



CAPEOUTO 42ND

4505 42ND AVENUE SW,

DESIGN REVIEW
RECOMMENDATION
DPD #3016195
DECEMBER 4TH, 2014

310 First Avenue S, Suite 4S,
Seattle, WA 98104
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PROJECT VISION

Set within the established but growing West Seattle Junction, this project combines richly textured materials with a clean and timeless architecture, resulting in a project that respects the neighborhood traditions while helping it grow and evolve. Street life along 42nd Ave and SW Oregon St will be enhanced through thoughtfully crafted retail spaces that activate the building's interface with the public right-of-way. The residential portion of the building will be accessed from a more private entry off of SW Oregon St.

PROJECT PROGRAM

Number of Residential Units: 41 units
 Number of Lodging Units: 9 units
 Area of Residential Use: 30,329 sf
 Area of Lodging Use: 6,901 sf
 Area of Commercial Uses: 3,598 sf
 Number of Parking Spaces: 15 spaces below grade
 Floor Area Ratio: 5.96
 Number of Stories: 7 stories
 Total Area: 48,022 sf
 Total Area Above Grade: 41,122 sf

OWNER

Leon Capelouto
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 Bothell, WA 98041

ARCHITECT

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SITE INFORMATION

ZONING CODE SUMMARY AND MAP

ADDRESS: 4505 42ND AVENUE SW
 PARCEL #: 095200-6385
 ZONING: NC3-85
 OVERLAYS: West Seattle Junction Hub Urban Village (including Frequent Transit).
 LOT AREA: 6,900 Square Feet

23.47A.004 PERMITTED USES, TABLE A

Permitted outright:

- Residential
- Residential Uses Restricted to 20% at the Ground Floor
- Commercial Uses
- Lodging

SMC 23.47A.005 STREET LEVEL USES

In all neighborhood commercial zones, residential uses may occupy, in the aggregate, no more than 20 percent of the street-level street-facing facade within a zone that has a height limit of 85 feet or higher.

SMC 23.47A.008 STREET LEVEL DEVELOPMENT STANDARDS

Blank segments of the street-facing facade between 2 feet and 8 feet above the sidewalk may not exceed 20 feet in width.

The total of all blank facade segments may not exceed 40 percent of the width of the facade of the structure along the street.

Street-level street-facing facades shall be located within 10 feet of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided

SMC 23.47A.013 FLOOR AREA RATIO

Single-purpose: 4.5 (maximum any single use)
 Mixed-use: 6.0

SMC 23.47A.012 STRUCTURE HEIGHT

- Base height limit of 85'.
- An additional 4' is allowed for open railings, planters, skylights, clerestories, greenhouses, solariums, parapets and firewalls.
- An additional 15' is allowed for mechanical equipment if less than 20% of the total roof surface or 25% of the roof surface if stair, elevator penthouses, and screen mechanical equipment are included in the calculation.
- An additional 16' is allowed for stair and elevator penthouses.

SMC 23.47A.014 SETBACK REQUIREMENTS

No setbacks required

SMC 23.47A.016 LANDSCAPING AND SCREENING STANDARDS

- Landscaping that achieves a Green Factor score of .30 or greater is required.
- Street trees are required when any development is proposed. Existing street trees shall be retained unless the Director of Transportation approves their removal. The Director, in consultation with the Director of Transportation, will determine the number, type and placement of street trees to be provided.

SMC 23.47A.024 AMENITY AREA

Amenity areas are required in an amount equal to 5 percent of the total gross floor area in residential uses. Amenity areas shall not be enclosed. Common amenity areas shall have a minimum horizontal dimension of 10 feet, and no common amenity area shall be less than 250 square feet in size. Private balconies and decks shall have a minimum area of 60 square feet, and no horizontal dimension shall be less than 6 feet.

23.47A.030 REQUIRED PARKING

No minimum parking requirement for residential uses in commercial within urban villages if the residential use is located within 1,320 feet of a frequent transit service.

SMC 23.47A.032 PARKING LOCATION AND ACCESS

Access to parking shall be from the alley if the lot abuts an alley.

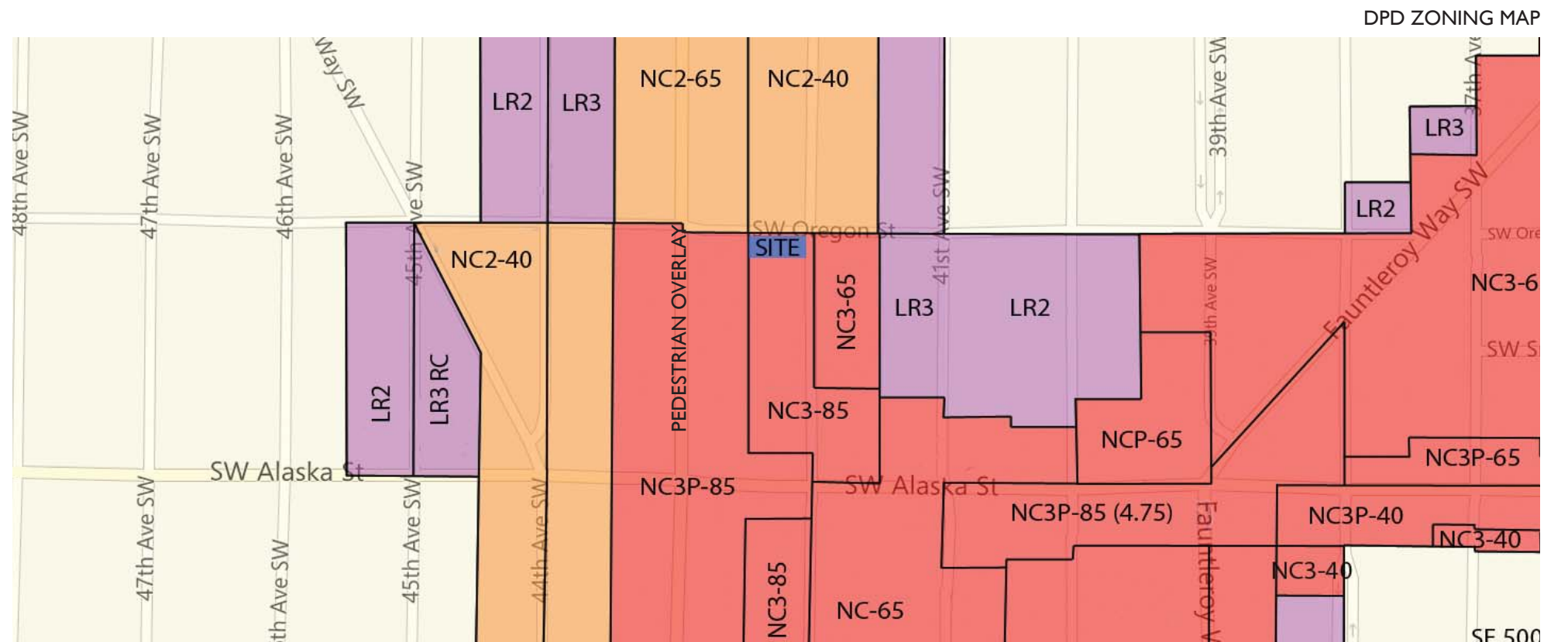
SMC 23.54.015 BICYCLE PARKING

Long term bicycle parking is required in equivalent of 1 space for every 4 units for residential use, 1 per 20 rooms, and 1 space for every 12,000 square feet of general commercial use.

Short term bicycle parking is required: 2 per 20 rooms of lodging, 1 per 2000sf of general commercial space.

SMC 23.54.040 SOLID WASTE AND RECYCLABLES

Primary trash and recyclable room shall be a minimum of 375 square feet for a structure containing 26-50 residential units or 375 square feet for project containing 50-100 units plus an additional 4 square feet per unit over 50. Mixed use developments required additional area for nonresidential uses (50% of stand alone requirements); 41 square feet for nonresidential uses less than 5,000 square feet and 63 additional square feet for nonresidential uses more than 5,000 but less than 15,000 square feet.



SITE INFORMATION

NEIGHBORHOOD CONTEXT

DEVELOPMENT OBJECTIVES

The development objective is to construct a new multi-story building containing residential units, short term nontransient lodging units, commercial retail space, and below grade automobile parking. Due to the proximity of the project to the West Seattle Junction, the retail component will play an important role in the building design. One goal of the retail space is to create a strong connection to the adjacent sidewalks by including an extensive amount of window transparency; thus enlivening the sidewalk realm with retail activity. The residential units will vary in size including Studios, 1-Bedroom, and 2-Bedroom units. A large common rooftop outdoor terrace is planned for resident use that will provide expansive territorial views of the region. Parking is not required due to the location of the project site within the West Seattle Junction Hub Urban Village, but some parking spots are proposed on the below-grade level.

The project team endeavors to reinforce the existing pedestrian scale of SW Oregon Street and 42nd Avenue SW, while increasing the density and community connectivity of the West Seattle Junction neighborhood. The project will take advantage of its site along the West Seattle ridge by maximizing views to the Puget Sound and Olympic Mountains to the west, Elliott Bay, downtown Seattle, and the Cascade Mountains to the east.

EXISTING SITE

The project site consists of a single parcel, located on the southwest corner of the SW Oregon Street and 42nd Avenue SW intersection. The site has a total area of 6,903 square feet. The project includes demolition of the single-family house that currently occupies the site. The site is bounded on the west by a 16'-0" wide alley (a 2' alley dedication is required in order to widen the alley). The site gradually slopes down from 42nd Avenue SW towards the alley. The site does not include any environmentally critical areas. Several trees currently stand on the site and an overgrown hedge in the right-of-way obscurs much of the site. None of the trees on the site or adjacent to the site are exceptional, per the tree survey; several are considered invasive and all the existing trees on site have been recommended for removal. Overhead high voltage power lines also run adjacent to the site both on 42nd Avenue SW and through the alley.

ZONING AND OVERLAY DESIGNATION

The parcel is zoned NC3-85 and is located within the West Seattle Junction Hub Urban Village. The area immediately surrounding the project site is zoned NC2-40 across SW Oregon Street to the north, NC3-65 across 42nd Avenue SW to the east, NC3-85 & NC3P-85 to the south, and NC3P-85 across the alley to the west. The site is also located within a Frequent Transit Corridor.

NEIGHBORING DEVELOPMENT

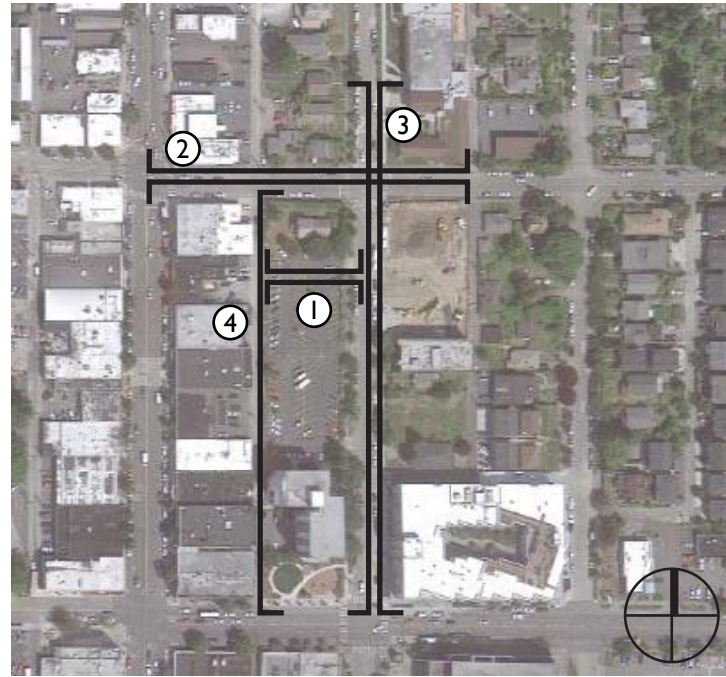
The project site is located within the West Seattle Junction Hub Urban Village, and only one long block from the lively intersection of California Avenue SW and SW Alaska Street. The general character of this block and surrounding blocks is a mix of uses and development densities. To the west across the alley and along California Avenue SW is the highly recognizable pedestrian friendly low-rise retail uses characterizing the Junction. To the north, the retail uses continue with a sprinkling of single family homes that have been converted to business uses; some of these single family homes are targeted for demolition due to the higher density development zone in which they are located. Also to the north are several churches and related religious functions. Directly to the east is a new 7-story mixed use development with a similar program to the proposed project. Directly to the south is a large surface parking lot that provides time limited parking for shopping patrons of the Junction. Beyond the parking lot to the south is more low-rise retail uses typical of the junction along with several large scale mixed use projects that have been more recently developed. The site is very pedestrian friendly and within good proximity to the major West Seattle Junction public transit hub. There are multiple shops, restaurants, cafés, and grocery stores all within walking distance of the site.



