

DEXTER AVE

REPUBLICAN

HARRISON

400 DEXTER AVE N

SEATTLE WA, 98109

DPD# 3016362

WEST DESIGN REVIEW BOARD:
EARLY DESIGN GUIDANCE MEETING
ON 19.MARCH.2014

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PROJECT INFORMATION

ADDRESS

400 Dexter Ave N
Seattle, WA 98109

PARCELS

198820-1380
198820-1390
198820-1400

DPD PROJECT #

3016362

PROJECT TEAM

OWNER

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DEVELOPMENT OBJECTIVES

PROJECT INFORMATION

The owner proposes to demolish three existing single story buildings and replace with a new 11 story office building with 375,000 SF, including 254,000 sf of office, approximately 15,500 sf of retail and 128,000 sf of parking. All parking will be below grade, with 349 spaces on 3.5 levels. Garage entry & loading will be accessed off the existing alley to the east.

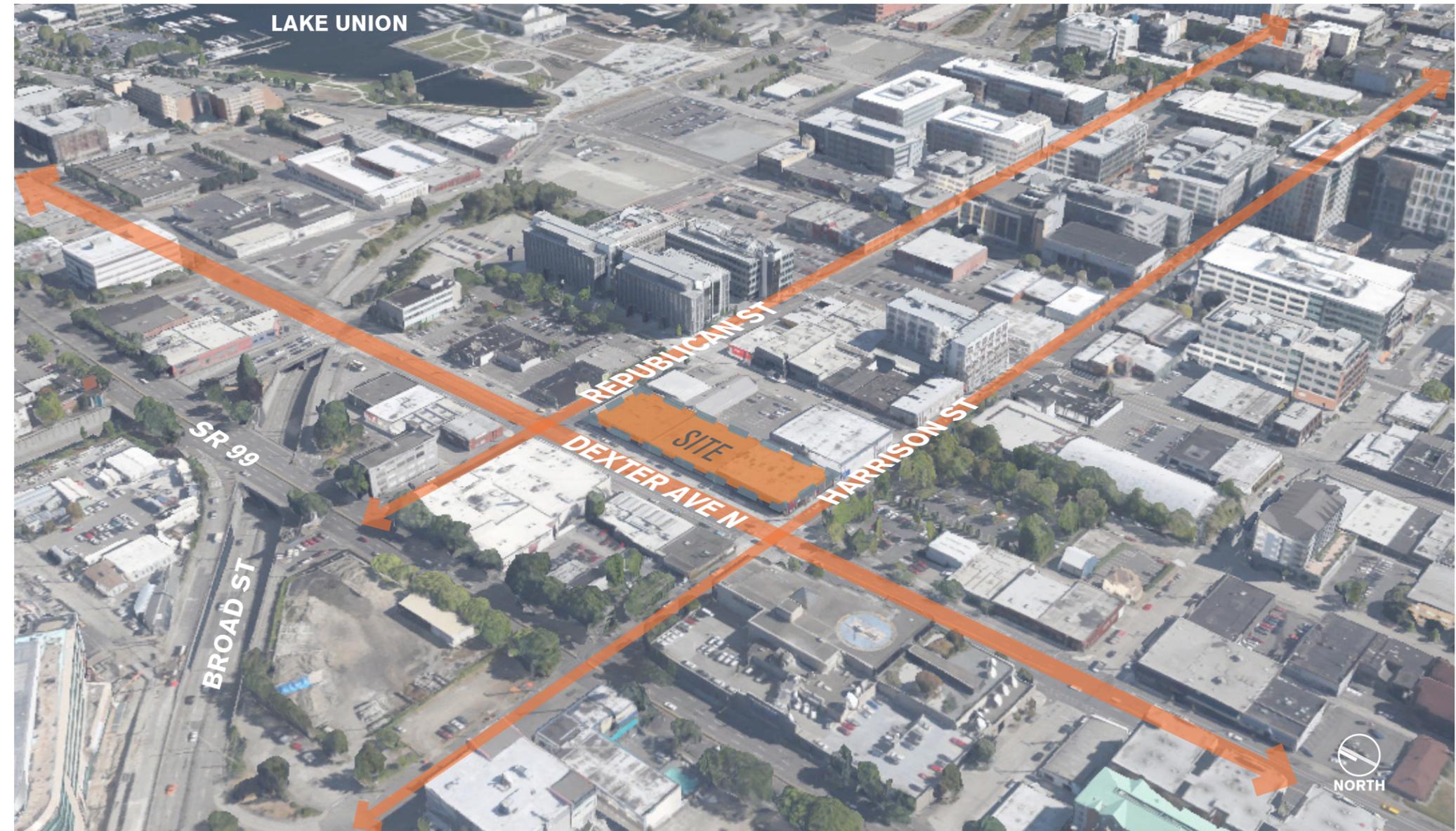
Public open space will be provided at the south end of the site along Harrison, with adjacent retail/restaurant space on the ground floor.

A new bus stop with shelter will be provided on Dexter.

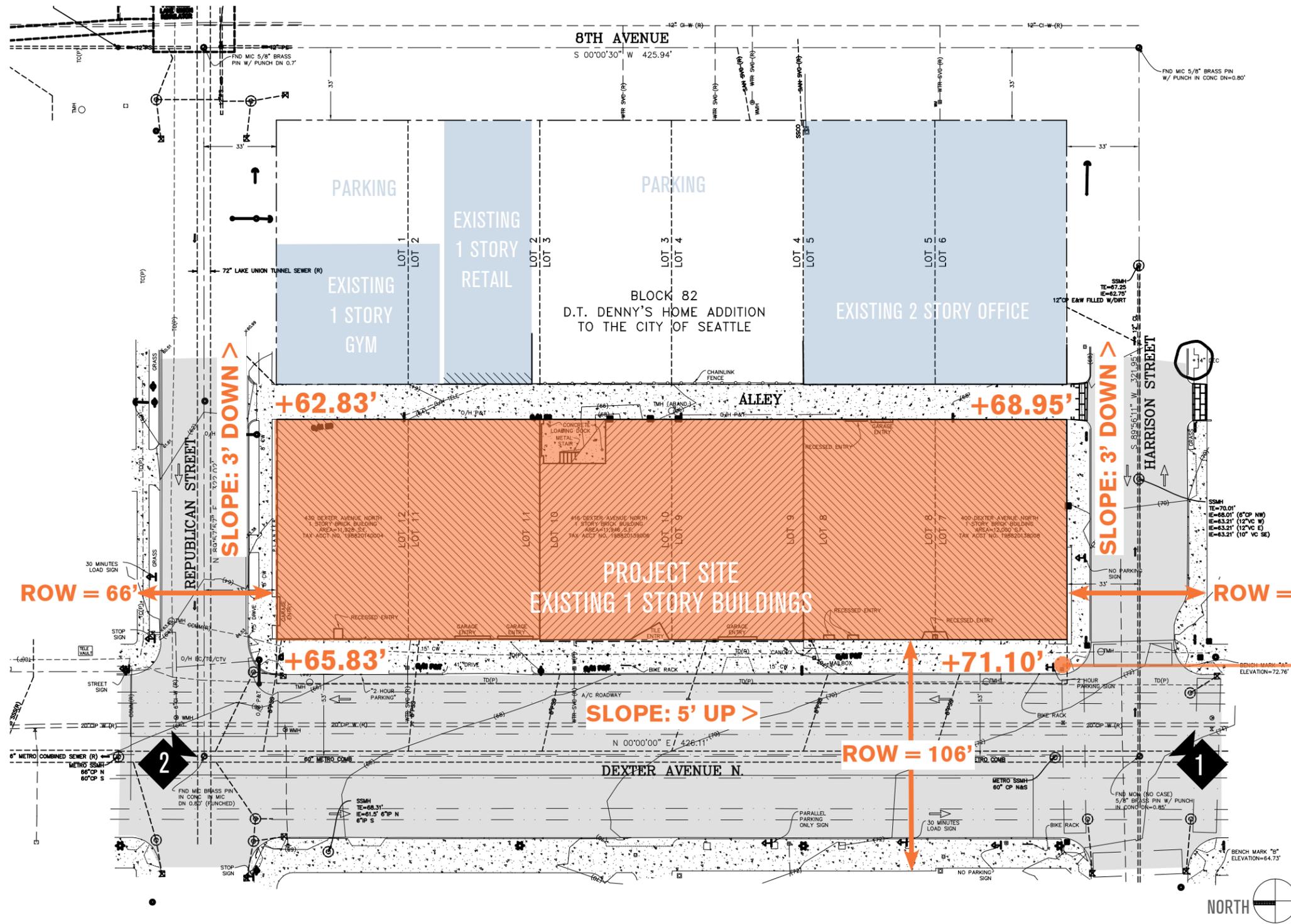
The client intends to refurbish the existing street clock to working order per title 15 of the SMC.

PROJECT GOALS

- Enhance new gateway from SR99 to SLU;
- Take advantage of nearly 360 views;
- Respond to & enhance the character of current & future development;
- Enhance pedestrian experience along Dexter, Harrison and Republican
- Provide open space to enhance pedestrian experience on Harrison and Dexter
- Maximize development potential;
- Achieve minimum LEED Silver.



SITE SURVEY



EXISTING SITE INFORMATION

SITE AREA

The site consists of 3 separate parcels (each comprised of 2 lots) for a total area of 35,874 sf. Each parcel has an existing 1 story brick structure built between 1928 - 1946 covering the entire buildable area. All existing structures have been extensively modified numerous times. The site borders a 16'-0" wide alley to the east.

TOPOGRAPHY

The site slopes roughly 3' up from east to west, and roughly 5' up from north to south. See site corner finish elevations at left.

TREES

No trees exist on the property or in the public right of way.

TRANSIT

Bus #26 & 28 stop at the south end of the site on Dexter, but there is no shelter or seating areas. Dexter also has dedicated north and south bicycle lanes adjacent to parallel parking.



1. VIEW FROM DEXTER & HARRISON

**DEPARTMENT OF NEIGHBORHOODS
HISTORIC BUILDINGS:**

Subject to Department of Neighborhoods & Landmark
Preservation Board review:

400 Dexter Ave N / E.J. Towle Company
Historic Building
Parcel ID 1988201380

400 Dexter Ave N / E.J. Towle Street
ClockLandmark



2. VIEW FROM DEXTER & REPUBLICAN

430 Dexter Ave N / Seattle Tent and Awning Company
Historic Building - 1928
Parcel ID 198821400

CLOCK HISTORY

"Cast iron street clocks, once commonplace, have become increasingly rare. this is one of only 10 or 11 still extant withing seattle. Probably built by the Mayer Company of Seattle, this clock has graced the NE corner of Dexter Ave N & Harrison st since sometime after 1936. By that year Joseph Mayer had moved his comany to this address. The E.J. Towle Company was also a tenant at 406 Dexter Ave N. By 1985 the building housed the West Earth Company. The clockworks, once visible within the clocks pedestal, are now hidden, and an electrical operation system has replaced the original weight-driven mechansim."

FROM [HTTP://WWW.HISTORYLINK.ORG/CYBERTOUR/PDF/LUWALKINGTOUR.PDF](http://www.historylink.org/cybertour/pdf/luwalkingtour.pdf)

INTENT

Per Title 15 of the Seattle Municipal Code, the clock must be functioning if it is to remain. The owner is committed to restoring the clock to original working condition.

URBAN DESIGN ANALYSIS - NEIGHBORHOOD CONTEXT

The Regrade / Belltown

SIGNIFICANT PLACE /FEATURES:

- Elliott Bay waterfront
- Density of mid-rise residential
- Popular restaurants and bars
- KIRO-TV headquarters

Seattle Center / Queen Anne / Uptown

SIGNIFICANT PLACE /FEATURES:

- Gates Foundation
- Seattle Center, Space Needle, McCaw Hall, Memorial Stadium, EMP
- KOMO and KCTS-TV headquarters
- Monorail
- Future North Portal Alaskan Way tunnel project

PRIMARY BUILDING TYPES:

- New mid-rise residential condo /apartment & hospitality mixed with low-rise motel
- Some light industrial / utility
- Nightclubs and bars

Aurora Corridor / South Lake Union

SIGNIFICANT PLACE /FEATURES:

- Denny Park
- Amazon
- Lake Union Park
- KING 5-TV headquarters

Denny Triangle

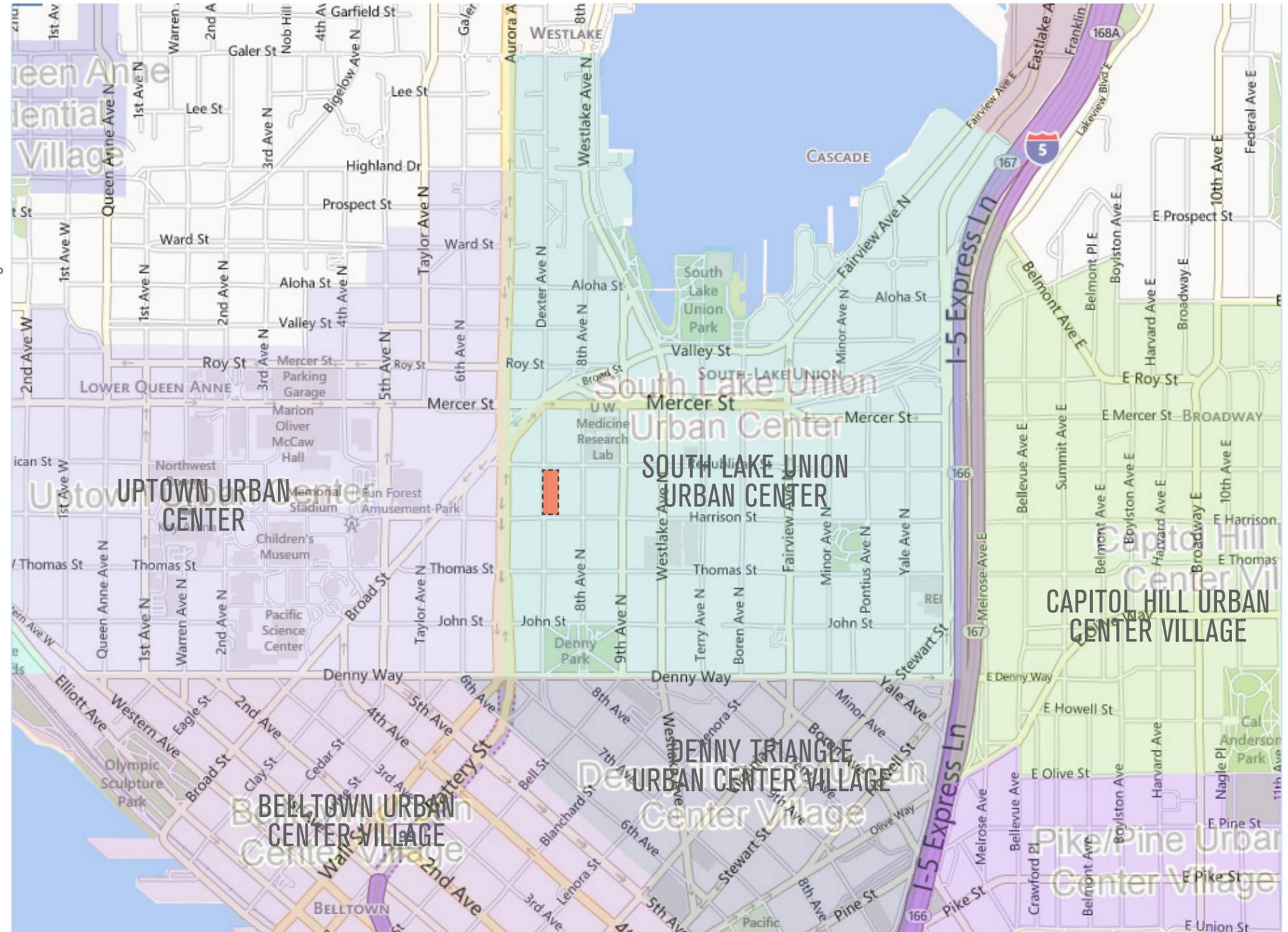
SIGNIFICANT PLACE /FEATURES:

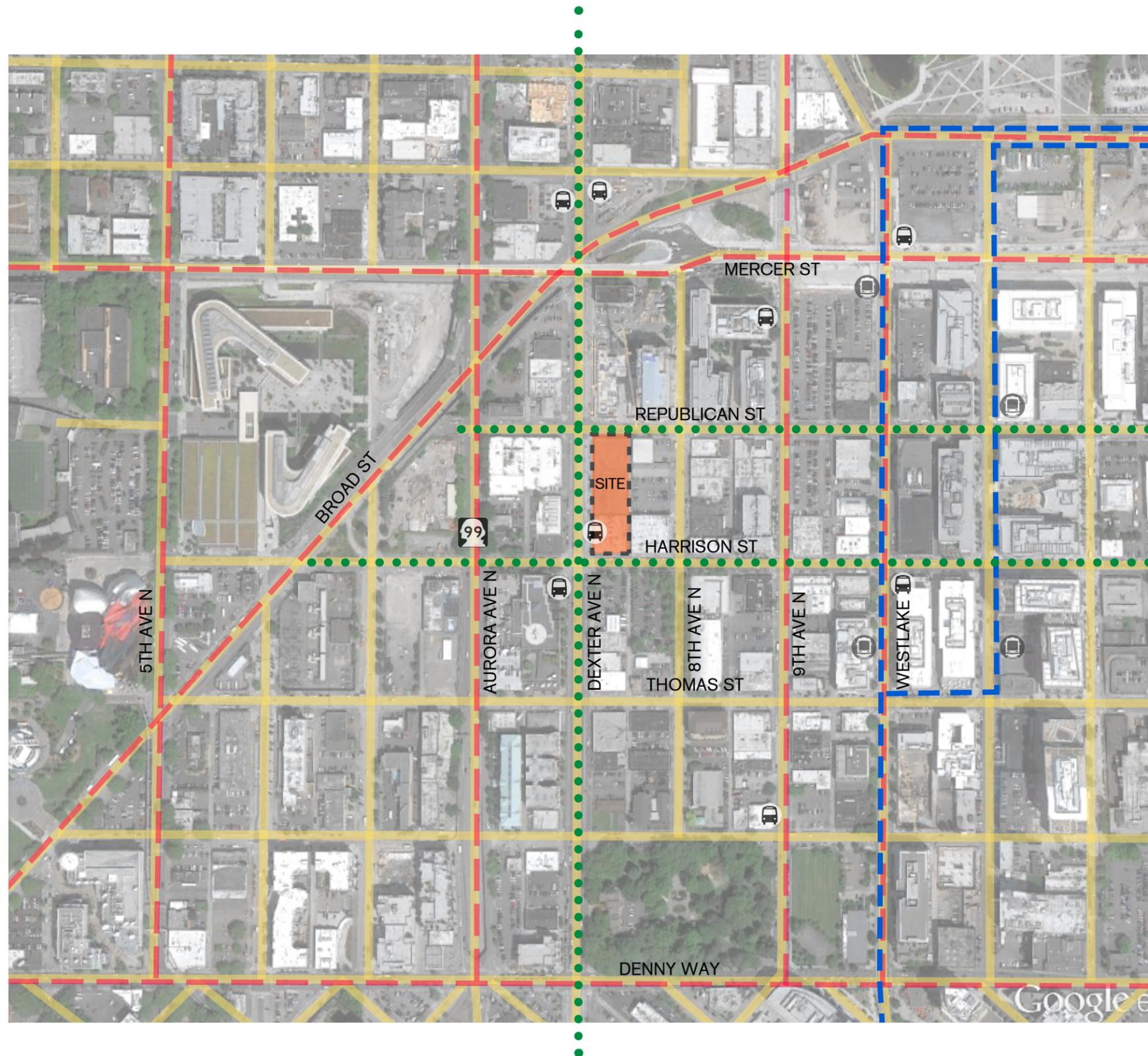
- Chief Seattle plaza
- Monorail
- Large development potential along 5th, 6th and 7th.

