



# ISOLA GREENWOOD

6800 GREENWOOD AVENUE N

DESIGN REVIEW

DPD #3016207

JUNE 23, 2014

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**PROJECT VISION**

Set within a growing commercial node in Phinney Ridge, this project combines richly textured materials with a clean and timeless architecture, resulting in a project that respects the neighborhood traditions while helping it grow and evolve. Street life along Greenwood will be enhanced through thoughtfully crafted retail spaces that fill the gap left by the former abandoned service station. The residential portion of the building will be accessed from a more private entry on 68th to ease the transition from the activity on Greenwood to the single family properties beyond.

Outdoor amenity spaces are provided with private yards and shared greenspace at ground level, along with private and shared rooftop decks. The decks will take advantage of stunning views to Green Lake, the Cascades, Mt. Rainier, and the Olympic Mountains.

**PROJECT PROGRAM**

Number of Residential Units:	32 units
Area of Residential Uses:	28,874 sf
Area of Commercial Uses:	4,018 sf
Number of Parking Spaces:	28 spaces below grade
Floor Area Ratio:	3.25
Number of Stories:	4 Stories
Total Area:	48,656 sf
Total Area Above Grade:	39,986 sf

**OWNER**

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 Seattle, WA 98134

**ARCHITECT**

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# ZONING CODE SUMMARY

**PARCELS:** 9468200016      **ZONING:** NC2-40  
**LOT AREA:** 11,700 sf  
**OVERLAYS:** Greenwood-Phinney Ridge Residential Urban Village, Frequent Transit

## 23.47A.004 PERMITTED USES (NC3-40)

Permitted outright:

- Residential
- Eating & Drinking Establishment
- General Sales
- Ground Floor Commercial Uses

## 23.47A.005 STREET-LEVEL USES (NC2-40)

- Blank facade requirements apply (segments no more than 20' in width, total blank facade segments may not exceed 40% of width)
- Street-level, street-facing facades must be located within 10' of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided
- 60% of street-facing facade between 2' and 8' above the sidewalk shall be transparent
- Non-residential uses must average 30' deep and be no less than 15' deep.

Proposed Development:

Blank Facades:

- 28% Street-Level Blank Facade on Greenwood Ave. N.
- 35% Street-Level Blank Facade on N. 68th Street.

Non-Residential Street-Level:

- 73% Transparency on Greenwood Ave. N.
- 65% Transparency on N. 68th Street.
- 33'-1" Average Depth, 28'-4" Minimum Depth.
- Facades Located 0'-0" from Lot Line

## 23.47A.012 STRUCTURE HEIGHT (NC2-40)

Allowed Maximum Base Height: 40'-0"

Maximum bonus height with incentives: 44'-0"

\* 4' Maximum height increase is allowed with 13' floor to floor at street level non-residential use (SMC 23.47A.012.A.1.a)

- 4' additional allowed for parapets: 48'-0"
- 5' additional allowed for clerestories: 49'-0"
- 15' additional allowed for stair penthouses: 59'-0"
- 16' additional allowed for elevator penthouses: 60'-0"

Proposed Development:

Minimum Floor-To-Floor Height: 13'-0"

Maximum Structure Height: 44'-0"

Calculated Average Datum Height: 326'-1"

- To Top of Roof: 370'-1" (44'-0")
- To Top of Parapet: 371'-1" (45'-0")
- To top of Stair Penthouse: 379'-11.5" (53'-11.5")
- To Top of Elevator Penthouse: 385'-10.5" (59'-10.5")

## 23.47A.013 FLOOR AREA RATIO (NC2-40)

Single purpose: 3.0      Mixed-use: 3.25

Proposed Development:

Mixed Use: 3.25

## 23.86.006 STRUCTURE HEIGHT MEASUREMENT

The height of a structure is the difference between the elevation of the highest point of the structure not excepted from applicable height limits and the average grade level ("average grade level" means the average of the elevation of existing lot grades at the midpoints, measured horizontally, of each exterior wall of the structure or at the midpoint of each side of the smallest rectangle that can be drawn to enclose the structure)

## 23.47A.014 SETBACK REQUIREMENTS (NC2-40)

Front setback: No setback required  
 Rear setback: No setback required  
 Side setback from interior lot line: No setback required  
 Additional setbacks: No setback required

Proposed Development:

Front Yard Setback (Greenwood): 0'-0"

Rear Yard Setback: 10'-0" (9'-9" @ Brick Walls)

Side Yard Setback (North): 5'-0" (4'-9" @ Brick Walls)

Side Yard Setback (68th): 0'-0"

## 23.47A.024 AMENITY AREAS (NC2-40)

General Requirements:

- All residents shall have access to at least one private or common amenity area
- Amenity areas shall not be enclosed
- Common amenity areas shall have a minimum dimension of 10 ft and be no less than 250 sf in size
- Private balconies and decks shall have a minimum area of 60 sf and no horizontal dimension less than 6 ft

Required:

- 5% of gross floor area in residential use
- Estimated requirement: 29,359 x 5% = 1,468 sf

Proposed Development:

Amenity Area Provided: 8,285 sf

## 23.47A.016 LANDSCAPING STANDARDS (NC2-40)

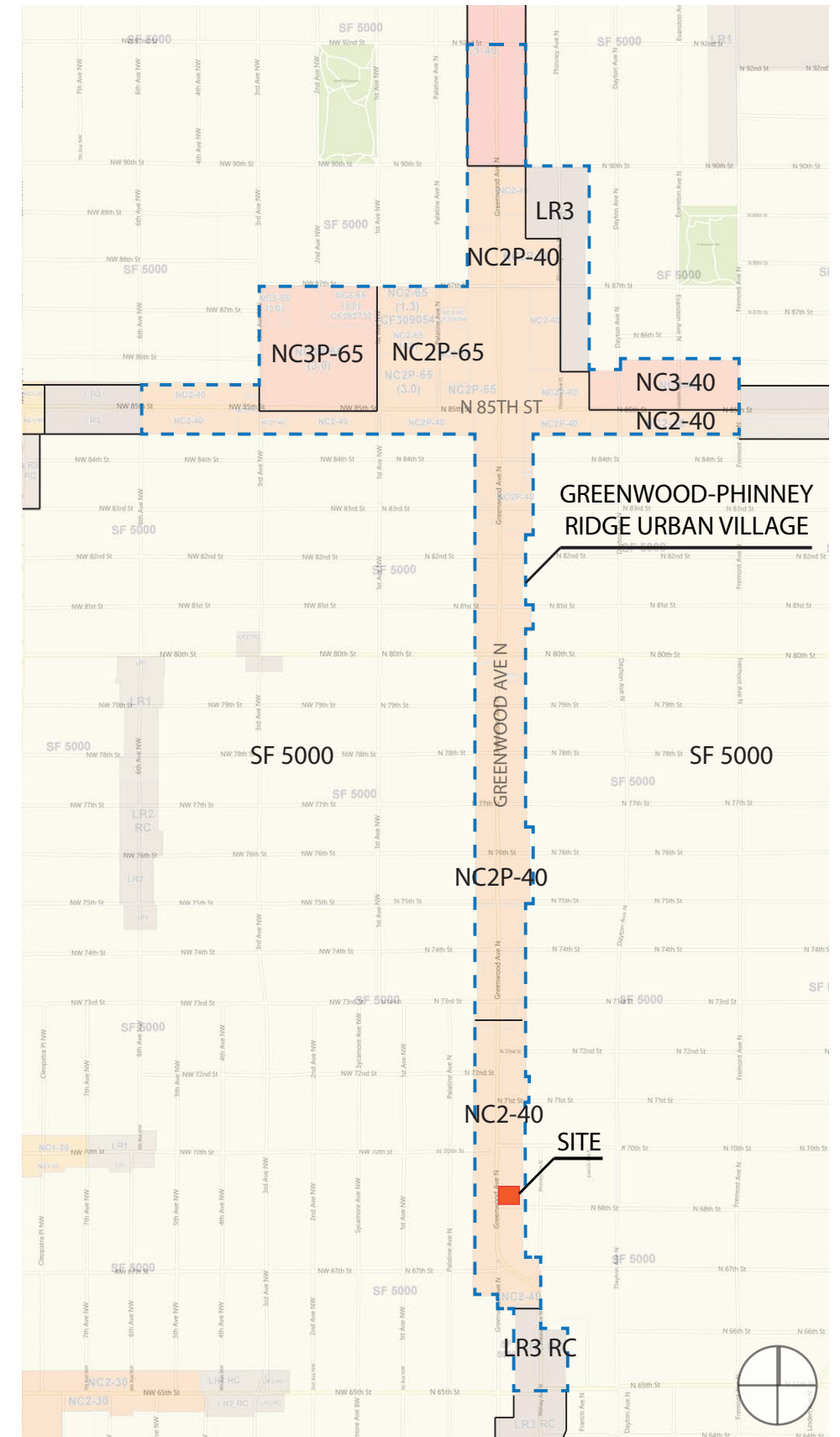
Green factor score minimum 0.3 required.

Street trees are required when any development is proposed

Proposed Development:

Green Factor Score: 0.441

Street Trees Provided: 5





**23.53.035 STRUCTURAL BUILDING OVERHANGS**

- Overhead horizontal projections of a purely architectural or decorative character shall be limited to a max. horizontal extension of 1', and a max. vertical dimension of 2'-6"
- Bay windows must be a minimum of 8'-0" clear above the sidewalk, and project no more than 3'-0" beyond the lot line, and portions of the bay parallel to the lot line may be no wider than 9'-0".
- 50% of the vertical surface area of the bay must be transparent.

**Proposed Development:**

**Clerestory Eaves (Architectural Projection):**

- Max. Horizontal Extension: 1'-0" Beyond Lot Line
- Max. Vertical Dimension: 1'-0"

**Structural Building Overhangs:**

- Clearance Above Sidewalk: 12'-0" Minimum
- Projection Beyond Lot Line: 3'-0"
- Max. SBO Width: 8'-0"
- Transparent Vertical Surface Area: 55%

**23.54.015 REQUIRED PARKING (NC2-40)**

Automobile Parking: No parking is required in commercial zones within urban villages if the residential use is located within 1,320 feet of a street with frequent transit service.

**Bicycle Parking:**

- Short Term Non-Residential: 1 per 4,000 sf or 2 spaces
- Long Term Non-Residential: 1 per 12,000 sf or 1 space
- Long Term Residential: 1 per 4 units or 9 spaces

**Proposed Development:**

Automobile Parking: 28 spaces in underground garage

**Bicycle Parking:**

- Non-Residential Bike Parking: 3 Spaces @ Sidewalk Bike Rack
- Since no covered parking is provided, no covered long-term bike parking is required.

Residential Bike Parking: 9 Spaces

**23.54.040 SOLID WASTE & RECYCLABLE MATERIALS STORAGE AND ACCESS (NC2-40)**

Residential (26-50 units): 375 sf

Non-Residential (0-5,000 sf): 82 sf

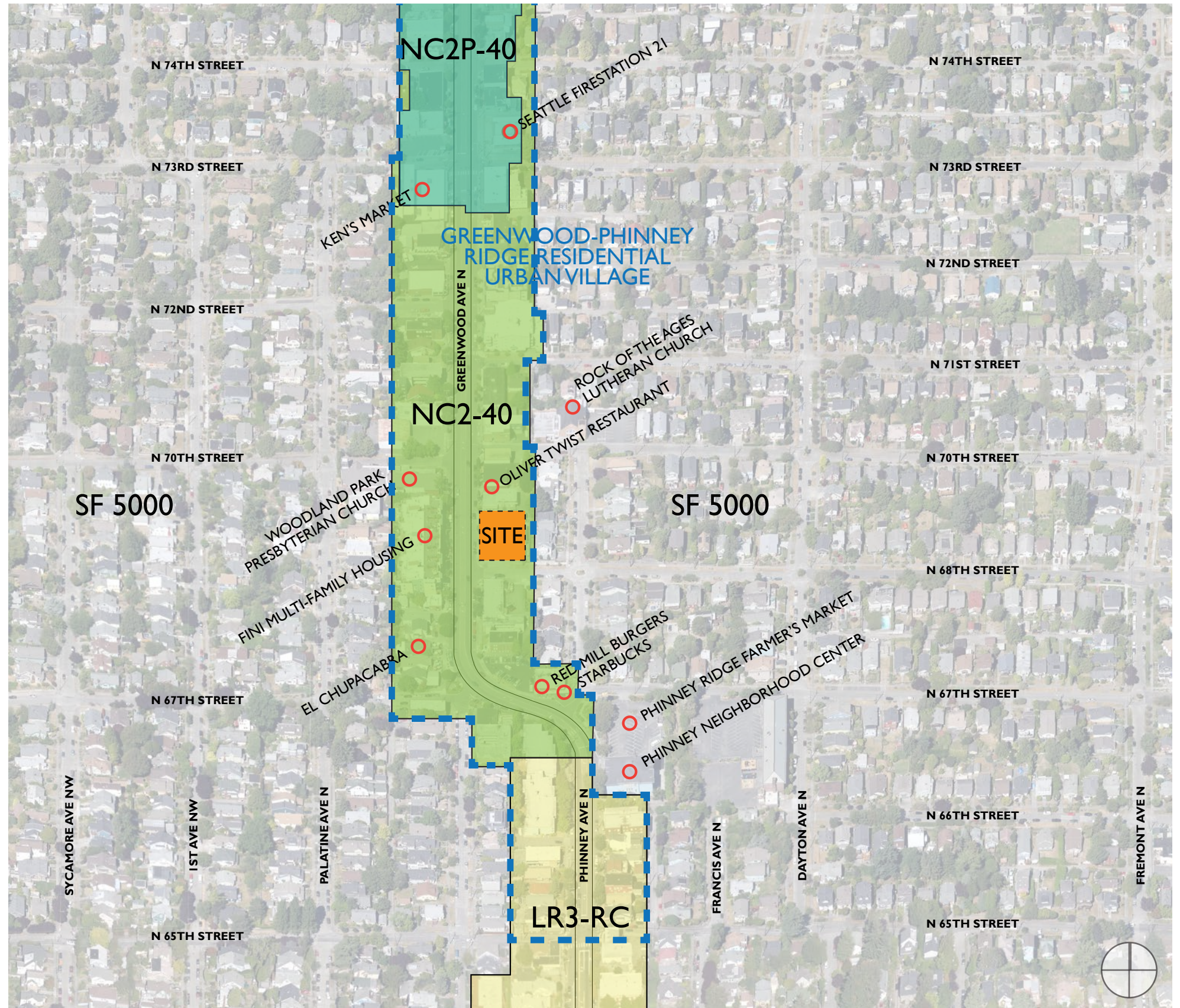
- Mixed use development that contains both residential and nonresidential uses shall meet the storage requirements for residential development, plus 50% of the requirement for nonresidential development

Required: 375 sf + (82 sf x 50%) = 416 sf

**Proposed Development:**

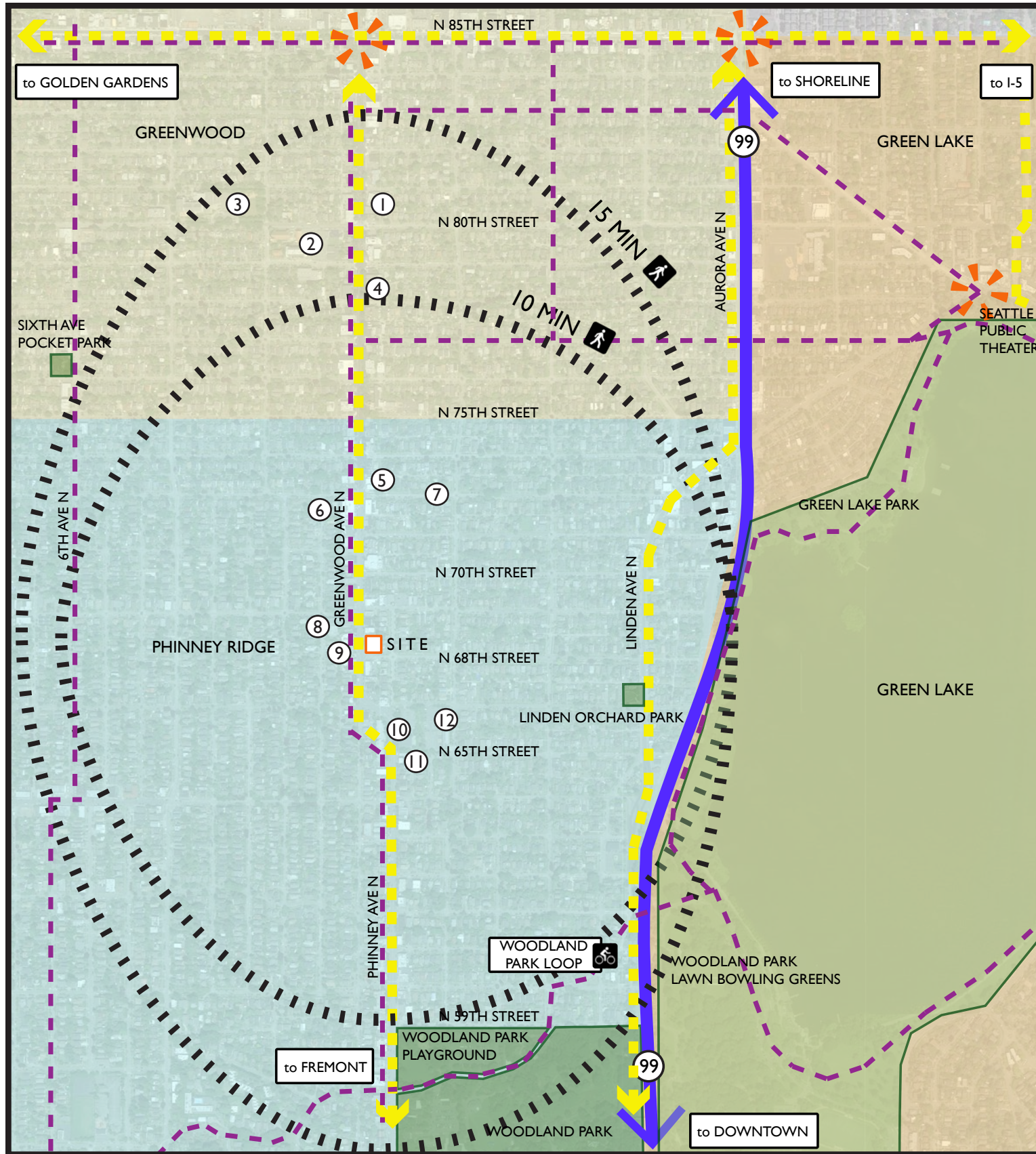
Mixed-Use Storage: 300 sf

Approval for reduced storage space provided by Liz Kain/SPU





# PROJECT DESCRIPTION



## DEVELOPMENT OBJECTIVES

The proposed project is a four story condominium building with 32 residential units combined with up to 4 street-level commercial spaces. Though not required, parking is proposed for 28 spaces located in a below-grade parking garage accessed from N 68th Street. Outdoor residential amenity space is provided through private yards and shared greenspace at ground level, along with private and shared rooftop decks.