SITE LOCATION

SITE INFORMATION

STREETSCAPE PHOTOMONTAGES



PROJECT SITE



① ADJACENT PARKING LOT LOOKING NORTH

ACROSS FROM PROJECT SITE



② SW OREGON STREET LOOKING NORTH

PROJECT SITE



② SW OREGON STREET LOOKING SOUTH

SITE INFORMATION

STREETSCAPE PHOTO MONTAGES

ACROSS FROM PROJECT SITE



③ 42ND AVENUE SW LOOKING EAST

PROJECT SITE



③ 42ND AVENUE SW LOOKING WEST

PROJECT SITE



④ THE ALLEY LOOKING EAST



① ADJACENT PARKING LOT LOOKING SOUTH

SITE INFORMATION

SITE CONTEXT



① HOPE LUTHERAN CHURCH



② CONNER BUILDING (CONSTR)



③ 4730 CALIFORNIA (CONSTR)



④ OREGON42 APARTMENTS (CONSTR)



⑤ CAPCO PLAZA & QFC



⑥ JEFFERSON SQUARE COMMERCIAL



⑦ MIXED-USE MURAL APARTMENTS



⑧ SHADOWLAND



⑨ EASY STREET RECORDS



⑩ CUPCAKE ROYALE



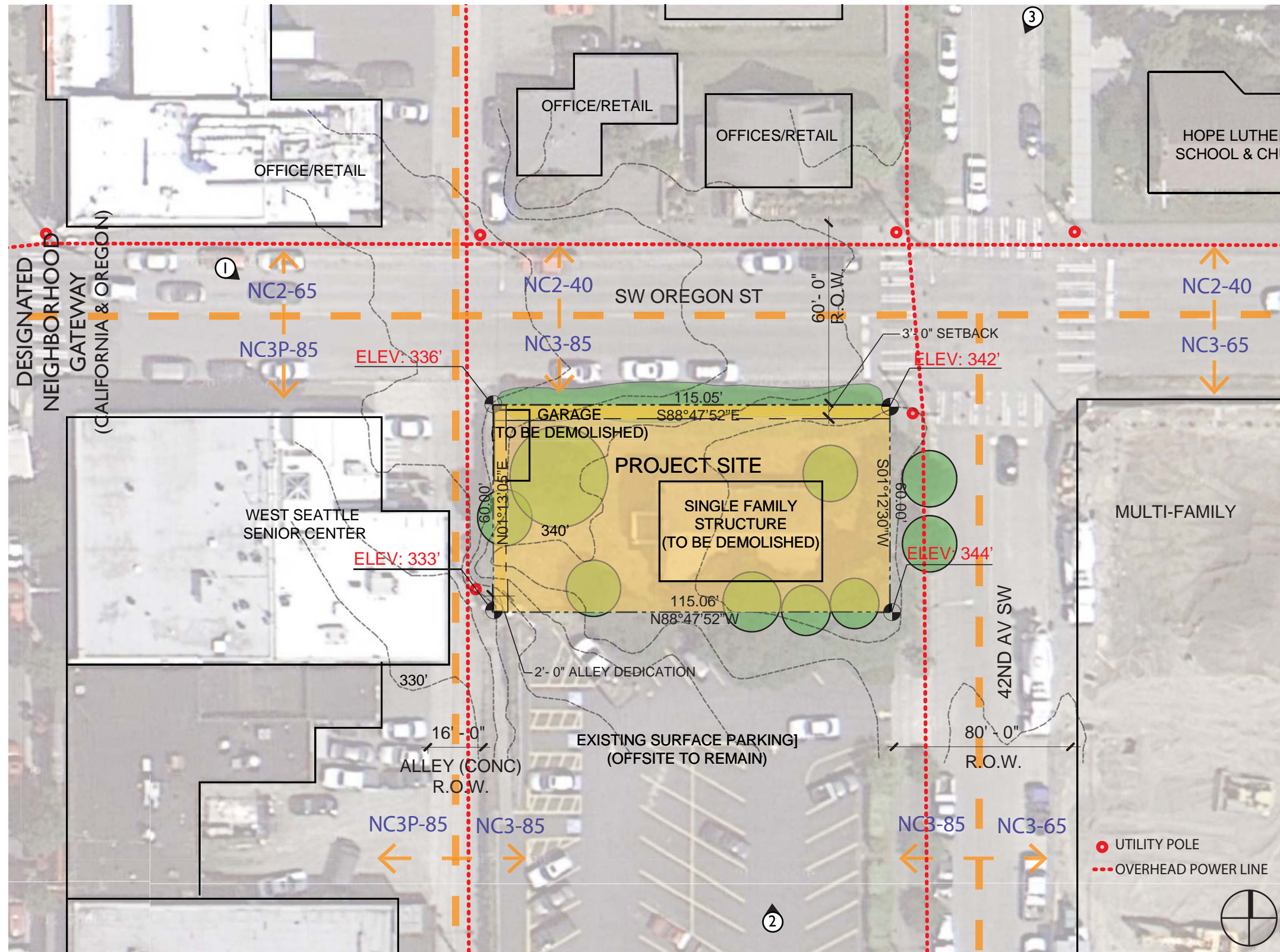
⑪ LINK APARTMENTS



⑫ NOVA APARTMENTS

SITE INFORMATION

EXISTING SITE PLAN



① PROJECT SITE LOOKING EAST UP SW OREGON ST



② PROJECT SITE LOOKING NORTH ACROSS PARKING LOT



③ PROJECT SITE LOOKING SOUTH ON 42 ND AVE SW
NOTE: BUILDING ENVELOPE IS APPROXIMATELY 85' IN ABOVE IMAGES

EARLY DESIGN GUIDANCE

PREFERRED EDG OPTION

Podium level is distinguished by material and physical offset



Different finish treatment separates building sections

Special treatment, such as higher roofline and overhang, at corner

Transparent corner

Varied roofline accentuates corner

Cornice strengthens stepping form

Bay windows articulate facade and provide prominence to corner

Continuous canopies on Oregon and 42nd

Retail entrances on both Oregon and 42nd

Wide sidewalks allow for retail use to spill out and activate pedestrian experience

Retail and residential entrances are inset



STREETVIEW LOOKING SOUTHWEST ON 42ND AVE SW

DISTINGUISHING FEATURES

- 36 residential flats, 9 mezzanine units, 2 retail spaces, and 17 parking stalls
- 48,700 gsf: 36,500sf residential, 4,300sf retail
- Large Retail space accessed from the Oregon St sidewalk
- Main building/lobby entrance from north side of site (Oregon)
- Setback mezzanine units at top level
- Bicycle storage area in garage
- Naturalized tree planting between building and parking lot
- Rooftop amenity area

PROS

- Retail space at grade provides a more active streetscape and connects new commercial development to the east with businesses on California.
- Strong corner presence on 42nd and Oregon.
- Power lines along alley moves underground improving neighborhood sight lines.
- Provides large inset area on South facade with windows and door to parking
- Steps back upper level to allow clerestory windows facing south, exterior space for residents, and relieves massing.

CONS

- 14' tall retail space
- Retail is 24" +/- below grade at 42nd Avenue
- Subdued residential entry
- Subdued relationship to California (Gateway Intersection)



GROUND LEVEL

EARLY DESIGN GUIDANCE

EDG BOARD COMMENTS

1 MASSING AND CORNER TREATMENT

- Encourage the building to make more of a statement:
- Provide a stronger podium
- Provide a strong design at the corner
- Design the building with more height at the corner
- Consider a taller tower
- Treat the tower differently
- Continue the stair tower design as shown in option 3
- Continue the massing and glazing along south elevation
- Maintain the building cap
- Consider the massing of option 1 with a tower



2 PEDESTRIAN EXPERIENCE

- Encouraged more work on the overhangs
- Encouraged more work on the relationship between retail and sidewalk



3. SOUTH ELEVATION

- Encourage the applicant to design stair tower as shown in Option 3
- encourage massing and glazing on South elevation



RESPONSE TO GUIDANCE

DESIGN GUIDELINE PRIORITIES

A. SITE PLANNING

A-2 Streetscape Compatibility

The siting of the building should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.

West Seattle Junction Supplement:

A pedestrian oriented streetscape is one of the most important characteristics to be achieved in new development in the Junction's mixed use areas. New development, particularly on SW Alaska, Genesee, Oregon, and Edmunds Streets, will set the precedent in establishing desirable siting and design characteristics in the right-of-way.

Considerations:

- Reduce the scale of the street wall with well-organized commercial and residential bays and entries, and reinforce this with placement of trees, drop lighting on buildings, benches and planters.
- Provide recessed entries and ground-related small spaces as appropriate breaks in the street wall.

Response

- The proposed project provides well organized bays and balconies on levels 2-5, and sets back the upper levels to provide relief and rhythm along Oregon. Lighting and canopies reinforce the break up of the streetscape in a pedestrian fashion that emphasize the interior uses and provide variety. Planting strips and trees are organized in a fashion to accentuate the corner element and coordinate with the facade.
- Entries along Oregon are recessed and sized to reinforce the different entry types.

A-4 Human Activity

New development should be sited and designed to encourage human activity on the street.

West Seattle Junction Supplement:

An active and interesting sidewalk engages pedestrians through effective transitions between the public and private realm.

Particularly in the California Avenue Commercial Core, proposed development is encouraged to set back from the front property line to allow for more public space that enhances the pedestrian environment. When such a setback is not appropriate or feasible, consider maximizing street level open space with recessed entries and commercial display windows that are open and inviting.

Response

- The project proposes to utilize the surprisingly large ROW area facing 42nd Avenue SW. The current sidewalk (approximately 6' wide) will be widened significantly enough to include two planting areas and a change in paving to define potential areas for future tables and chairs. The total distance from curb to face of building is approximately 24'.
- Both ground floor facades maximize commercial glazing and are intended to facilitate active retail/commercial experiences.

- The pedestrian experience along Oregon will be change dramatically from a narrow sidewalk adjacent to the street, to a sidewalk similar to those on California Street and in keeping with conditions as the street continues East. Street trees and planters will buffer pedestrians from traffic and the sidewalk will extend to the building face, three feet beyond the property line.



SIDEWALK AT OREGON 42



SIDEWALK ALONG CALIFORNIA

A-10 Corner Lots

Building on corner lots should be oriented to the corner and public street fronts. Parking and automobile access should be located away from corners.

West Seattle Junction Supplement:

Pedestrian activities are concentrated at street corners. These are places of convergence, where people wait to cross and are most likely to converse with others. New development on corner lots should take advantage of this condition, adding interest to the street while providing clear space for movement. New buildings should reinforce street corners, while enhancing the pedestrian environment.

