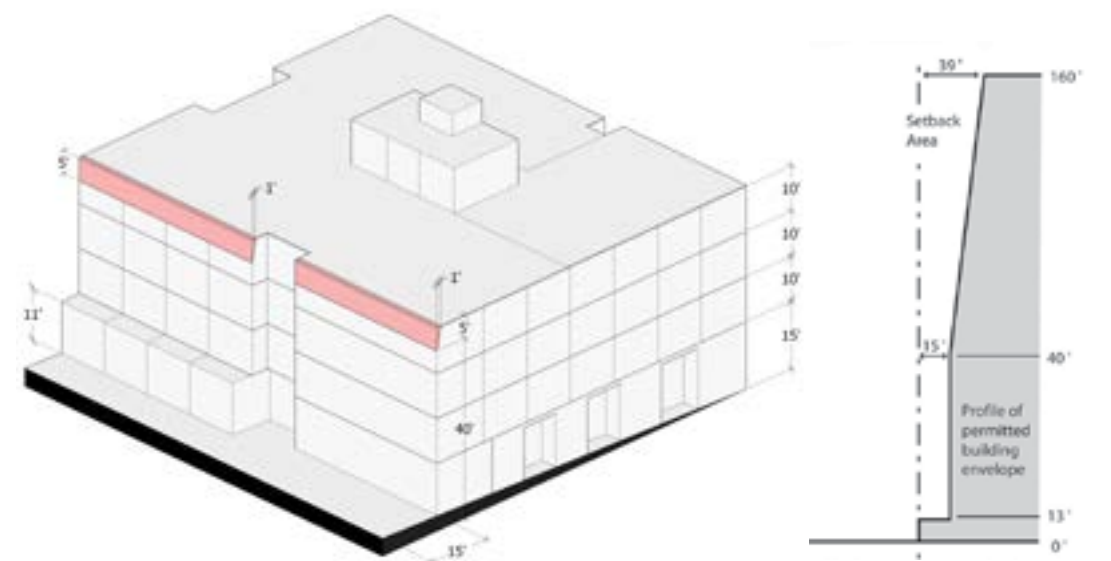
- **CS2/D.4 MASSING CHOICES:** THE 15' SETBACK IS CONSISTENT FROM GROUND TO SKY, CREATING A STRONG MASSING FORM AND CLEAR EDGE FOR THE BUILDING.

- THE AVERAGE UPPER LEVEL SETBACK IS 15'-6". A 12' WIDE RECESS (4' DEEP) BREAKS DOWN THE LENGTH OF THE FACADE AND REDUCES THE IMPACT OF THE REDUCED UPPER LEVEL SETBACK. THE SOUTH PORTION OF LEVEL 1 MAINTAINS THE 15' SETBACK FOR ALL LEVELS, OFFSETTING ANY PERCEIVED BULK OF THE 12" SETBACK REDUCTION AT THE UPPER LEVEL.

## DEPARTURE DIAGRAM

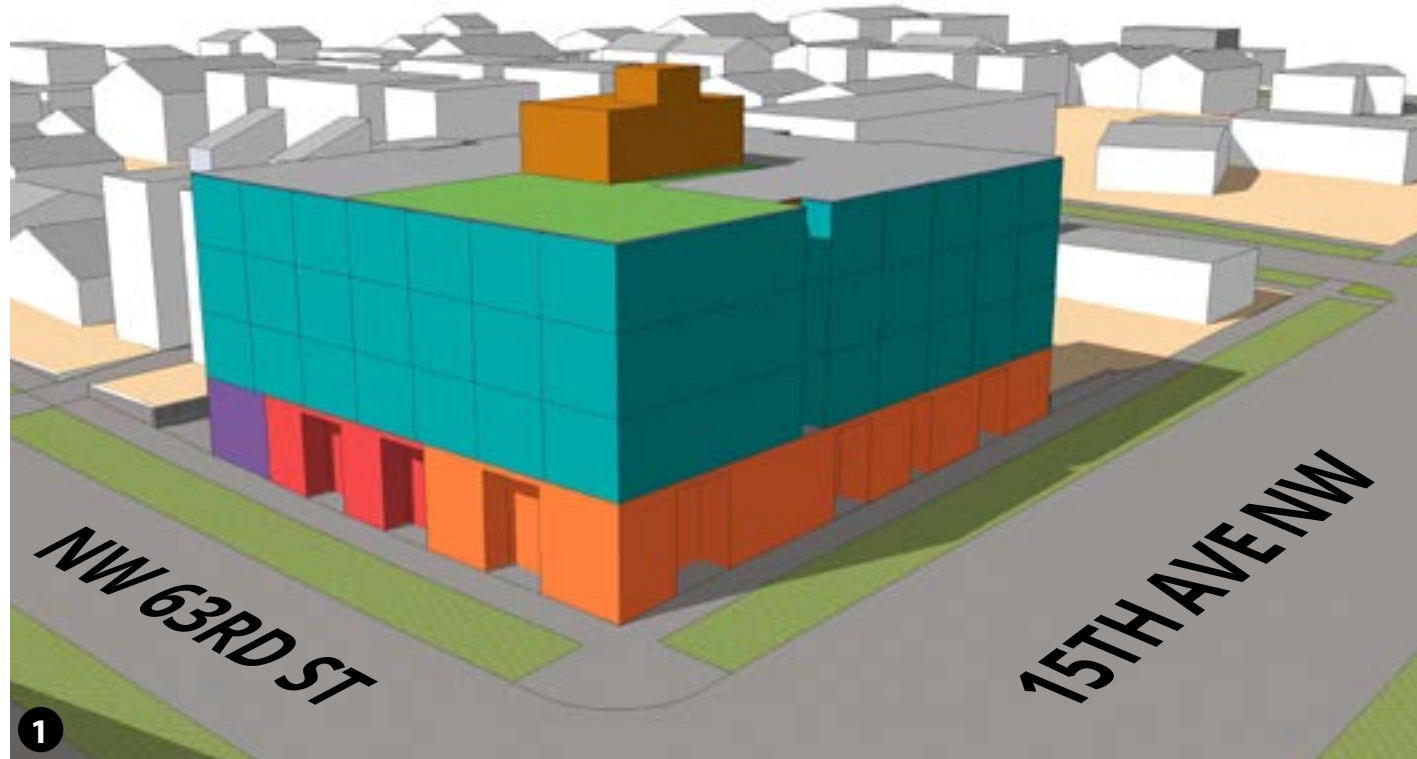
 Building envelope encroaching required setback