Capitol Hill

SIGNIFICANT PLACE /FEATURES:

- Volunteer Park
- Cal Anderson Park





ACCESS OPPORTUNITIES

VEHICULAR ACCESS

The site lies at the crossing point of two Minor Arterials - Dexter Ave N and Republican St. these two streets will provide access to the site from nearby Principal Arterials Aurora Ave N to the west, Westlake Ave N to the east, Mercer Street to the north, and Denny Way to the south.

TRANSIT

There is a bus stop at the corner of Harrison Street and Dexter Ave N serving bus routes 26 and 28 to Fremont, Green Lake, and Whittier Heights to the north, and Downtown transit hubs to the south.

PEDESTRIAN

Pedestrian access to the site will arise primarily from the public transportation stops around the site. Aurora Ave and Mercer Street act as barriers to foot traffic, while the smaller streets to the south and east of the site will bring in visitors from the Amazon campus and downtown.

ACCESS CONSTRAINTS

Access from the northwest will have to navigate across Broad Street, Mercer Street, and Aurora Ave North, all three are major streets with few opportunities for cross access. To the south of the site the street grids of South Lake Union and downtown meet at an odd angle creating difficult intersections.

STREET CLASSIFICATIONS ●●●●●

- Dexter Ave N**
 - Minor Arterial
 - Major Transit Street
 - On-Street Urban Trail (bike)
 - Class II Pedestrian Street
 - Neighborhood Green Street
- Republican**
 - Minor Arterial
 - Class II Pedestrian Street
- Harrison**
 - Class II Pedestrian Street

ACCESS OPPURTUNITIES

SOUTH LAKE UNION

BUILT ENVIRONMENT

SLU is a rapidly changing neighborhood. High-tech and biomedical office buildings, most of which were built within the last 10 years, are quickly replacing the single story light industrial & commercial businesses. A large amount of residential buildings & retail/dining/services are also being developed to support the influx of jobs.

TRAFFIC INFRASTRUCTURE

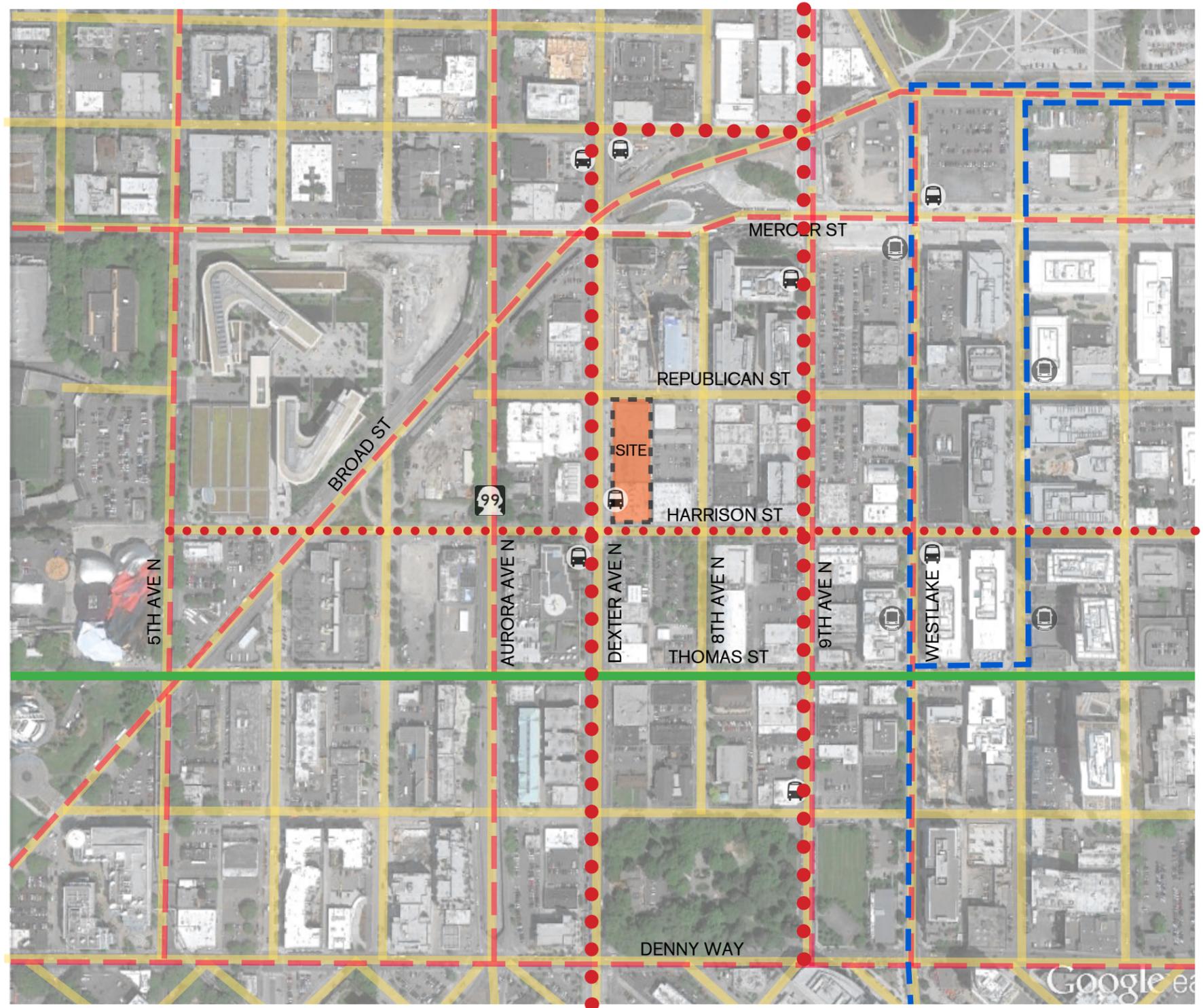
With Dexter, Aurora & Mercer streets all within one block of the site, the area is car-based but many provisions for pedestrian traffic are being built as part of the new developments. Direct connection to downtown, Belltown, Seattle Center & Lower Queen Anne, and Westlake provides many opportunities.

Major north-south bike corridor on Dexter.

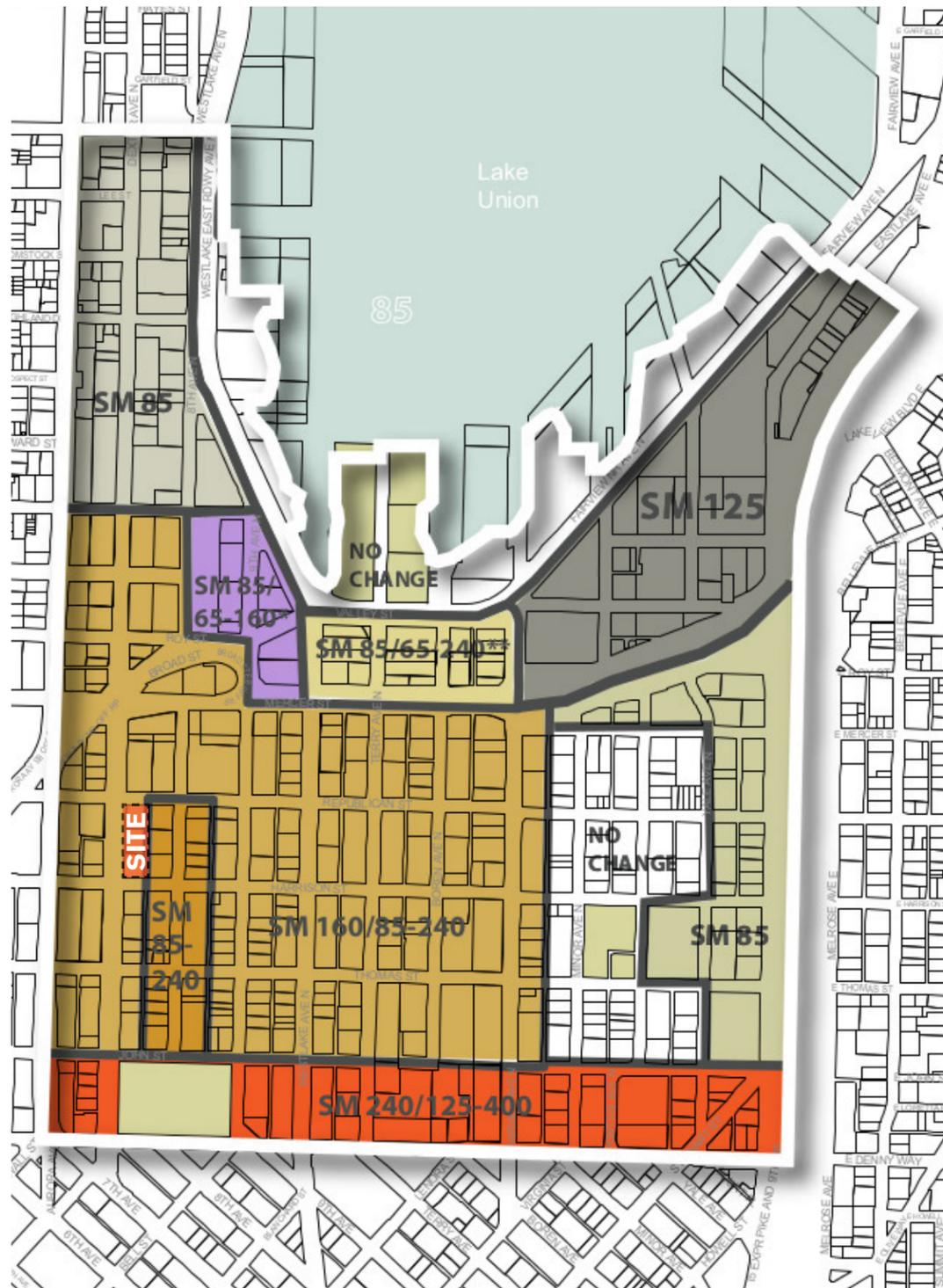
Mercer corridor is currently being revised to handle increased E/W traffic.

The north portal of the new SR 99 tunnel will exit (northbound) onto Republican directly adjacent to the site.

Aurora Ave will become a surface street and multiple E/W connectors will tie SLU to lower Queen Anne & Seattle Center, which is currently cut off by SR 99. (See pg 18).



-  Bus Stop
-  Streetcar Stop
-  Primary Arterial
-  Streetcar Route
-  Protected Bike Lane (Proposed)
-  In-Street Bike Lane (Proposed)
-  Neighborhood Greenway (Proposed)



LAND USE CODE ANALYSIS

(SEE PG 38 FOR DEPARTURE REQUESTS)

ZONE	SM 160/85-240
OVERLAY	South Lake Union Urban Center Airport Height Overlay ??
LOT AREA	36,097 SF
PERMITTED USES	All (23.47.004,B)
LEED REQUIREMENT	Silver (23.48.017,D)

Approx 6'-0" of grade difference
Existing Alley will be widened by 2'

SMC 23.48.009 FLOOR AREA RATIO

Base FAR	4.5
Maximum FAR	7
<i>(all below grade & street floor retail exempt (23.48.009,D))</i>	
Proposed FAR	7

SMC 23.48.010 GENERAL STRUCTURE HEIGHT

Maximum Height	160'
Maximum Podium Height	65'
<i>(penthouse may exceed height by 25' (23.48.010,G,5))</i>	
Proposed Height	160'

SMC 23.48.012 SETBACKS

Max Street Level Setback	12' (to be landscaped)
Upper Level Setbacks	45'

apply above

(A setback of one foot for every two additional feet of height is required for any portion of a structure exceeding the max height permitted, up to a maximum setback of 15')

SMC 23.48.013 UPPER LEVEL STANDARDS

Maximum Floor Plate	24,000 sf
Allowed Towers (+85') per block	1

Facade Modulation required for structures over 85' in height and within 15' of the lot lines
Departure request 1 & 2 (See pg 38)

SMC 23.48.014 STREET LEVEL STANDARDS

Street Level Facade Transparency	60% required
Min height for street-facing facades	25'

Dexter Ave N, Republican & Harrison are Class II Pedestrian Streets

SMC 23.48.022 OPEN SPACE REQUIREMENTS FOR OFFICE

- Minimum 15% of lot area at ground level for lots over 30,000 sf, 45% of which must be open to the sky and with easy access to the street
- Office uses exceeding 85,000 SF are required to provide open space at a ratio of 20 SF per 1,000 SF of office space provided

SMC 23.48.024 SCREENING & LANDSCAPING

Maximum Green Factor 0.30

SMC 23.48.030 LIGHT & GLARE

Mitigate potential adverse impact

SMC 23.48.032 REQUIRED PARKING AND LOADING

Parking:

Office	1 per 1,000 gsf (Maximum)
Eating & Drinking	1 per 250 sf
Stalls Required	253
Proposed	354 stalls
<i>Type 1 parking exception request (1.4 / 1000 at commercial space)</i>	
<i>Departure Request #3 - 25% of large stalls (See pg 44)</i>	

Bicycle Parking

Office	1 per 4,000/1 per 40,000
Eating & Drinking	1 per 12,000/1 per 4,000
Loading Required	4 docks

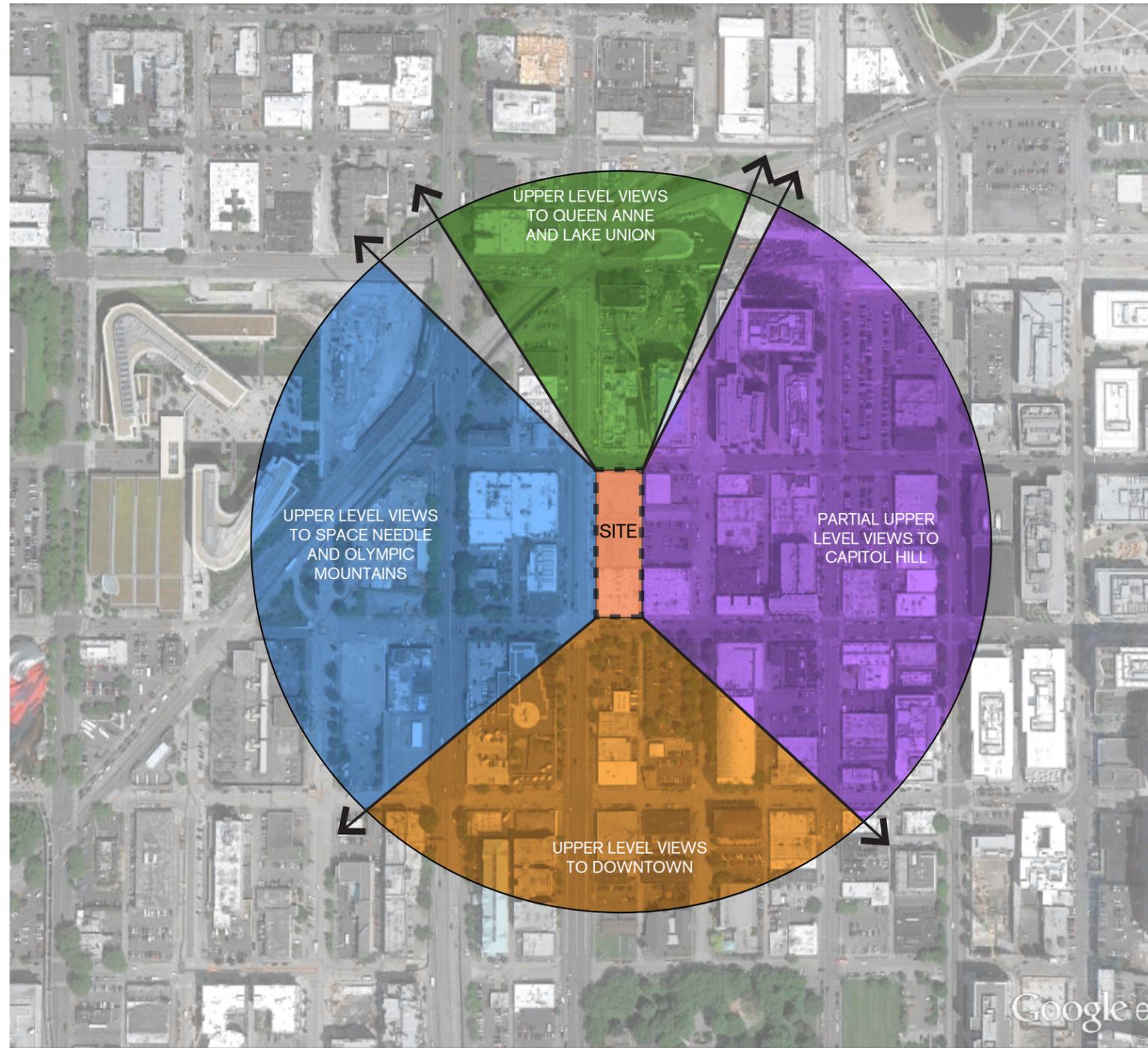
Parking must be below street level

SEPA 25.05.675.P/2/C-SPECIAL ENVIRONMENTAL POLICIES

It is the City's policy to protect public views of the Space Needle from the following public places. A proposed project may be conditioned or denied to protect such views, whether or not the project meets the criteria of the Overview Policy set forth in SMC Section 25.05.665

- i. Alki Beach Park (Duwamish Head); ii. Bhy Kracke Park;
- iii. Gasworks Park; iv. Hamilton View Point; v. Kerry Park;
- vi. Myrtle Edwards Park; vii. Olympic Sculpture Park; viii. Seacrest Park; ix. Seattle Center; x. Volunteer Park

