



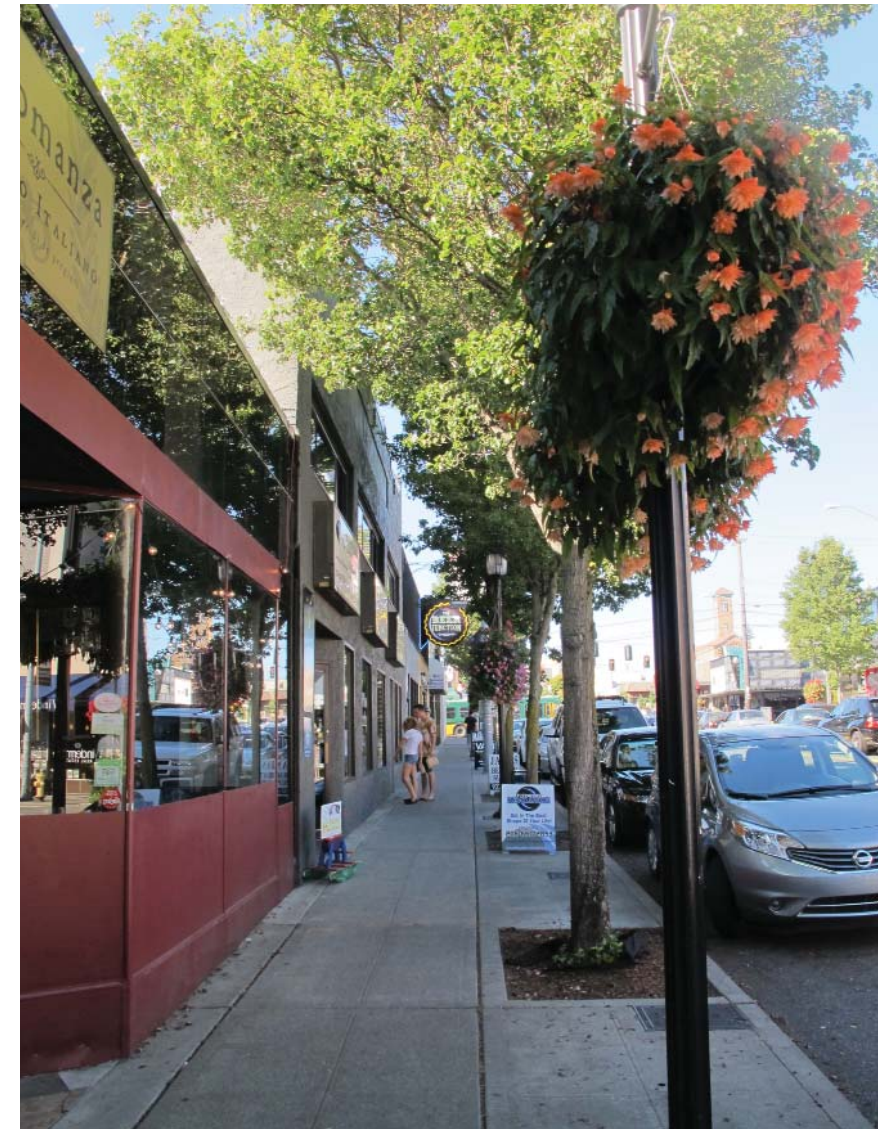
CAPELOUTO 42ND

4505 42ND AVENUE SW

DESIGN REVIEW
RECOMMENDATION II
DPD #3016195
FEBRUARY FIFTH, 2015

310 First Avenue S, Suite 4S,
Seattle, WA 98104
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DEVELOPMENT OBJECTIVES

The development objective is to construct a new multi-story building containing residential units, short term nontransient lodging units, commercial retail space, and below grade automobile parking. Due to the proximity of the project to the West Seattle Junction, the retail component will play an important role in the building design. One goal of the retail space is to create a strong connection to the adjacent sidewalks by including an extensive amount of window transparency; thus enlivening the sidewalk realm with retail activity. The residential units will vary in size including Studios, 1-Bedroom, and 2-Bedroom units. A large common rooftop outdoor terrace is planned for resident use that will provide expansive territorial views of the region. Parking is not required due to the location of the project site within the West Seattle Junction Hub Urban Village, but some parking is proposed on the below-grade level.

ZONING AND OVERLAY DESIGNATION

The parcel is zoned NC3-85 and is located within the West Seattle Junction Hub Urban Village. The area immediately surrounding the project site is zoned NC2-40 across SW Oregon Street to the north, NC3-65 across 42nd Avenue SW to the east, NC3-85 & NC3P-85 to the south, and NC3P-85 across the alley to the west. The site is also located within a Frequent Transit Corridor.

NEIGHBORING DEVELOPMENT

The project site is located within the West Seattle Junction Hub Urban Village, and only one block from the lively intersection of California Avenue SW and SW Alaska Street. The general character of this block and surrounding blocks is a mix of uses and development densities.

PROJECT VISION

Set within the established but growing West Seattle Junction, this project is intended to add to the vibrant commercial area at the Junction as well as provide a variety of housing types. The project intends to respect the neighborhood traditions while helping it grow and evolve. Street life along 42nd Ave and SW Oregon St will be enhanced through thoughtfully crafted retail spaces that activate the building's interface with the public right-of-way.

Commercial storefronts and retail entries will primarily be accessed off of Oregon Street, the pedestrian designated arterial. The retail entries are designed to address and draw from the activity along California Avenue, the nearby major commercial street. Secondary access will be provided from 42nd Avenue as well as the parking area to the south.

The residential entry and office are set at grade off of 42nd Avenue SW, directly across the street from the residential entry to Oregon 42. Treatments in the large Right-of-Way are designed to delineate the residential entry as well as provide an exterior amenity that enhances the entry to the building.

PROJECT PROGRAM

Number of Residential Units:	46 units
Number of Lodging Units	9 units
Area of Residential Use: (FAR)	30,764 sf
Area of Lodging Use (FAR):	6,243 sf
Area of Commercial Uses (FAR)	3,806 sf
Number of Parking Spaces:	15 spaces below grade
Floor Area Ratio:	6.00
Number of Stories:	7 stories
Total Gross Area:	50,402 sf
Total FAR Area:	41,403 sf

OWNER

Leon Capelouto
PO Box 448
Bothell, WA 98041

ARCHITECT

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DESIGN RECOMMENDATION

SITE INFORMATION

ZONING CODE SUMMARY AND MAP

ADDRESS: 4505 42ND AVENUE SW
 PARCEL #: 095200-6385
 ZONING: NC3-85
 OVERLAYS: West Seattle Junction Hub Urban Village (including Frequent Transit).
 LOT AREA: 6,903 Square Feet

23.47A.004 PERMITTED USES, TABLE A

Permitted outright:

- Residential
- Residential Uses Restricted to 20% at the Ground Floor
- Commercial Uses
- Lodging

SMC 23.47A.005 STREET LEVEL USES

In all neighborhood commercial zones, residential uses may occupy, in the aggregate, no more than 20 percent of the street-level street-facing facade within a zone that has a height limit of 85 feet or higher.

SMC 23.47A.008 STREET LEVEL DEVELOPMENT STANDARDS

Blank segments of the street-facing facade between 2 feet and 8 feet above the sidewalk may not exceed 20 feet in width.
 The total of all blank facade segments may not exceed 40 percent of the width of the facade of the structure along the street.
 Street-level street-facing facades shall be located within 10 feet of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided

SMC 23.47A.013 FLOOR AREA RATIO

Single-purpose: 4.5 (maximum any single use)
 Mixed-use: 6.0

SMC 23.47A.012 STRUCTURE HEIGHT

- Base height limit of 85’.
- An additional 4’ is allowed for open railings, planters, skylights, clerestories, greenhouses, solariums, parapets and firewalls.
- An additional 15’ is allowed for mechanical equipment if less than 20% of the total roof surface or 25% of the roof surface if stair, elevator penthouses, and screen mechanical equipment are included in the calculation.
- An additional 16’ is allowed for stair and elevator penthouses.

SMC 23.47A.014 SETBACK REQUIREMENTS

No setbacks required

SMC 23.47A.016 LANDSCAPING AND SCREENING STANDARDS

- Landscaping that achieves a Green Factor score of .30 or greater is required.
- Street trees are required when any development is proposed. Existing street trees shall be retained unless the Director of Transportation approves their removal. The Director, in consultation with the Director of Transportation, will determine the number, type and placement of street trees to be provided.

SMC 23.47A.024 AMENITY AREA

Amenity areas are required in an amount equal to 5 percent of the total gross floor area in residential uses. Amenity areas shall not be enclosed. Common amenity areas shall have a minimum horizontal dimension of 10 feet, and no common amenity area shall be less than 250 square feet in size. Private balconies and decks shall have a minimum area of 60 square feet, and no horizontal dimension shall be less than 6 feet.

23.47A.030 REQUIRED PARKING

No minimum parking requirement for residential uses in commercial within urban villages if the residential use is located within 1,320 feet of a frequent transit service.

SMC 23.47A.032 PARKING LOCATION AND ACCESS

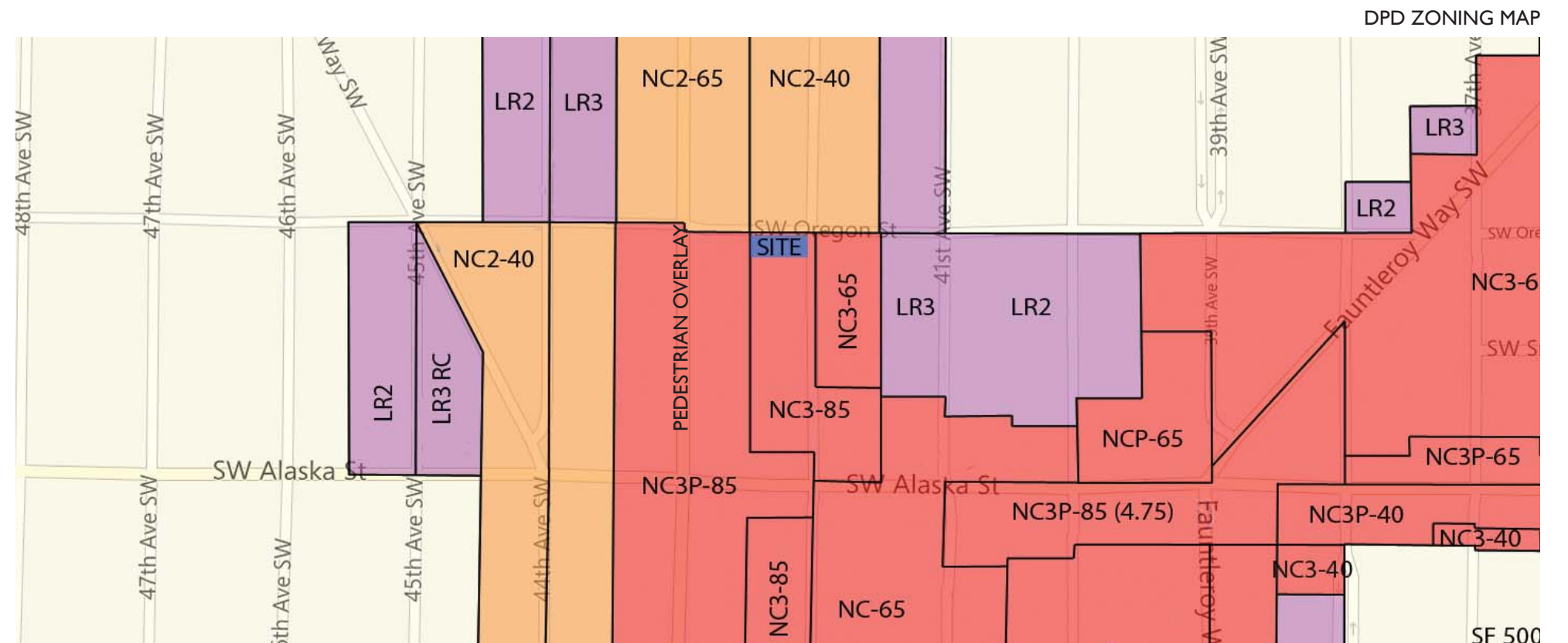
Access to parking shall be from the alley if the lot abuts an alley.

SMC 23.54.015 BICYCLE PARKING

Long term bicycle parking is required in equivalent of 1 space for every 4 units for residential use, 1 per 20 rooms of lodging, and 1 space for every 12,000 square feet of general commercial use.
 Short term bicycle parking is required: 2 per 20 rooms of lodging, 1 per 2000sf of general commercial space.

SMC 23.54.040 SOLID WASTE AND RECYCLABLES

Primary trash and recyclable room shall be a minimum of 375 square feet for a structure containing 26-50 residential units. Mixed use developments required additional area for nonresidential uses (50% of stand alone requirements); 41 square feet for nonresidential uses less than 5,000 square feet and 63 additional square feet for nonresidential uses more than 5,000 but less than 15,000 square feet.



SITE INFORMATION

NEIGHBORHOOD CONTEXT



① HOPE LUTHERAN CHURCH



② OREGON 42 APARTMENTS



③ 4730 CALIFORNIA (CONSTR)



④ CAPCO PLAZA & QFC



⑤ JEFFERSON SQUARE COMMERCIAL



⑥ SHADOWLANDS



⑦ EASY STREET RECORDS



⑧ CUPCAKE ROYALE



⑨ MIXED USE MURAL APARTMENTS



SITE

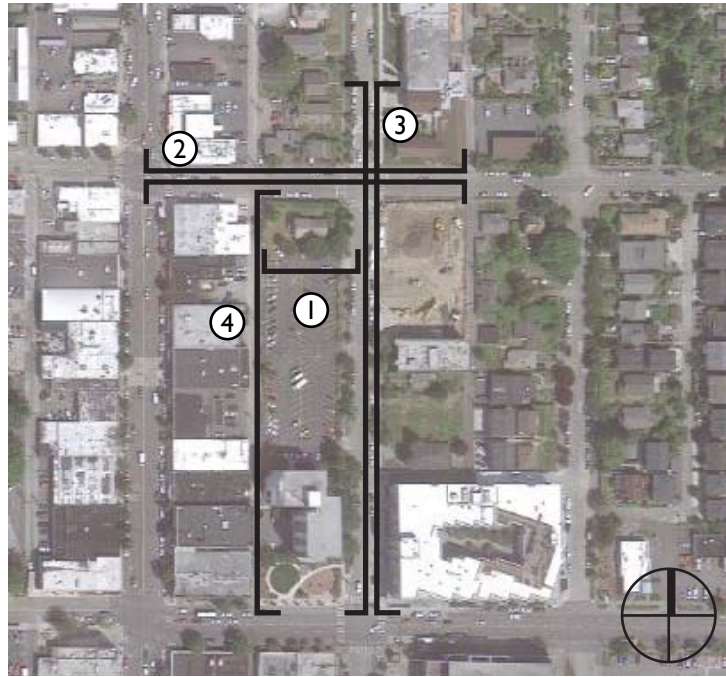


Capelouto 42nd - DPD #3016195

DESIGN RECOMMENDATION

SITE INFORMATION

STREETSCAPE PHOTO MONTAGES



PROJECT SITE



① ADJACENT PARKING LOT LOOKING NORTH

ACROSS FROM PROJECT SITE



② SW OREGON STREET LOOKING NORTH

PROJECT SITE



② SW OREGON STREET LOOKING SOUTH

SITE INFORMATION

STREETSCAPE PHOTO MONTAGES

ACROSS FROM PROJECT SITE



③ 42ND AVENUE SW LOOKING EAST

PROJECT SITE



③ 42ND AVENUE SW LOOKING WEST

PROJECT SITE



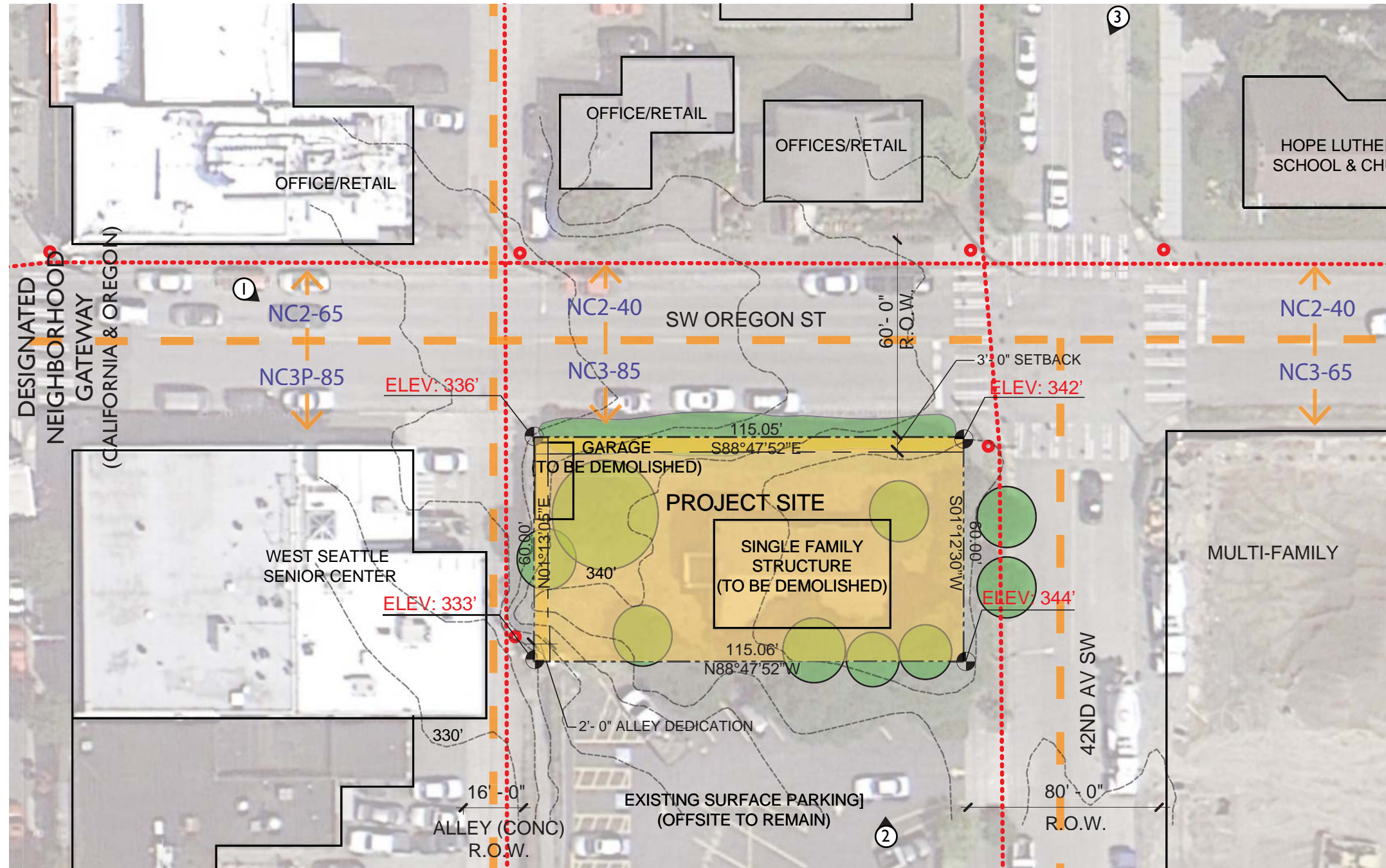
④ THE ALLEY LOOKING EAST



① ADJACENT PARKING LOT LOOKING SOUTH

SITE INFORMATION

EXISTING SITE PLAN



EXISTING SITE

The project site consists of a single parcel, located on the southwest corner of the SW Oregon Street and 42nd Avenue SW intersection. The site has a total area of 6,903 square feet. The project includes demolition of the single-family house that currently occupies the site. The site is bounded on the west by a 16'-0" wide alley (a 2' alley dedication is required in order to widen the alley). The site gradually slopes down from 42nd Avenue SW towards the alley. The site does not include any environmentally critical areas. Several trees currently stand on the site and an overgrown hedge in the right-of-way obscurs much of the site. None of the trees on the site or adjacent to the site are exceptional, per the tree survey; several are considered invasive and all the existing trees on site have been recommended for removal. Overhead high voltage power lines also run adjacent to the site both on 42nd Avenue SW and through the alley.