The commercial storefronts will connect the existing retail and office buildings on adjacent parcels and fill in the gap created by the former service station. Modulation and detail of these entries will provide for an active pedestrian space. The upper levels of the project will maximize views to Green Lake and the Cascades to the east, and the Olympics to the west.

## EXISTING SITE

The site is a single parcel located at the northeast corner of the intersection at Greenwood Avenue North and North 68th Street. The site abuts an office building to the north and single-family residences to the east. The lot is currently vacant.

The parcel is rectangular, measuring 90 feet wide by approximately 131 feet long and contains approximately 11,801 sf. The site slopes approximately 4 feet from NW to SE.

## ZONING AND OVERLAY DESIGNATION

The project site is within the Greenwood-Phinney Ridge Residential Urban Village and is zoned NC2-40 in its entirety. Parcels to the north, west and south are also zoned NC2-40. The property to the east is partially zoned NC2-40 along the shared property line and SF for the remainder.

## NEIGHBORHOOD DEVELOPMENT

The site is located in a small commercial node at the borders of the Greenwood and Phinney Ridge neighborhoods. The area blends the small retail, office spaces, and multi-family apartments along Greenwood and Phinney Avenues with single family homes on adjacent blocks. The existing retail is primarily restaurants and coffee shops, with a small mix of stores selling goods and services. There are several churches nearby, the Phinney Neighborhood Center a block to the south, and an elementary school is a short walk to the north. There is a new multi-family project planned one block north of the site.





① GREENWOOD LIBRARY



② ST JOHN CATHOLIC CHURCH



③ GREENWOOD ELEMENTARY SCHOOL



④ WOODLAND PARK UNITED METHODIST



⑤ SEATTLE FIRE STATION 21



⑥ KEN'S MARKET



⑦ ROCK OF THE AGES LUTHERAN CHURCH



⑧ WOODLAND PARK PRESBYTERIAN CHURCH



⑨ FINI APARTMENTS



⑩ RED MILL BURGERS



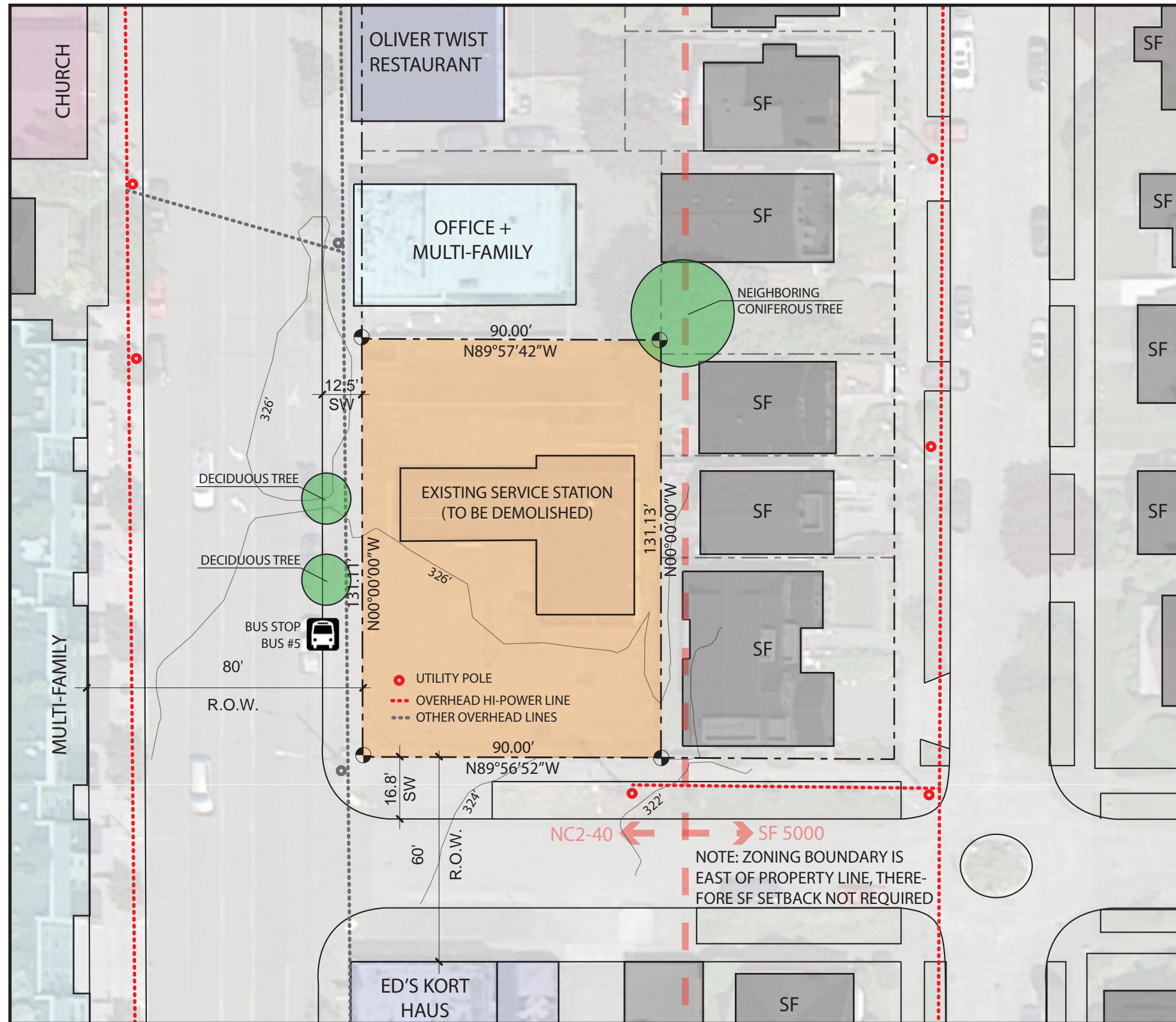
⑪ PHINNEY NEIGHBORHOOD CENTER



⑫ MONTESSORI GARDEN



# EXISTING SITE PLAN



## CURRENT SITE CONDITIONS

The site consists of a single tax parcel located at the northeast corner of the intersection at Greenwood Avenue North and North 68th Street. The site abuts an office building to the north and single-family residences to the east. The former service garage and canopy on the site were recently demolished. The site is almost completely paved with no significant landscaping, and there are two street trees along Greenwood.

The parcel is rectangular, measuring 90 feet wide by 131.11 feet along the west property line and 131.13 feet along the east property line. The site contains approximately 11,700 sf, and slopes almost 4 feet from NW to SE with the slope increasing slightly at the SE corner.

Greenwood and 68th feature right-of-way widths of 80' and 60' respectively. However, the roadway along Greenwood is wider than typical (approximately 55') to accommodate a traffic lane, bike lane and parallel parking in each direction along with a center turn lane. There is no alley located on this block.

There is a SCL pole and overhead line located near the southeast corner of the site. Due to building massing and location of entry to parking, this pole will be relocated as part of the project.



## A-1 RESPONDING TO SITE CHARACTERISTICS

The siting of buildings should respond to specific site conditions and opportunities such as non-rectangular lots, location on prominent intersections, unusual topography, significant vegetation and views or other natural features.

Greenwood/Phinney specific supplemental guidance:

- Numerous east-west streets offer excellent views of Green Lake, Puget Sound and the Olympic and Cascade Mountains from Greenwood Avenue North. Where possible, buildings should be located to take advantage of these views and to enhance views from the public right-of-way. Examples of methods to do this include setbacks from view corridors, landscape elements and street trees to frame views rather than block them, and pedestrian spaces with views of the water and mountains.

Combining this project's corner-anchoring retail space with existing retail and office spaces on adjacent properties will enhance the area as a neighborhood node and encourage pedestrian life along this section of Greenwood. Although not required to provide parking, the proposed design will access an underground garage, easing potential parking pressure on the neighborhood. The garage ramp is located at the lowest point of the property to take advantage of existing grades. The site's location atop Phinney Ridge allows the project to capture light and superb views, especially to the south and east. Roof decks will be oriented to capture views of Green Lake, the Cascades, Mt. Rainier, and the Olympics.

## A-2 STREETSCAPE COMPATIBILITY

The siting of buildings should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.

Greenwood/Phinney specific supplemental guidance:

- Reinforcement of Commercial and Residential Development Patterns Commercial development in the Greenwood/Phinney corridor has historically been oriented toward the street, with buildings up against the sidewalks. Most residential developments have modest landscaped setbacks and first floors are built slightly above grade to allow for privacy and a sense of transition from the street. Continuing this pattern will reinforce the character of both the business districts and residential areas.
- Treatment of Side Streets: Some treatment of side-streets off of Greenwood Avenue North and 85th Street is important to create an effective transition to residential neighborhoods.

The project will be designed to reinforce emerging development patterns in the neighborhood. Higher intensity and more public spaces will be located along the Greenwood frontage, including a retail space that wraps the corner onto 68th. This continues the pattern of pedestrian friendly storefronts along the commercial corridor established on adjacent properties while creating an anchoring opportunity at the corner location. Entrance to the residential areas will be on 68th, through a dedicated lobby space that is recessed into the building massing and affording shelter and additional opportunities for lighting and landscaping. Upper level decks step the massing of the building along 68th, easing the transition to the residential neighborhood to the east. Entrance to parking will be from 68th to minimize disruption to pedestrians along Greenwood and take advantage of the lower grade.

## A-5 RESPECT FOR ADJACENT SITES

Buildings should respect adjacent properties by being located on their sites to minimize disruption of the privacy and outdoor activities of residents in adjacent buildings.

The project massing favors the west property line to maintain the street edge established along Greenwood. Although the NC zoning extends into the neighboring single family properties and would allow for a 0' setback, the building will be held back 10' from the east property line to provide a buffer for light and air. There is a 5' setback from the north property line to allow for additional light into this project as well as reduce shadowing on the neighbor. Parking will be located underground to maximize above grade uses and minimize the impact of light and noise on adjacent properties.

## A-8 PARKING AND VEHICLE ACCESS

Siting should minimize the impact of automobile parking and driveways on the pedestrian environment, adjacent properties, and pedestrian safety.

The project includes 28 parking stalls located in an underground parking garage to minimize the impact of light and noise on surrounding properties and allow for higher-use development at grade. The driveway access to the garage is located at the southeast corner of the property off of N 68th Street to minimize the disruption to pedestrians on Greenwood and take advantage of the site topography. Sight triangles have been preserved in the building design, with a small departure for a column in the eastern triangle.

## B-1 HEIGHT, BULK, AND SCALE COMPATIBILITY

Projects should be compatible with the scale of development anticipated by the applicable Land Use Policies for the surrounding area and should be sited and designed to provide a sensitive transition to near-by, less intensive zones. Projects on zone edges should be developed in a manner that creates a step in perceived height, bulk, and scale between anticipated development potential of the adjacent zones.

Greenwood/Phinney specific supplemental guidance:

- Impact of New Buildings on the Street: Also, new commercial development should respect the small-scale historical pattern of storefronts on Greenwood Avenue North. Typically, the older storefronts are about 50 feet in width and feature brick, stone or other masonry units. Some also feature architectural details that provide interest and a human scale to the buildings.
- Zone Edges: Careful siting, building design and massing are important to achieve a sensitive transition between more intensive and less intensive zones.

The site is part of a NC zone that extends a half-block either side of Greenwood Avenue North. The street-front massing is consistent with recent and proposed buildings in the neighborhood, and will allow the building to blend with future projects as they are developed. The residential bays above street level, combined with broad canopies over storefronts and entries, create sheltered exterior spaces for an improved pedestrian experience.

The site borders single-family properties to the east, and this transition is softened by holding the building back from the property line to provide a buffer space between the

new building and neighboring houses. Additionally, a fourth floor terrace is carved out of the southeast corner and flows into private roof decks all along the east side, further reducing scale and impact to the single-family homes.

## C-1 ARCHITECTURAL CONTEXT

New buildings proposed for existing neighborhoods with a well-defined and desirable character should be compatible with or complement the architectural character and siting pattern of neighboring buildings.

Greenwood/Phinney specific supplemental guidance:

- Signage: The design and placement of signs plays an important role in the visual character and identity of the community. Key aspects of this effort are to ensure that the signs are at an appropriate scale and fit in with the building's architecture and the local district. Small signs are encouraged in the building's architecture, along a sign band, on awnings or marquees, located in windows or hung perpendicular to the building façade.
- Façade Articulation and Modulation: Façade modulation and articulation are less critical in commercial or mixed-use structures as long as appropriate levels of detail are present to break up the façade. Many of these structures are simple boxes that are well-fenestrated and contain a number of details that add interest at the ground level and lend buildings a human scale. Modulation of commercial and mixed-use structures at the street level is discouraged unless the space or spaces created by the modulation are large enough to be usable by pedestrians.

Our project presents simple brick planes punctuated by a series of rectangular bay windows over canopies that provide shelter to the entries below. The corner element is a highly transparent break in the brick walls with an elevated canopy for emphasis of the prime retail space at street level, and is topped by a clerestory that pushes this element slightly above the building mass. These simple movements provide for ample visual interest, but do so in a way that respects the traditionally restrained commercial buildings in the neighborhood. The building holds the lot line on both streets to continue the pattern set by adjacent structures with minimal modulation of the storefronts. Retail signage will be blade signs attached to the canopies, and building signage will be a perpendicular metal sign for the building name facing Greenwood as well as raised lettering mounted on the brick at the residential entry.

## C-2 ARCHITECTURAL CONCEPT AND CONSISTENCY

Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept. Buildings should exhibit form and features identifying the functions within the building. In general, the roofline or top of the structure should be clearly distinguished from its facade walls.

Greenwood/Phinney specific supplemental guidance:

- Architectural Styles: The Greenwood Avenue North/Phinney Avenue North and North/Northwest 85th Street corridors are characterized by their utilitarian, non-flamboyant, traditional architectural styles (except for churches).

With its traditional brick walls, strong but not overwhelming corner element, details that transition the building's scale at ground level, and restrained elevations facing adjacent



## RESPONSE TO DESIGN GUIDELINES

structures, this project presents a coherent approach that respects the neighborhood while providing a substantial new destination for retail and living. The articulation of the two street-facing façades reflects their more intense and public role, while the eastern elevation features cleaner lines and more residentially scaled windows and doors to reflect the lower-intensity of the adjacent SF zoned parcels. The northern elevation will be clean with minimal openings reflecting the potential for development of a zero-setback building on the adjacent parcel in the future.

### C-4 EXTERIOR FINISH MATERIALS

*Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.*

*Greenwood/Phinney specific supplemental guidance:*

- *Building Materials in the Greenwood Avenue North/Phinney Avenue North and North/Northwest 85th Street Corridors: Again, buildings within these corridors are characterized by their utilitarian, nonflamboyant, traditional architectural styles. Brick is the most common surface treatment in the commercial areas and should be encouraged. Plastic awnings should be strongly discouraged. As an alternative, architectural canopies are encouraged to provide weather protection and a place for business signage.*

The building is designed with high quality materials that include substantial use of brick, aluminum-composite panels, commercial grade fiber cement panels, cedar finishes, storefront glazing, glass and wood guardrails, metal and glass canopies, and attractive exterior light fixtures.

### C-5 STRUCTURED PARKING ENTRANCES

*The presence and appearance of garage entrances should be minimized so that they do not dominate the street frontage of a building.*

The garage entrance is located away from the retail and residential entrances and the coiling garage door is recessed 12' away from the face of the building and sidewalk to allow a transition space and improved visibility for vehicles exiting the building. The opening in the east wall along the driveway has been made large enough to accommodate the necessary sight triangle, but otherwise minimized to preserve the separation from adjacent open space and the single-family residence beyond. The use of brick continues into the transition space to better blend it with the rest of the elevation.

### D-1 PEDESTRIAN OPEN SPACES AND ENTRANCE

*Convenient and attractive access to the building's entry should be provided. To ensure comfort and security, paths and entry areas should be sufficiently lighted and entry areas should be protected from the weather. Opportunities for creating lively, pedestrian-oriented open space should be considered.*

*Greenwood/Phinney specific supplemental guidance:*

- *Pedestrian Open Spaces: Small, usable open spaces are an important design objective. Open space incorporating the following features are encouraged with*

*new commercial and mixed-use development: good sun exposure during most of the year, located in areas with significant pedestrian traffic, storefront and/or residential windows face onto open space, at or above the ground level, a variety of places to sit, pedestrians have something to look at, whether it is a view of the street, landscaping, a mural, etc.*

- *North/Northwest 85th Street Corridor and Greenwood Avenue North Corridor, North of North 87th Street: New development should enhance the pedestrian environment and encourage pedestrian activity along the North/Northwest 85th Street corridor and the Greenwood Avenue North corridor, north of North 87th Street.*

The retail spaces are accessed through recessed entries at the sidewalk along Greenwood. When combined with the ample canopies directly overhead, they create well sheltered spaces to view goods and services through large street-facing windows as well as safe, well-lit entries. Entry to the residential spaces will be through a dedicated and secure lobby that is recessed into the building massing, affording shelter and additional opportunities for lighting and landscaping. The lobby will have large windows to provide a visual connection with activity on the street. There will be attractive lighting fixtures placed appropriately around the building for occupant and pedestrian safety.