23.47A.014, EXHIBIT C  
Setbacks for structures with more than one residential unit along a rear or side lot line abutting a residential-zoned lot

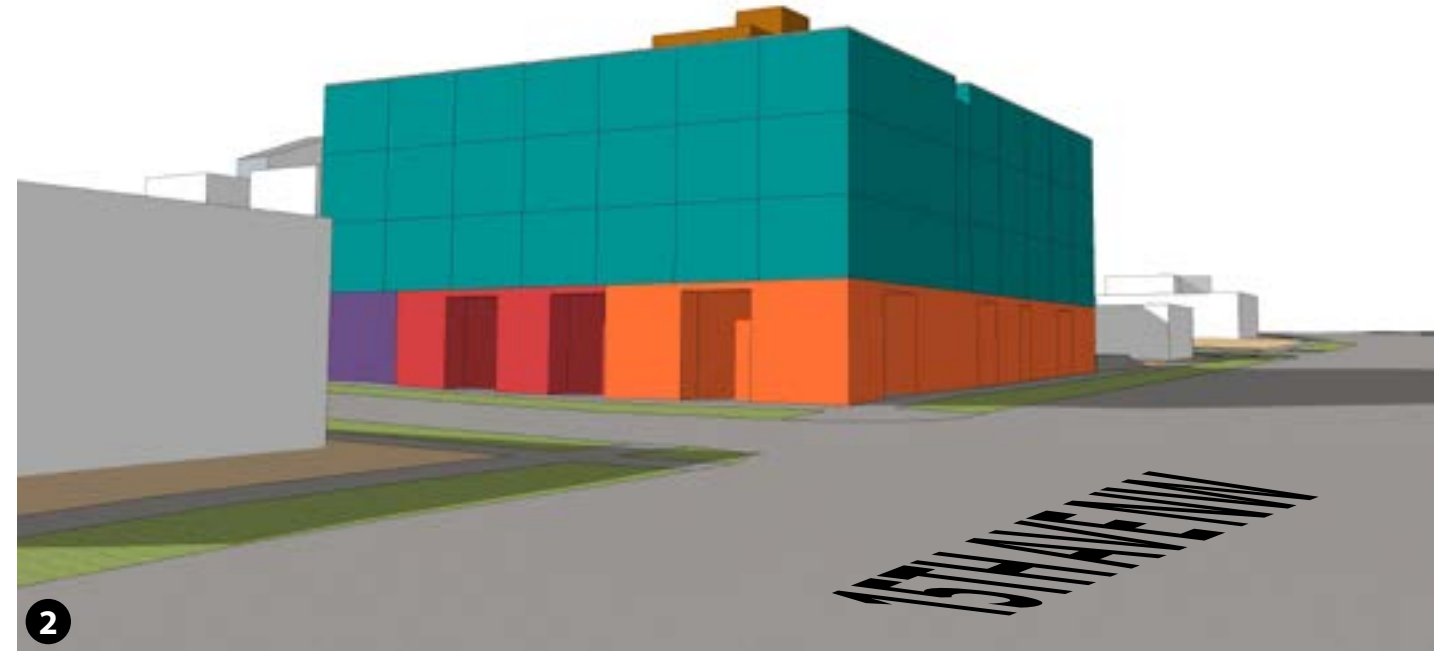


RESIDENTIAL COMMERCIAL

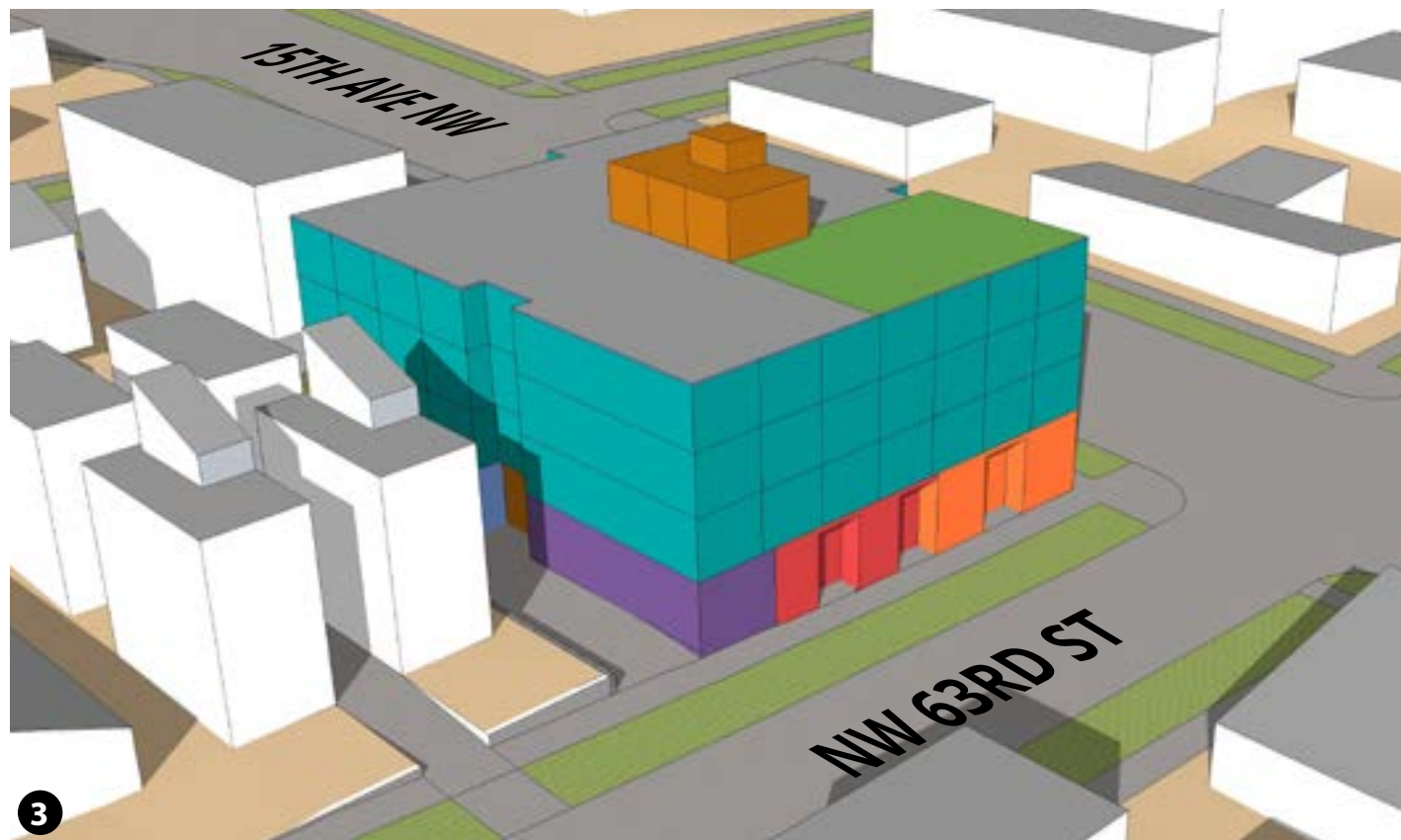