① PROJECT SITE LOOKING EAST UP SW OREGON ST



② PROJECT SITE LOOKING NORTH ACROSS PARKING LOT



③ PROJECT SITE LOOKING SOUTH ON 42 ND AVE SW

NOTE: BUILDING ENVELOPE IS APPROXIMATELY 85' IN ABOVE IMAGES

EARLY DESIGN GUIDANCE

PREFERRED EDG OPTION



1 MASSING AND CORNER TREATMENT

- Encourage the building to make more of a statement:
- Provide a stronger podium
- Provide a strong design at the corner
- Design the building with more height at the corner
- Consider a taller tower
- Treat the tower differently
- Continue the stair tower design as shown in option 3
- Continue the massing and glazing along south elevation
- Maintain the building cap
- Consider the massing of option 1 with a tower

2 PEDESTRIAN EXPERIENCE

- Encouraged more work on the overhangs
- Encouraged more work on the relationship between retail and sidewalk

3 SOUTH ELEVATION

- Encourage the applicant to design stair tower as shown in Option 3
- encourage massing and glazing on South elevation



GROUND LEVEL

DESIGN RECOMMENDATION

STREETVIEW LOOKING SOUTHWEST ON 42ND AVE SW

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RESPONSE TO EDG GUIDELINES

DESIGN GUIDELINE PRIORITIES

A. SITE PLANNING

A-2 Streetscape Compatibility

The siting of the building should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.

West Seattle Junction Supplement:

A pedestrian oriented streetscape is one of the most important characteristics to be achieved in new development in the Junction's mixed use areas. New development, particularly on SW Alaska, Genesee, Oregon, and Edmunds Streets, will set the precedent in establishing desirable siting and design characteristics in the right-of-way.

Considerations:

- Reduce the scale of the street wall with well-organized commercial and residential bays and entries, and reinforce this with placement of trees, drop lighting on buildings, benches and planters.
- Provide recessed entries and ground-related small spaces as appropriate breaks in the street wall.

Response

- The proposed project provides well organized bays and balconies on levels 3-6, and sets back the upper level to provide relief and rhythm along Oregon and 42nd. Lighting and canopies reinforce the break up of the streetscape in a pedestrian fashion that emphasize the interior uses and provide variety. Planting strips and trees are organized to accentuate the corner element and coordinate with the facade.
- Retail entries along Oregon are recessed and sized to reinforce the different entry types.

A-4 Human Activity

New development should be sited and designed to encourage human activity on the street.

West Seattle Junction Supplement:

An active and interesting sidewalk engages pedestrians through effective transitions between the public and private realm.

Considerations:

- Particularly in the California Avenue Commercial Core, proposed development is encouraged to set back from the front property line to allow for more public space that enhances the pedestrian environment. When such a setback is not appropriate or feasible, consider maximizing street level open space with recessed entries and commercial display windows that are open and inviting.

Response

- The project proposes to utilize the surprisingly large ROW area facing 42nd Avenue SW. The current sidewalk (approximately 6' wide) will be widened significantly, enough to include planting areas and a changes in paving to define potential areas for future tables and chairs. The total distance from curb to face of building is approximately 24'.
- Both ground floor facades maximize commercial glazing and are intended to facilitate active retail/commercial experiences.

- The pedestrian experience along Oregon will change dramatically from a narrow sidewalk adjacent to the street, to a sidewalk similar to those on California Street and in keeping with conditions as the street continues East. Street trees and planters will buffer pedestrians from traffic and the sidewalk will extend to the building face, three feet beyond the property line.



SIDEWALK AT OREGON 42

SIDEWALK ALONG CALIFORNIA

A-10 Corner Lots

Building on corner lots should be oriented to the corner and public street fronts. Parking and automobile access should be located away from corners.

West Seattle Junction Supplement:

Pedestrian activities are concentrated at street corners. These are places of convergence, where people wait to cross and are most likely to converse with others. New development on corner lots should take advantage of this condition, adding interest to the street while providing clear space for movement. New buildings should reinforce street corners, while enhancing the pedestrian environment.

Public space at the corner, whether open or enclosed, should be scaled in a manner that allows for pedestrian flow and encourages social interaction. To achieve a human scale, these spaces should be well defined and integrated into the overall design of the building

Considerations:

- Provide seating
- Incorporate art that engages people
- Setting back corner entries to facilitate pedestrian flow and allow good visibility at the intersection

Response

- The proposed project engages the corner with a larger canopy cover and utilizes the large sidewalk area as an open space. The corner of the building has the largest windows and is meant to be the focal point of the building.

B. HEIGHT, BULK AND SCALE

B-1 Height, Bulk, and Scale Compatibility

Projects should be compatible with the scale of development anticipated by the applicable Land Use Policies for the surrounding area and should be sited and designed to provide a sensitive transition to less intensive zones nearby. Projects on zone edges should be developed in a manner that creates a step in perceived height, bulk, and scale between anticipated development potential of the adjacent zones.

West Seattle Junction Supplement:

Current zoning in the Junction has created abrupt edges in some areas between intensive, mixed-use development potential and less-intensive, multifamily development potential. In addition, the Code-complying building envelope of NC-65' (and higher) zoning designations permitted within the Commercial Core would result in development that exceeds the scale of existing commercial/mixed-use development. More refined transitions in height, bulk and scale—in terms of relationship to surrounding context and within the proposed structure itself—must be considered.

Response

- The project provides a more sensitive transition between the NC-65 and the NC-85 zones by limiting itself to 70'. Future adjacent development will likely exceed the height of the building.

C. ARCHITECTURAL ELEMENTS AND MATERIALS

C-2 Architectural Concept and Consistency

Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept. Buildings should exhibit form and features identifying the functions within the building. In general, the roofline or top of the structure should be clearly distinguished from its facade walls.

West Seattle Junction Supplement:

New multi-story developments should employ methods that integrate the building's upper and lower levels. The levels of the building should function as a composition – not necessarily requiring the top and bottom to be identical, but rather extending or repeating elements throughout the facade.

Response

- The facades have been treated differently on the four sides of the building. Each facade intends to make connections both horizontally and vertically, often reinforcing the various uses through alignment and material. Although the size of the openings vary, they are intended to provide a harmonious pattern in conjunction with finish materials, setbacks, and color.

RESPONSE TO GUIDELINES

DESIGN GUIDELINE PRIORITIES

C-3 Human Scale

The design of new buildings should incorporate architectural features, elements and details to achieve a good human scale.

West Seattle Junction Supplement:

Facades should contain elements that enhance pedestrian comfort and orientation while presenting features with visual interest that invite activity. Overhead weather protection should be functional and appropriately scaled, as defined by the height and depth of the weather protection. It should be viewed as an architectural amenity, and therefore contribute positively to the design of the building with appropriate proportions and character.

Response

- Overhead weather protection is provided above both sidewalks approximately 5' deep. This will allow space for street trees to grow and still provide pedestrians with protection from rain and lighting at night. The canopies are separated from the overhanging bays and decks thus providing a unified experience along the street as the entrances and storefronts respond to interior changes.



D-10 Commercial Lighting

Appropriate levels of lighting should be provided in order to promote visual interest and a sense of security for people in commercial districts during evening hours. Lighting may be provided by incorporation into the building façade, the underside of overhead weather protection, on and around street furniture, in merchandising display windows, in landscaped areas, and/or on signage.

Response

- Wall mounted lights are planned on each brick 'pilaster' along the sidewalk to provide a consistent, attractive level of lighting that promotes safety and walkability.



D. PEDESTRIAN ENVIRONMENT

D-1 Pedestrian Open Spaces and Entrances

Convenient and attractive access to the building's entry should be provided. To ensure comfort and security, paths and entry areas should be sufficiently lighted and entry areas should be protected from the weather. Opportunities for creating lively, pedestrian-oriented open space should be considered.

Response

- Recessed entries have been located along the sidewalk at appropriate locations and are to be well lit.

D-2 Blank Walls

Buildings should avoid large blank walls facing the street, especially near sidewalks. Where blank walls are unavoidable they should receive design treatment to increase pedestrian comfort and interest.

Response

- There are no blank walls facing the sidewalk.

D-9 Commercial Signage

Signs should add interest to the street front environment and should be appropriate for the scale and character desired in the area.

Response

- Although the number and exact appearance of the signs is as of yet unknown, small blade signs hanging from the canopy will be encouraged.

D-11 Commercial Transparency

Commercial storefronts should be transparent, allowing for a direct visual connection between pedestrians on the sidewalk and the activities occurring on the interior of a building. Blank walls should be avoided.

Response

- The ground floor of the building, along the sidewalk, is intended to maximize the area of glazing to promote visual connections from the street into the retail spaces. Glazing has been designed to avoid divisions that interfere with views out of the space and the natural grade of the sidewalk allows a variety of perspectives into retail spaces. There are no blank walls facing the sidewalk.

E. LANDSCAPING

E-2 Landscaping to Enhance the Building and/or Site

Landscaping, including living plant material, special pavements, trellises, screen walls, planters, site furniture, and similar features should be appropriately incorporated into the design to enhance the project.

Response

- The project incorporates a variety of planting areas suited to their specific place on the site. Street tree planting areas incorporate low growing plantings allowing easy connections to the street. The area along 42nd utilizes paving types and planters with larger plants to create a more private space protected from the parking lot and Oregon street that will serve well for small tables and chairs, if a cafe or restaurant leases the corner space.



RESPONSE TO GUIDANCE

RESPONSE TO EDG BOARD RECOMMENDATIONS

1. MASSING AND CORNER TREATMENT

a The board preferred the one story podium.

The proposed design enhances the distinction between the one story retail podium and the building above through the use of changing wall planes, a solid canopy, and a change in the relationship between glazed and solid.

b The board encouraged the design to make more of a statement, particularly at the corner.

The proposed design utilizes color and form to give the building identity. The base of the building relates to the pedestrian and retail needs; the primary corner is tall masonry, with large glazed areas; secondary corners are metal, the middle of the building adds light and movement with bay window type modulation, and the top floors are set back in a warm orange to allow the primary corner to appear taller. Overall, the building projects a residential feel compatible with both the newer development and the existing fabric of the neighborhood.

c Design the building with more height at the corner

The tower at the corner maximizes the allowable building height. Setting back and changing the color of the upper levels allows the corner tower to appear taller.

d Tower facades should be treated differently along SW 42nd Street and SW Oregon Street.

The design has evolved such that the entirety of each street facade responds to that street, and the tower is the hinge that binds them. The treatment of the tower is consistent along both street facades, addressing the corner as a singular element.

e The board encouraged the applicant to design the stair tower as shown in Option 3 and continue the massing and glazing along the south elevation.

The south facade of the building has been a challenge. Although the general concept of the south facade remains the same, the configuration has changed. The orange metal cladding is used to accentuate the inset at the stair tower. Major portions of the building have been pulled back to allow for windows.

f Maintain the Building cap as shown in option 3.

The building cap remains as shown, with the guard rail of the rooftop amenity area set back from the edge quite some distance. It is the intent of the design that the rooftop amenity not be seen from the street.

g Consider a higher tower.

The corner tower has been designed to compliment the building.

h Consider the massing and design of Option 1 with a taller tower.

The proposed design takes the active bay design of Option 1 and melds it with the taller tower of Option 3.

2. PEDESTRIAN EXPERIENCE

a The proposed structural building overhangs are suitable if the sidewalk is 17'.

The proposed design eliminates structural building overhangs, pulling the building back to the property line. The distance between the first floor and the curb is approximately 13'. The proposed walking area on Oregon is approximately 7' next to an additional 5' of planter. Comparable sidewalks on California provide between 4' and 6' of walking area. Street trees, benches, and other landscaping features occupy up to an additional 5' of width. The overhead pedestrian protection along 42nd is shown at a minimum of 8' above the sidewalk and extends over the sidewalk.

b Encouraged more work on overhangs and the relationship of the retail space to the streets.

The proposed design includes broad expanses of glazing, inset entries, and flexible retail space. The canopies along 42nd provide pedestrian protection, light at night, and create a familiar, comfortable setting for window shoppers.

c Provide information about signage and lighting.

A lighting plan is included in this packet. Overall, the lighting is meant to be subdued. Down lights and wall mounted sconces under the canopy will provide a sense of safety at night. Lighting will punctuate entries. Lighting along the alley will be directed downward to provide security.

Signage is intended to be directed at the pedestrian. An integrated building name will be located near the residential entry.

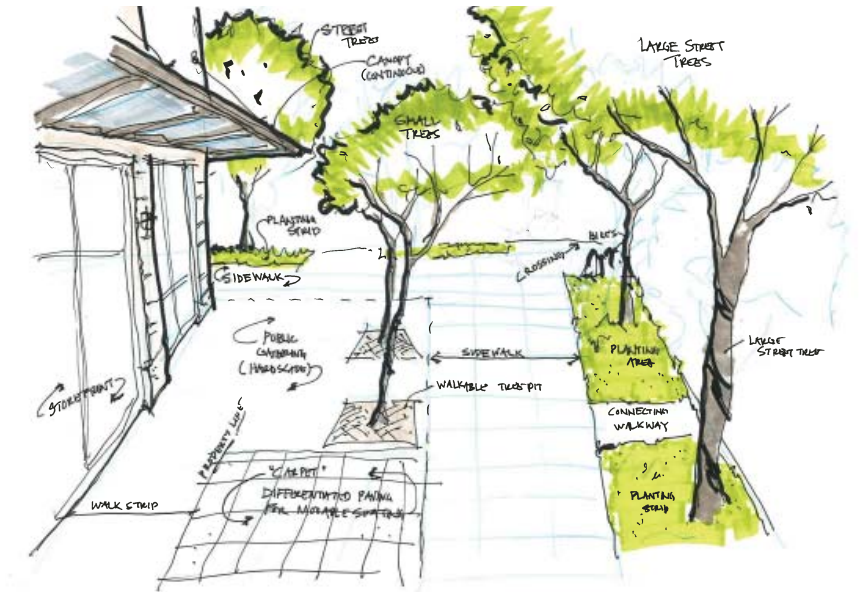
3. SOUTH ELEVATION

a The board encouraged the applicant to design the stair tower as shown in Option 3 continuing the massing and glazing along the south elevation.

As previously noted, the south facade of the building has been a challenge. Materials and patterns from the primary facades wrap the corners and transition to a more utilitarian material. A set back from the property line allows for windows in the upper levels.

DEPARTURES

No departures are sought.