### D-6 SCREENING OF DUMPSTERS, UTILITIES, & SERVICE AREAS

*Building sites should locate service elements like trash dumpsters, loading docks and mechanical equipment away from the street front where possible. When elements such as dumpsters, utility meters, mechanical units and service areas cannot be located away from the street front, they should be situated and screened from view and should not be located in the pedestrian right-of-way.*

The solid waste and recycling storage is located next to the driveway which will be utilized to roll containers out to the street for service. The space is accessed through a pair of standard doors instead of a roll-up door to minimize visual impact as well as noise. The doors are angled away from the sidewalk to minimize their presence as much as possible while maintaining the sight triangles for the driveway.

### D-7 PERSONAL SAFETY AND SECURITY

*Project design should consider opportunities for enhancing personal safety and security in the environment under review.*

Recessed lighting will be used at each retail entry and multiple recessed fixtures located over the residential entry space. There will be a combination of sconce fixtures that project light up and down under the canopies to bounce additional light onto the sidewalks, as well as downlight-only sconces for security lighting at spaces where glare should be minimized above. Entrances to the residential lobby and parking garage will be secured to minimize loitering and unwanted access. The high degree of transparency on the street-facing elevations will provide for additional "eyes on the street".

## E-1 LANDSCAPING TO REINFORCE DESIGN CONTINUITY WITH ADJACENT SITES

*Where possible, and where there is not another overriding concern, landscaping should reinforce the character of neighboring properties and abutting streetscape.*

Street trees located in large planting strips along Greenwood and 68th are incorporated to reinforce the continuity along the sidewalk of the adjacent sites. The private yards on the east side of the project will utilize privacy fencing to minimize the impact on the adjacent single family home back yards. The southern half of this yard is a shared greenspace that is lushly planted and provides a buffer space next to the single family home located closer to the lot line than its neighbors to the north.



**HEIGHT BULK AND SCALE**

The Board expressed that the proximity to the zone change means this project is essentially at the zone edge and should follow the Greenwood-Phinney guidelines for Height, Bulk and Scale.

- Increase the building setback from the east property line.
- The massing should be oriented towards Greenwood Ave N.
- Consider extending the upper level amenity deck further north.
- Setback the south elevation at grade.

The preferred scheme presented at EDG pushed the driveway and associated retaining wall for the parking garage tight to the east lot line along half the length of the property. The driveway has now been relocated internal to the building and enclosed within its walls, providing a 10' setback along the length of the building and minimizing light, noise and odor spillover into adjacent properties. The 4th floor decks along Greenwood have been removed to allow for larger decks on the east side. Please refer to the diagrams on page 29 for additional information.

The existing sidewalk along N. 68th Street is approximately 17' wide, over 4' wider than the sidewalk along Greenwood. Given this, instead of pulling the entire facade away from the lot line, we worked to use recesses at the residential and driveway entrances, carved away a corner of the trash room to preserve full sight triangles when exiting by car, and preserved the deep decks at the top floor above to modulate the interaction with the sidewalk. There is still ample space for street trees, planting beds, bike racks, and sheltered space under the canopies, while allowing the building to better hold the street corner and maintain buildable area.

**STREETSCAPE COMPATIBILITY**

The Board agreed that the design proportions, rhythm, materials and character should be informed by both the older simple brick apartment structures and single story storefronts in the neighborhood. Follow the Greenwood-Phinney Design Guidelines.

- Maintain a filled out corner at Greenwood Ave N. and N 68th St., at the upper stories.
- The retail space along Greenwood Ave N should be broken into smaller storefronts.
- Setback the lower level of the south elevation along N 68th St. to provide view opportunities for pedestrians.
- Modulation should be avoided unless it provides usable space and is "traditional" in form and materials.

This project maintains a strong corner and combines it with simply detailed brick walls, four distinct retail spaces along Greenwood, and minimal modulation that blends with the character of the neighborhood.

**STREET LEVEL SAFETY**

Set back the south elevation at grade level to provide for views and safety. The location of a bus stop on Greenwood Ave N. and code requirements stipulate that parking access be located off of N. 68th St. Due to the close proximity to a residential neighborhood, pedestrian safety needs to be considered.

- Set back the south elevation along N. 68th St. to provide a wider sidewalk.
- Provide generous visual clearance at the parking entry/exit along N. 68th St.
- The width of the parking driveway and curb cut should be the minimum allowable size.

Portions of the building at grade have been deeply recessed along 68th to allow additional space next to the existing 17' sidewalk. Full sight triangles have been preserved, with one minor departure requested for a column in the eastern triangle (see page 31). The driveway has been reduced from 16' down to 12' wide to minimize pedestrian crossing distance while allowing for a functional turning radius in the garage below without reducing parking count.

**BUILDING MATERIALS**

The Board strongly encouraged the use of "timeless materials" such as brick as the exterior building material.

- Use brick as the main exterior building material, which is identified as one of the preferred materials in the Greenwood-Phinney Design Guidelines.
- If modulation is provided, consider wood as the material in contrast to the brick.
- The spacing and size of the fenestration should provide the main visual interest and detailing of the street facing facades.

The primary building material for the street-facing facades is traditional brick. Metal panels are proposed for the bay windows to better blend with the windows and present an impression of a traditional commercial building that has been converted to loft spaces. As demonstrated in the enclosed renderings, the window elements provide the primary patterning on these elevations.

**ADDITIONAL INFORMATION**

At the Recommendation meeting, the applicant should provide the following additional information:

- a. A materials board.
- b. A landscape plan.
- c. Detailed elevation of the storefronts along Greenwood Ave N.
- d. A site plan detail showing how the site triangles for the parking access will work.

These additional materials have been included.



AERIAL OF SOUTHEAST CORNER



# DESIGN PROCESS AND CONCEPT



EDG PREFERRED OPTION

## DESIGN PROCESS

The current design has remained true to the essence of the EDG preferred scheme, with massing changes that integrate guidance from the Board, feedback from neighbors, and thoughtful consideration of the site and its surroundings. The small roof decks along Greenwood were removed allowing for a continuous plane of brick rising from the ground to the roofline, punctuated by bay windows and a line of canopies that provide shelter as well as a human scale to the retail entries. A simple rhythm was established by the bay windows which were refined to just the right scale, then subtly punctuated at the corner by interrupting the brick walls, revealing a substantially glazed element rising just above the roof line. To keep the retail in character with the neighborhood, earlier continuous ribbons of glass were broken up into smaller scale storefronts wrapped in brick, helping integrate them with the wall above.

Importantly, the driveway into the garage was pulled away from the property line beside the single family homes and enclosed completely within the building, allowing for more greenspace at grade and an additional buffer for the neighbors. Access to the roof was added to take advantage of the spectacular views and provide a prime gathering space for the tenants. A palette of timeless materials combines with these design moves to present a building that adds to the design of the neighborhood.



EARLY DESIGN DEVELOPMENT



FURTHER DESIGN DEVELOPMENT



PROPOSED DESIGN







# WEST ELEVATION



## MATERIAL PALETTE

	<p>① METAL PANEL COLOR: BLACK</p>		<p>② BRICK COLOR: INCA</p>		<p>③ EXPOSED ARCHITECTURAL CONCRETE</p>		<p>④ ALUMINUM STOREFRONT GLAZING SYSTEM COLOR: BLACK</p>		<p>⑤ FIBERGLASS WINDOWS: COLOR: BLACK</p>
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# SOUTH ELEVATION



	<p>⑥ FIBER CEMENT PANELS COLOR: BENJAMIN MOORE - WHITALL BROWN</p>		<p>⑦ FIBER CEMENT PLANKS COLOR: BENJAMIN MOORE - WHITALL BROWN</p>		<p>⑧ GLASS RAILING ROOF</p>		<p>⑨ GLASS RAILING FRAMELESS - PATIO</p>		<p>⑩ WOOD RAILING: COLOR: BUTTERNUT STAIN</p>
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WEST ELEVATION

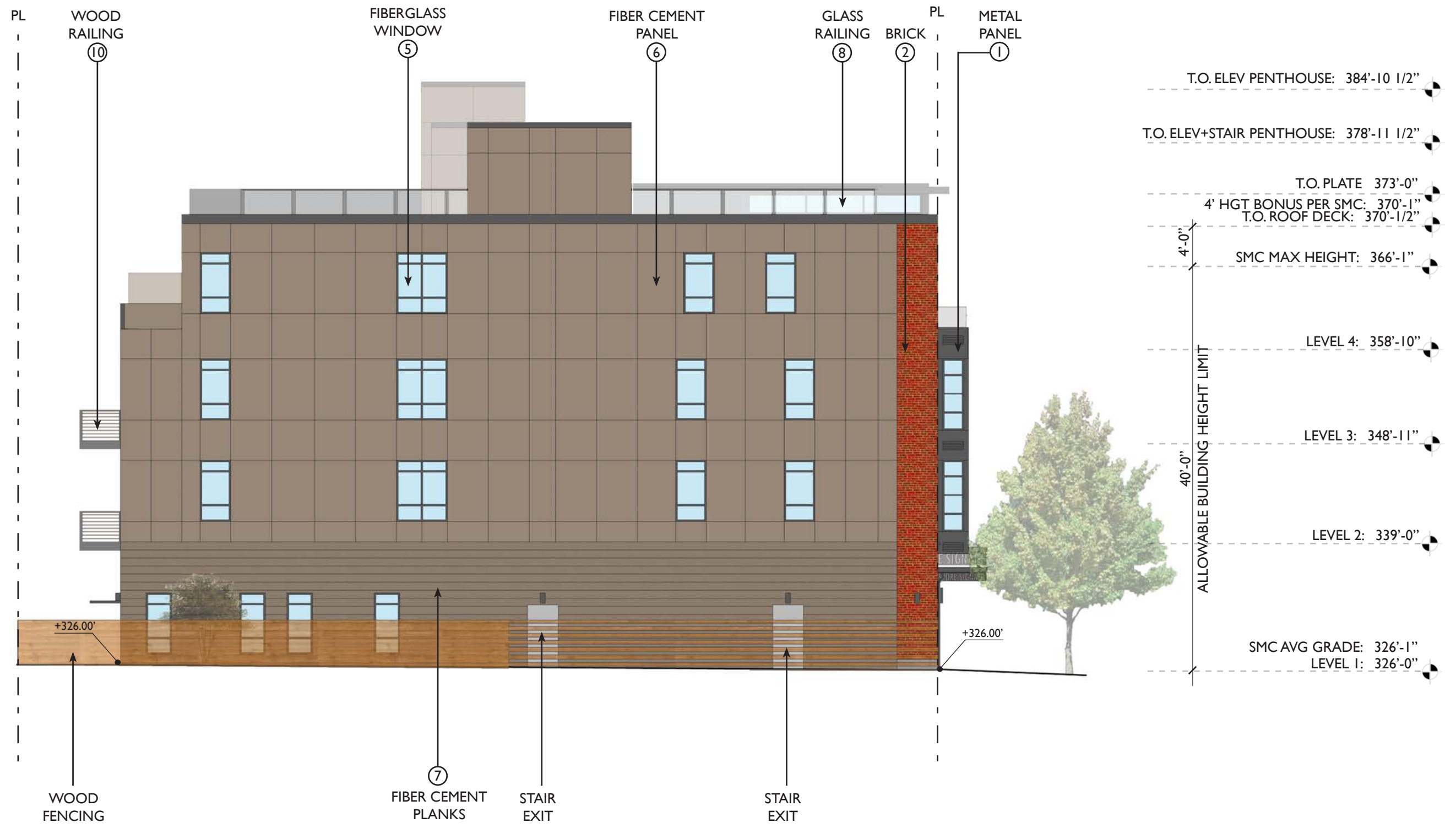


MATERIAL PALETTE

	<p>① METAL PANEL COLOR: BLACK</p>		<p>② BRICK COLOR: INCA</p>		<p>③ EXPOSED ARCHITECTURAL CONCRETE</p>		<p>④ ALUMINUM STOREFRONT GLAZING SYSTEM COLOR: BLACK</p>		<p>⑤ FIBERGLASS WINDOWS: COLOR: BLACK</p>
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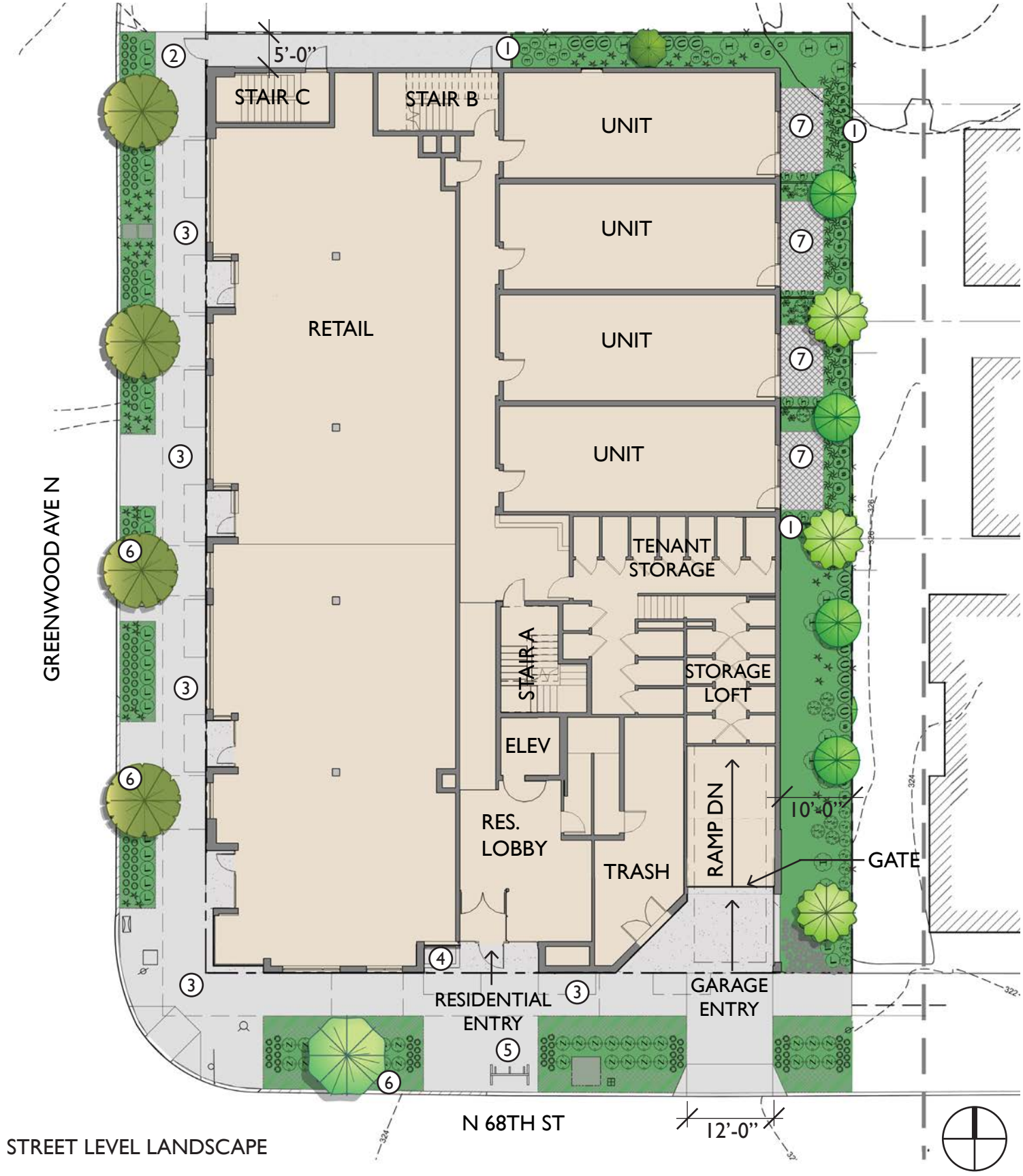
# NORTH ELEVATION



	<p>⑥ FIBER CEMENT PANELS COLOR: BENJAMIN MOORE - WHITALL BROWN</p>		<p>⑦ FIBER CEMENT PLANKS COLOR: BENJAMIN MOORE - WHITALL BROWN</p>		<p>⑧ GLASS RAILING ROOF</p>		<p>⑨ GLASS RAILING FRAMELESS - PATIO</p>		<p>⑩ WOOD RAILING: COLOR: BUTTERNUT STAIN</p>
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STREET LEVEL LANDSCAPE PLAN



