



435 DEXTER



January 08, 2014

FISH
MACKAY
ARCHITECTS LLC



Early Design Guidance

DPD Project# 3016347





NEIGHBORHOOD AERIAL VIEW

435 Dexter

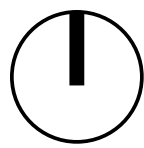
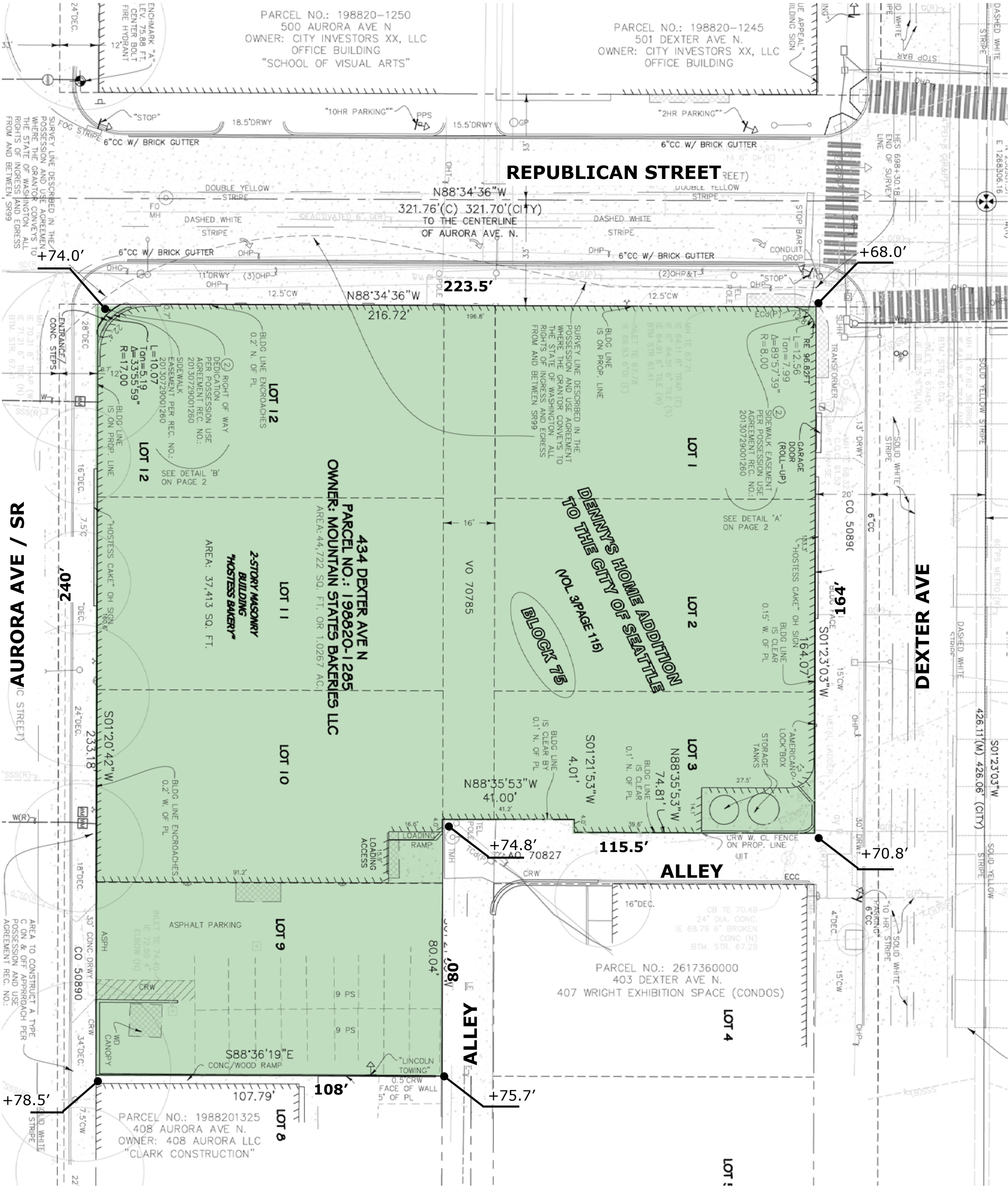
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HOSTESS BUILDING FROM SR 99 & REPUBLICAN ST



HOSTESS BUILDING FROM DEXTER AVE



EXISTING SITE

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Development Objectives

- 294 Apartments
- 248 Parking stalls
- 70' Height from average grade
- 7 Stories above grade

Project Goals

- Provide a building that responds to the changing architectural character and uses of the neighborhood
- Respond to the prominent edge condition of the reconfigured SR 99 corridor
- Provide a safe, functional, attractive pedestrian environment along Dexter Ave



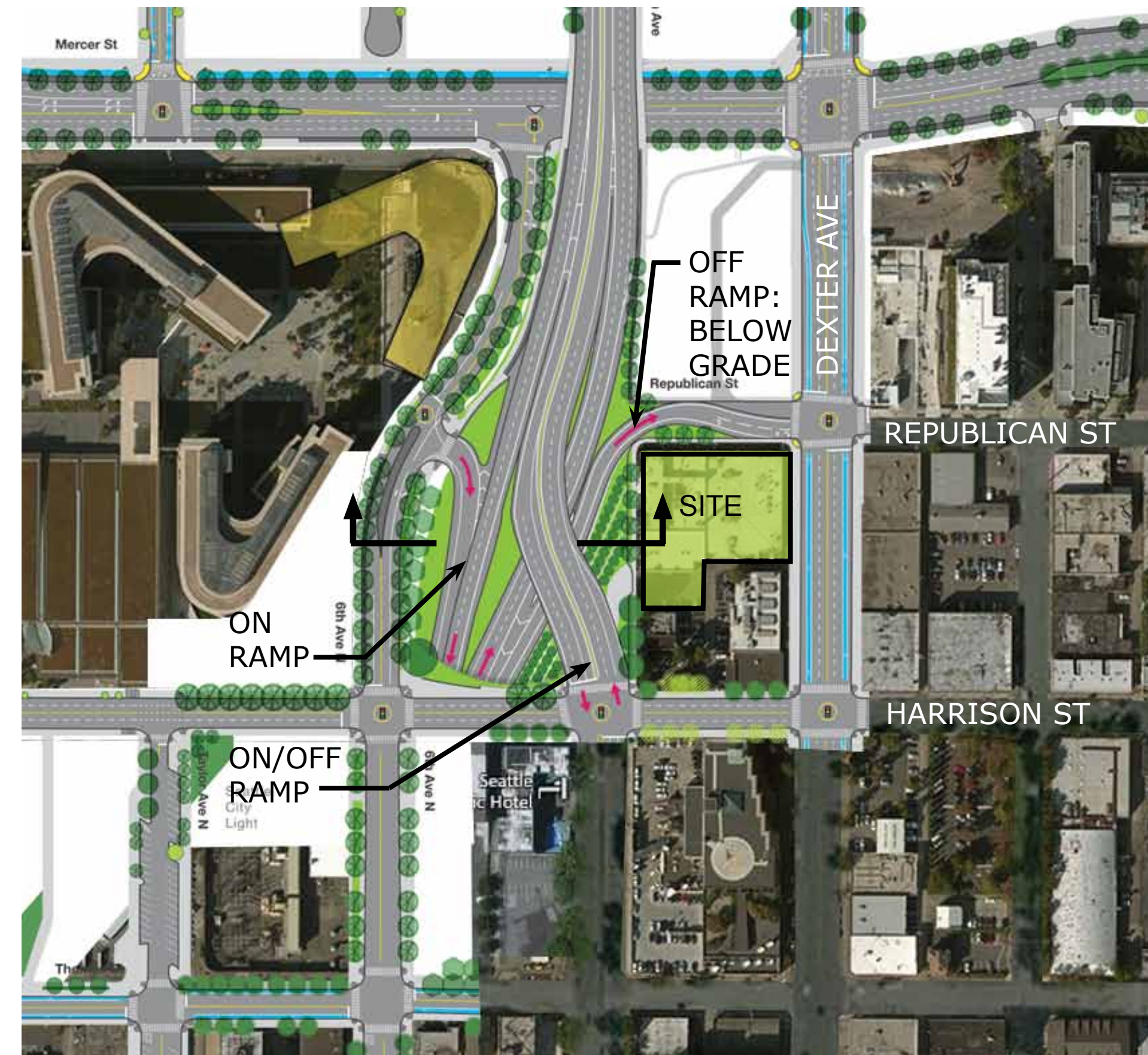
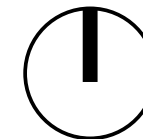
SITE PLAN

435 Dexter

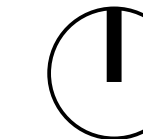
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EXISTING ROAD CONDITIONS



FUTURE NORTH PORTAL

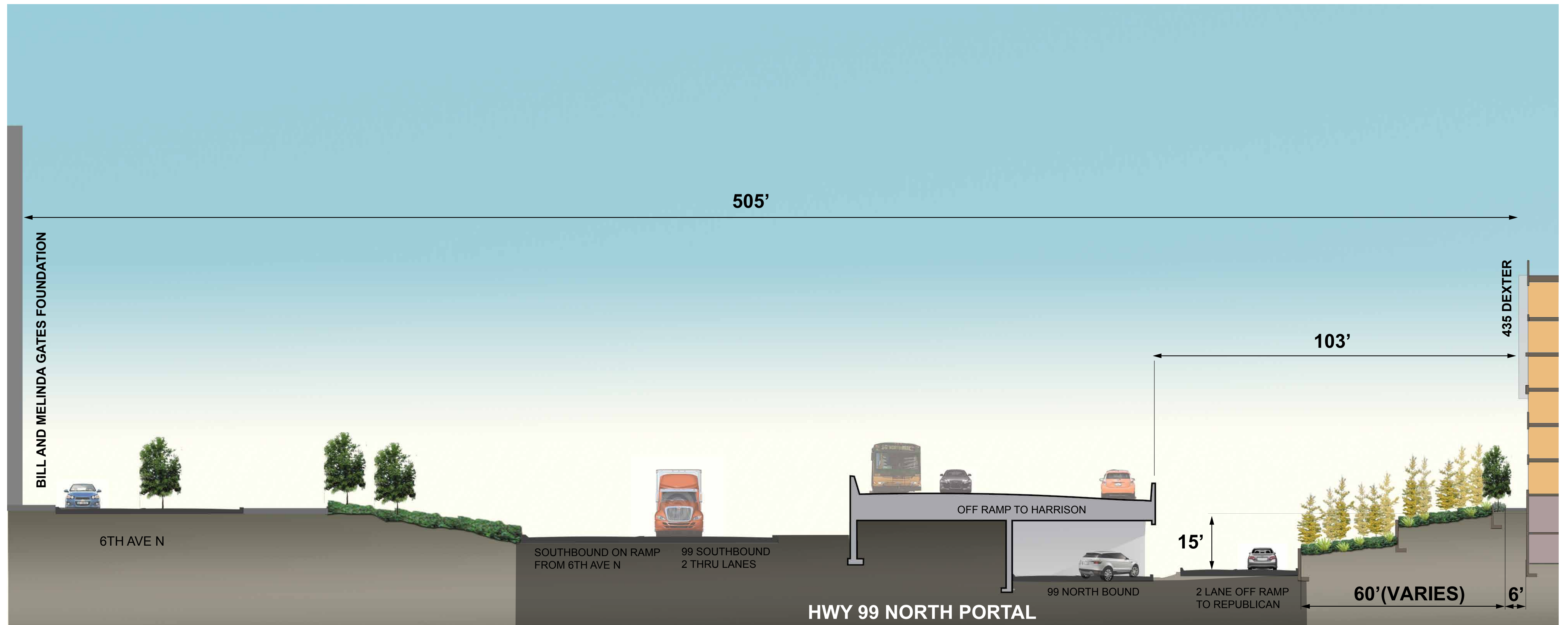


PERSPECTIVE VIEW LOOKING SOUTH

SR 99 NORTH PORTAL PROJECT

435 Dexter

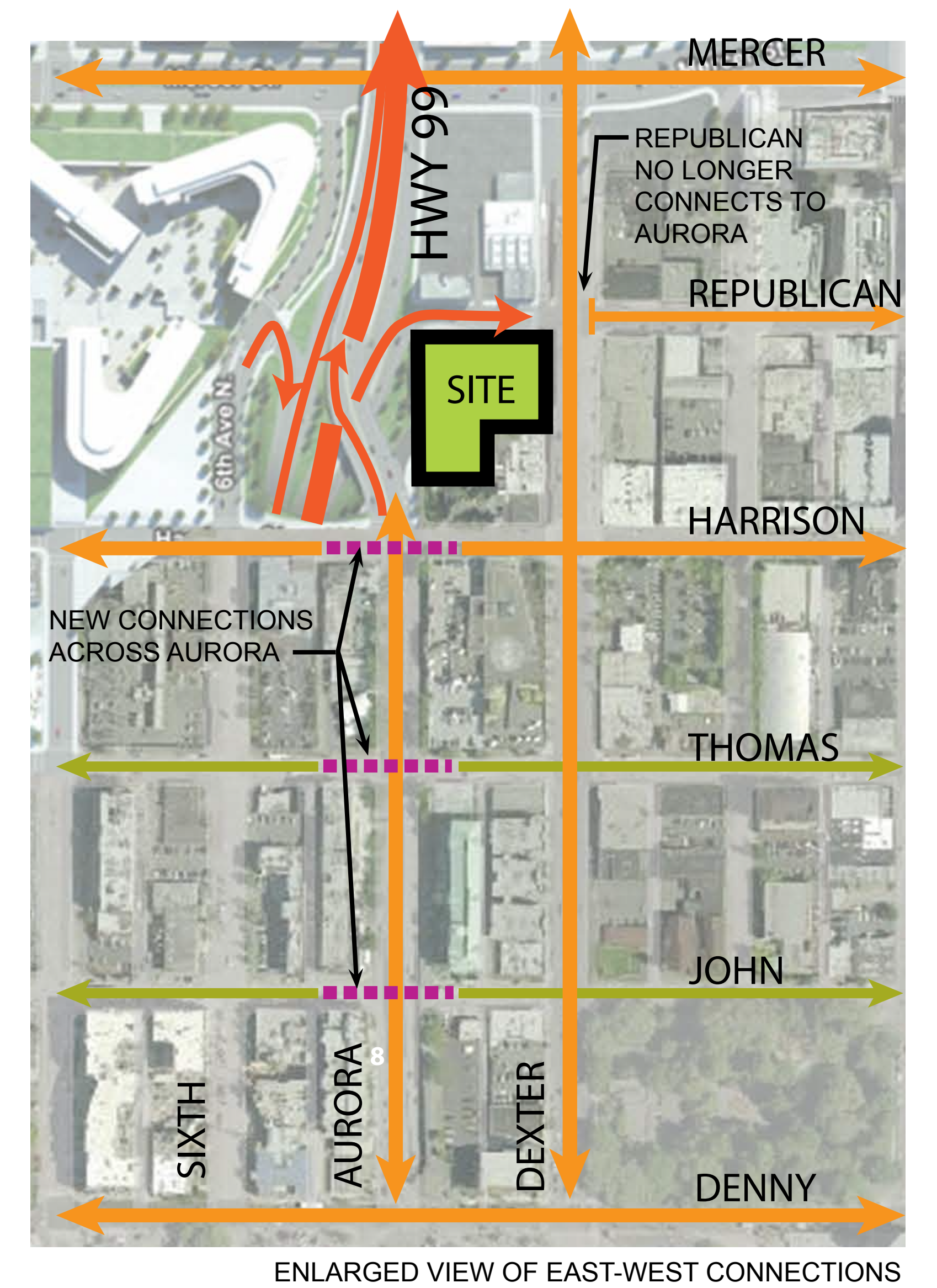
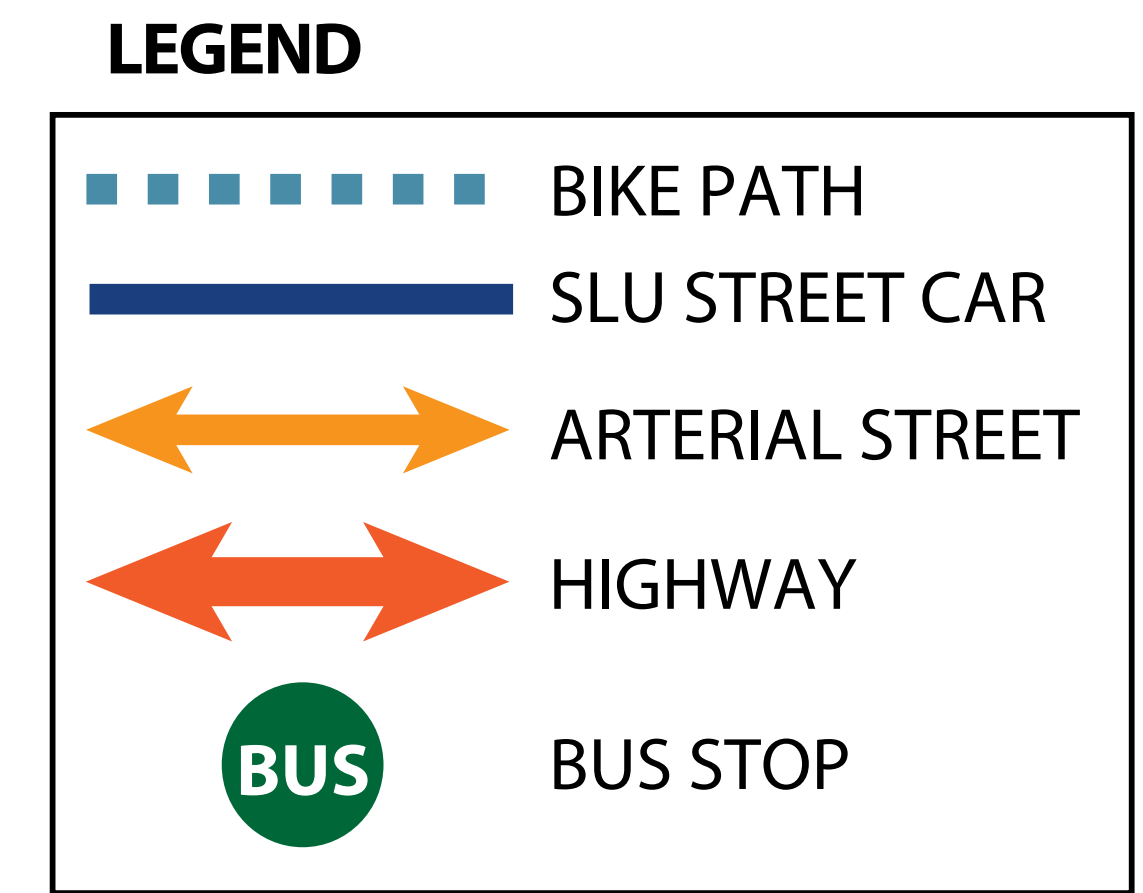
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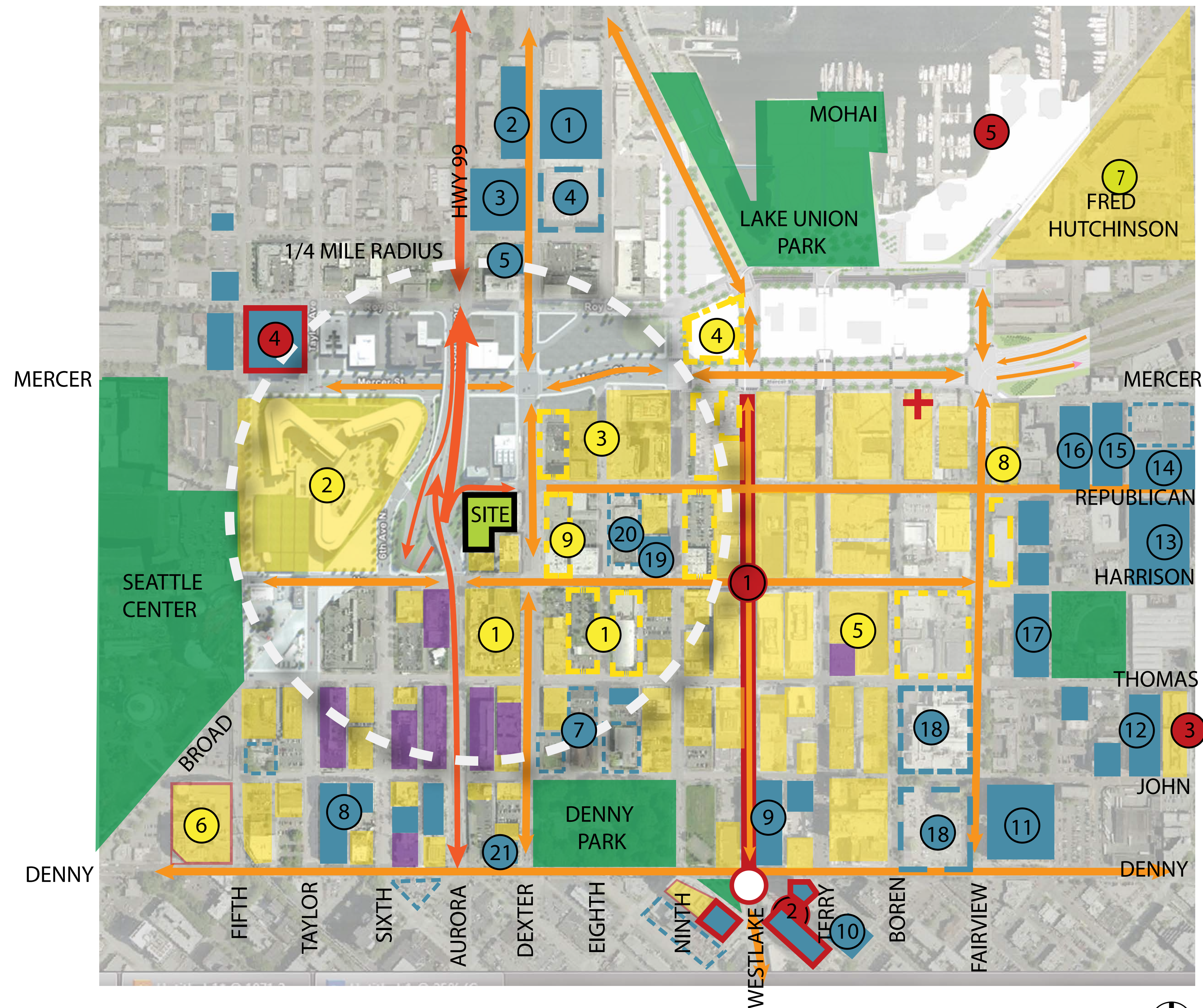
SR 99 NORTH PORTAL PROJECT: SECTION

435 Dexter

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SITE ANALYSIS: TRANSPORTATION & CIRCULATION



COMMERCIAL & INSTITUTIONAL

- 1 KING 5 TV
- 2 BILL AND MELINDA GATES FOUNDATION
- 3 UW MEDICINE LABS
- 4 FUTURE ALLEN INST. FOR BRAIN SCIENCE
- 5 AMAZON HEADQUARTERS
- 6 FISCHER PLAZA
- 7 300 & 333 8TH AVE N
- 8 500 FAIRVIEW
- 9 FUTURE OFFICE AND LAB SPACE

RESIDENTIAL

- 1 NEPTUNE APARTMENTS
- 2 UNION APARTMENTS
- 3 TRUE NORTH APARTMENTS
- 4 FUTURE MULTIFAMILY
- 5 HUE APARTMENTS
- 6 NOT USED
- 7 777 THOMAS APARTMENTS
- 8 TAYLOR 28 APARTMENTS
- 9 ROLLINS STREET APARTMENTS
- 10 2200 WESTLAKE HOTEL, CONDOS AND RETAIL
- 11 MIRABELLA SENIOR COMMUNITY
- 12 ALLEY 24 APARTMENTS
- 13 STACKHOUSE APARTMENTS
- 14 AMLI SLU APARTMENTS
- 15 AMLI 535
- 16 RIVET APARTMENTS
- 17 ALCYONE APARTMENTS
- 18 PROPOSED RESIDENTIAL TOWER
- 19 VEER LOFTS
- 20 8TH & REPUBLICAN
- 21 BOREALIS

RETAIL

- 1 WESTLAKE AND TERRY RETAIL NODE
- 2 2200 WESTLAKE -GROCERY AND RETAIL
- 3 REI FLAGSHIP STORE RETAIL AREA
- 4 QFC
- 5 LAKE SIDE DINING- MULTIPLE

HOTELS

VARIOUS

SITE ANALYSIS: NEIGHBORHOOD USES



2 : GATES FOUNDATION



2 : UNION APARTMENTS



3 : UW MEDICINE SLU RESEARCH



3 : TRUE NORTH APARTMENTS



7: 300 & 333 8TH AVE N



5 : HUE APARTMENTS



7 : 777 THOMAS APARTMENTS



8: TAYLOR 28 APARTMENTS



19 : VEER LOFTS

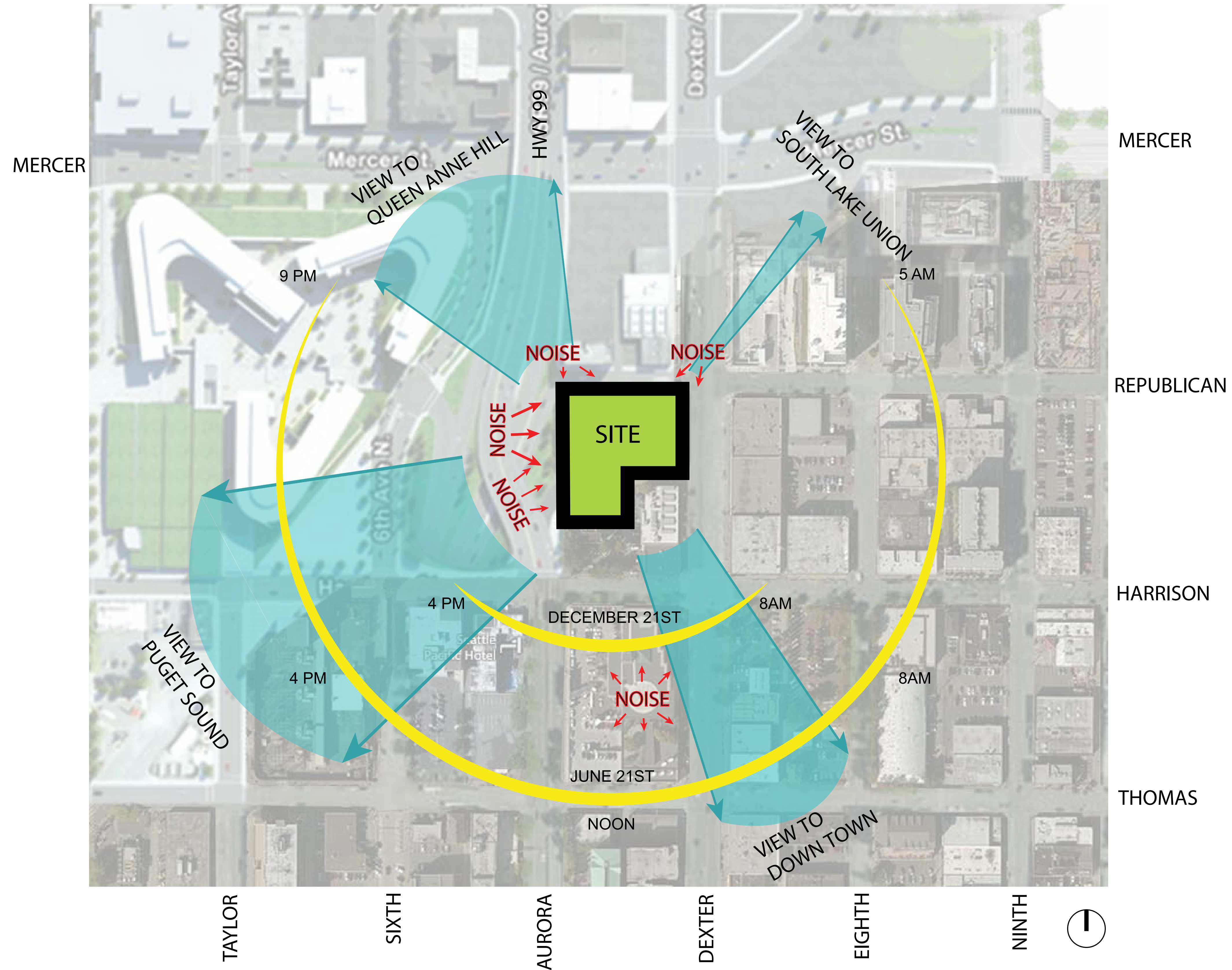


20 : 8TH & REPUBLICAN

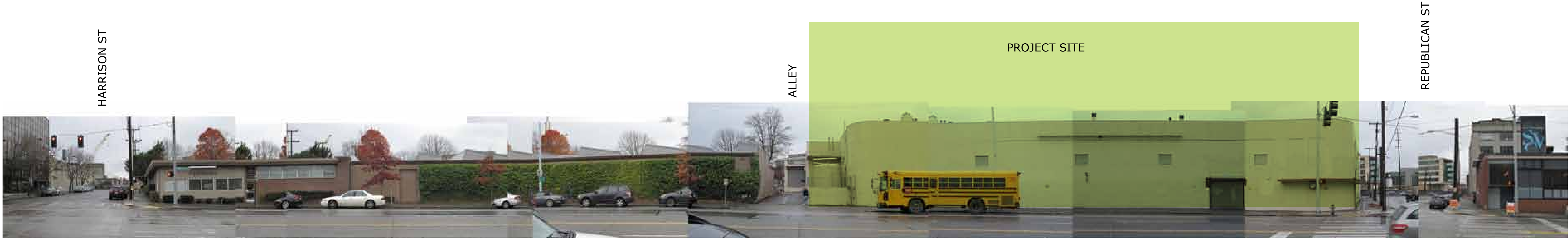


21 : BOREALIS APARTMENTS

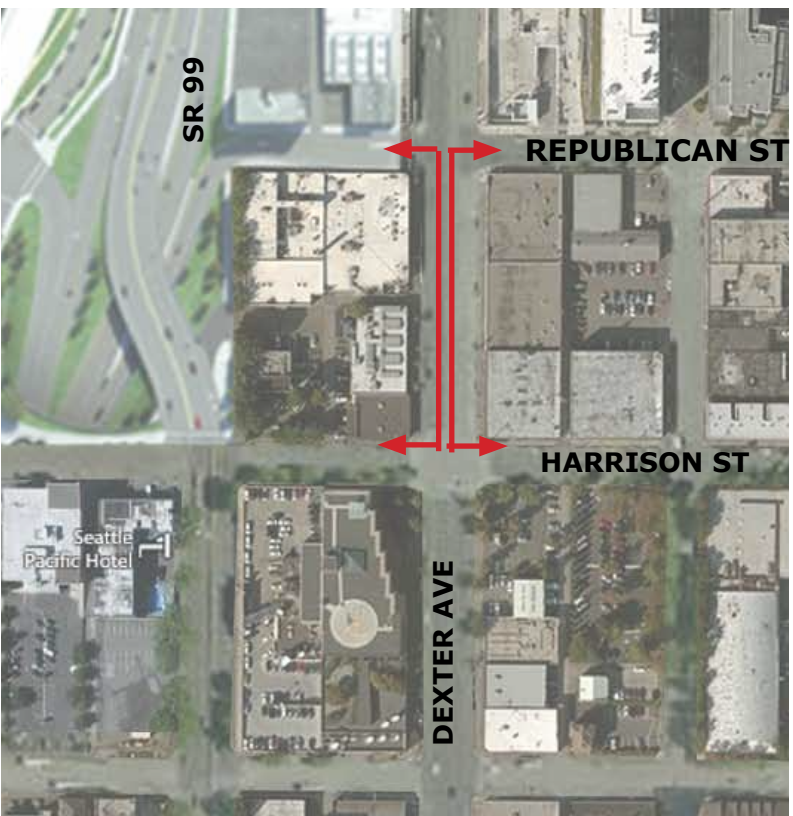
SITE ANALYSIS: NEARBY BUILDINGS



SITE ANALYSIS: ENVIRONMENTAL



VIEW TO THE WEST



KEY MAP



VIEW TO THE EAST

EXISTING STREETScape: DEXTER AVENUE

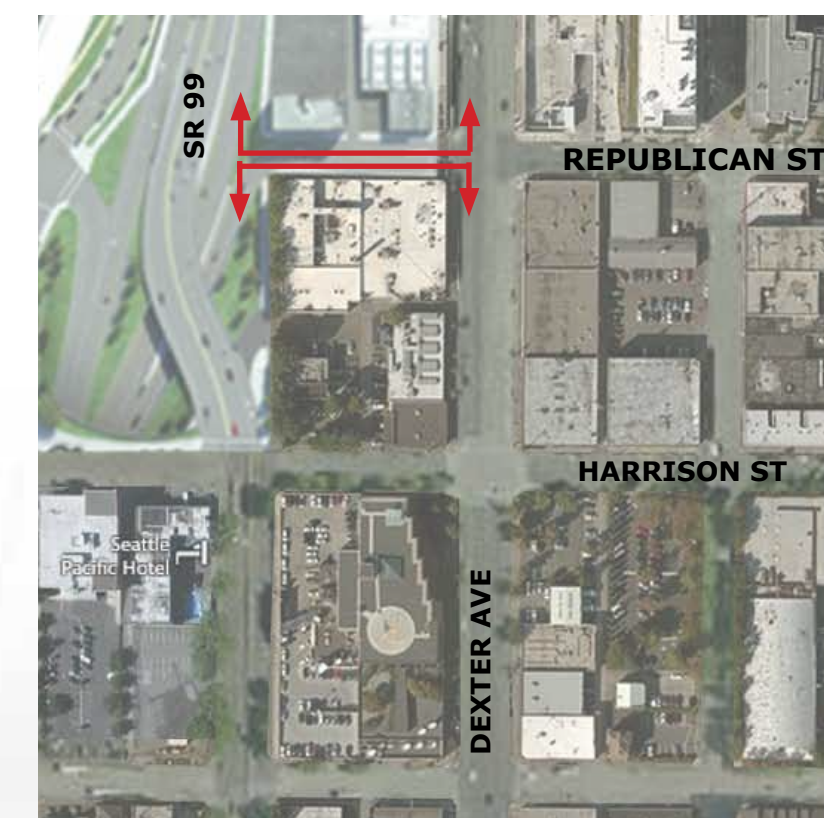


DEXTER AVE

PROJECT SITE

SR 99

VIEW TO THE SOUTH



KEY MAP



SR 99

DEXTER AVE

VIEW TO THE NORTH

EXISTING STREETScape: REPUBLICAN STREET

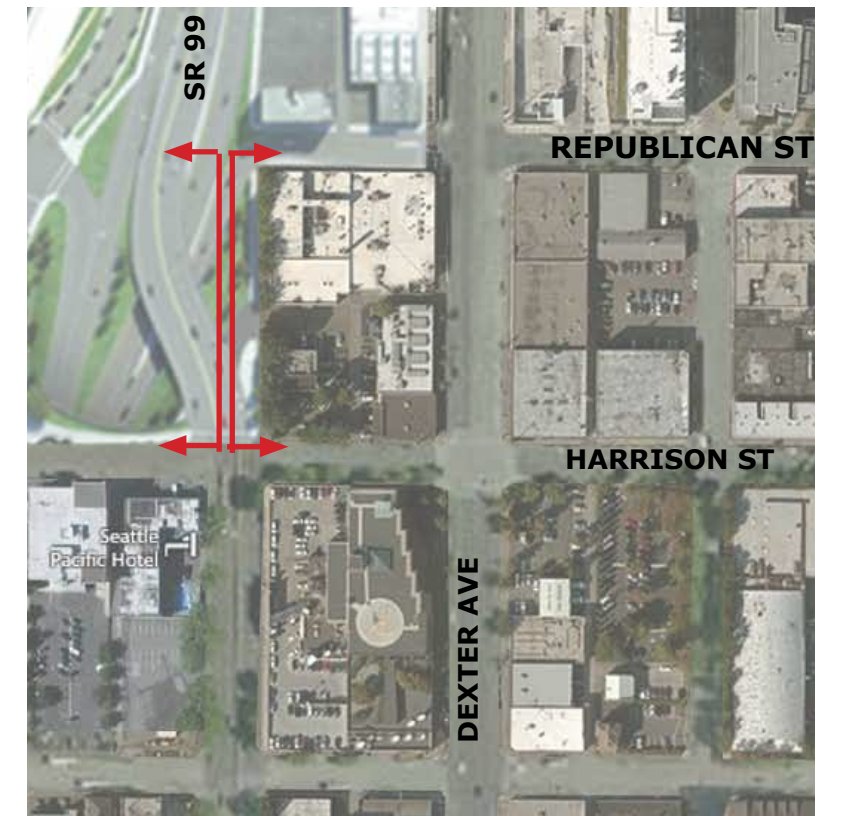


REPUBLICAN ST.