Concept Sketch of 42nd Ave SW Corner Pedestrian Experience

- Define corner by creating edges with planters and the building edge
- Align existing sidewalk with walkway as separate from seating area
- Continue retail experience along sidewalk
- Provide smaller scale trees between building and sidewalk
- Uplight trees for ambient lighting and to create a sense of place
- Provide brick facade to ground level for continuity
- differentiate the seating area with a "carpet" of different paving

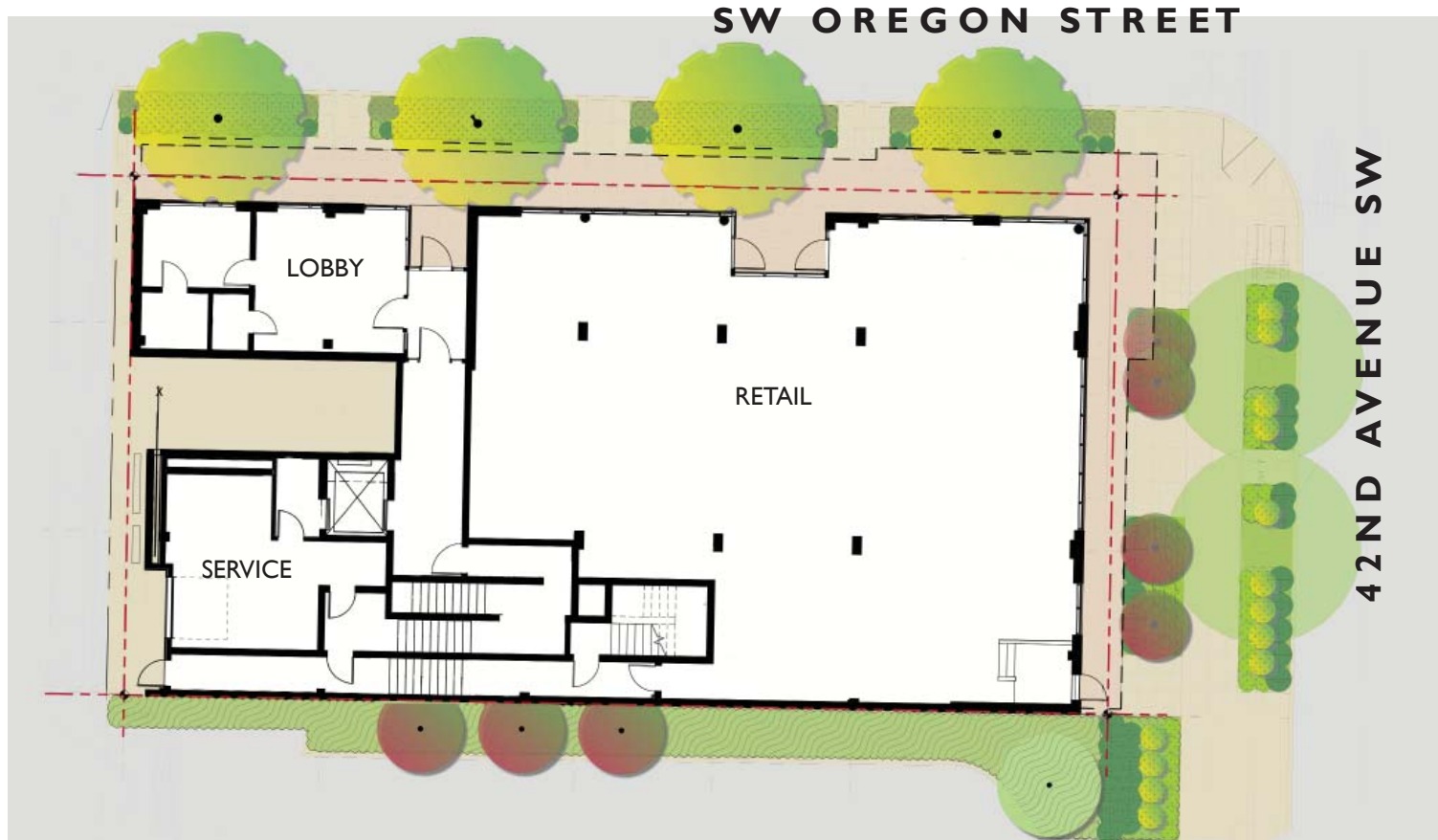


Concept Sketch of 42nd Ave SW Retail

- Separate retail and sidewalk with plantings and access
- Minimize apparnt grade separation with low sill (30" on inside)
- Provide access to street level

RECOMMENDATION

DECEMBER 4, 2015



Capelouto 42nd - DPD #3016195

DESIGN RECOMMENDATION

INITIAL RECOMMENDATION

DECEMBER 4 BOARD COMMENTS

1. DESIGN CONCEPT: COMPARING THE MASSING OF THE PREFERRED OPTION AT THE 2ND EDG AND THE CURRENT DESIGN, THE BOARD NOTED THE BUILDING FACADES ARE DISJOINTED AND GAVE GUIDANCE TO SIMPLIFY THE ELEVATIONS. THEY EXPRESSED THAT THE PODIUM WORKS WELL ON SW OREGON ST AND THE ALLEY, BUT IS TOO SQUAT ON THE 42ND AVE SW, EAST FACADE.

- Simplify the north facade; there are too many elements.
- Simplify the east facade and design with vertical articulation.
- There was concern about the appearance of the lighting sconces at the upper levels and that they could create 'hot spots'. Light the facade from below or above.
- Articulate the windows; either project or recess the window frames.

2 DESIGN OF THE STREET-LEVEL ALONG 42ND AVE SW: THE BOARD WAS CONCERNED THAT THE GENEROUS WIDTH FROM THE CURB TO THE BUILDING FACE WOULD NOT BE ACTIVATED WITH THE PROPOSED DESIGN.

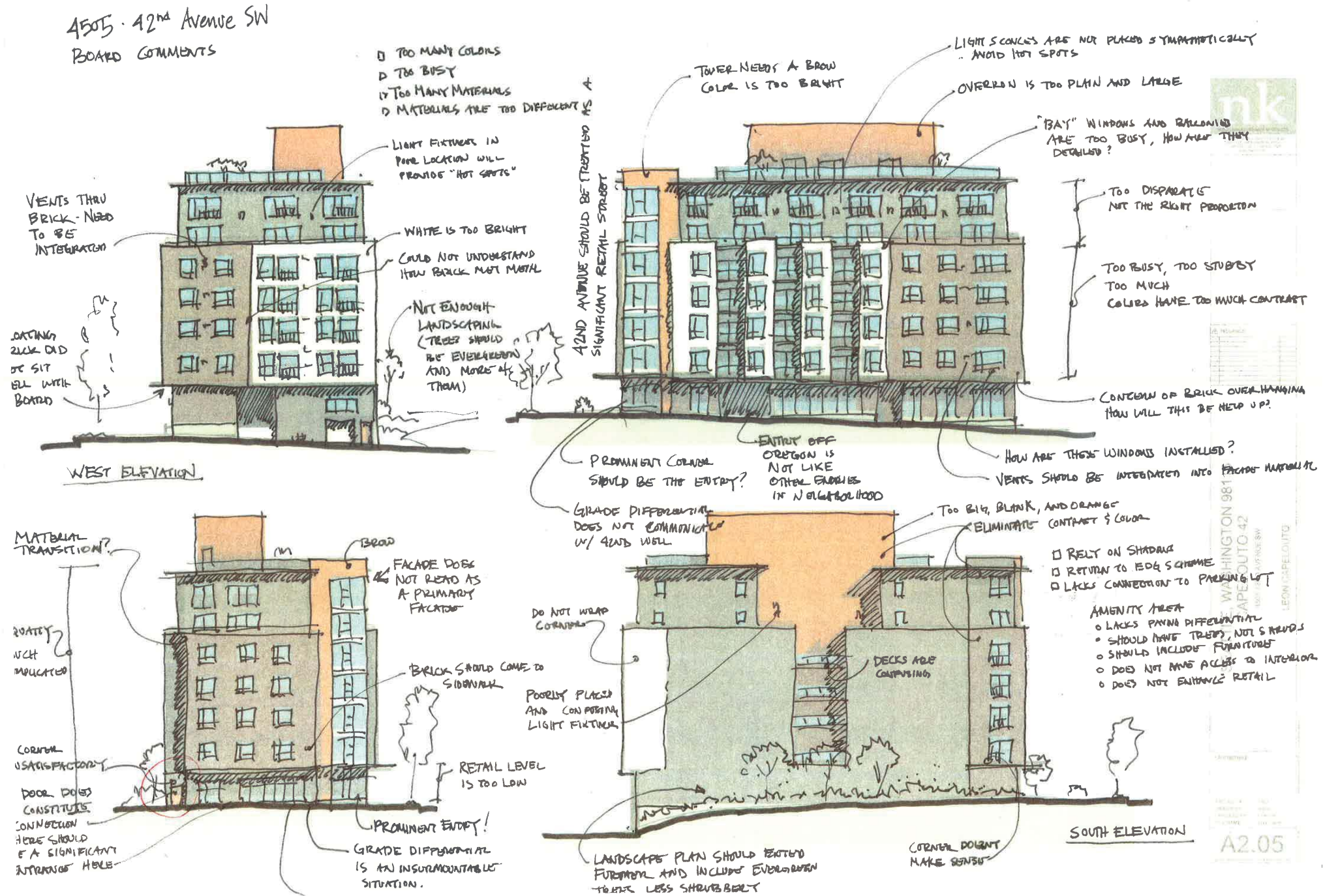
- Provide a main corner entry or a main entry on 42nd Ave SW to serve the plaza. Design for an additional door further south on the elevation.
- Design a sitting area for the plaza to show how it would work.
- Change the paving at the property line.

3. MATERIALS AND COLOR: THE BOARD EXPRESSED THAT THE DETAILING OF MATERIALS WILL BE CRITICAL IN ACHIEVING A SUCCESSFUL DESIGN AND GAVE THE FOLLOWING GUIDANCE:

- Use brick vents in the brick portions of the facade.
- choose a color palette that will provide depth; the current colors read too flat
- add depth to the brick portions of the facade with detailing.
- provide building lighting using fixtures other than the presented wall sconces.

4 SOUTH ELEVATION: THE BOARD NOTED THAT DEPTH HAD BEEN ADDED TO THE ELEVATION BY SHIFTING THE MASSING. HOWEVER, THEY WERE NOT SUPPORTIVE OF THE CHOICE OF MATERIALS AND COLOR ON THE SOUTH FACADE.

- Use timeless materials and mute the color palette, especially the bright orange.
- Vary the materials and colors of the different planes.
- Consider a brow near the roof line.
- Change the color of the white portion of the facade.
- Provide more trees along the south facade on the adjacent property. The trees should be evergreen species to provide year round foliage.

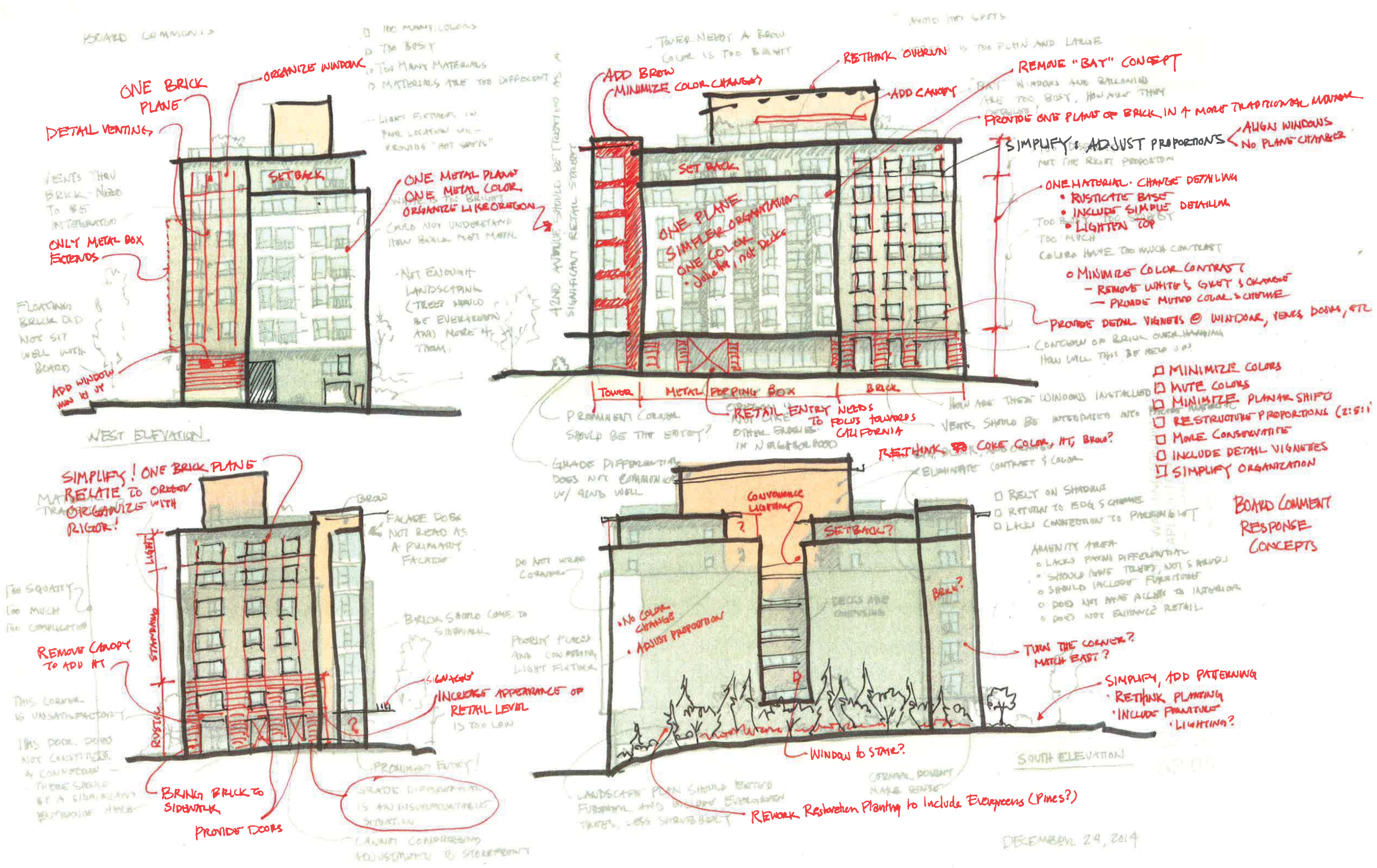


5. AT THE SECOND RECOMMENDATION MEETING PROVIDE THE FOLLOWING:

- Show detailing of how the materials will transition
- Show detailing of the window in relationship to the exterior wall plane
- Provide a plan of the plaza at 42nd Ave SW showing materials, scoring pattern, seating layout and landscaping.
- Provide commercial signage information

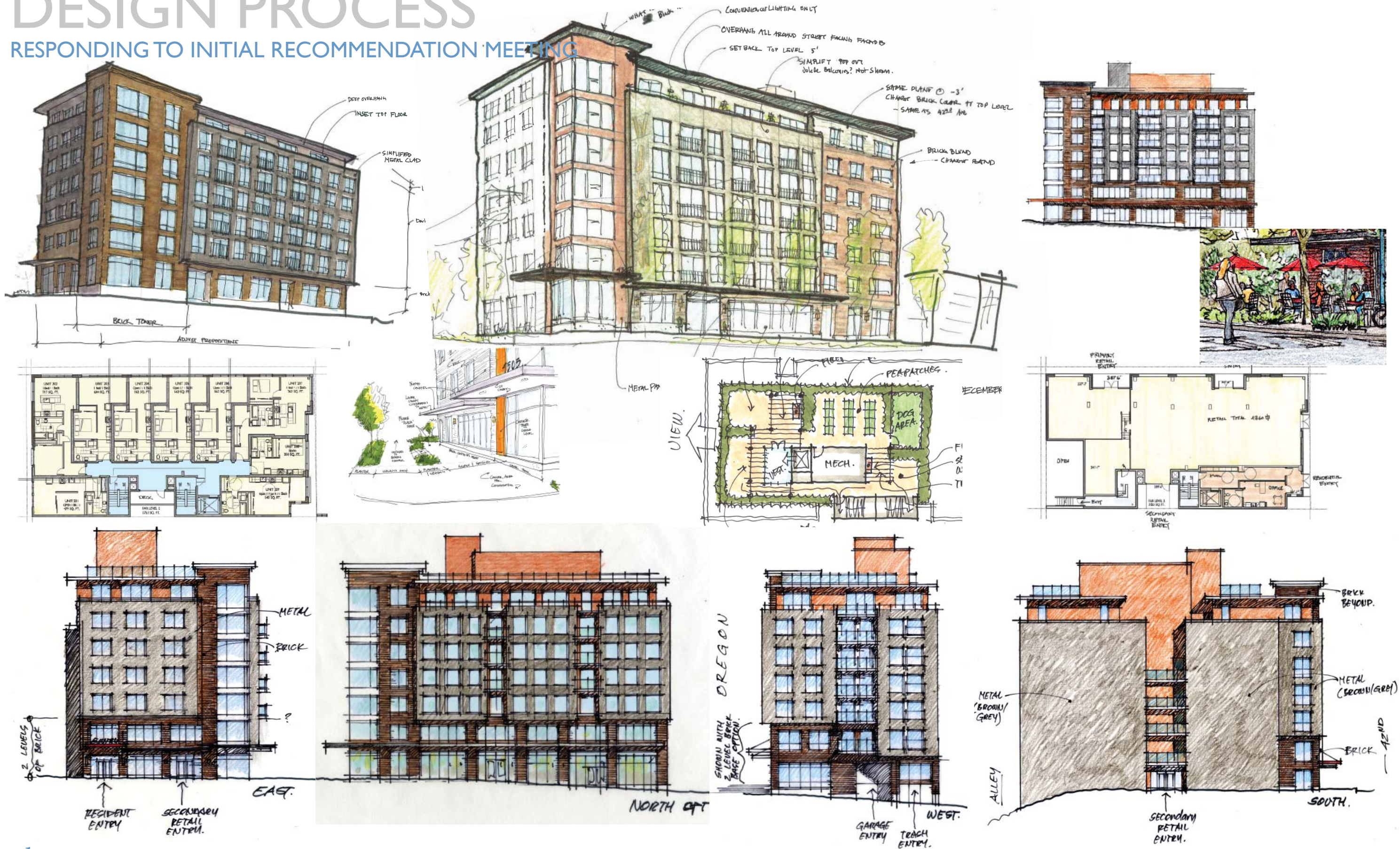
RESPONSE TO RECOMMENDATIONS

RESPONSE TO BOARD RECOMMENDATIONS



DESIGN PROCESS

RESPONDING TO INITIAL RECOMMENDATION MEETING



DESIGN PROPOSAL

CORNER RENDERING



Capelouto 42nd - DPD #3016195

CURRENT CONDITIONS

DESIGN RECOMMENDATION

COMPOSITE SITE PLAN

4505 - 42ND AVENUE SW

SW OREGON STREET



BUILDING ELEVATIONS

EAST ELEVATION



Glass Roof Railing



Metal Panel System: Cayenne



Metal Panel System: Tuscan



Brick on Concrete



Brick Corbelling and Metal Flashing



Vinyl Windows: White



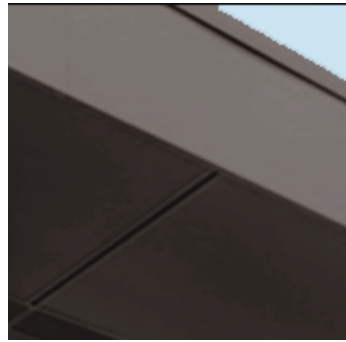
Sconce Lighting



Metal Storefront: Clear Anodized

BUILDING ELEVATIONS

NORTH ELEVATION



Metal Soffit : Tuscan



Brick: Forest Blend



Canopy: Black Frame



Metal Storefront: Clear Anodized



Sconce lighting



Metal Panel System: Cayenne



Vinyl Windows: White



Brick over Concrete (rusticated brickwork)