**SCHEME C: PREFERRED**



BIRDSEYE PERSPECTIVE ABOVE CORNER OF NW 63RD ST & 15TH AVE NW, LOOKING NW



15TH AVE NW, LOOKING NW



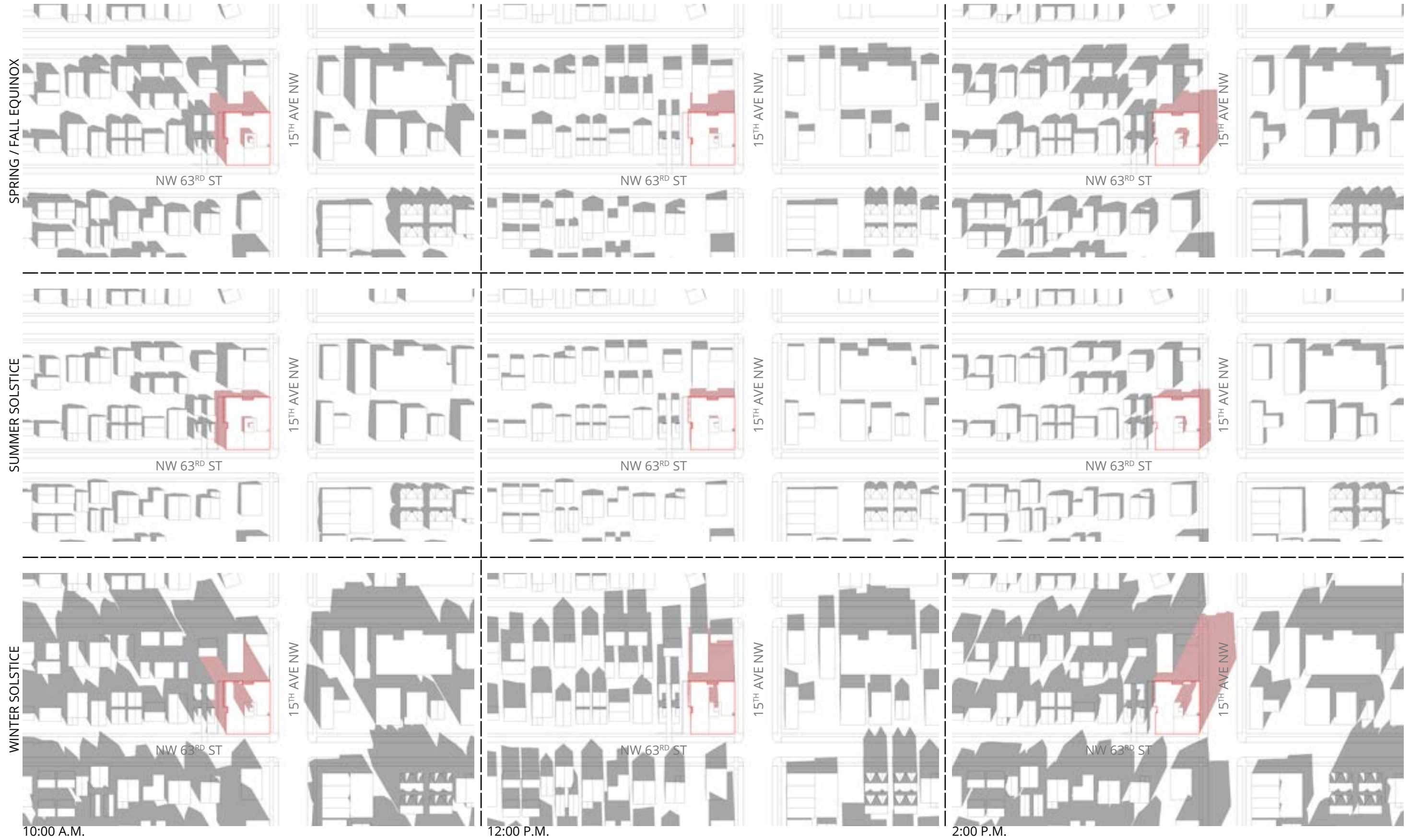
BIRDSEYE PERSPECTIVE ABOVE NW 63RD ST, LOOKING NE