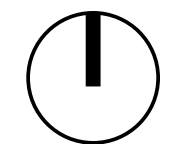
PROJECT SITE

HARRISON ST.

VIEW TO THE EAST



KEY MAP



VIEW TO THE WEST

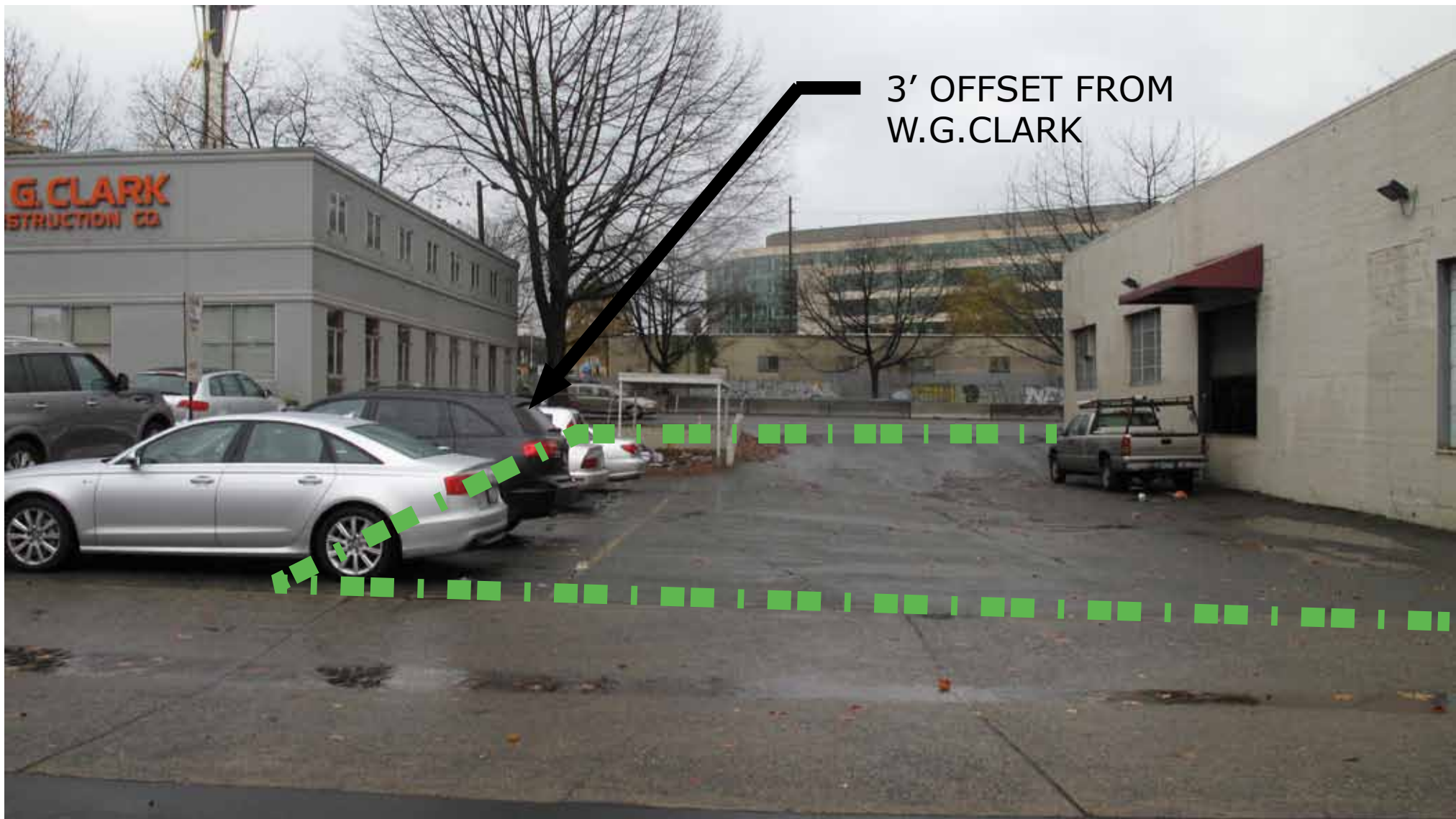
EXISTING STREETScape: STATE ROUTE 99

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ALLEY LOOKING WEST



ALLEY LOOKING WEST TO AURORA



ALLEY LOOKING WEST TO AURORA



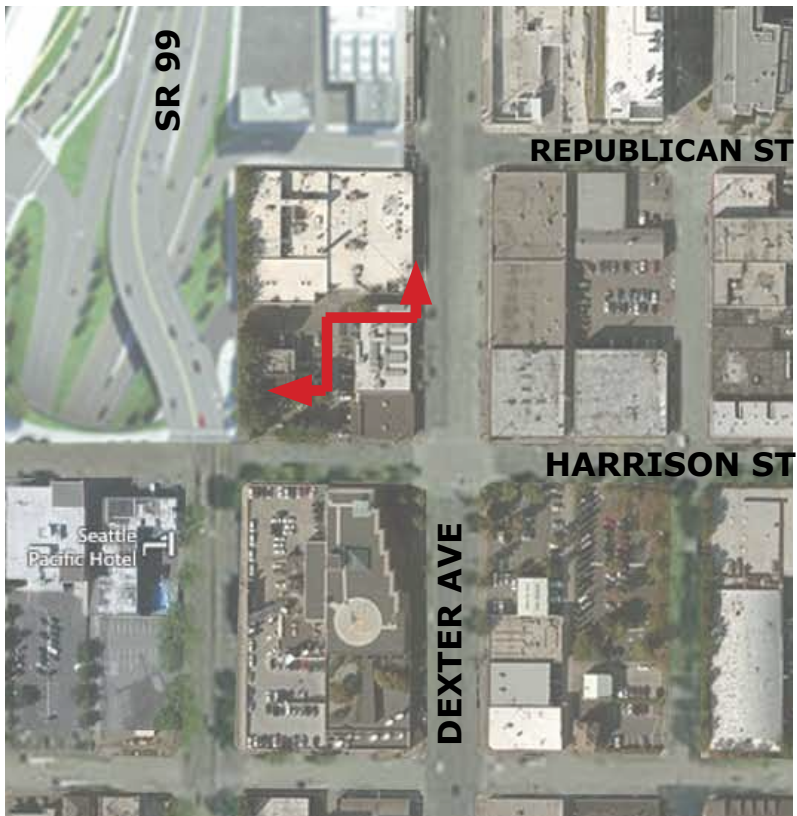
ALLEY LOOKING WEST



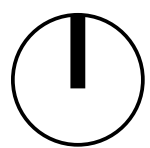
ALLEY LOOKING NORTH TOWARDS HOSTESS



ALLEY LOOKING NORTHEAST TOWARDS DEXTER



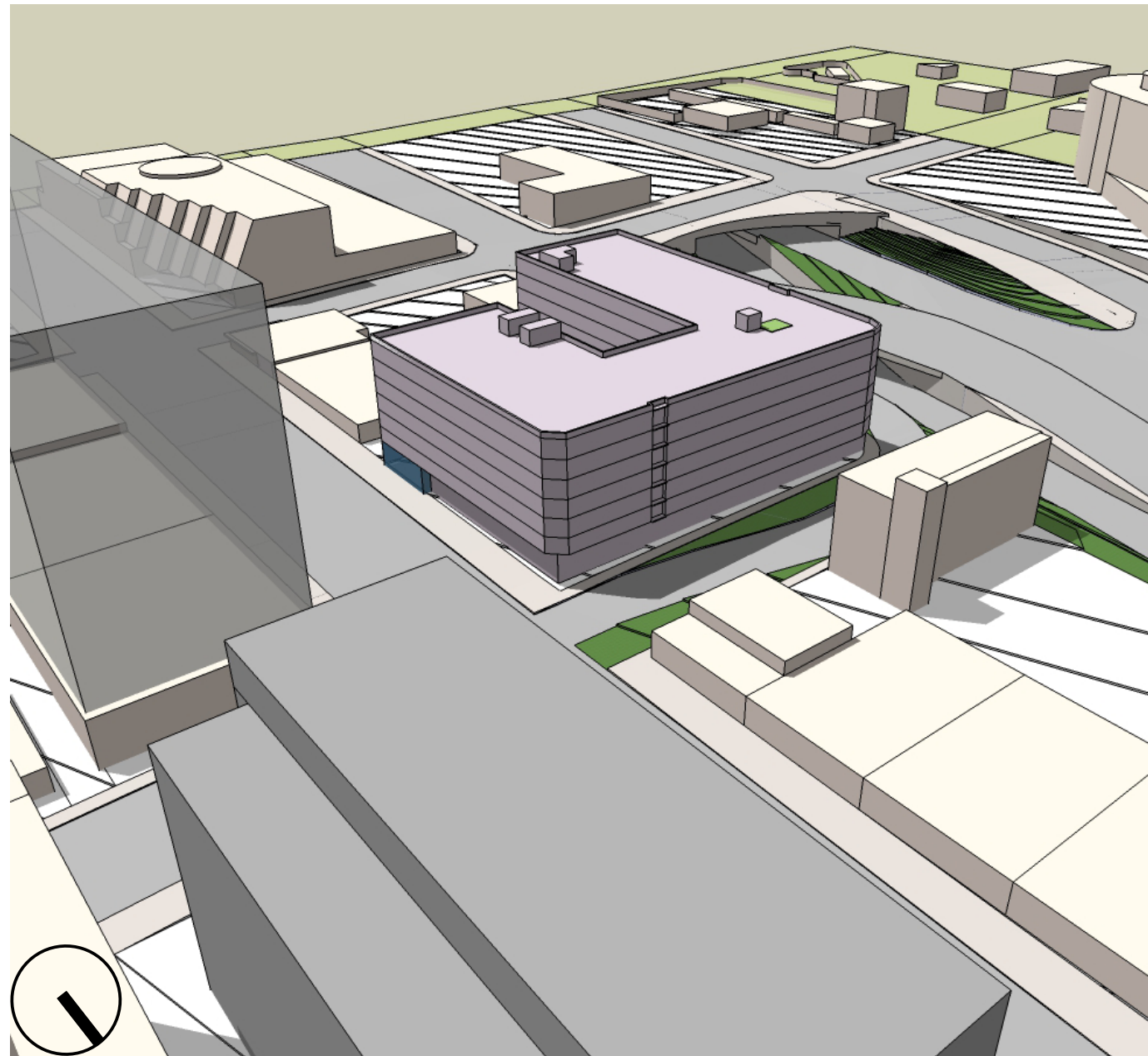
KEY MAP



EXISTING STREETScape: ALLEY

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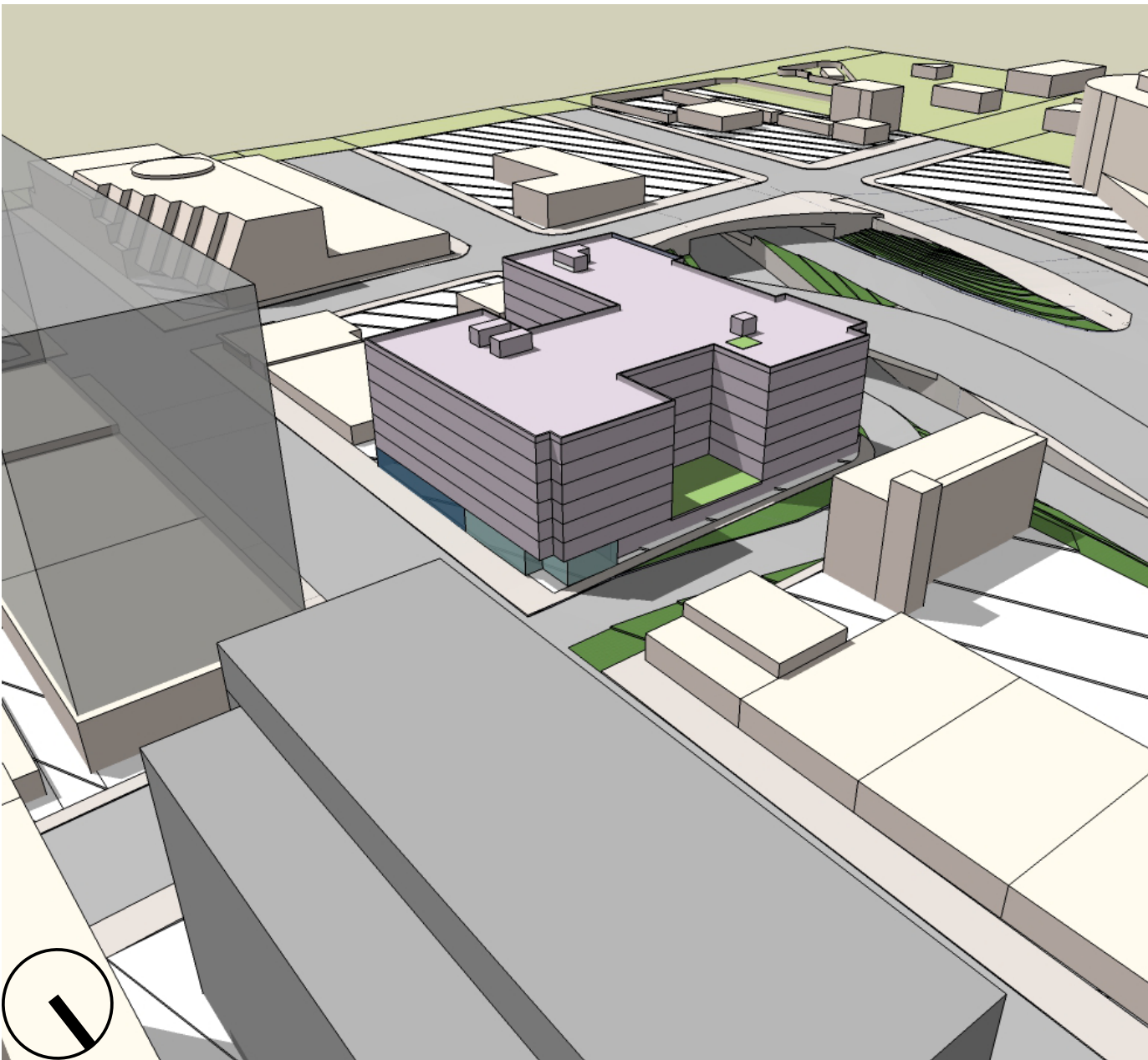
ALTERNATIVE A

PROS

- Entire building mass oriented toward property lines allowing large podium courtyard-approx. 63'x165' (**C-2** SLU Specific)
- No street-facing setbacks allow maximum building layout efficiency
- Parking access and services from alley minimizes traffic and pedestrian conflict (**A-10, C-5, D-5**)

CONS

- Large building mass with little articulation or human scale
- No enhancement of corners at Republican/SR99 and Republican/Dexter, misses opportunity to respond to gateway to Downtown Seattle
- Main building lobby at Dexter and alley does not address prominent corner of Dexter and Republican
- Scale of building appropriate to edge condition of SR 99, not Dexter
- Residential units oriented along Republican and SR 99 at street level susceptible to noise, headlights, and air-quality issues
- Residential amenity spaces located along SR 99 at street level impacted by traffic
- Rooftop terrace oriented toward West impacted by noise and air quality



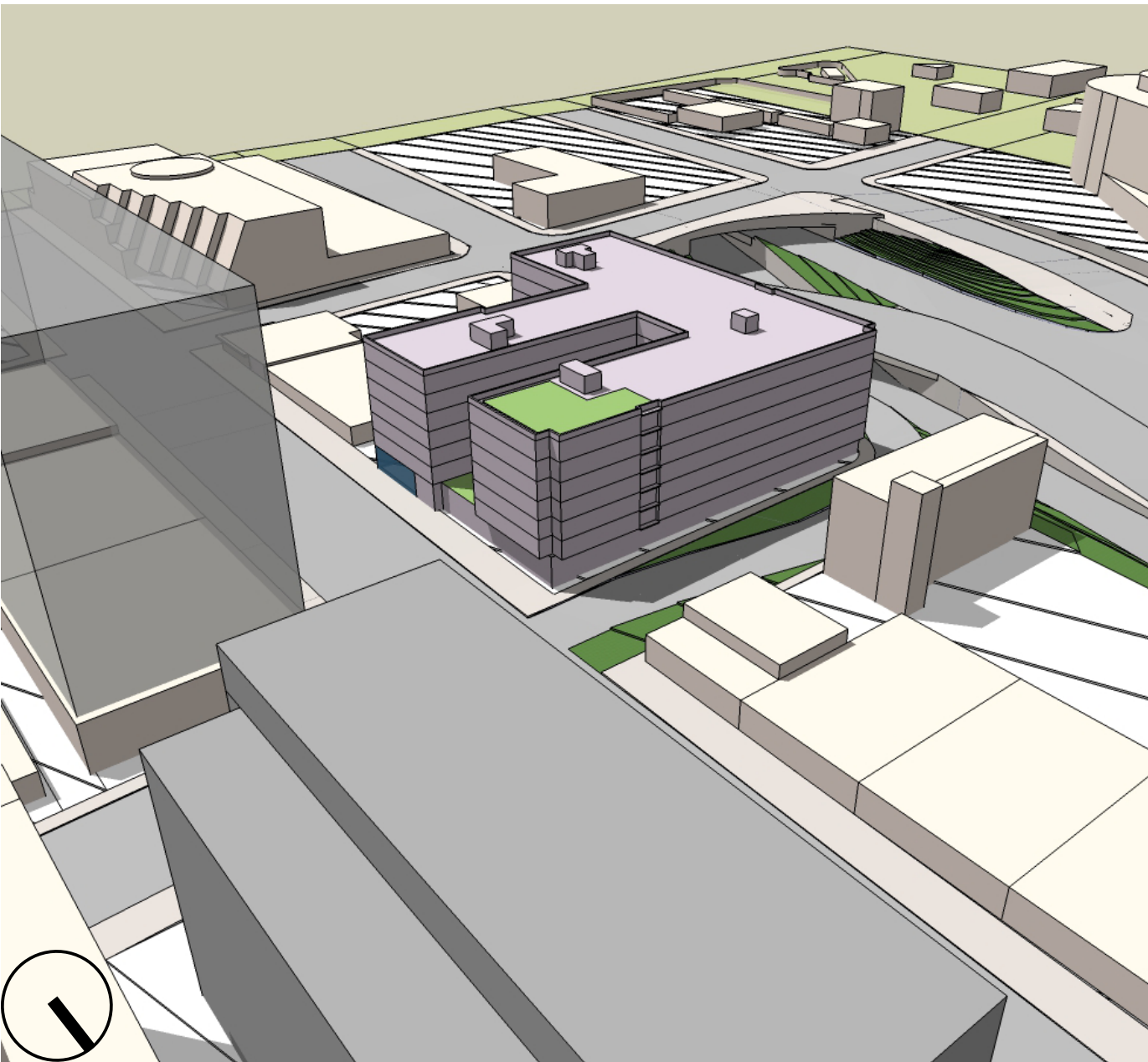
ALTERNATIVE B

PROS

- Setbacks distributed to North and West provide articulation and access to views of landscaped areas at SR 99 and Republican (**A-1, B-1**)
- Main building lobby entrance reinforces prominent corner of Dexter and Republican (**A-1, A-3, A-10, D-1**)
- Parking access and services from alley minimizes traffic and pedestrian conflict (**A-10, C-5, D-5**)

CONS

- 'H'-shaped building creates interior corners with inefficient building layout and loss of exterior walls with glazing
- Building articulation creates setbacks along SR 99 and Republican, less pedestrian oriented than Dexter and impacted more from traffic noise and air quality
- North-facing setback along Republican receives little sun
- Smaller South-facing courtyard-53'x115'
- Little articulation or scale breakdown along Dexter
- Street-level residential oriented along Republican off-ramp impacted by traffic exiting SR 99.
- Large setback along SR 99 impacted by traffic noise and air quality
- Rooftop terrace oriented toward Northwest impacted by noise and air quality



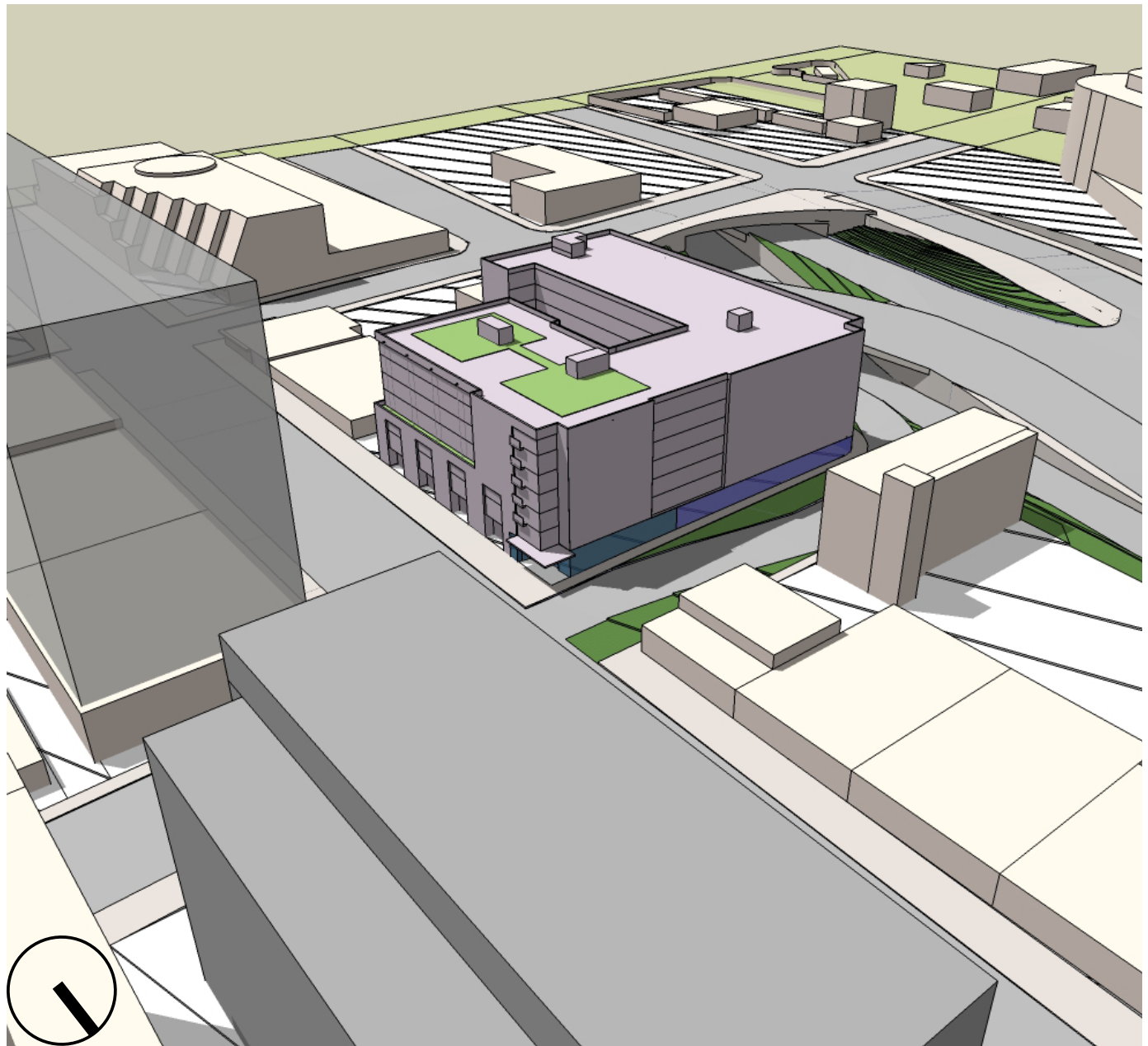
ALTERNATIVE F

PROS

- Setback at East creates modulation on Dexter and large podium courtyard-approx. 40'x136' (**C-2** SLU Specific)
- Parking access and services from alley minimizes traffic and pedestrian conflict (**A-10, C-5, D-5**)

CONS

- 'F' shaped building creates additional inside corner units
- East facing courtyard has limited exposure to daylight
- No enhancement of corners at Republican/SR99 and Republican/Dexter, misses opportunity to respond to gateway to Downtown Seattle
- Main building lobby at Dexter and alley does not address prominent corner of Dexter and Republican
- Residential units oriented along Republican and SR 99 at street level susceptible to noise, headlights, and air-quality issues
- Residential amenity spaces located along SR 99 at street level impacted by traffic
- Rooftop terrace oriented toward Northwest impacted by noise and air quality



ALTERNATIVE C: PREFERRED

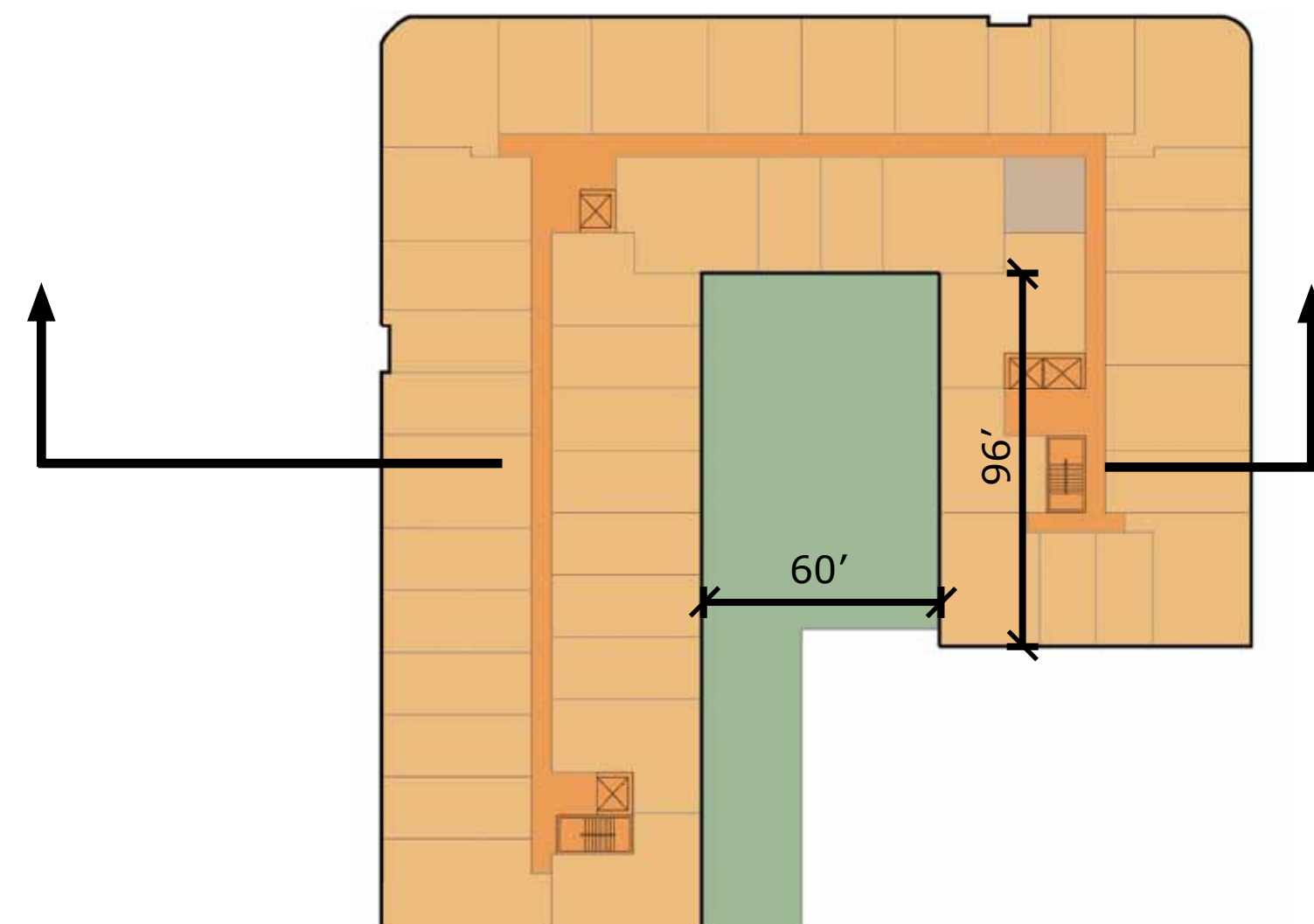
PROS

- Corners differentiated at Republican/SR 99 and Republican/Dexter to create gateway elements (**A-10**)
- Setback along Dexter to respond to smaller scale, lower speed, pedestrian/bicycle orientation (**A-2, B-1**)
- Main building lobby entrance reinforces prominent corner of Dexter and Republican (**A-1, A-3, A-10, D-1**)
- Street-level residential appropriately oriented toward Dexter, fewer traffic impacts than SR 99 or Republican (**A-3, A-6**)
- Parking access and services from alley minimizes traffic and pedestrian conflict (**A-10, C-5, D-5**)
- South-facing courtyard 53'x140'
- Rooftop terrace oriented toward Dexter, views to Lake Union and Downtown, minimizes traffic impact from SR 99 and Republican (**A-1, C-2** SLU Specific)