BUILDING ELEVATIONS

WEST ELEVATION



Glass Roof Railing



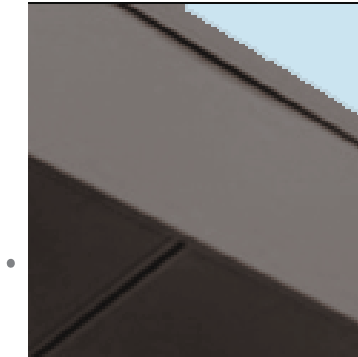
Glass Juliet Balcony



Vinyl Windows: White



Brick over Concrete



Metal Panel System: Tuscan (& Coffee)



Metal Panel System: Cayenne



Shore Pine



Sectional Grille: Black

BUILDING ELEVATIONS

SOUTH ELEVATION



Metal Panel System: Cayenne



Glass Roof Railing



Metal Panel System: Tuscan



Light Fixture



DESIGN PROPOSAL

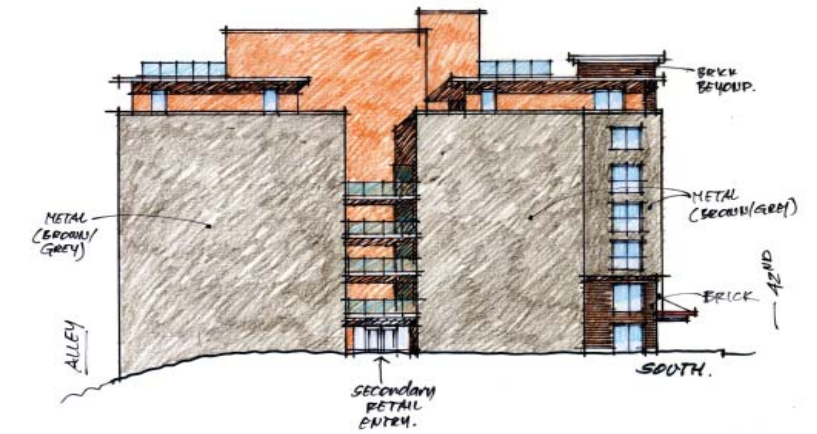
SOUTH FACADE



Alternate panel coloring and patterning on the south facade.



Looking at the southwest corner of the building



Concept Sketch



DESIGN RECOMMENDATION

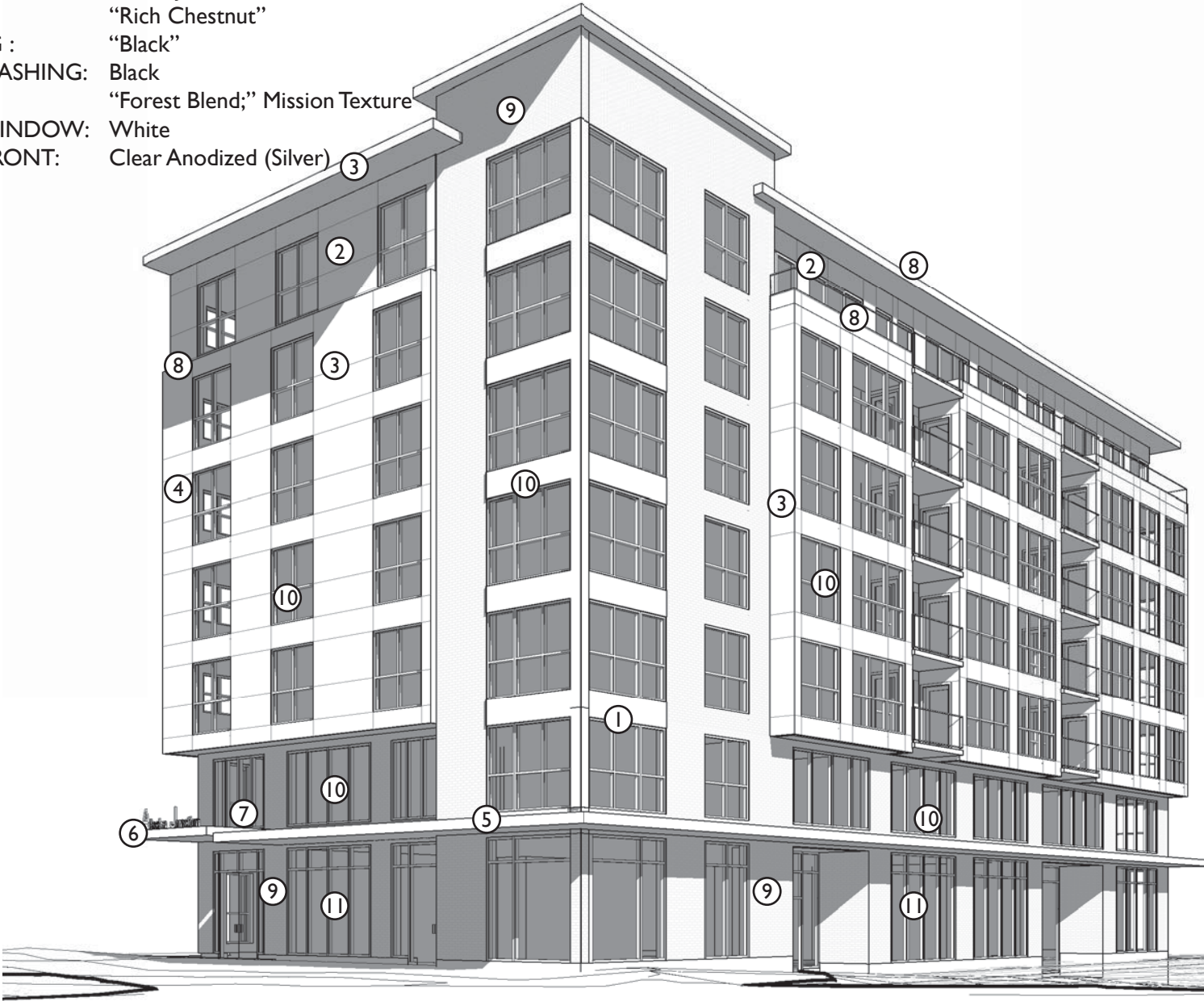
DESIGN PROPOSAL

MATERIALS BOARD



Proposed Materials

- 1. METAL SIDING: "White"
- 2. METAL SIDING: "Cayenne"
- 3. METAL SIDING: "Tuscan"
- 4. METAL SIDING: "Coffee"
- 5. PAINT : "Black Jack"
- 6. PAINT : "Rich Chestnut"
- 7. ROOFING : "Black"
- 8. METAL FLASHING: Black
- 9. BRICK : "Forest Blend;" Mission Texture
- 10. VINYL WINDOW: White
- 11. STOREFRONT: Clear Anodized (Silver)



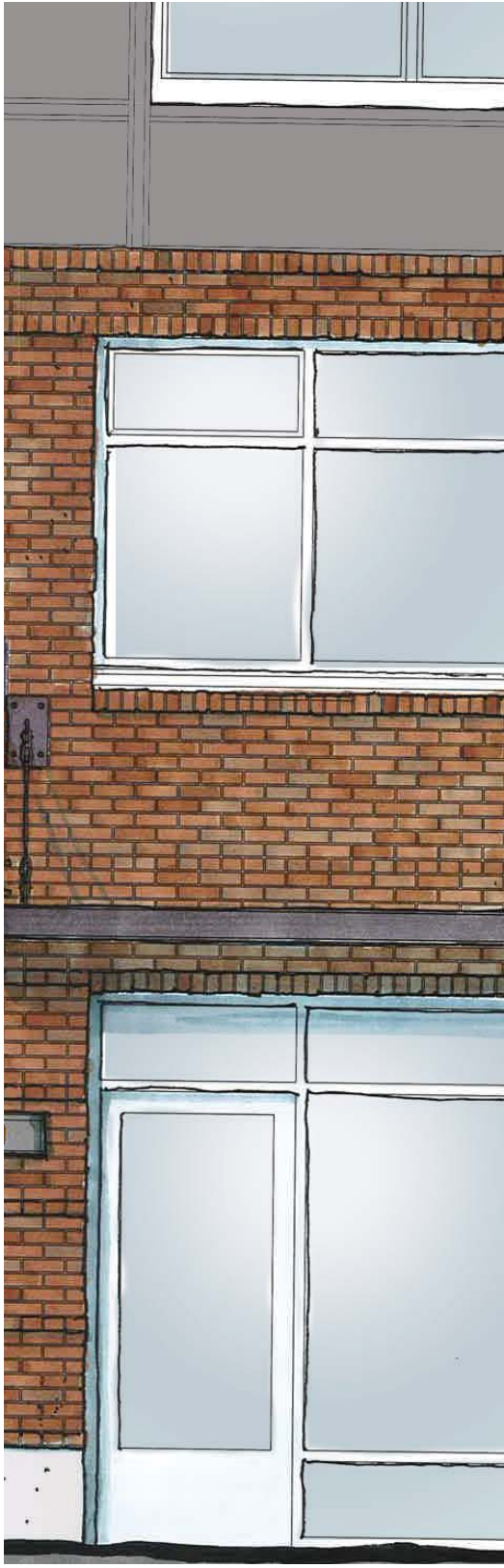
DESIGN FEATURES

MATERIAL DETAILS



CORNER OF OREGON & 42ND

Capelouto 42nd - DPD #3016195



BUILDING BASE

- VINYL WINDOW
- METAL PANELS LEVEL 3 AND ABOVE
- ROWLOCK COURSE AT UNDERSIDE OF PROJECTION
- RUNNING BOND FIELD
- ROWLOCK COURSE ABOVE WINDOWS
- VINYL WINDOWS INSET INTO BRICK FIELD
- VENTING WILL BE LOCATED INTEGRATED INTO SOFFITS FOR SECOND FLOOR APARTMENTS
- ANGLED ROWLOCK BELOW WINDOWS
- STEEL PLATES AND STRUTS FOR CANOPY IN BRICK PLANE
- RUNNING BOND FEILD
- HEADER COURSE AT FIRST FLOOR
- METAL FRAME CANOPY WITH METAL ROOF
- ROWLOCK COURSE ABOVE STOREFRONT
- UP/DOWN LIGHT FIXTURE ON BRICK PILASTER
- STOREFRONT SYSTEM
- CONCRETE BASE UNDER BRICK

DESIGN RECOMMENDATION

DESIGN FEATURES

MATERIAL DETAILS



WINDOW AT BRICK

RUNNING BOND BRICK FIELD (FOREST BLEND)

ROWLOCK COURSE

PROJECTS 1/2" ABOVE WINDOW

VINYL WINDOW (WINDOW SIZE VARIES)

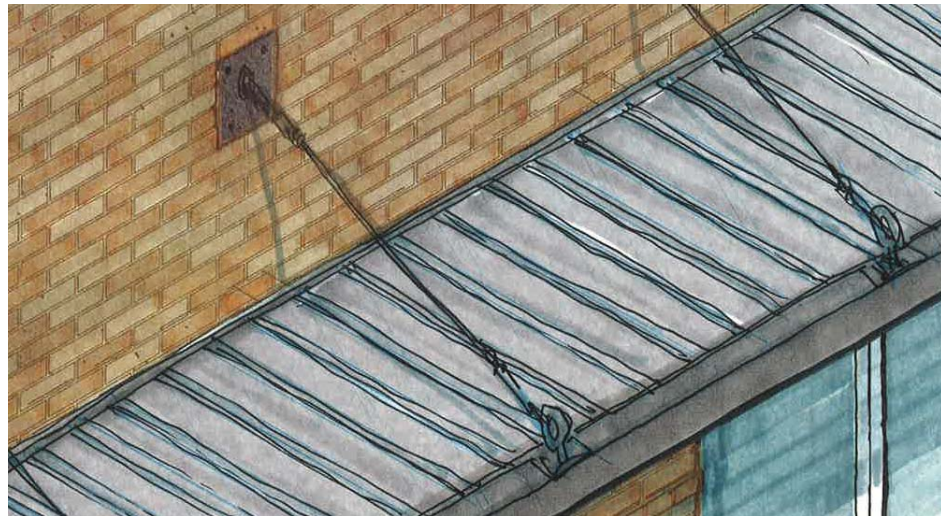
INSET 3" FROM FACE OF BRICK

ANGLED ROWLOCK COURSE

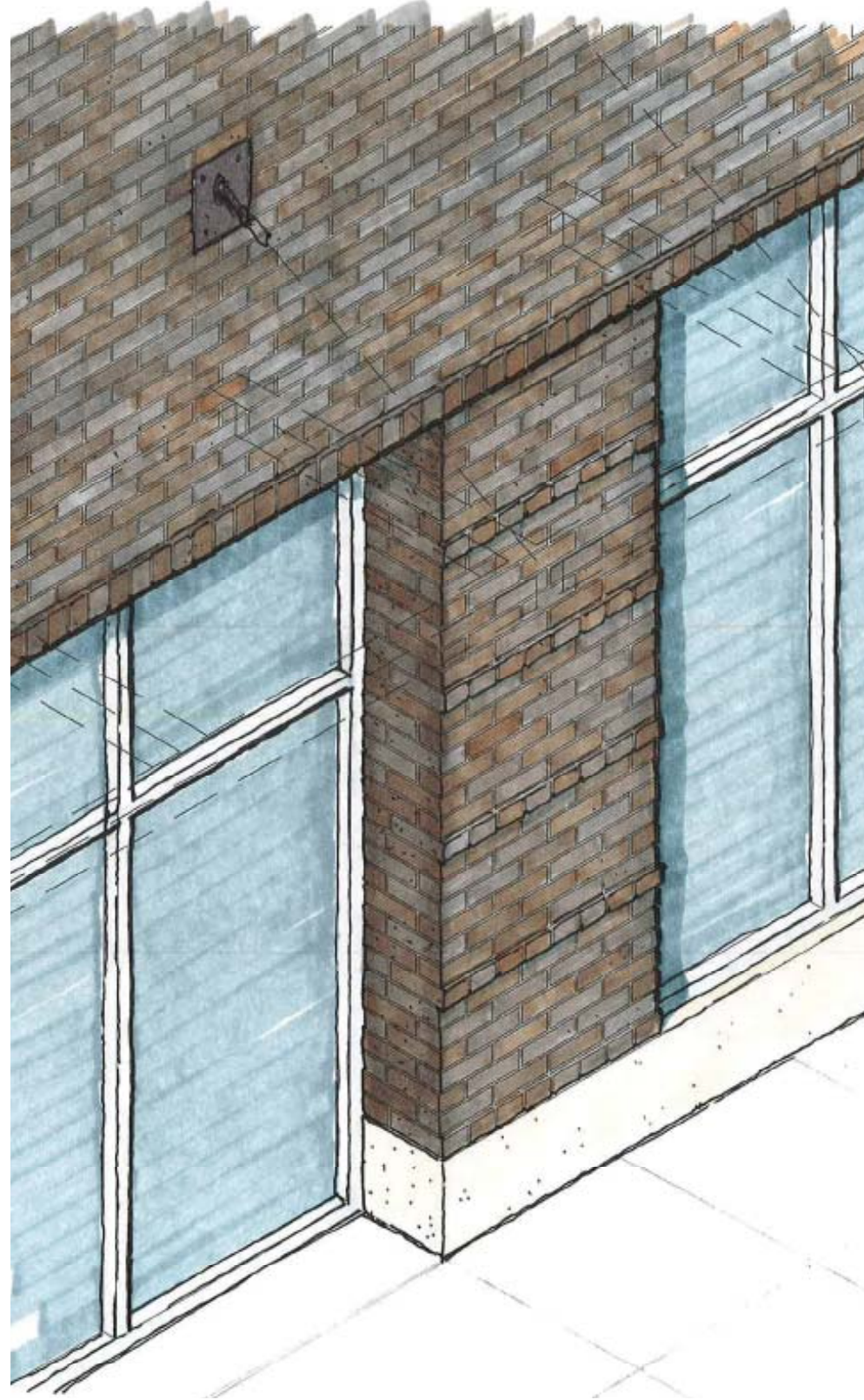
PROJECTS 1" BELOW WINDOW

RUNNING BOND FIELD

CANOPY SHOWN DASHED
STRUTS ATTACH TO METAL PLATE
IN PLANE WITH BRICK FIELD



CANOPY STRUT TO BRICK



RUNNING BOND BRICK FIELD (FOREST BLEND)