Public space at the corner, whether open or enclosed, should be scaled in a manner that allows for pedestrian flow and encourages social interaction. To achieve a human scale, these spaces should be well defined and integrated into the overall design of the building

Considerations:

- Provide seating
- Incorporate art that engages people
- Setting back corner entries to facilitate pedestrian flow and allow good visibility at the intersection

Response

- The proposed project engages the corner with a larger canopy cover and utilizes the large sidewalk area as an open space. The corner of the building has the largest windows and is meant to be the focal point of the building.

B. HEIGHT, BULK AND SCALE

B-1 Height, Bulk, and Scale Compatibility

Projects should be compatible with the scale of development anticipated by the applicable Land Use Policies for the surrounding area and should be sited and designed to provide a sensitive transition to less intensive zones nearby. Projects on zone edges should be developed in a manner that creates a step in perceived height, bulk, and scale between anticipated development potential of the adjacent zones.

West Seattle Junction Supplement:

Current zoning in the Junction has created abrupt edges in some areas between intensive, mixed-use development potential and less-intensive, multifamily development potential. In addition, the Code-complying building envelope of NC-65' (and higher) zoning designations permitted within the Commercial Core would result in development that exceeds the scale of existing commercial/mixed-use development. More refined transitions in height, bulk and scale—in terms of relationship to surrounding context and within the proposed structure itself—must be considered.

Response

- The project provides a more sensitive transition between the NC-65 and the NC-85 zones by limiting itself to 70'. Future adjacent development will likely exceed the height of the building.

C. ARCHITECTURAL ELEMENTS AND MATERIALS

C-2 Architectural Concept and Consistency

Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept. Buildings should exhibit form and features identifying the functions within the building. In general, the roofline or top of the structure should be clearly distinguished from its facade walls.

West Seattle Junction Supplement:

New multi-story developments should employ methods that integrate the building's upper and lower levels. The levels of the building should function as a composition – not necessarily requiring the top and bottom to be identical, but rather extending or repeating elements throughout the facade.

Response

- The facades have been treated differently on the four sides of the building. Each facade intends to make connections both horizontally and vertically, often reinforcing the various uses through alignment and material. Although the size of the openings vary, they are intended to provide a harmonious pattern in conjunction with finish materials, setbacks, and color.

RESPONSE TO GUIDANCE

DESIGN GUIDELINE PRIORITIES

C-3 Human Scale

The design of new buildings should incorporate architectural features, elements and details to achieve a good human scale.

West Seattle Junction Supplement:

Facades should contain elements that enhance pedestrian comfort and orientation while presenting features with visual interest that invite activity. Overhead weather protection should be functional and appropriately scaled, as defined by the height and depth of the weather protection. It should be viewed as an architectural amenity, and therefore contribute positively to the design of the building with appropriate proportions and character.

Response

- Overhead weather protection is provided above both sidewalks approximately 7' deep. This will allow space for street trees to grow and still provide pedestrians with protection from rain and lighting at night. Tucking the canopies under the overhanging bays and decks provides for a unified experience along the street as the entrances and storefronts respond to interior changes. The corner canopy is slightly deeper and slightly higher to add prominence to the corner.



D-10 Commercial Lighting

Appropriate levels of lighting should be provided in order to promote visual interest and a sense of security for people in commercial districts during evening hours. Lighting may be provided by incorporation into the building façade, the underside of overhead weather protection, on and around street furniture, in merchandising display windows, in landscaped areas, and/or on signage.

Response

- Wall mounted lights are planned on each brick 'pilaster' along the sidewalk to provide a consistent, attractive level of lighting that promotes safety and walkability.



D. PEDESTRIAN ENVIRONMENT

D-1 Pedestrian Open Spaces and Entrances

Convenient and attractive access to the building's entry should be provided. To ensure comfort and security, paths and entry areas should be sufficiently lighted and entry areas should be protected from the weather. Opportunities for creating lively, pedestrian-oriented open space should be considered.

Response

- Recessed entries have been located along the sidewalk at convenient locations and are to be well lit.

D-2 Blank Walls

Buildings should avoid large blank walls facing the street, especially near sidewalks. Where blank walls are unavoidable they should receive design treatment to increase pedestrian comfort and interest.

Response

- There are no blank walls facing the sidewalk.

D-9 Commercial Signage

Signs should add interest to the street front environment and should be appropriate for the scale and character desired in the area.

Response

- Although the number and exact appearance of the signs is as of yet unknown, small blade signs hanging from the canopy will be encouraged.

D-11 Commercial Transparency

Commercial storefronts should be transparent, allowing for a direct visual connection between pedestrians on the sidewalk and the activities occurring on the interior of a building. Blank walls should be avoided.

Response

- The ground floor of the building, along the sidewalk, is intended to maximize the area of glazing to promote visual connections from the street into the retail spaces. Glazing has been designed to avoid divisions that interfere with views out of the space and the natural grade of the sidewalk allows a variety of perspectives into retail spaces. There are no blank walls facing the sidewalk.

E. LANDSCAPING

E-2 Landscaping to Enhance the Building and/or Site

Landscaping, including living plant material, special pavements, trellises, screen walls, planters, site furniture, and similar features should be appropriately incorporated into the design to enhance the project.

Response

- The project incorporates a variety of planting areas suited to their specific place on the site. Street tree planting areas incorporate low growing plantings allowing easy connections to the street. The area along 42nd utilizes paving types and planters with larger plants to create a more private space protected from the parking lot and Oregon street that will serve well for small tables and chairs, if a cafe or restaurant leases the corner space.



RESPONSE TO GUIDANCE

RESPONSE TO EDG BOARD RECOMMENDATIONS

1. MASSING AND CORNER TREATMENT

a The board preferred the one story podium.

The proposed design enhances the distinction between the one story retail podium and the building above through the use of changing wall planes, a solid canopy, and a change in the relationship between glazed and solid.

b The board encouraged the design to make more of a statement, particularly at the corner.

The proposed design utilizes color and form to give the building identity. The building has been separated into five distinct pieces, each responding to its particular position. The base of the building relates to the pedestrian and retail needs; the primary corner is tall, sleek, glazed, and colorful; secondary corners are brick with punched windows, the middle of the building adds light and movement with bay window type modulation, and the top floors are set back in a neutral grey to allow the primary corner to appear taller. Overall, the building projects a residential feel compatible with both the newer development and the existing fabric of the neighborhood.

c Design the building with more height at the corner

The tower at the corner maximizes the allowable building height. Setting back and changing the color of the upper levels allows the corner tower to appear taller.

d Tower facades should be treated differently along SW 42nd Street and SW Oregon Street.

The design has evolved such that the entirety of each street facade responds to that street, and the tower is the hinge that binds them. The treatment of the tower is consistent along both street facades, addressing the corner as a singular element.

e The board encouraged the applicant to design the stair tower as shown in Option 3 and continue the massing and glazing along the south elevation.

The south facade of the building has been a challenge. Although the general concept of the south facade remains the same, the configuration has changed. The orange material of the corner tower is used to accentuate the inset stair tower. Decks have been included on this side of the building to activate the facade, and a portion of the building near 42nd has been pulled back to allow for windows.

f Maintain the Building cap as shown in option 3.

The building cap remains as shown, with the guard rail of the rooftop amenity area set back from the edge quite some distance. It is the intent of the design that the rooftop amenity not be seen from the street.

g Consider a higher tower.

The corner tower has been designed to compliment the building.

h Consider the massing and design of Option 1 with a taller tower.

The proposed design takes the active bay design of Option 1 and melds it with the taller tower of Option 3.

2. PEDESTRIAN EXPERIENCE

a The proposed structural building overhangs are suitable if the sidewalk is 17'.

The proposed design eliminates structural building overhangs, pulling the building back to the property line. The distance between the first floor and the curb is approximately 13'. The proposed walking area on Oregon is approximately 7' next to an additional 5' of planter. Comparable sidewalks on California provide between 4' and 6' of walking area. Street trees, benches, and other landscaping features occupy up to an additional 5' of width. The overhead pedestrian protection along 42nd is shown at a minimum of 8' above the sidewalk and extends substantially over the sidewalk.

b Encouraged more work on overhangs and the relationship of the retail space to the streets.

The proposed design includes broad expanses of glazing, inset entries, and flexible retail space. The canopies along 42nd provide pedestrian protection, light at night, and create a familiar, comfortable setting for window shoppers.

c Provide information about signage and lighting.

A lighting plan is included in this packet. Overall, the lighting is meant to be subdued. Down lights and wall mounted sconces under the canopy will provide a sense of safety at night. Lighting will punctuate entries and accent the corner. Lighting along the alley will be directed downward to provide security.

Signage is intended to be directed at the pedestrian. Blade signs and window identification will indicate retail spaces and a low scale, integrated building name will be located near the residential entry. The building address will be integrated into the canopy at the corner of 42nd and Oregon.

3. SOUTH ELEVATION

a The board encouraged the applicant to design the stair tower as shown in Option 3 continuing the massing and glazing along the south elevation.

As previously noted, the south facade of the building has been a challenge. Materials and patterns from the primary facades wrap the corners and transition to a more utilitarian material. The set back massing continues from the primary facade, allowing for windows in the upper levels. Decks face south in the stair tower setback, which has increased significantly, and additional relief is provided at Oregon as the brick facade turns the corner.

DEPARTURES

No departures are sought.



Concept Sketch of 42nd Ave SW Corner Pedestrian Experience

- Define corner by creating edges with planters and the building edge
- Align existing sidewalk with walkway as separate from seating area
- Accent the tower by bringing the accent color to the ground level
- Differentiate the corner from typical retail with elevated canopy
- Downlight at corner to provide additional security and emphasis
- Provide brick facade to ground level for continuity
- Provide large retail glazing; contrast with punched residential windows