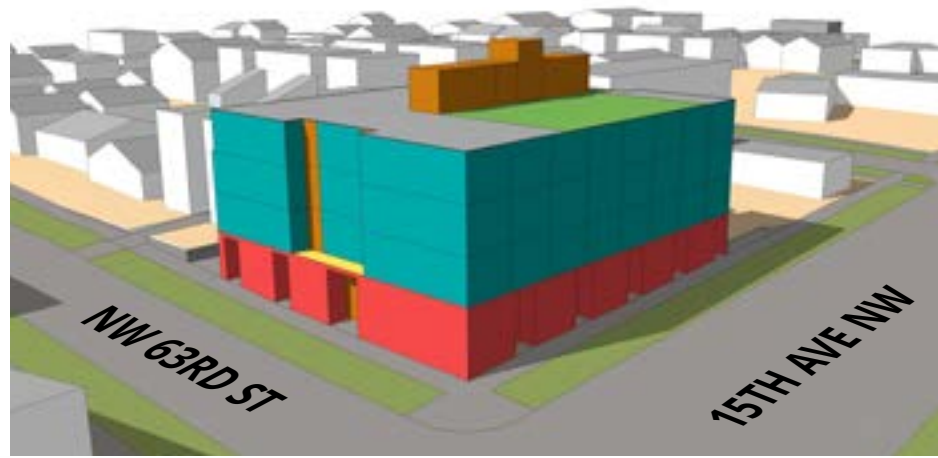
BIRDSEYE PERSPECTIVE ABOVE CORNER OF NW 64TH ST & 15TH AVE NW, LOOKING SW



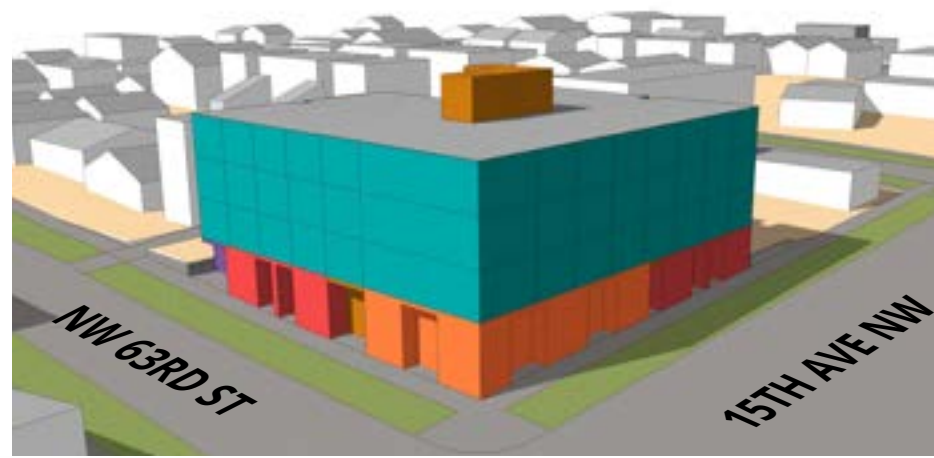
# SUN PATH / SHADOW STUDY: SCHEME C



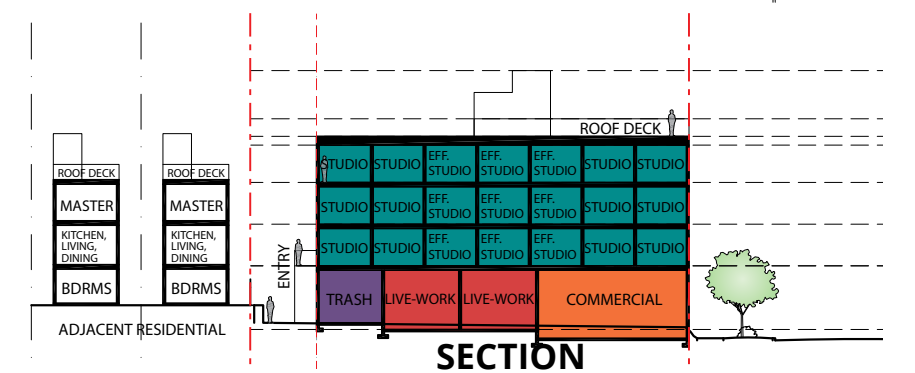