① PATIO FENCE



② SECURITY FENCE



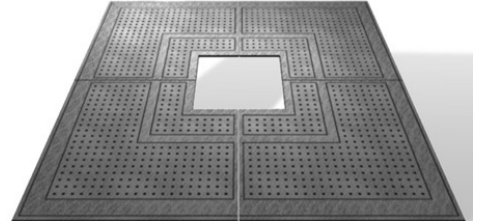
③ METAL CANOPY -FROSTED GLASS



④ BUILT-IN CONCRETE PLANTER



⑤ BIKE RACK



⑥ TREE GRATE

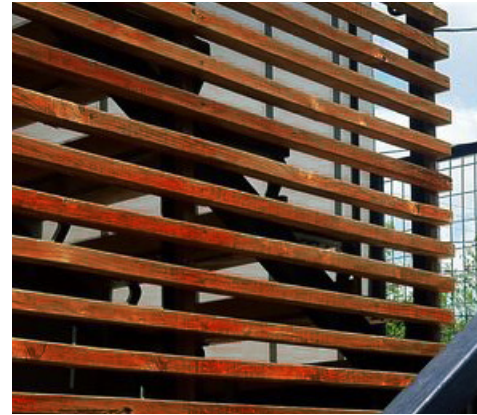


⑦ PATIO PAVING

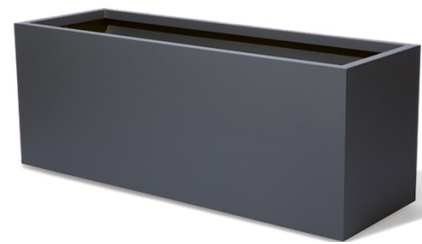




⑧ ROOF HATCH



⑨ WOOD RAILING



⑩ PLANTER



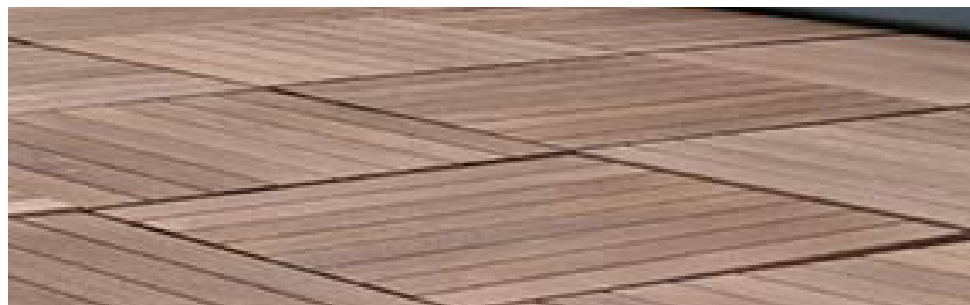
⑪ PLANTER WITH LIGHTING



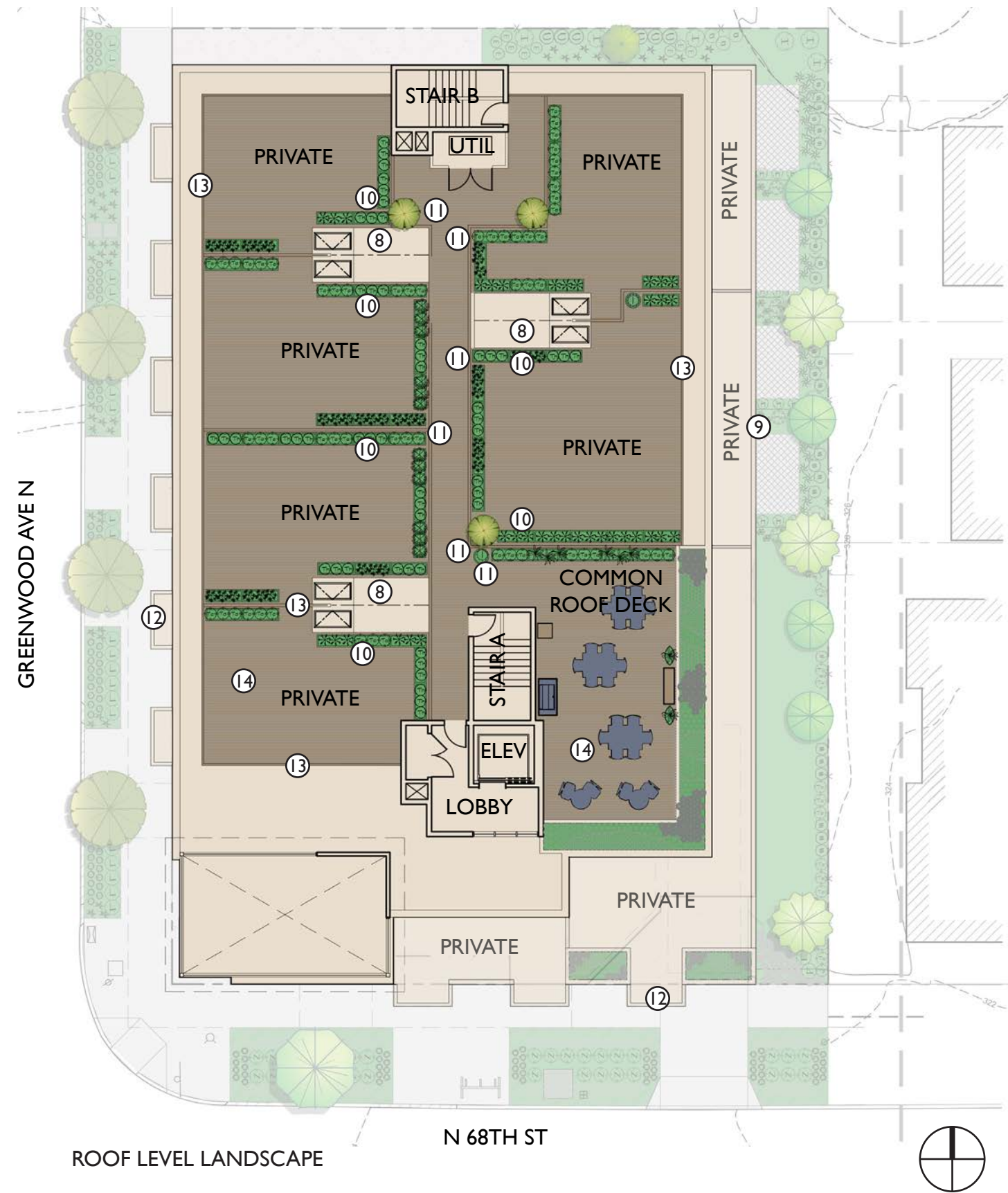
⑫ GLASS RAILING - ROOF DECK



⑬ GLASS RAILING - PATIOS



⑭ WOOD DECKING





PLANTING PALETTE



GOLDEN FULL MOOM MAPLE



BLACK MONDO GRASS



BLACK STEM BAMBOO



BRONZE NEW ZEALAND FLAX



CHARITY OREGON MAHONIA



DEER FERN



FIREFLY HEATHER



PEE WEE OAKLEAF  
HYDRANGEA



GOLDEN VARIEGATED  
JAPANESE SWEET FLAG



GULF STREAM NADINA



RAINBOW DROOPING  
FETTERBUSH



SKY PENCIL JAPANESE HOLLY



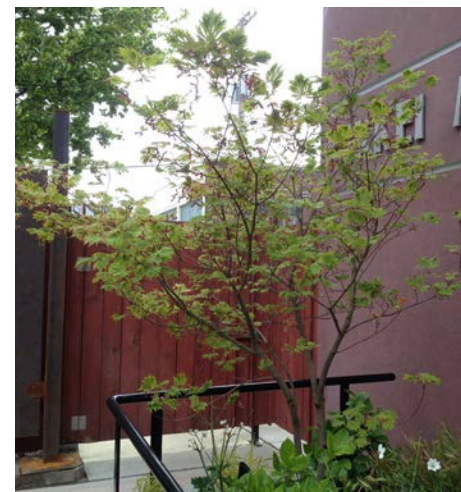
LONGLEAF MAHONIA



FRAGRANT SWEET BOX



PURPLE EMPEROR SEDUM



VINE MAPLE



GUMPO WHITE AZALEA



H.M. EDDIE YEW





DIANTHIUS EVERLAST



ELIJAH BLUE FESCUE



LITTLE BUNNY FOUNTAIN GRASS



HOSTA FIRE AND ICE



LENTON ROSE CINNAMON AND SNOW



JAPANESE FOREST GRASS



PYRAMIDAL EUROPEAN HORNBEAM



EMERALD GREEN ARBORVITAE



DAWYCK PURPLE BEECH



SUNSET GLOW BAMBOO



IVORY HALO DOGWOOD



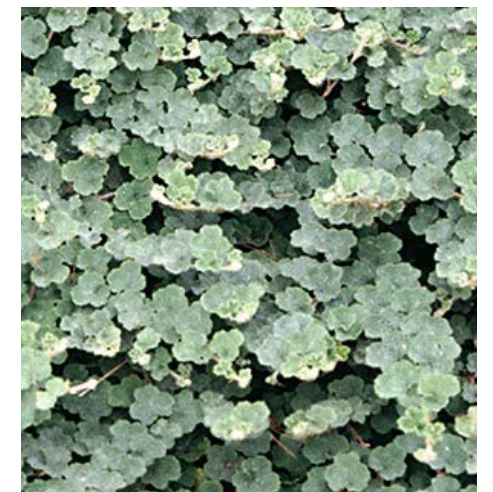
JAPANESE HORNBEAM



EMERALD SUNSHINE ELM



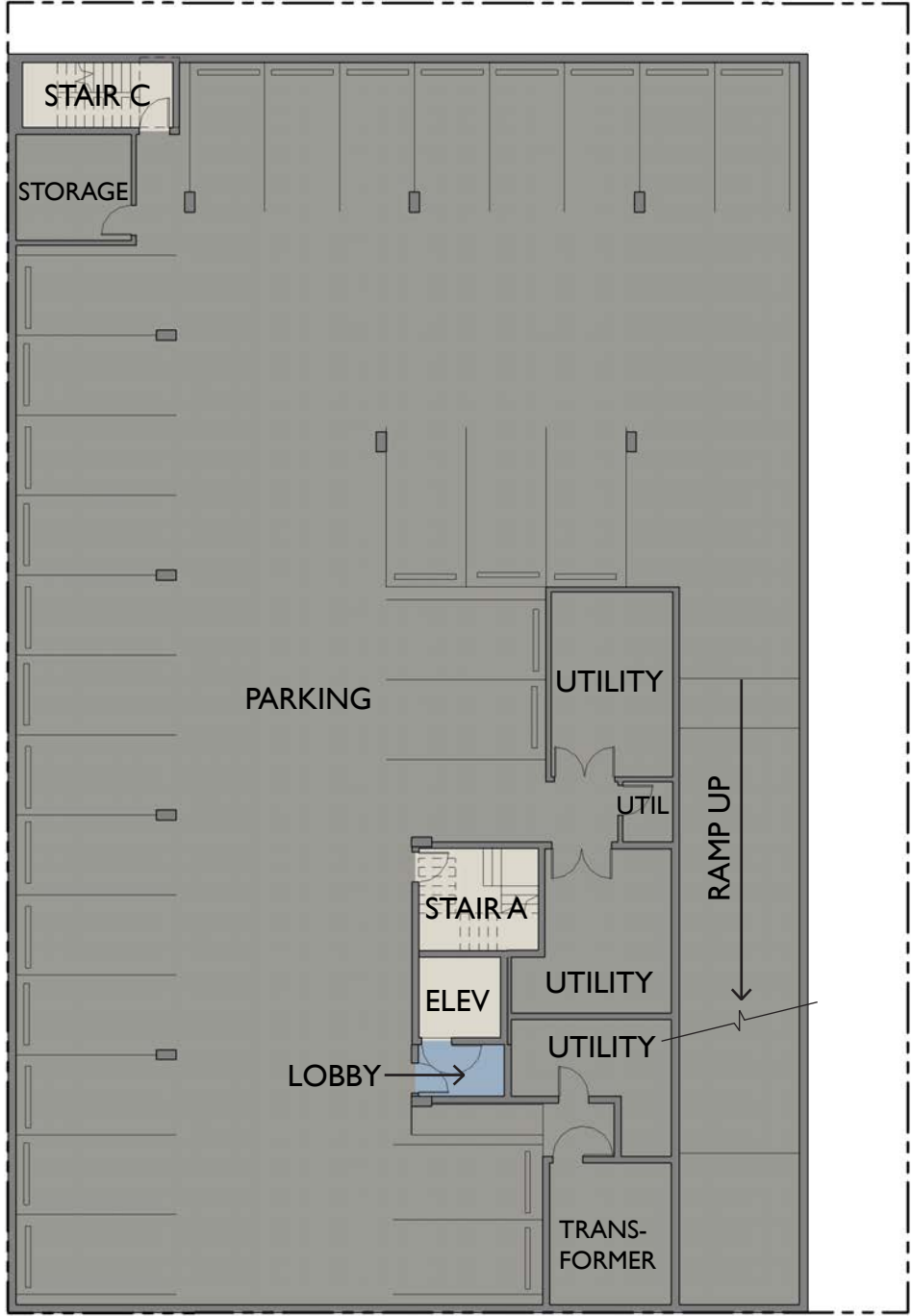
UMBRELLA BAMBOO



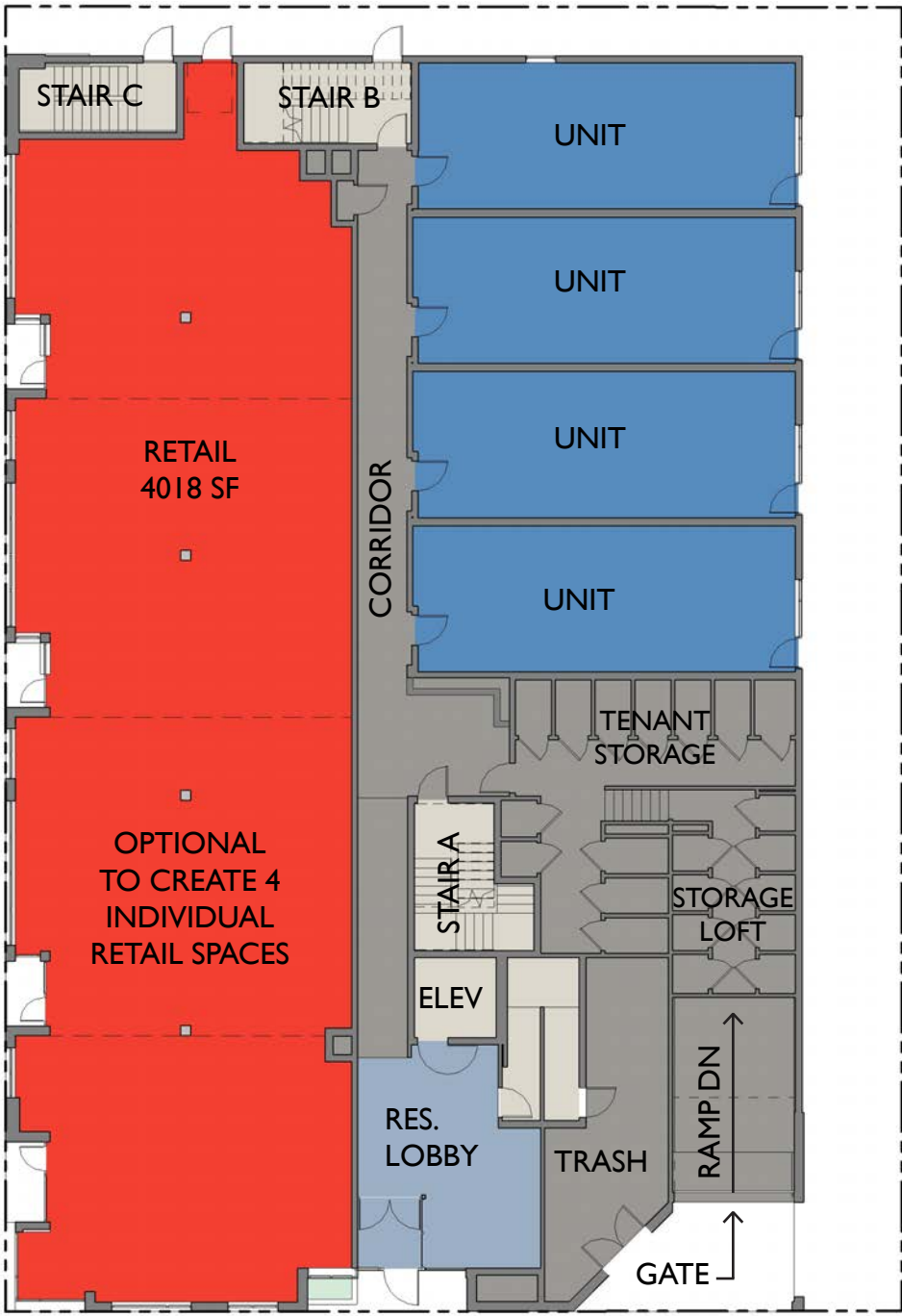
EMERALD CARPET BRAMBLE



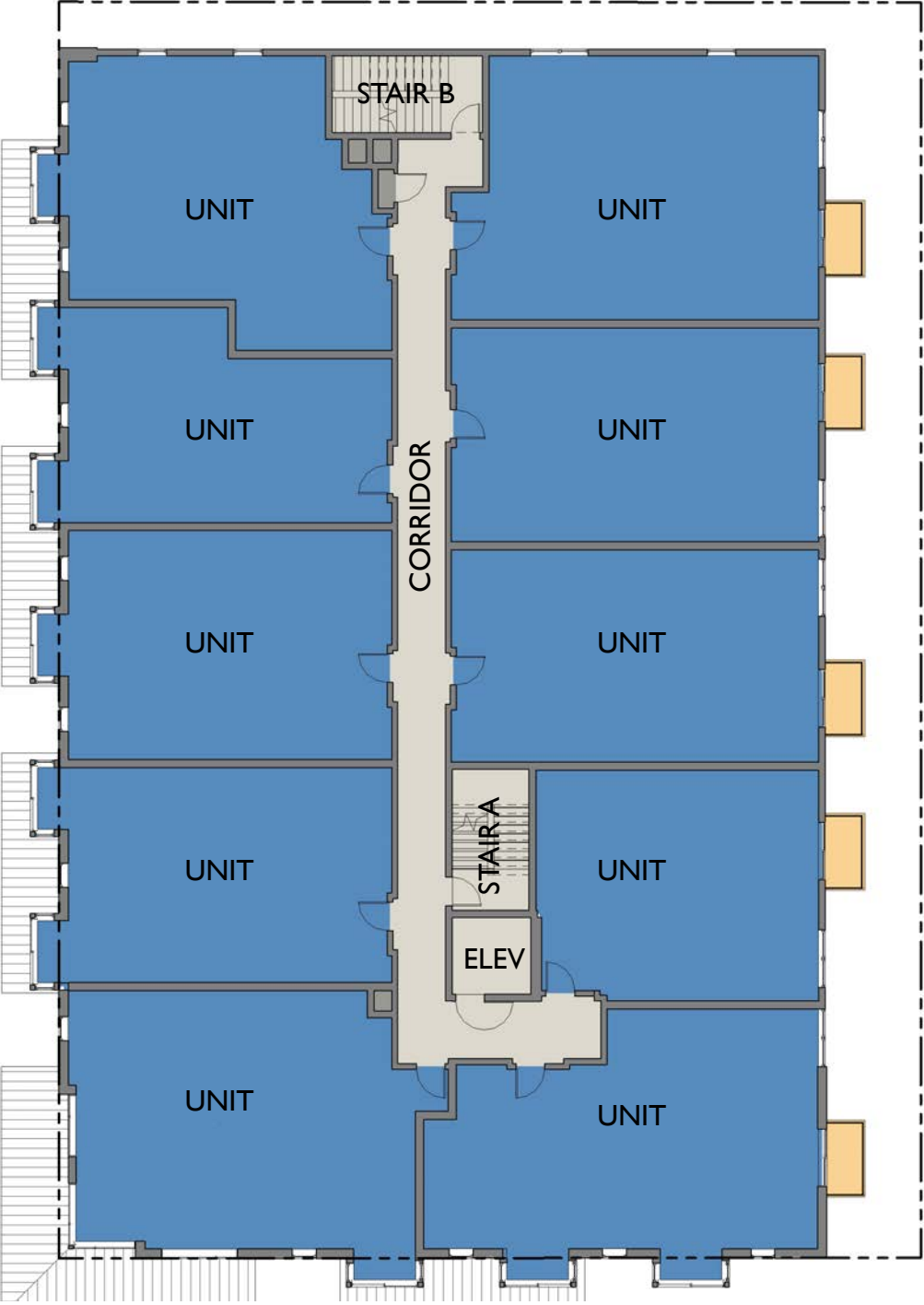
FLOOR PLANS



LEVEL P1

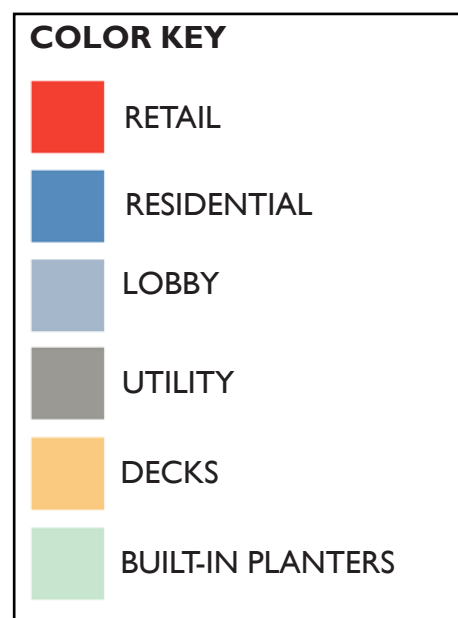
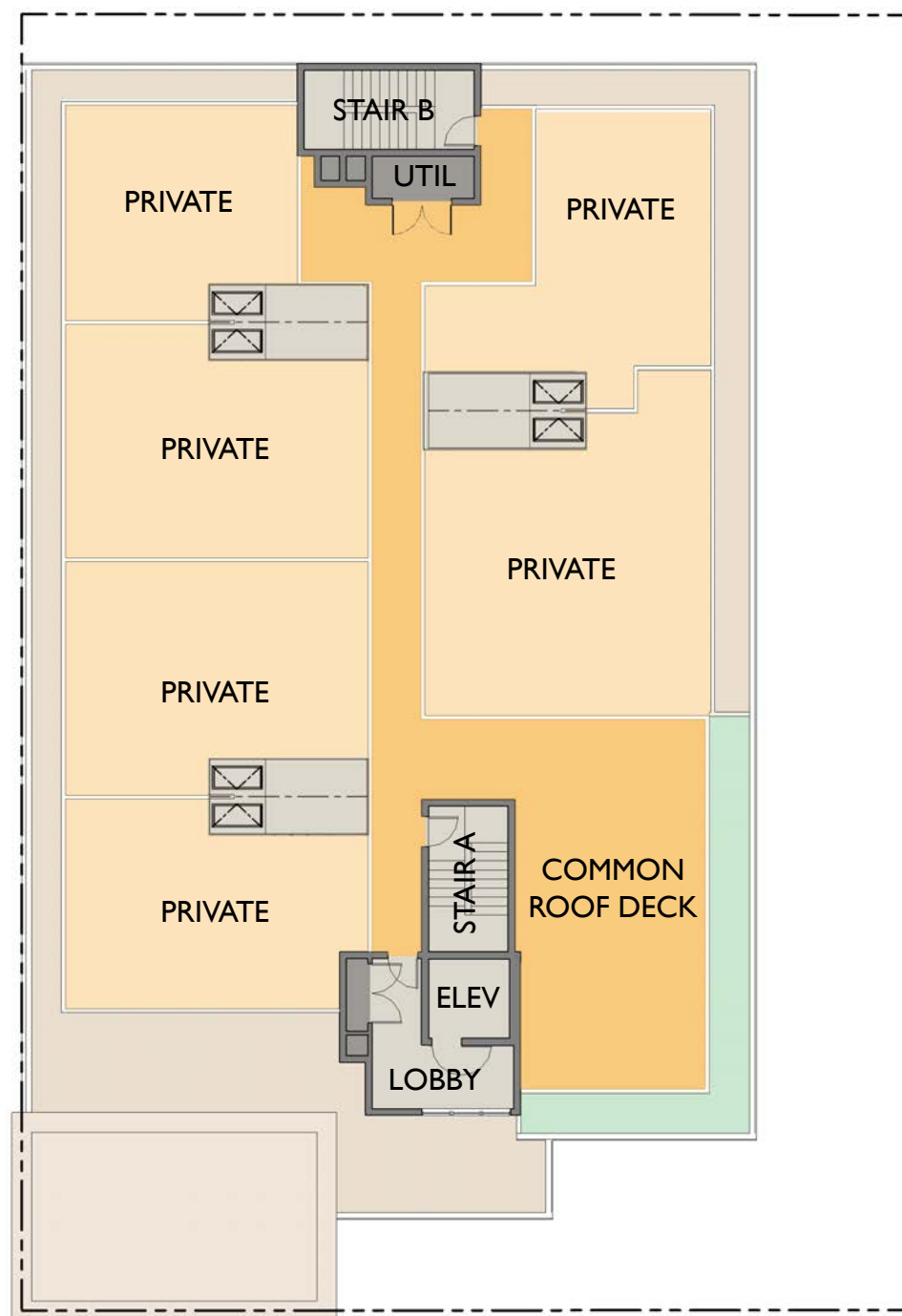