ROWLOCK COURSE

PROJECTS 1/2" ABOVE STOREFRONT

PROJECTS 1/2" ABOVE OPENINGS

BRICK AT FIRST FLOOR

RUSTICATED PILASTER - EVERY SEVENTH COURSE+/-

HEADER COURSE PROJECTS OUT 1/2" FROM FIELD

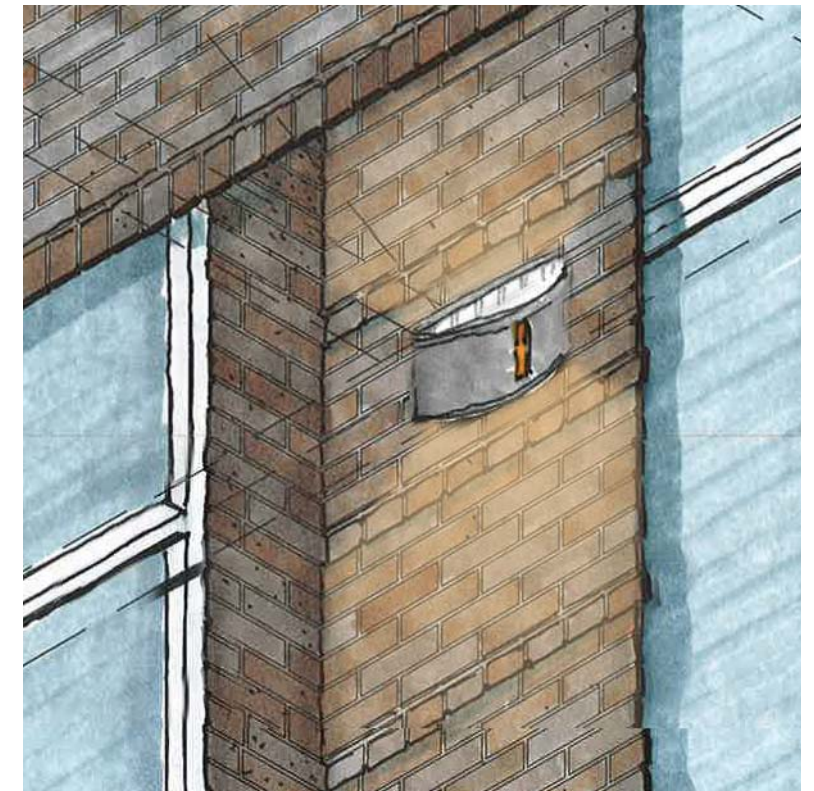
STOREFRONT SYSTEM (DIMENSIONS VARY

INSET 3" FROM FACE OF BRICK, TYPICAL

INSET AT DOORWAYS VARIES

CONCRETE BASE (HEIGHT VARIES)

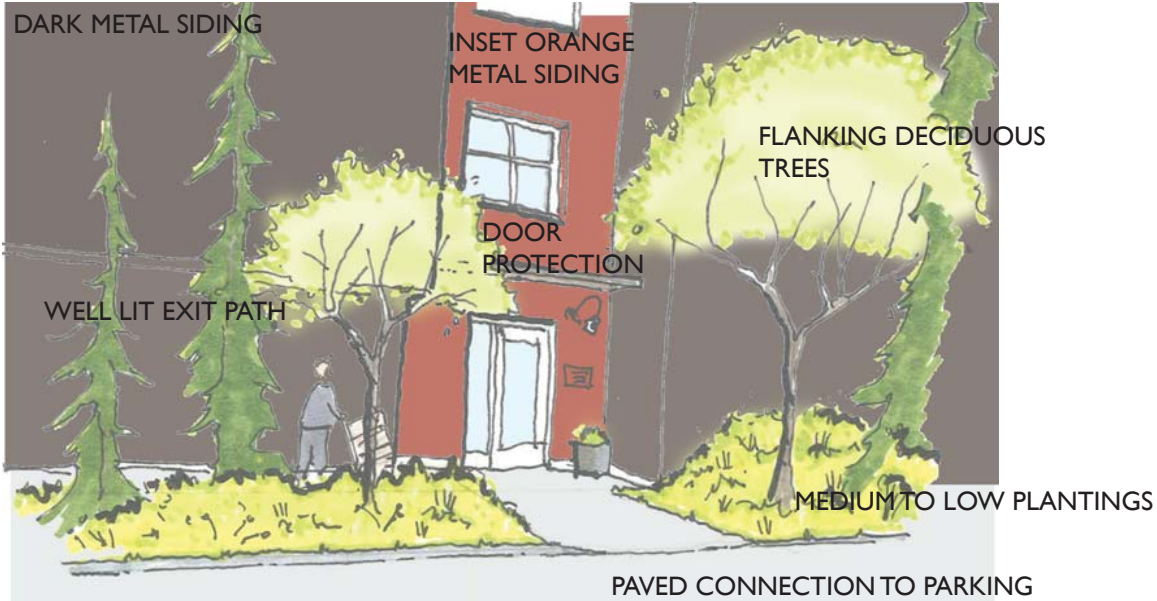
CONCRETE SIDEWALK (SEE PLAN FOR PATTERN)



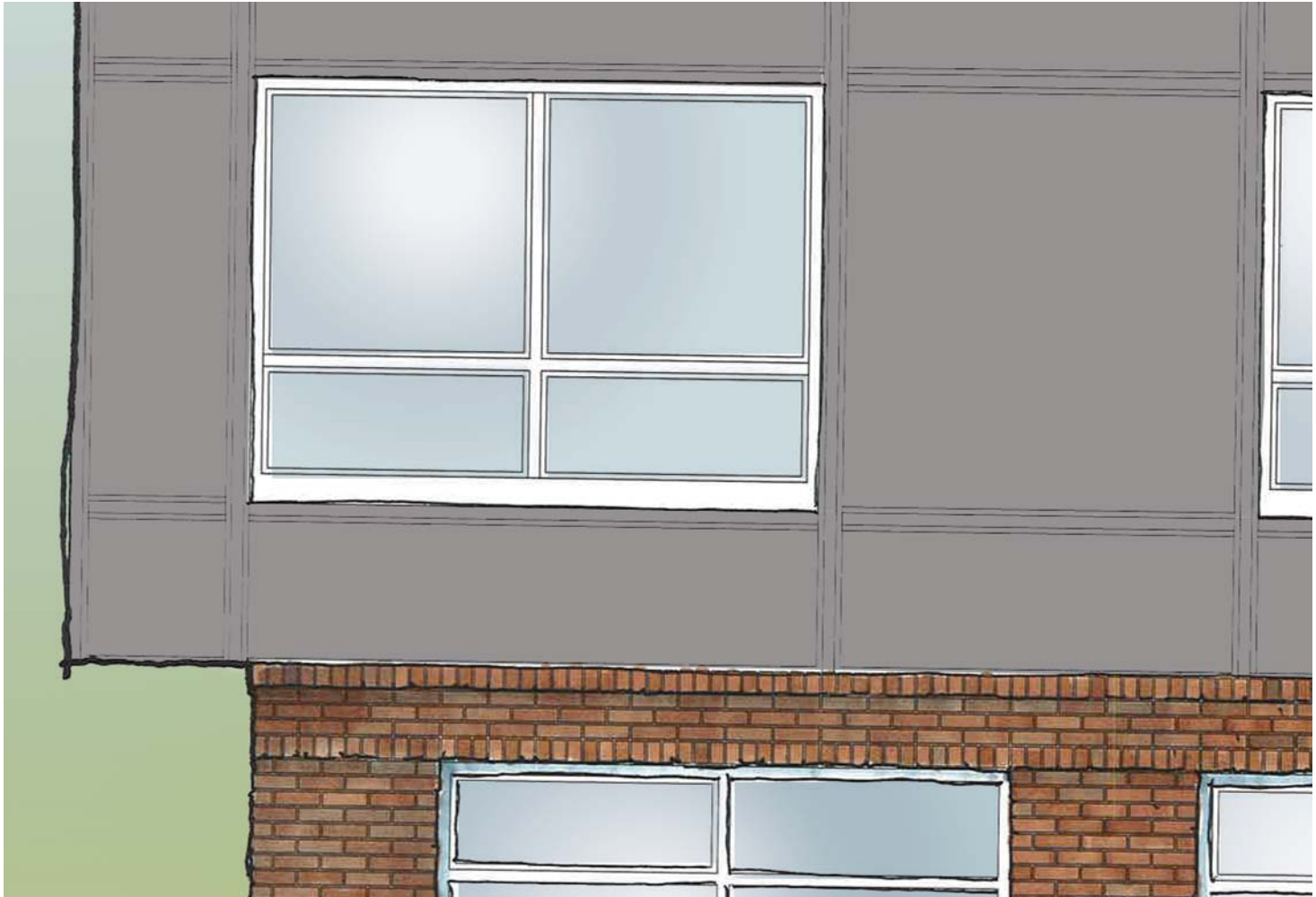
LIGHT FIXTURE AT BRICK
(CANOPY SHOWN DASHED)

DESIGN FEATURES

MATERIAL DETAILS



SERVICE ENTRANCE FROM PARKING LOT



METAL CLADDING AT DOORWAY



METAL CLADDING AT WINDOWS



METAL CLADDING WITH VENT

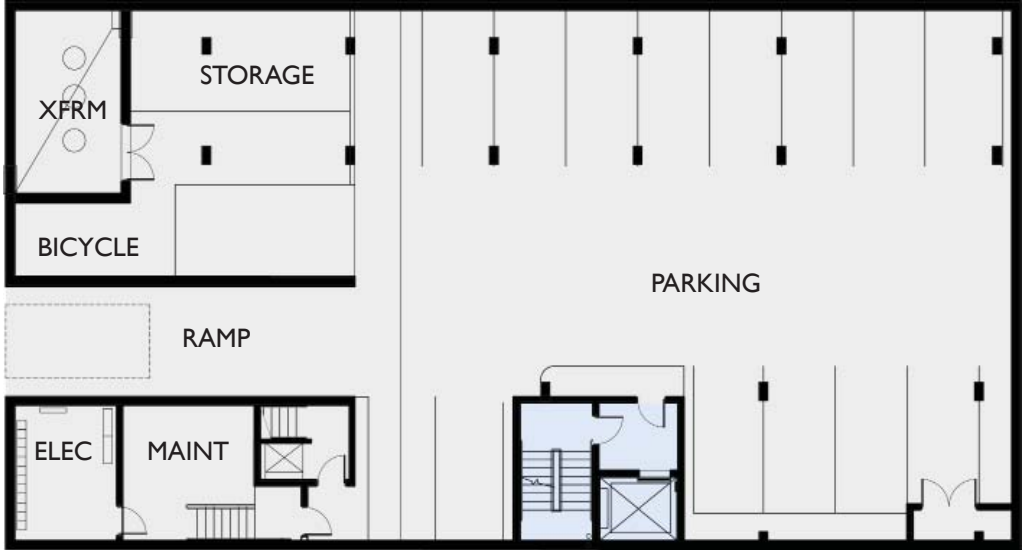


ROOF EDGE

FLOOR PLANS

LOWER LEVELS

LEVEL P1
PARKING GARAGE AND STORAGE



LEVEL 1
RETAIL, RES. ENTRY + SERVICE



LEVEL 2
LODGING



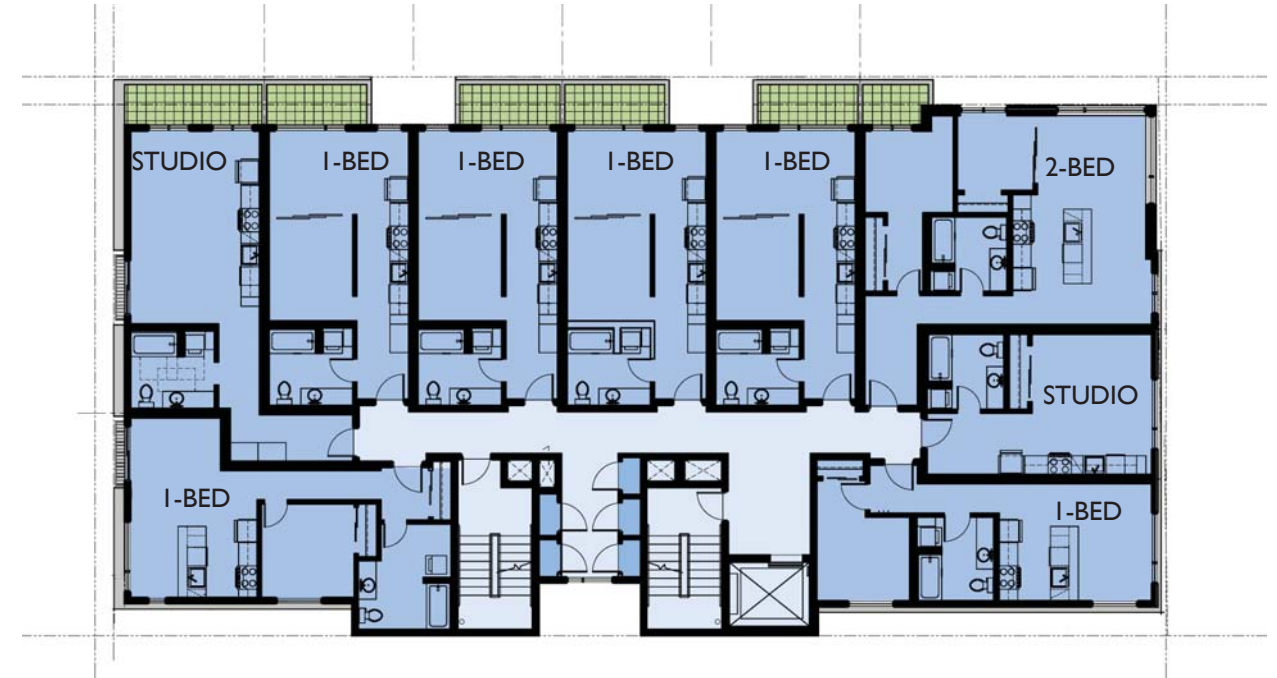
FLOOR PLANS

UPPER LEVELS

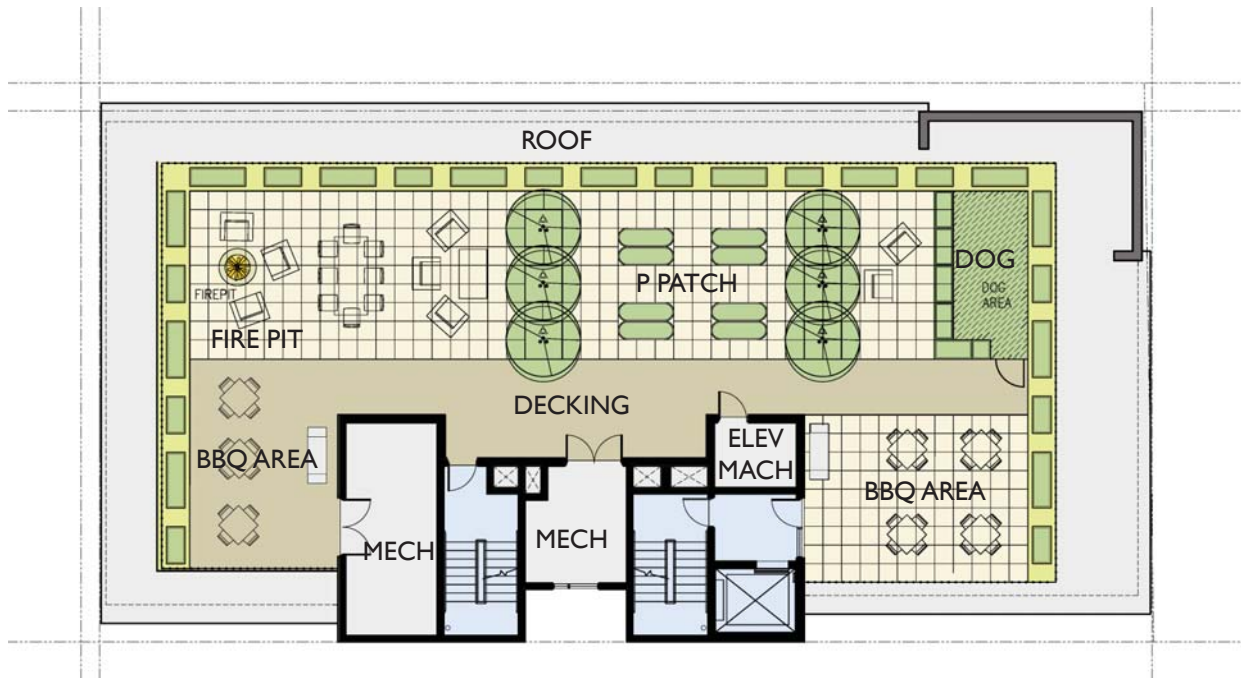
LEVELS 3-6
TYPICAL RESIDENTIAL FLOOR



LEVEL 7
RESIDENTIAL LEVEL



LEVEL 8
ROOFTOP AMENITY



LANDSCAPING

STREET LEVEL PLAN



LANDSCAPING CONCEPT

The Junction has been and always will be a pedestrian-oriented community anchored by transit. Within this context, 42nd and Oregon are pedestrian-designated streets and form the edge of the 'plateau' and present distinct faces to the community. The project provides an enhanced pedestrian streetscape with building overhangs and widened sidewalks on Oregon in order to encourage pedestrian activity and easy access to California, the primary shopping street. The treatment is extended around the corner to 42nd Avenue SW to help define the "gateway" from the east. A well planted corner reinforces the pedestrian nature and provides a visual cue to the pedestrian gateway entry to the neighborhood.

At the 42nd Avenue / Oregon corner, there is a landscaped sidewalk buffer in place, which is further enhanced by the project. The landscape plan consists of street trees and complementary concentrated landscape treatment along the wide 42nd Avenue frontage.

Plantings are proposed to buffer the site from the parking lot to the south, and vice versa.

The Landscape for the roof identifies activity zones towards the NW, SW, and SE that provide the same elements – gathering area, seating, view zones. BBQ, raised garden beds and a screened dog run contribute to a vibrant year-round amenity zone.

At each level, the landscape is designed to look good year-round, with the majority of ground plantings broadleaf evergreens with some seasonal interest (spring flowering, fall coloring). Plants are tightly spaced to ensure complete ground coverage.