CONS

- Setbacks and articulation increase construction complexity, decrease building layout efficiency

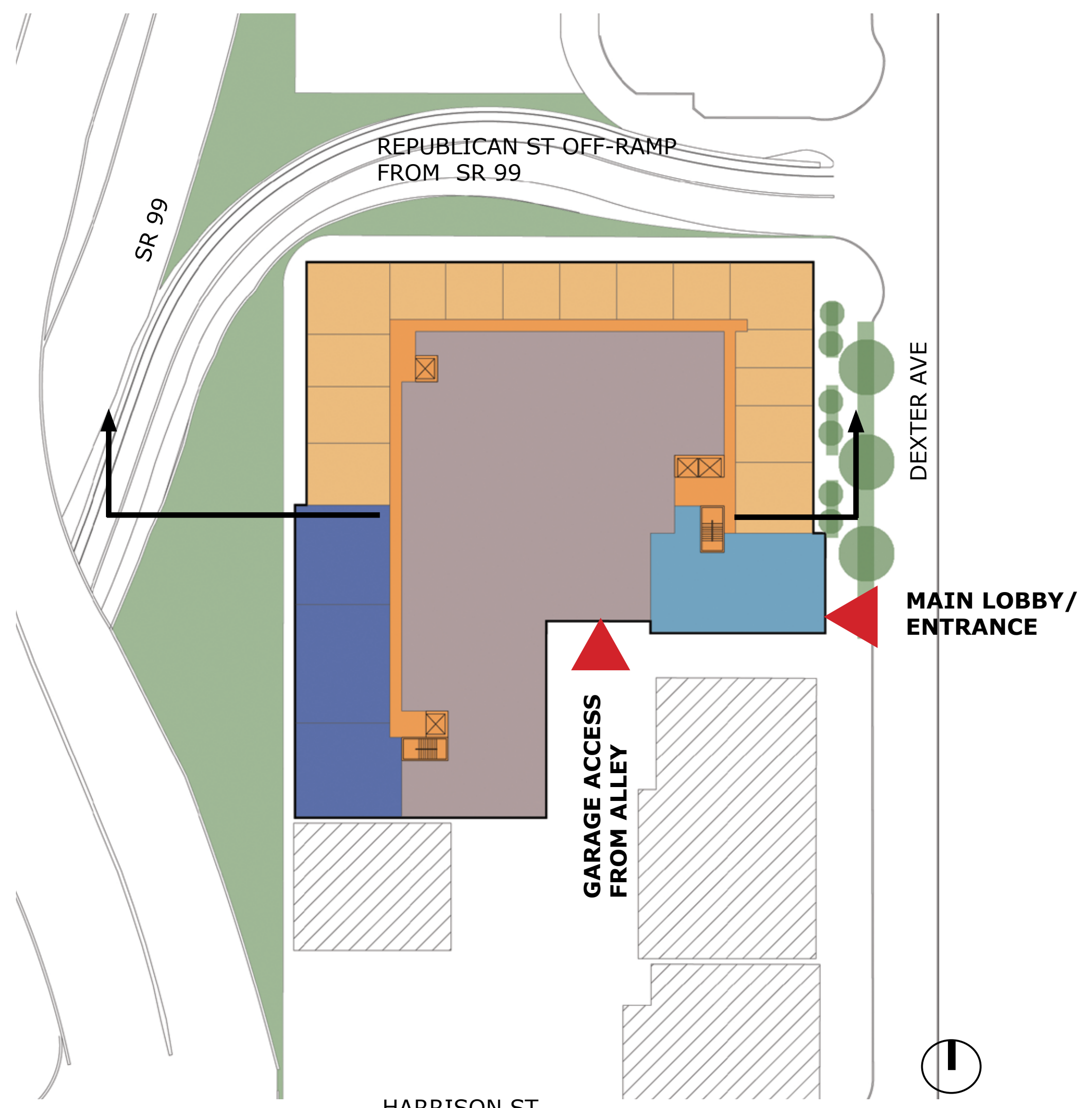
CONCEPT DESIGN ALTERNATIVES



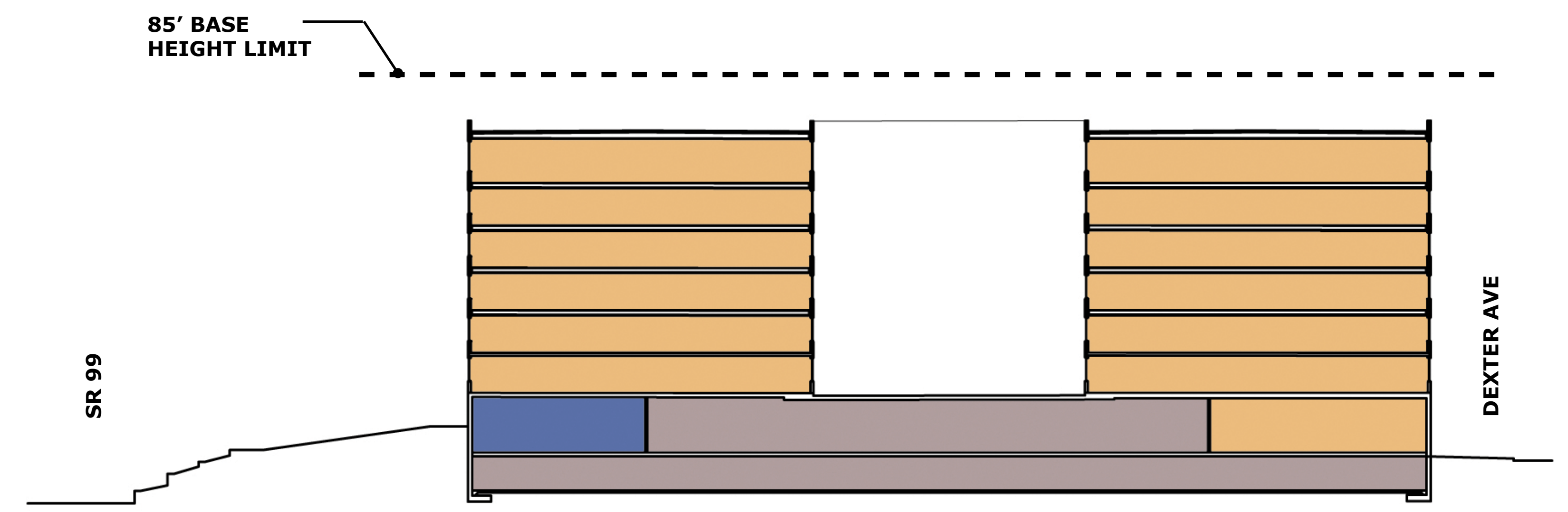
FLOOR 2 (3-7 SIM)

- KEY**
- LOBBY
 - AMENITY
 - RESIDENTIAL
 - CIRCULATION
 - PARKING
 - GREEN SPACE

- DATA**
- 7 FLOORS
 - 302 UNITS
 - 216 PARKING STALLS
 - 2 STAIR TOWERS



SITE PLAN/ FLOOR 1

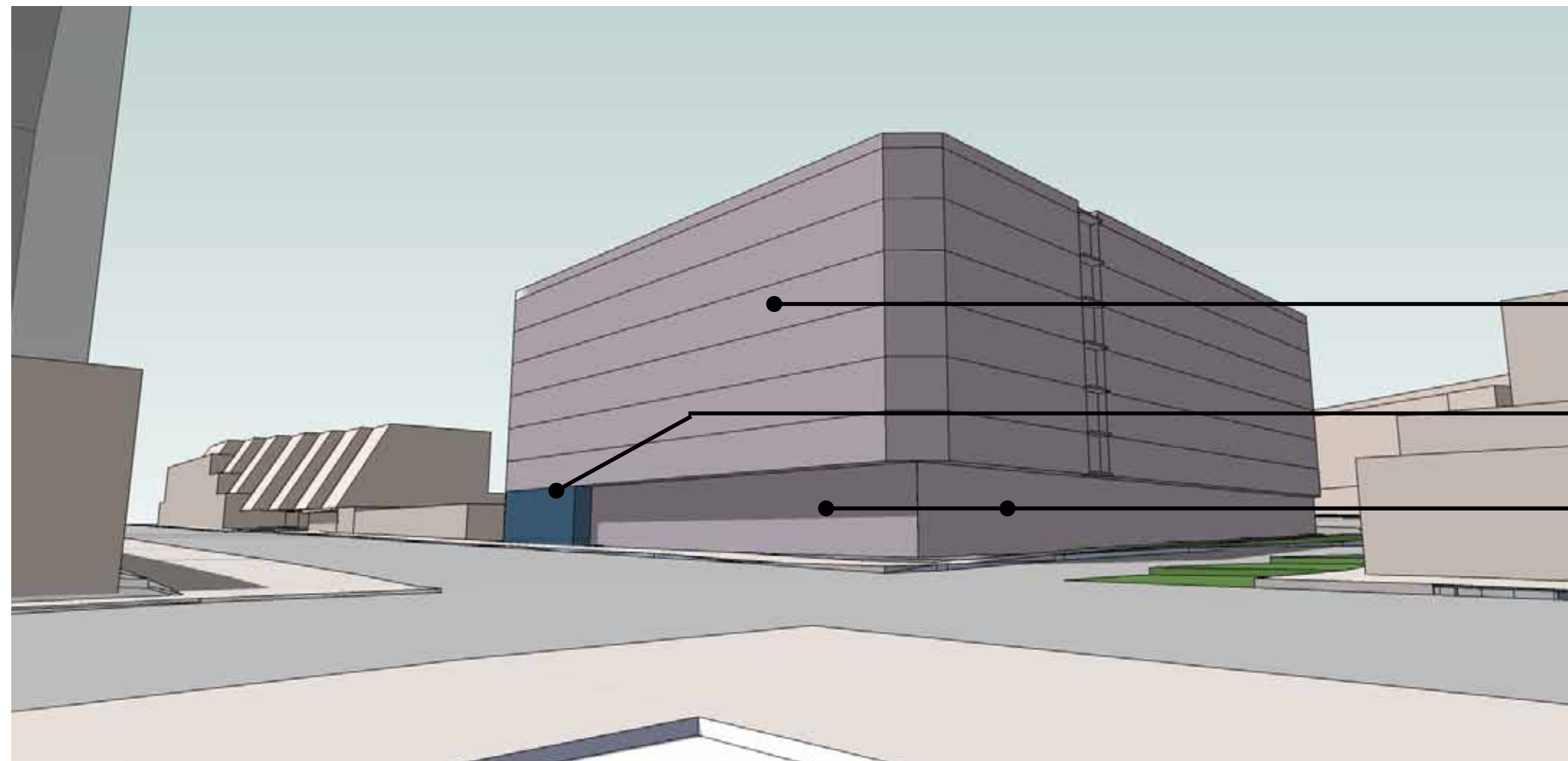


SECTION LOOKING NORTH

DESIGN ALTERNATIVE A

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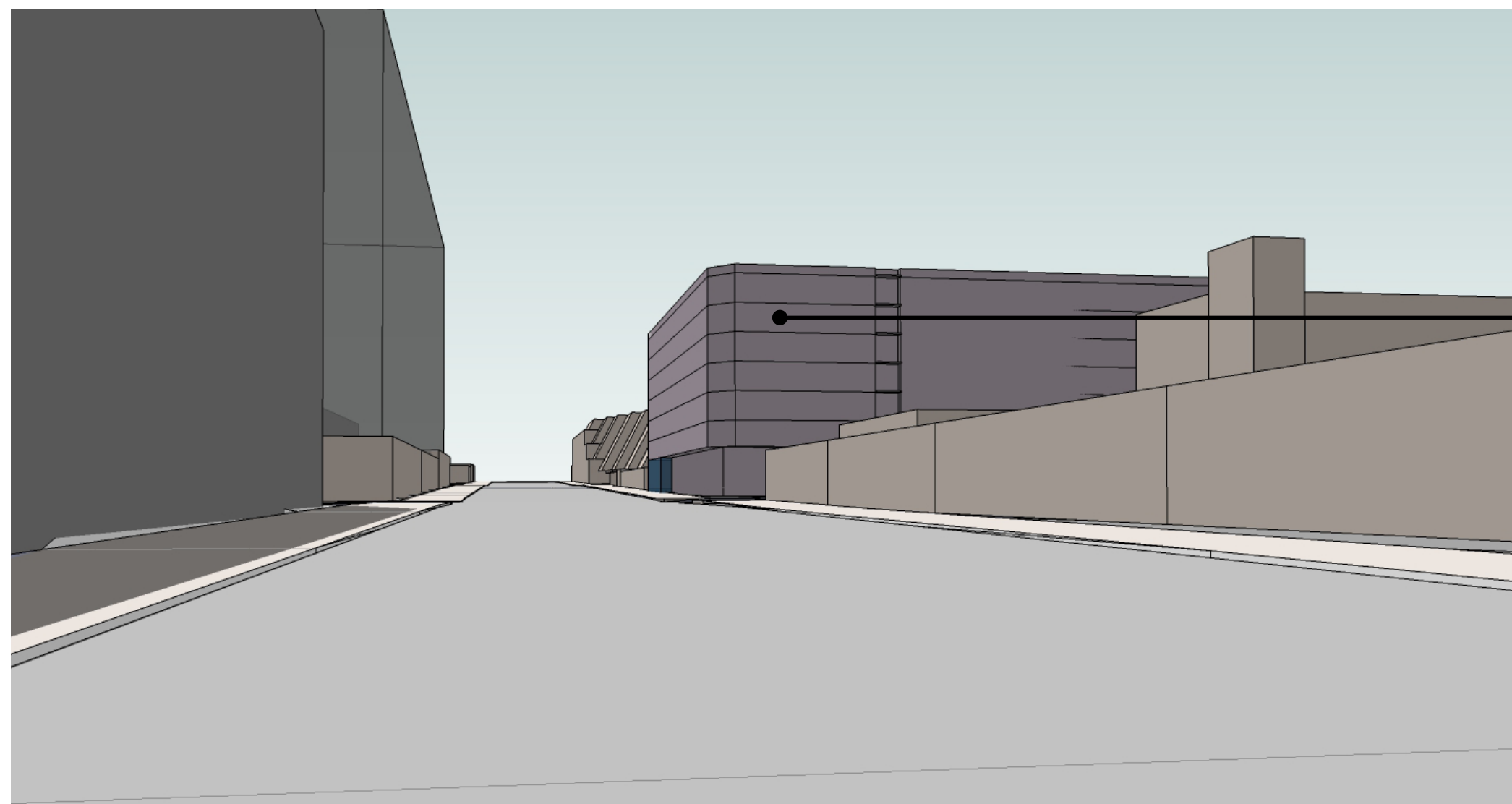
VIEW FROM NORTHEAST AT DEXTER/REPUBLICAN

- BUILDING MASS ALONG DEXTER PUSHED TO PROPERTY LINE
- MAIN BUILDING ENTRY AT CORNER OF ALLEY & DEXTER
- STREET-LEVEL RESIDENTIAL ALONG DEXTER & REPUBLICAN



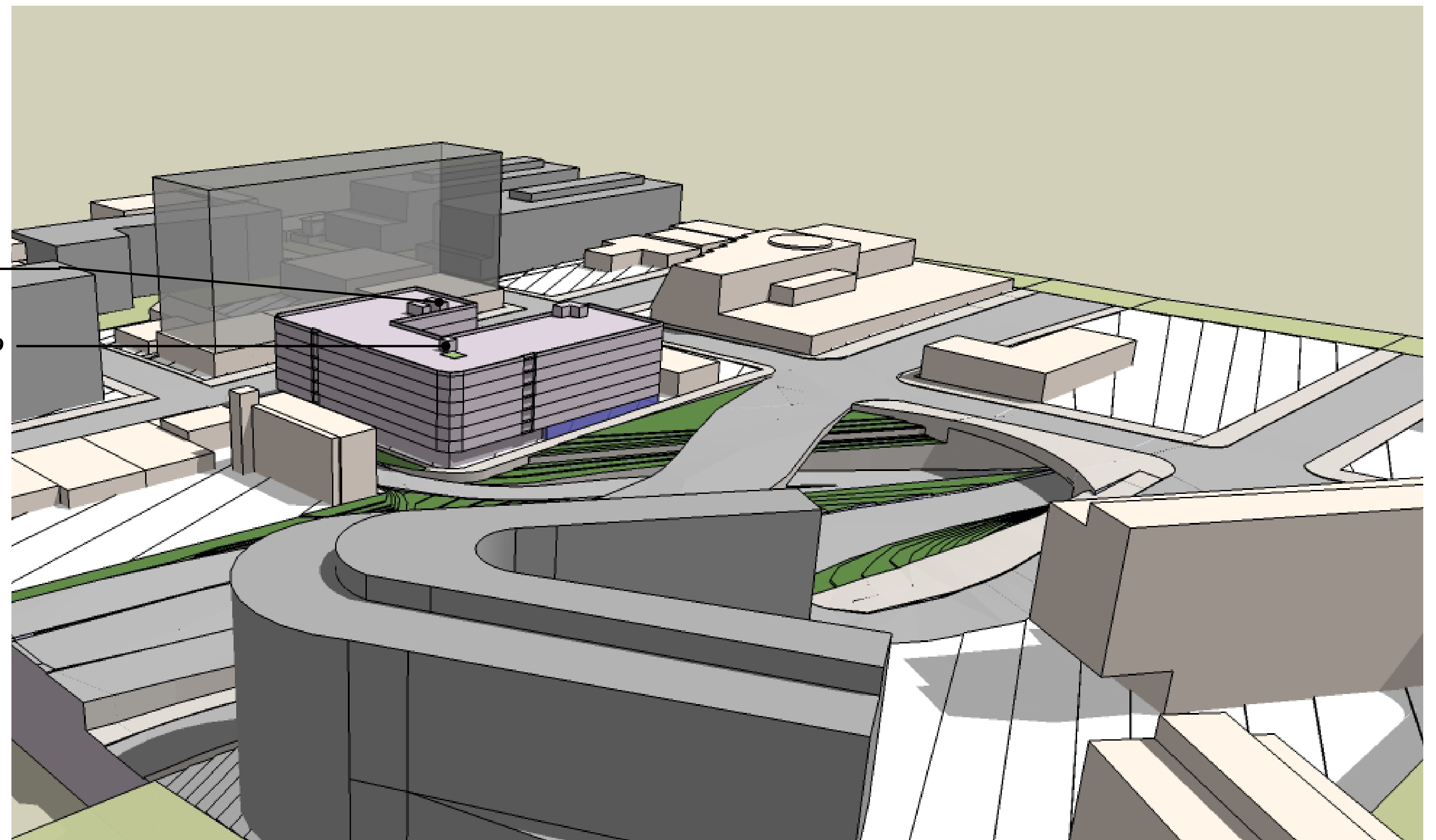
VIEW FROM NORTHWEST ON SR 99

- STREET-LEVEL RESIDENTIAL ALONG REPUBLICAN & SR 99
- STREET-LEVEL RESIDENTIAL AMENITY SPACE ALONG SR 99

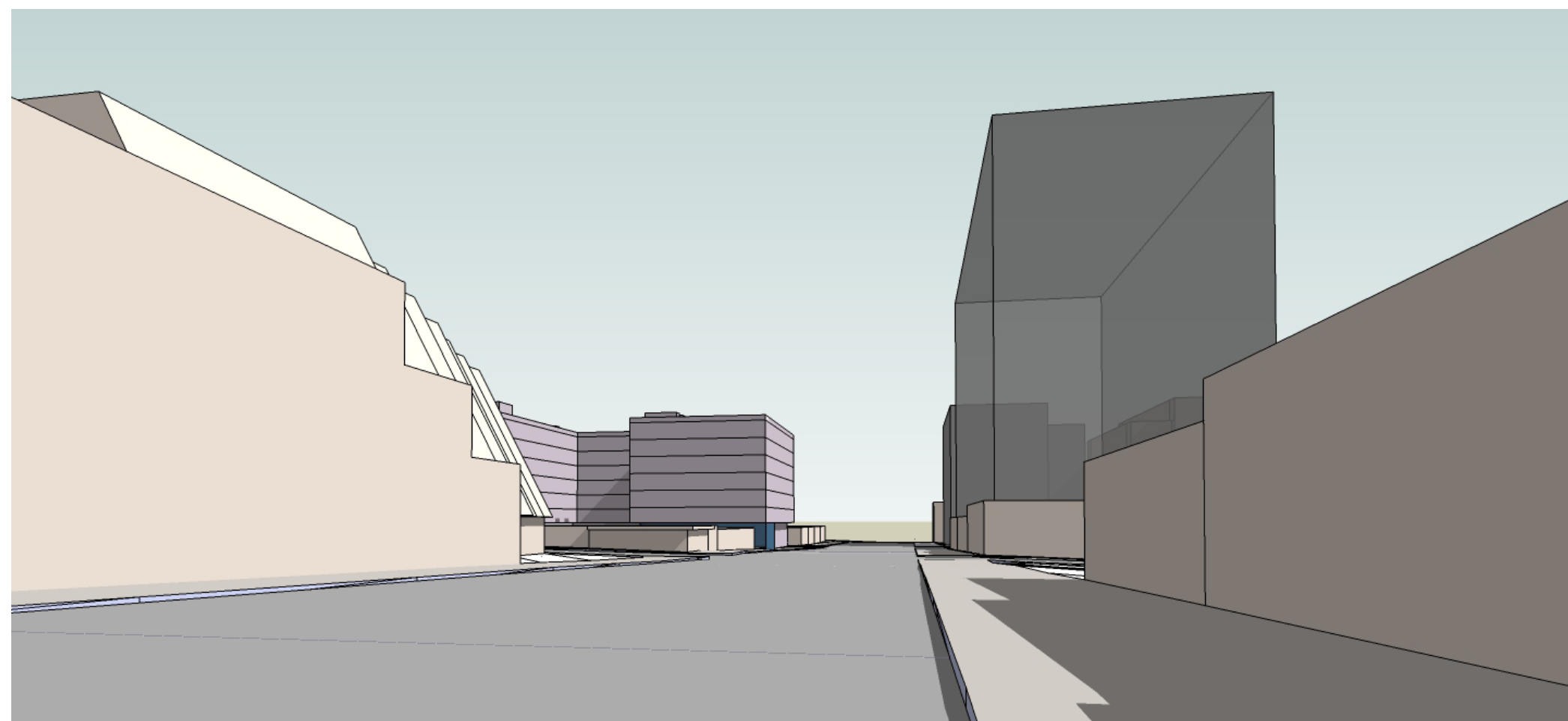


VIEW FROM NORTH ALONG DEXTER

- UNDIFFERENTIATED CORNER AT DEXTER & REPUBLICAN
- LARGER COURTYARD ALLOWED BY MINIMAL MODULATION FACING STREETS
- ROOF TERRACE ORIENTED TO SR 99



AERIAL VIEW FROM NORTHWEST

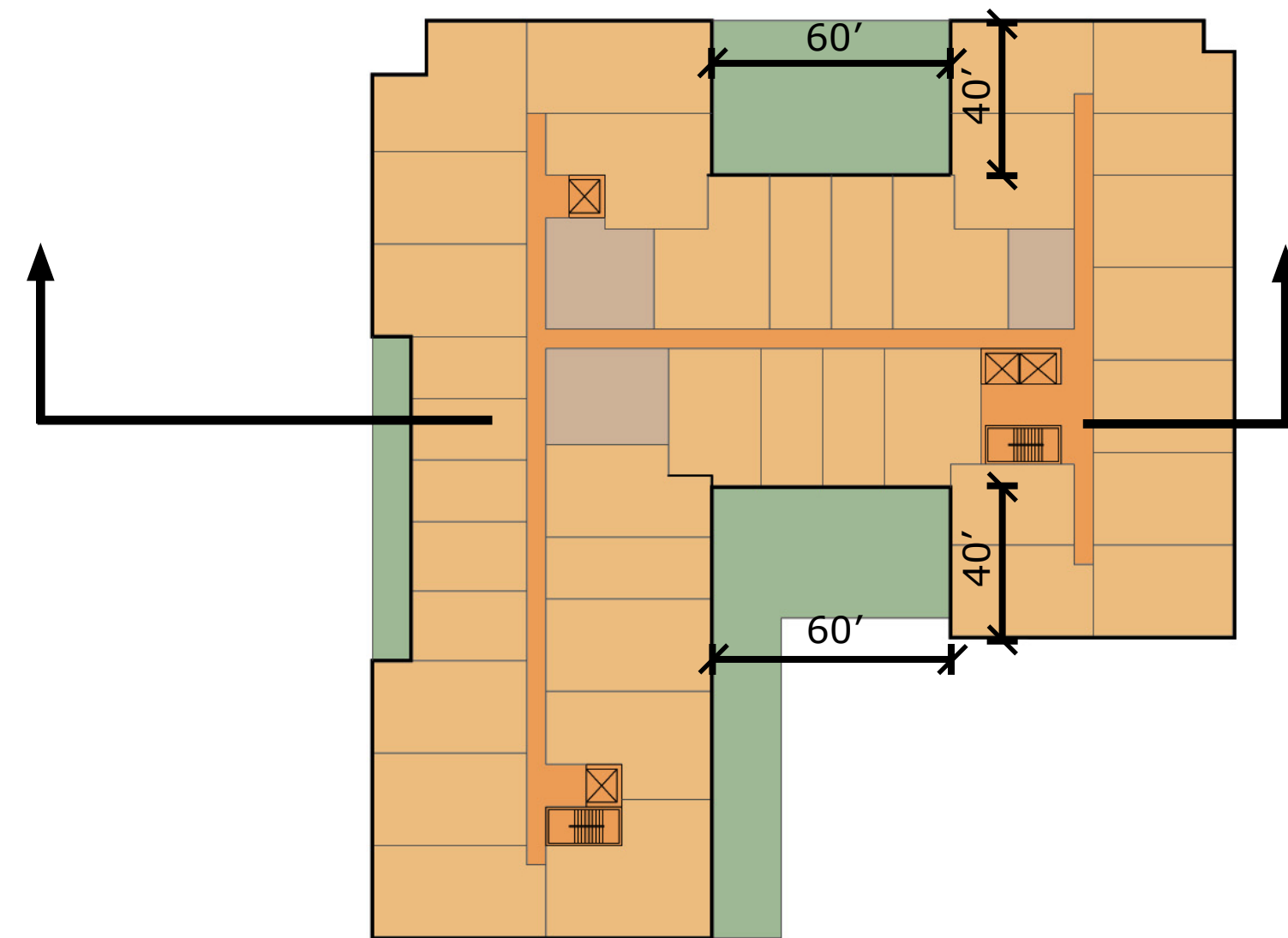


VIEW FROM SOUTH ALONG DEXTER

DESIGN ALTERNATIVE A

435 Dexter

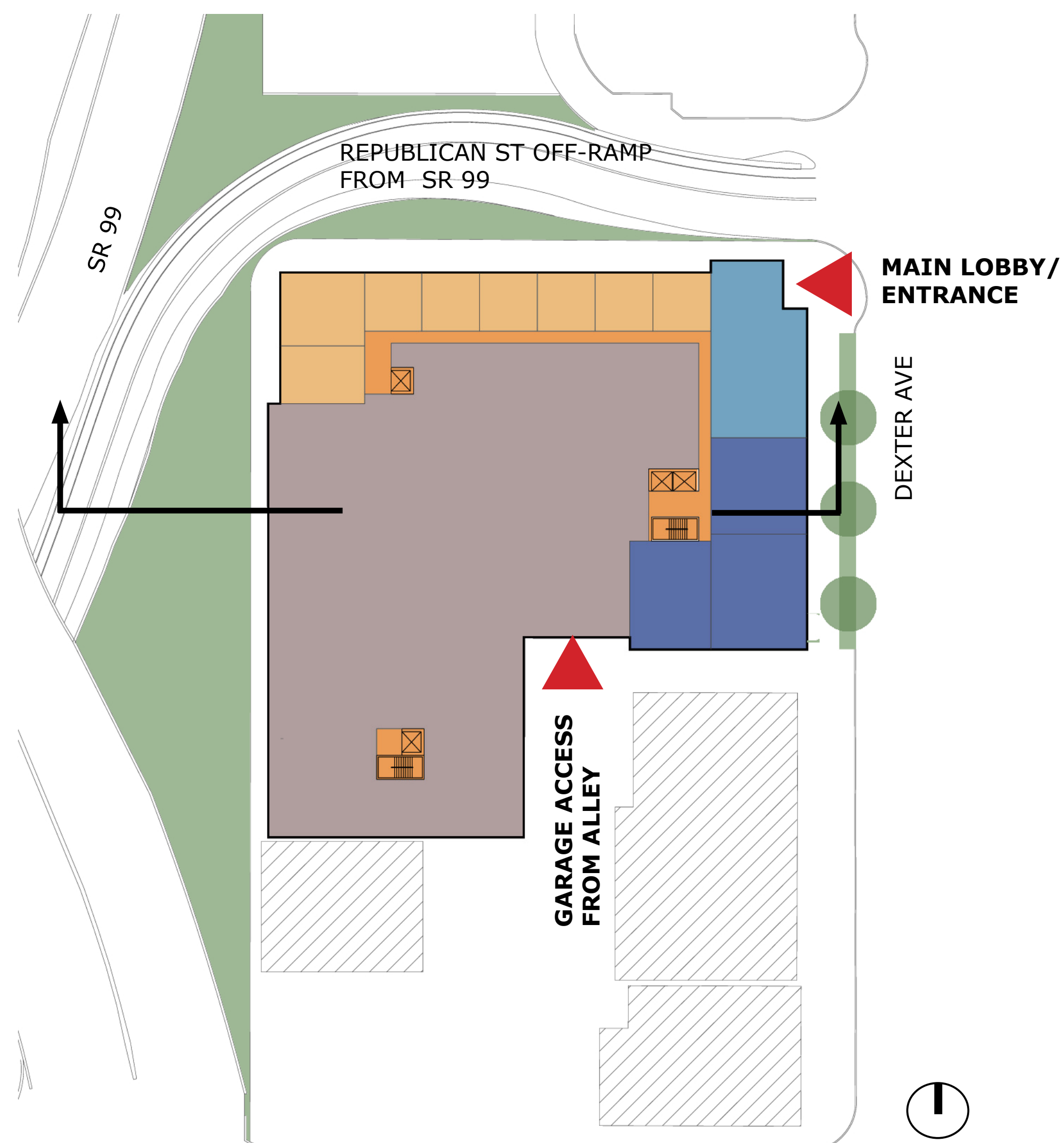
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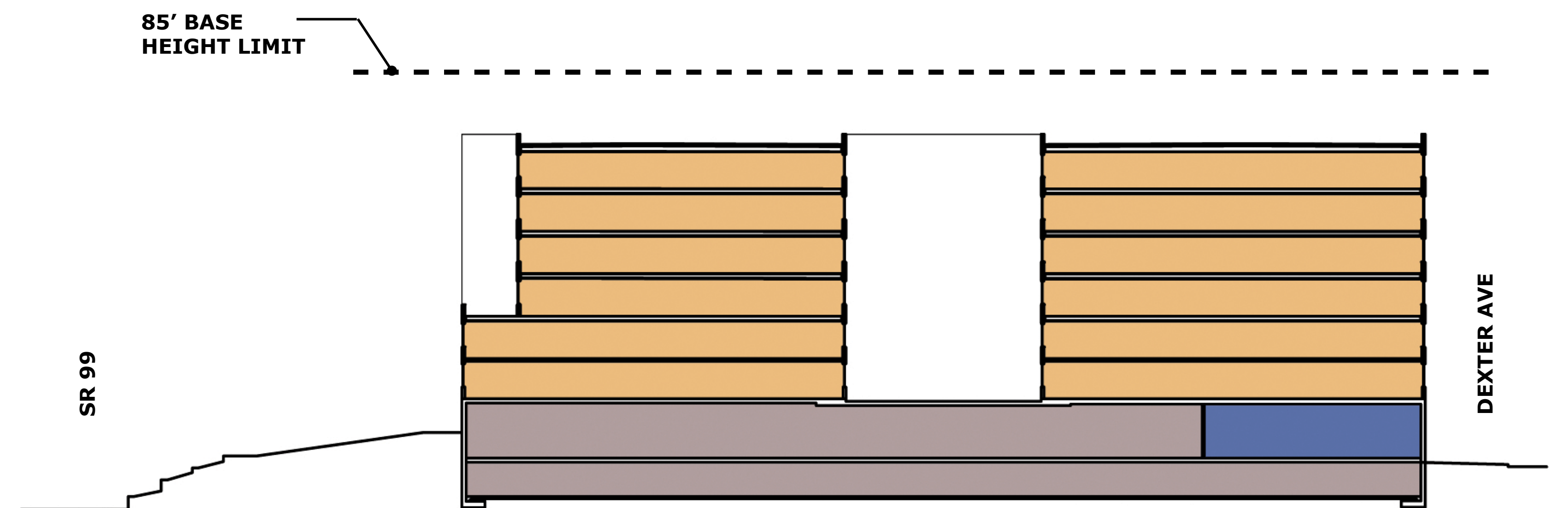
FLOOR 2 (3-7 SIM)