URBAN DESIGN ANALYSIS - VIEWS



VIEWS TAKEN FROM APPROX. 80' ABOVE GRADE



LOOKING SOUTH TO DOWNTOWN



LOOKING NORTHEAST TO LAKE UNION / CAPITOL HILL

VIEWS TAKEN FROM APPROX. 80' ABOVE GRADE



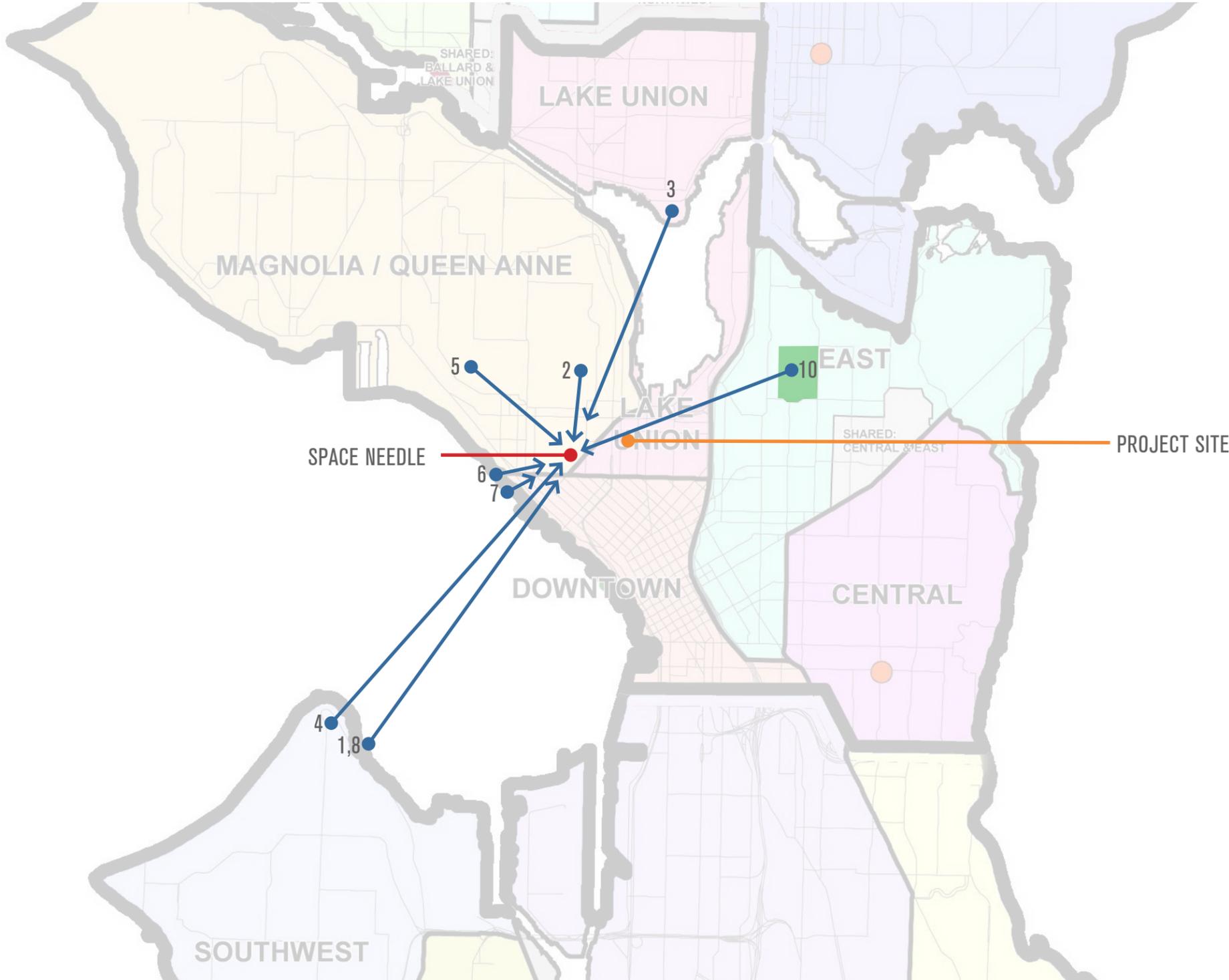
PROJECT SITE

LOOKING SOUTH / WEST - DOWNTOWN TO SEATTLE CENTER/QUEEN ANNE

URBAN DESIGN ANALYSIS - BUILDING HEIGHTS



- Project Site (11 Stories Proposed)
- 1-2 Story Building
- 5+ Story Building (P = Proposed)
- Green or Public Open Space



SPACE NEEDLE VIEW CORRIDORS

Per SMC SubChapter VII - SEPA & Agency Decisions, Section 25.05.675/P/2/c states that views of the Space Needle shall be protected from the following public places:

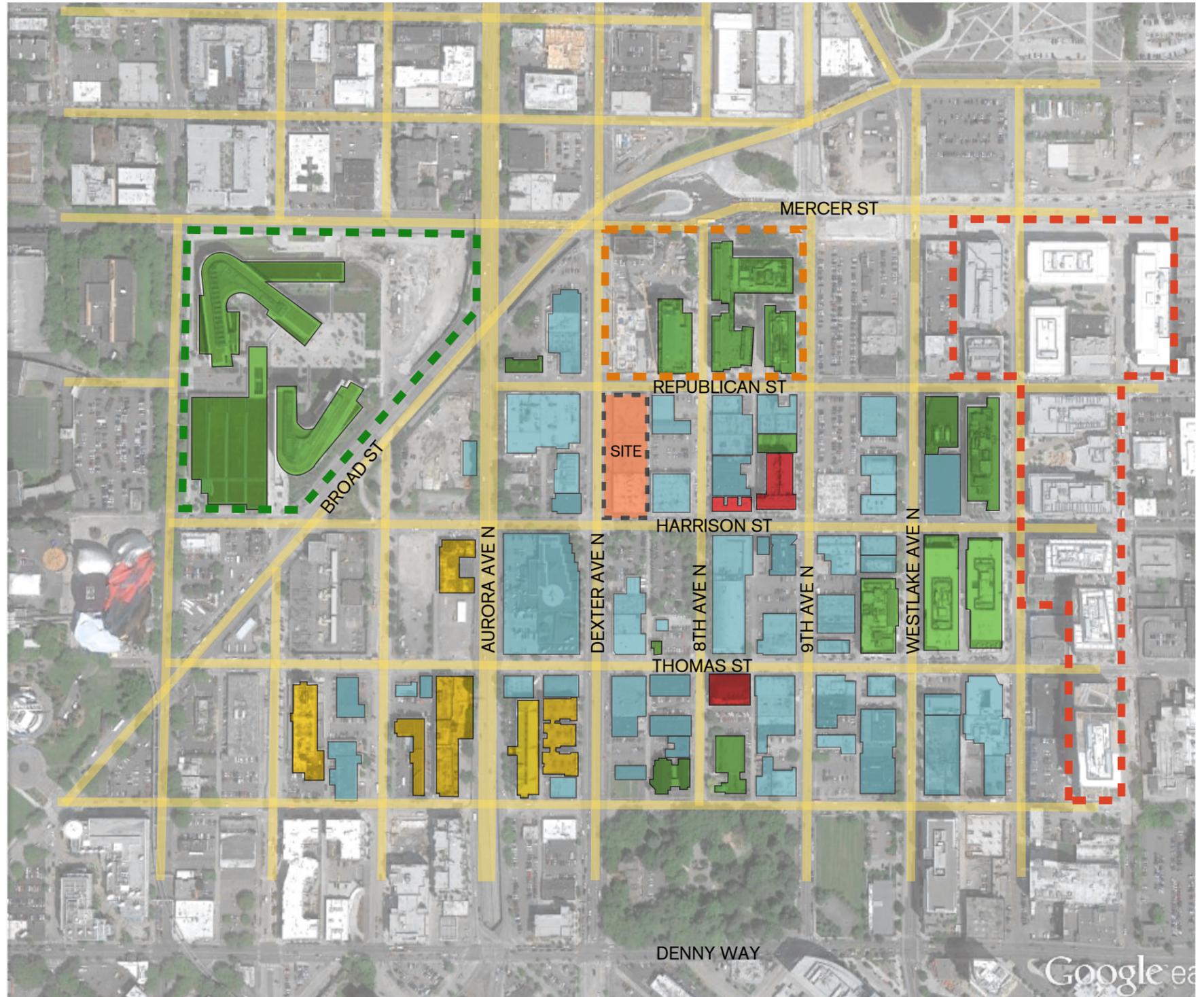
1. Alki Beach Park (Duwamish Head)
2. Bhy Kracke Park
3. Gasworks Park
4. Hamilton View Point
5. Kerry Park
6. Myrtle Edwards Park
7. Olympic Sculpture Park
8. Seacrest Park
9. Seattle Center
10. Volunteer Park

	ELEVATION	HEIGHT	TOP ELEV
Space Needle	135'	605'	+740'
400 Dexter	67'	160'	+227'
Volunteer Park	400'	-	

SITE ANALYSIS - SURROUNDING USES (EXISTING)

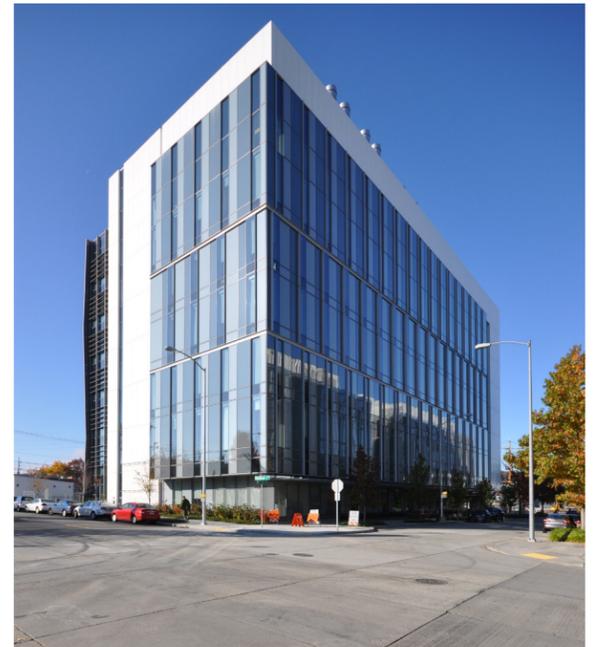
SURROUNDING USES

The immediate context consists of a mix of commercial and institutional laboratory uses. Directly to the north of the site lies the University of Washington Medicine facilities. To the east and west of the site reside small commercial facilities. The block to the southwest of the site houses the headquarters for KCTS-TV.



- Amazon Campus
- Uw Medicine Campus
- Gates Foundation Campus
- Institutional
- Commercial
- Residential
- Hotel

SITE ANALYSIS - SURROUNDING ARCHITECTURE (EXISTING)



EXAMPLES OF NEW DEVELOPMENT *(Mostly mid to high-rise)*



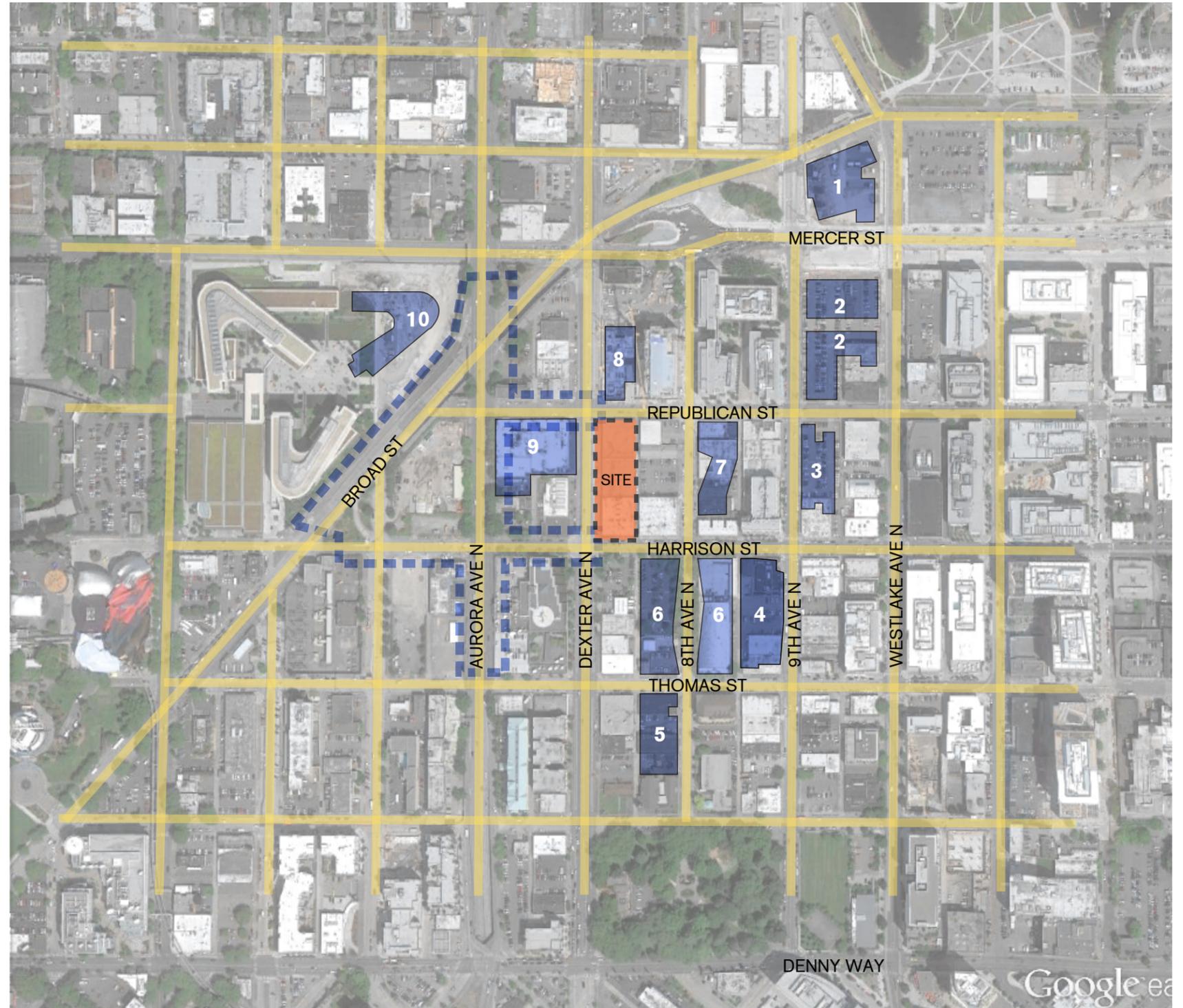
EXAMPLES OF OLDER BUILDINGS *(Mostly low-rise, one and two story)*

SITE ANALYSIS - SURROUNDING USES (FUTURE DEVELOPMENT)

Projects currently under development:

1. 3014287 - ALLEN INSTITUTE FOR BRAIN SCIENCE
2. 3012563 - OFFICE / RETAIL
3. 3014653 - OFFICE / RETAIL
4. 3014639 - OFFICE / RETAIL
5. 3014045 - RESIDENTIAL
6. 3014982 - OFFICE / RETAIL
7. 3014781 - RESIDENTIAL
8. 3011312 - UW MEDICINE PHASE 3.2
9. 3016347 - RESIDENTIAL
10. GATES FOUNDATION PHASE 3

-  Future Project Site
-  Sr99 North Portal Realignment



SITE ANALYSIS - SURROUNDING USES (FUTURE DEVELOPMENT)



1. ALLEN INSTITUTE FOR BRAIN SCIENCE - DPD #3014287



2. OFFICE/RETAIL - DPD #3012563 + 3012560



3. OFFICE/RETAIL - DPD #3014653



4. OFFICE/RETAIL - DPD #3014639



5. RESIDENTIAL - DPD #30144045



6. OFFICE/RETAIL - DPD #3014981 + 3014982



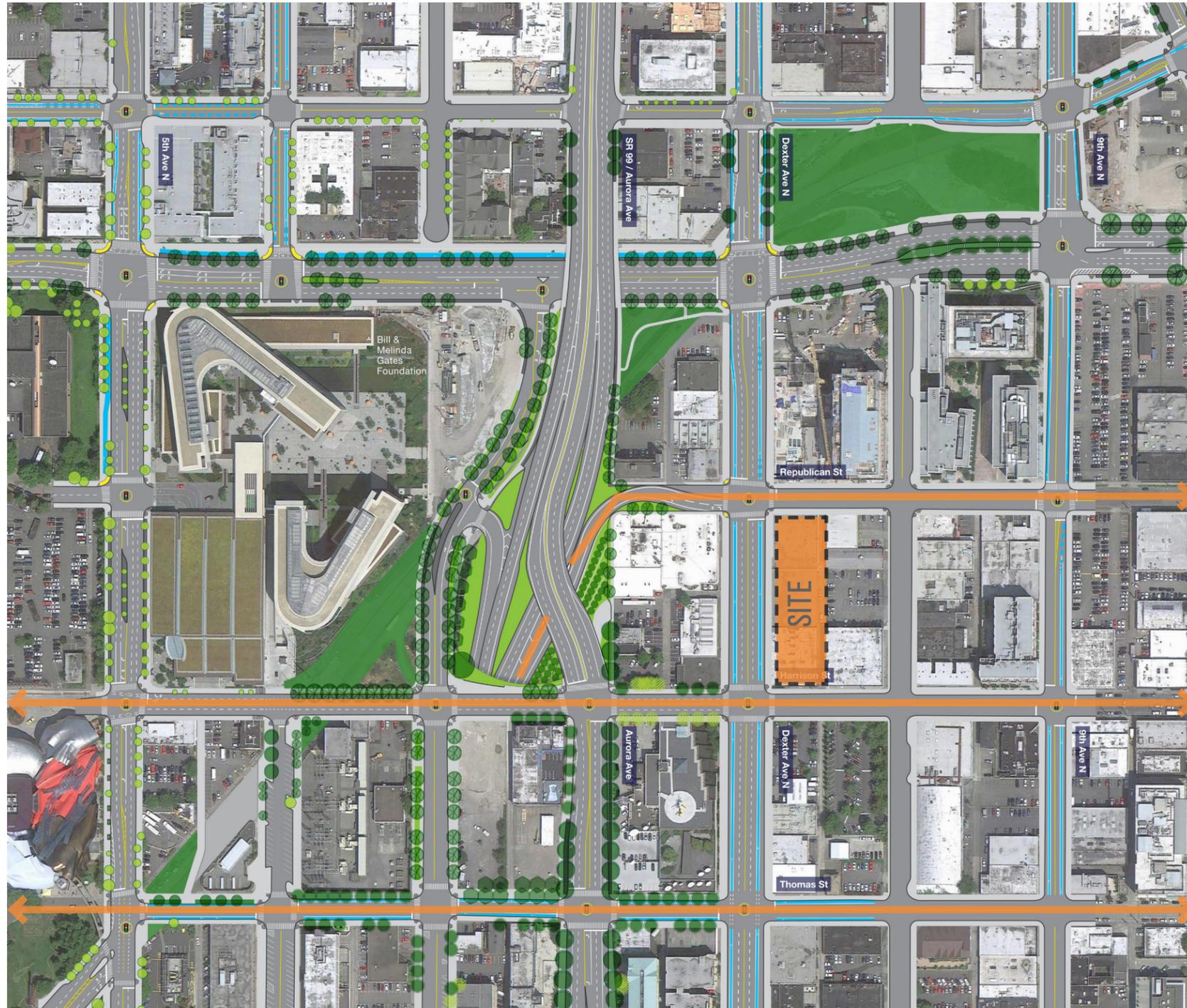
7. RESIDENTIAL - DPD #3014781



8. UW MEDICINE PHASE 3.2 - DPD #3011312



9. RESIDENTIAL - DPD #3016347



STREET IMPROVEMENTS

As part of the current viaduct removal & tunnel project, major street improvements will be made immediately adjacent to the site within the next 5 years. The north portal for the tunnel will exit onto Republican St heading east, creating a new gateway to South Lake Union for traffic coming from the south. Aurora Ave N will change from a highway to a surface street, with new east/west connections on Harrison, Thomas, & John. This will significantly enhance the connection between SLU & LQA, particularly for pedestrians. Currently only Mercer & Denny Way link the two, and both streets are heavily oriented towards car traffic.