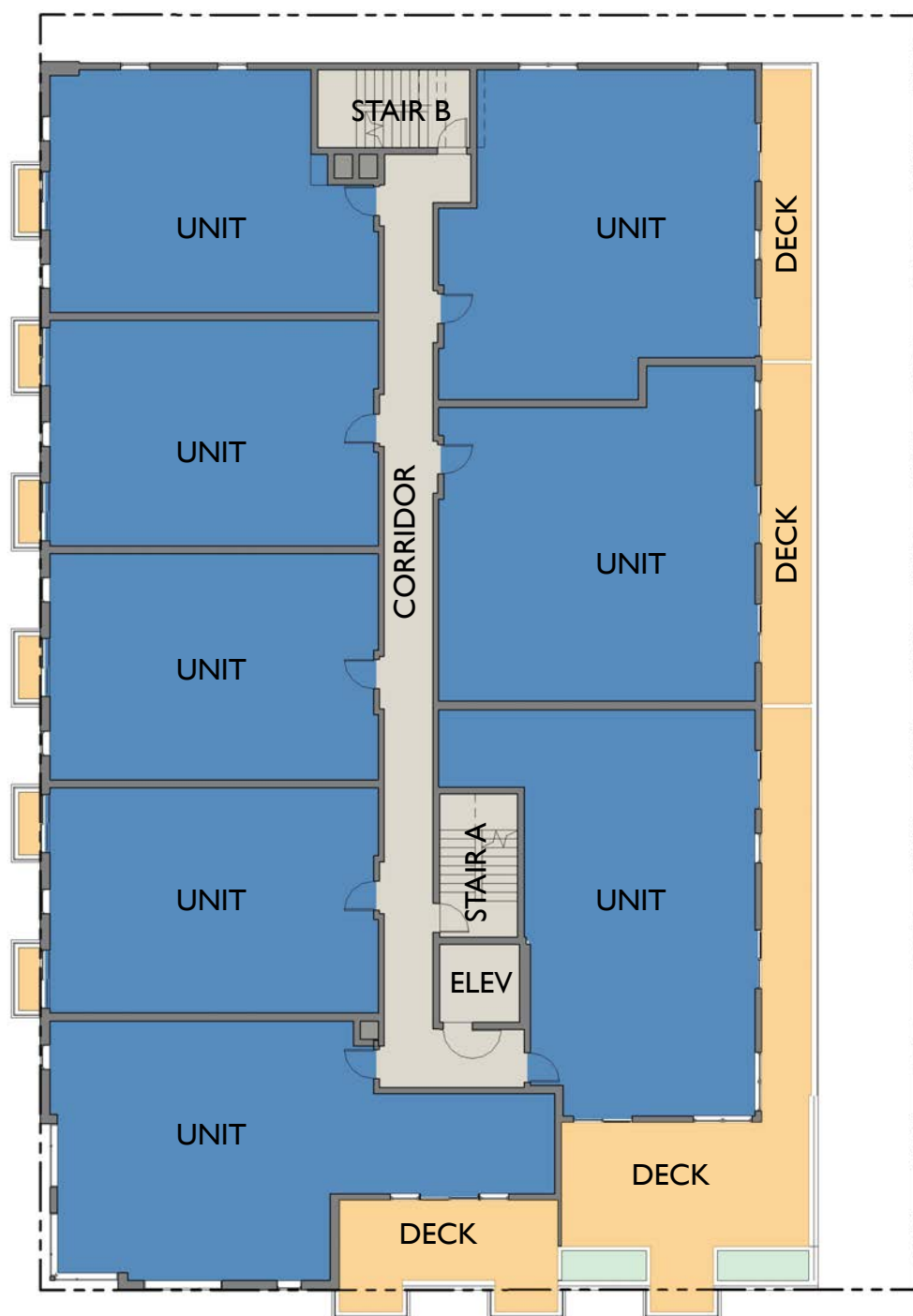


LEVEL I



LEVEL 2-3





LEVEL 4



ROOF LEVEL



CORNER DESIGN: GREENWOOD AND 68TH









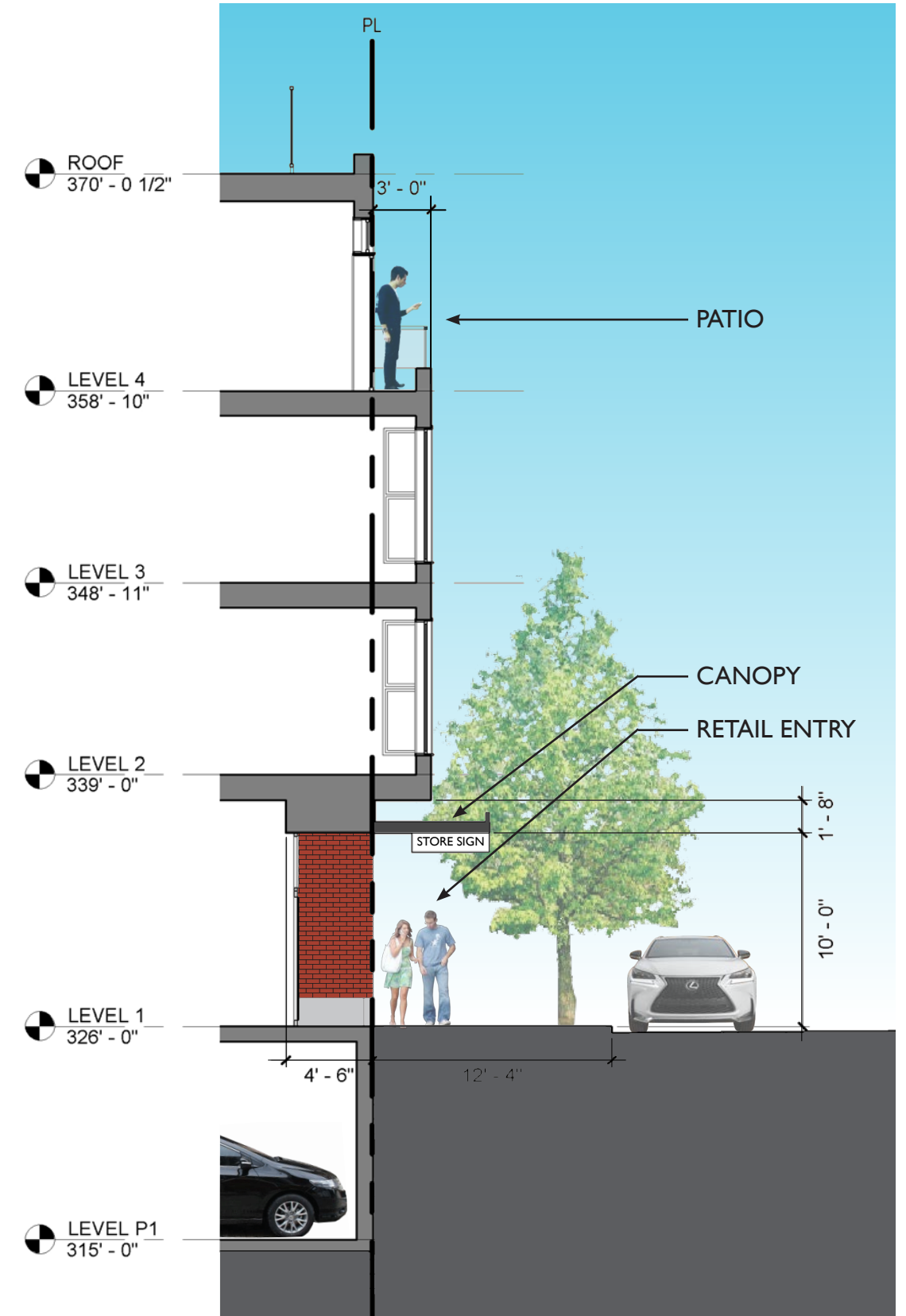
# GREENWOOD AVENUE STOREFRONTS



STOREFRONT EXPERIENCE ALONG GREENWOOD

## GREENWOOD PEDESTRIAN EXPERIENCE

New street trees soften the street edge and add color and shade for pedestrians, while canopies above offer shelter from Seattle’s frequent rainy days. Generously sized planting strips around the trees provide a buffer between pedestrians and traffic as well as break up the concrete walk into more human-scaled sections. The canopies have gaps located at each street tree to allow for their growth and create a pleasing engagement between the building and nature. Doorways into the retail spaces are recessed to provide additional shelter and a momentary landing space outside the flow of foot traffic prior to entry. Signage is attached to the canopies and wall sconces light the sidewalk and bounce off the underside of the canopies to provide a safe, glowing light at night.



SECTION THROUGH RETAIL ENTRANCE CANOPY





RESIDENTIAL ENTRY ALONG 65TH

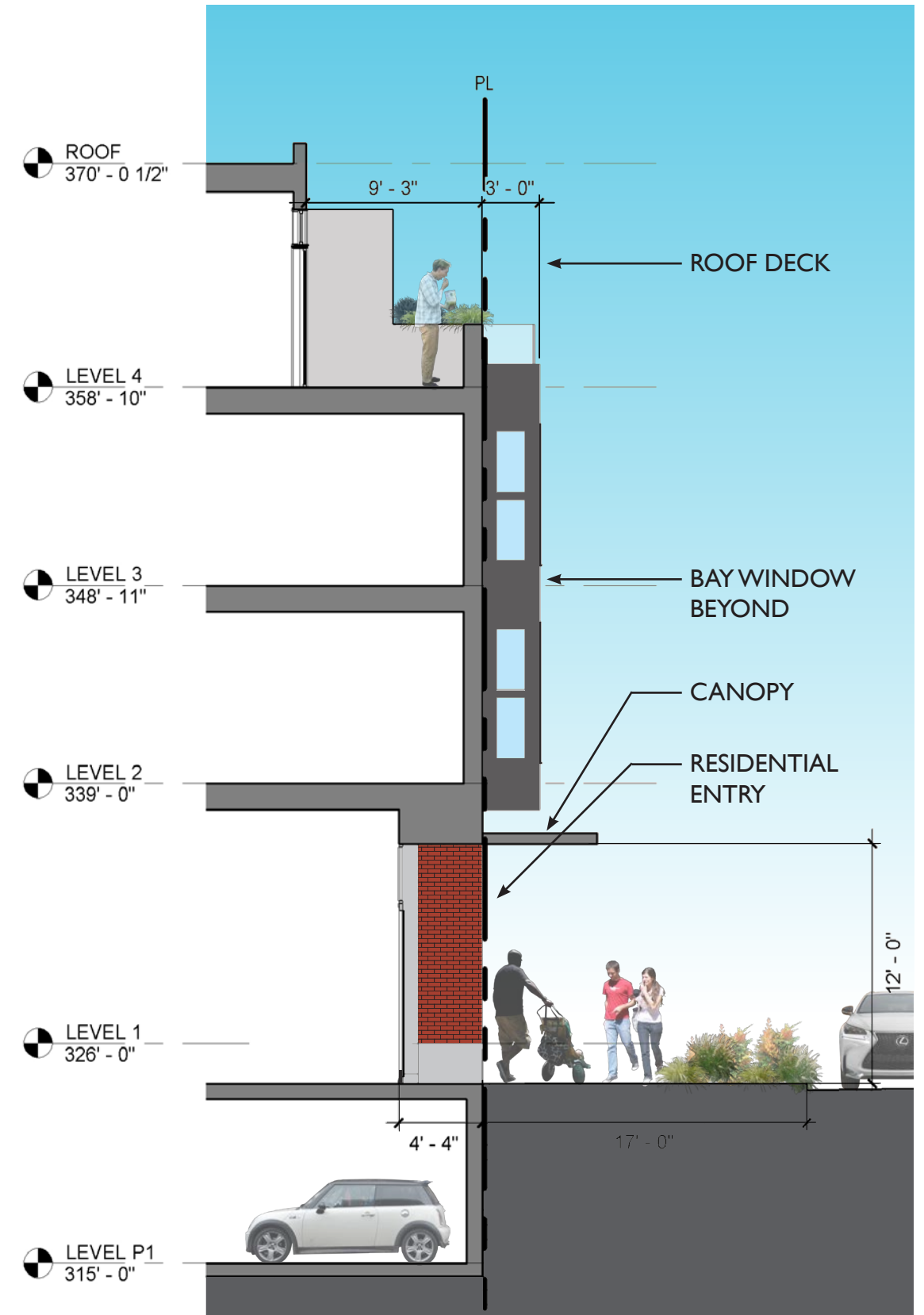
### 68TH PEDESTRIAN EXPERIENCE

The broader sidewalk along 68th, combined with recesses at the residential entry and garage driveway, allow for larger planting strips that enhance the residential character of this street. The shading and sheltering canopies wrap around the corner from Greenwood and help preserve the pedestrian scale of this elevation. Visibility is ample at all entries, and there are numerous opportunities for eyes on the street. A bike rack is provided for guests.