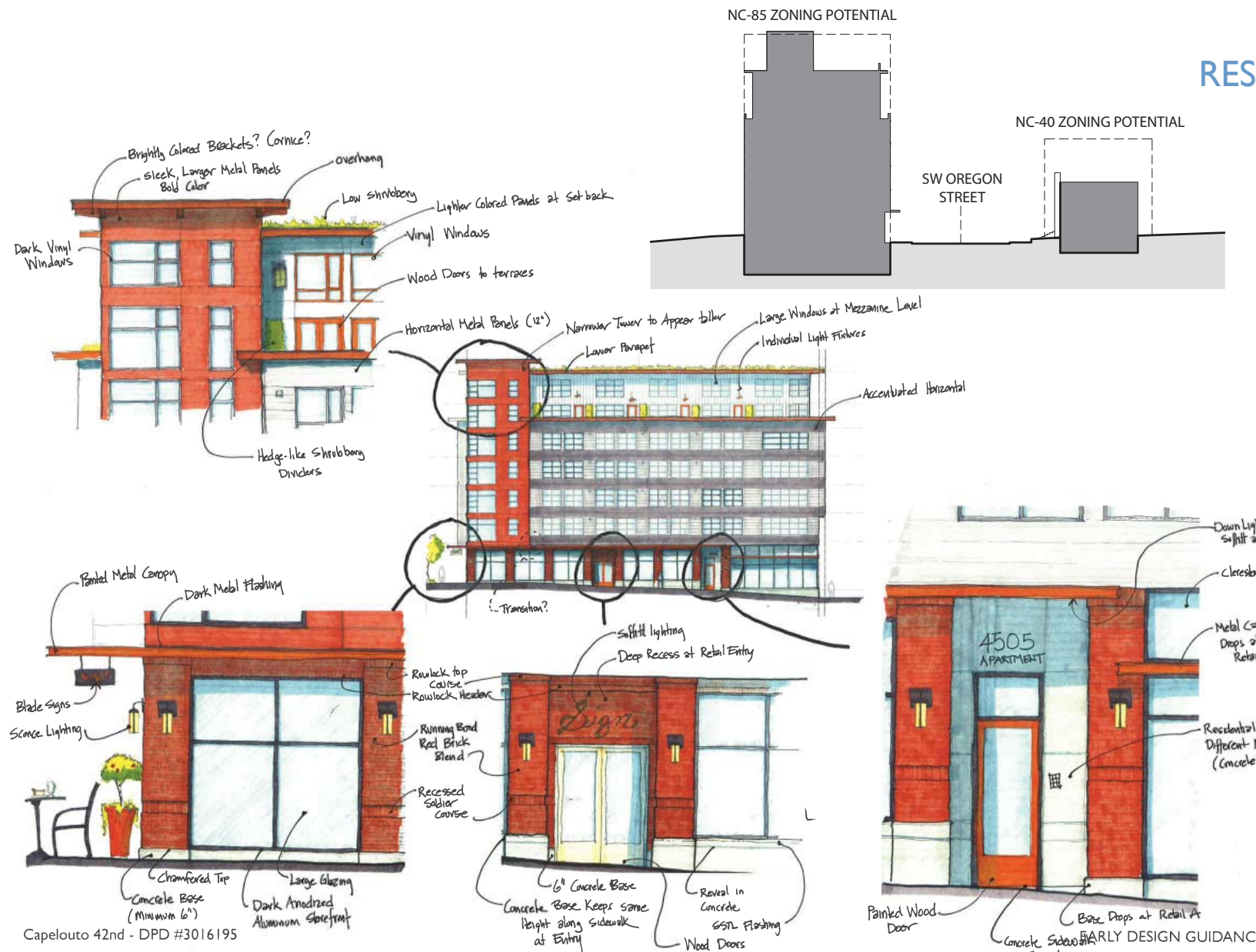


Concept Sketch of 42nd Ave SW Retail

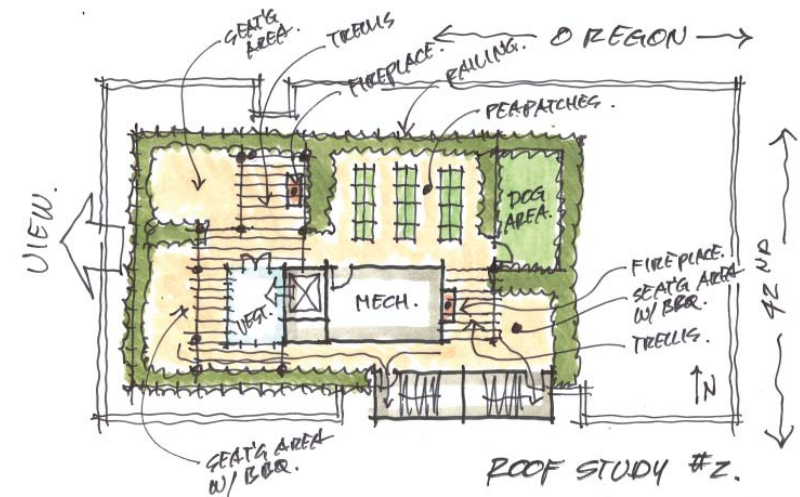
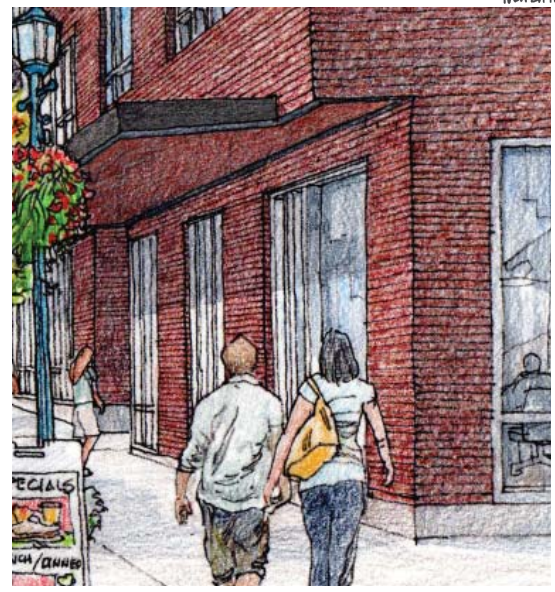
- Separate retail and sidewalk with plantings and access
- Minimize apparnt grade separation with low sill (30" on inside)
- Provide access to street level

DESIGN PROCESS

RESPONDING TO EARLY DESIGN GUIDANCE - DESIGN PROCESS



- Concept sketches from EDG
- Give the tower more presence with contrasting and set back materials
 - Set the tower apart from ground to cornice
 - Provide a harmonious and continuous vocabulary
 - Investigate proportions of setbacks
 - Investigate where to highlight with color
 - Enhance the single story plinth through use of canopies and color
 - Investigate signage
 - Differentiate various entries and uses
 - Provide appropriate lighting and amenities
 - Revise the level of detail



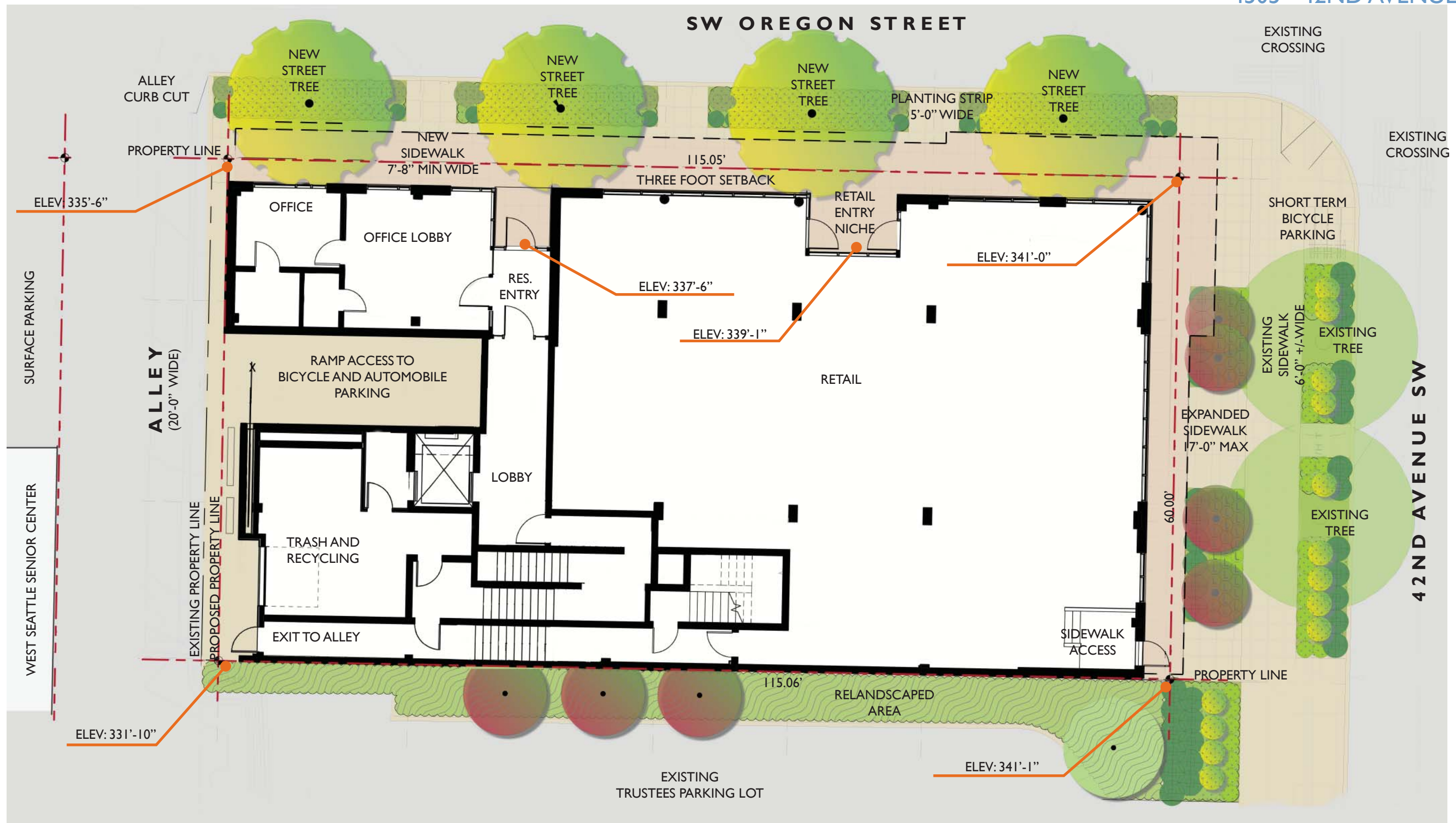
DESIGN PROPOSAL

CORNER RENDERING



COMPOSITE SITE PLAN

4505 - 42ND AVENUE SW



BUILDING ELEVATIONS

EAST ELEVATION



Glass Roof Railing



Black Metal Juliet Balcony



Vinyl Windows: White



Brick on Concrete



Metal Panel System: Orange



Metal Panel System: Grey



Sconce lighting



Metal Storefront: Clear Anodized

BUILDING ELEVATIONS

NORTH ELEVATION



Wood Soffit



Vinyl Windows: White



Canopy: Black Frame



Metal Storefront: Clear Anodized



Metal Panel System: Grey



Sconce lighting



Brick: Forest Blend



Brick on Concrete

BUILDING ELEVATIONS

WEST ELEVATION



Glass Roof Railing



Glass Juliet Balcony



Vinyl Windows: White



Brick on Concrete



Metal Panel System: Orange



Metal Panel System: Grey



Rolling Gate: Black



Overhead Door: Grey

BUILDING ELEVATIONS

SOUTH ELEVATION



Metal Panel System: Orange



Glass Roof Railing



Metal Panel System: Grey



Metal Panel System: White



Sconce lighting



Brick: Forest Blend



Canopy: Black Frame



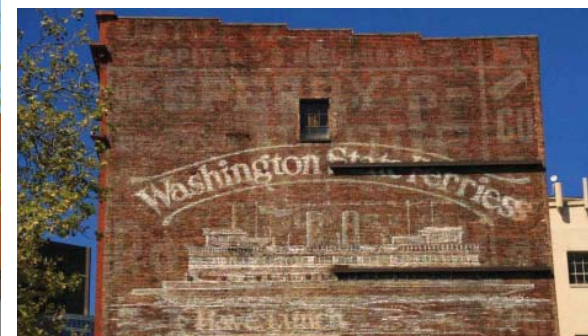
Concrete: Cast-in-Place

DESIGN PROPOSAL

SOUTH FACADE



Looking up at the corner of the southwest building



UNPRETENTIOUS, YET CHARMING GRAPHICS ON A TERTIARY FACADE.