- KEY**
- LOBBY
 - AMENITY
 - RESIDENTIAL
 - CIRCULATION
 - PARKING
 - GREEN SPACE

- DATA**
- 7 FLOORS
 - 236 UNITS
 - 248 PARKING STALLS
 - 2 STAIR TOWERS



SITE PLAN/ FLOOR 1



SECTION LOOKING NORTH

DESIGN ALTERNATIVE B

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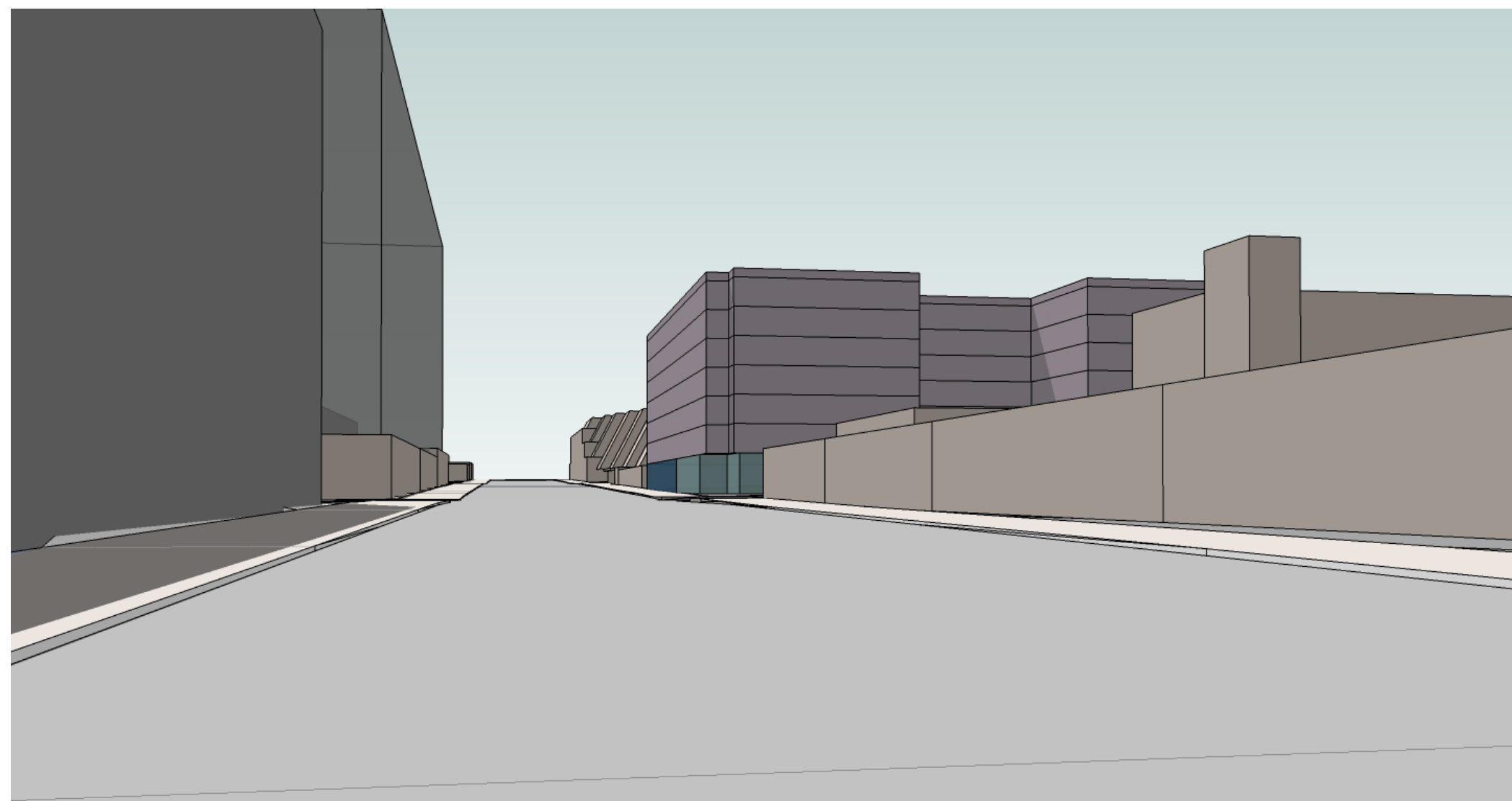
VIEW FROM NORTHEAST AT DEXTER/REPUBLICAN

- NORTH-FACING COURTYARD ALONG REPUBLICAN OFF-RAMP
- BUILDING MASS ALONG DEXTER PUSHED TO PROPERTY LINE
- MAIN BUILDING ENTRY AT CORNER OF ALLEY & DEXTER
- STREET-LEVEL RESIDENTIAL ALONG REPUBLICAN OFF-RAMP



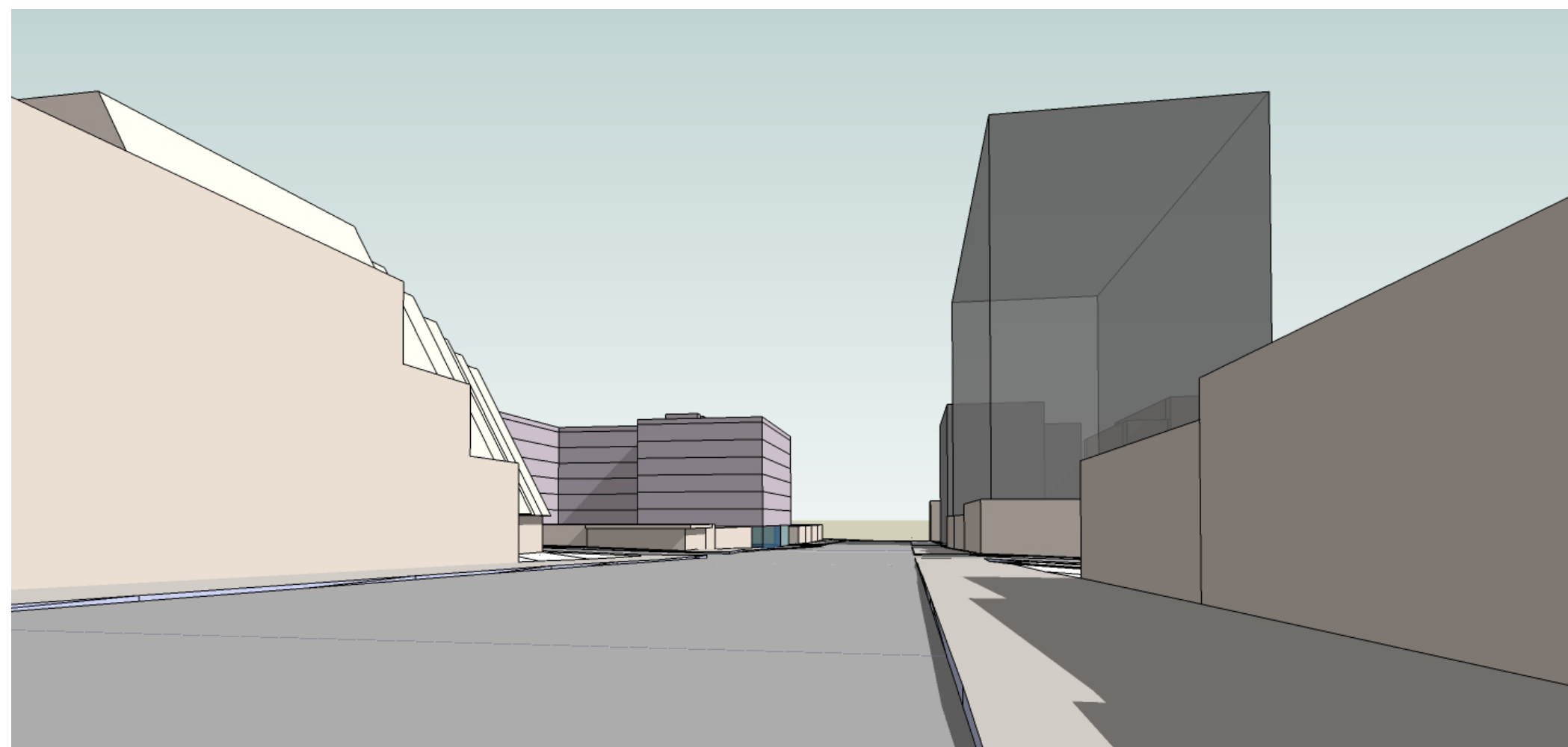
VIEW FROM NORTHWEST ON SR 99

- STREET-LEVEL RESIDENTIAL ALONG REPUBLICAN & SR 99

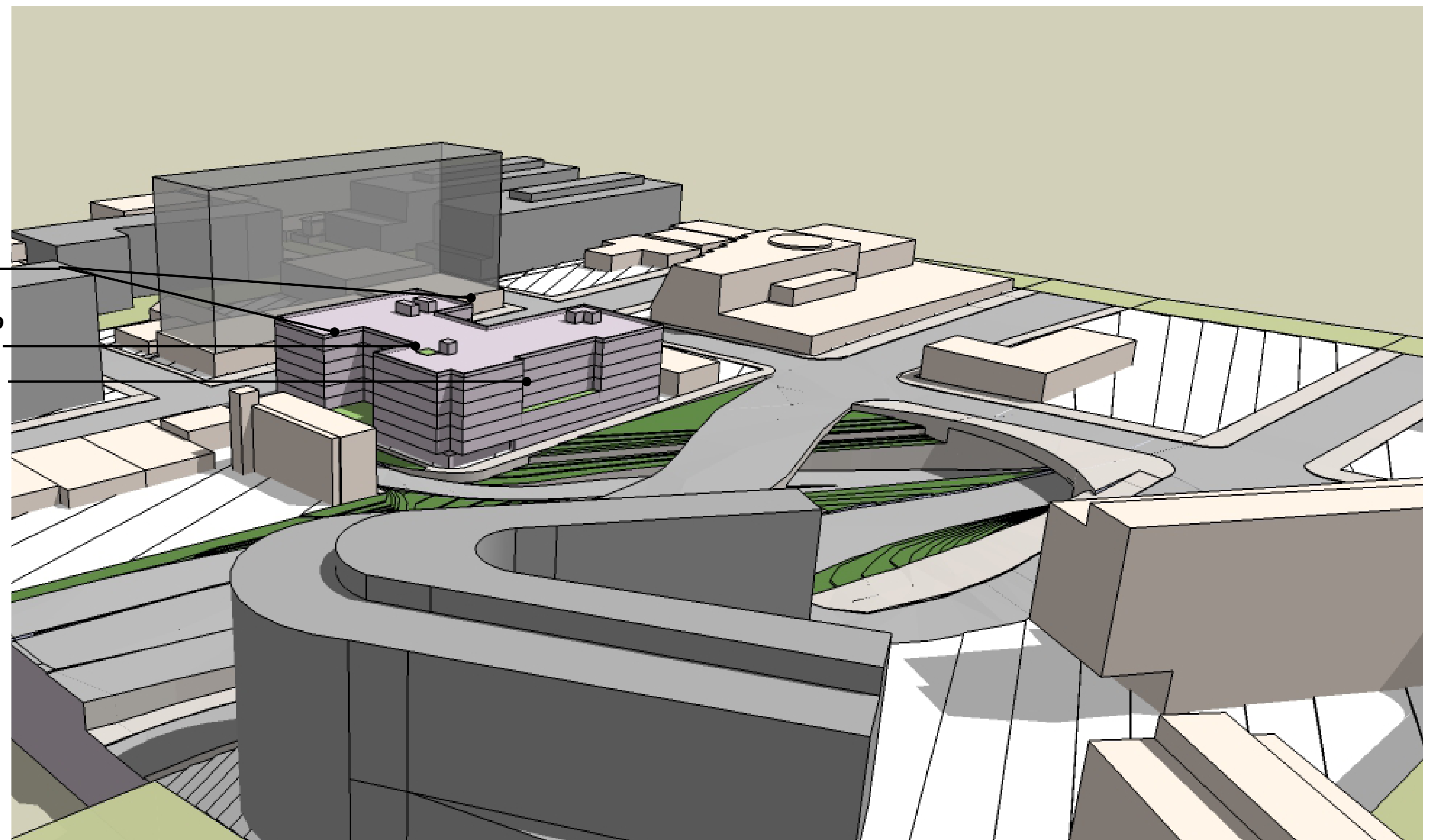


VIEW FROM NORTH ALONG DEXTER

- COURTYARDS BROKEN UP INTO NORTH & SOUTH FACING
- ROOF TERRACE ORIENTED TO SR 99 & REPUBLICAN
- UPPER-LEVEL SETBACK FACING SR 99

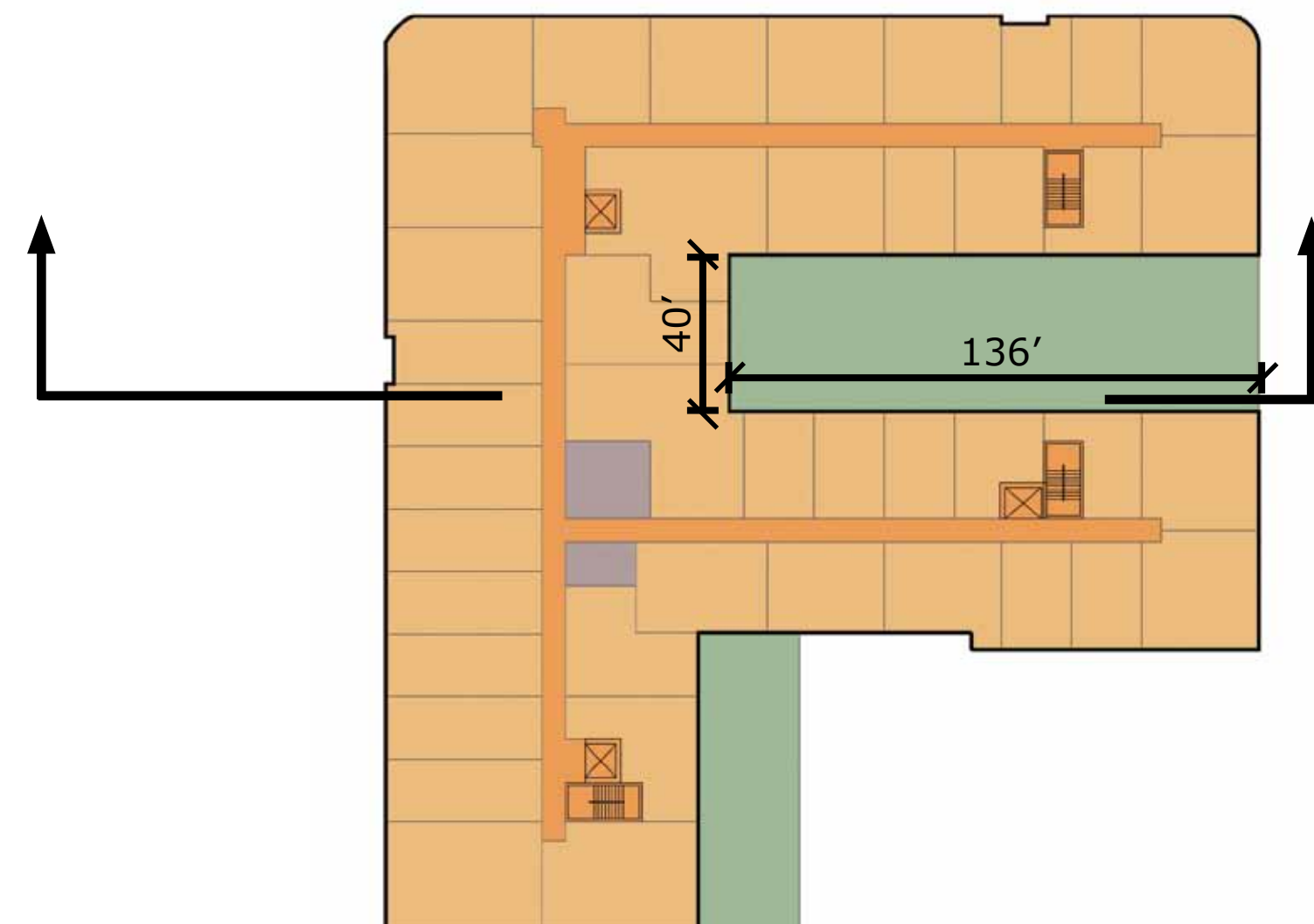


VIEW FROM SOUTH ALONG DEXTER



AERIAL VIEW FROM NORTHWEST

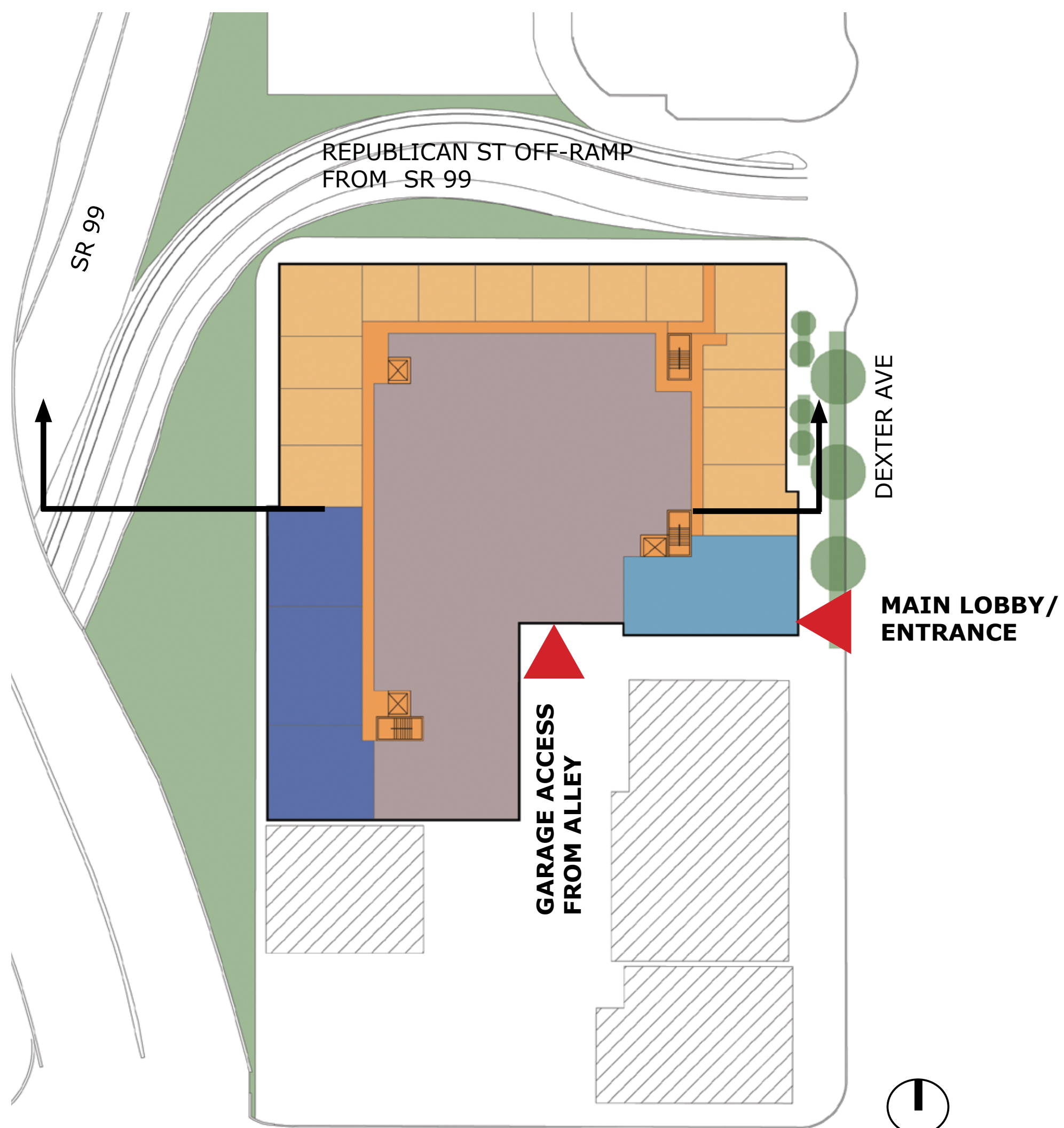
DESIGN ALTERNATIVE B



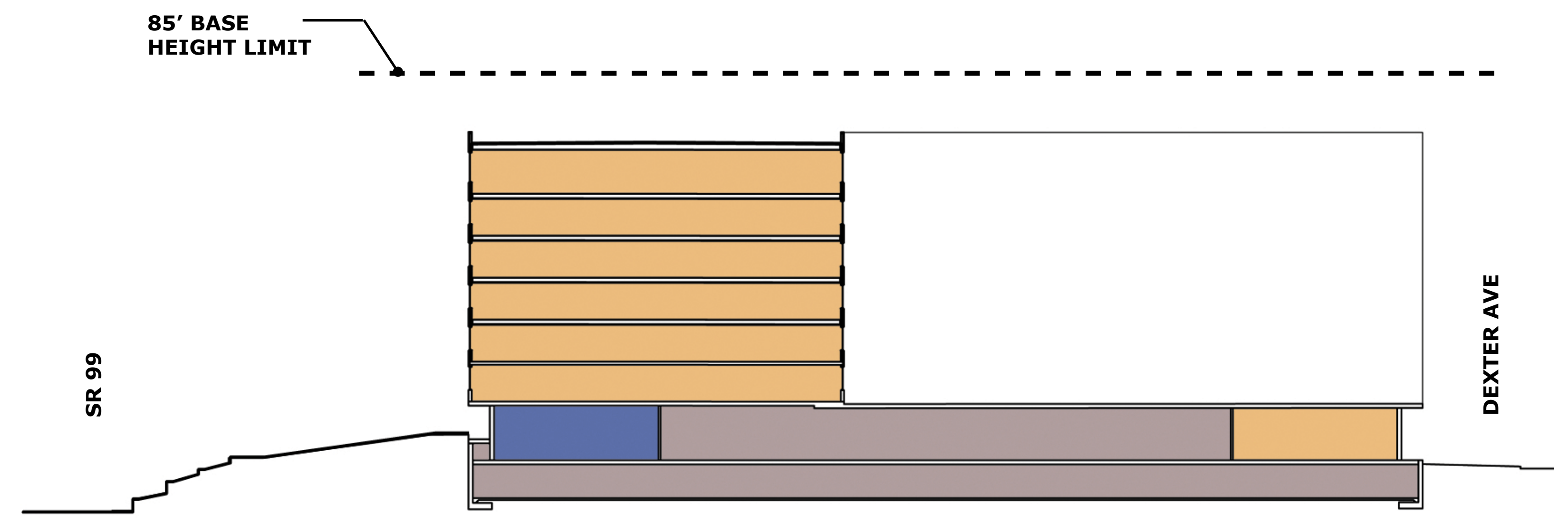
FLOOR 2 (3-7 SIM)

- KEY**
- LOBBY
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 - PARKING
 - GREEN SPACE

- DATA**
- 7 FLOORS
 - 267 UNITS
 - 216 PARKING STALLS
 - 3 STAIR TOWERS



SITE PLAN/ FLOOR 1



SECTION LOOKING NORTH

DESIGN ALTERNATIVE F

435 Dexter

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VIEW FROM NORTHEAST AT DEXTER/REPUBLICAN

BUILDING MASS ALONG DEXTER
MODULATED BY EAST FACING
COURTYARD

MAIN BUILDING ENTRY AT CORNER
OF ALLEY & DEXTER

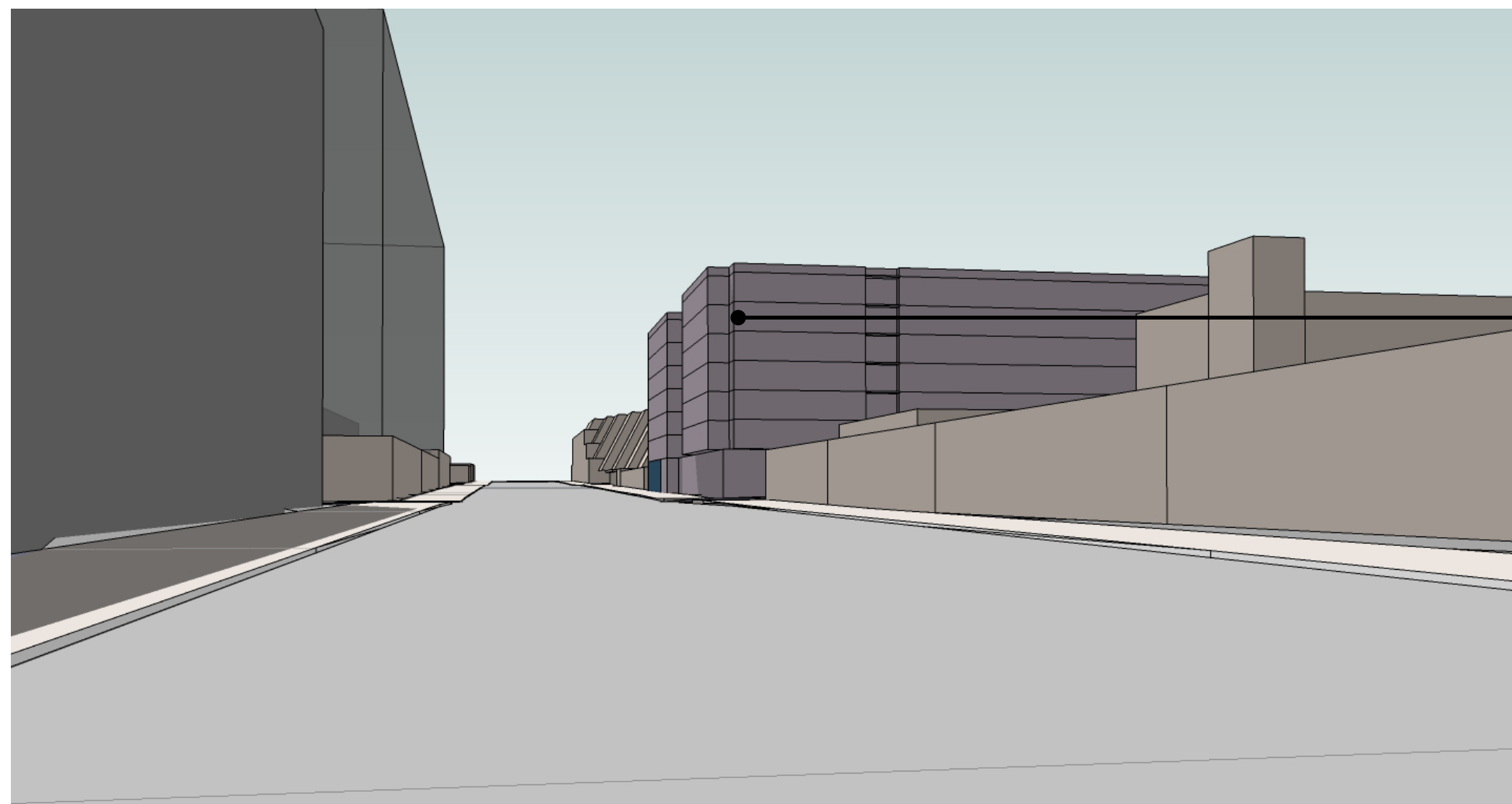
STREET-LEVEL RESIDENTIAL ALONG
DEXTER & REPUBLICAN