NYSSA SYLVATICA
(STREET TREE)



ACER FREEMANII
AUTUMN BLAZE



CARPINUS BETULUS
FASTIGIATA



CONTORTA PINE



PARROTIA PERSICA
(ROOF)



PAVING AND TREES



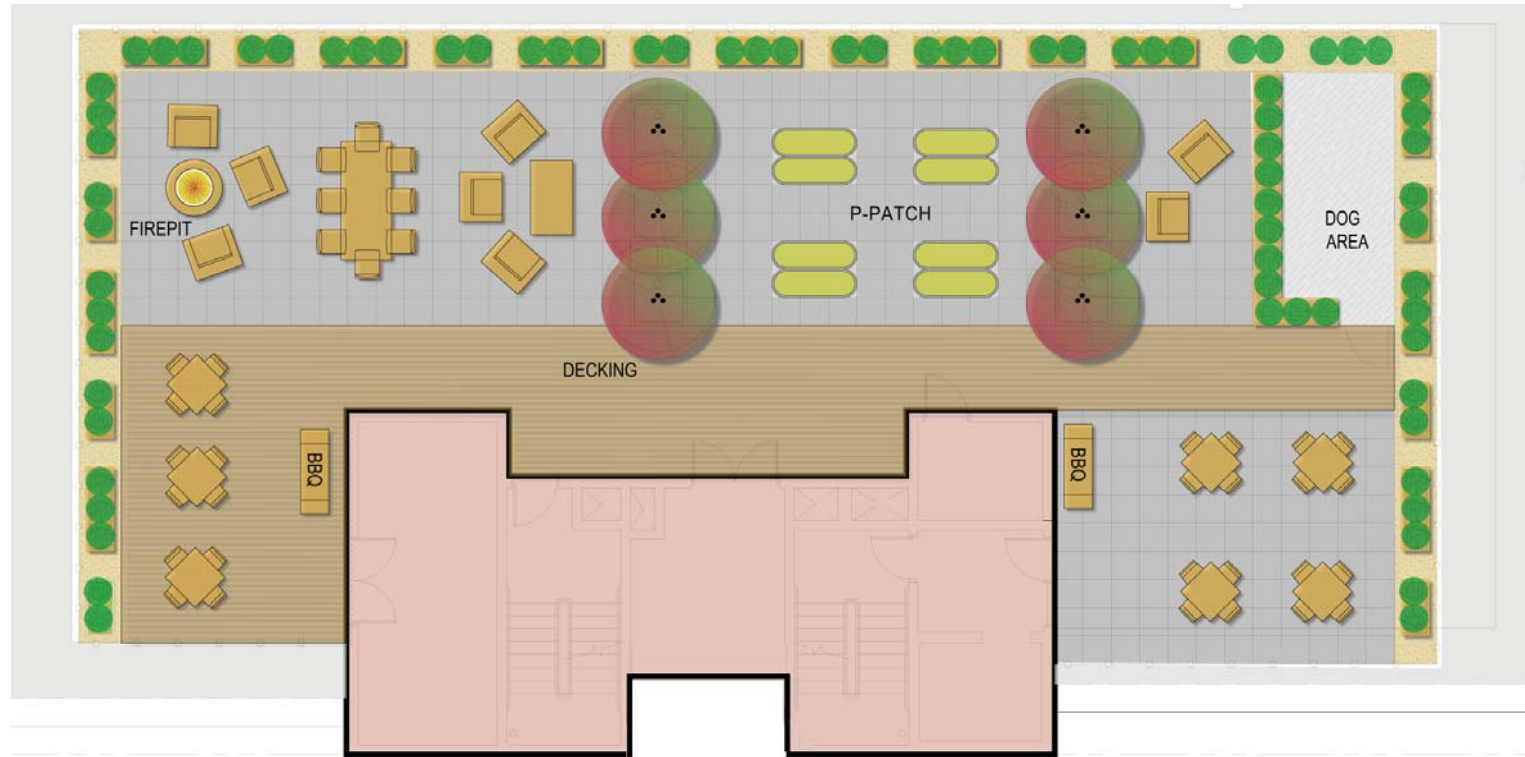
CONTINUOUS PLANTER



CEANOTHUS VICTORIA

LANDSCAPING

ROOF LEVEL PLAN



PLANTING SCHEDULE

P-PATCH PLANTERS: 2X6 HORSE TROUGH

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE/CONDITION/SPACING
TREES:			
EXISTING STREET TREE TO REMAIN			
STREET TREE - SW OREGON STREET: APPROVED BY BILL AMES, SDOT URBAN FORESTRY VIA EMAIL ON FEB. 7, 2014			
	NYSSA SYLVATICA 'DAVID ODOM'	AFTERBURNER TUPELO	2" CAL./B&B/PER PLAN
	ACER FREEMANII 'AUTUMN BLAZE'	AUTUMN BLAZE MAPLE	2" CAL./B&B/PER PLAN
	CARPINUS BETULUS 'FASTIGIATA'	PYRAMIDAL EUROPEAN HORNBEAM	2" CAL./B&B/PER PLAN
	PARROTIA PERSICA	PERSIAN IRONWOOD	2" CAL./B&B/PER PLAN
	STEWARTIA PSUEDOCAMELLIA	JAPANESE STEWARTIA	
	LAGERSTROEMIA INDICA X FAURIEI 'NATCHEZ'	NATCHEZ CRAPE MYRTLE	2" CAL./B&B/PER PLAN
	MAGNOLIA 'ELIZABETH'	ELIZABETH MAGNOLIA	
	PINUS CONTORTA 'CONTORTA'	SHORE PINE	8-10'HT./B&B/PER PLAN
SHRUBS & GROUNDCOVER			
	VIBURNUM DAVIDII	DAVID'S VIBURNUM	1 GAL./ CONT./ 36" O.C.
	PENNISETUM ALOPECUROIDES 'LITTLE BUNNY'	LITTLE BUNNY DWARF FOUNTAIN GRASS	1 GAL./ CONT./ 24" O.C.
	HELLEBORUS ORIENTALIS	HELLEBORE (WHITE AND PINK)	1 GAL./ CONT./ 24" O.C.
	SPIRAEA JAPONICA 'SHIROBANA**'	SHIROBANA SPIREA	1 GAL./ CONT./ 24" O.C.
	LIRIOPE SPICATA	LILYTURF	1 GAL./ CONT./ 18" O.C.
	ILEX CRENATA 'HELLERI'	JAPANESE HOLLY	1 GAL./ CONT./ 36" O.C.
	PHYLLOSTACHYS NIGRA W/ OPHIOPOGON JAPONICUS	BLACK BAMBOO MONDO GRASS (GREEN)	5 GAL./ CONT/ AS SHOW/ 1 GAL./ CONT/18" O.C.
	CISTUS 'GRAYSWOOD PINK'	GRAYSWOOD PINK ROCK ROSE	1 GAL./ CONT./ 36" O.C.
	POLYSTICHUM MUNITUM	SWORDFERN	1 GAL./ CONT./ 36" O.C.
	VIBURNUM DAVIDII	DAVID'S VIBURNUM	1 GAL./ CONT./ 36" O.C.
	GAULTHERIA SALAL	SALAL	1 GAL./ CONT./ 36" O.C.
	FRAGARIA CHILOENSIS*	BEACH STRAWBERRY	4" POT/ CONT./ 18" O.C.



FURNISHINGS, PLANTS



SECLUDED LOUNGING



FIRE PIT



PLANTERS SET IN PEBBLES



CISTUS GRAYSWOOD PINK



HELLEBORUS ORIENTALIS



LONICERA PILEATA



'LITTLE BUNNY'



PHYLLOSTACHYS-AUREA



ROSMARINUS OFFICINALIS



SPIREA JAPONICA SHIROBANA



VIBURNUM DAVIDII



P-PATCH PLANTERS

DESIGN FEATURES

FORTY-SECOND AVENUE

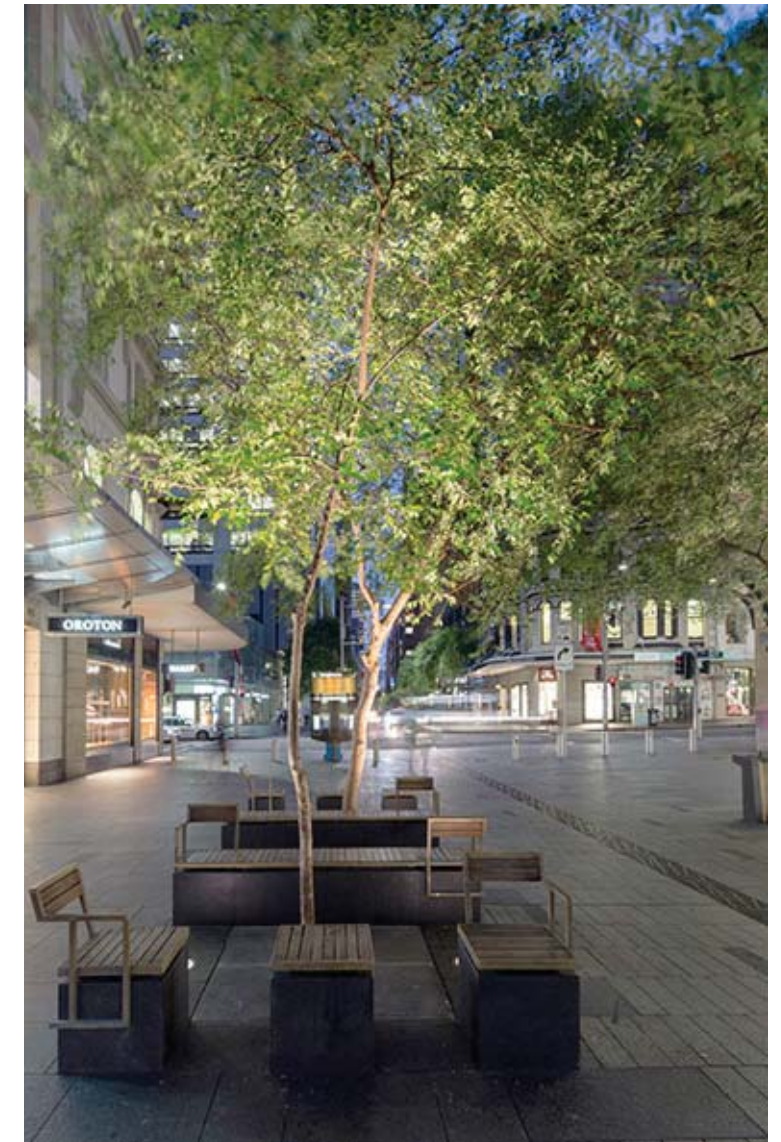
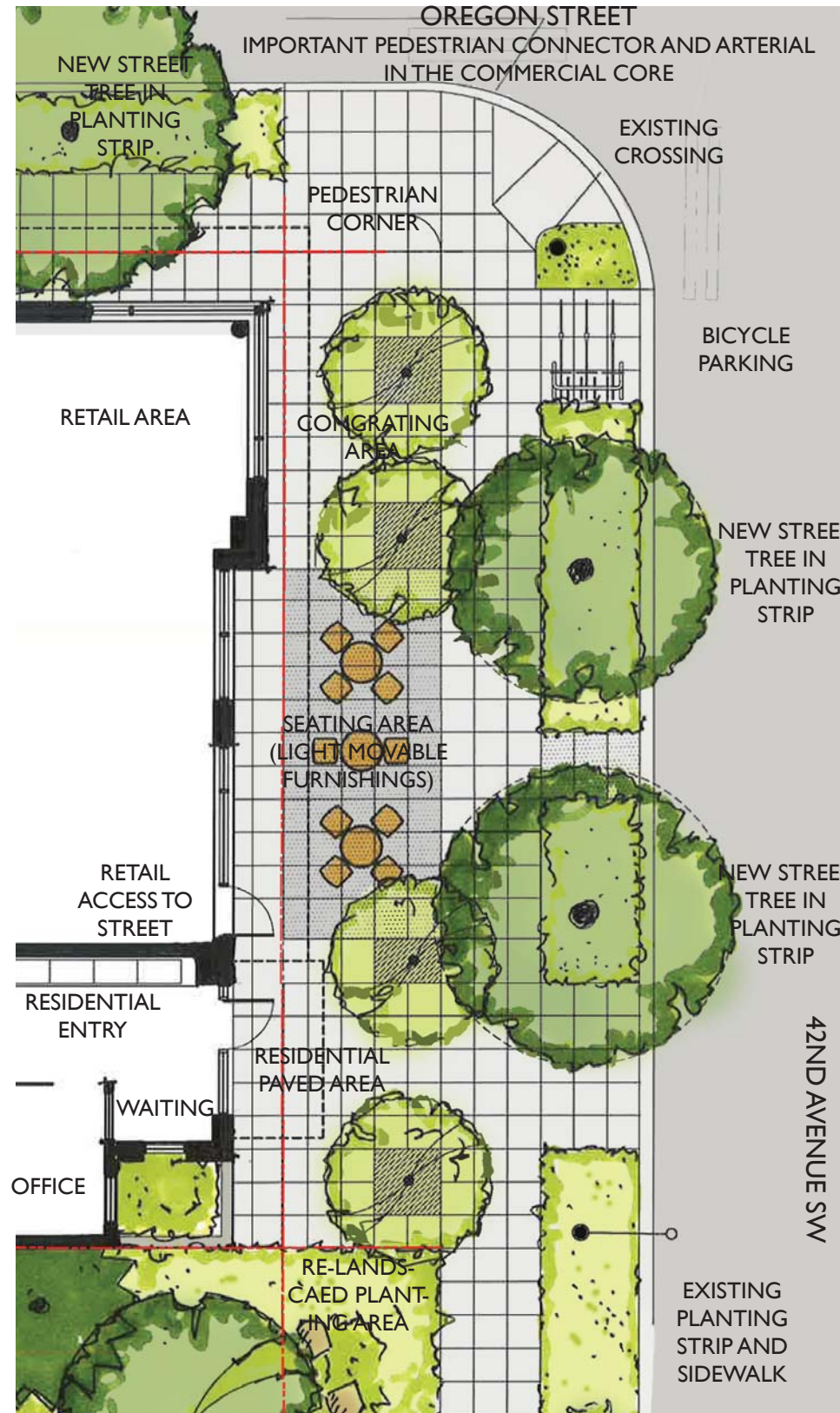


DESIGN FEATURES

FORTY-SECOND AVENUE

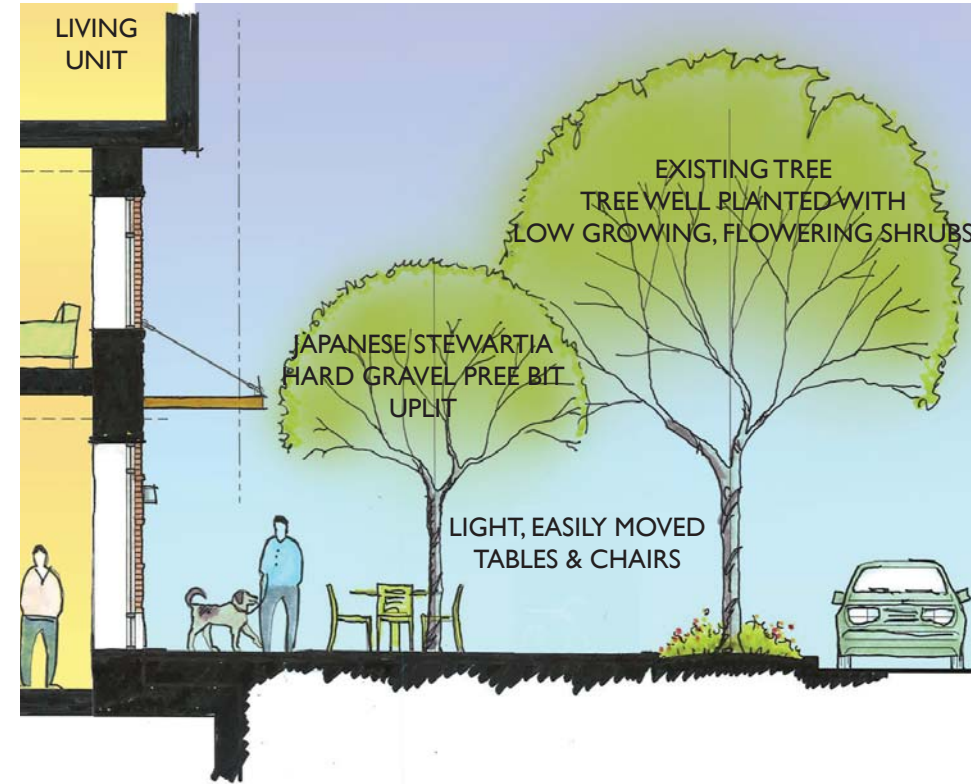
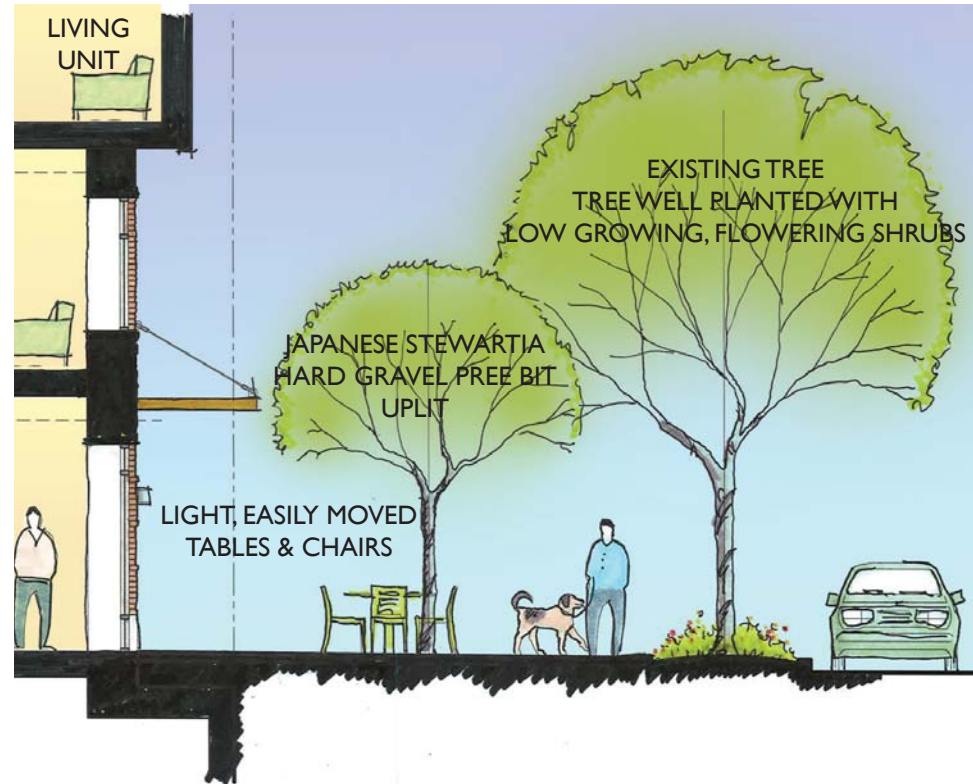