View of the southeast corner of the building from the alley

DESIGN PROPOSAL

MATERIALS BOARD

Proposed Materials

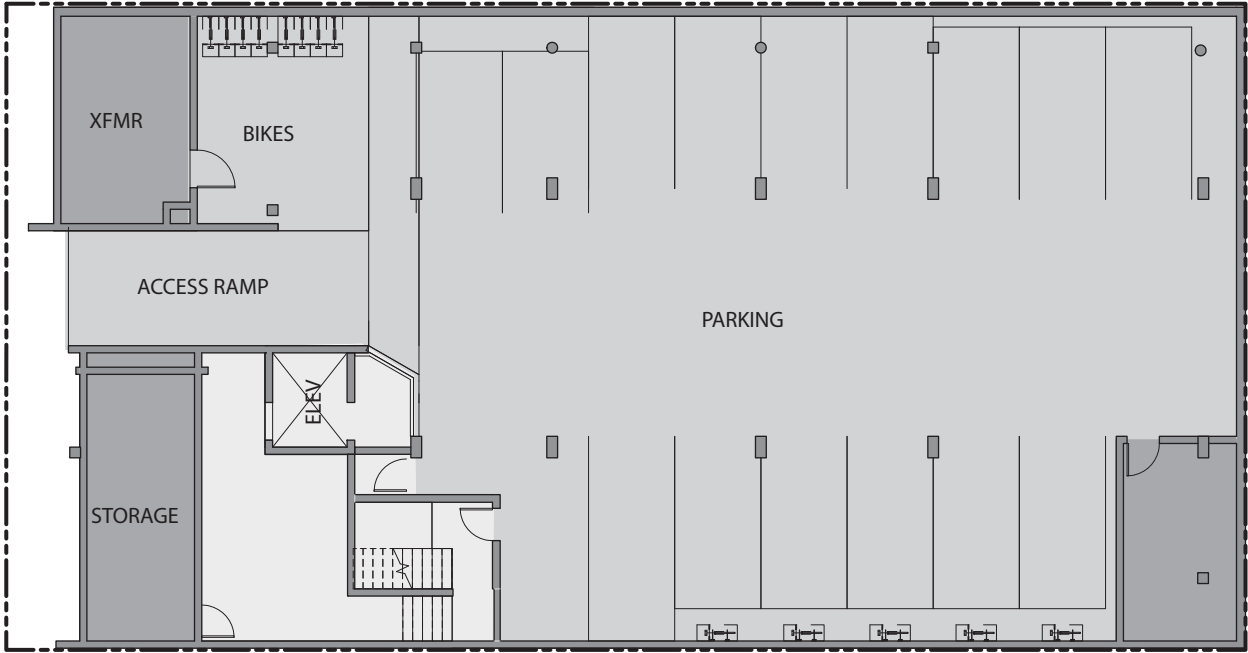
1. Stained Wood
 - underside of soffi
 - residential doors
2. Metal Panel Grey
3. Metal Panel : Orange
4. Metal panel :White
5. Metal roofing: Black
 - canopies
6. Paint: "jack Black" (Black)
 - railings
 - miscellaneous
7. Storefront: Clear Anodized
8. Vinyl Windows: White
9. Brick: Forest Blend, Mission