REPUBLICAN ST (SR99 OFFRAMP)

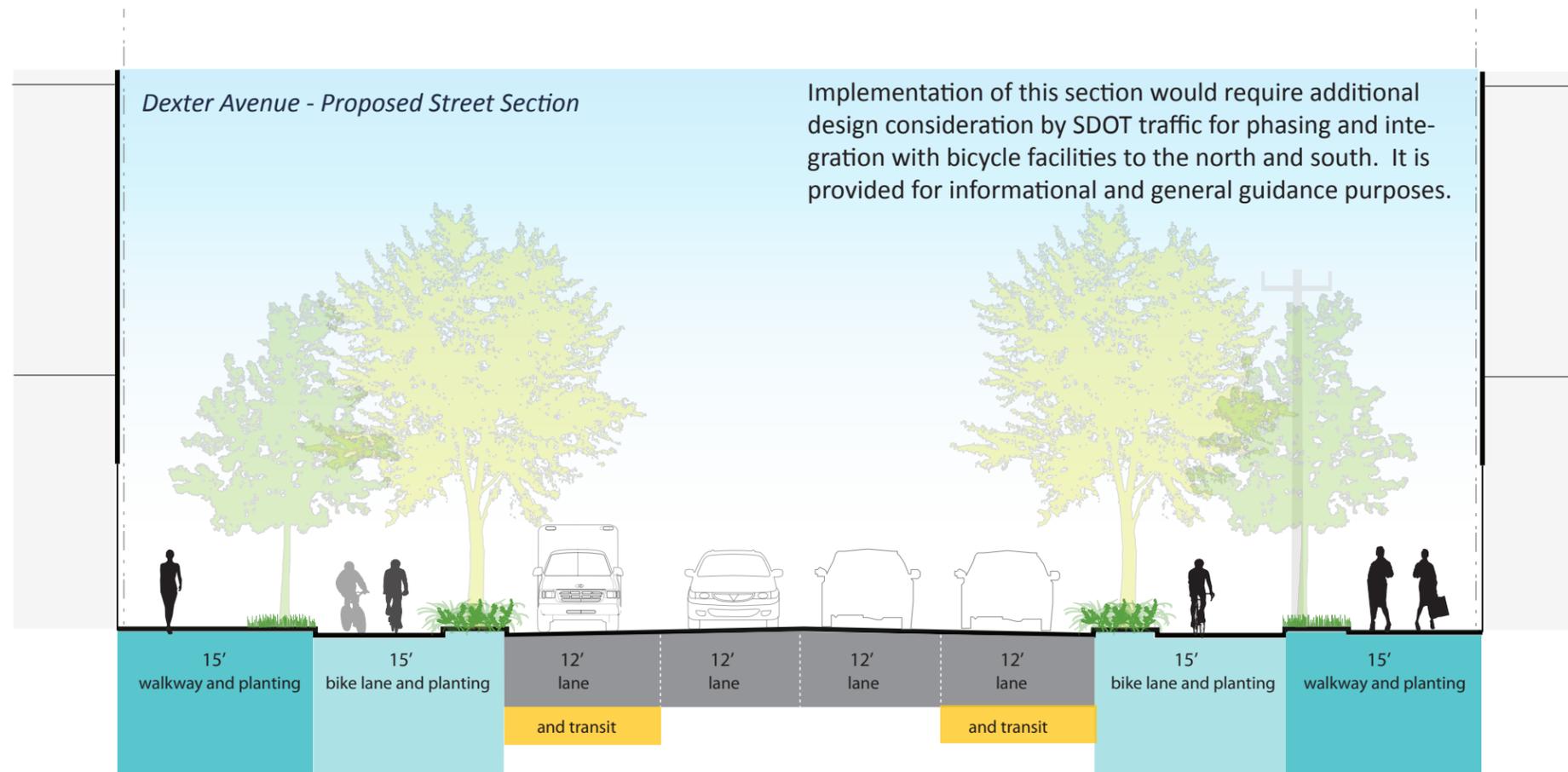
HARRISON ST

THOMAS ST



RENDERING OF NEW SR99 NORTH PORTAL LOOKING SOUTH

FUTURE TRAFFIC IMPROVEMENTS - DEXTER AVE. NORTH



CHARACTERISTICS AND DESIGNATIONS

Dexter is designated as a Minor Arterial on the City of Seattle Arterial Classifications Map. Several bus routes are located on Dexter Ave. And Dexter Ave. is identified for a future cycle track in this section of the study area. Currently this portion of Dexter Ave. also includes on street parking on both sides of the street.

PROPOSED CONFIGURATION - POTENTIAL

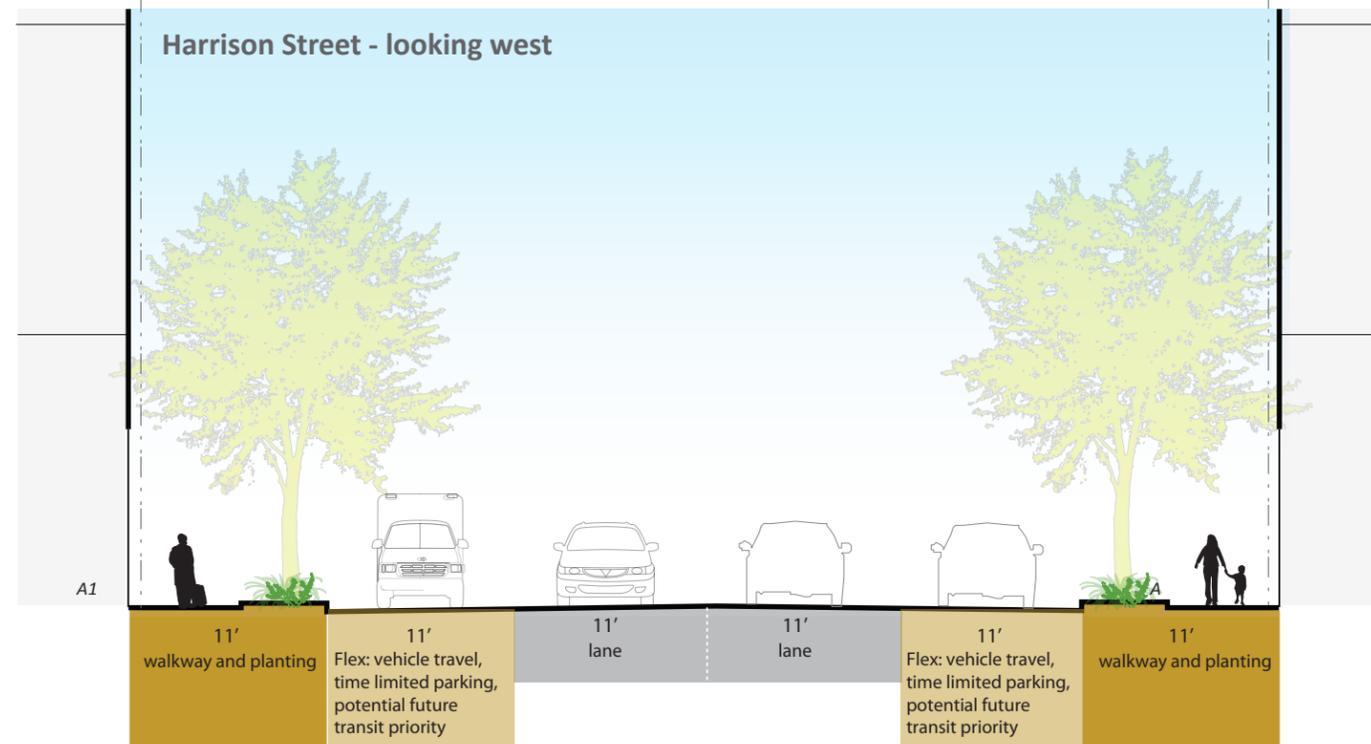
The section above explores a configuration that accommodates the proposed cycle track on Dexter Ave. This configuration would allow for a wide 15' sidewalk and planted buffer. The proposed section removes on street parking in this area. Outside lanes would accommodate transit use. Modification of the curbside zone adjacent the cycle track would be required to locate bus zones at the curb edge.



Dexter Avenue - existing conditions

STUDY AREA STREETScape CONCEPTS

HARRISON STREET CONCEPT

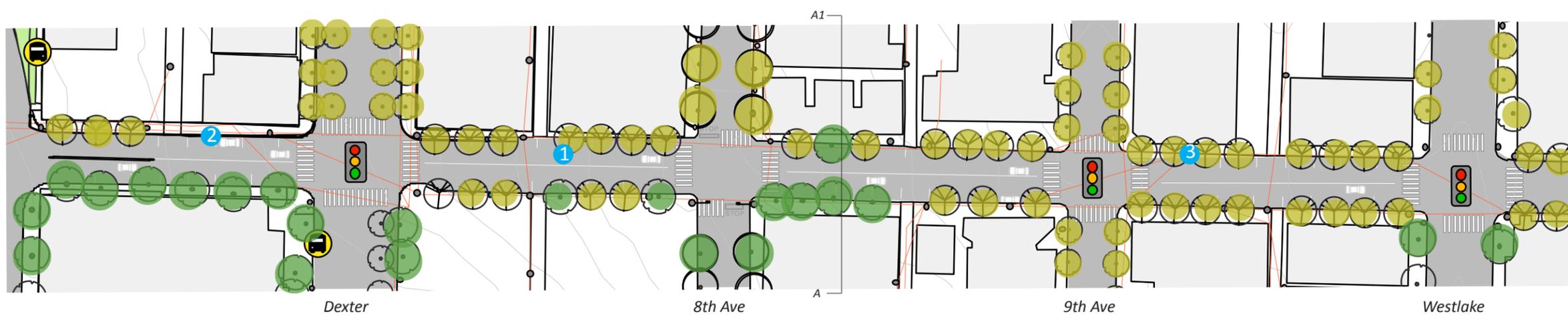


Proposed Configurations

- 1 Inside lane ---1 travel lane each way at 11'
- 2 curbside lane is flexible: may be used as time limited parking or future transit priority lane
- 3 11' sidewalk with planting area - keep existing trees where healthy and add new Cimmarron Ash trees where possible

Encourage building setbacks near transit stops, and integration of transit zone features with building facades

Encourage undergrounding of utilities



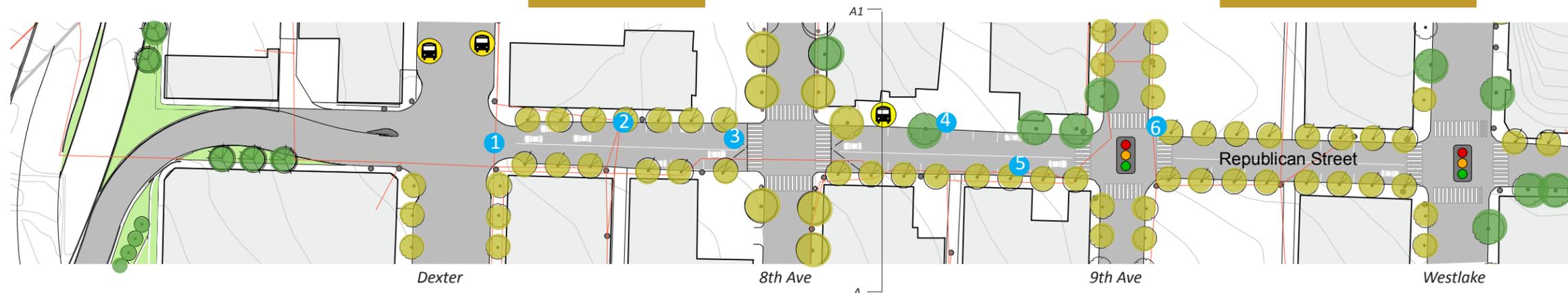
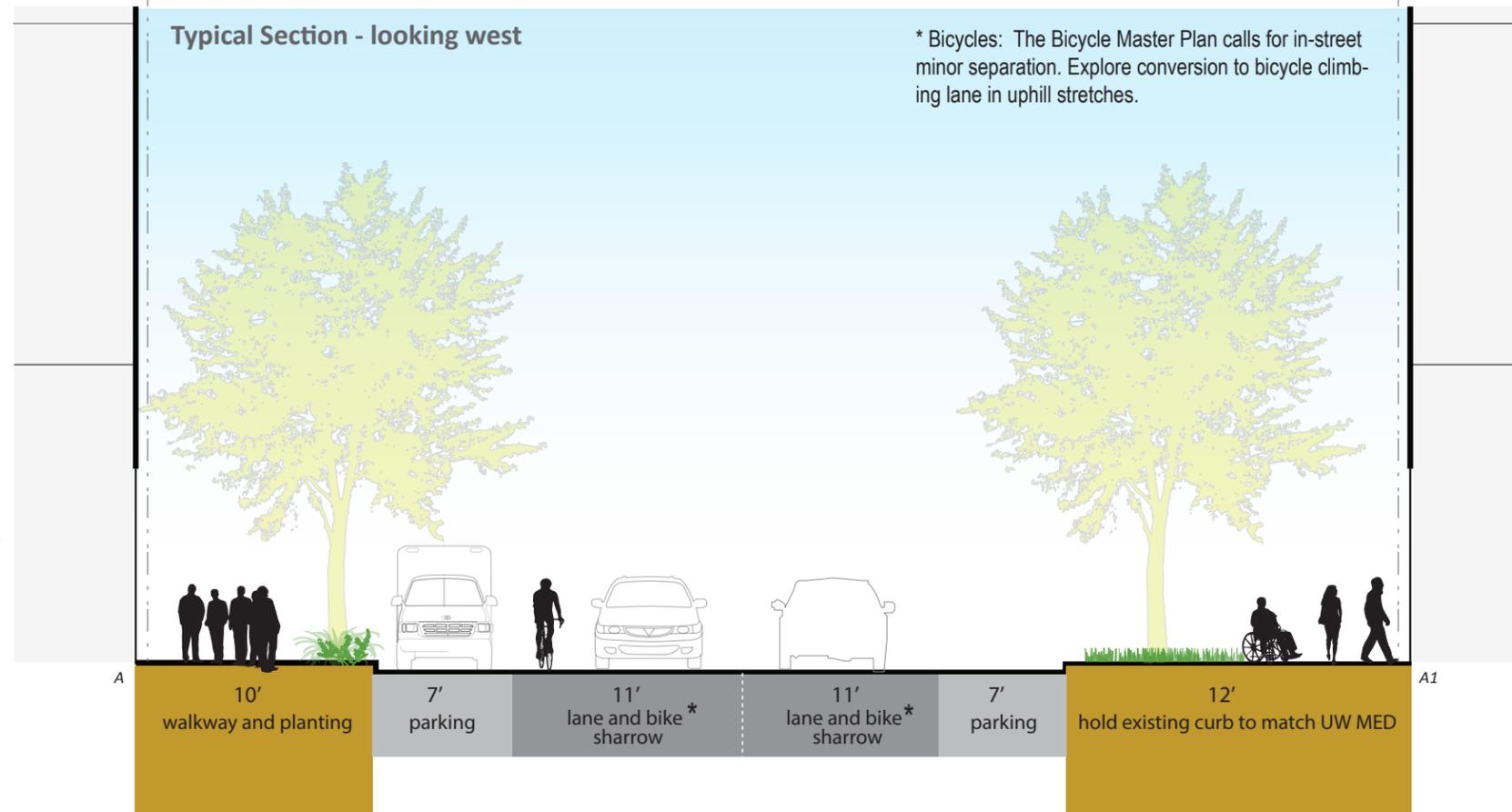
FUTURE TRAFFIC IMPROVEMENTS - REPUBLICAN ST.