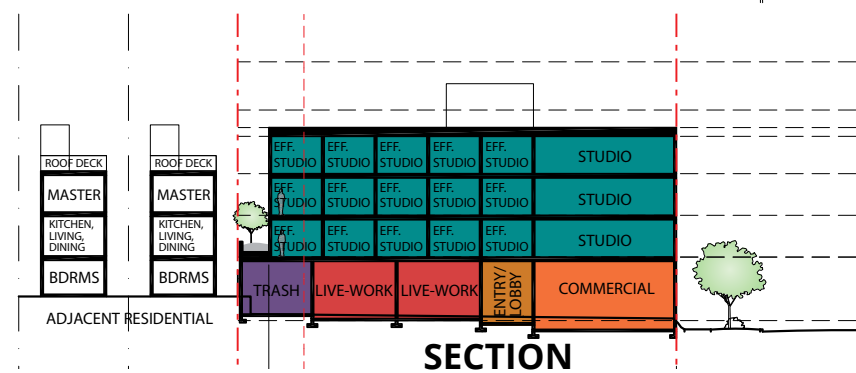
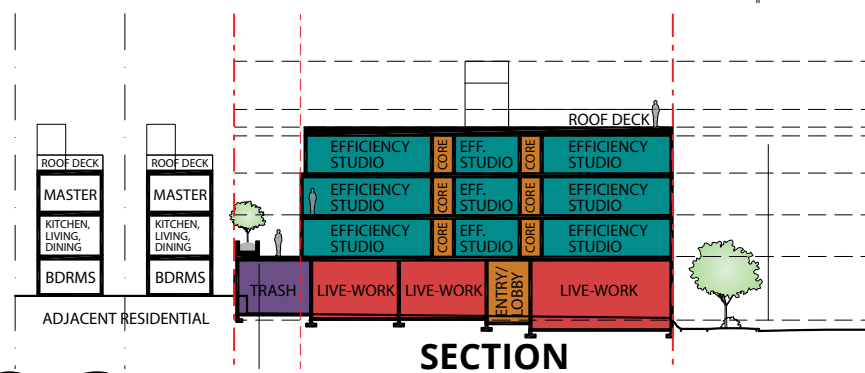
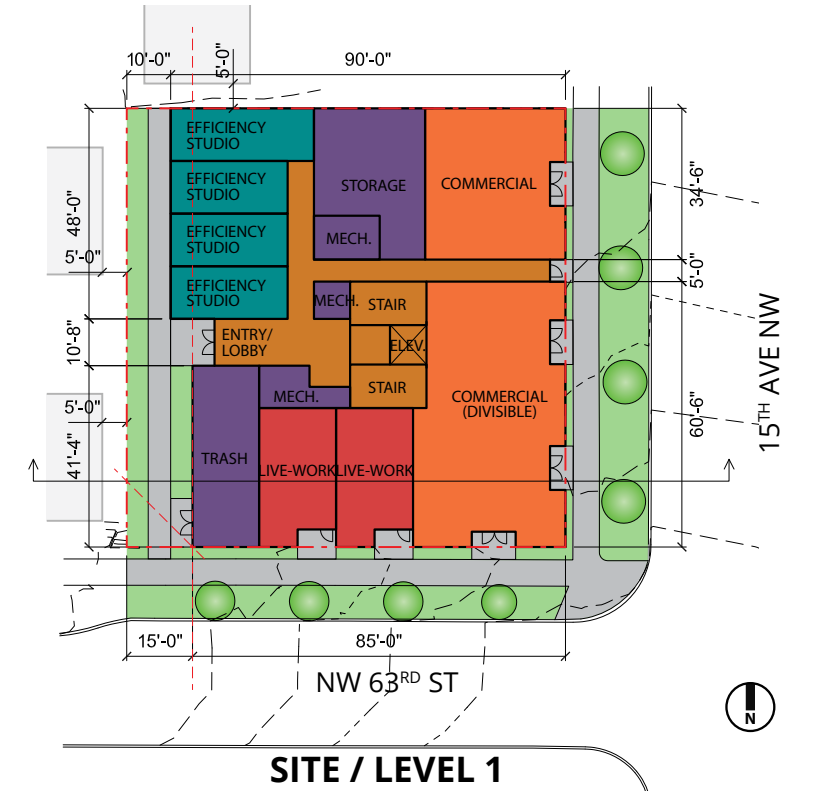
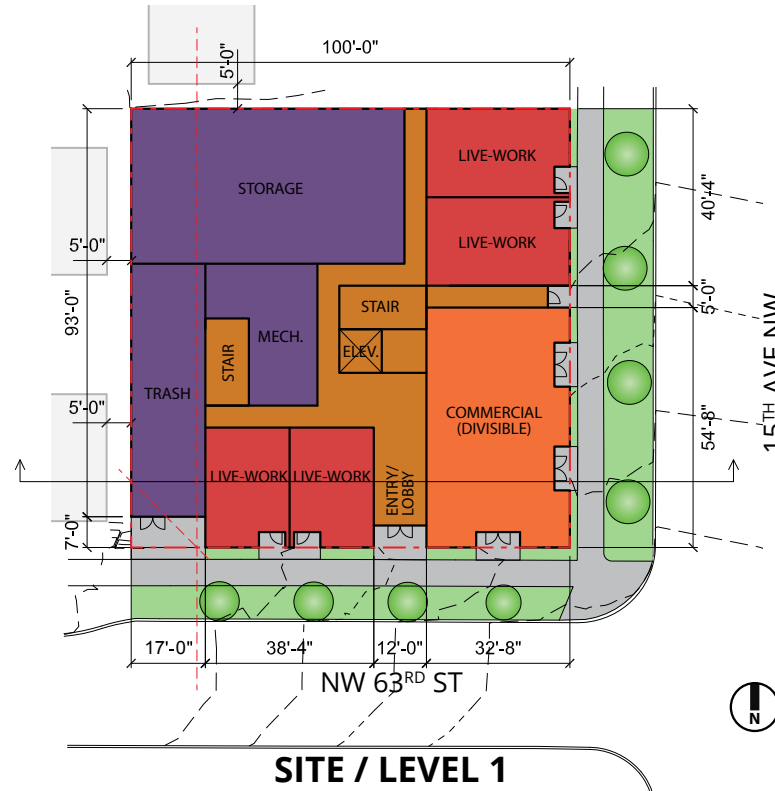
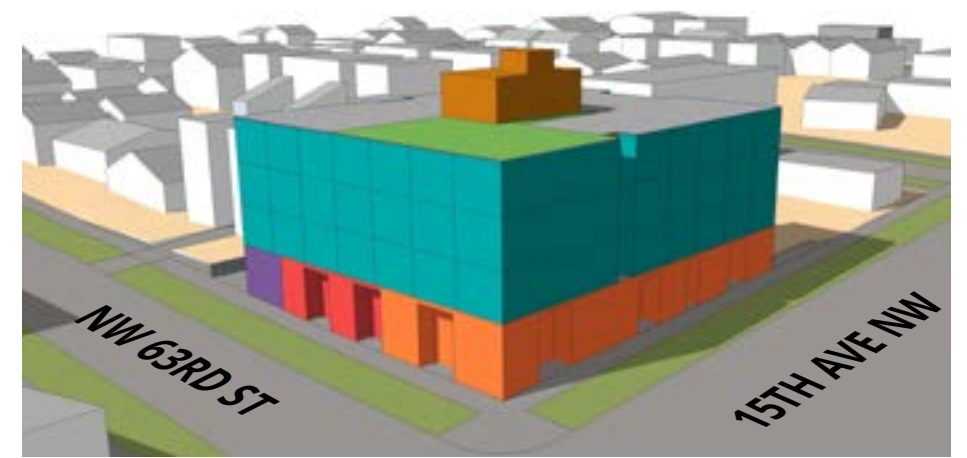
**SCHEME A (CODE COMPLIANT)**