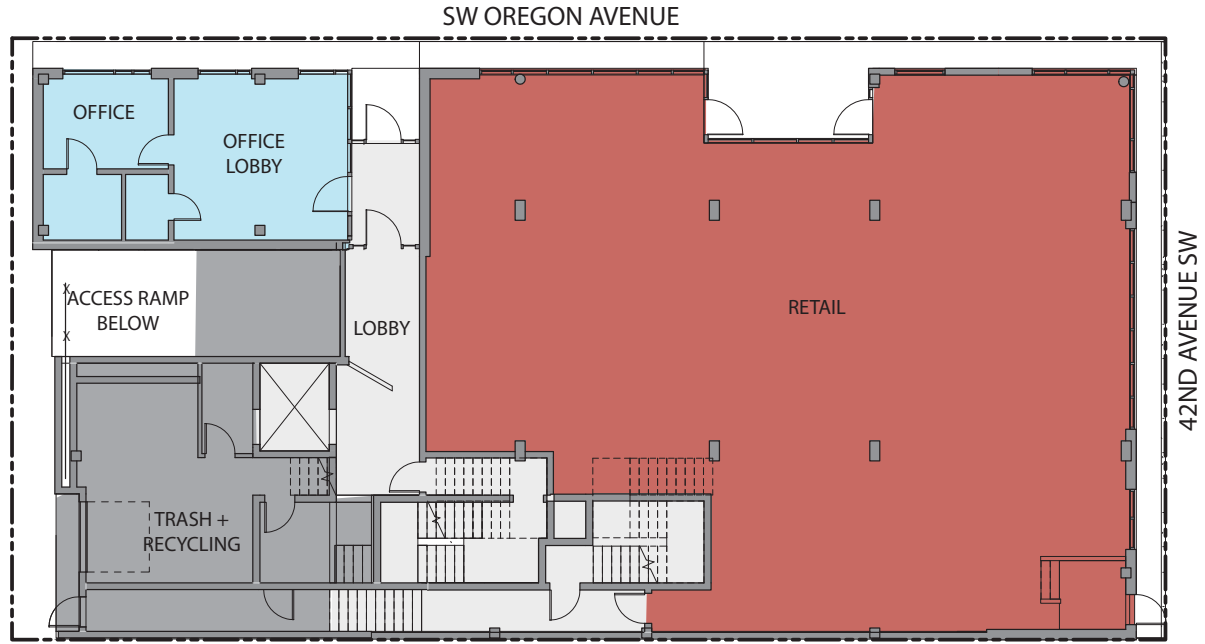
FLOOR PLANS

LOWER LEVELS

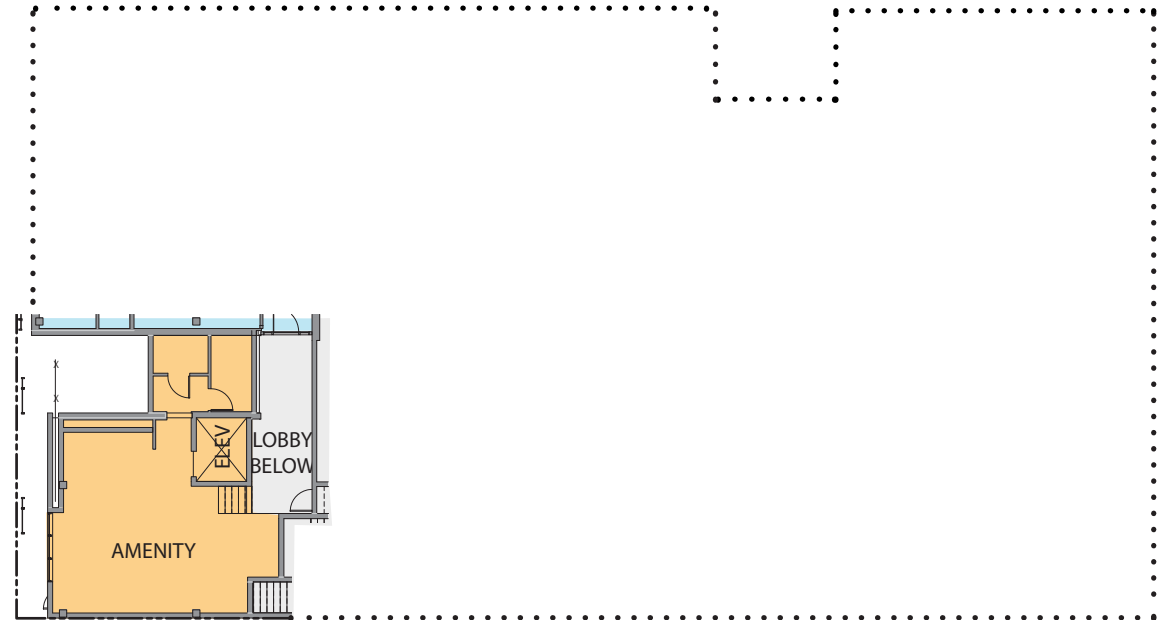
LEVEL P1
PARKING GARAGE AND STORAGE



LEVEL I
RETAIL, RES. ENTRY + SERVICE



LEVEL IM
MEZZANINE AMENITY ROOM



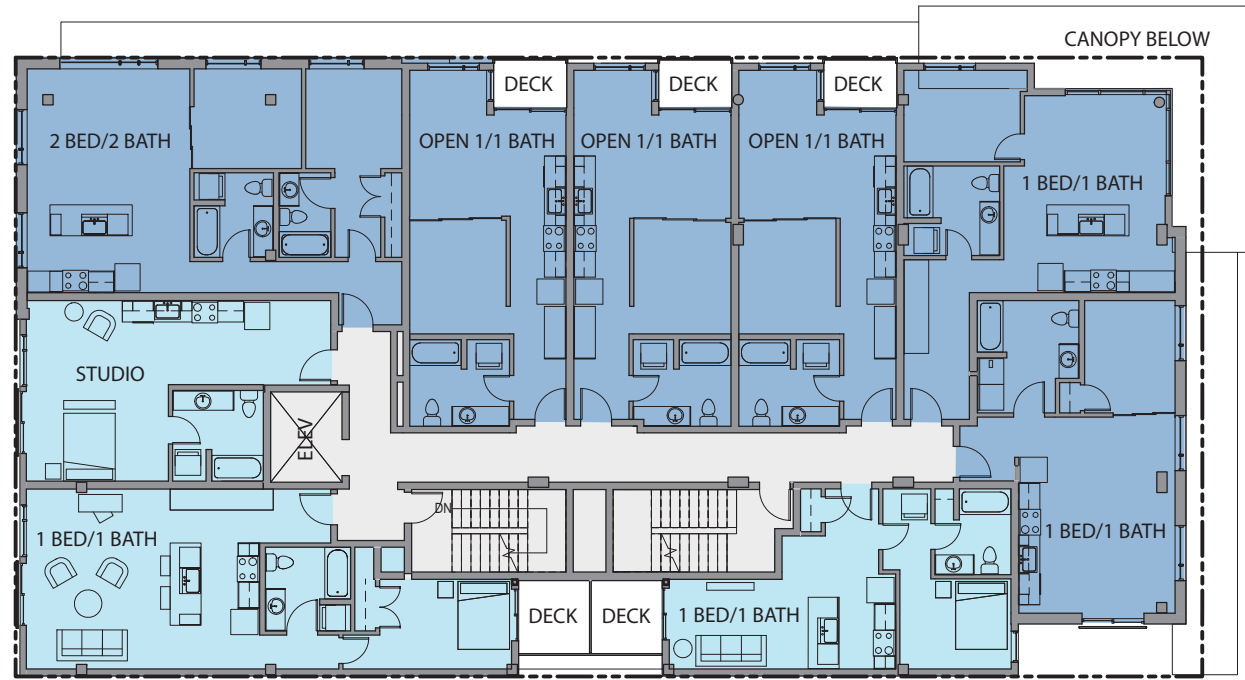
FLOOR PLAN KEY

	UTILITY
	PARKING
	CIRCULATION
	RETAIL
	RESIDENTIAL
	LODGING
	GREEN ROOF

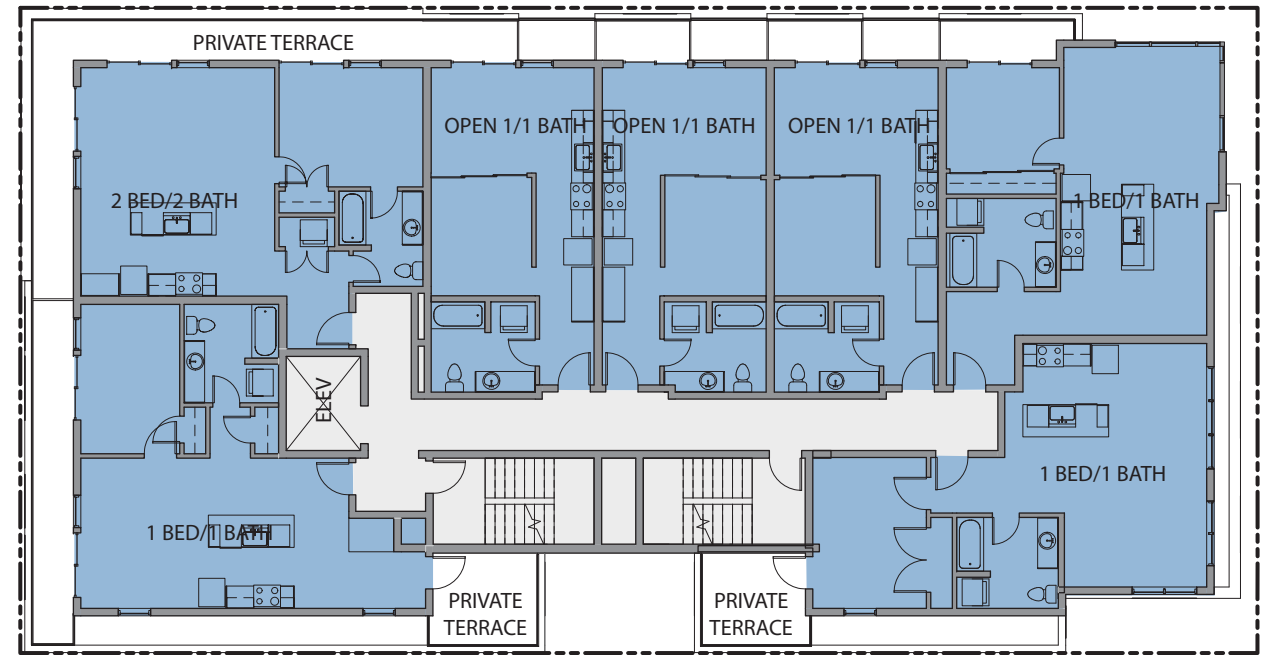
FLOOR PLANS

UPPER LEVELS

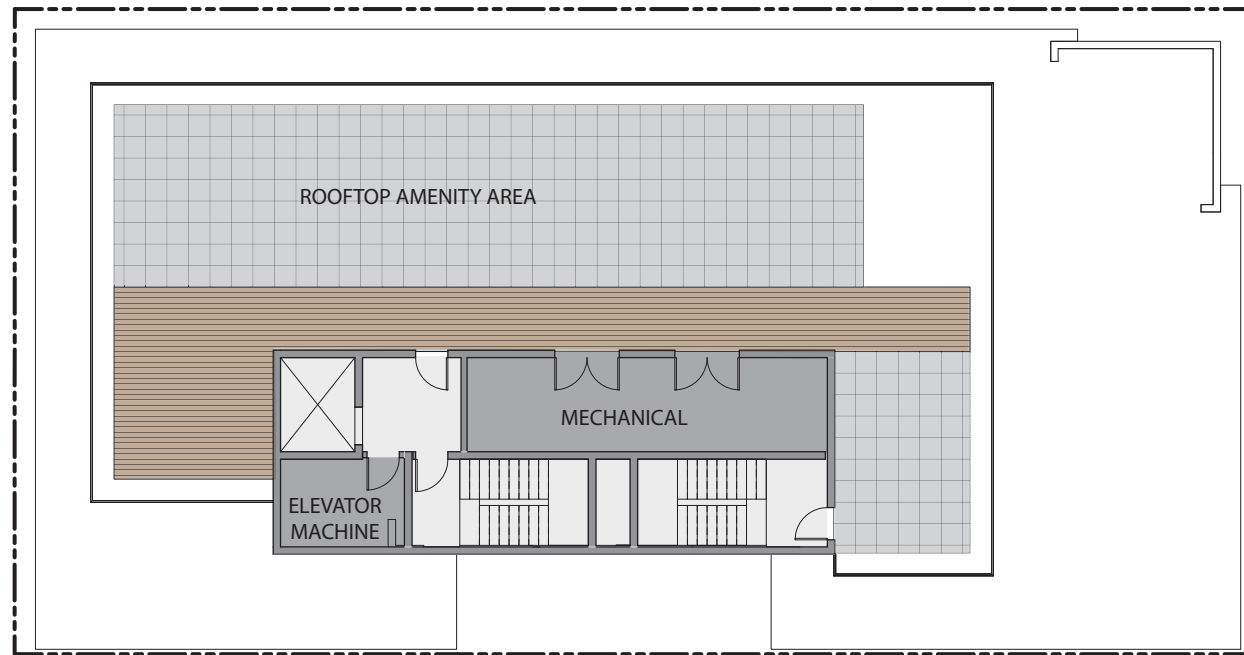
LEVELS 2-5
TYPICAL RESIDENTIAL FLOOR



LEVEL 6
RESIDENTIAL LEVEL (LEVEL 7 SIMILAR)



LEVEL 8
ROOFTOP AMENITY



FLOOR PLAN KEY

- UTILITY
- PARKING
- CIRCULATION
- RETAIL
- RESIDENTIAL
- LODGING
- GREEN ROOF



LANDSCAPING

STREET LEVEL PLAN



LANDSCAPING CONCEPT

The Junction has been and always will be a pedestrian-oriented community anchored by transit. Within this context, 42nd and Oregon are pedestrian-designated streets and form the edge of the 'plateau' and present distinct faces to the community. The project provides an enhanced pedestrian streetscape with building overhangs and widened sidewalks on Oregon in order to encourage pedestrian activity and easy access to California, the primary shopping street. The treatment is extended around the corner to 42nd Avenue SW to help define the "gateway" from the east. A well planted corner reinforces the pedestrian nature and provides a visual cue to the pedestrian gateway entry to the neighborhood.

At the 42nd Avenue / Oregon corner, there is a landscaped sidewalk buffer in place, which is further enhanced by the project. The landscape plan consists of street trees and complementary concentrated landscape treatment along the wide 42nd Avenue frontage.

Plantings are proposed to buffer the site from the parking lot to the south, and vice versa.

The Landscape for the roof identifies activity zones towards the NW, SW, and SE that provide the same elements – gathering area, seating, view zones. BBQ, raised garden beds and a screened dog run contribute to a vibrant year-round amenity zone.

At each level, the landscape is designed to look good year-round, with the majority of ground plantings broadleaf evergreens with some seasonal interest (spring flowering, fall coloring). Plants are tightly spaced to ensure complete ground coverage.



FURNISHINGS, PLANTS



SECLUDED LOUNGING



FIRE PIT



PLANTERS SET IN PEBBLES



ROOFTOP P-PATCH



CAFE SEATING



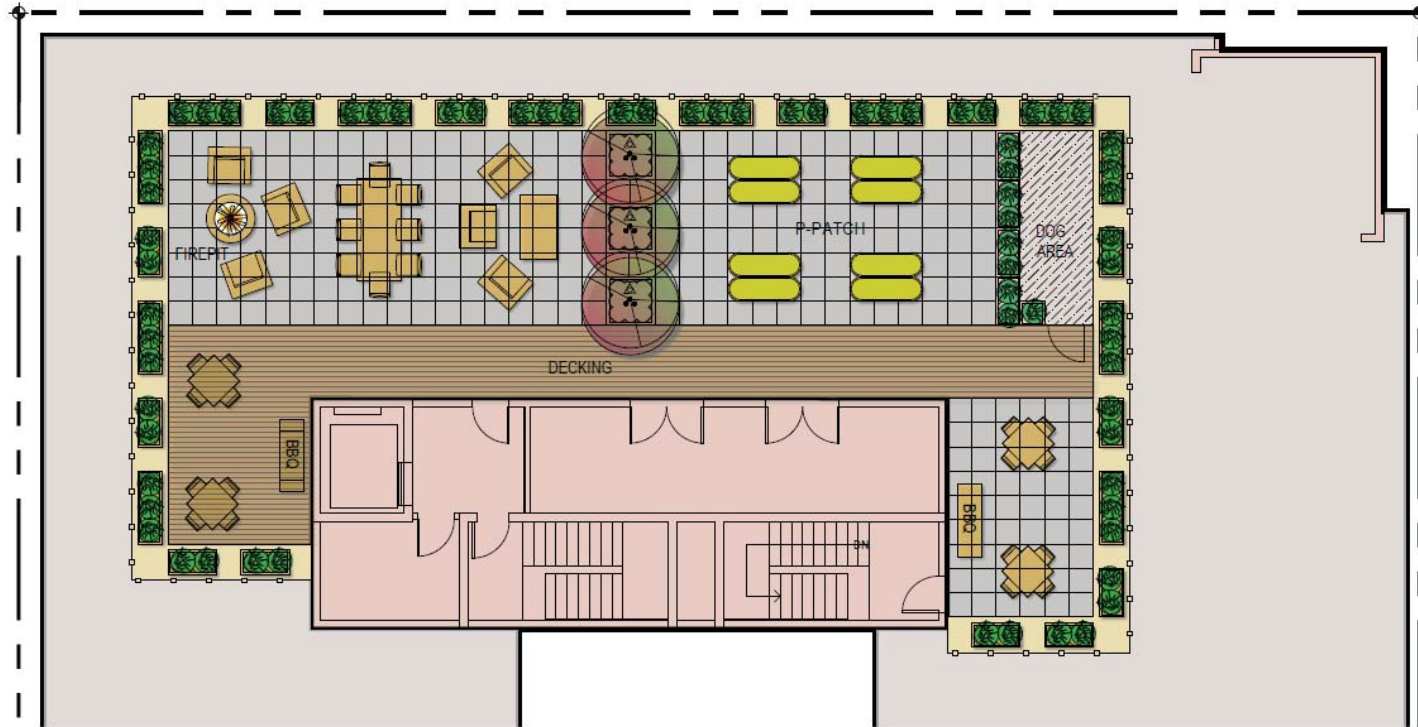
CONTINUOUS PLANTER



CEANOTHUS VICTORIA

LANDSCAPING

ROOF LEVEL PLAN



PARROTIA PERSICA
(ROOF)



NYSSA SYLVATICA
(STREET TREE)



ACER FREEMANII
AUTUMN BLAZE



CARPINUS BETULUS
FASTIGIATA



ROSA AMBER



CISTUS GRAYSWOOD PINK



HELLEBORUS ORIENTALIS



LONICERA PILEATA
AMBER ROSES



'LITTLE BUNNY'



PHYLLOSTACHYS-AUREA



ROSMARINUS OFFICINALIS



SPIREA JAPONICA
SHIROBANA



VIBURNUM DAVIDII

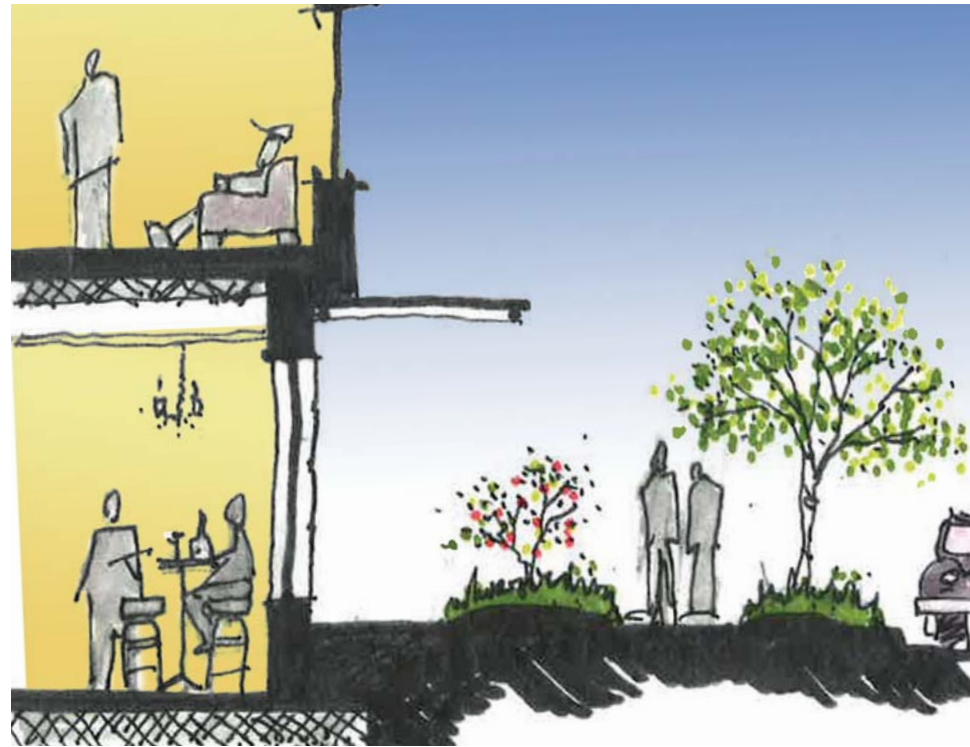
DESIGN FEATURES

42ND AVE SW STOREFRONT



GRADE DIFFERENTIAL

EXPLORING RETAIL WITH FLOORS BELOW THE SIDEWALK



PROPOSED RELATIONSHIP OF INTERIOR RETAIL GRADE WITH SIDEWALK ALONG 42ND INCLUDING (FROM RIGHT TO LEFT) :