STUDY AREA STREETScape CONCEPTS

REPUBLICAN STREET CONCEPT

Proposed Configurations

- 1 11' travel lanes; one each direction
- 2 7' parallel parking both sides
- 3 Bicycles in shared roadway with addition of dedicated climbing lane in uphill sections of the roadway.
- 4 Keep north curb line set by UW Med development.
- 5 Extend landscape character from UW Medicine on north side of street
- 6 Encourage undergrounding of utilities with development



key
 existing trees
 proposed trees
 utility lines



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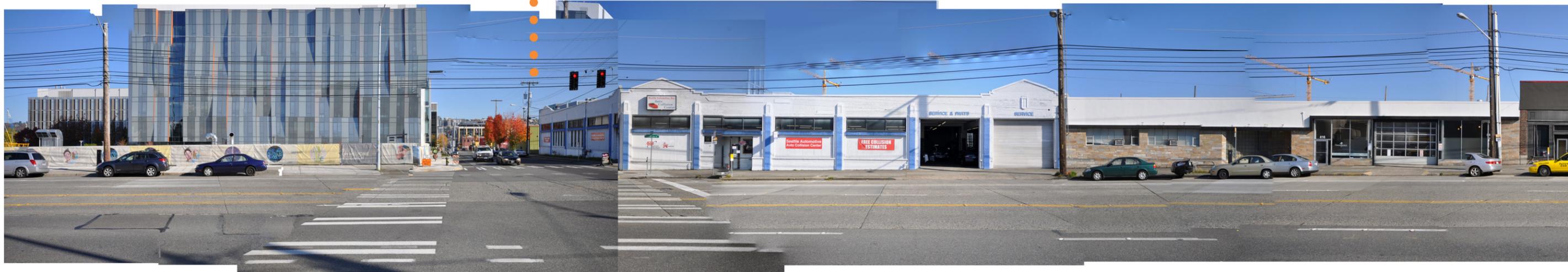
URBAN DESIGN ANALYSIS - STREETSCAPES

HARRISON ST



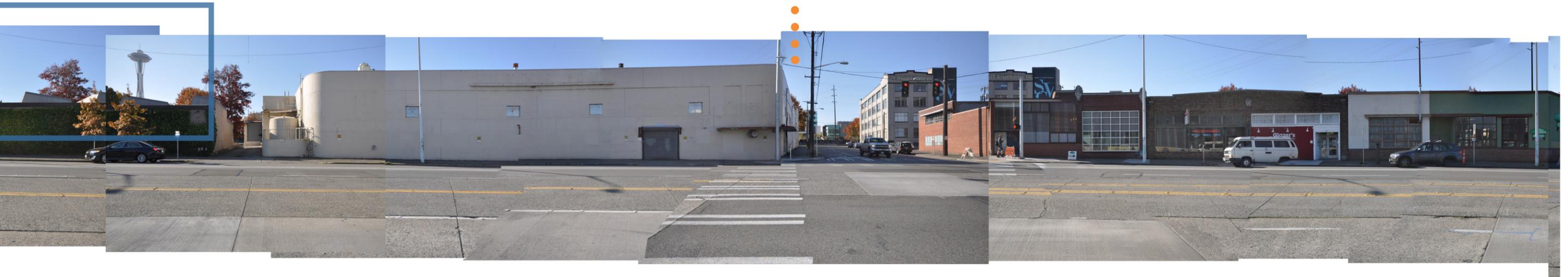
1 DEXTER AVE - WEST

SITE

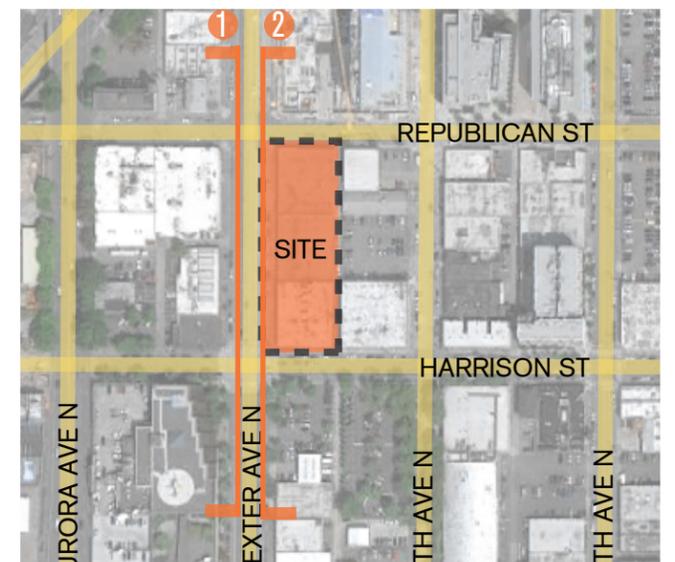
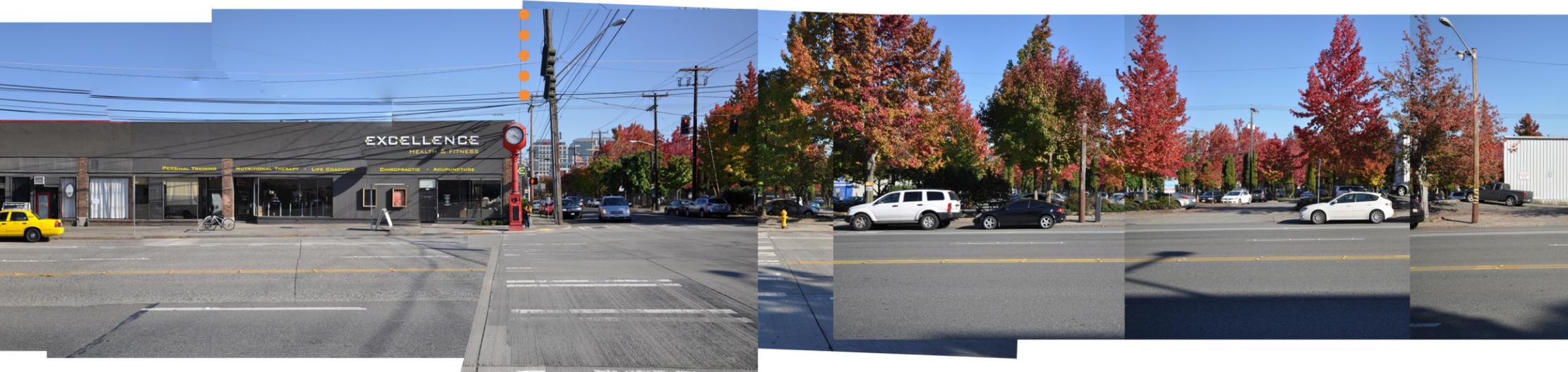


2 DEXTER AVE - EAST

ADJACENT TO SITE ACROSS DEXTER



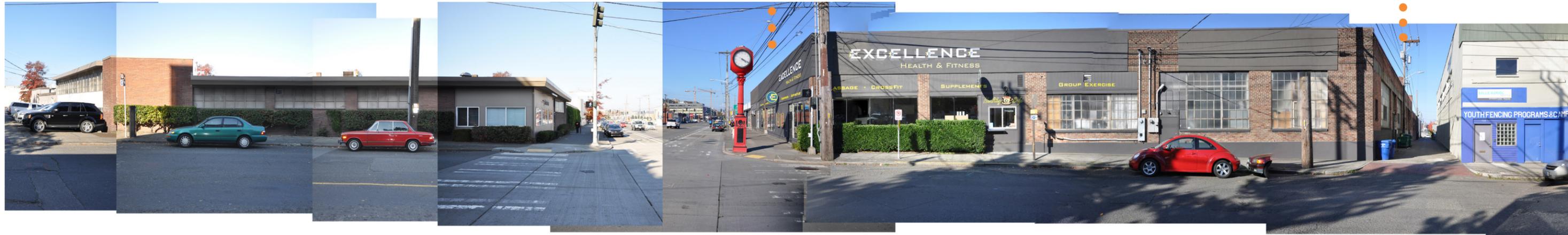
REPUBLICAN ST



URBAN DESIGN ANALYSIS - STREETSCAPES

DEXTER AVE

SITE



1 HARRISON ST - NORTH



ADJACENT TO SITE A

2 HARRISON ST - SOUTH



CROSS HARRISON



URBAN DESIGN ANALYSIS - STREETSCAPES



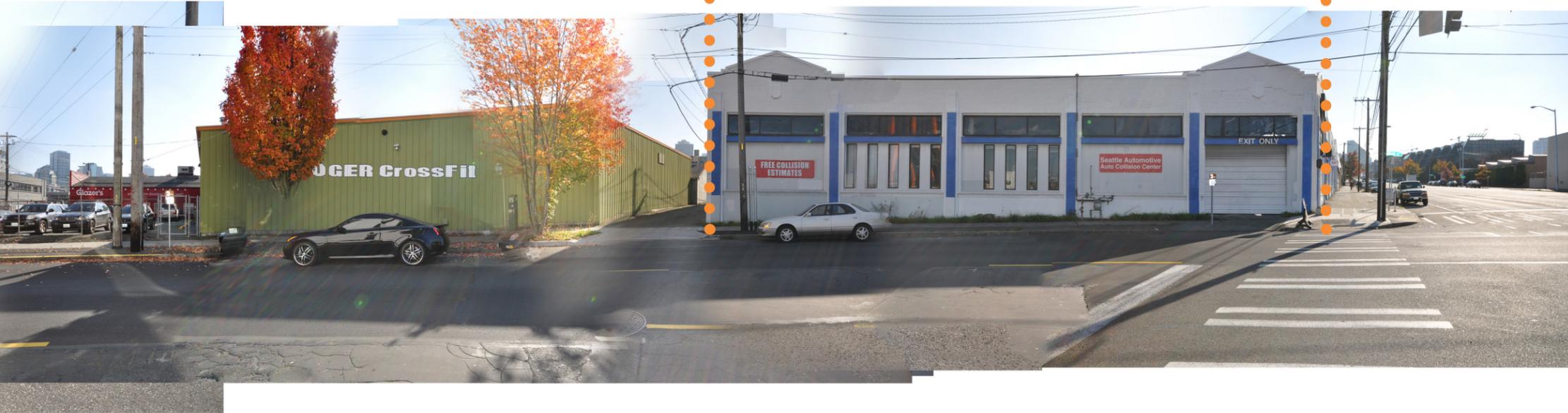
1 REPUBLICAN ST - SOUTH



ADJACENT TO SITE ACROSS REPUBLICAN

2 REPUBLICAN ST - NORTH

SITE



PERTINENT DESIGN GUIDELINES

CS1 - Natural Systems and Site Features

Use natural systems and features of the site and its surroundings as a starting point for project design.

CS2 - Urban Pattern and Form

Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces in the surrounding area.

CS3 - Architectural Context and Character

Contribute to the architectural character of the neighborhood.

PL1 - Connectivity

Complement and contribute to the network of open spaces around the site and the connections among them.

PL2 - Walkability

Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features.

PL3 - Street Level Interaction

Encourage human interaction and activity at the street-level with clear connections to building entries and edges.

DC1 - Project Uses and Activities

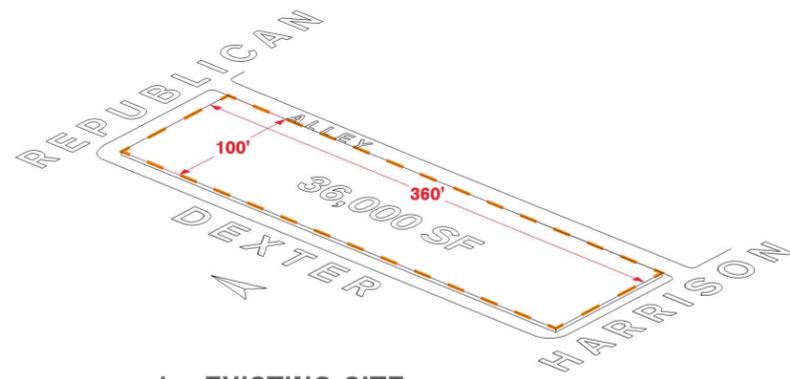
Optimize the arrangement of uses and activities on site.

DC2 - Architectural Concept

Develop an architectural concept that will result in a unified and functional design that fits well on the site and within its surroundings.

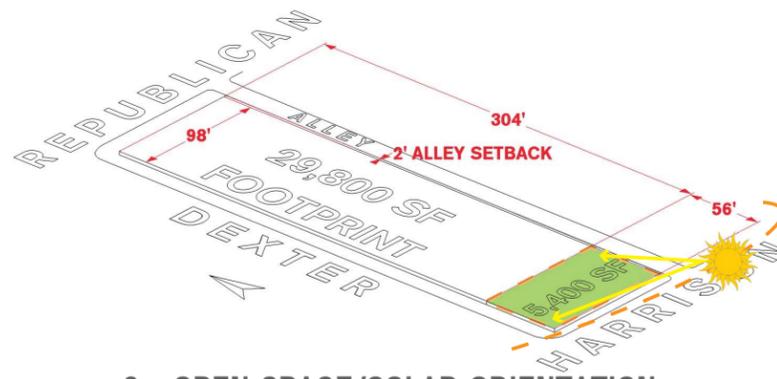
DC3 - Open Space Concept

Integrate open space design with the design of the building so that each complements the other.



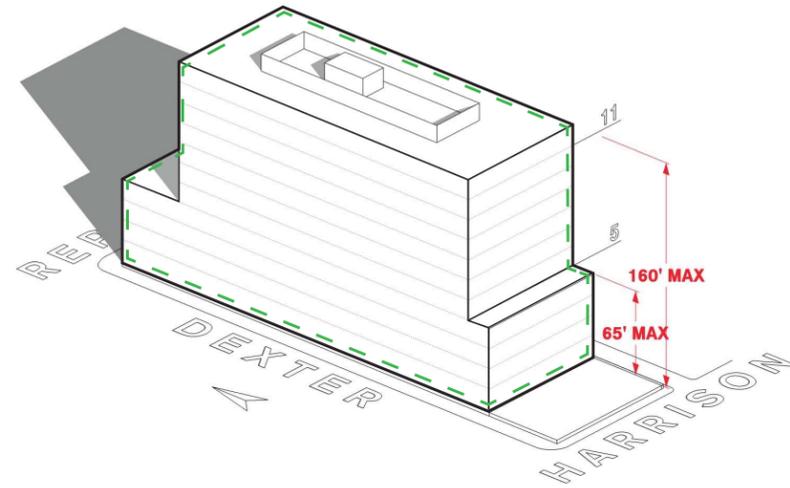
1 - EXISTING SITE

- THE SITE OCCUPIES 36,000 SF OF SPACE.



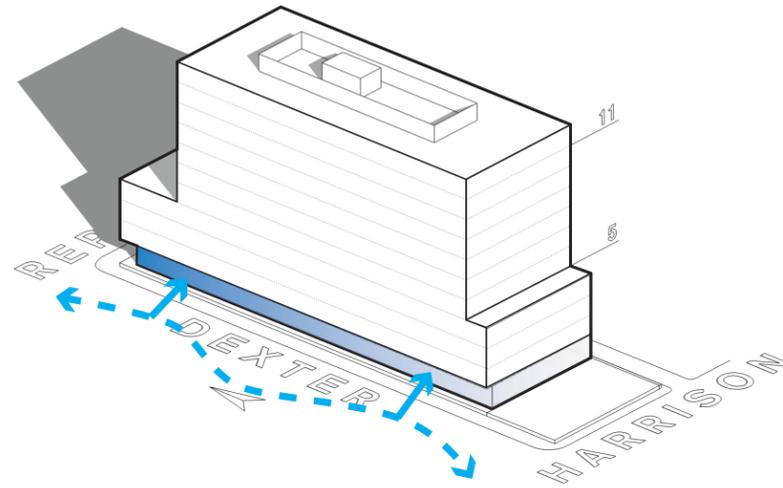
2 - OPEN SPACE/SOLAR ORIENTATION

- 15% OPEN SPACE THAT IS SOUTH FACING.



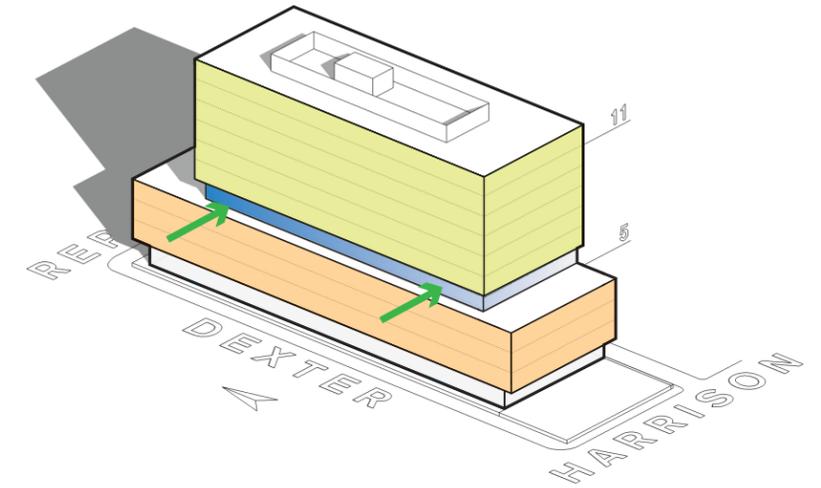
3 - ZONING ENVELOPE

- THE PROJECT INTENDS TO FILL MAJORITY OF ZONING ENVELOPE. (FULL F.A.R. OF 7)



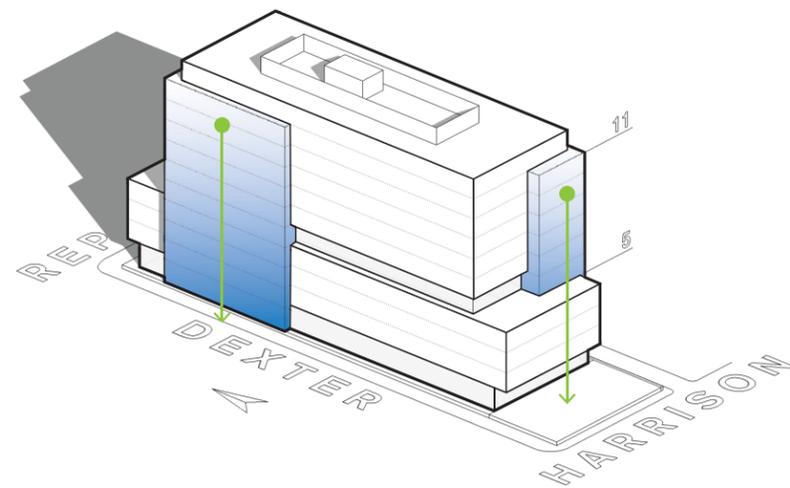
4 - DEXTER AVE

- THE BUILDING RESPONDS TO THE TRAFFIC ON DEXTER BY PROVIDING A SETBACK FOR A MORE GENEROUS SIDEWALK AND BETTER PEDESTRIAN EXPERIENCE.



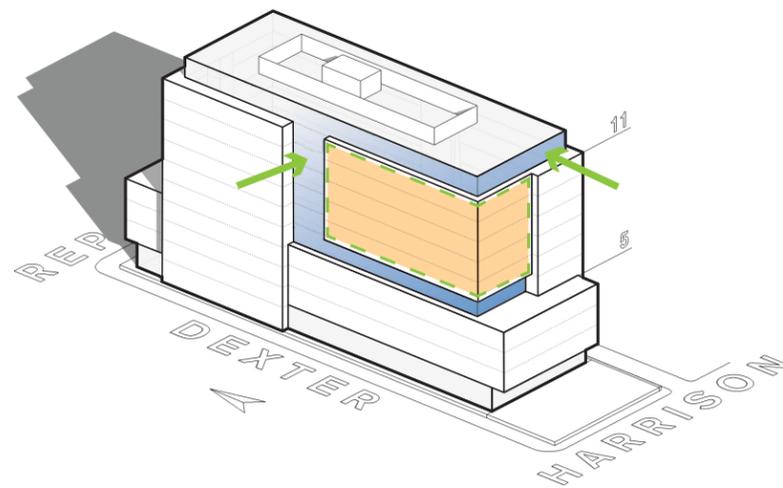
5 - BUILDING SCALE

- FILLING THE ZONING ENVELOPE CALLS FOR WAYS TO BREAK THE SCALE OF THE BUILDING DOWN INTO MULTIPLE PIECES. THE **PODIUM** AND THE **TOWER**.