VIEW LOOKING WEST ALONG N 68TH ST

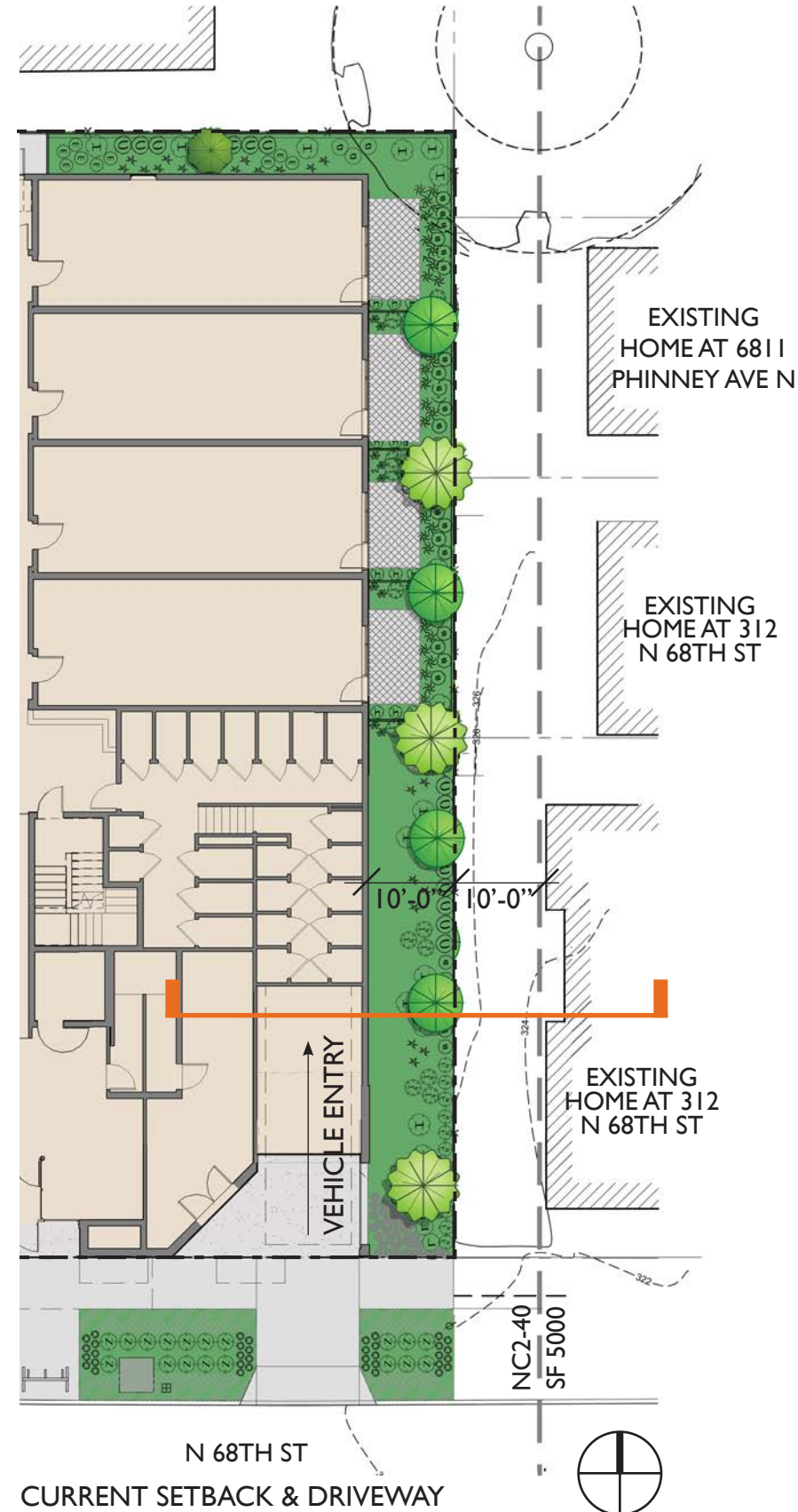
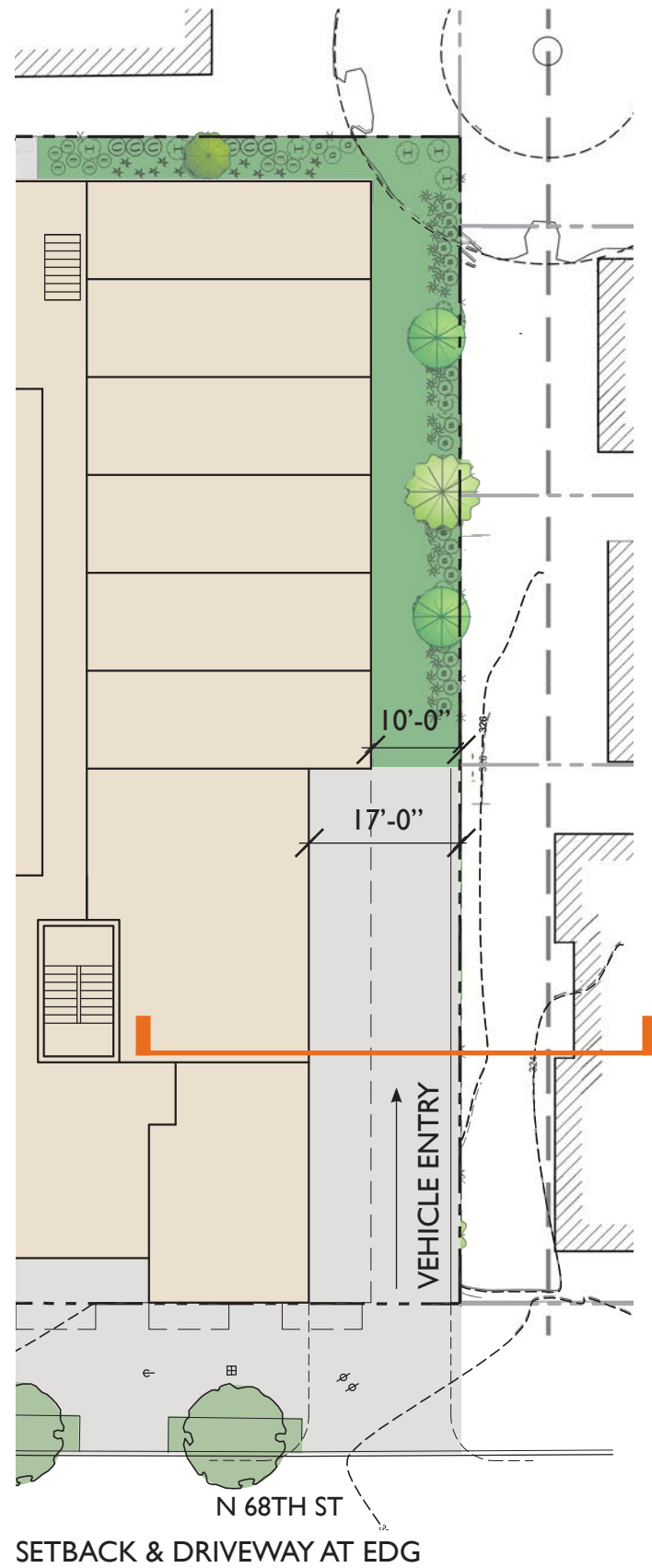
### N 68TH STREET AND RESIDENTIAL ENTRY



SECTION THROUGH RESIDENTIAL ENTRY CANOPY



# SETBACK AT ADJACENT SINGLE FAMILY



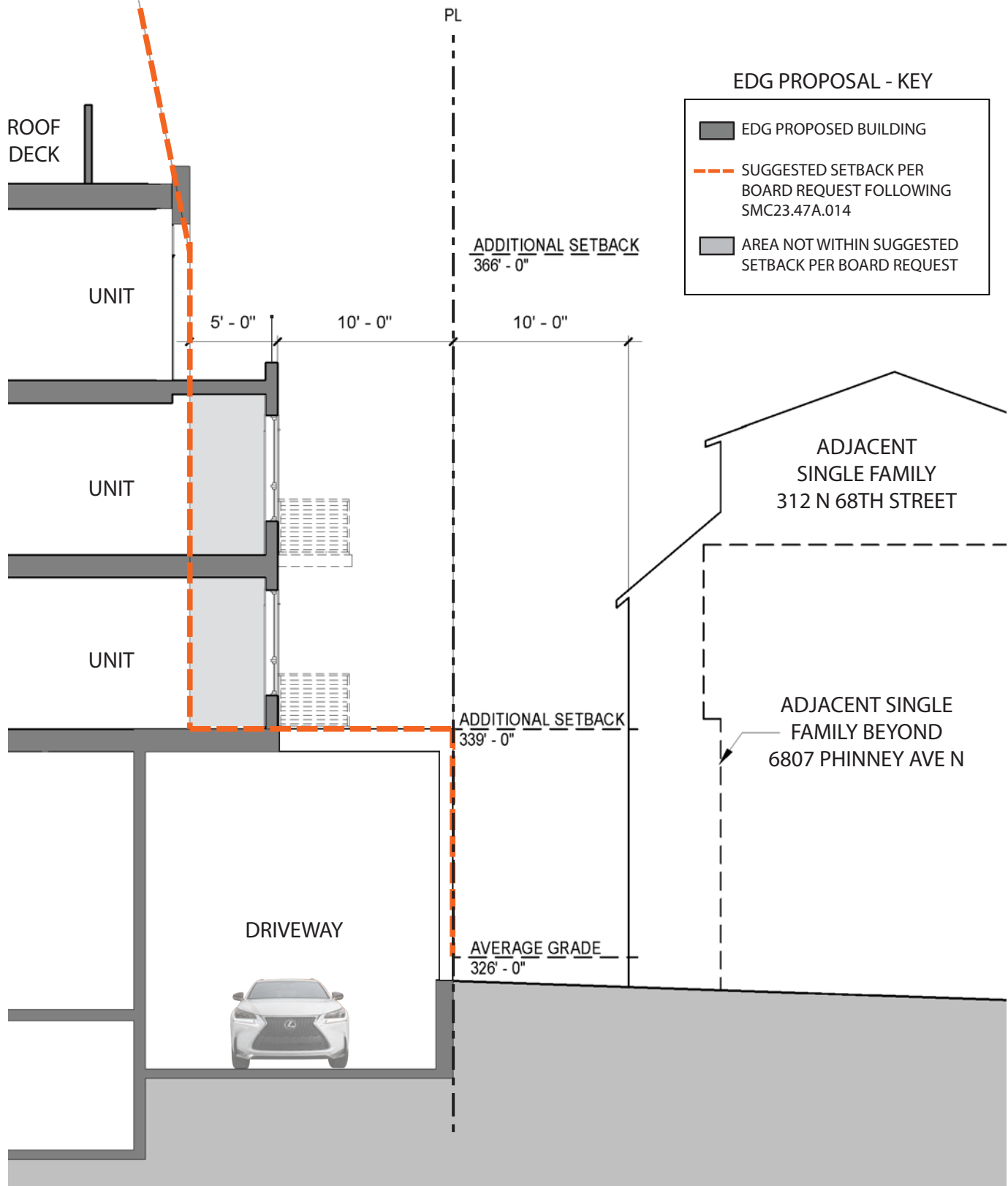
PROPOSED SETBACK AT EAST PROPERTY LINE, LOOKING SOUTH TOWARDS 68TH

## CREATING A THOUGHTFUL TRANSITION

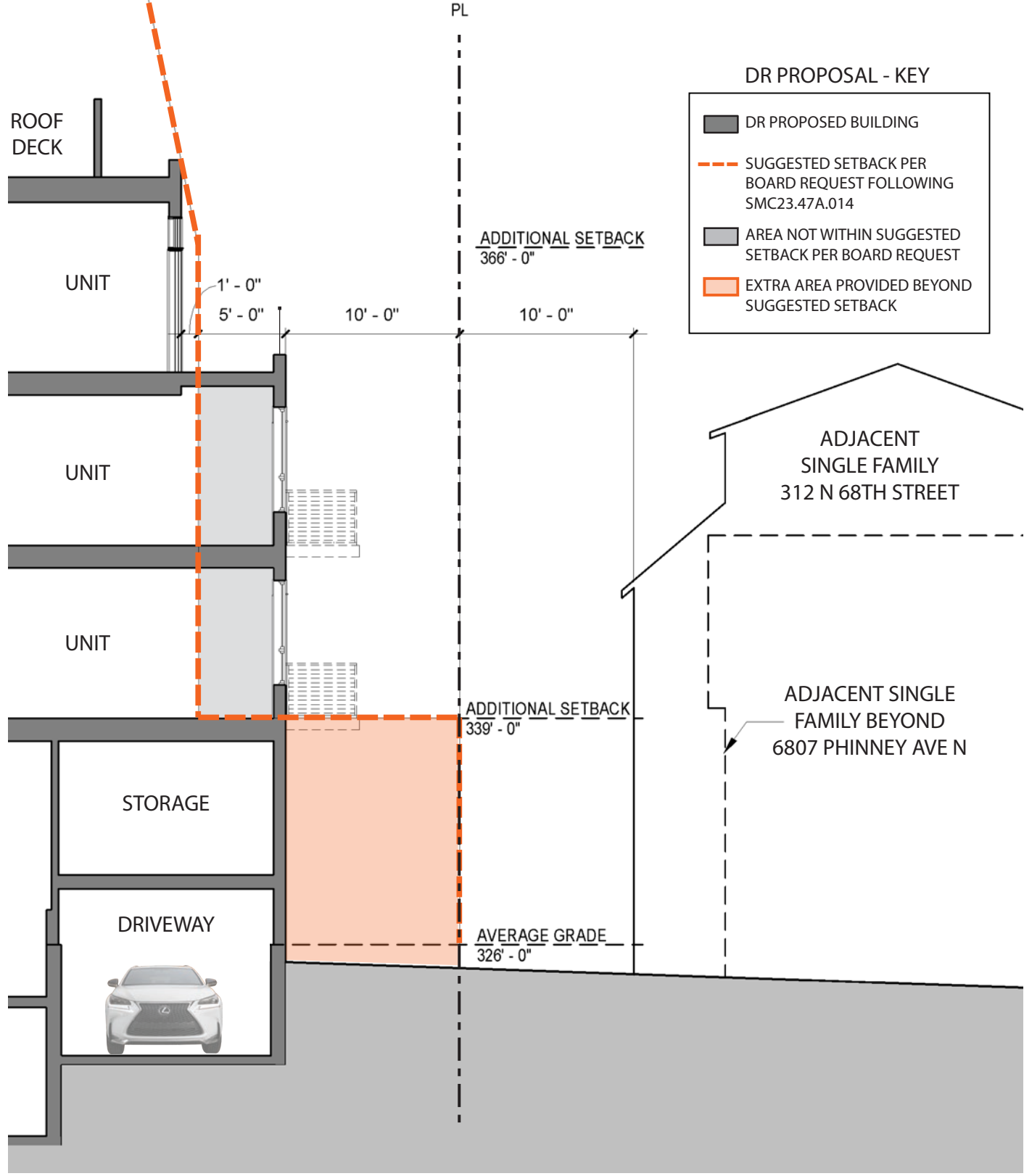
By taking the 60' long garage driveway ramp that was on the property line at EDG and pulling it back flush to the building, over 600 square feet of additional greenspace was added to the setback facing the single-family neighbors. Additionally, the driveway has been enclosed within the building walls to minimize light, noise and odors. The private decks on Level Four are now 6' deep and the main facade has been modulated with small decks that utilize railings with horizontal slats, allowing views to the horizon while blocking line-of-sight to the yards below. The east elevation now provides substantially more buffer space/volume than would typically be required by SMC23.47A.014.B.3 as shown graphically at right.