VIEW FROM NORTHWEST ON SR 99

STREET-LEVEL RESIDENTIAL ALONG
REPUBLICAN & SR 99

STREET-LEVEL RESIDENTIAL
AMENITY SPACE ALONG SR 99

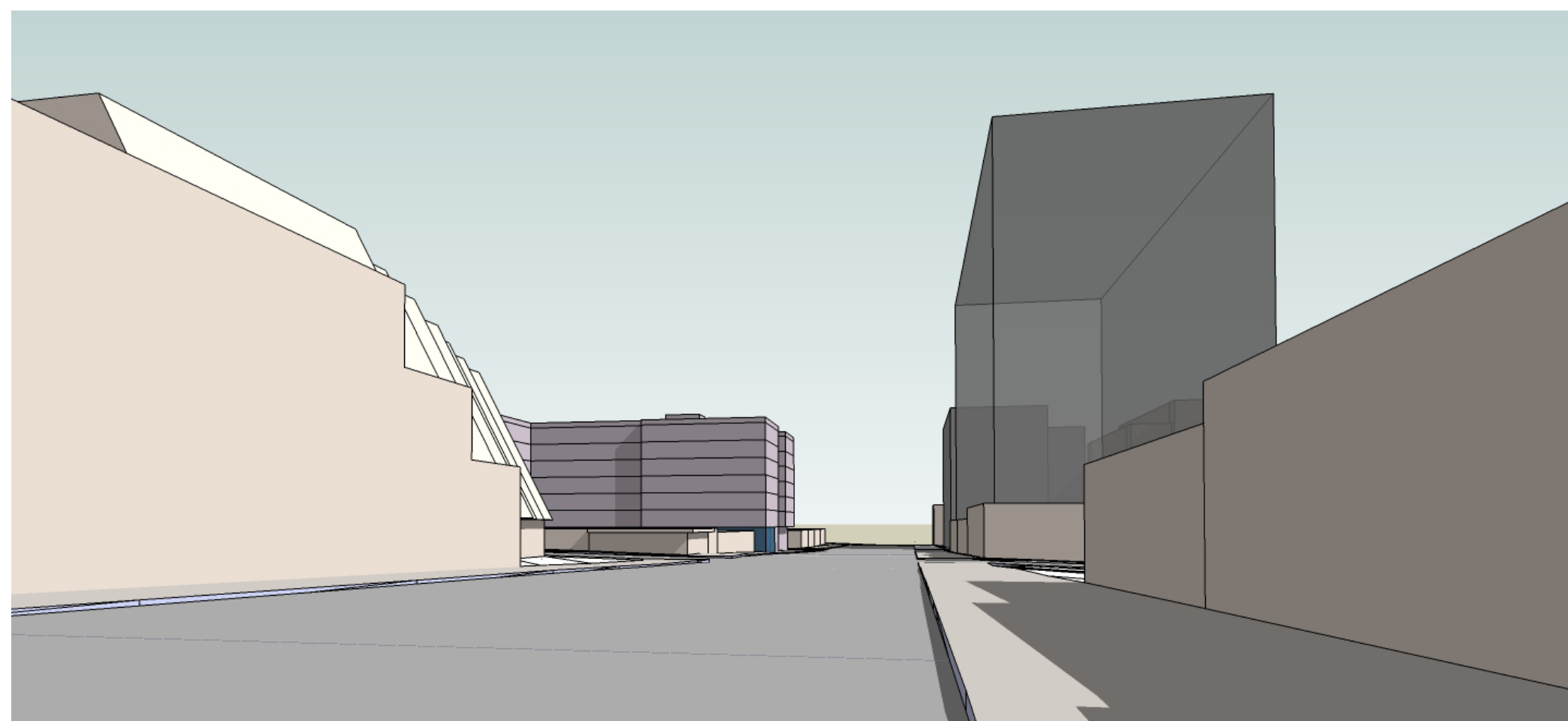


VIEW FROM NORTH ALONG DEXTER

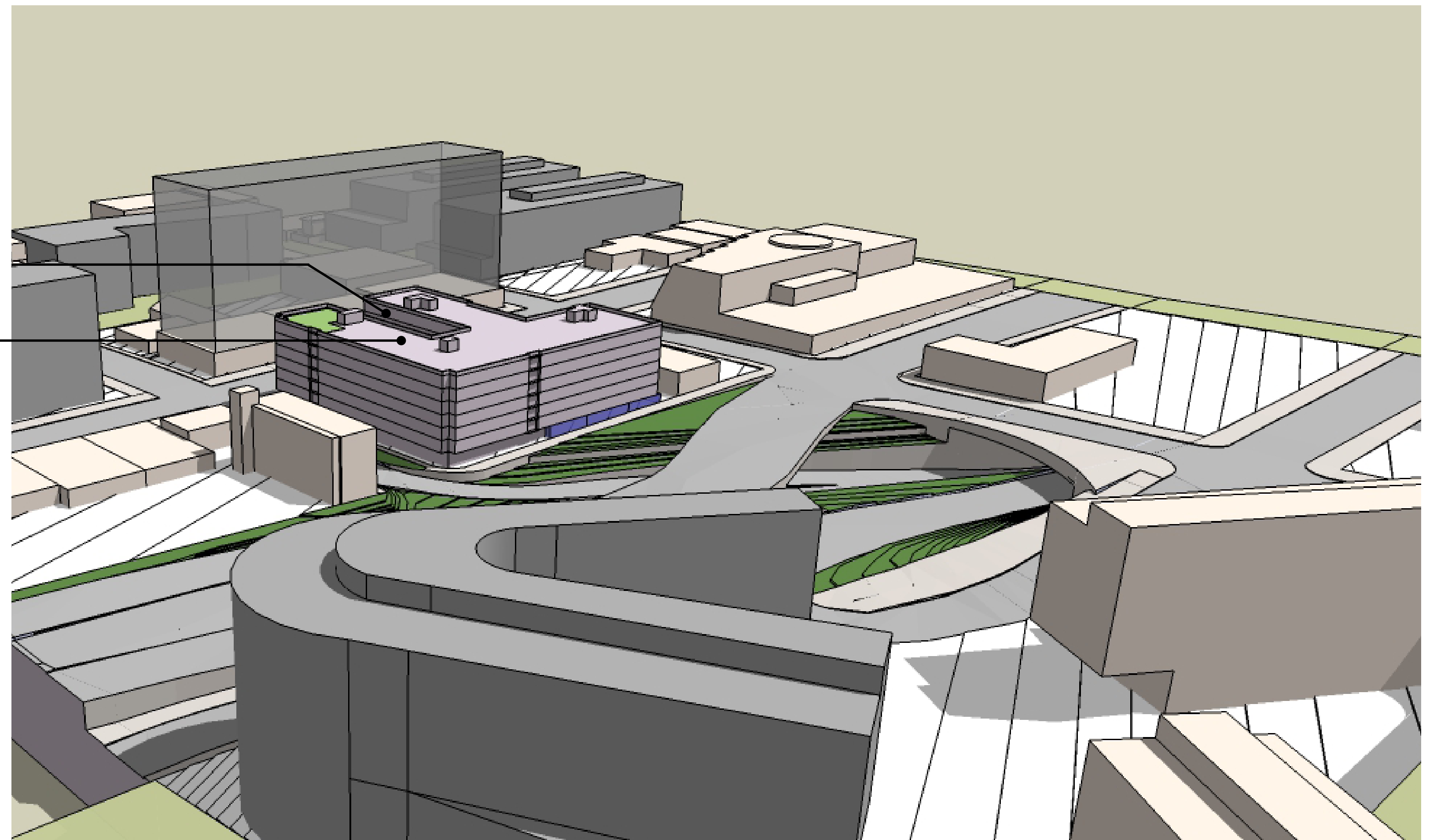
UNDIFFERENTIATED CORNER
AT DEXTER & REPUBLICAN

DEEP EAST FACING COURTYARD
ALLOWED BY MINIMAL MODULA-
TION AT FACING STREETS

ROOF TERRACE ORIENTED TO
DEXTER AVE



VIEW FROM SOUTH ALONG DEXTER

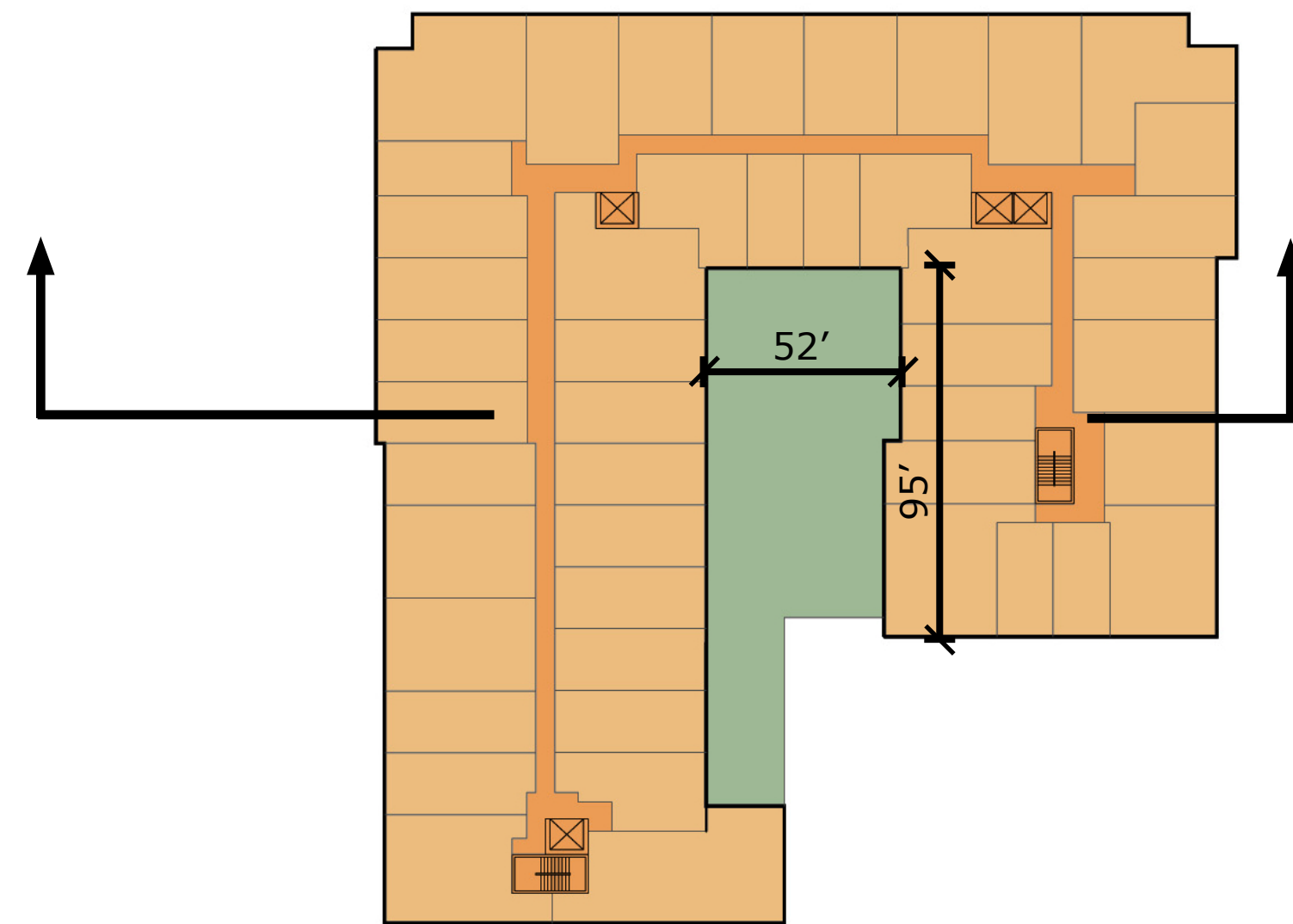


AERIAL VIEW FROM NORTHWEST

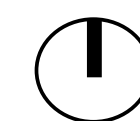
DESIGN ALTERNATIVE F

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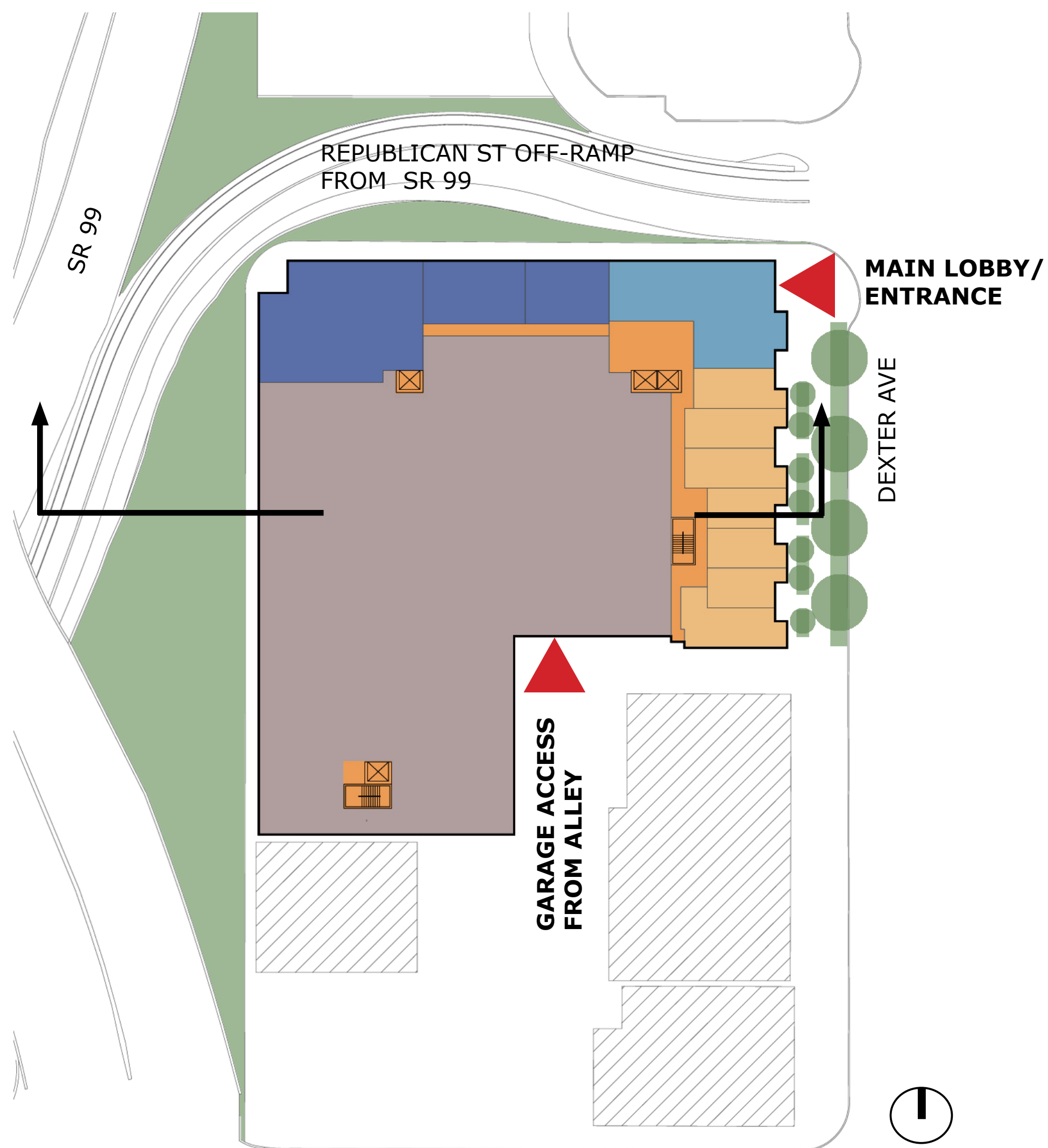


FLOOR 2 (3-7 SIM)



- KEY**
- LOBBY
 - AMENITY
 - RESIDENTIAL
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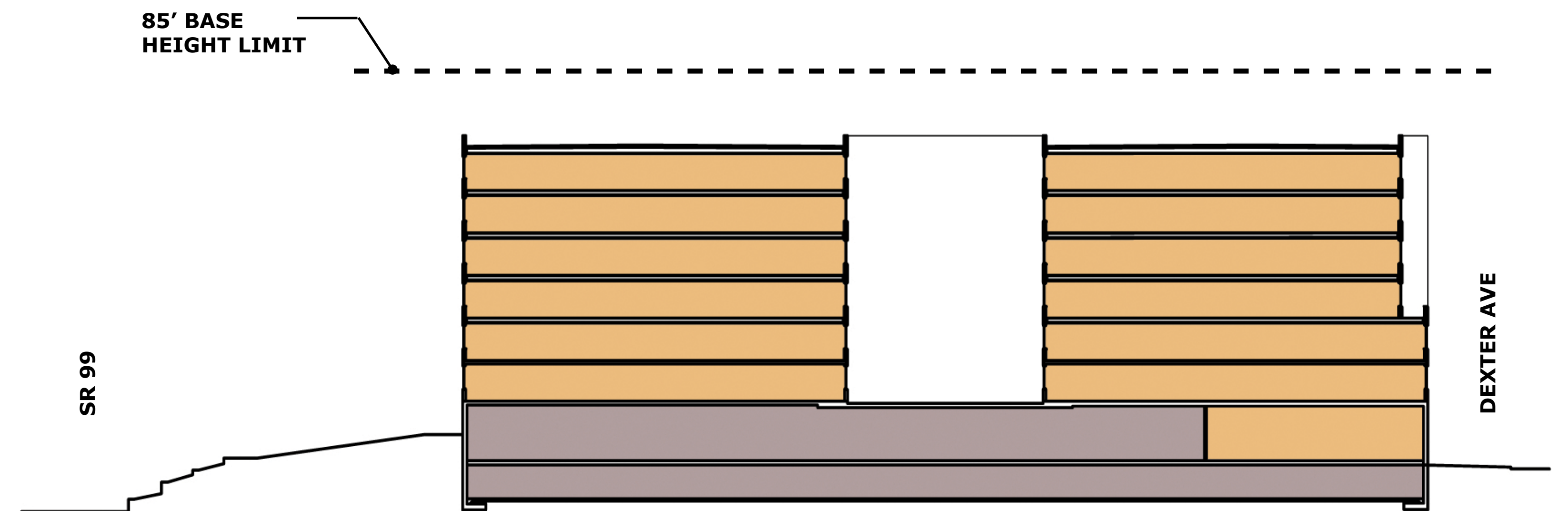
- DATA**
- 7 FLOORS
 - 294 UNITS
 - 248 PARKING STALLS
 - 2 STAIR TOWERS



SITE PLAN/ FLOOR 1



85' BASE
HEIGHT LIMIT



SECTION LOOKING NORTH

DESIGN ALTERNATIVE C (PREFERRED)



VIEW FROM NORTHEAST AT DEXTER/REPUBLICAN

- UPPER-LEVEL SETBACK ALONG DEXTER
- MAIN BUILDING ENTRY AT CORNER OF ALLEY & DEXTER
- STREET-LEVEL RESIDENTIAL AMENITY ALONG REPUBLICAN
- STREET-LEVEL RESIDENTIAL ALONG DEXTER WITH INDIVIDUAL ENTRIES ALONG SIDEWALK

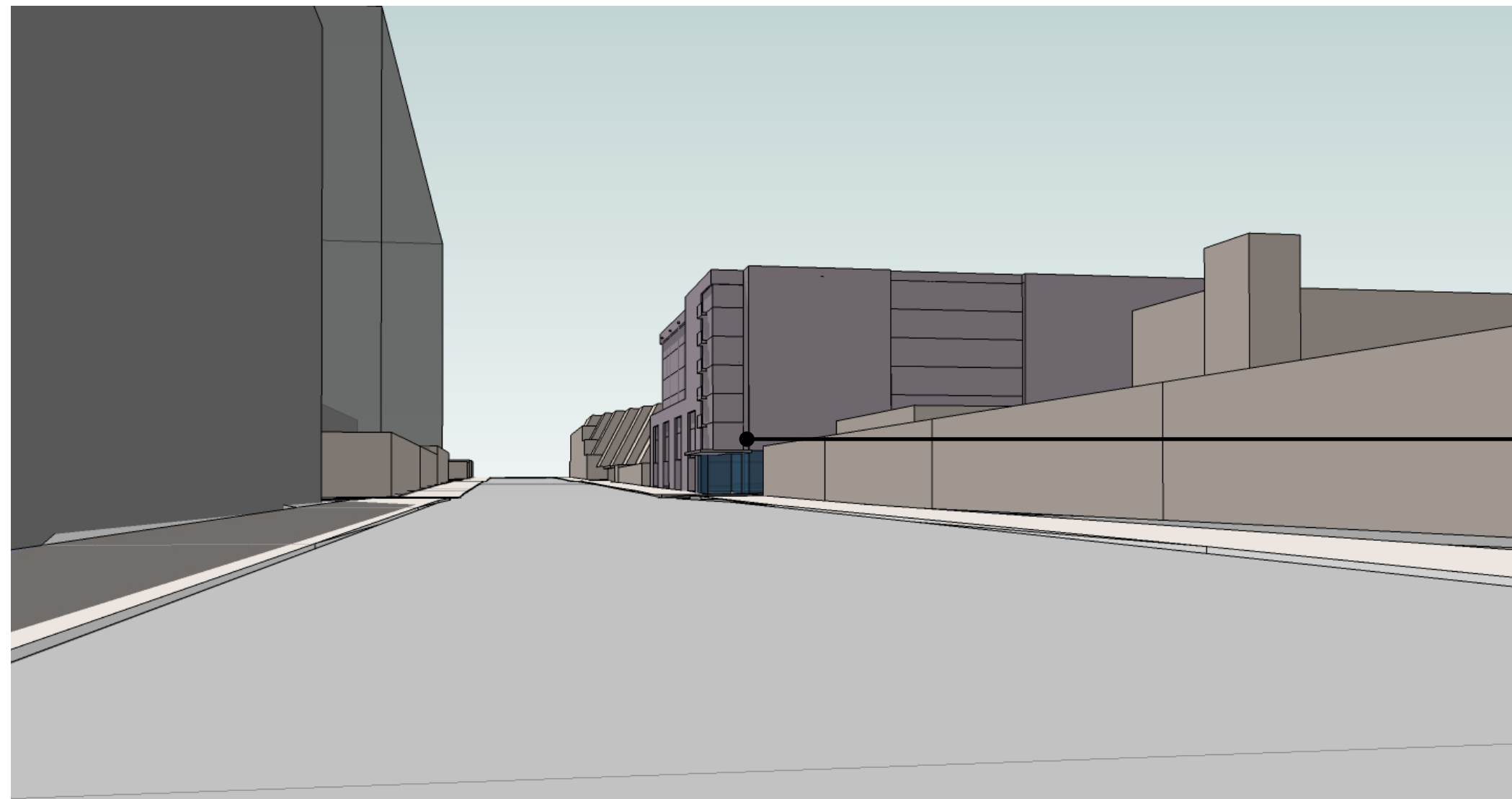
EDGE CONDITION ALONG SR 99-MAJOR HIGHWAY, FEW PEDESTRIANS, VISIBILITY FROM DISTANCE

STREET-LEVEL RESIDENTIAL AMENITY SPACES ALONG REPUBLICAN

OPPORTUNITY FOR SCREENING BETWEEN PARKING & SR 99 ROW TO DISCOURAGE GRAFFITI & PROVIDE VISUAL INTEREST



VIEW FROM NORTHWEST ON SR 99



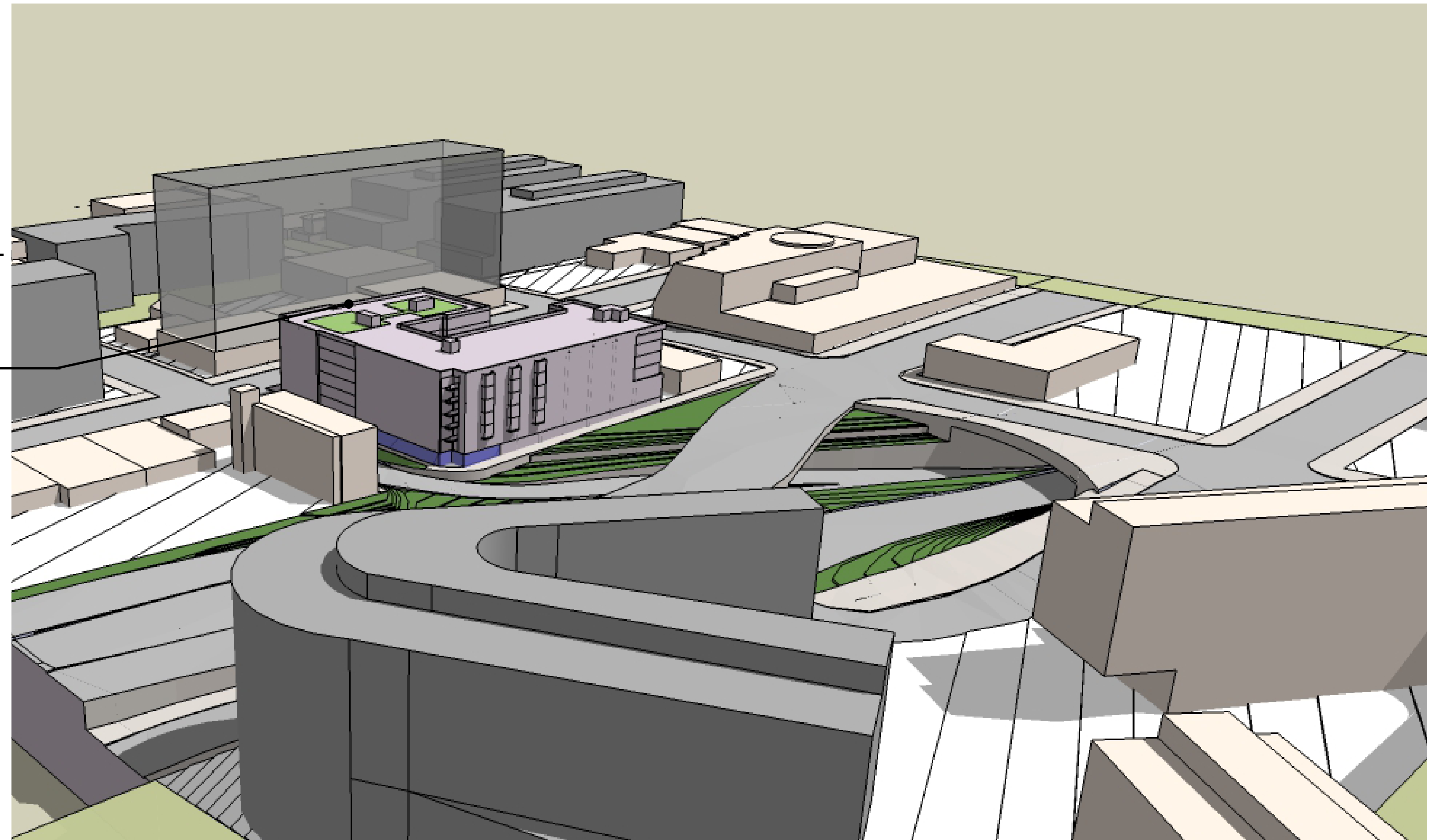
VIEW FROM NORTH ALONG DEXTER

- OPPORTUNITY FOR CORNER TREATMENT THAT ADDRESSES UNIQUE GATEWAY AT DEXTER & REPUBLICAN
- ROOF TERRACE ORIENTED TO DEXTER, AWAY FROM SR 99



VIEW FROM SOUTH ALONG DEXTER

- VISIBILITY TO CORNER OF DEXTER & ALLEY FROM SOUTH



AERIAL VIEW FROM NORTHWEST

DESIGN ALTERNATIVE C (PREFERRED)



VIEW FROM 6TH AVENUE LOOKING EAST

DESIGN ALTERNATIVE C (PREFERRED): BUILDING CHARACTER



VIEW FROM DEXTER AND REPUBLICAN

DESIGN ALTERNATIVE C (PREFERRED): BUILDING CHARACTER



VIEW ALONG DEXTER LOOKING NORTH

DESIGN ALTERNATIVE C (PREFERRED): STREETScape CHARACTER



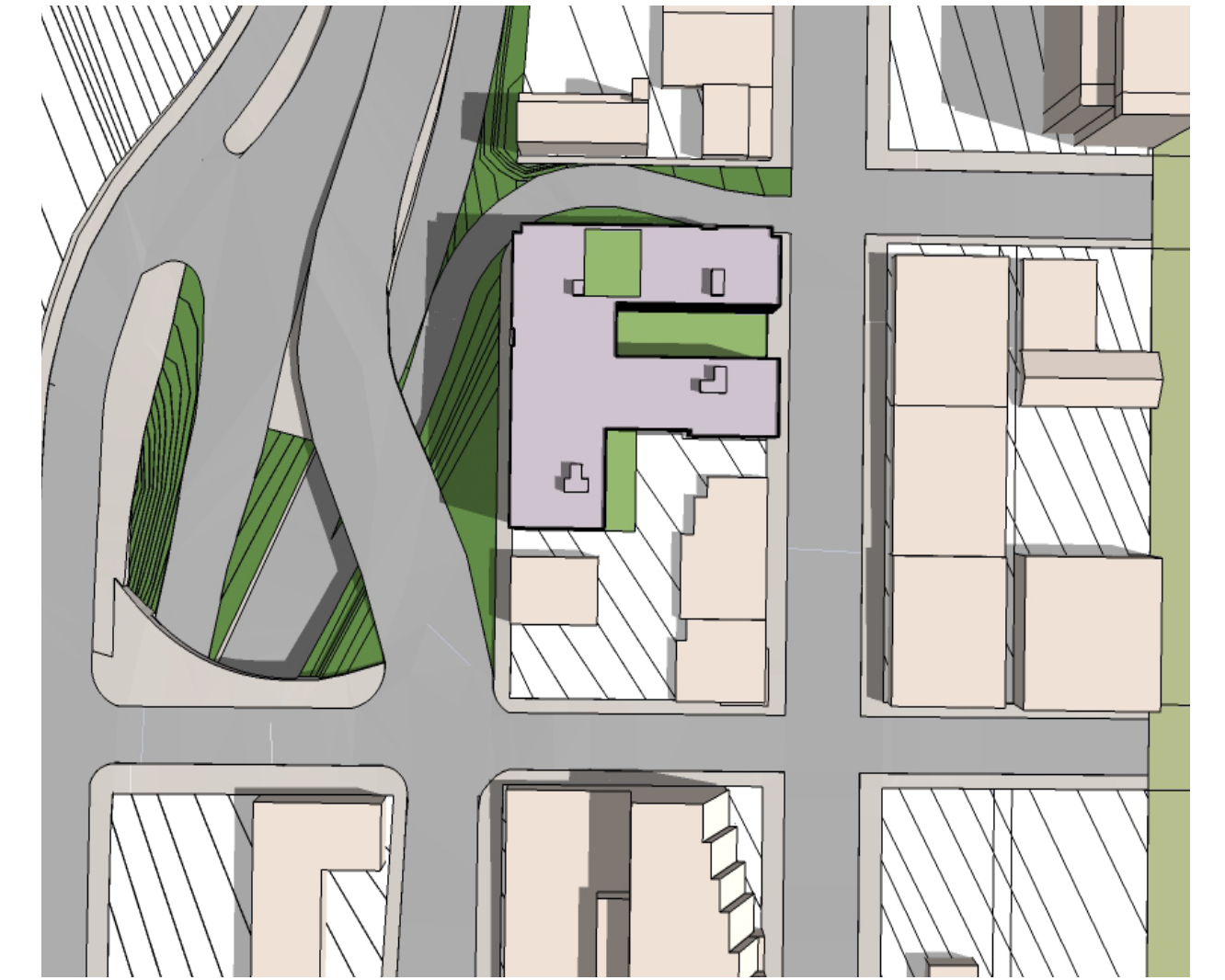
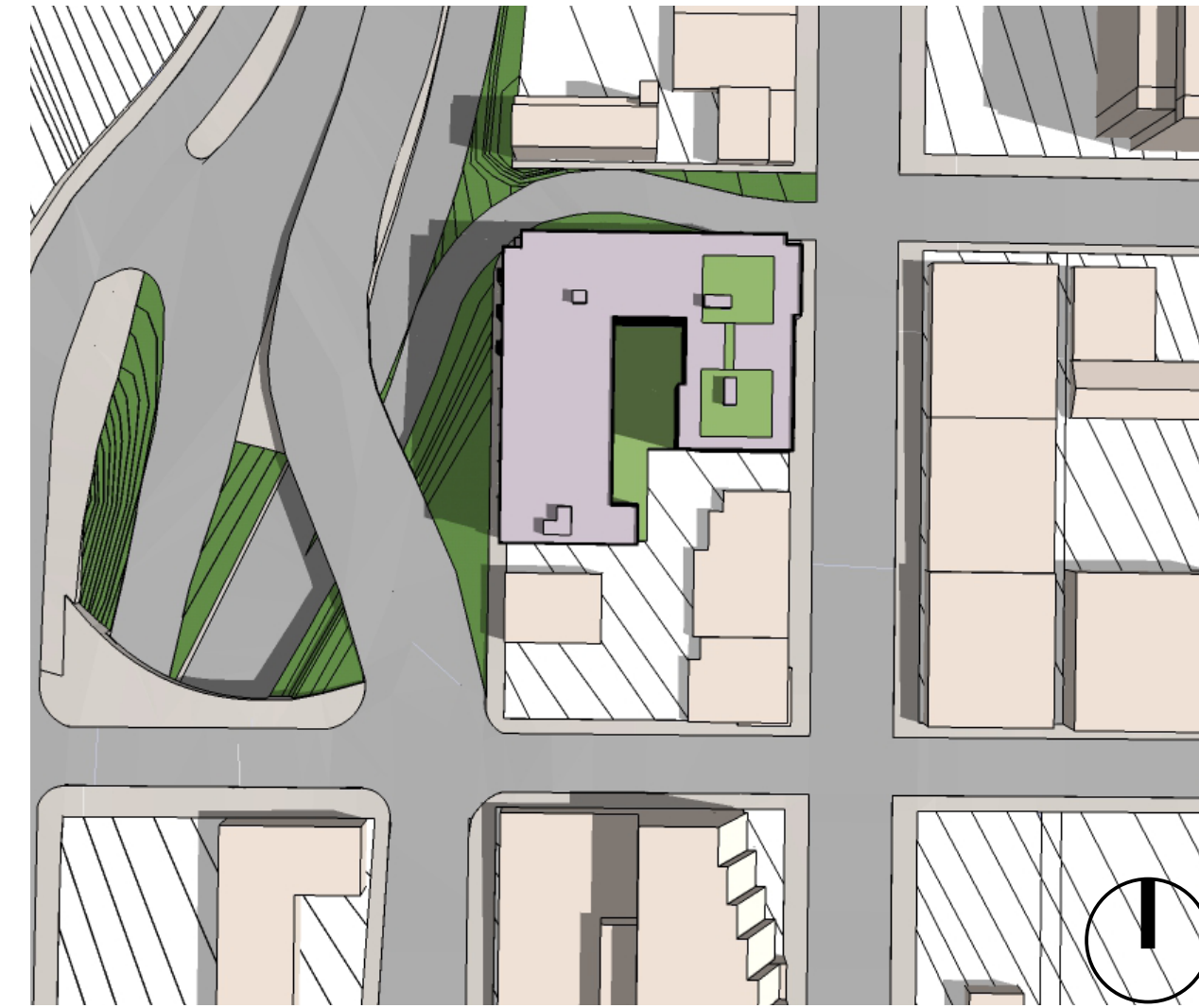
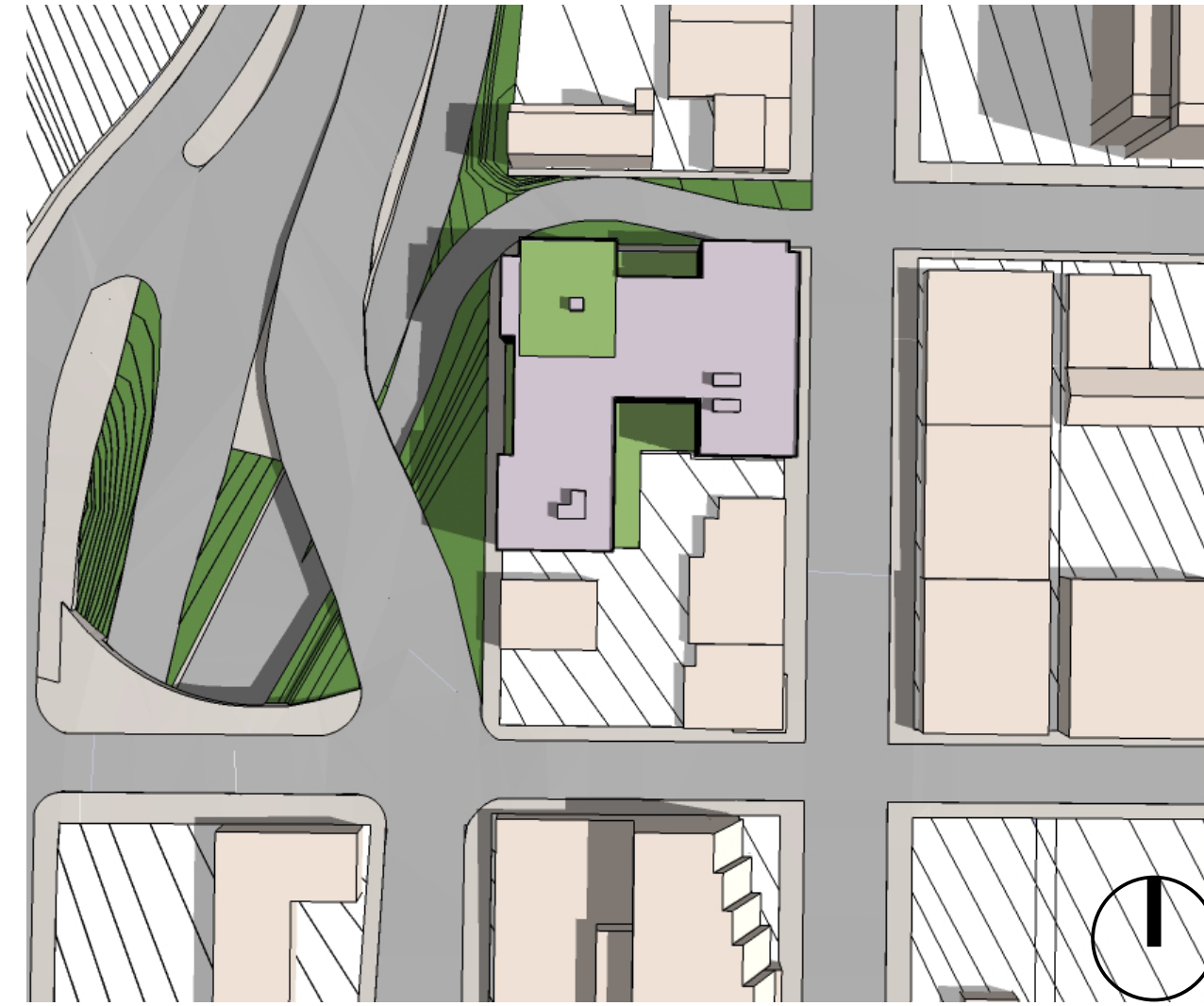
ALTERNATIVE A