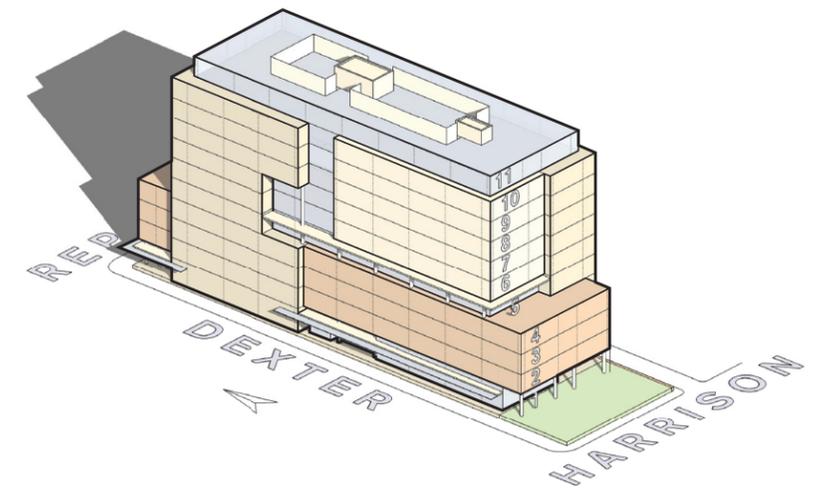
6 - GROUNDING ELEMENTS

- THE SEPERATION OF THE TOWER AT THE PODIUM LEVEL NEEDS TO BE GROUNDED TO THE STREET.



7 - TOWER SCULPTING

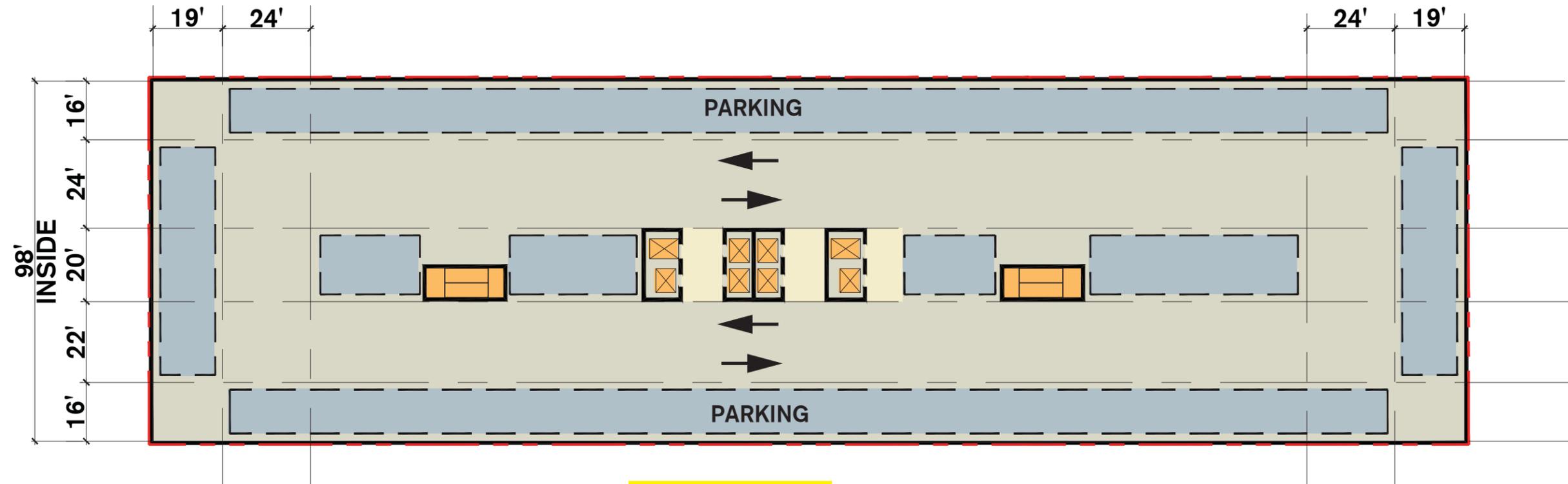
- THE LAND USE CODE REQUIRES UPPER LEVEL MODULATION.



8 - PREFERRED MASSING

- THE CUES FROM THE SURROUNDING AREA PROVIDE A RATIONAL, DIVERSE & ENGAGING MASSING.

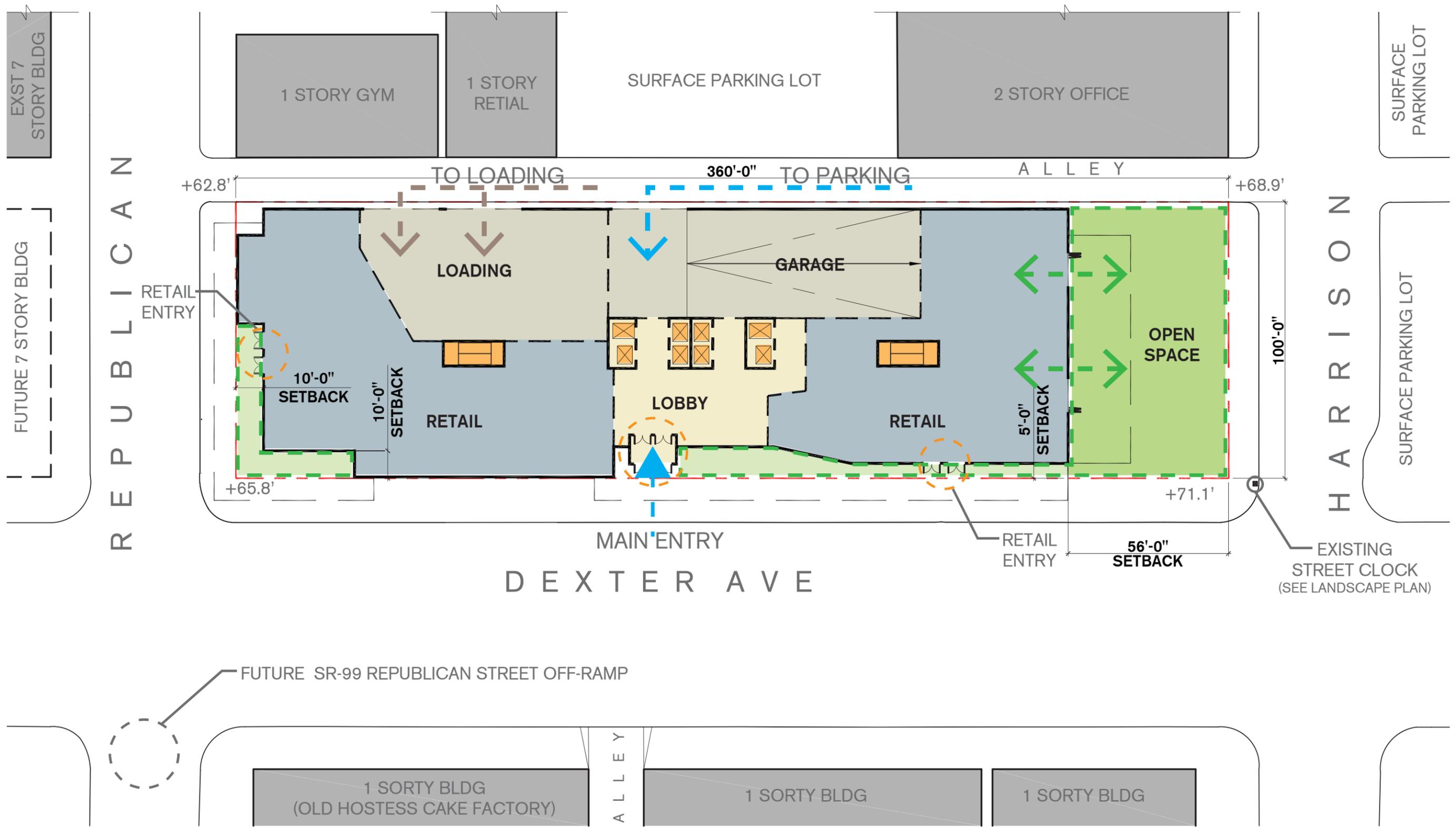
TYPICAL PARKING GARAGE PLAN (ALL OPTIONS)



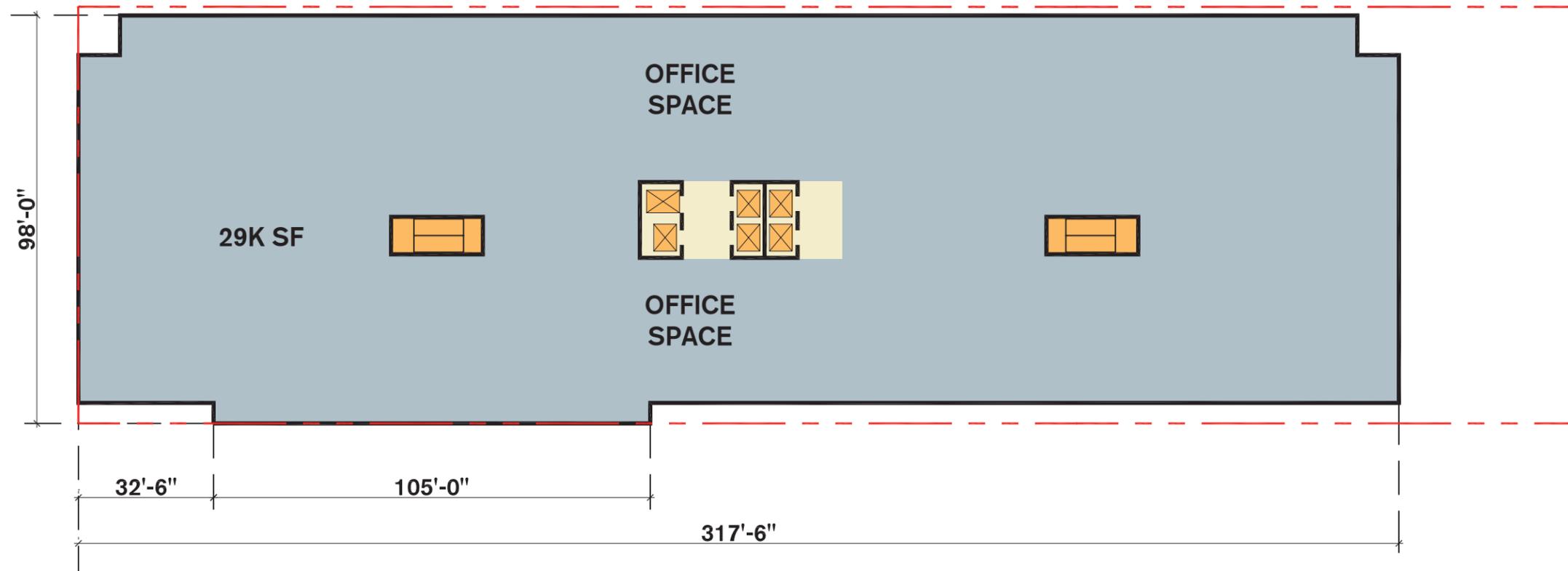
DEPARTURE FOR ALL OPTIONS
(SEE PG 44)

25% LARGE STALLS
75% MEDIUM/SMALL STALLS

TYPICAL LEVEL 1 FLOOR PLAN (ALL OPTIONS)

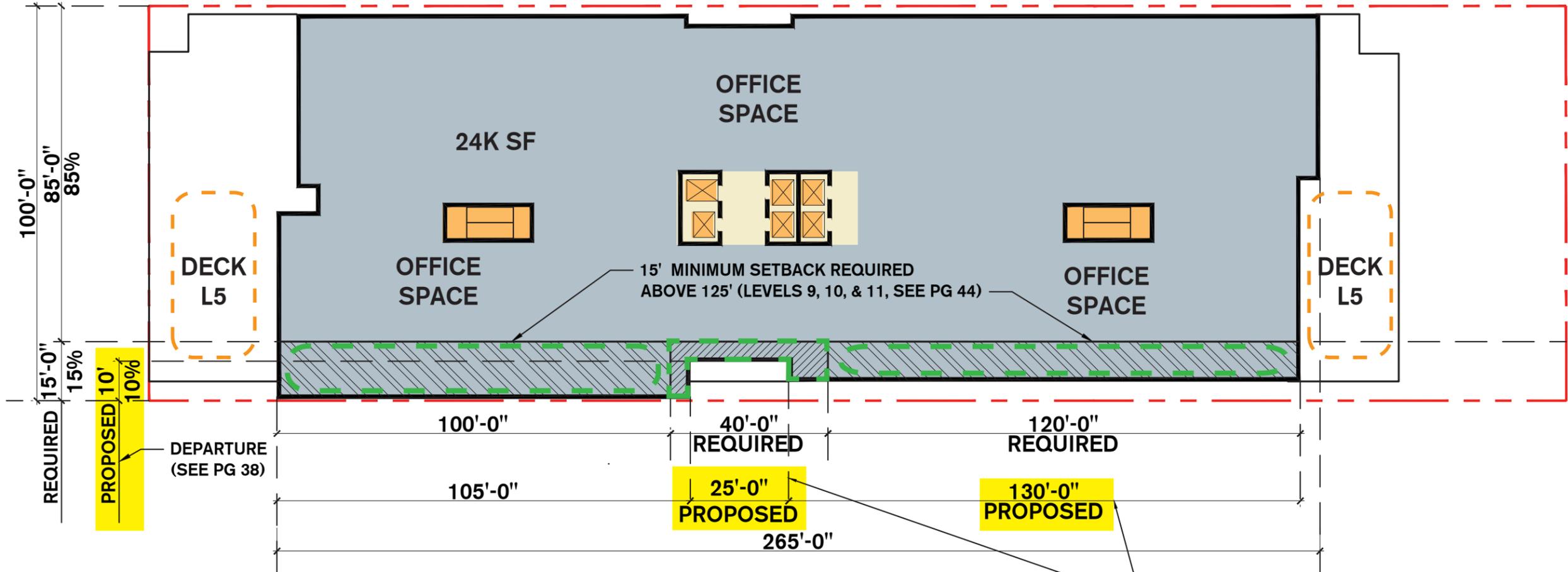


TYPICAL PODIUM LEVEL PLAN (ALL OPTIONS)

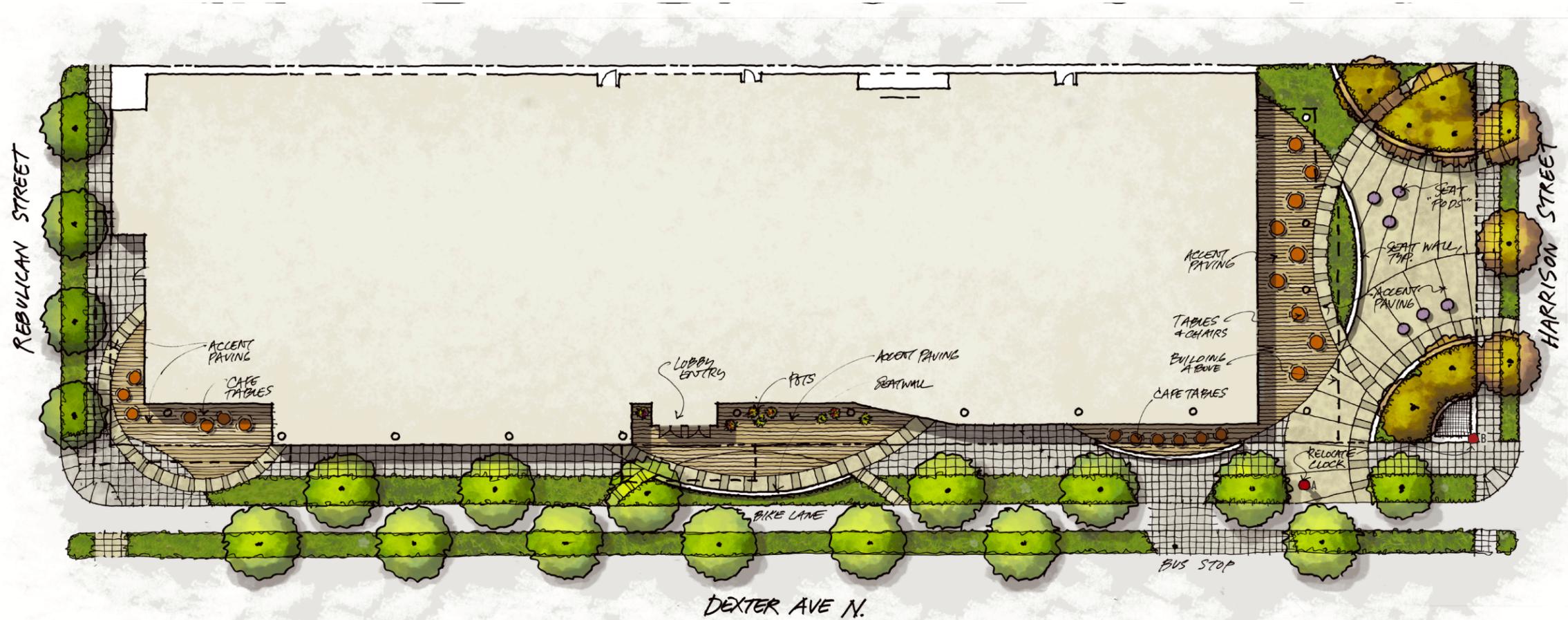


TYPICAL TOWER FLOOR PLAN (OPTIONS 2 & 3)

Illustrating Potential Design Departure Request
(Upper Level Modulation)



LANDSCAPE PLAN - GROUND LEVEL

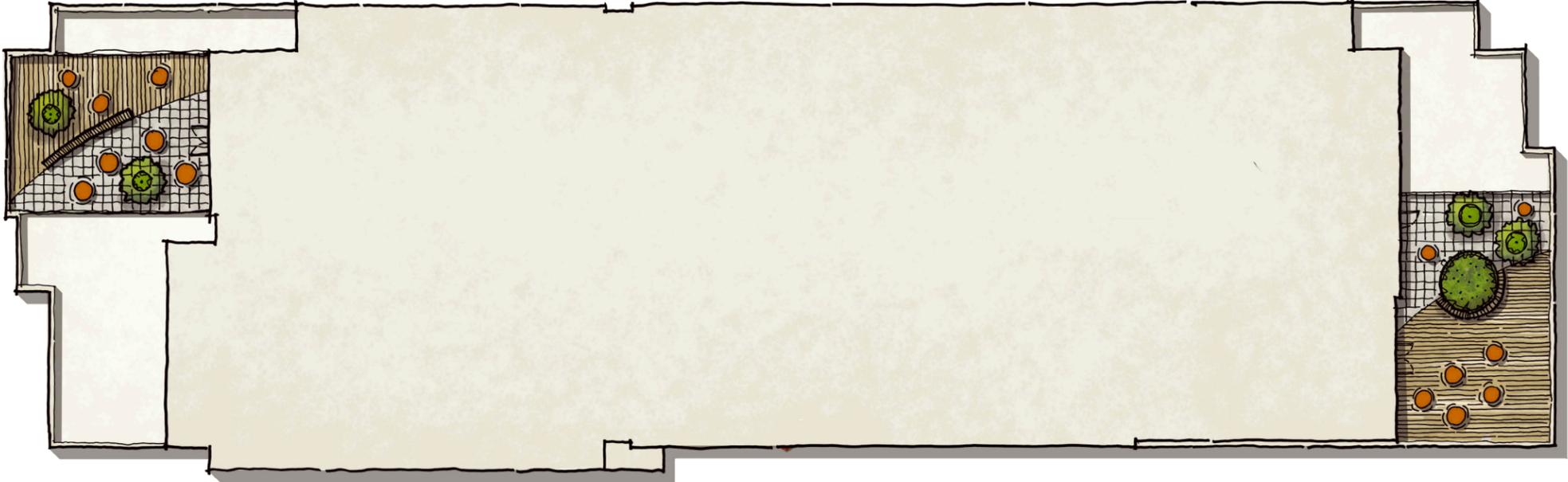


400 DEXTER AVE N. - "SITE CONCEPT PLAN"
 WDG - FEBRUARY 27, 2014 1" = 10'

The landscape concept for the project takes advantage of its site in a way that offers welcoming, vibrant, and highly useable pedestrian spaces. To the south, a large landscaped plaza, envisioned as an outdoor room, will provide areas for informal gathering, sunny places to sit and a new home for the existing historic clock. The curved, interlocking forms of the plaza seat walls, planters and paving, take cues from the internal gears of a clock and are intended to provide a sense of whimsy for people within and above the space. The plaza will have a

gentle east to west slope to provide seamless accessibility from sidewalks on the south and west sides. A large planting area on the eastern side of the plaza is intended to provide screening of alley views and activities. New planting areas and street trees in the Harrison Street right-of-way will help complete the space.