**SCHEME B (L-SHAPED)**



**SCHEME C (PREFERRED)**



## DEPARTURE MATRIX

SCHEME A: CODE COMPLIANT	SCHEME B: L-SHAPED	SCHEME C: PREFERRED																
NO DEPARTURES REQUIRED.	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 50%;">1) SMC 23.47A.014.B.1</td> <td style="width: 50%;">SETBACK FOR LOTS ABUTTING RESIDENTIAL ZONES</td> </tr> <tr> <td colspan="2">REQUIRED: 15' TRIANGULAR SETBACK ABUTTING RESIDENTIAL ZONE.</td> </tr> <tr> <td colspan="2">PROPOSED: 8' REDUCTION OF TRIANGULAR SETBACK.</td> </tr> <tr> <td colspan="2">                     GUIDELINES &amp; JUSTIFICATION:                      - CS2/D.3 ZONE TRANSITIONS: The reduced setback creates a recessed alcove which buffers and mitigates the transition to the residential zone by matching the front setback of the adjacent development.                      - DC1/C.4 PARKING AND SERVICE USES: The setback reduction allows the building edge to be parallel to the sidewalk, keeping the trash room doors away from the adjacent residential use and pulled back from the property line.                      - The building facade is already recessed 7' from the property line, reducing the magnitude of the departure request.                 </td> </tr> </table>	1) SMC 23.47A.014.B.1	SETBACK FOR LOTS ABUTTING RESIDENTIAL ZONES	REQUIRED: 15' TRIANGULAR SETBACK ABUTTING RESIDENTIAL ZONE.		PROPOSED: 8' REDUCTION OF TRIANGULAR SETBACK.		GUIDELINES & JUSTIFICATION: - CS2/D.3 ZONE TRANSITIONS: The reduced setback creates a recessed alcove which buffers and mitigates the transition to the residential zone by matching the front setback of the adjacent development. - DC1/C.4 PARKING AND SERVICE USES: The setback reduction allows the building edge to be parallel to the sidewalk, keeping the trash room doors away from the adjacent residential use and pulled back from the property line. - The building facade is already recessed 7' from the property line, reducing the magnitude of the departure request.		<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 50%;">1) SMC 23.47A.014.B.3</td> <td style="width: 50%;">REAR SETBACK ABUTTING RESIDENTIAL ZONES</td> </tr> <tr> <td colspan="2">REQUIRED: 15' BETWEEN 13 TO 40' HEIGHT, PLUS 2' FOR EVERY 10' ABOVE 40' HEIGHT</td> </tr> <tr> <td colspan="2">PROPOSED: 15' SETBACK BETWEEN 40' TO 45' HEIGHT</td> </tr> <tr> <td colspan="2">                     GUIDELINES &amp; JUSTIFICATION:                      - CS2/D.3 ZONE TRANSITIONS: The reduced setback above 40' allows the massing to logically terminate at the top of the building. Because the 15' setback is maintained at level 1, a stronger buffer transition is established for the residential zone.                      - CS2/D.4 MASSING CHOICES: The 15' setback is consistent from ground to sky, creating a strong massing form and clear edge for the building.                      - The average upper level setback is 15'-6". A 12' wide recess (4' deep) breaks down the length of the facade and reduces the impact of the reduced upper level setback. The south portion of level 1 maintains the 15' setback for all levels, offsetting any perceived bulk of the 12" setback reduction at the upper level.                 </td> </tr> </table>	1) SMC 23.47A.014.B.3	REAR SETBACK ABUTTING RESIDENTIAL ZONES	REQUIRED: 15' BETWEEN 13 TO 40' HEIGHT, PLUS 2' FOR EVERY 10' ABOVE 40' HEIGHT		PROPOSED: 15' SETBACK BETWEEN 40' TO 45' HEIGHT		GUIDELINES & JUSTIFICATION: - CS2/D.3 ZONE TRANSITIONS: The reduced setback above 40' allows the massing to logically terminate at the top of the building. Because the 15' setback is maintained at level 1, a stronger buffer transition is established for the residential zone. - CS2/D.4 MASSING CHOICES: The 15' setback is consistent from ground to sky, creating a strong massing form and clear edge for the building. - The average upper level setback is 15'-6". A 12' wide recess (4' deep) breaks down the length of the facade and reduces the impact of the reduced upper level setback. The south portion of level 1 maintains the 15' setback for all levels, offsetting any perceived bulk of the 12" setback reduction at the upper level.	
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# PREFERRED SCHEME SKETCHES