- ROADWAY WITH PARKING
- 0'-6" CURB
- 5'-0" TREE WELL WITH LOW HEIGHT UNDER PLANTINGS
- 6'-0" +/- SIDEWALK
- 7'-0" +/- PLANTING BUFFER WITH MEDIUM HEIGHT PLANTINGS
- 5'-0" +/- ACCESS STRIP
- BUILDING WITH 6" SILL AND TALL WINDOWS
- RETAIL SPACE



EXAMPLE OF PLANTING AREA INSET INTO HARDSCAPE



BEER JUNCTION
4511 CALIFORNIA AVE SW

INTERIOR FLOOR LEVEL IS APPROXIMATELY 10" BELOW ADJACENT SIDEWALK

EXTERIOR INCLUDES TALL WINDOWS AND A LOW WALL BELOW



SHADOWLANDS
4458 CALIFORNIA AVE SW

INTERIOR FLOOR LEVEL VARIES BETWEEN 0" AND 18" +/- BELOW ADJACENT SIDEWALK AT WINDOWS ALONG OREGON



WEST SEATTLE COIN
1404 PINE STREET

INTERIOR FLOOR LEVEL VARIES BETWEEN 0" AND 24" +/- BELOW ADJACENT SIDEWALK

BARRIO
1420 TWELFTH AVENUE

INTERIOR FLOOR LEVEL VARIES BETWEEN 24" AND 36" +/- BELOW ADJACENT



POQUITOS
1000 PIKE STREET

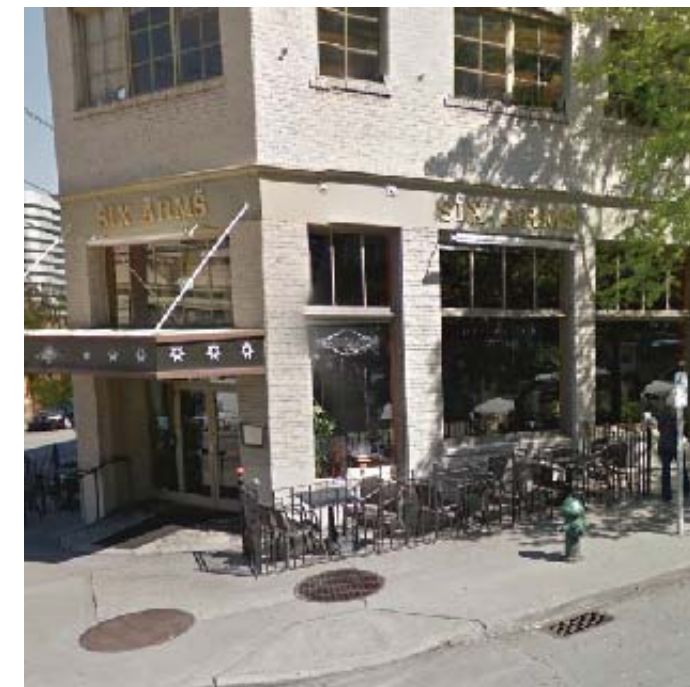
INTERIOR FLOOR LEVEL IS APPROXIMATELY 40" BELOW ADJACENT SIDEWALK



BELOW GRADE RETAIL

Below grade retail brings to mind dark, dank basement spaces with no connection to the street or sidewalk. Now, there are many instances where this is a benefit and becomes the defining characteristic of the space, such as Marcus' Martini Heaven in Pioneer Square. However, the "below grade" space in this project is approximately only 24" below the adjacent sidewalk. The primary entrance(s) off of Oregon are at grade in a rather large inset. large storefront windows begin at 30" above finished floor, typical of retail spaces.

Many successful businesses thrive with such a situation, and it is a common condition given Seattle's hilly topography. This page shows a variety of businesses with entrances at grade and a portion of the space with floors lower than the adjacent sidewalk. The feeling inside and out is comfortable and understandable; common and enjoyable.



SIX ARMS
300 E PIKE STREET

INTERIOR FLOOR LEVEL VARIES BETWEEN 12" AND 60" BELOW ADJACENT SIDEWALK. SIX ARMS ALSO HAS BOTH INTERIOR AND EXTERIOR SEATING ON EITHER SIDE OF THE WINDOW AT THIS LOCATION, BOTH OF WHICH ARE POPULAR SEATING OPTIONS.

DESIGN FEATURES

SW OREGON ST SW OREGON ROW SETBACK

The project is providing a three foot setback at the ground level per section 23.53.0153h. The setback is sized to provide adequate space for the sidewalk, planting strip, and streetscape amenities.

An Exception Request was made to the Department of Planning and Development, zoning, via Lori Swallows. The setback exception request is consistent with other projects in the vicinity.

The existing sidewalk feels particularly narrow as it is partially blocked by the overgrown laurel hedge and its proximity to the street.

The proposed sidewalk is sized to exceed those found on California Avenue SW and to exceed the sidewalk further east along SW Oregon Street. The project team also feels the sidewalk layout is superior to the existing sidewalk directly east of the site, in front of the West Seattle Senior Center, as it provides a wide walking area that is separated from the street by a five foot planting strip, with street trees, that can also accommodate street signage.

The request was granted with the following reasoning:

“The street is not developed to zoned capacity but the proposal is to set back the street level portion of the structure the required three feet and up to 10’ above grade which will allow for adequate vertical clearance and allow for the required sidewalk and planting strip to be provided.

The sidewalk provided will be adequate for pedestrian traffic.

The current roadway width is approximately 39’ which exceeds the 36’ required for the NC3 zones.”



SW OREGON STREET SIDEWALK EAST OF SITE - NOTE : THE PLANTING STRIP IS LESS THAN 4' WIDE AND THE 3' SETBACK IS USED FOR STORMWATER DETENTION AGAINST THE BUILDING



PROPOSED STREETScape WITH CURB, PLANTING STRIP AND WALKING SURFACE RIGHT UP TO THE BUILDING EDGE WITH OVERHEAD WEATHER PROTECTION

SIDEWALKS

EXPLORING NEARBY STREETSCAPES



TYPICAL CALIFORNIA AVENUE SW SIDEWALK

SW OREGON SIDEWALK AT SR CENTER - LOOKING EAST

NO OVERHEAD PEDESTRIAN PROTECTION BETWEEN CALIFORNIA AND 42ND ALONG OREGON

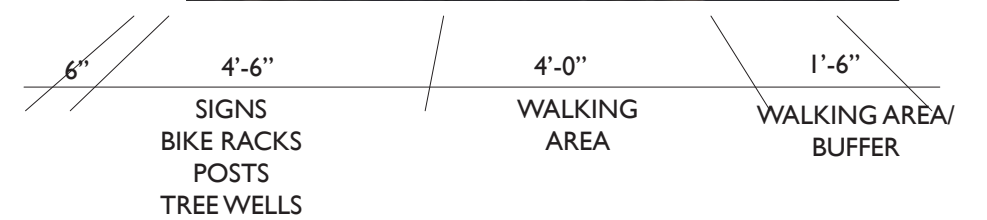
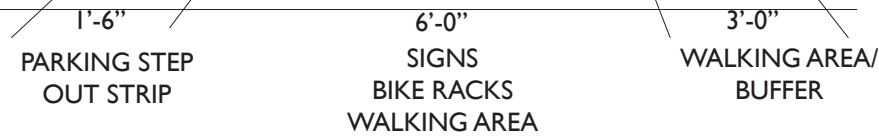