SETBACK AT ADJACENT SINGLE FAMILY - EDG + DR PHASE



SECTION AT EAST PROPERTY LINE SETBACK - EDG PROPOSAL



SECTION AT EAST PROPERTY LINE SETBACK - DR PROPOSAL

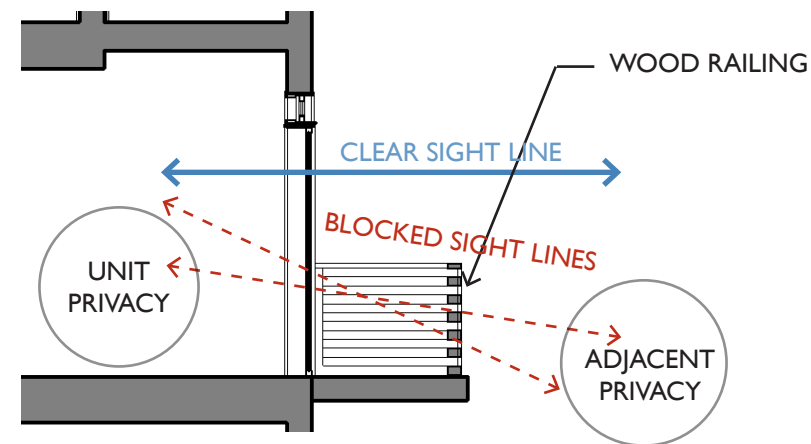




EAST ELEVATION WITH OVERLAY OF ADJACENT FACADES





**PROVIDING LIGHT AND VIEWS WHILE RESPECTING PRIVACY**

This project uses several techniques to minimize visual intrusion upon its neighbors to the east. Privacy fencing and a floor level that is several feet lower than the adjacent houses means that existing homes need not worry about any views from these spaces. Windows on the upper floors are mostly misaligned, and the decks utilize privacy railings to prevent views through the glass doors down into the yards below. The upper floor and roof decks utilize setbacks to block views to the areas below while maintaining views to the lake and mountains beyond.



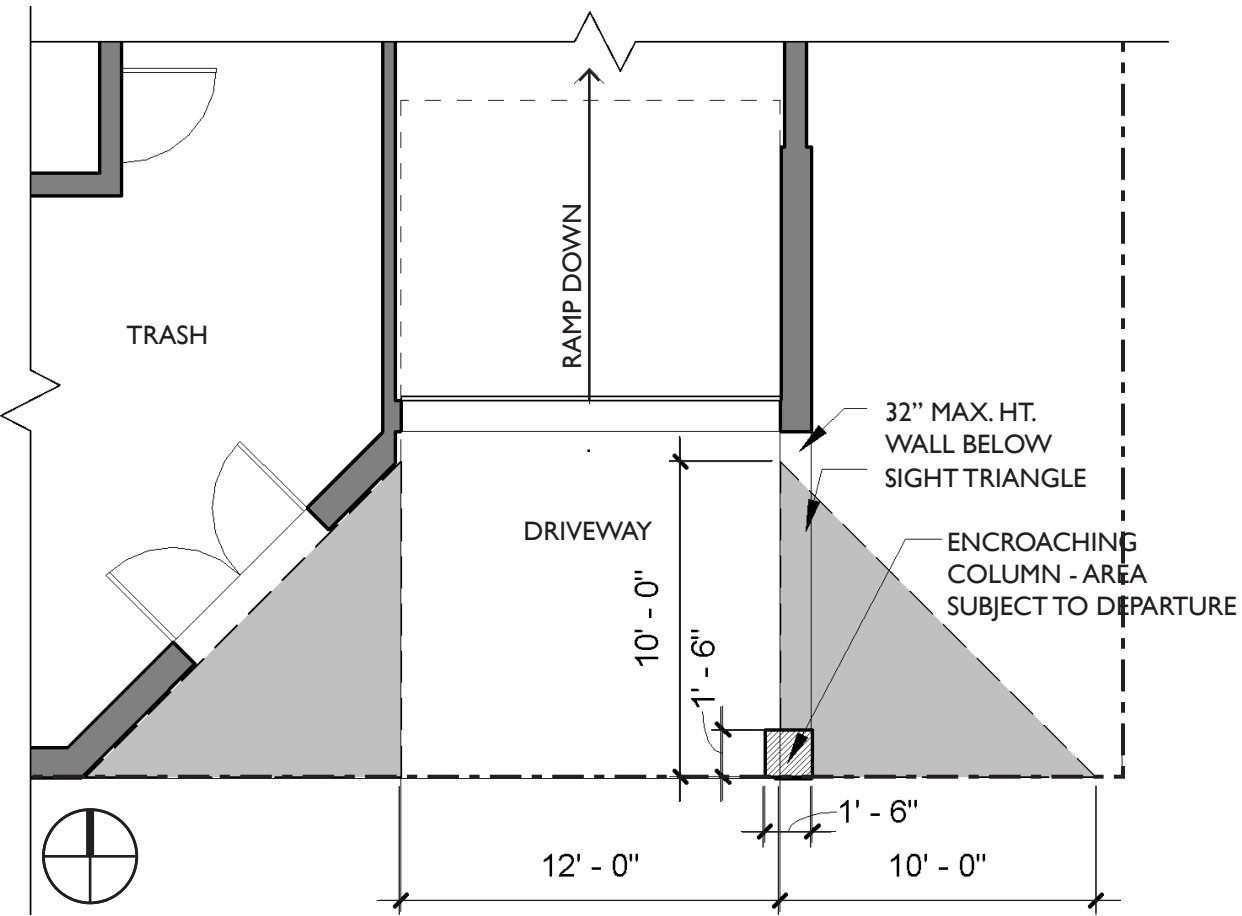
VISIBILITY CONTROL AT PRIVATE DECKS

**COLOR KEY**

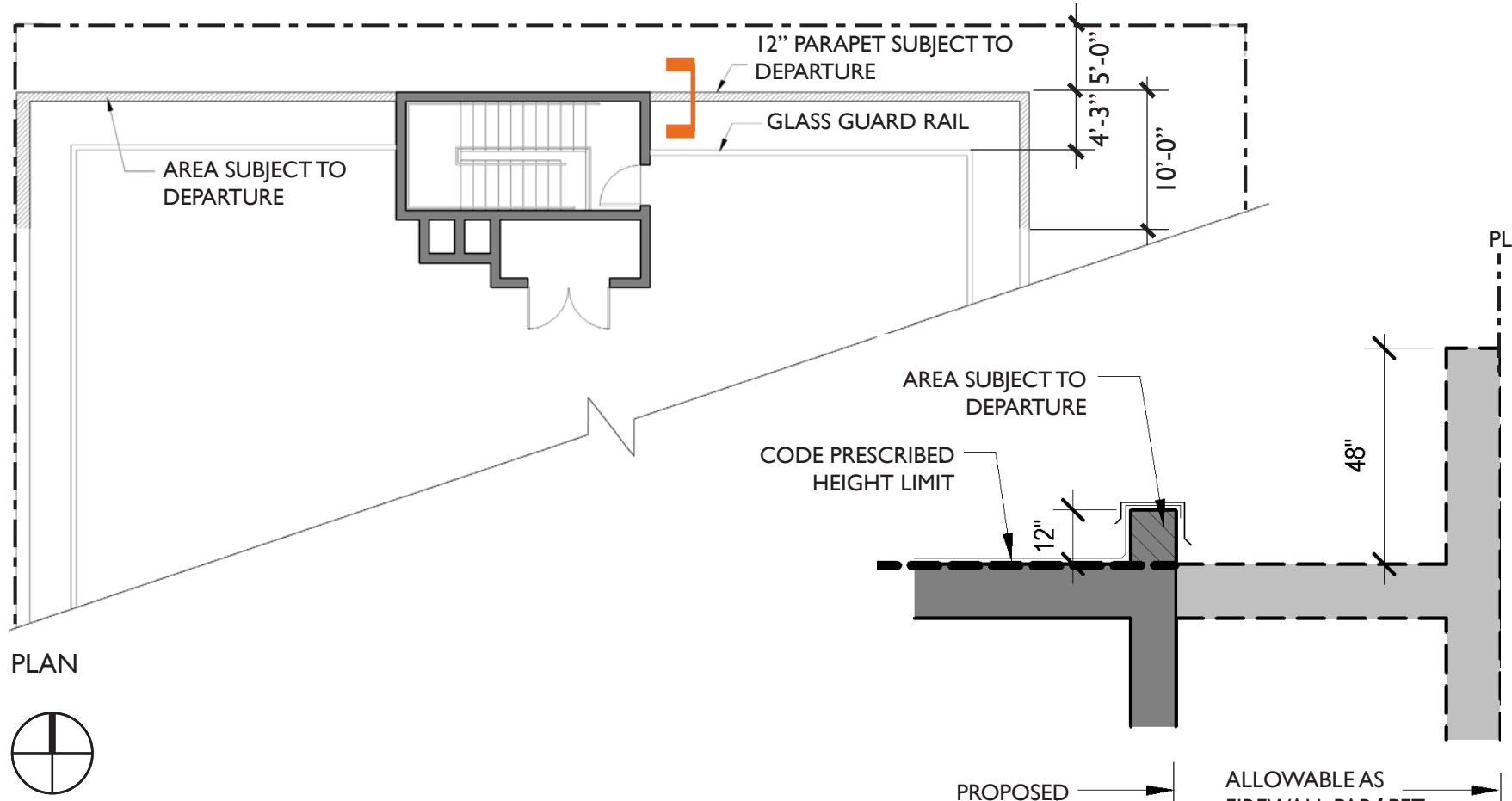
-  FENCE
-  OUTLINE OF ADJACENT HOMES
-  OUTLINE OF ADJACENT GLAZING
-  OVERLAY OF PROPOSED AND ADJACENT GLAZING



DEVELOPMENT STANDARD	REQUIREMENT	PROPOSED	DEPARTURE AMOUNT	REASON FOR DEPARTURE	DESIGN REVIEW GUIDELINES
#1 SMC 23.54.030.G Driveway Sight Triangle Obstruction	A sight triangle shall be kept clear of any obstruction	A structural column shall be located within the sight triangle	Area of sight triangle: $10' \times 10' / 2 = 50 \text{ SF}$  Area of column: $1' - 6" \times 1' - 6" = 2.25 \text{ SF}$  Departure Amount: 4.5%	Placing the structural column within the sight triangle allows the project to better meet the following Design Review Guidelines: <b>A-5 Respect for Adjacent Sites</b> <b>A-7 Residential Open Space</b> <b>D-5 Visual Impacts of Parking Structures:</b> By allowing the column in the sight triangle, the driveway structure is fully located within the building footprint and preserves the rear yard area as a buffer between the building and the single family residence to the east. Without the departure, the structural columns and overhead beams would be located within the green space.	A-5 Respect for Adjacent Sites A-7 Residential Open Space D-5 Visual Impacts of Parking Structures
#2 SMC 23.47A.012-C.7 Rooftop Features at North Edge of Roof	Non-firewall parapets shall be located at least 10' from the north edge of the roof unless demonstrated that they will not shade property to the north on January 21st at noon more than would a structure built to maximum permitted height and FAR.	A continuous 12 inch parapet located along the roof edge and within 10' of the north edge.	12" above the height limit	Allowing the small parapet proposed for this building to continue uninterrupted into the northern 10' of the roof would provide for a superior waterproofing solution and allow the project to better meet the following Design Review Guidelines: <b>A-5 Respect for Adjacent Sites:</b> Zoning allows the project to be built to the north property line with a firewall parapet or for a railing to be installed along the entirety of the roof edge, either of which would shadow northern properties far more than the proposed departure which, when combined with the proposed 5' setback, will allow for additional open space, light, and air between the structures. <b>C-2 Architectural Concept and Consistency:</b> Without the departure, the parapet would have a break at an arbitrary point along the top of the west masonry wall, creating a visually distracting interruption in the top of a wall that is a defining element of the building's character.	A-5 Respect for Adjacent Sites C-2 Visual Impacts of Parking Structures



DEPARTURE #1 - SIGHT TRIANGLE AT DRIVEWAY ENTRANCE



DEPARTURE #2

SECTION

ALLOWABLE AS FIREWALL PARAPET



# NORTH PROPERTY LINE

## ACCESS AND SECURITY AT NORTH WALKWAY

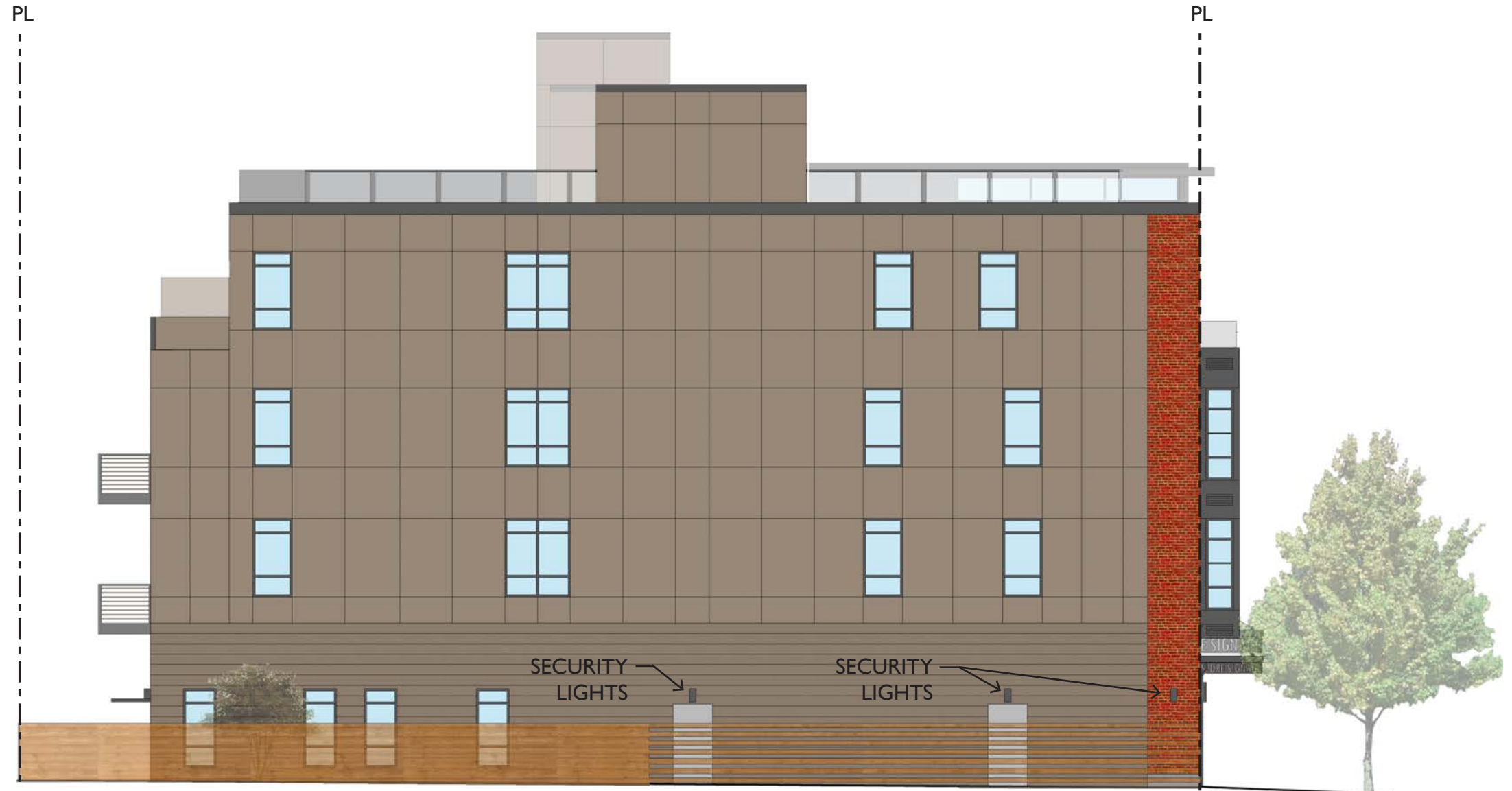
In order to allow light and air into residential units along the north facade and reduce shadowing on the neighboring property, the building is held back from the north property line 5' along most of its length. The east portion of this yard will be fenced and used as a private greenspace by the adjacent ground-floor tenant, while the western half will be a paved walkway providing an egress path for two of the building's stairwells. To enhance security in this small space, these doors will be exit only and the area will be well lit by wall sconces that will minimize light and glare on the neighbors.