ALTERNATIVE B

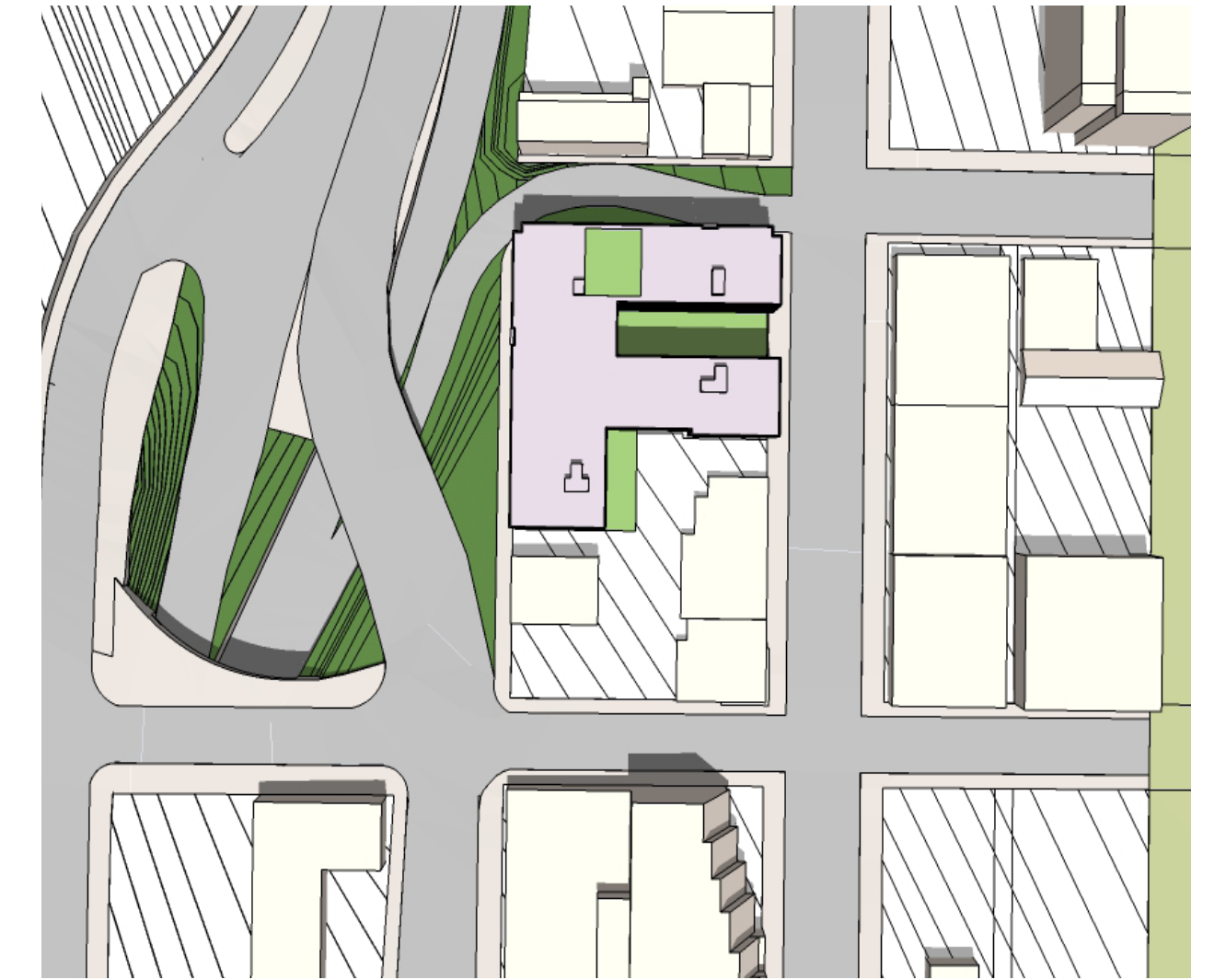
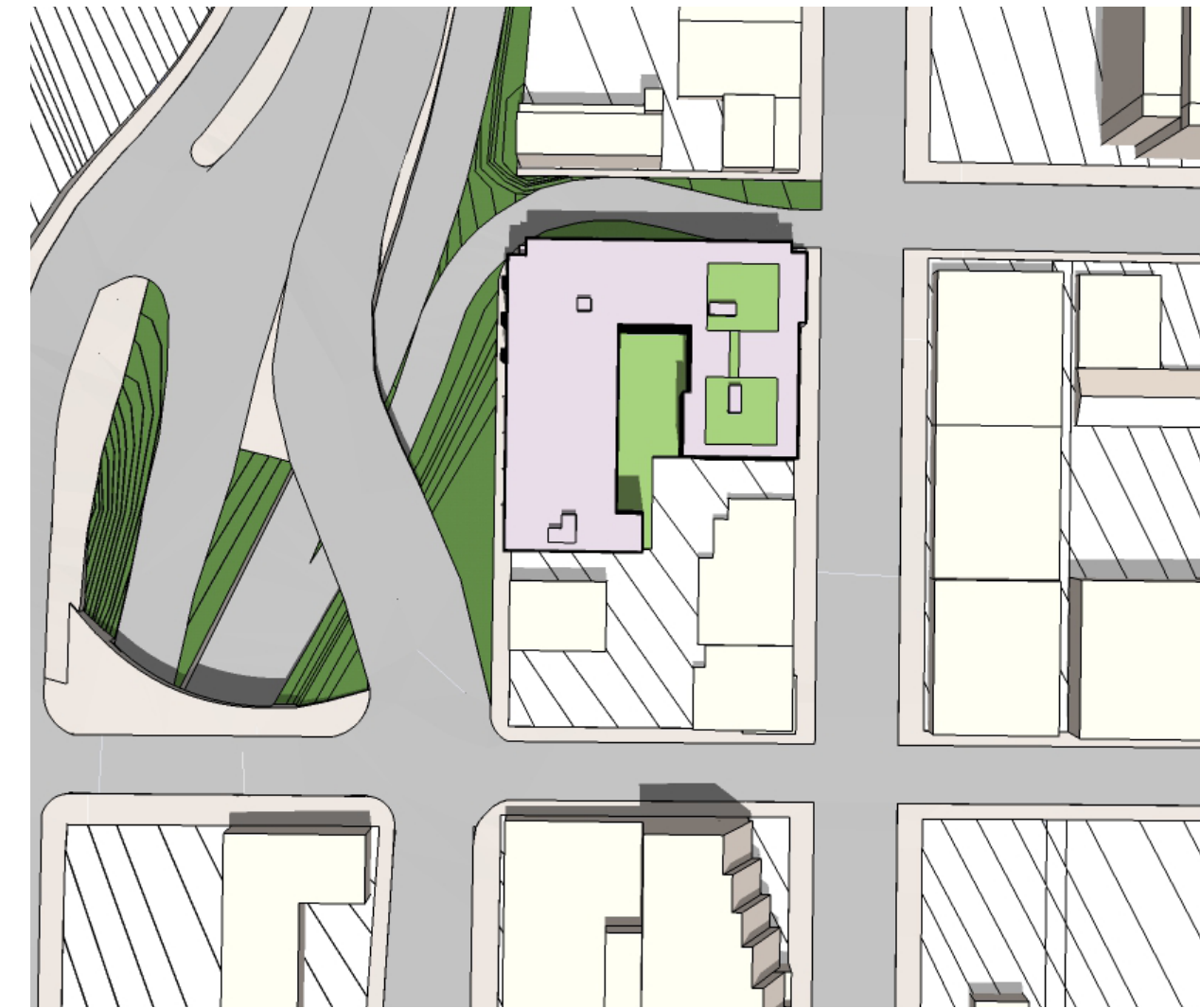
ALTERNATIVE C: PREFERRED

ALTERNATIVE F

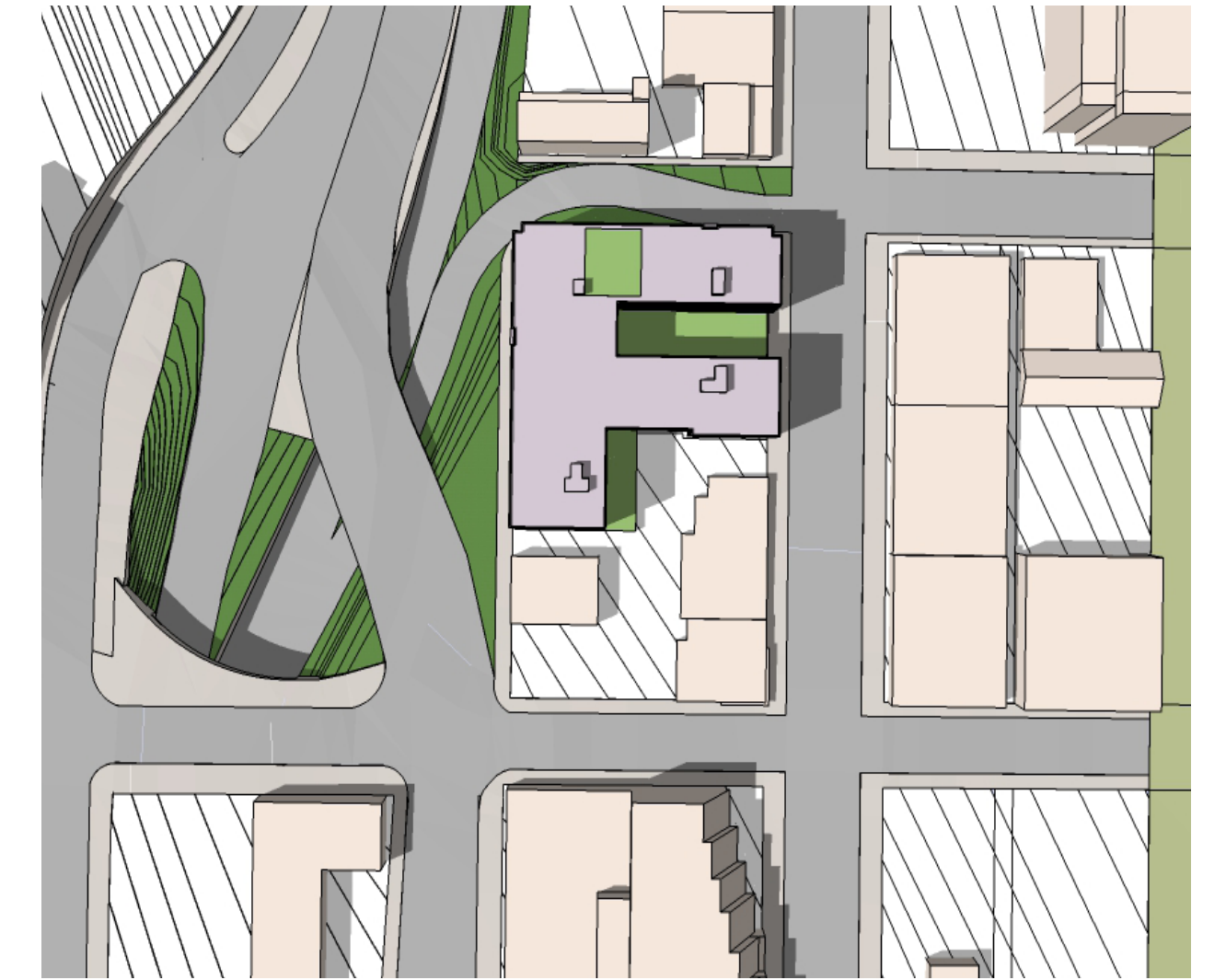
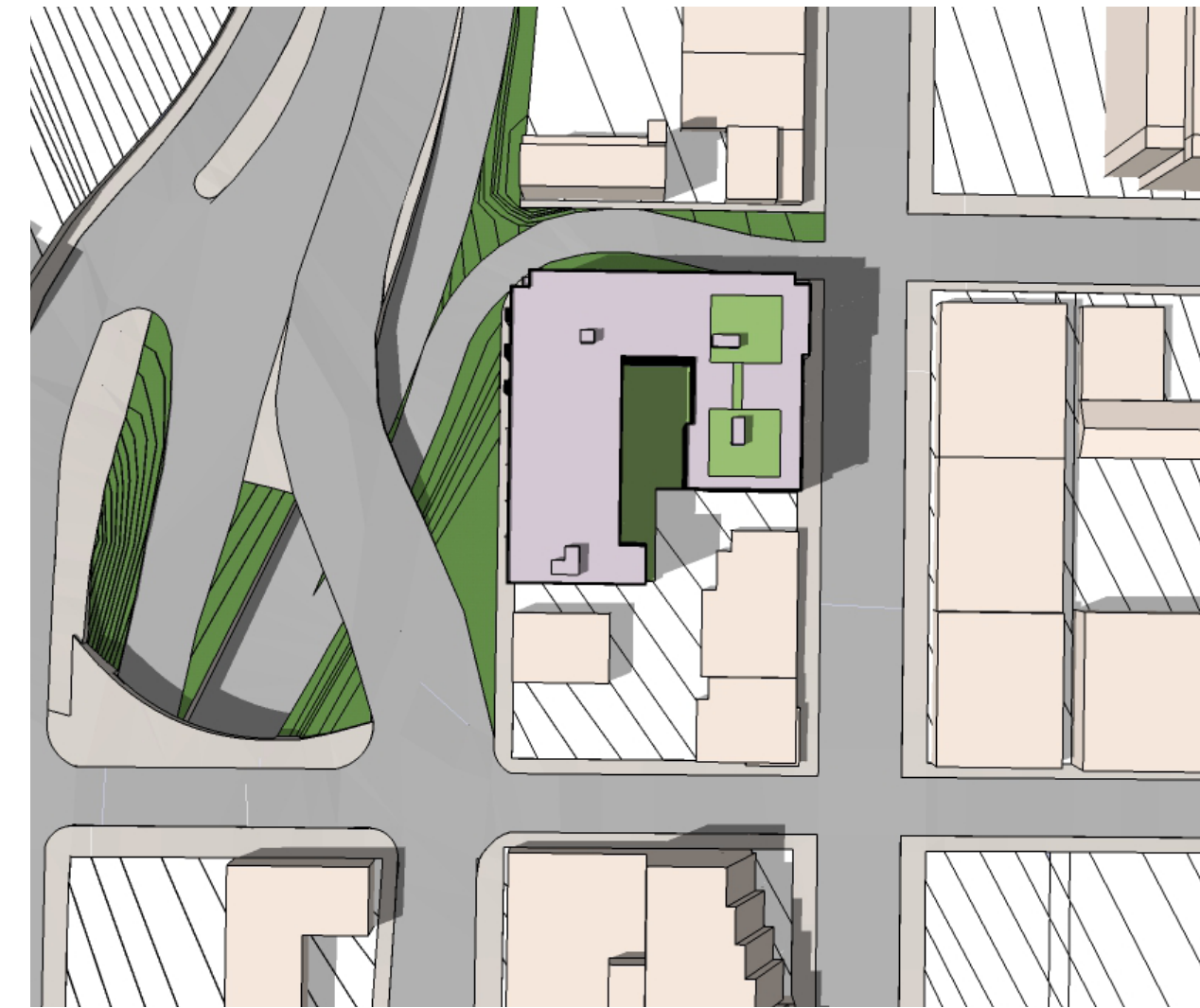
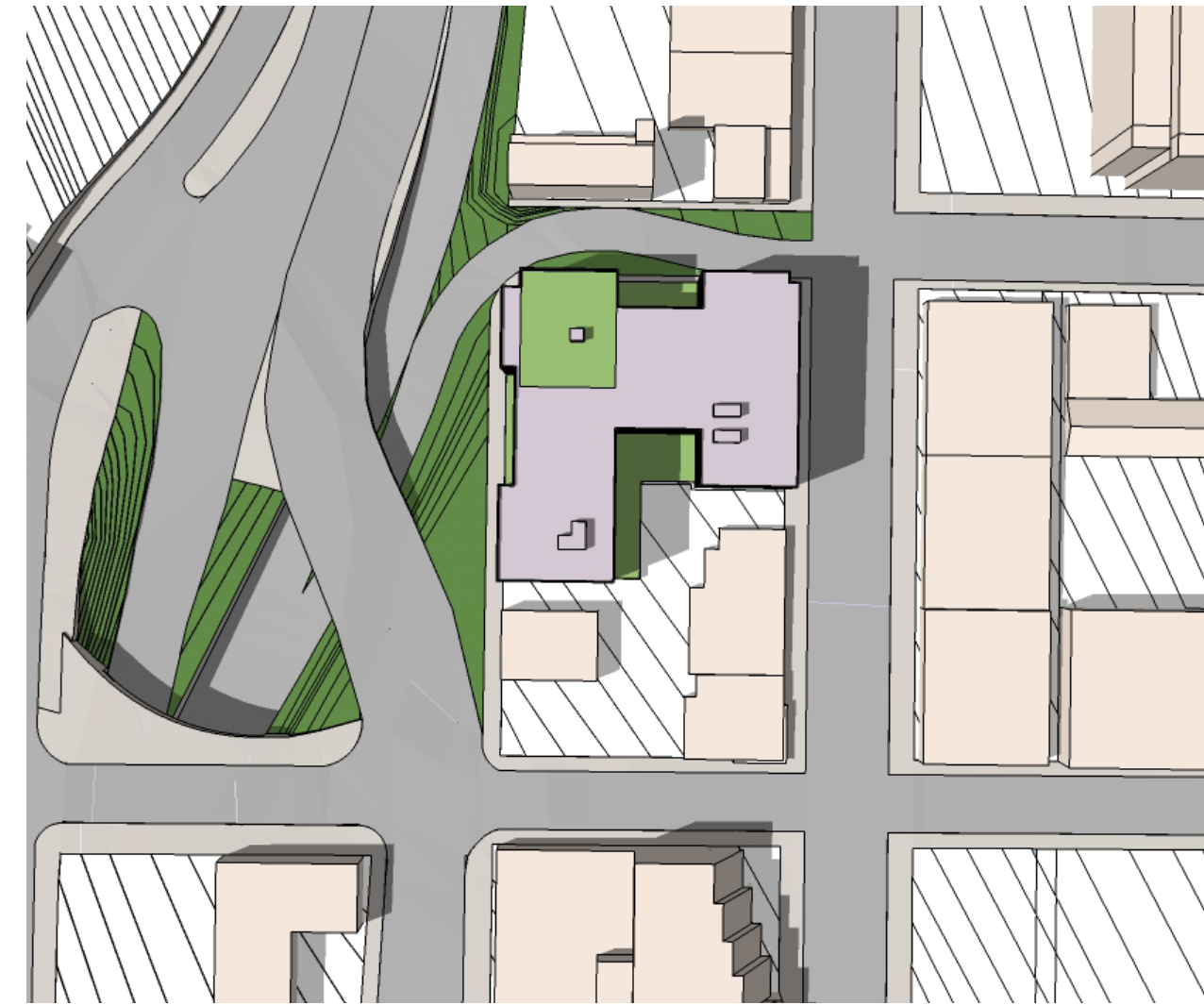
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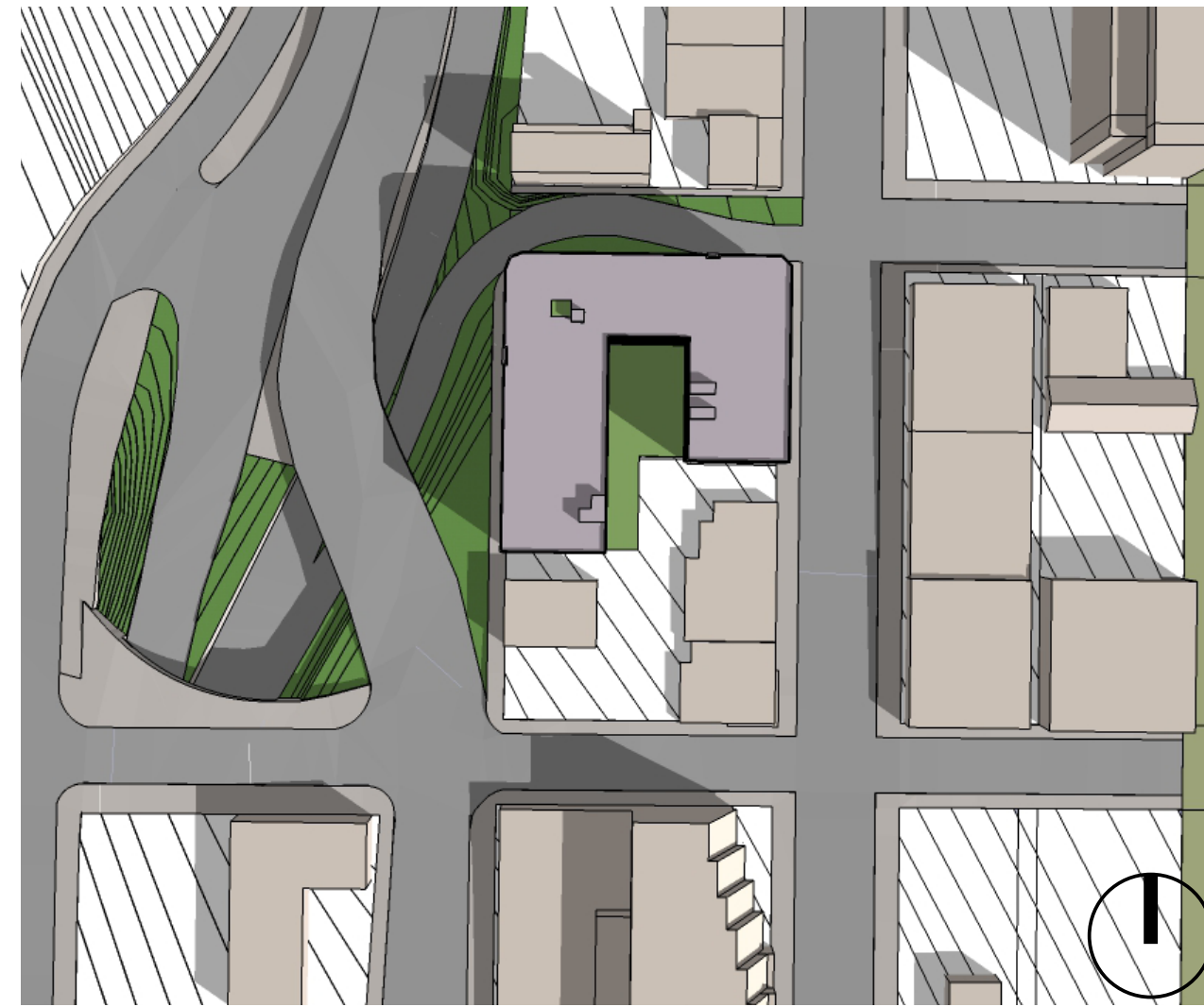