The curved forms continue along the western side of the project adjacent Dexter Avenue North, where they provide additional seating opportunities and announce the main lobby and retail entrances. Pulling the southern half of the building back from the right-of-way creates opportunity for generous paved and landscaped areas. This additional width can be used for café tables, pots, bike racks, and other street furnishings to enliven the pedestrian experience. Incorporation of the proposed bike lane and landscaped median along Dexter



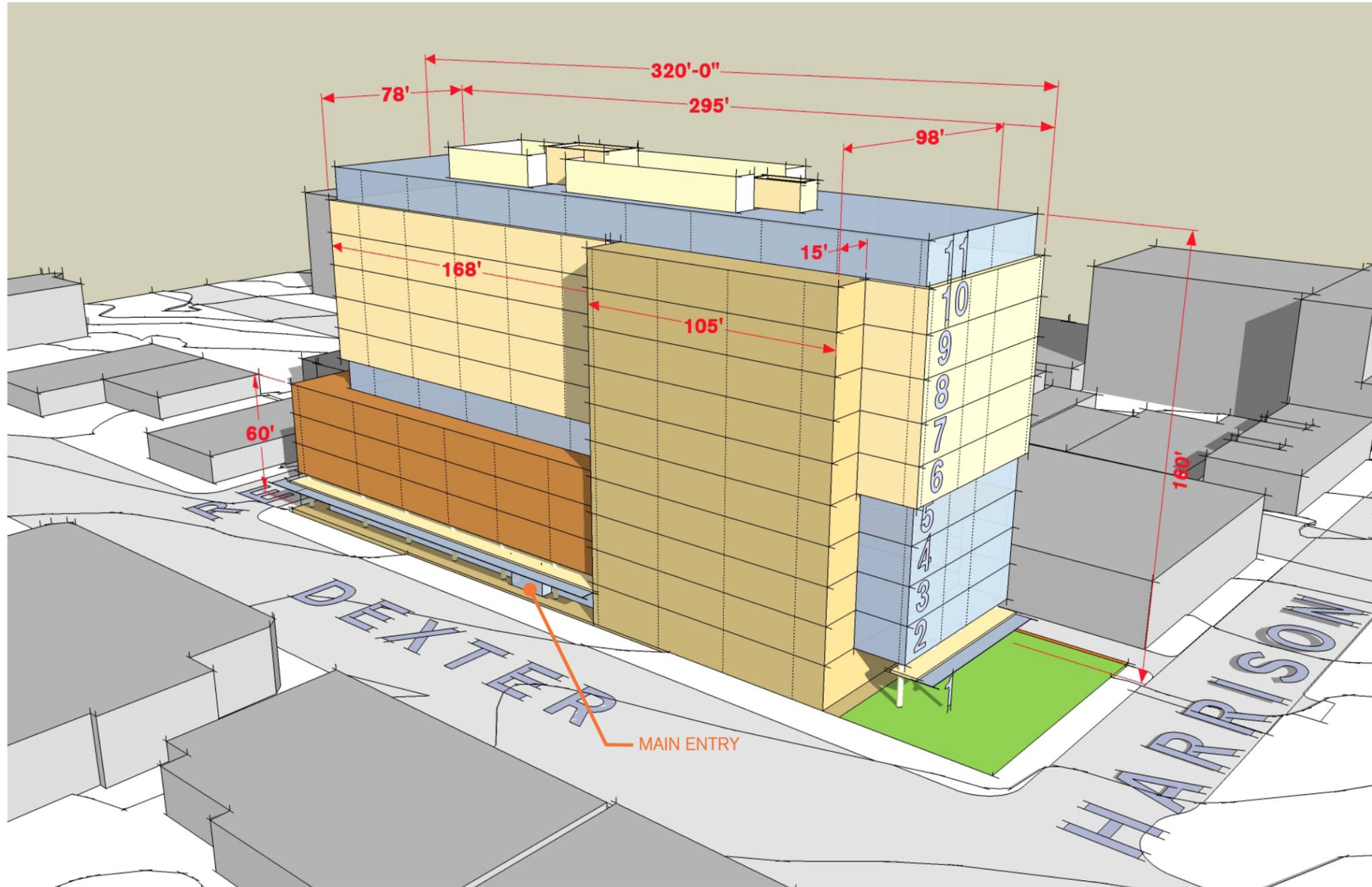
Avenue North provides a second layer of planting and street trees and will transform the current bleak streetscape to one that is lush and layered.

to the street life. New planting areas and street trees will be provided in the right-of-way adjacent Republican Street and to offer buffering and shade for pedestrians.

By again stepping the building back, plaza space is provided at the NW corner of the site. Materials and forms developed for the southern and western spaces will continue here providing continuity for the entire block. More café tables and street furnishings are pictured to be at this location adding

An upper deck on the south side of Level 5 provides additional outdoor area and visual connection to the street. Gathering spaces, tables and chairs, and landscaping will provide an inviting place with views and great southern sun exposure.

OPTION 1



MASSING OPTION 1 (CODE COMPLIANT)

- Thin and elongated tower floor plate (295'L x 78'W)
- Grounding elements to the south

FEATURES

- 10 floors of office space + ground level retail
- Ground floor will have retail and amenity spaces
- Green space located at street level at south end of building
- Long and slender floor plates provide exceptional daylighting for the office uses
- The massing is code compliant and provides a 15' setback from the street lotline

CHALLENGES

- A slender floor plate at 78' wide does not provide a viable depth for office planning
- Although the massing is code compliant, the 15' setback causes the building to be 30' longer than options 2 & 3
- The 15' setback forces the massing to have less modulation than options 2 & 3 in order to accommodate the desired square footage
- The length required to reach the desired square footage doesn't allow for generous setbacks on the North and South sides of the site

AERIAL VIEW FROM SOUTHWEST

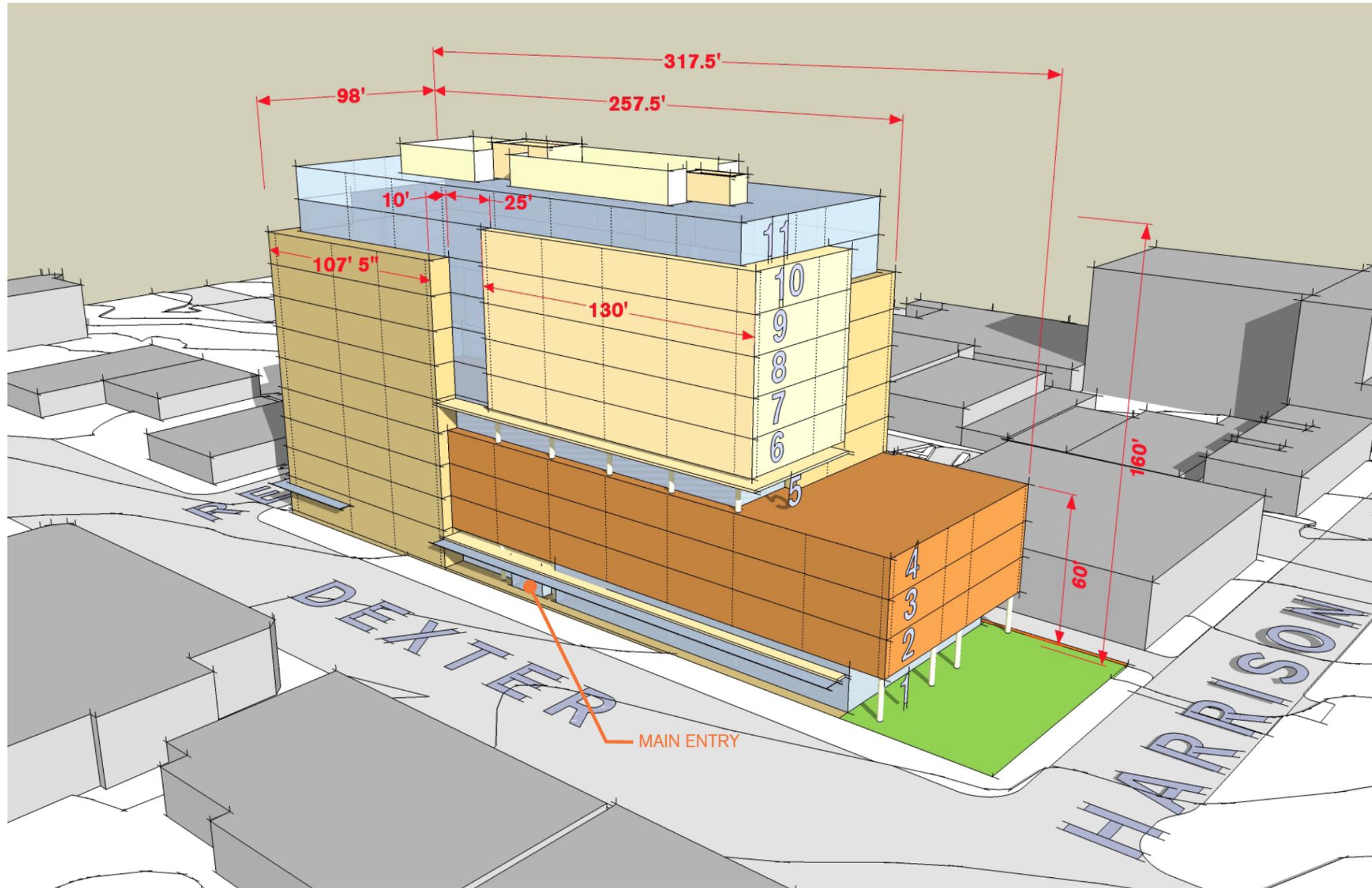


AERIAL VIEW FROM NORTHWEST



AERIAL VIEW FROM THE SOUTHEAST

OPTION 2



MASSING OPTION 2

- Departure Required (See pg38-39)
- More compact tower floor plate (258'L x 98'W)
- Grounding elements to the North

FEATURES

- 10 floors of office space + ground level retail
- Ground floor will have retail and amenity spaces
- Green space located at street level at south end of building
- A more compact tower floor plate than Option 1
- Generous tower setback on the south side of the site
- More mid-block modulation that meets the intent of the land use code, while also providing opportunities for exterior material changes
- A powerful and grounded massing element on the north side of the site

CHALLENGES

- The massing does not comply with the upper level modulation requirements (see design departures). This may require additional gestures to attain the differentiation sought by the landuse code
- Placing the grounding elements to the North side of the site does not provide a ground level setback at the intersection of Dexter & Republican
- Placing the grounding elements to the north does not allow for a tower setback along Republican

AERIAL VIEW FROM SOUTHWEST

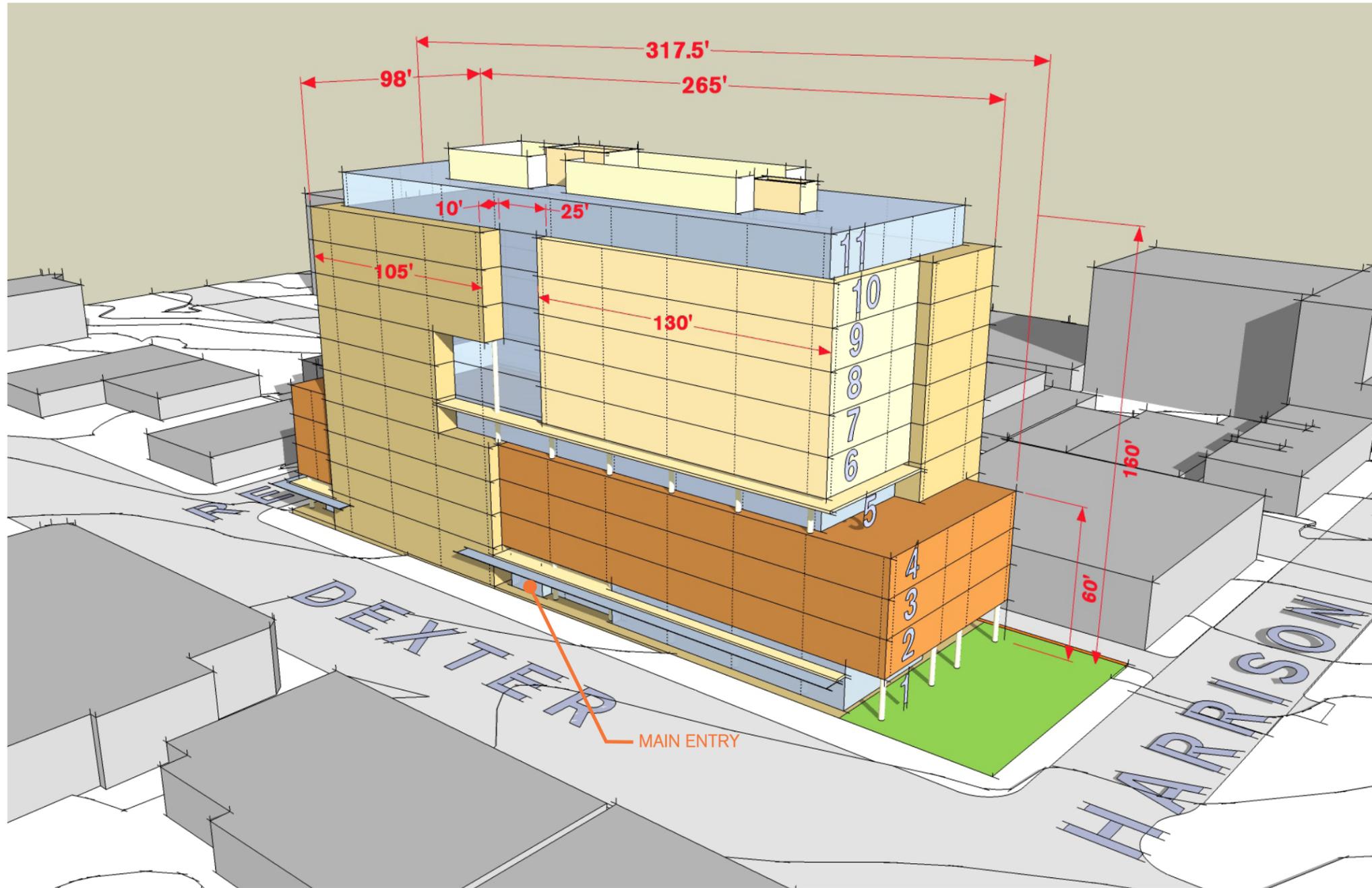


AERIAL VIEW FROM NORTHWEST



AERIAL VIEW FROM THE SOUTHEAST

OPTION 3 - PREFERRED



AERIAL VIEW FROM SOUTHWEST

MASSING OPTION 3 (PREFERRED)

- Departure Required (See pg38-39)
- More compact tower floor plate (265'L x 98'W)
- Grounding elements more central to the site

FEATURES

- 10 floors of office space + ground level retail
- Ground floor will have retail and amenity spaces
- Green space located at street level at south end of building
- A more compact tower floor plate than Option 1
- Provides a tower setback on both the north and south sides of the site
- More mid-block modulation that meets the intent of the land use code, while also providing opportunities for exterior material changes
- A powerful and grounded massing element located mid-block that helps break up the long podium facade, and allows for the NW and SW corners of the site to have different identities
- Generous tower setback above the podium level

CHALLENGES

- The massing does not comply with the upper level modulation requirements (see design departures). This may require additional gestures to attain the differentiation sought by the landuse code