SMALL SCALE, MOVABLE FURNITURE



GRADE DIFFERENTIAL

EXPLORING RETAIL WITH FLOORS BELOW THE SIDEWALK

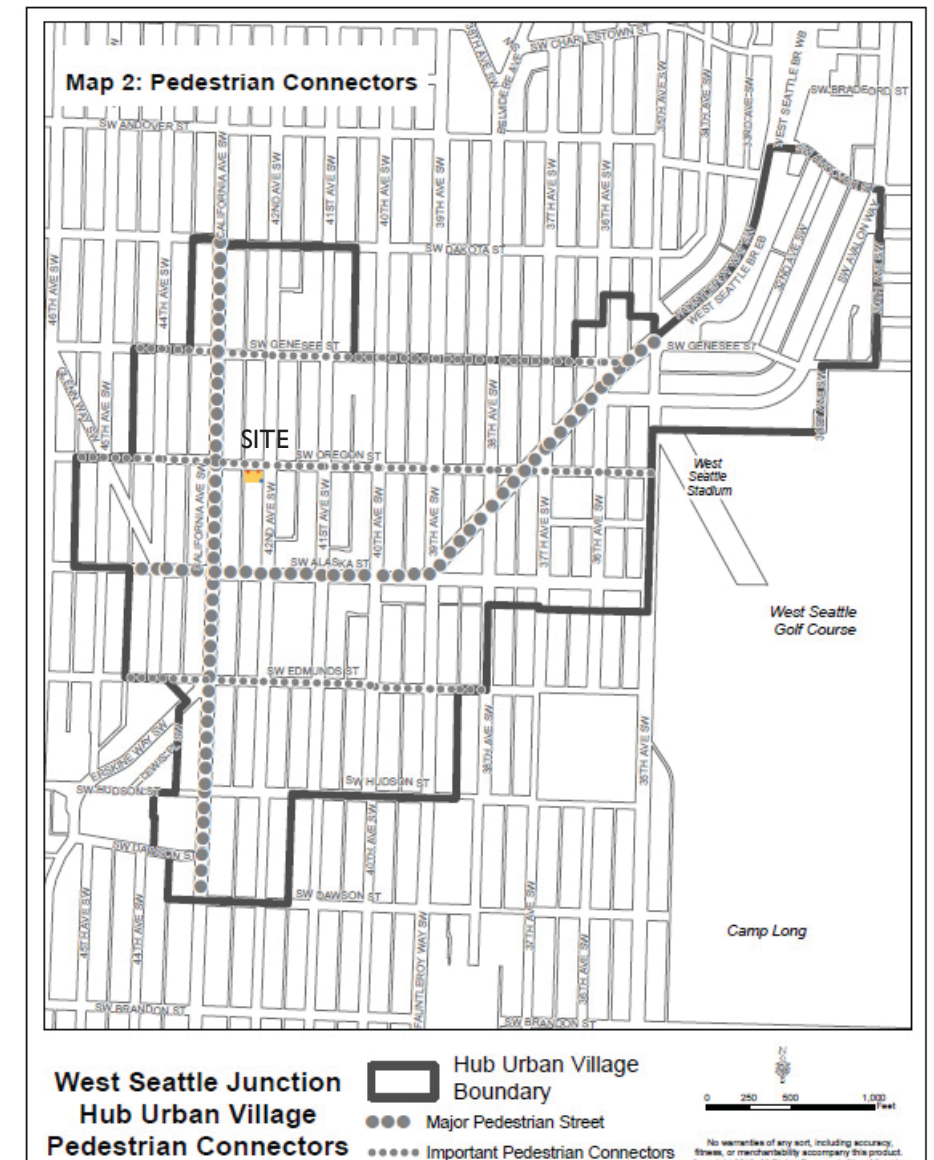


PROPOSED RELATIONSHIP OF RESIDENTIAL ENTRY AND GRADE WITH SIDEWALK ALONG 42ND INCLUDING (FROM RIGHT TO LEFT) :

- ROADWAY WITH PARKING
- 0'-6" CURB
- 5'-0" TREE WELL WITH LOW HEIGHT UNDER PLANTINGS
- 6'-0" +/- SIDEWALK
- 9'-6" +/- PAVING "CARPET" FOR FURNISHINGS
- DAVIS LIGHT GREY COLORED CONCRETE, DEEP TOOLED JOINTS, NO SHINERS, LT SANDBLAST FINISH
- 3'-0" +/- ACCESS STRIP (ALIGNS WITH PROPERTY LINE)
- BUILDING WITH NO SILL AND STOREFRONT TO GRADE
- RESIDENTIAL ENTRY AT SIDEWALK GRADE, CANOPY ABOVE

PROPOSED RELATIONSHIP OF INTERIOR RETAIL GRADE WITH SIDEWALK ALONG 42ND INCLUDING (FROM RIGHT TO LEFT) :

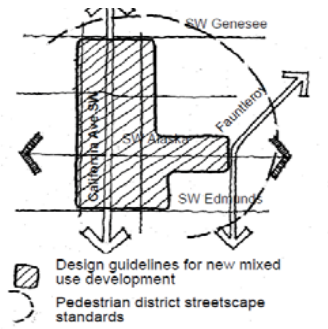
- ROADWAY WITH PARKING
- 0'-6" CURB ELEV: 340'-6"
- 5'-0" TREE WELL WITH LOW HEIGHT UNDER PLANTINGS
- 6'-0" +/- SIDEWALK ELEV: 341'-0"
- 9'-6" +/- PAVING "CARPET" FOR FURNISHINGS
- DAVIS LIGHT GREY COLORED CONCRETE, DEEP TOOLED JOINTS, NO SHINERS, LT SANDBLAST FINISH
- 3'-0" +/- ACCESS STRIP (ALIGNS WITH PROPERTY LINE) ELEV: 341'-1"
- BUILDING WITH NO SILL AND STOREFRONT TO GRADE
- RETAIL LEVEL AT ELEV: 339'-6"



West Seattle Junction Hub Urban Village Pedestrian Connectors

- Hub Urban Village Boundary
- Major Pedestrian Street
- Important Pedestrian Connectors

Pedestrian Environment: In general, the pedestrian environment (sidewalks, pathways, entries and crossings) should be safe, accessible to all, connect to places people want to go, and provide good places to be used for many things. New development should reflect these principles by enhancing commercial district streetscapes with development that makes pedestrian activity at the street level a priority (see Map 2, page 3).



The overall goal of these guidelines is to aid in creating a district in which new development supports a mix of uses and engages the public realm (i.e. sidewalk) in a pedestrian-oriented manner. The commercial core is considered to include California Avenue SW from SW Edmonds Street to SW Genesee Street, SW Alaska from 44th Avenue SW to 39th Avenue SW, and SW Genesee, Oregon and Edmonds Streets from 44th Avenue SW to 42nd Avenue SW. California Avenue SW is recognized as the area's current pedestrian-oriented business district, however the neighborhood envisions SW Alaska Street from California Avenue SW to Fauntleroy Way SW to become an extension of this mixed use district with a continuous pedestrian scale and high level of comfort at the street level.

The neighborhood recognizes that a successful pedestrian environment is really a pedestrian "network", extending beyond sidewalks to include paths, crossings and building entries. Mid-block pedestrian connections are encouraged to be incorporated into larger new development to link parking and surrounding streets to the commercial core.

WEST SEATTLE JUNCTION DESIGN GUIDELINES

Context Priority Issues:

I. Pedestrian Environment: ...The overall goal is to aid in creating a district in which new development supports a mix of uses and engages the public realm in a pedestrian-oriented manner. the commercial core is considered to include California Avenue SW... Oregon and Edmonds Streets from 44th ave SW to 42nd Ave W. California Avenue SW is recognized as the areas current pedestrian oriented business district.



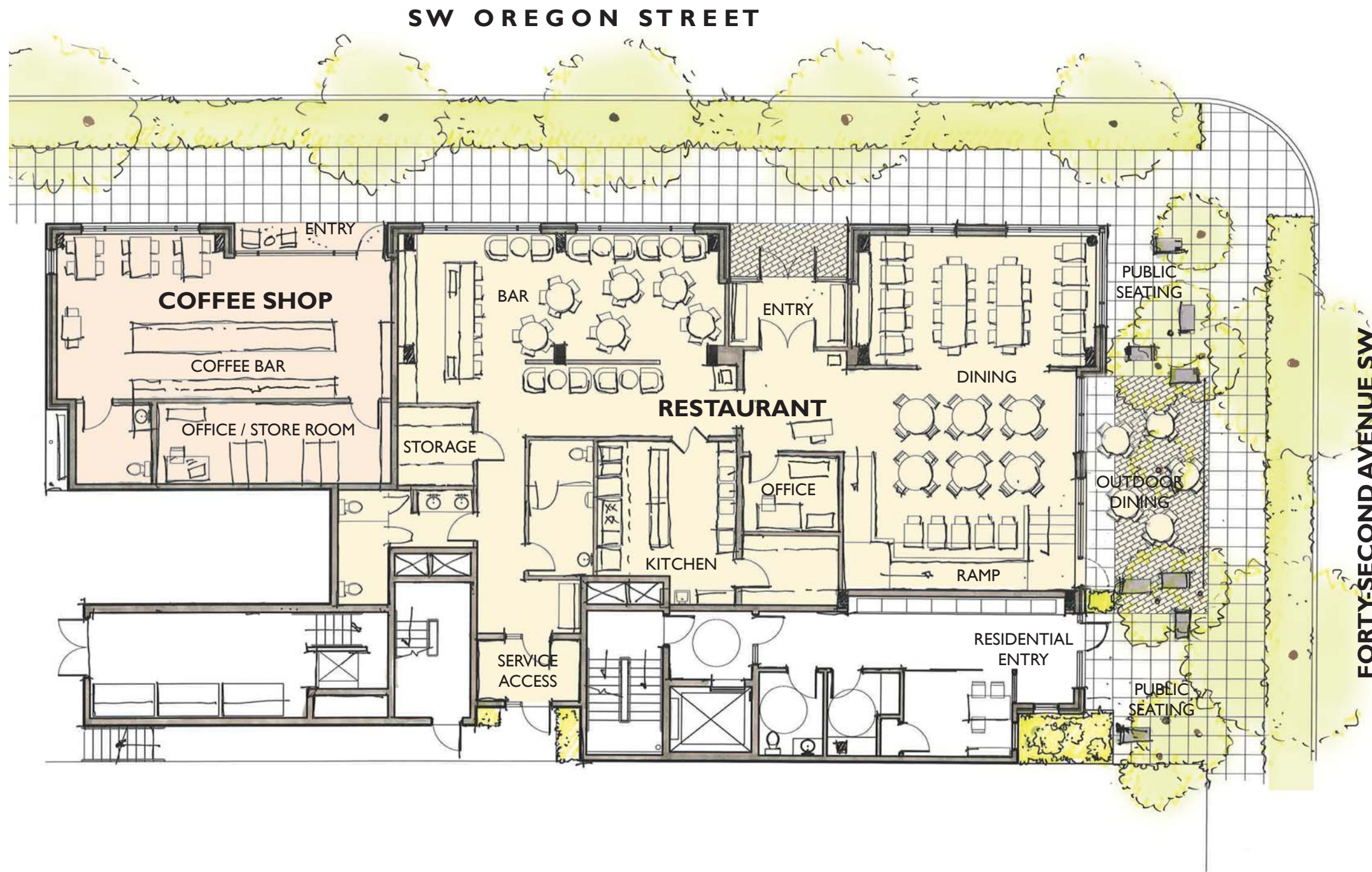
BARRIO
1420 TWELFTH AVENUE

INTERIOR FLOOR LEVEL VARIES BETWEEN 24" AND 36" +/- BELOW ADJACENT SIDEWALK ENTRY IS SLIGHTLY BELOW GRADE, AT RIGHT.



GROUND FLOOR

POTENTIAL RETAIL TENANT LAYOUT: COFFEE SHOP AND RESTAURANT



CONCEPTUAL TENANT IMPROVEMENT PLANS

Capelouto 42nd - DPD #3016195

DESIGN RECOMMENDATION

DESIGN FEATURES

SW OREGON ST

SW OREGON ROW SETBACK

The project is providing a three foot setback at the ground level per section 23.53.0153h. The setback is sized to provide adequate space for the sidewalk, planting strip, and streetscape amenities.

An Exception Request was made to the Department of Planning and Development, zoning, via Lori Swallows. The setback exception request is consistent with other projects in the vicinity.

The existing sidewalk feels particularly narrow as it is partially blocked by the overgrown laurel hedge and its proximity to the street.

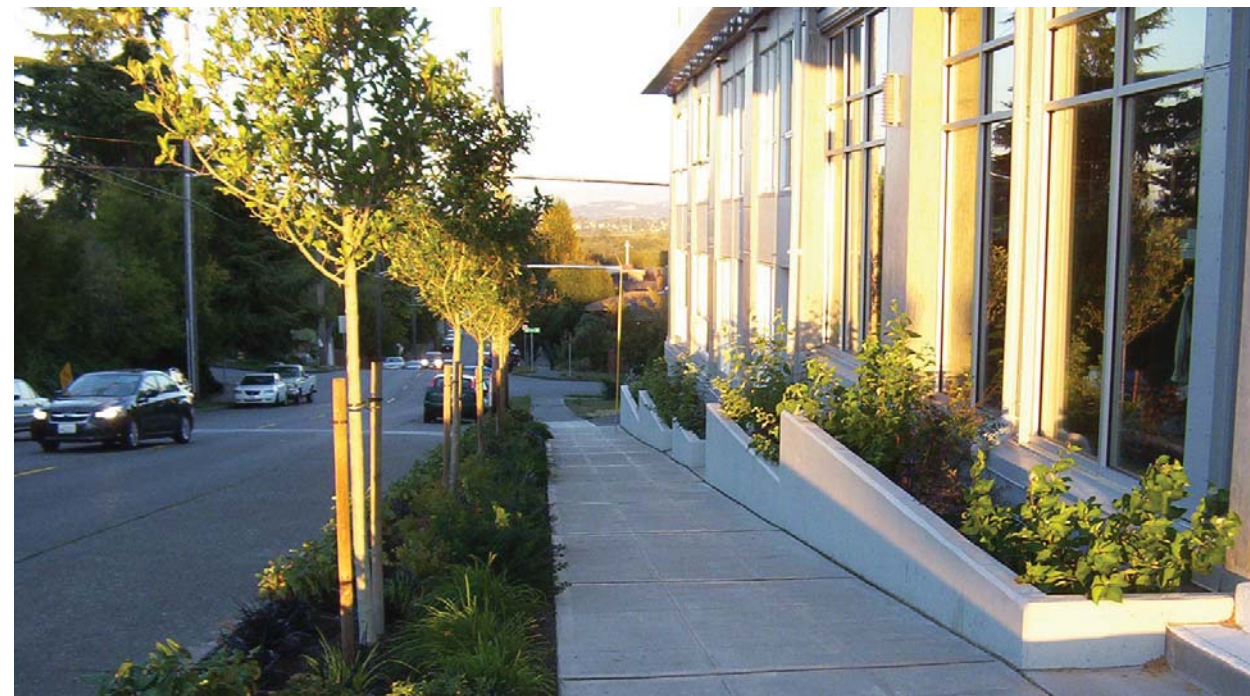
The proposed sidewalk is sized to exceed those found on California Avenue SW and to exceed the sidewalk further east along SW Oregon Street. The project team also feels the sidewalk layout is superior to the existing sidewalk directly east of the site, in front of the West Seattle Senior Center, as it provides a wide walking area that is separated from the street by a five foot planting strip, with street trees, that can also accommodate street signage.

The request was granted with the following reasoning:

“The street is not developed to zoned capacity but the proposal is to set back the street level portion of the structure the required three feet and up to 10’ above grade which will allow for adequate vertical clearance and allow for the required sidewalk and planting strip to be provided.

The sidewalk provided will be adequate for pedestrian traffic.

The current roadway width is approximately 39’ which exceeds the 36’ required for the NC3 zones.”