3PM



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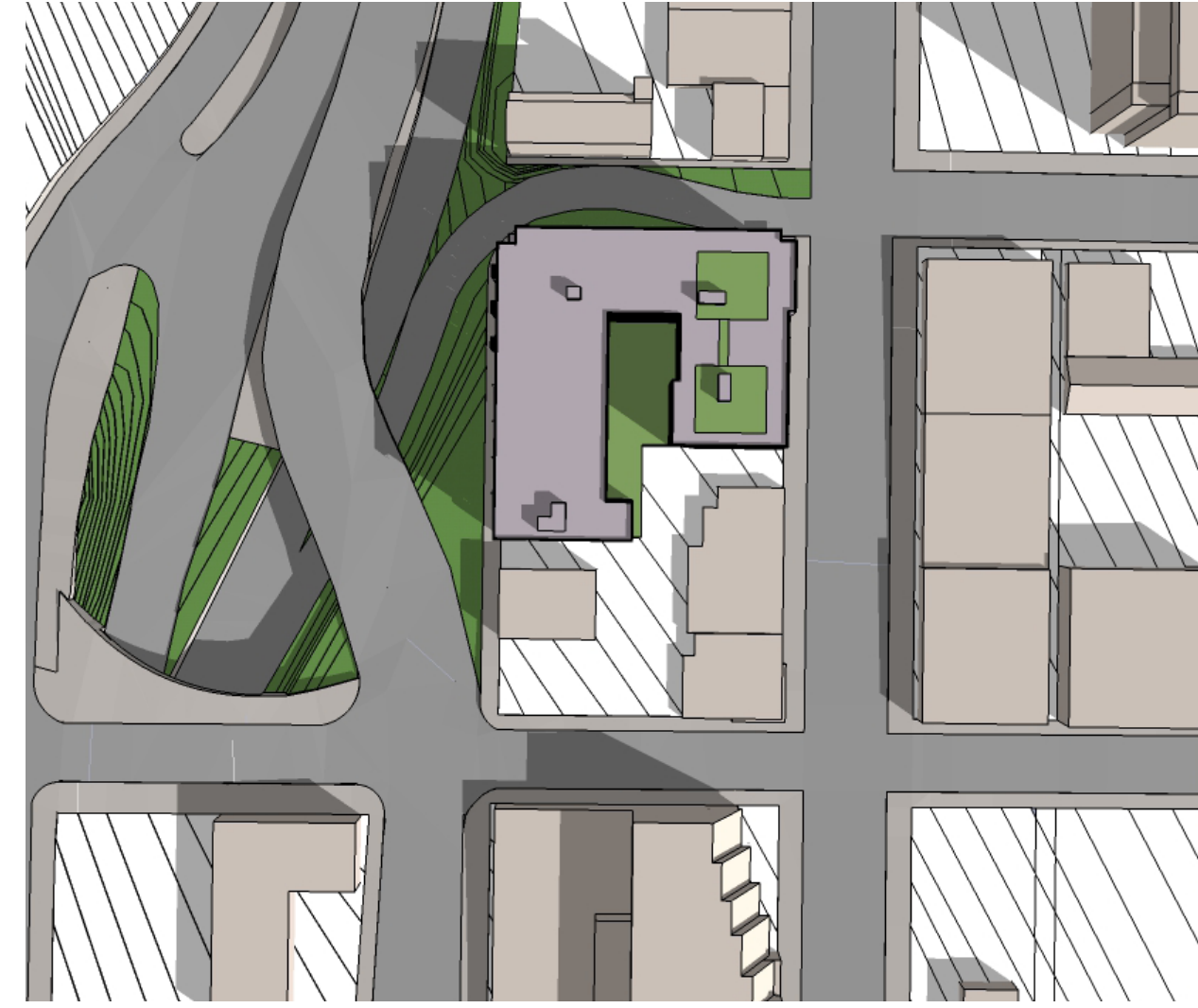
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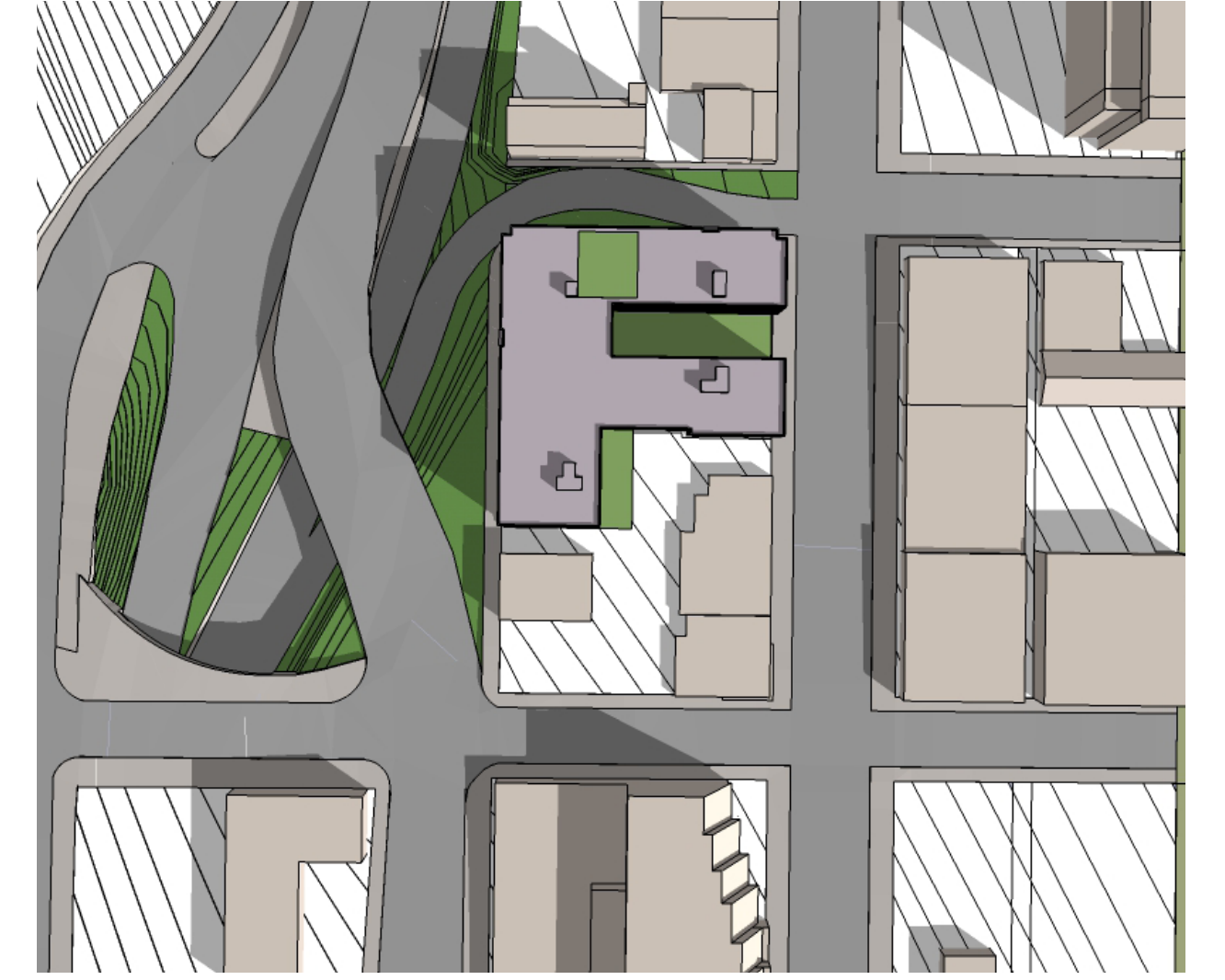
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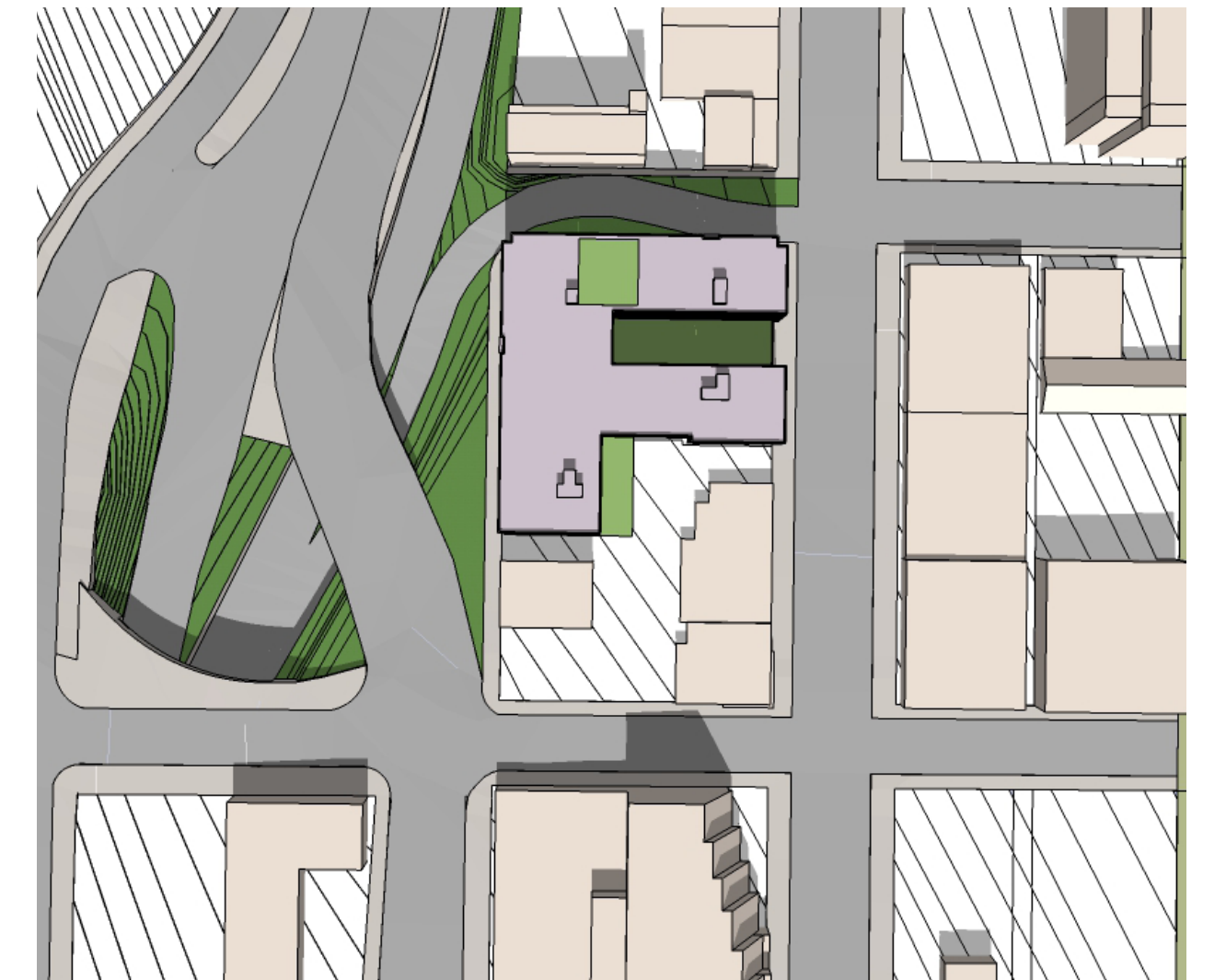
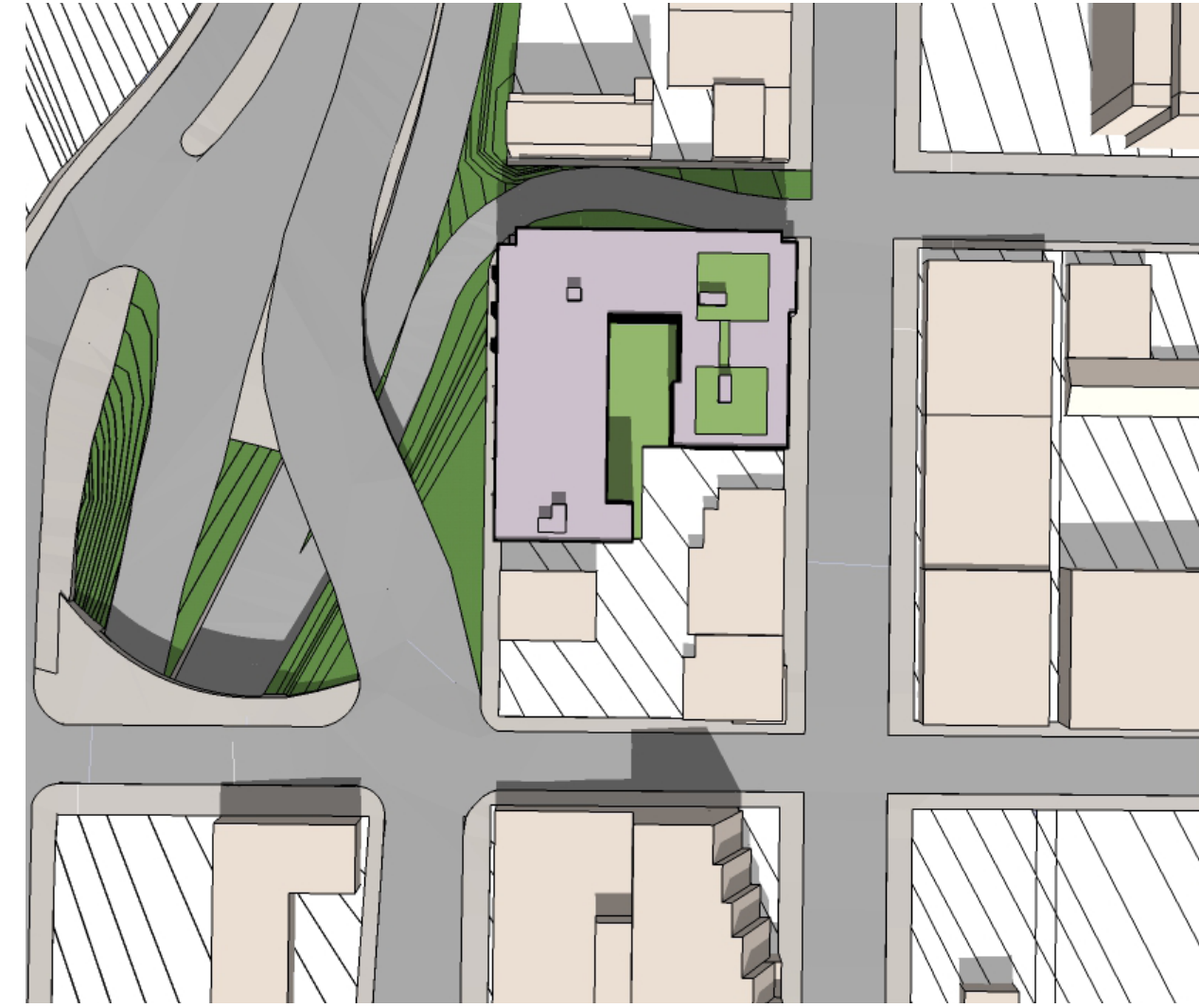
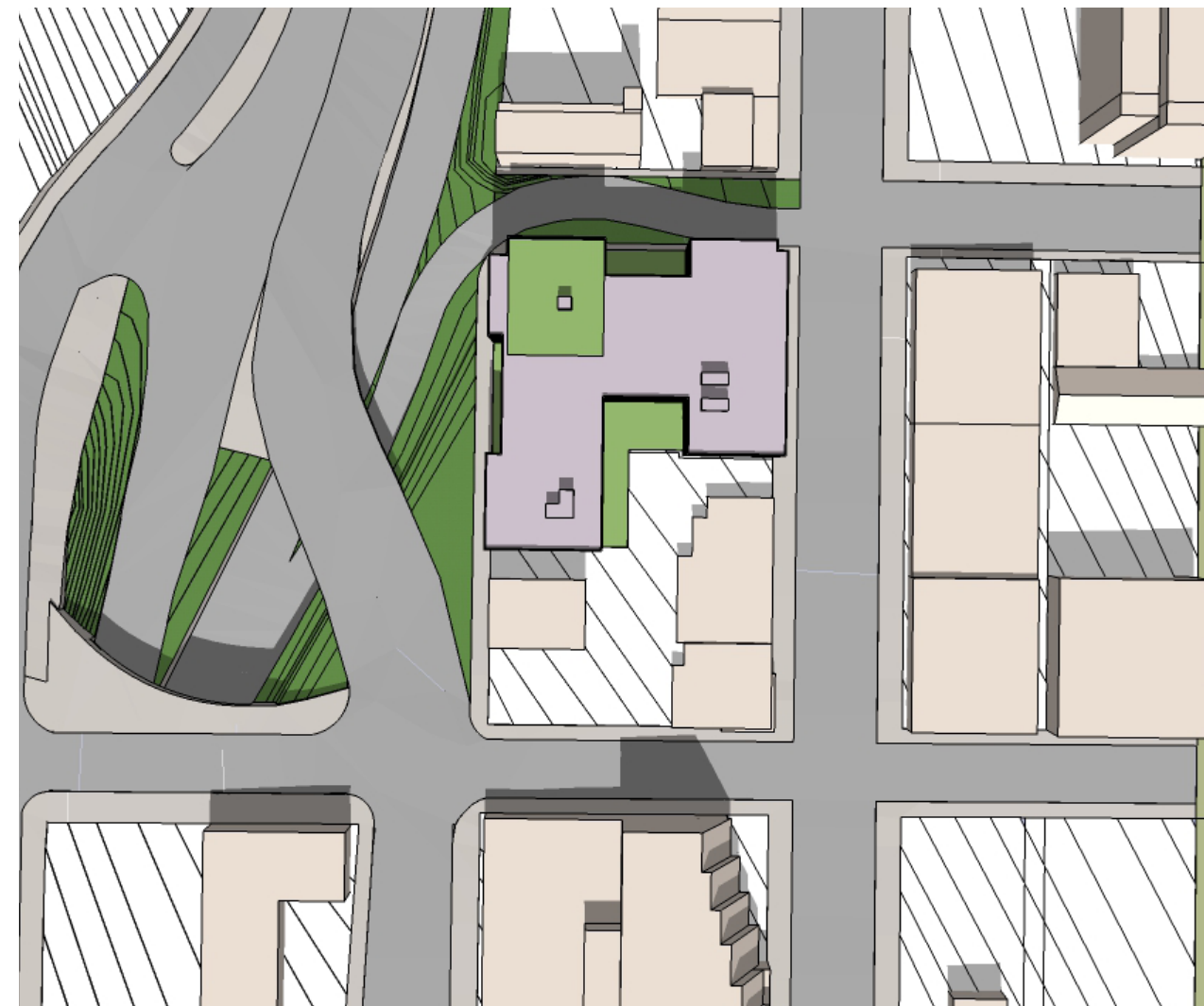
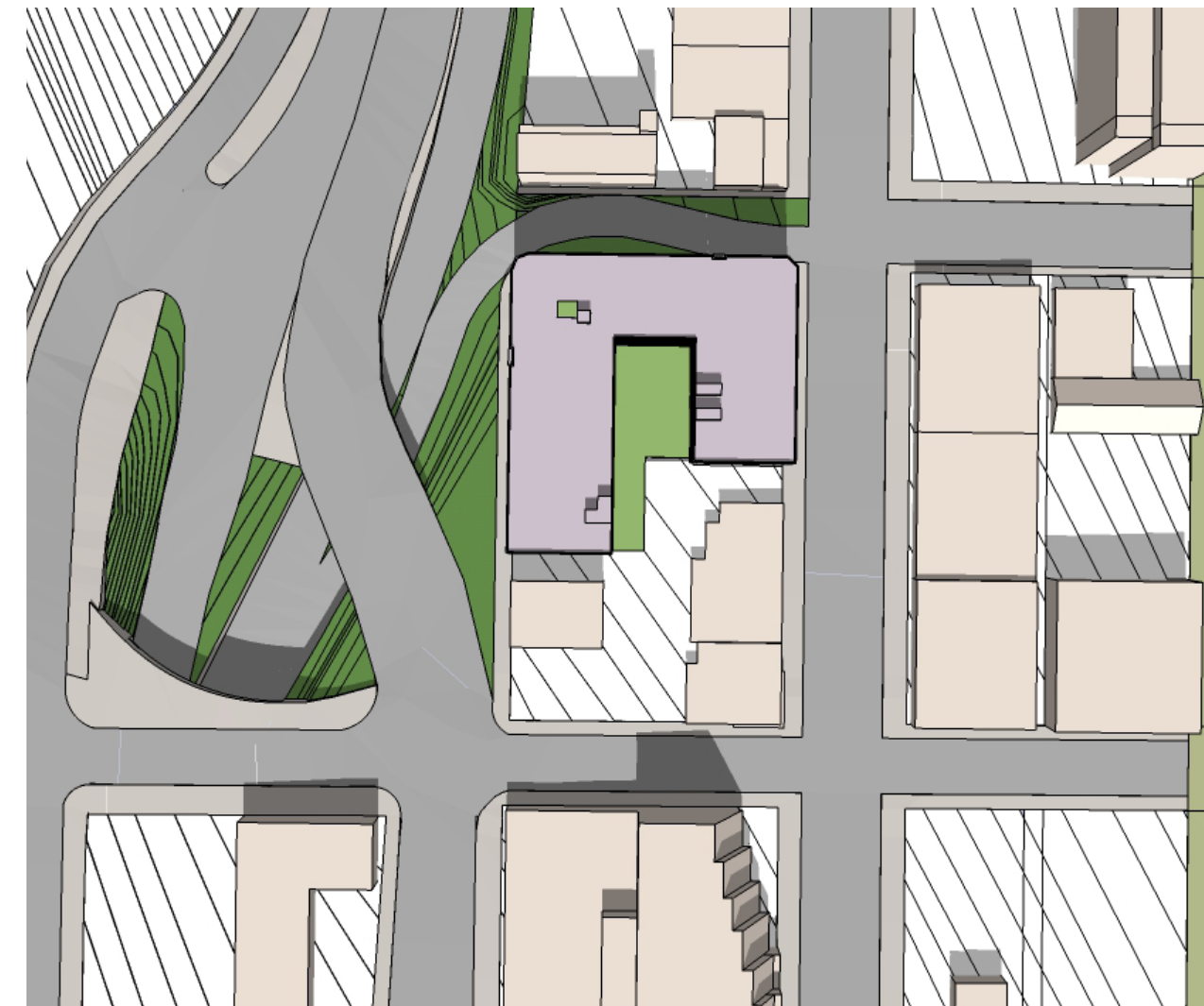
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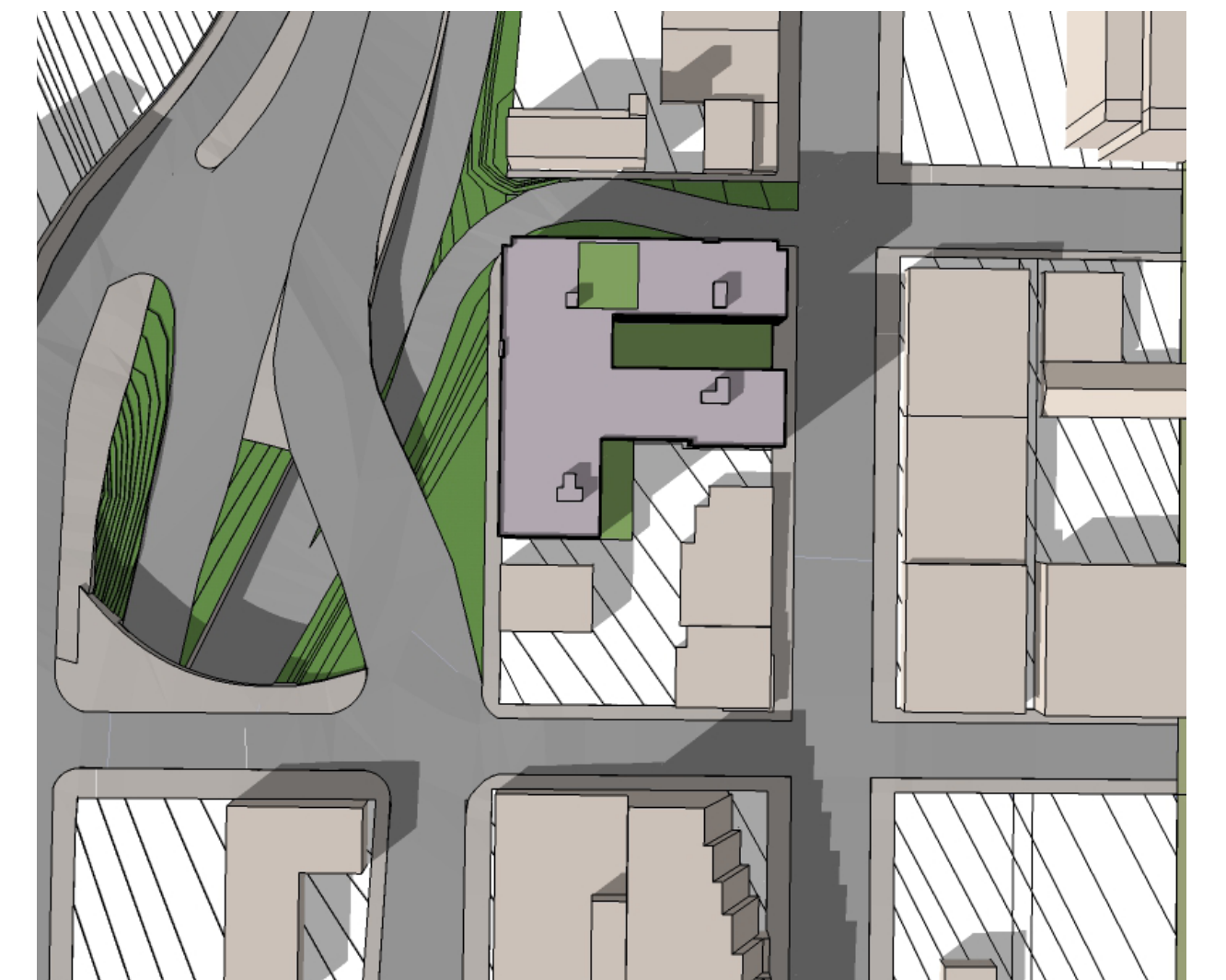
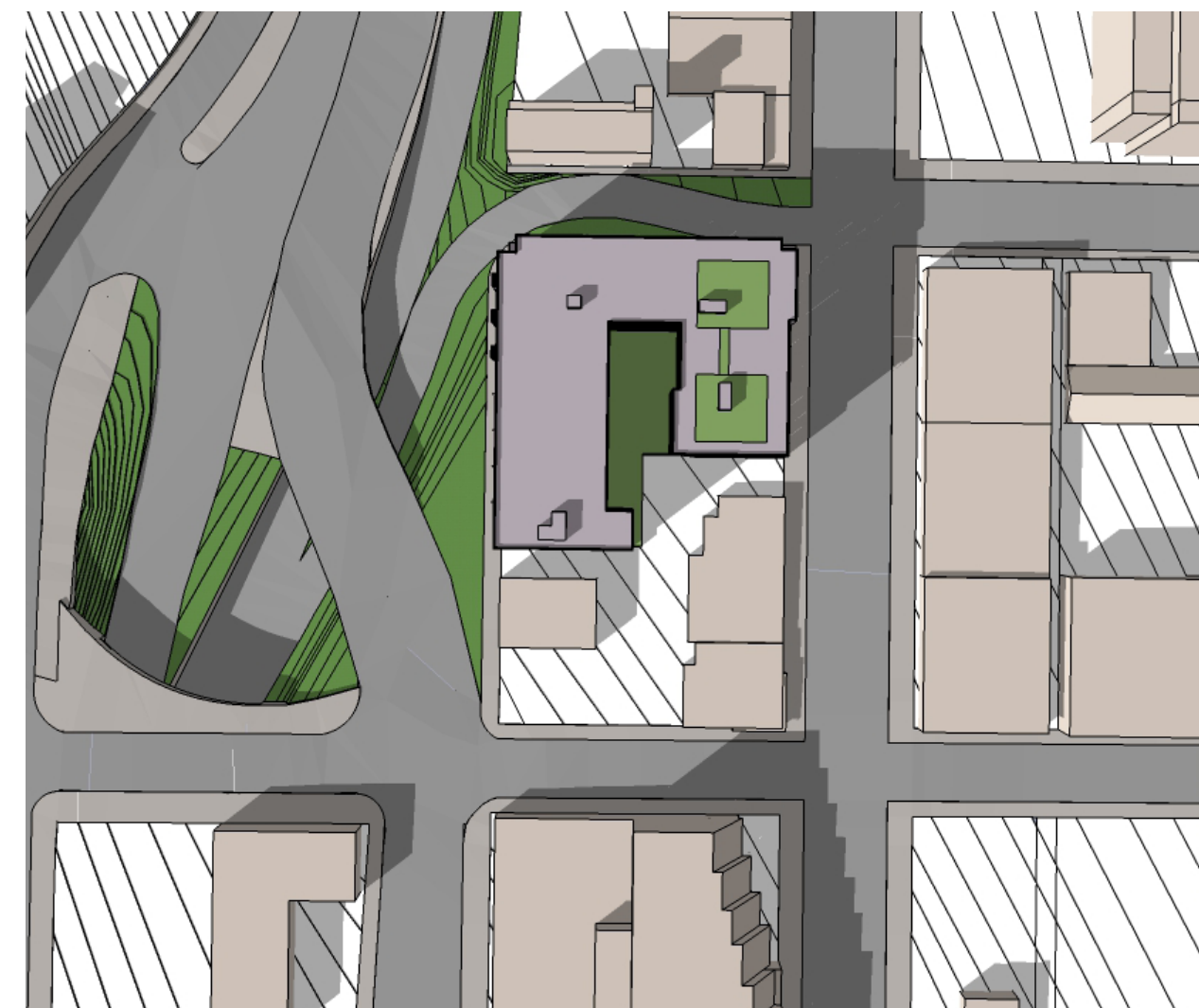
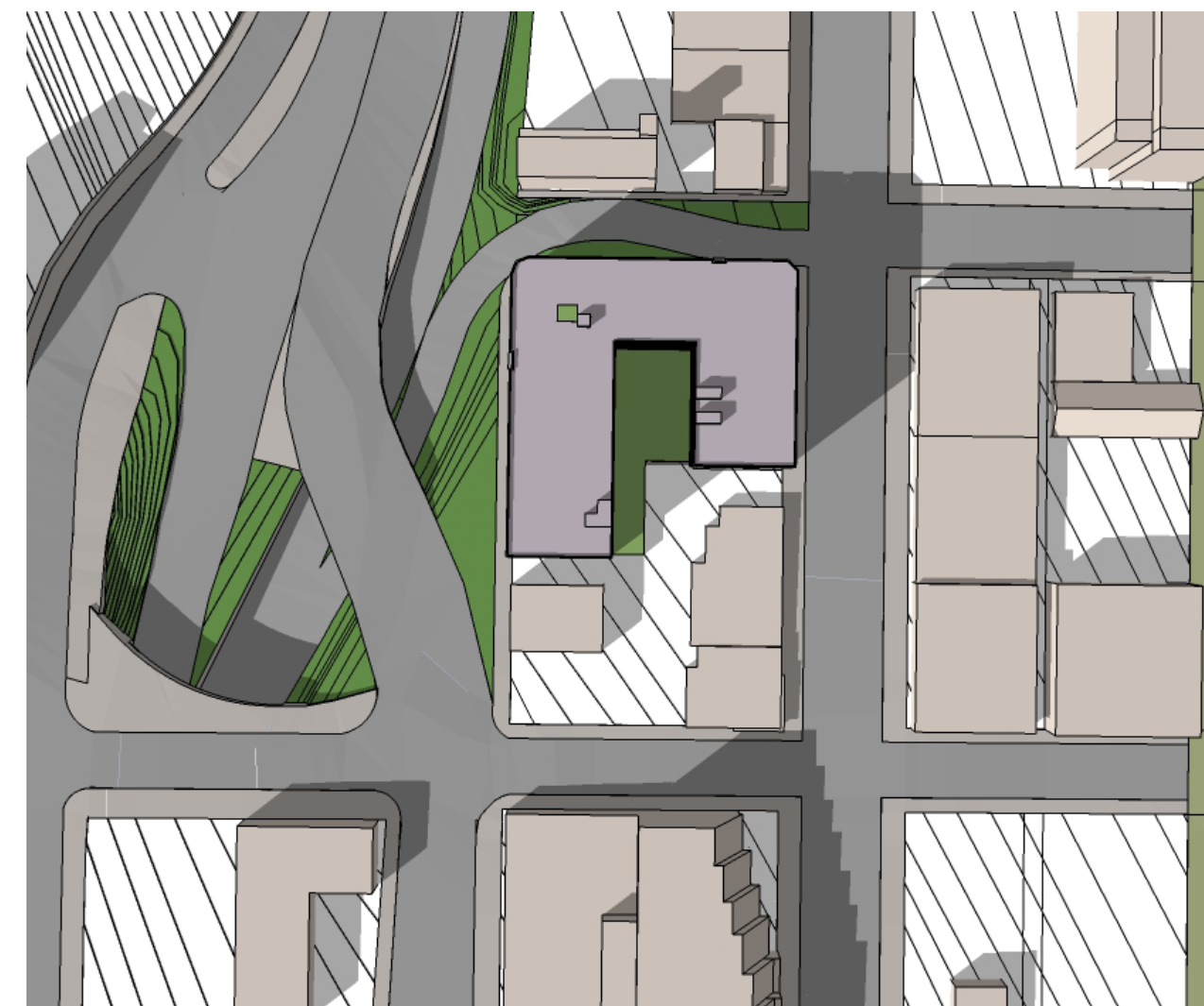
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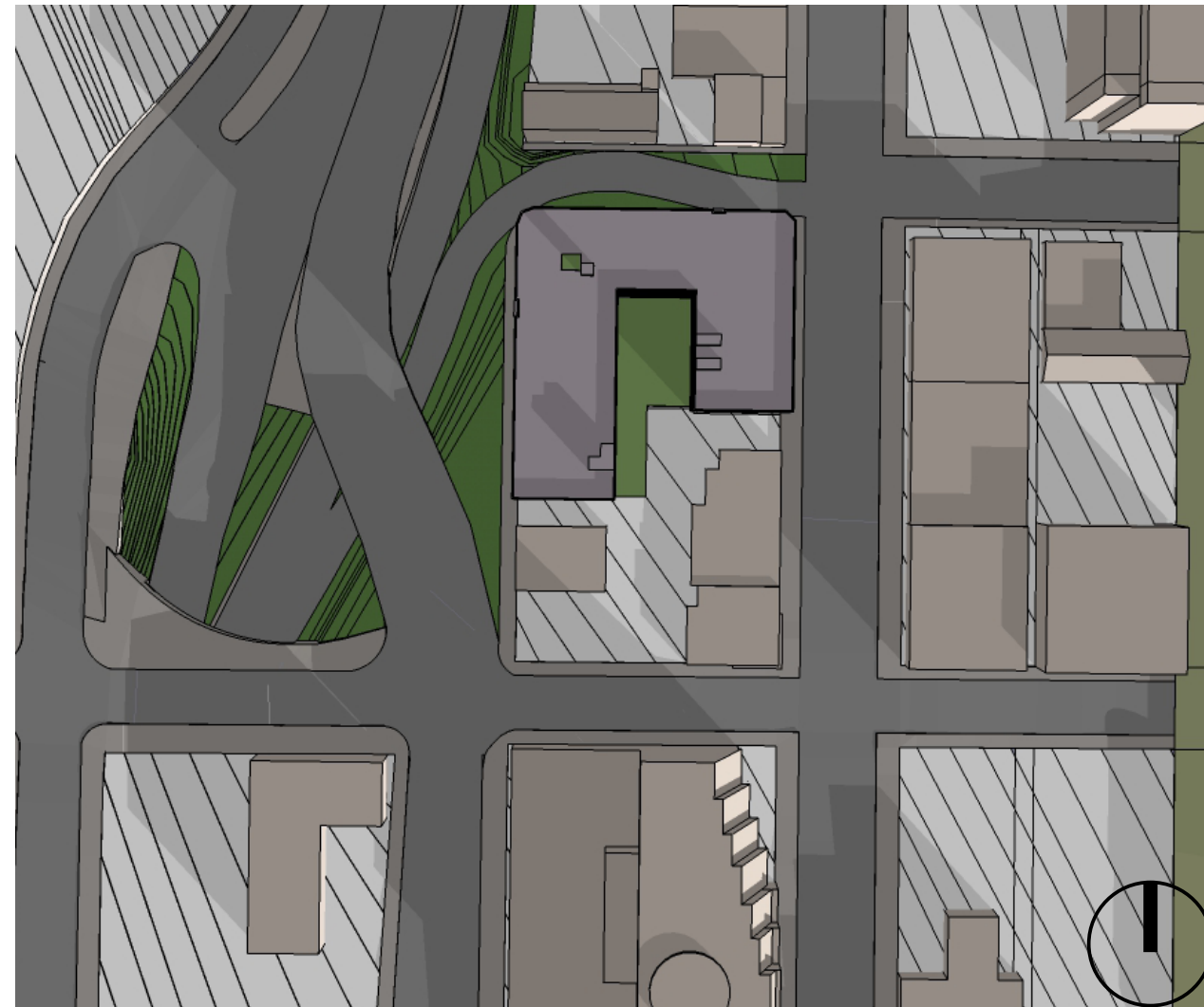
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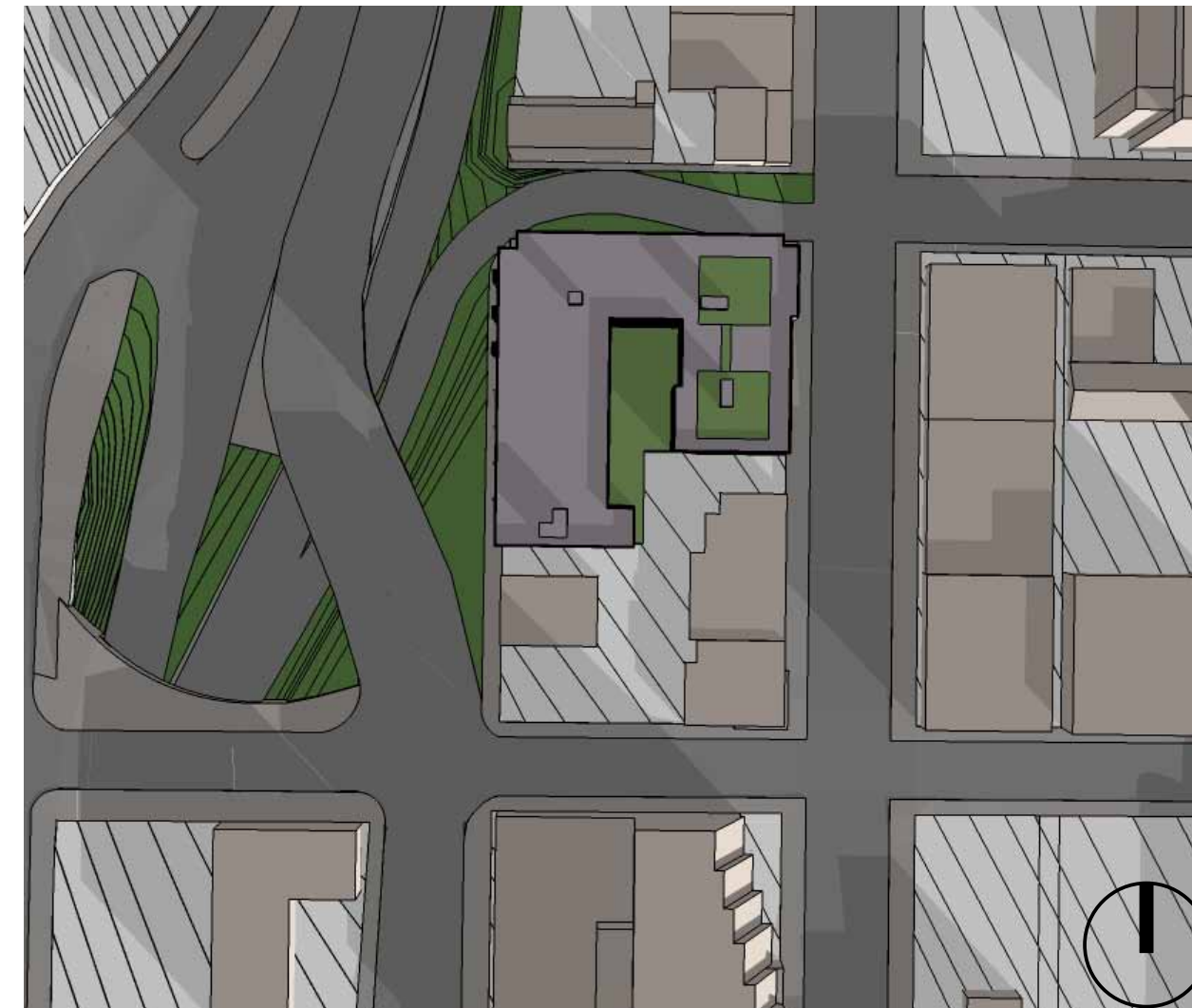
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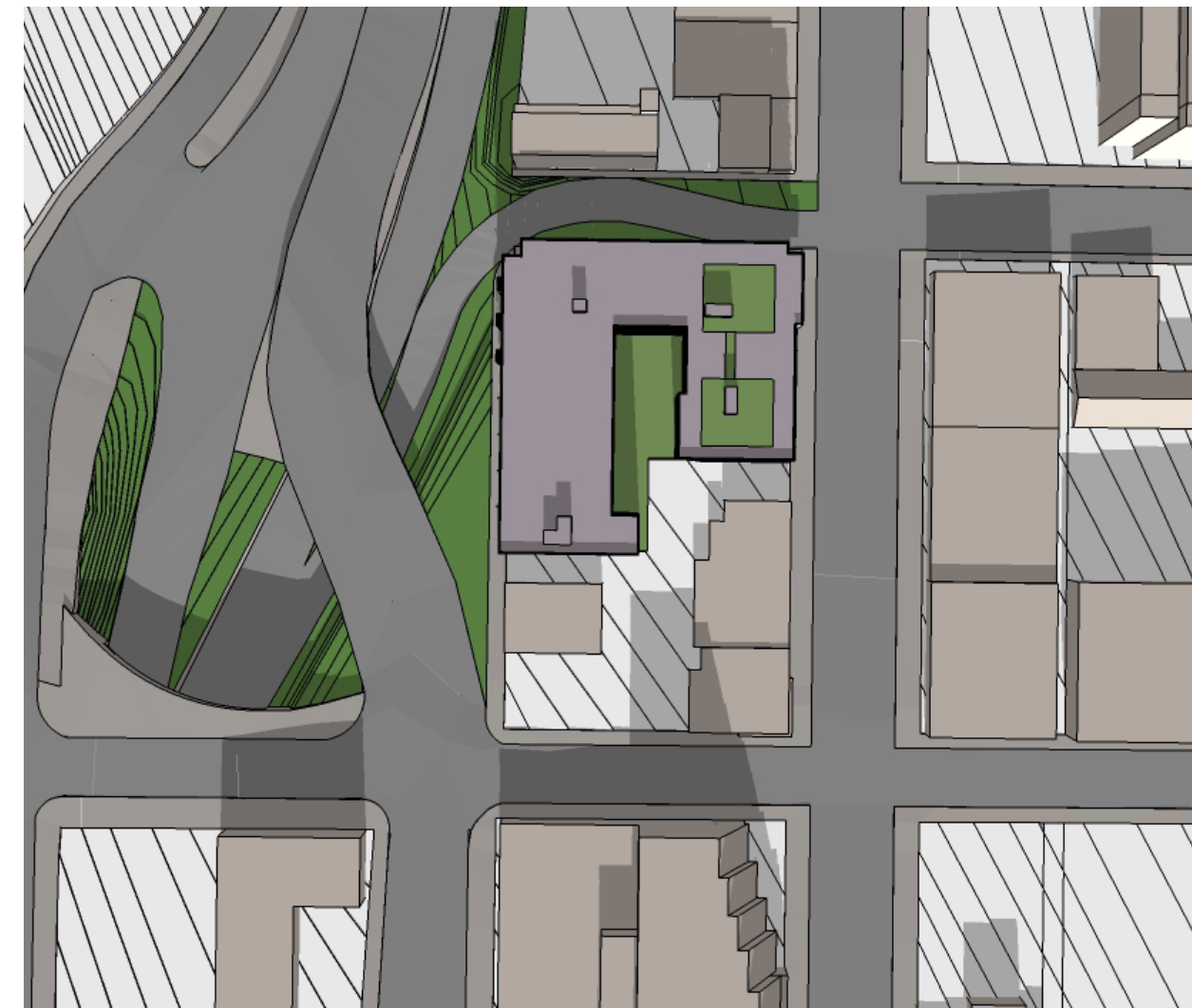
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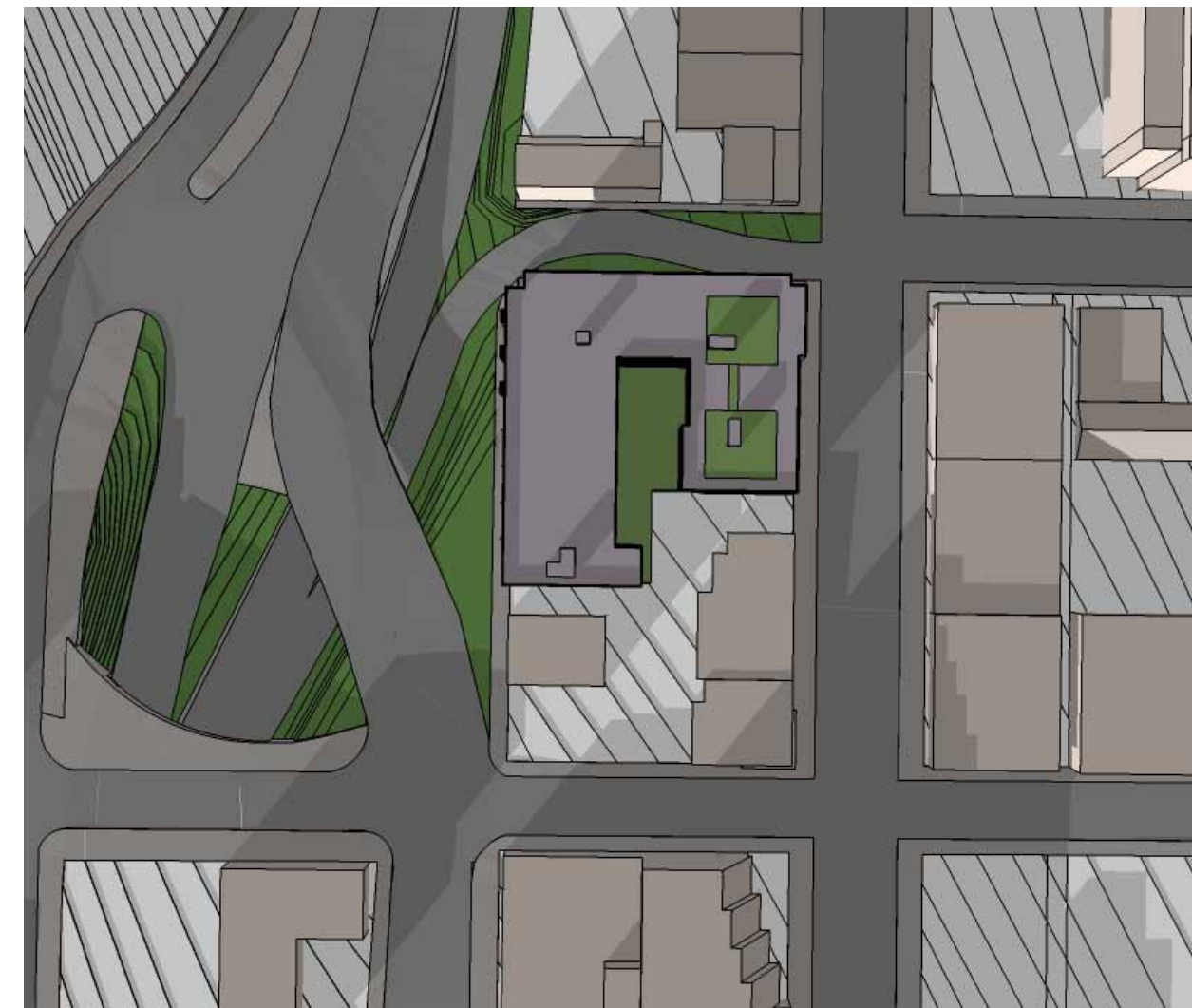
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9AM