AERIAL VIEW FROM NORTHWEST



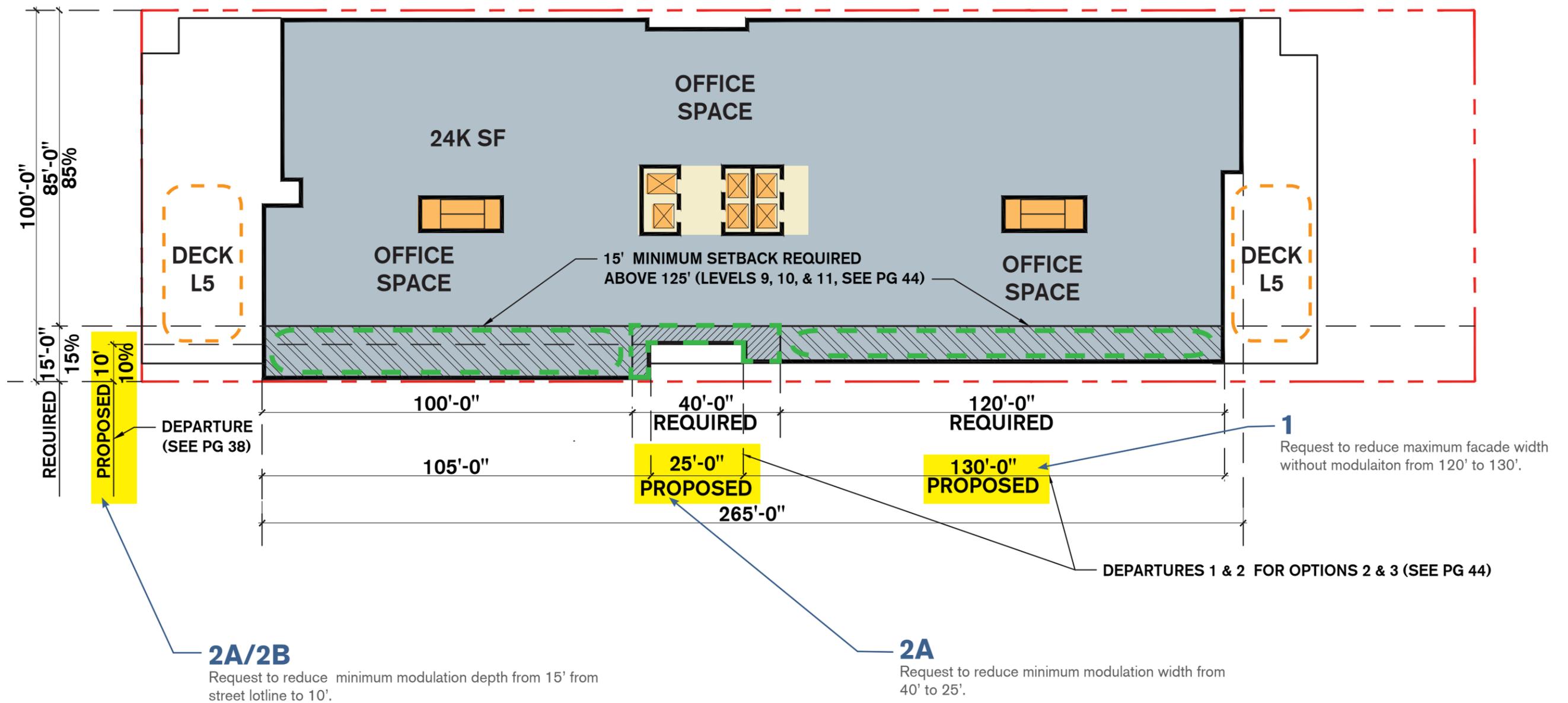
AERIAL VIEW FROM THE SOUTHEAST

POTENTIAL DESIGN DEPARTURES

ITEM	REQUIREMENT	REQUEST	RATIONALE						
1	<p>23.48.013.D.1</p> <p>FACADE MODULATION - MAXIMUM FACADE LENGTH</p> <p>The maximum length of a facade without modulation is prescribed in Table B for 23.48.013, Facade Modulation. This maximum length shall be measured parallel to each street lot line, and shall apply to any portion of a facade (between 65' and 125' in height), including projections such as balconies, that is located within 15 feet of street lot lines.</p> <p>Table B for 23.48.013 Facade Modulation</p> <table border="1"> <thead> <tr> <th>Height of structure</th> <th>Maximum length within 15 feet of street lot line</th> </tr> </thead> <tbody> <tr> <td>Up to 125 feet</td> <td>150 feet</td> </tr> <tr> <td>Above 125 feet</td> <td>120 feet</td> </tr> </tbody> </table>	Height of structure	Maximum length within 15 feet of street lot line	Up to 125 feet	150 feet	Above 125 feet	120 feet	<p>1.</p> <p>Increase the maximum facade width for the portion above 125' (Levels 9, 10 & 11) from 120' in width to 130 in width (increase of 9%).</p>	<p>The required maximum facade width of 120' does not work well with the proposed structural bay spacing of 30' since the exterior wall needs to sit in front of the the structural frame. Additionally, this reduces the proposed modulation on the south side of the tower that helps provide relief and differentiation in material. (see pages 45 & 46)</p>
Height of structure	Maximum length within 15 feet of street lot line								
Up to 125 feet	150 feet								
Above 125 feet	120 feet								
2	<p>23.48.013.D.2</p> <p>2A - FACADE MODULATION - WIDTH & DEPTH</p> <p>If a portion of a facade that is within 15 feet of the street lot line is the maximum length permitted for an un-modulated facade, the length of the facade may be increased only if additional portions of the facade set back a minimum of 15 feet from the street lot line for a minimum distance of 40 feet. If the required setback is provided, additional portions of the facade may be located within 15 feet of the street lot line.</p>	<p>2A.</p> <p>For the 1.5 stories above 125' (Levels 9,& 10), we request the code required modulation depth and width be reduced from a slot 15' minimum deep x 40' ft minimum long, to a slot 10' minimum deep x 25' long.</p> <p>2B.</p> <p>For the top floor above 125' (Level 11), we request the code required modulation depth and width be reduced from a slot 15' minimum deep x 40' ft minimum long, to a minimum 10' setback for the entire length of facade.</p>	<p>2A.</p> <p>The modulation width of 40' required by code adversely interupts the 30' structural bay spacing of the proposed design. Additionally, the modulation depth of 15' negatively impacts the depth of a leasable floor plate by reducing the distance between the elevator core and exterior wall to 25'. The proposed design requests this to be reduced to 10' to maintain 30' between the elevator core and exterior wall. (see pages 45-47)</p> <p>2B.</p> <p>The modulation depth of 15' negatively impacts the depth of a leasable floor plate by reducing the distance between the elevator core and exterior wall to 25'. The proposed design requests this to be reduced to 10' to maintain 30' between the elevator core and exterior wall. (see pages 45-47)</p>						
3	<p>23.54.030.B.2.c</p> <p>MINIMUM OF 35% LARGE PARKING STALLS</p> <p>A minimum of 35% of the off street parking stalls required by the SMC shall be large parking stalls</p>	<p>3.</p> <p>Request that the minimum large parking stall percentage be reduced to 25%.</p>	<p>The narrowness of the site dictates how below grade parking can be used to provide an efficient parking garage and avoid excavating further. We can only provide a 25% distribution of large parking stalls, given the limited locations where the project can accommodate a 19' deep parking stall depth. In order to provide the code required large stall %, the central structural bay at gridlines D & E, measuring 17'-6", would have to be reduced to less than 15', making it impossible to provide enough width to accomodate two elevators. (See departure Diagram 3, pg 48).</p>						

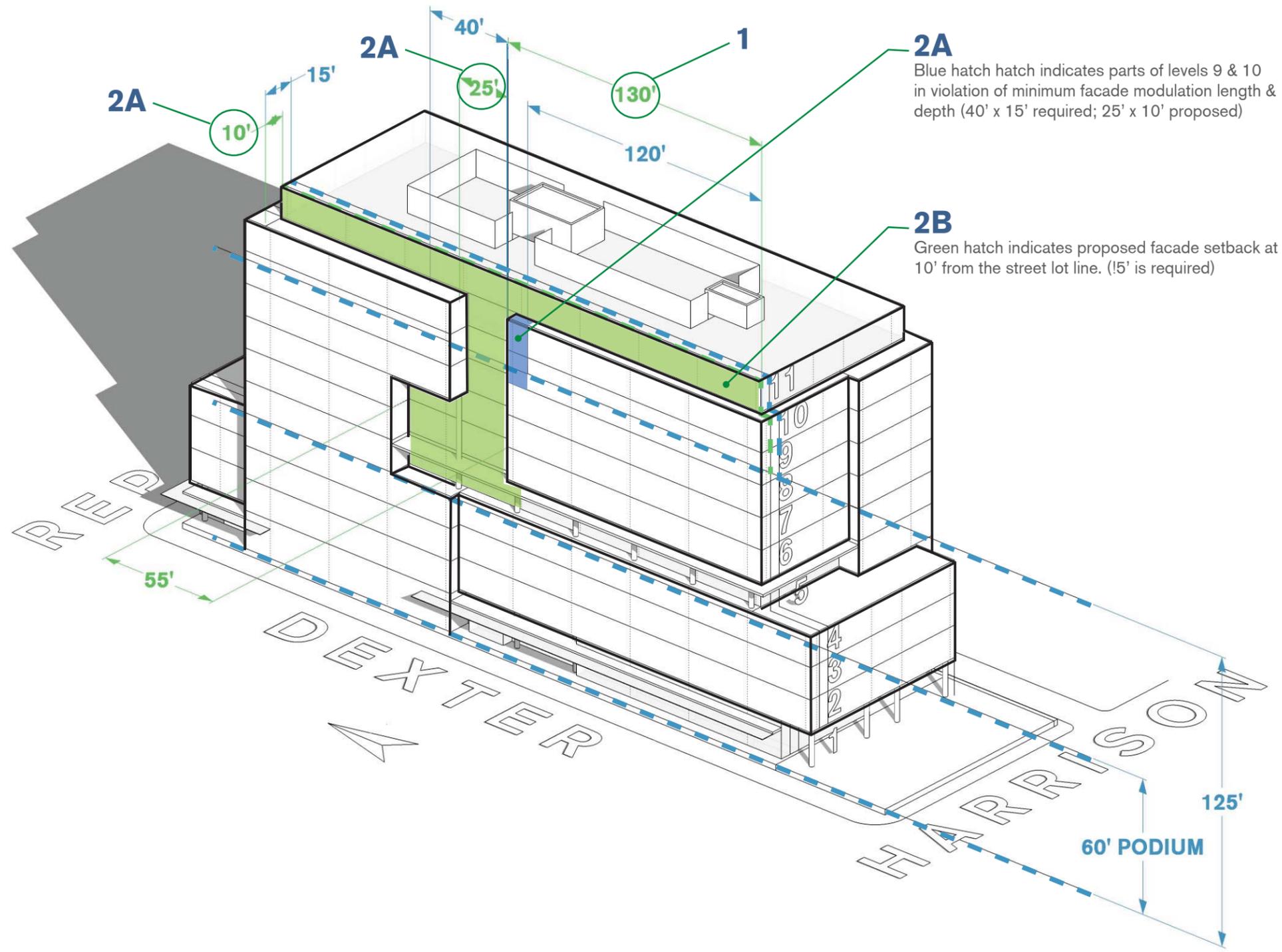
FACADE MODULATION - TYPICAL TOWER FLOOR PLAN

1, 2A, 2B



DEPARTURE DIAGRAMS

1, 2A, 2B FACADE MODULATION - 3D VIEW



1. Increase the maximum facade width for the portion above 125' (Levels 9, 10 & 11) from 120' in width to 130 in width (increase of 9%).

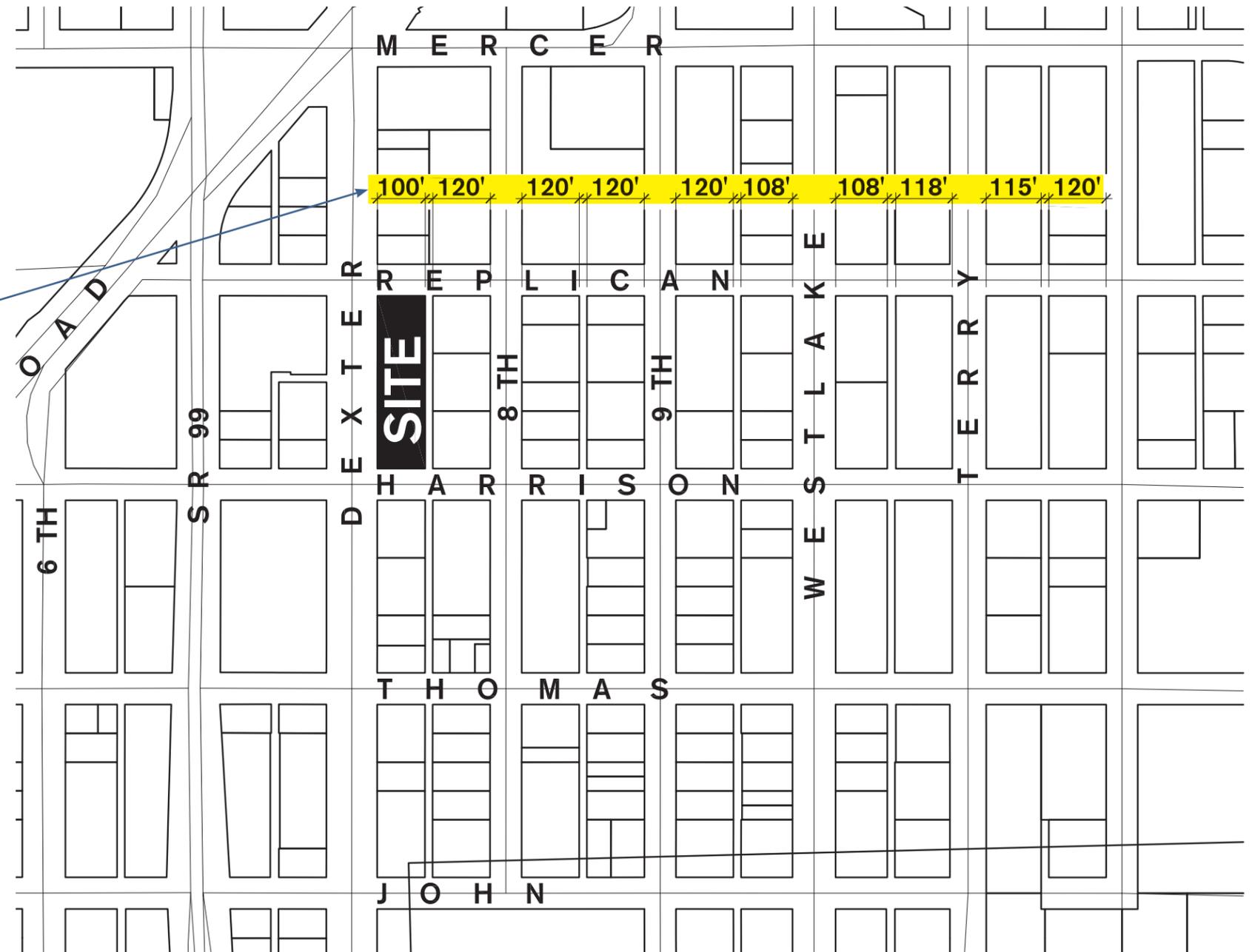
2A Blue hatch indicates parts of levels 9 & 10 in violation of minimum facade modulation length & depth (40' x 15' required; 25' x 10' proposed)

2B Green hatch indicates proposed facade setback at 10' from the street lot line. (15' is required)

2A. For the 1.5 stories above 125' (Levels 9,& 10), we request the code required modulation depth and width be reduced from a slot 15' minimum deep x 40' ft minimum long, to a slot 10' minimum deep x 25' long.

2B. For the top floor above 125' (Level 11), we request the code required modulation depth and width be reduced from a slot 15' minimum deep x 40' ft minimum long, to a minimum 10' setback for the entire length of facade.

FACADE MODULATION - COMPARATIVE SITE DIMENSIONS IN THE AREA **2B & 3**
 (SUPPORTING INFO)

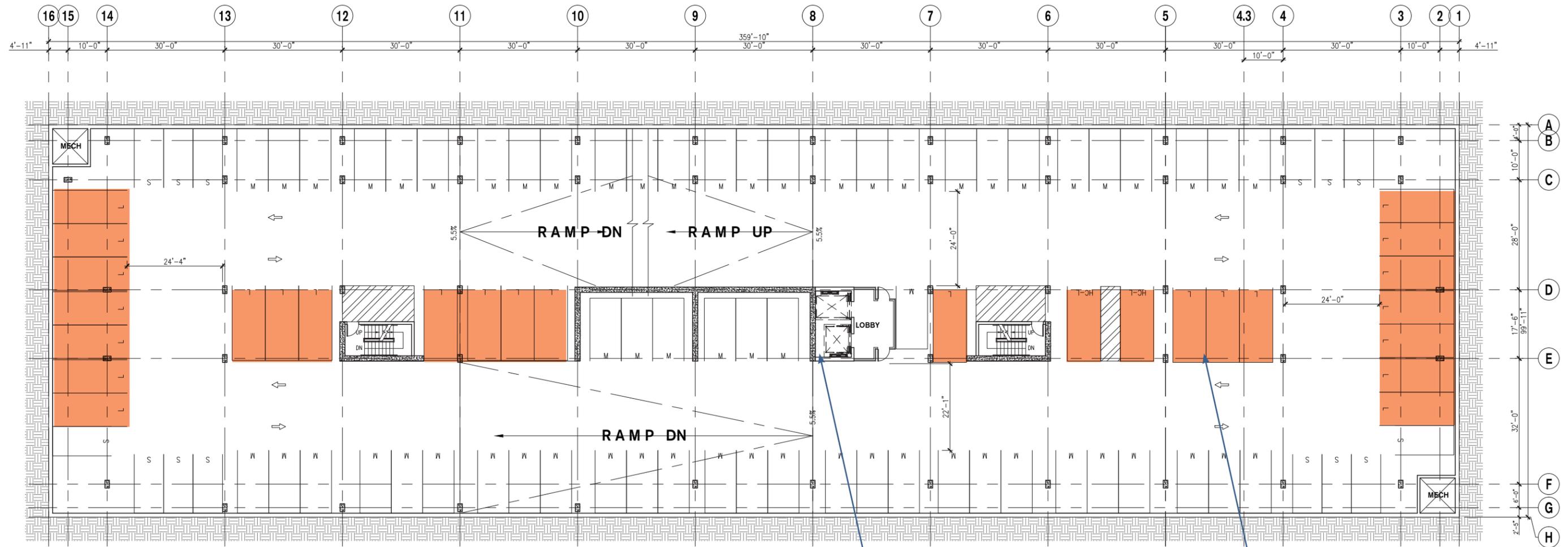


2B & 3

Other sites within the South Lake Union urban center have a more generous dimension in the east/west direction. This proposed site has a width of 100', which is anywhere between 10-15' smaller in width than neighboring properties. This provides challenges with accommodating modulation requirements and large parking stalls below grade.

DEPARTURE DIAGRAMS

3 MINIMUM 35% LARGE PARKING STALLS



35% Code required = 35 large spaces per parking level floor plate

25% Request = 25 large spaces per parking floor plate

3

In order to provide code required large parking stalls, the structural bay between E & D would need to be reduced to less than 15', which would not be wide enough to accommodate two elevators.

3

Orange hatch indicates the locations in the garage that can accommodate large parking stalls