SW OREGON STREET SIDEWALK EAST OF SITE - NOTE :THE PLANTING STRIP IS LESS THAN 4' WIDE AND THE 3' SETBACK IS USED FOR STORMWATER DETENTION AGAINST THE BUILDING

PROPOSED STREETScape WITH CURB, PLANTING STRIP AND WALKING SURFACE RIGHT UP TO THE BUILDING EDGE WITH OVER-HEAD WEATHER PROTECTION

DESIGN FEATURES

SIGNAGE

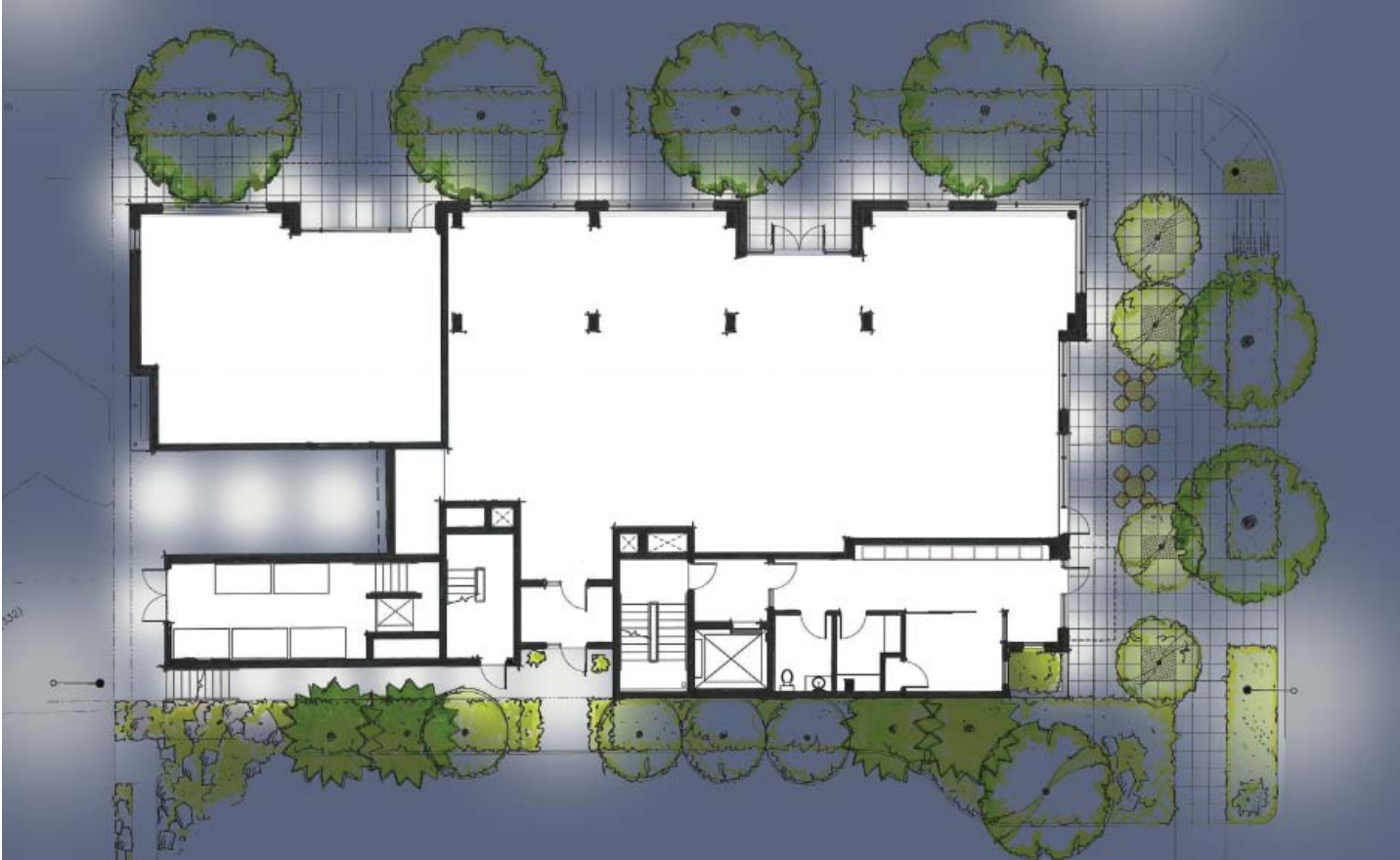
RESIDENTIAL ENTRY SIGNAGE
BLACK METAL BUILDING NAME HIGHLIGHTING INITIALS MOUNTED ON CANOPY
BLACK METAL LETTERS WITH ADDRESS ON FACE OF CANOPY
CANOPY AT RESIDENTIAL ENTRY ONLY: PAINTED :“RICH CHESTNUT” BY BENJAMIN MOORE



EXAMPLE OF CANOPY MOUNTED LETTERING

DESIGN FEATURES

EXTERIOR LIGHTING



GROUND FLOOR LIGHTING PLAN



ROOF LIGHTING PLAN



Ground Level Wall Mounted Fixture



Can Light (entries)



Utility Fixture (Alley)



In-Ground Up Light (Trees)



Light Fixture - Rear Entry



Path Lighting

SIDEWALKS

EXPLORING NEARBY STREETSCAPES



TYPICAL CALIFORNIA AVENUE SW SIDEWALK

SW OREGON SIDEWALK AT SR CENTER - LOOKING EAST

NO OVERHEAD PEDESTRIAN PROTECTION BETWEEN CALIFORNIA AND 42ND ALONG OREGON



TREE WELLS LOCATED AT THE CURB NARROW THE SIDEWALK TO 6' +/-

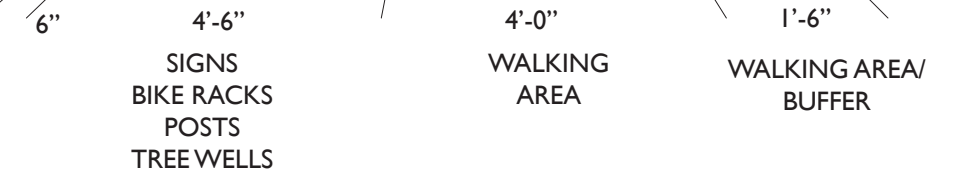
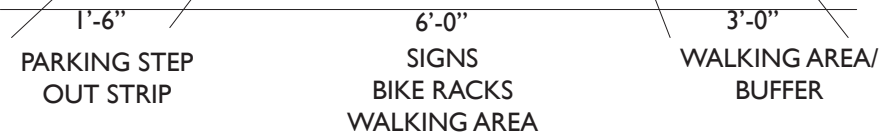
BUILDING ENTRANCES EXTEND SIDEWALK



TYPICAL CALIFORNIA AVENUE SW SIDEWALK - LOOKING NORTH



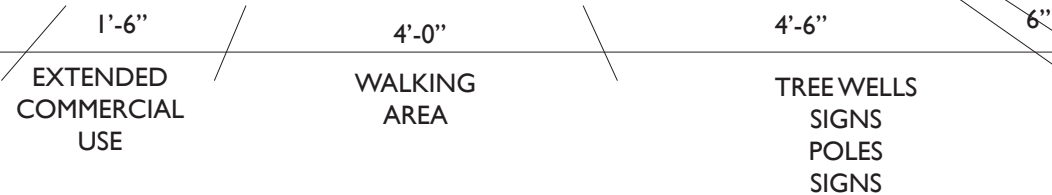
TYPICAL CALIFORNIA AVENUE SW SIDEWALK



TYPICAL CALIFORNIA AVENUE SW SIDEWALK - LOOKING SOUTH

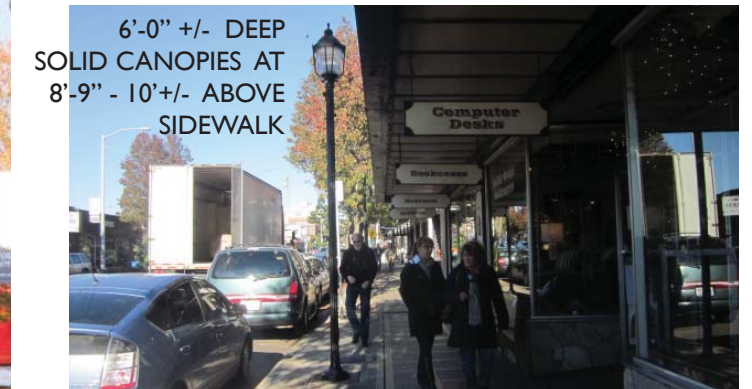


BUSINESSES OFTEN SPILL OUT INTO STREET, UTILIZING A PORTION OF THE SIDEWALK, NARROWING THE WALK SPACE



EAST SIDE OF CALIFORNIA AVENUE SW

6'-0" +/- DEEP SOLID CANOPIES AT 8'-9" - 10'+/- ABOVE SIDEWALK



VILLAGE WOODWORKS 4538 CALIFORNIA AVENUE SW

INSET DOORWAYS PROVIDE RELIEF TO SIDEWALK AND ALLOW FOR CHANGES IN GRADE



TRASH CANS NEXT TO CURB ALONG CALIFORNIA

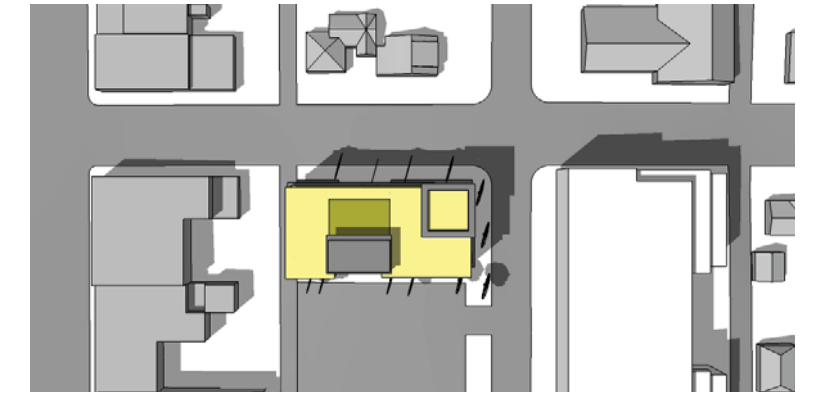
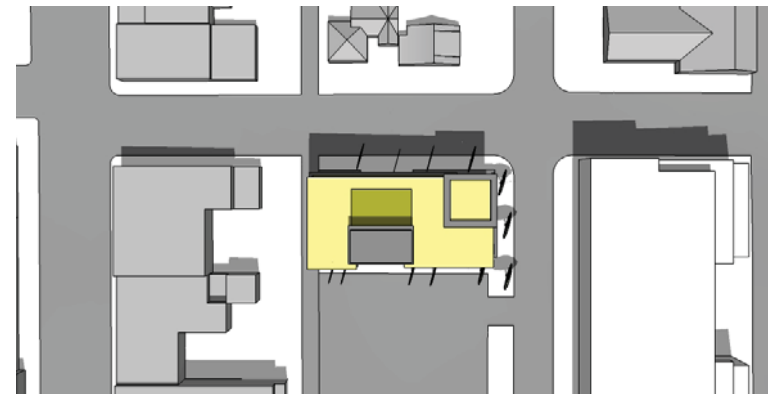
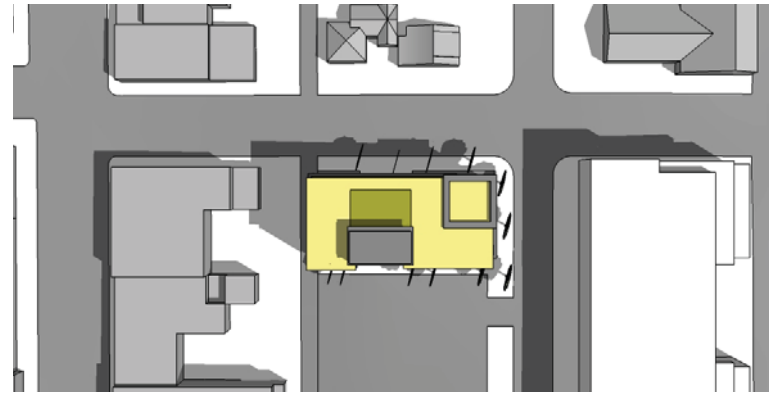
SHADOW STUDIES

10:00 AM

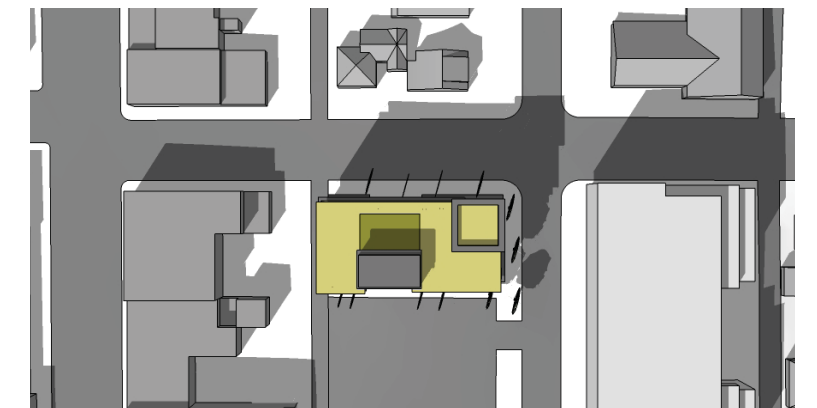
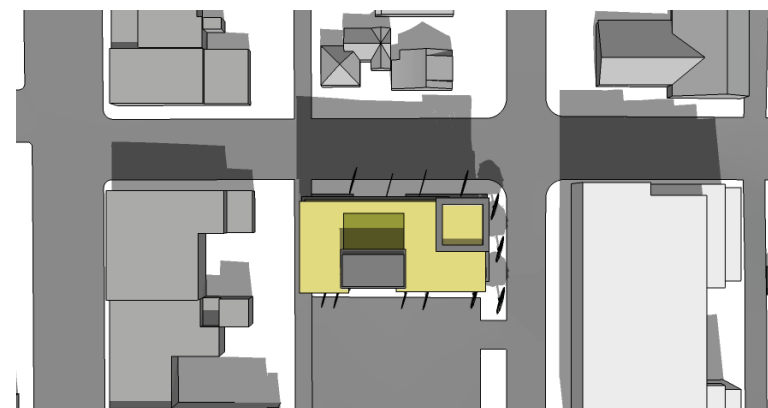
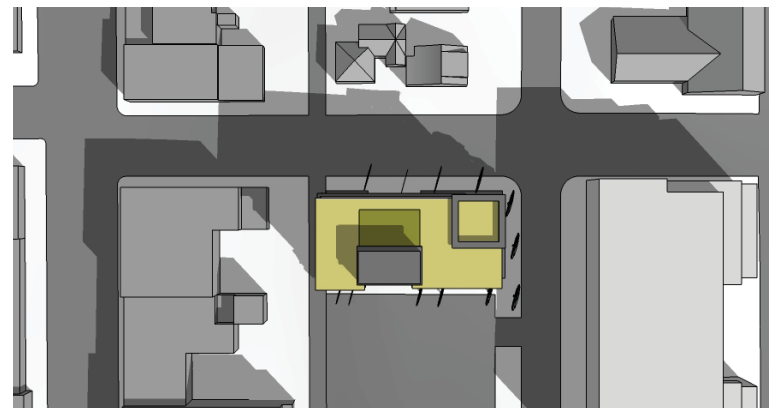
NOON

2:00 PM

JUNE



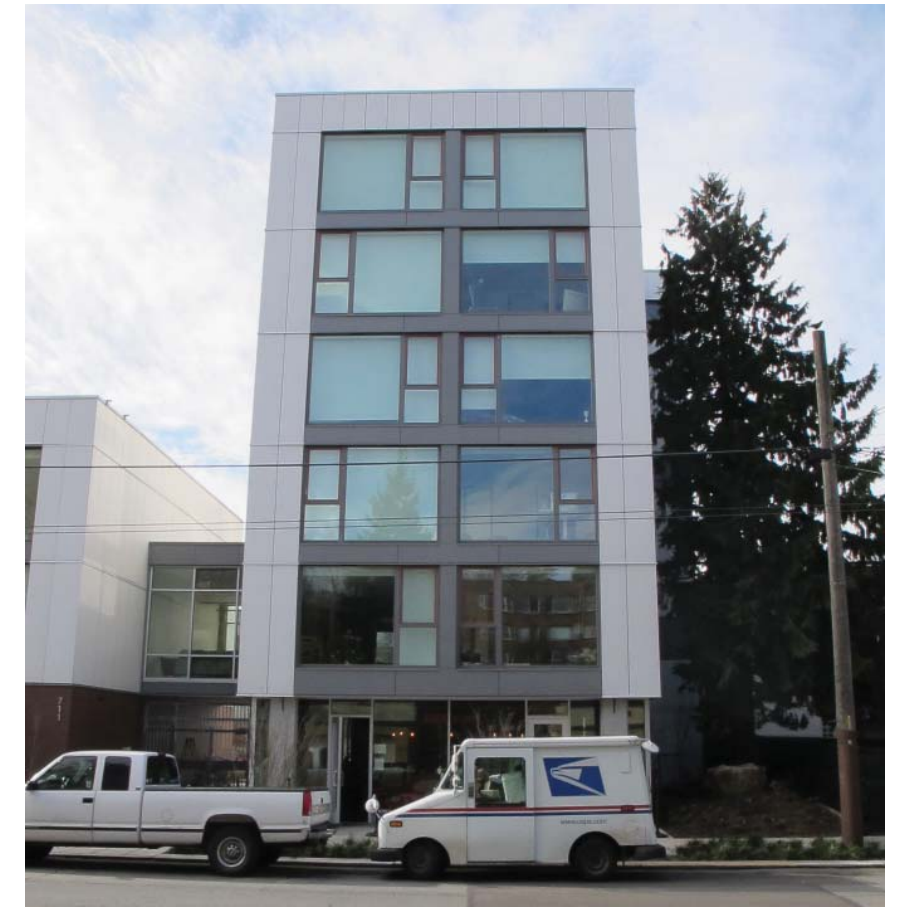
MARCH/SEPTEMBER



DECEMBER



DESIGN INSPIRATION



Capelouto 42nd - DPD #3016195

DESIGN RECOMMENDATION

RECENT NK PROJECTS



THE VUE



DAKOTA



STREAM BELMONT



CAMPUS ACQUISITIONS



PARK PASSIVE: AIA NATIONAL HOUSING AWARD WINNER

nk NICHOLSON KOVALCHICK ARCHITECTS



H2O APARTMENTS - LEED-H PLATINUM TARGET



BROADSTONE KOI - LEED-NC CERTIFIED TARGET



ARTHOUSE



APERTURE - BUILT GREEN 3-STAR TARGET