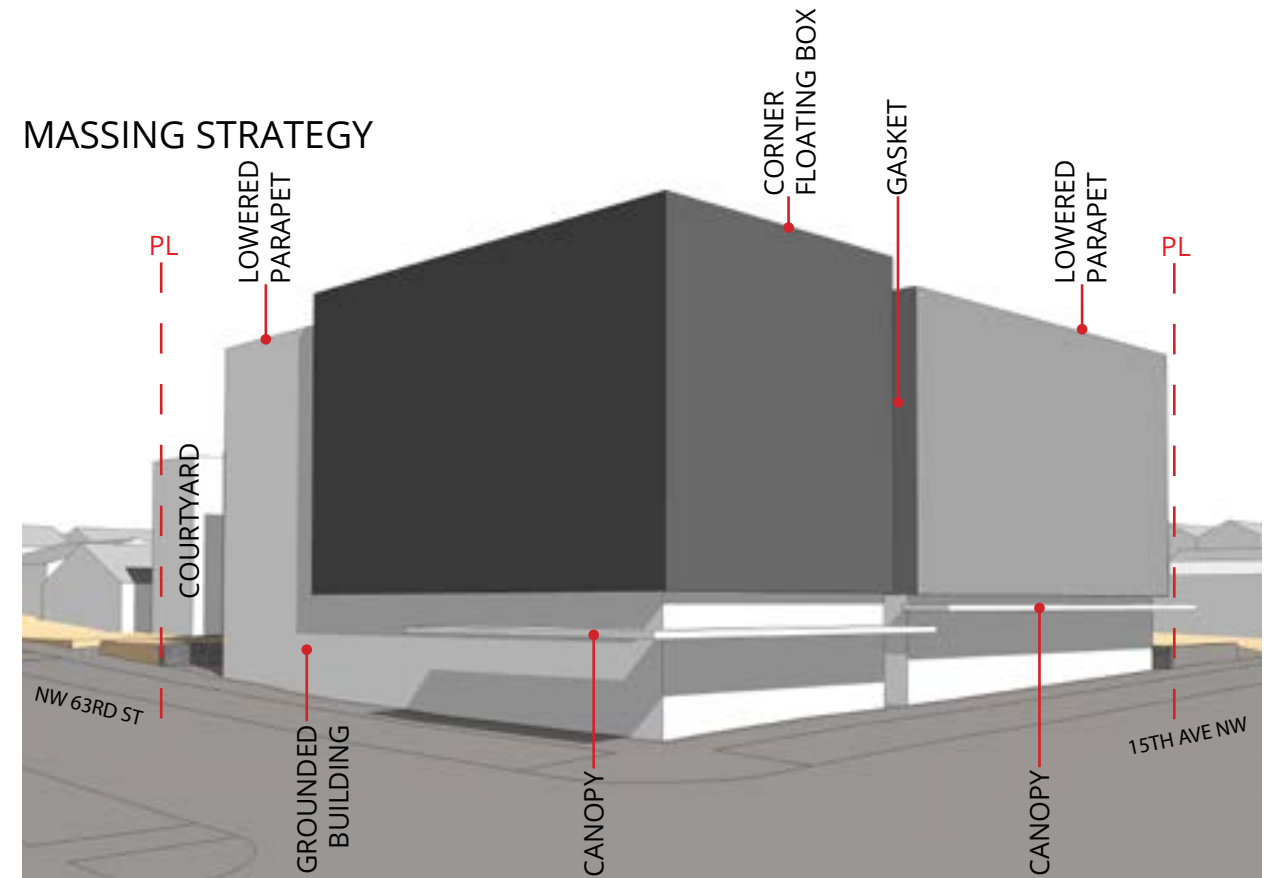
## COMPOSITE SITE PLAN



NW 63RD ST

15TH AVE NW

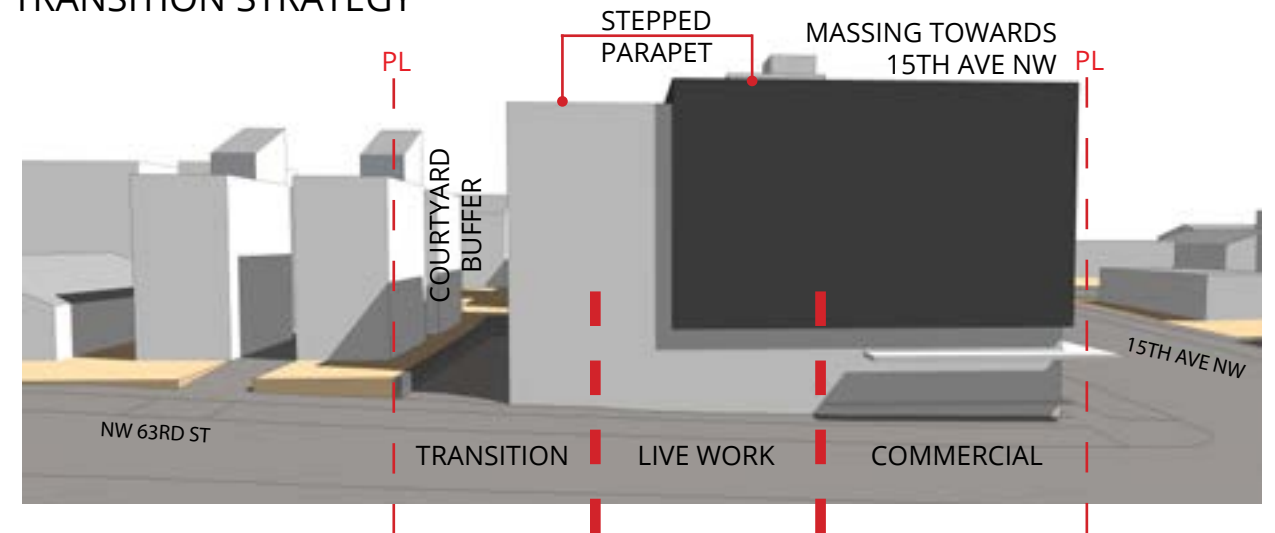
## MASSING STRATEGY



## MASSING ELEMENTS

- STRONG CORNER ELEMENT
- BOLD MASSING MOVES INSTEAD OF DECORATING A BOX
- DISTINCT ELEMENTS REDUCE BULK TO CREATE BUILDING DEFINITION

## TRANSITION STRATEGY



## TRANSITION ELEMENTS

- ORGANIZATION OF GROUND FLOOR USE
- SHIFTING OF PERCEIVED MASS TOWARDS CORNER, CREATING A BUFFER AT GRADE
- DEVELOPMENT OF WINDOW PALETTE AND MATERIALS WILL FURTHER ENHANCE TRANSITION

# HUMAN SCALE ELEMENTS

ENTRY



STOREFRONT



CANOPY



SIGNAGE



LANDSCAPE



RECENT WORK

NORĒN  
DEVELOPMENT

S+HWorks<sup>LLC</sup>  
ARCHITECTURE & DESIGN