TREE WELLS LOCATED AT THE CURB NARROW THE SIDEWALK TO 6' +/-

BUILDING ENTRANCES EXTEND SIDEWALK



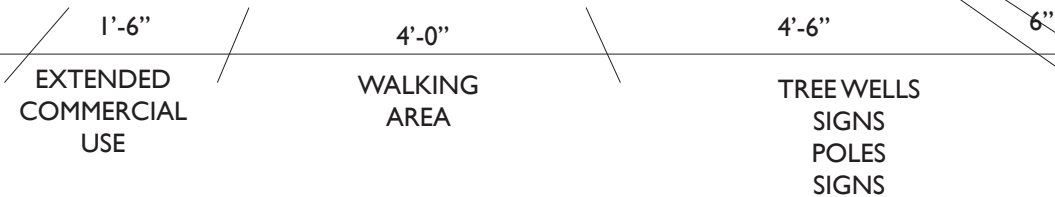
TYPICAL CALIFORNIA AVENUE SW SIDEWALK



TYPICAL CALIFORNIA AVENUE SW SIDEWALK - LOOKING SOUTH



BUSINESSES OFTEN SPILL OUT INTO STREET, UTILIZING A PORTION OF THE SIDEWALK, NARROWING THE WALK SPACE



EAST SIDE OF CALIFORNIA AVENUE SW

6'-0" +/- DEEP SOLID CANOPIES AT 8'-9" - 10'+/- ABOVE SIDEWALK



VILLAGE WOODWORKS
4538 CALIFORNIA AVENUE SW

INSET DOORWAYS PROVIDE RELIEF TO SIDEWALK AND ALLOW FOR CHANGES IN GRADE

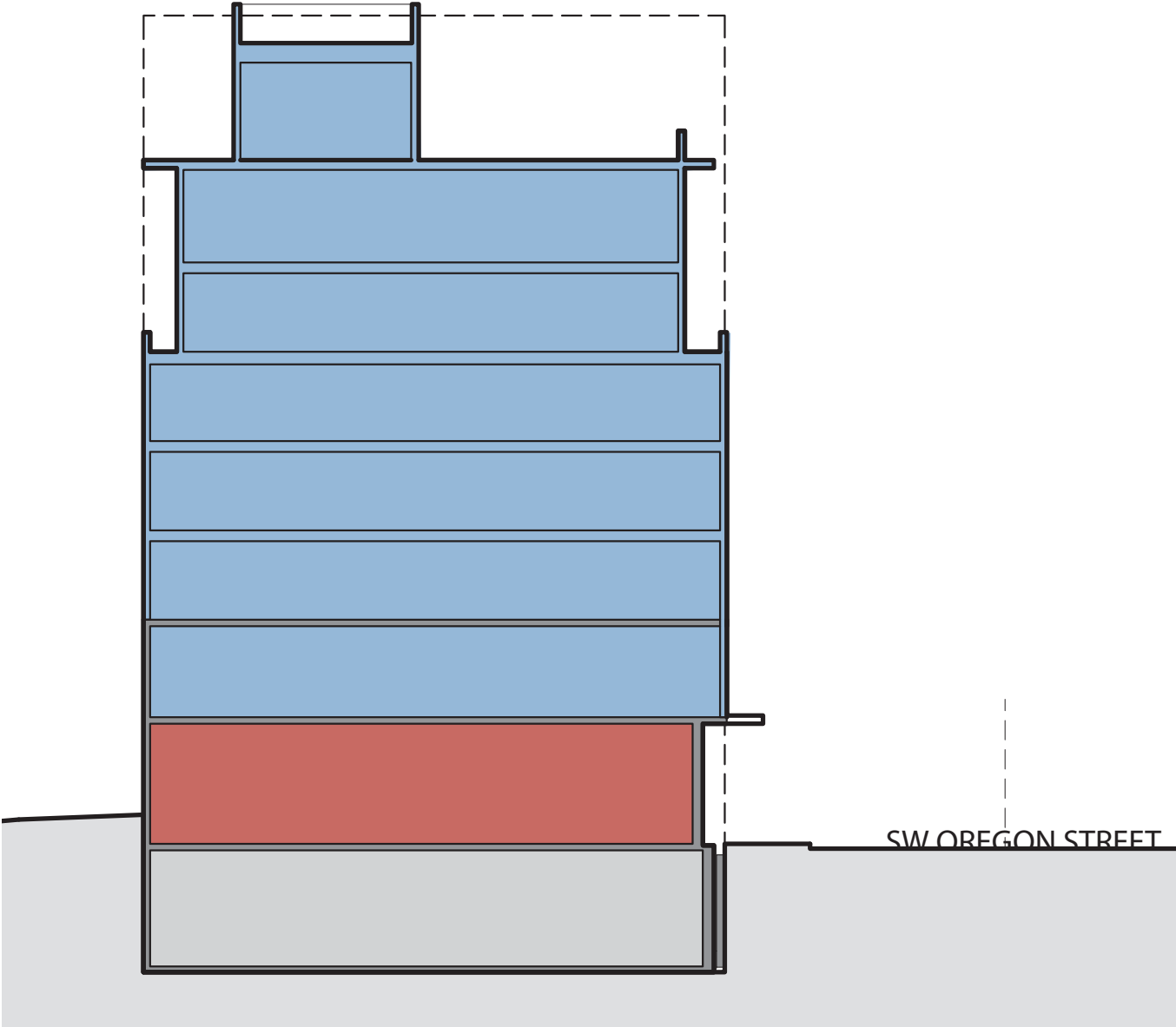


TRASH CANS NEXT TO CURB ALONG CALIFORNIA

DESIGN FEATURES

SECTIONS

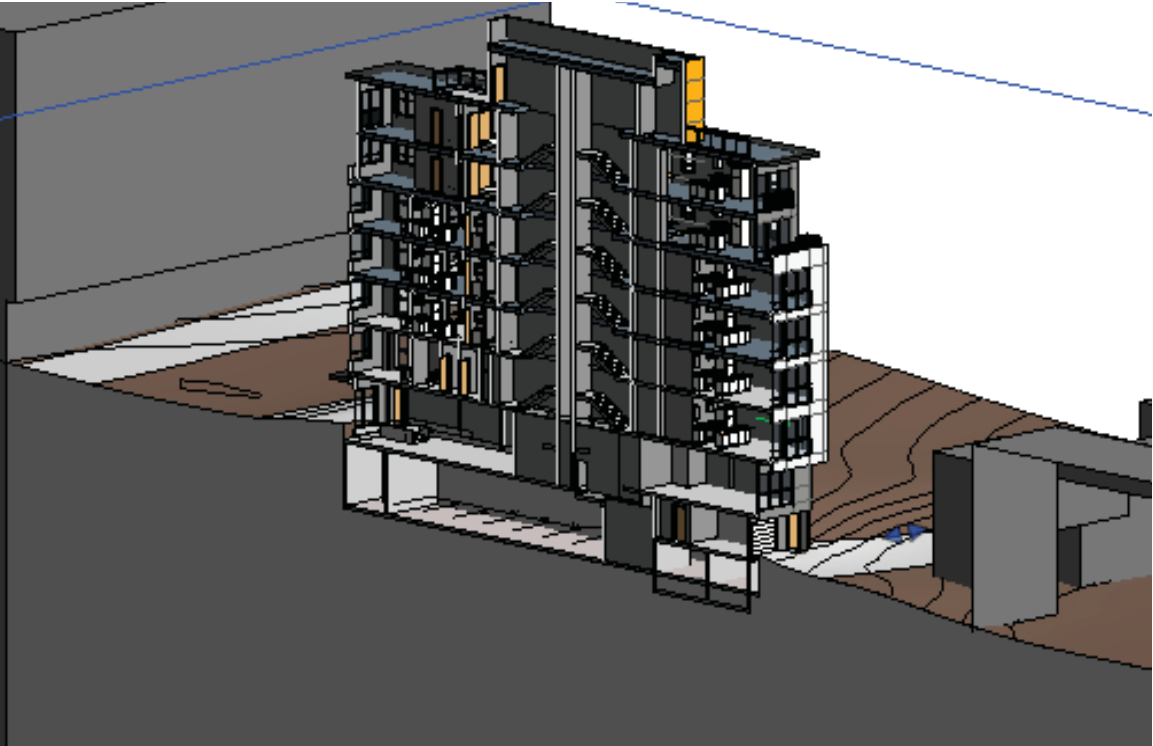
NC-85 ZONING POTENTIAL



BUILDING SECTION



BUILDING CUTAWAY THROUGH RETAIL



BUILDING CUTAWAY THROUGH STAIRS

DESIGN FEATURES

SIGNAGE

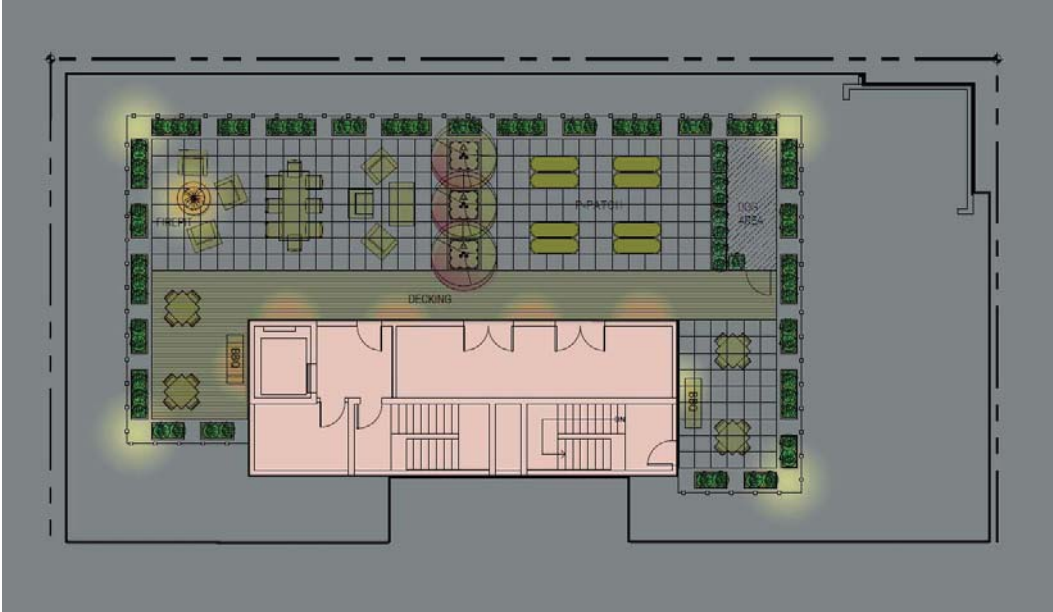


DESIGN FEATURES

EXTERIOR LIGHTING



GROUND FLOOR LIGHTING PLAN



ROOF LIGHTING PLAN



upper level wall mounted fixture



can light (canopy and entries)

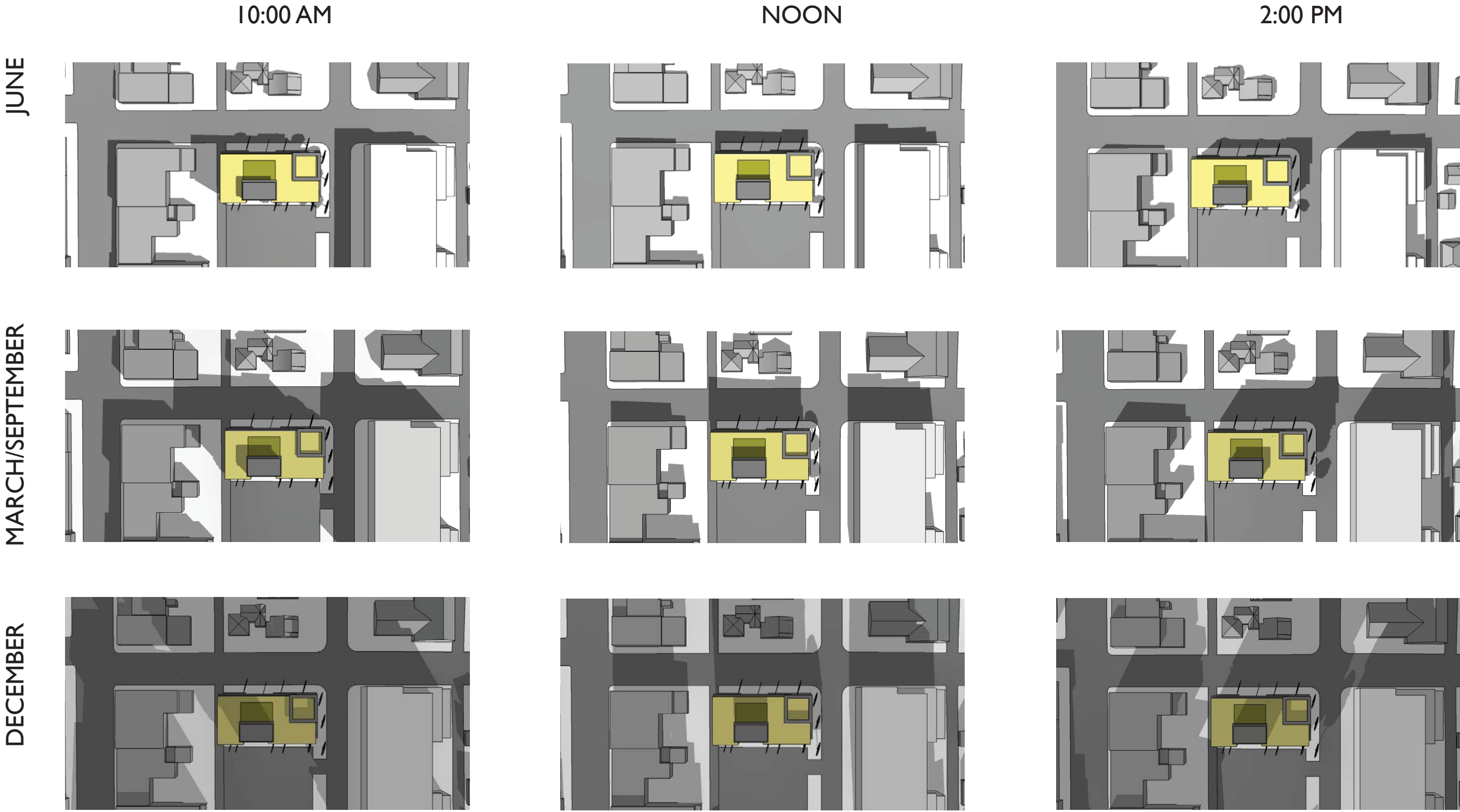


ground level wall mounted fixture



utility fixture (alley)

SHADOW STUDIES



DESIGN INSPIRATION



RECENT NK PROJECTS



WESTLAKE VILLAGE



DAKOTA



STREAM BELMONT



TRIAD 12TH



PARK PASSIVE: AIA NATIONAL HOUSING AWARD WINNER



H2O APARTMENTS - LEED-H PLATINUM TARGET



BROADSTONE KOI - LEED-NC CERTIFIED TARGET



ARTHOUSE



APERTURE - BUILT GREEN 3-STAR TARGET