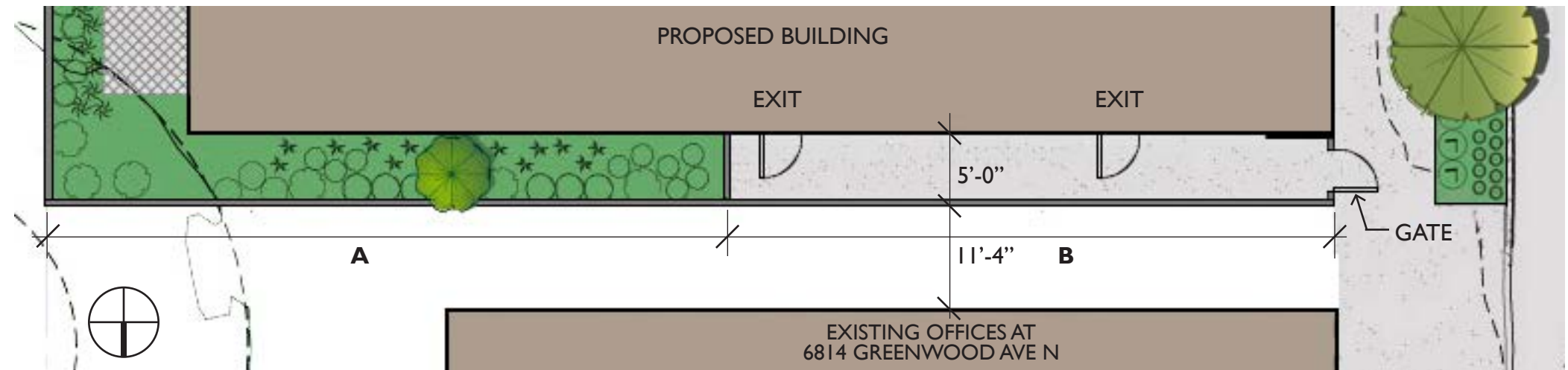
A. PRIVATE PATIO FENCE



B. SECURITY FENCE



NORTH ELEVATION



FENCING AT NORTH PROPERTY LINE





RETAIL SIGN AT CORNER OF GREENWOOD AND 68TH



RESIDENTIAL ENTRY SIGN



BUILDING ADDRESS AT CORNER OF GREENWOOD AND 68TH



EXAMPLE RETAIL BLADE SIGN UNDER CANOPY



EXAMPLE RETAIL BLADE SIGN UNDER CANOPY



BLADE SIGN EXAMPLES





EXTERIOR LIGHTING



STREET LEVEL LIGHTING PLAN



ROOF LEVEL LIGHTING PLAN

① RECESSED LIGHTING  
MANUFACTURER / MODEL



② SINGLE LIGHT SCONCE  
MANUFACTURER / MODEL



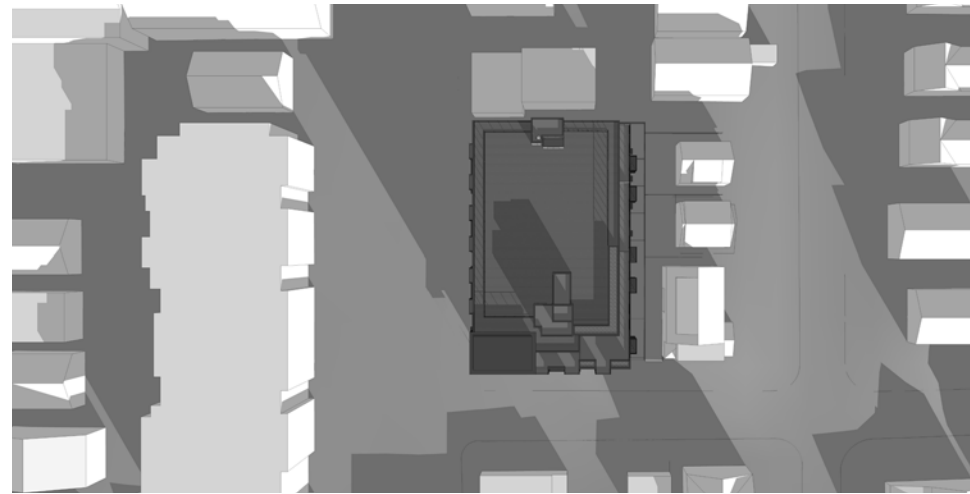
③ DOUBLE LIGHT SCONCE  
MANUFACTURER / MODEL



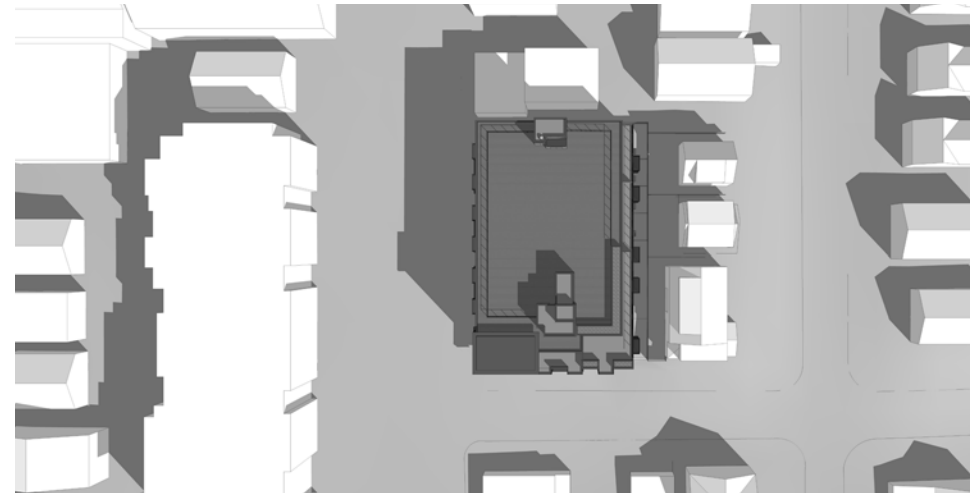
④ PLANTER PATH LIGHTING  
MANUFACTURER / MODEL







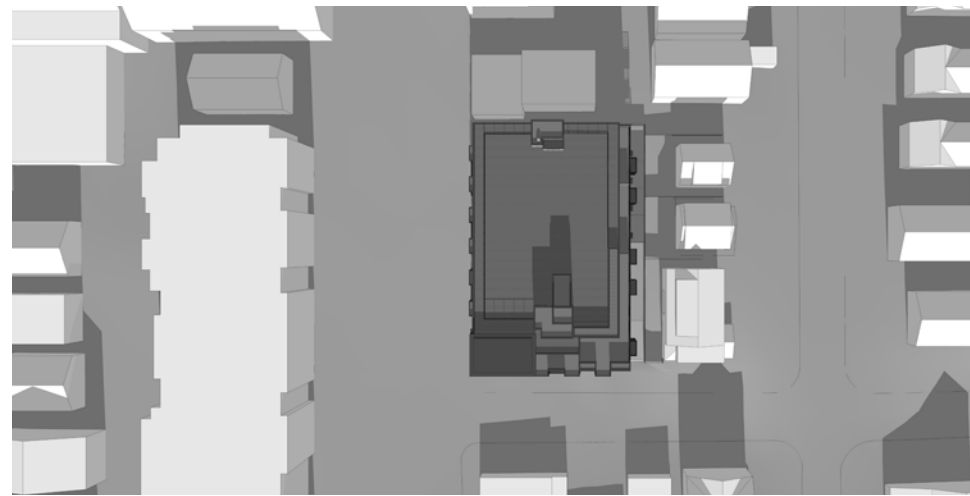
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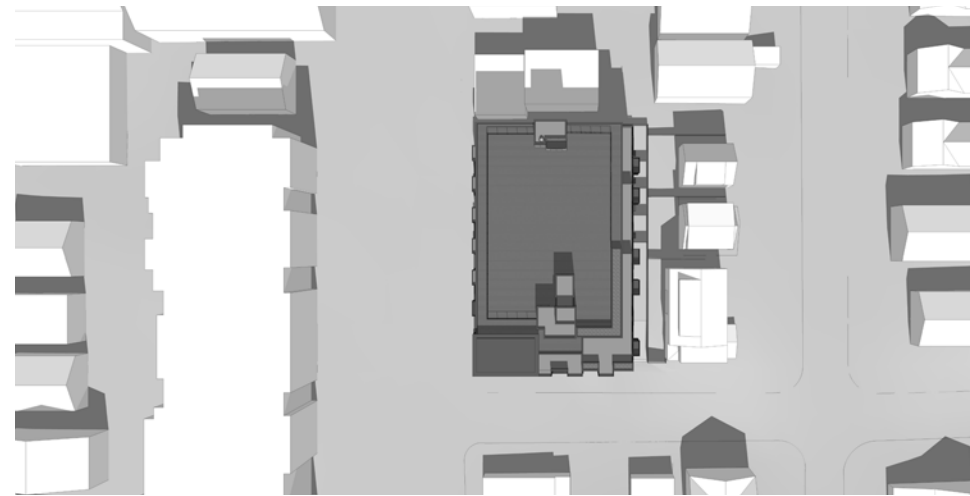
MARCH 21, 9AM



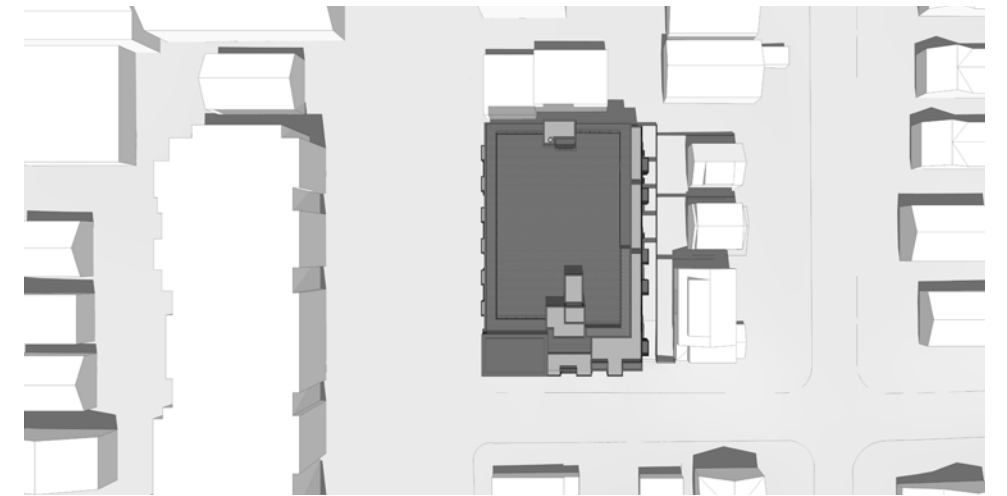
JUNE 21, 9AM



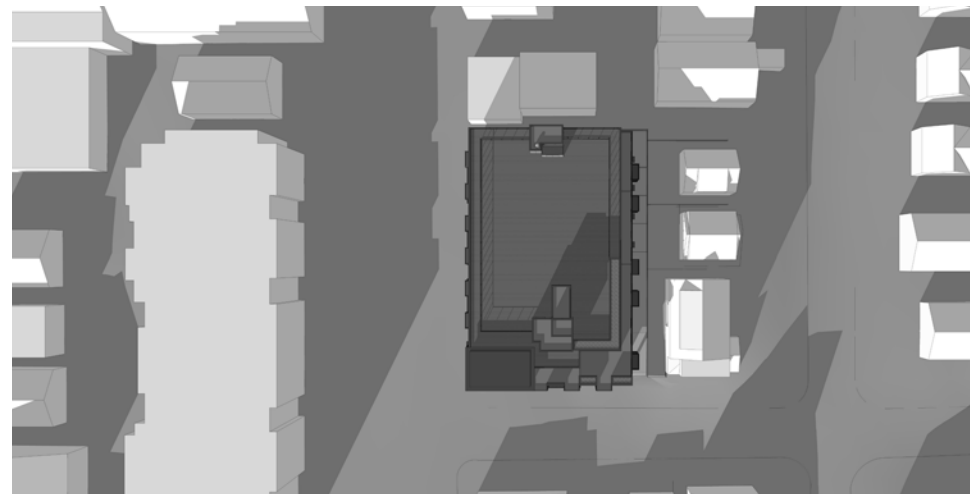
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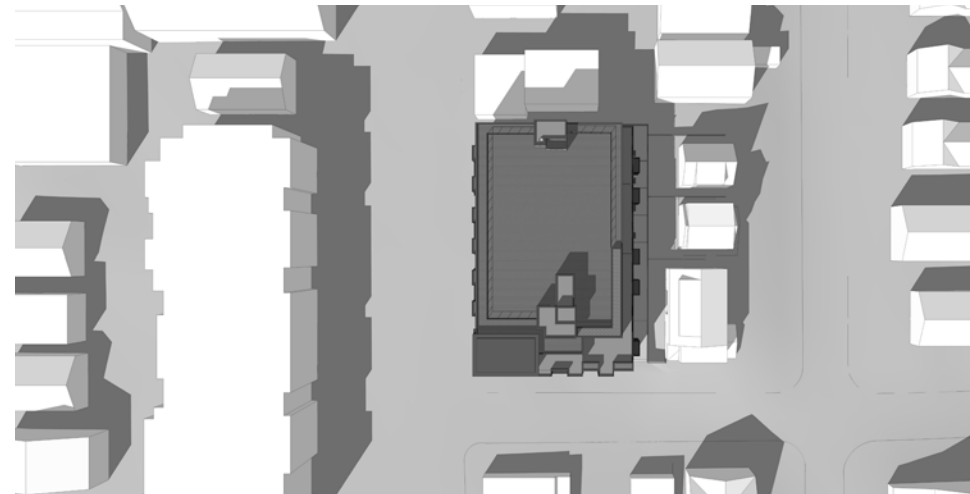
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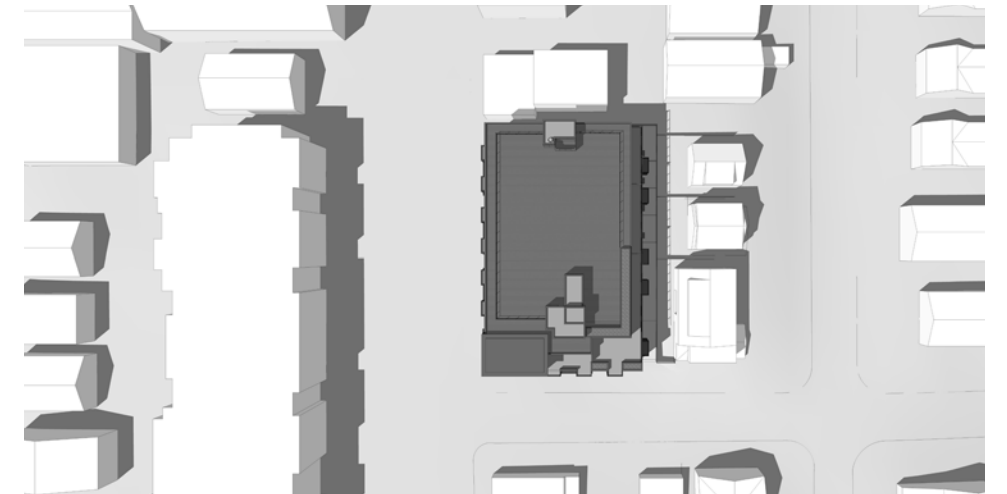
JUNE 21, NOON



DECEMBER 21, 3 PM



MARCH 21, 3 PM



JUNE 21, 3 PM



RECENT ISOLA HOMES PROJECTS





RECENT NK PROJECTS



HARBOR WORK/LIVE



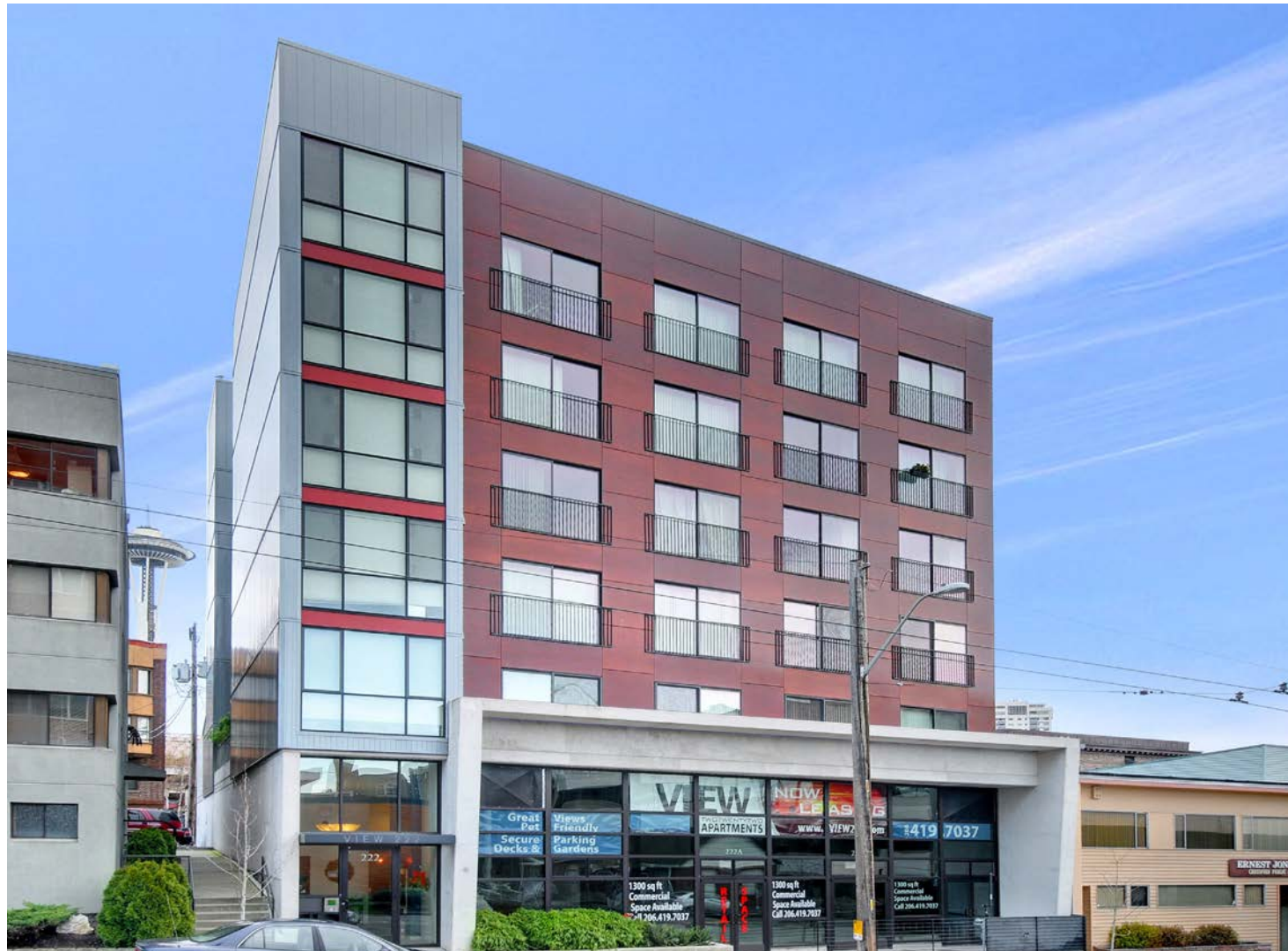
DAKOTA



CHELAN RESORT SUITES



TRIAD 12TH



VIEW 222



H2O APARTMENTS - LEED-H GOLD



BROADSTONE KOI - LEED-NC CERTIFIED



ARTHOUSE



APERTURE - BUILT GREEN 3-STAR TARGET

6800 GREENWOOD AVENUE N - DPD #3016207

DESIGN REVIEW RECOMMENDATION