12PM



3PM

SOLAR STUDIES: WINTER

ZONING CODE SUMMARY

435 Dexter

ADDRESS	435 Dexter Avenue North 98109
Parcel	Parcel # 198820-1285
	Land Sq. Ft.: 44,780
	Acres: 1.03
CODE:	SEATTLE LAND USE & ZONING CODE
ZONE:	Seattle Mixed 160/85-240
Overlay	South Lake Union Urban Center
Republican and Dexter designated as Class II Pedestrian Streets	
SECTION / TOPIC	REQUIRED / ALLOWED
23.48.004 Permitted Uses	Residential, retail, and parking permitted outright
23.48.009 FAR	<p>For nonresidential uses: Base FAR: 4.5, Max FAR: 7, Max FAR for structures that do not exceed base residential height limit and include any residential use: 6</p> <p>extra floor area for nonresidential uses above base FAR (up to 6 max)</p> <p>development shall not exceed the lower of nonres max FAR or max FAR for structures not exceeding base residential height limit (so FAR max is 6)</p> <p>portions of a story that extend no more than 4' above grade (excluding access) is exempt from FAR limits of residential is directly above</p> <p>allowance for mech. Equipment: 3.5% of GFA is exempt. Mech equipment on roof, enclosed or not, is not included.</p> <p>General sales and services, eating and drinking establishments at street level exempt</p>
23.48.010 Maximum Height	85' base residential height, 240' if providing incentive housing
23.48.014	<ul style="list-style-type: none"> Primary entrance required from street, no more than 3' above or below sidewalk grade Minimum height of street facing facades on Class II
Street-Level Development Standards	<p>Pedestrian Streets 25'.</p> <ul style="list-style-type: none"> 12' max setback from prop line (if landscaped) Additional setbacks permitted up to 30% of setback street wall, if setback located 20' min from street corner
23.48.014 D	<ul style="list-style-type: none"> Shall apply to area of façade from 2'-8" above sidewalk

Transparency and Blank Façade Requirements	<ul style="list-style-type: none"> · applies to all street facing, street level facades, except for structures in residential use, as follows: Class II street, min. 60% of street facing façade must be transparent, 30% other streets, 45% if Class II street if slope >7.5% · for class II streets, blank facades limited to 15' wide, except garage doors, increased to 30' if director determines façade is enhanced by arch. Detailing, landscaping, art, or other sim features · Blank segments to be separated by 2' transparent areas · for other streets, blank facades limited to 30' wide, except garage doors, increased to 60' if director determines façade is enhanced by arch. Detailing, landscaping, art, or other sim features · Blank segments to be separated by 2' transparent areas · does not apply to portions of structures in residential use
23.48.014 E Development standards for street level uses exempt from FAR	<p>75% min. of each street frontage occupied by sales/services, eating/drinking, entertainment.</p> <p>min. 13' flr to flr, extend at least 30' deep from street level façade</p> <p>max. 10' from street lot line unless outdoor amenity area abuts lot line, then the street level use may abut amenity.</p> <p>pedestrian access directly from street, amenity area, or required open space. Entrances located no more than 3' above or below sidewalk grade</p>
23.48.020 Residential amenity area	<ul style="list-style-type: none"> · 5% of residential gross floor area · On-site, available to all residents, at or above ground level, max 50% enclosed, min. horiz. dimension 15', no amenity area < 225 sf, exterior portion landscaped with solar access and seating, ADA pedestrian access can count toward required area
23.48.024 Screening and landscape standards	<p>Green Factor of .3 required</p> <p>Class II streets-parking not permitted at street level unless separated by other uses</p> <p>Other streets: Parking in structures-permitted at street level when at least 30% of the street frontage of the parking area (excluding garage doors) is separated from the street by other uses</p> <ul style="list-style-type: none"> · Facades of separating uses subject to transparency and blank wall standards. · Remaining parking shall be screened at street level and street façade shall be enhanced by architectural detailing, artwork, landscaping or similar visual interest features · Perimeter of each floor of parking garages above street level shall have opaque screen at least 3.5' high

<p>23.48.024D</p>	<p>1. Street trees shall be provided in all planting strips. Existing street trees may count toward meeting the street tree requirement.</p>
<p>Street trees requirements</p>	<p>2. Exceptions to street tree requirements</p> <p>a. Street trees are not required when a change of use is the only permit requested.</p> <p>b. Street trees are not required for temporary use permits.</p> <p>3. If it is not feasible to plant street trees according to City standards, either a landscaped setback a minimum of 5 feet deep is required along the street lot line, or landscaping other than trees may be located in the planting strip according to Depa</p>
<p>23.48.030 Light and glare</p>	<p>Exterior lighting must be shielded and directed away from adjacent uses, interior lighting in garages must be shielded to minimize nighttime glare affecting nearby uses.</p>
<p>23.48.031 Solid waste and recyclable materials storage</p>	<p>· Multifamily-575 sf for first 100 units plus 4 sf for each unit above 100</p> <p>· Commercial-82 sf up to 5,000 sf, 125 sf 5,001 -15,000 sf</p>
<p>23.48.032 Required parking and loading (per 23.54.015 and 23.54.035)</p>	<p>No minimum requirement for Urban Centers</p> <p>· Loading may be waived if use less than 16,000 sf</p>
<p>23.48.034 Parking and loading location, access and curbcuts</p>	<p>· Access to parking and loading from alley</p> <p>· If lot fronts on an alley and an east/west street, parking and loading may be on east/west street</p>
<p>23.48.034 B Parking location within structures</p>	<p>Parking at Street Level:</p> <p>b. Due to physical site conditions such as topographic or geologic conditions, parking is permitted in stories that are partially below street-level and partially above street level without being separated from the street by other uses, if:</p> <p>1) The street front portion of the parking that is at or above street-level does not abut a Class 1 Pedestrian Street requiring street-level uses; and</p> <p>2) The street front portion of the parking that is at or above street-level, excluding garage and loading doors and permitted access to parking, is screened from view at the street-level; and</p> <p>3) The street-facing facade is enhanced by architectural detailing, artwork, landscaping, stoops and porches providing access to residential uses, or similar visual interest features.</p>

Bike Parking	1 long term and 2 short term bike spaces required Residential: 1 long term/4 units
23.54.030.B1	RESIDENTIAL PARKING RATIO 60% MEDIUM REQUIRED, 40% ANY SIZE
23.54.030.B2	NON-RESIDENTIAL PARKING RATIO 35% LARGE REQUIRED, 35% SMALL REQUIRED 30% ANY SIZE

ZONING SUMMARY

435 Dexter

Early Design Guidance: January 08, 2014

The following Design Guidelines for Multi-family and Commercial building listed below have been identified by the design team as having the most relevance to this development site.

A-1 Responding to Site Characteristics

The siting of buildings should respond to specific site conditions and opportunities such as location on prominent inter-section, unusual topography, and views.

SLU-specific supplemental guidance

Encourage “outlooks and overlooks”: change the form or facade setbacks of the bulding to enhance opportunities for views.
Reinforce community gateways through landscaping, artwork, and historical references to create a sense of place.

A-2 Streetscape Compatibility

The siting of buildings should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.

SLU-specific supplemental guidance

Provide pedestrian-friendly amenities.

A-3 Entrances Visible from the Street

Entries should be clearly identifiable and visible from the street.

A-4 Human Activity

New development should be sited and designed to encourage human activity on the street.

SLU-specific supplemental guidance

Create graceful transitions at streetscape level between public and private uses.

A-6 Transition between Residence and Street

For residential projects, the space between the building and the sidewalk should provide security and privacy for resi-dents and encourage social interaction among residents and neighbors.

SLU-specific supplemental guidance

Design entries of residential buildings to enhance the character of the streetscape to create a transition between public and private areas.

A-10 Corner Lots

Buildings on corner lots should be oriented to the corner and public street fronts. Parking and automobile access should be located away from the corners.

B-1 Height, Bulk and Scale Compatibility

Projects should be compatible with the scale of development anticipated by the applicable Land Use Policies for the sur-rounding area and should be sited and designed to provide a sensitive transition to near-by, less intensive zones. Proj-ects on zone edges should be developed in a manner that creates a step in perceived height, bulk and scale between the anticipated development potential of the adjacent zones.

SLU-specific supplemental guidance

Address both pedestrian and auto experience with specific attention to regional transportation corridors.
Relate proportions of building to width and scale of street.
Articulate facades to relate to existing pattern of developement in vicinity.
Consider architectural features to reduce buliding scale.



A-2 Streetscape Compatibility



A-3 Entrances Visible from Street

RELEVANT DESIGN GUIDELINES

C-2 Architectural Concept and Consistency

Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept.

SLU-specific supplemental guidance

Design the roofscape in addition to the streetscape.

C-3 Human Scale

The design of new buildings should incorporate architectural features, elements, and details to achieve a good human scale.

C-5 Structured Parking Entrances

The presence and appearance of garage entrances should be minimized so that they do not dominate the street front-age of a building.

D-1 Pedestrian Open Space and Entrances

Convenient and attractive access to the building’s entry should be provided.

SLU-specific supplemental guidance

Provide features that enhance the public realm-the transition zone between private property and and public right-of-way.

D-2 Blank Walls

Buildings should avoid large blank walls facing the street, especially near sidewalks. Where blank walls are unavoidable, they should receive design treatment to increase pedestrian comfort and interest.

D-5 Visual Impacts of Parking Structures

The visibility of all at-grade parking structures or accessory parking garages should be minimized. The parking portion of a structure should be architecturally compatible with the rest of the structure and streetscape.

D-7 Personal Safety and Security

Enhance personal safety and security in the environment under review.

SLU-specific supplemental guidance

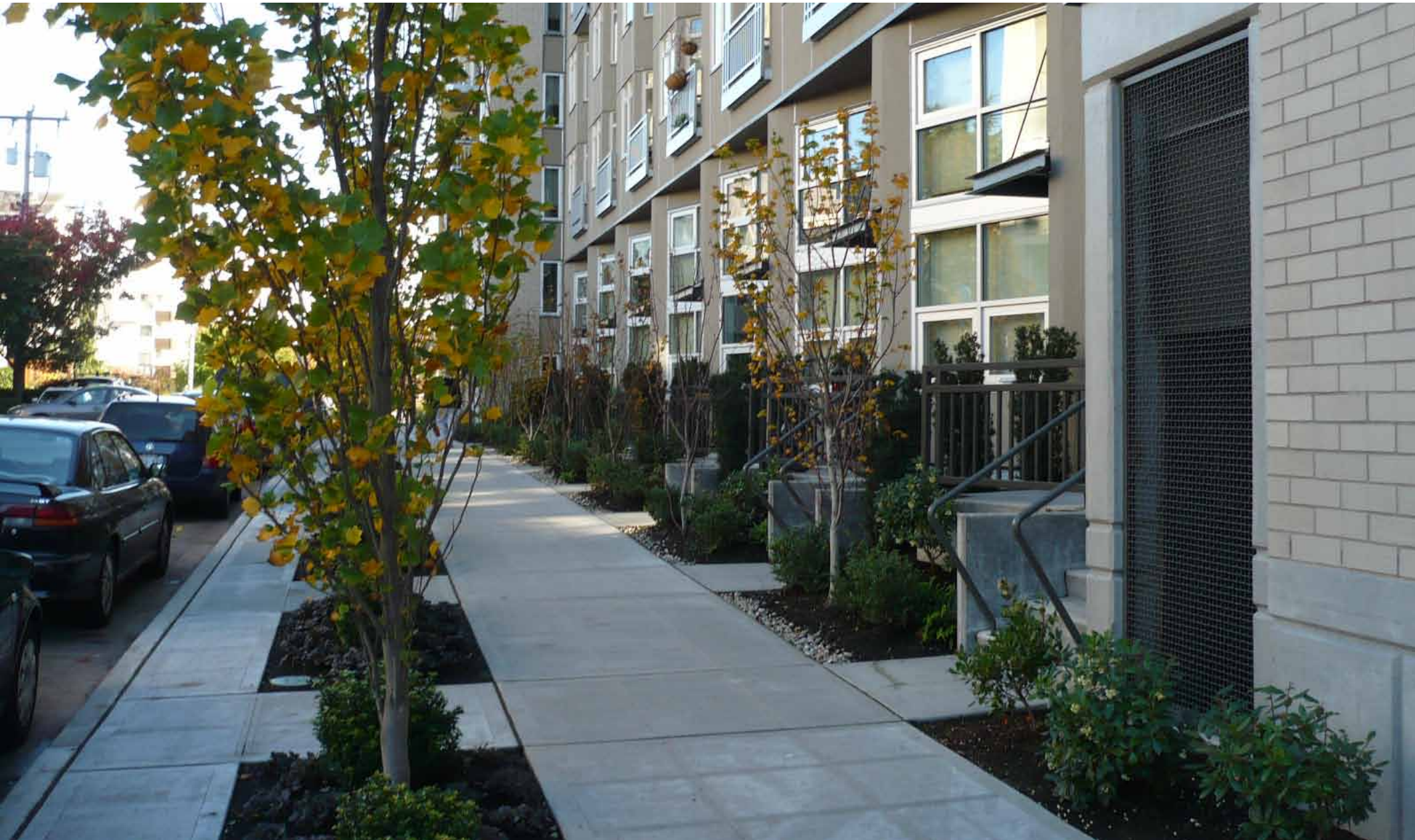
Enhance public safety throughout the neighborhood to foster 18-hour public activity.

E-2 Landscaping to Enhance the Building and/or Site

Landscaping should be incorporated into the design to enhance the project.

SLU-specific supplemental guidance

Consider integrating artwork and landscape that evokes the previous use of the area.



A-6 Transition between Residence and Street



C-2 Architectural Concept and Consistency



E-2 Landscaping to Enhance Building and Site

RELEVANT DESIGN GUIDELINES



AUTUMN FERN



PINK BARREN STRAWBERRY



BLUE STAR CREEPER



ORANGE SEDGE



SALAL



ROOF TOP



DWARF HEAVENLY BAMBOO



MONDO GRASS



MAGIC CARPET SPIRAEA



ROOF TOP



JAPANESE FOREST GRASS



SWEET BOX

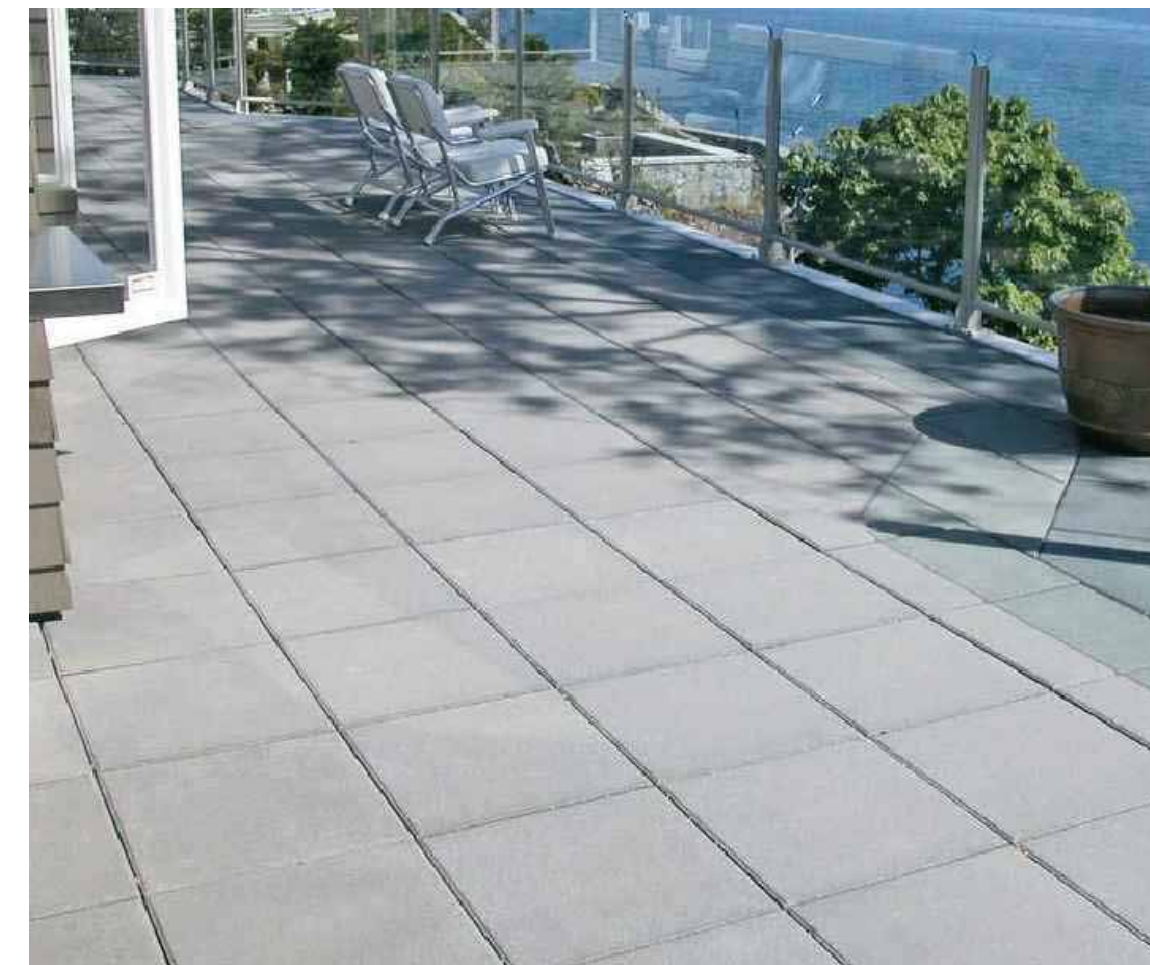
LANDSCAPE AND STREETScape CONCEPTS



ROOF TOP



LAVENDER



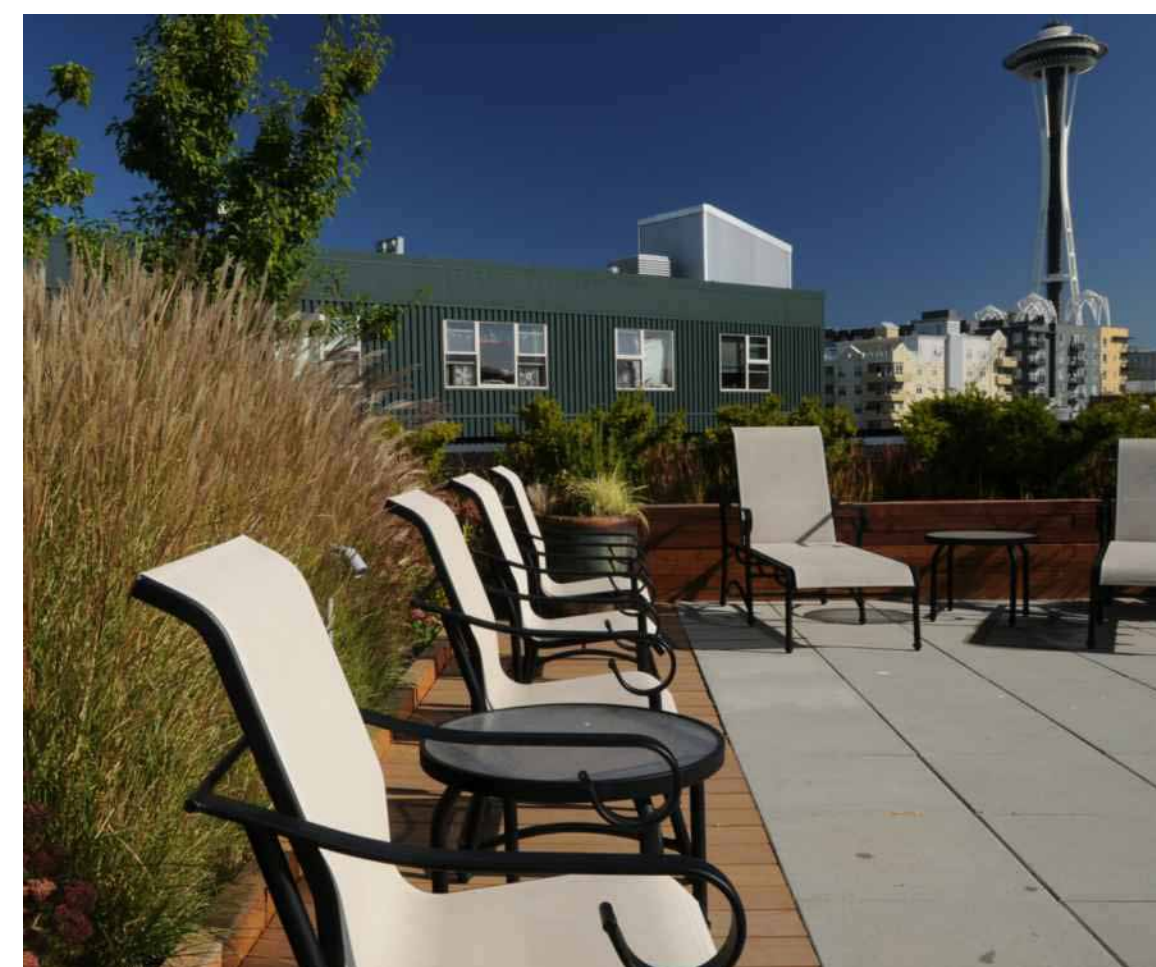
PAVERS



BIO RETENTION PLANTER



FIRE PIT



ROOF TOP



TUMBLER GLASS



FIRE PIT



ROOF TOP



BIO RETENTION PLANTER

LANDSCAPE AND STREETScape CONCEPTS