



7016 15TH
AVE.NW

caron

PROJECT HISTORY

The Early Design Guidance meeting was held on January 13, 2014

The Master Use Permit was submitted on February 14, 2014

EXISTING SITE

The proposed project is zoned NC2-40 and abuts a SF-5000 zone to the East. A approximately elevation 10' difference between the South and the North end of the site. The building height reflects the requirements and bonus found in the SMC 23.47A.012 which says: The height of a structure may exceed the otherwise applicable limit by up to 4 feet, subject to subsection 23.47A.012.A.1.c, provided the following conditions are met:

Either a floor-to-floor height of 13 feet or more is provided for nonresidential uses at street level; or A residential use is located on a street-level, street-facing facade, and the first floor of the structure at or above grade is at least 4 feet above sidewalk grade; and
The additional height allowed for the structure will not allow an additional story beyond the number that could be built under the otherwise applicable height limit.

The site is located on 15th Ave. NW, just north of Ballard High School and approximately 1 mile North of NW Market St. 15th Ave is a major arterial that connects Queen Anne to Ballard and bridges the Lake Washington Ship Canal. This stretch of 15th Ave. lies just outside the Urban Center of Ballard and includes a variety of commercial and multi-family structures ranging from small markets and restaurants, to townhomes and apartment buildings. The site is within a half mile of the Salmon Bay Park, due west and Salmon Bay School a few blocks southwest.

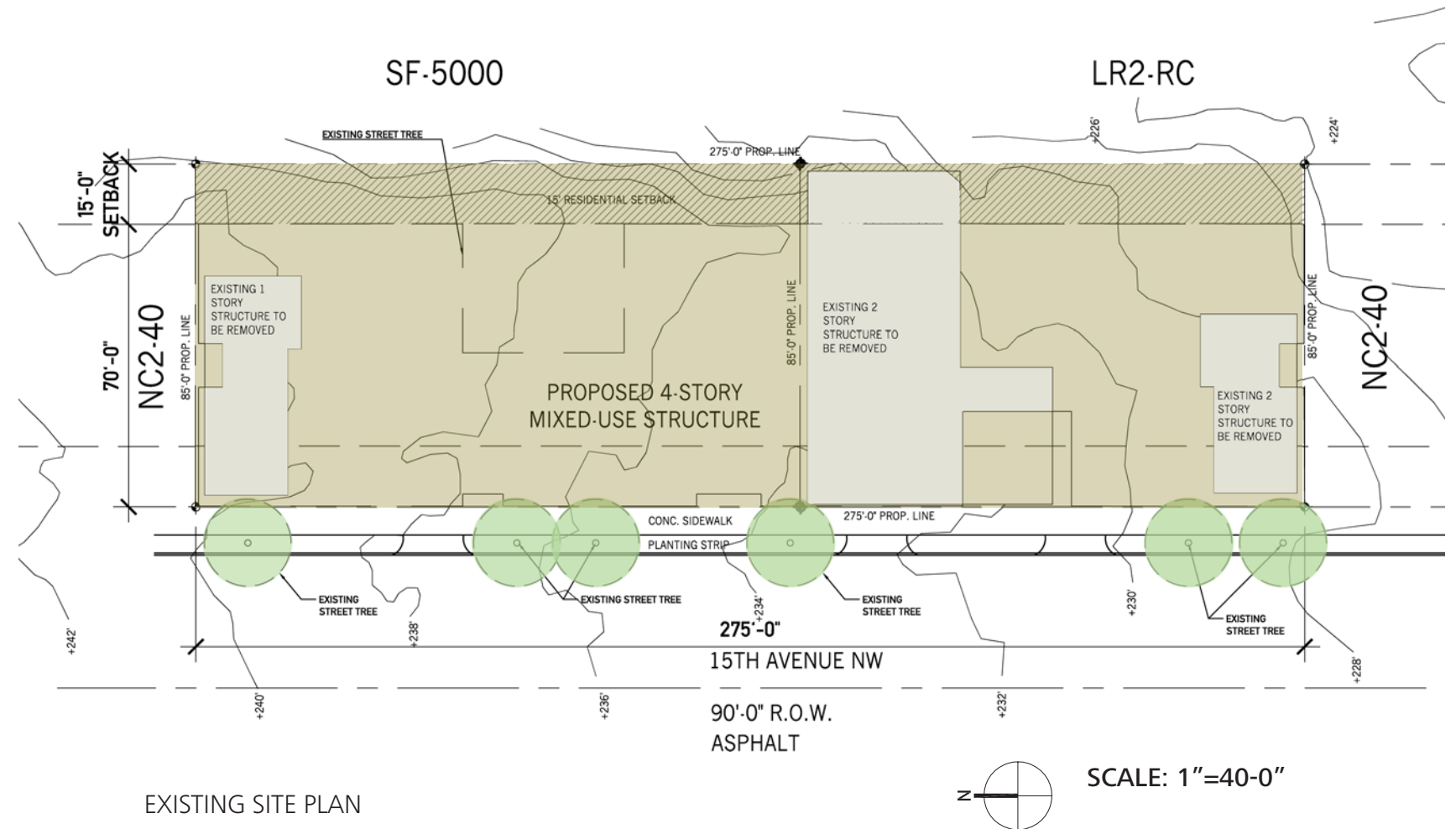
Vehicular traffic is quite heavy along 15th Ave. NW and there are numerous frequent transit options both north-south along the avenue and east-west along NW 65th St. two blocks south of the site.

The neighborhood is an eclectic combination of small mom-and-pop shops, professional offices, restaurants and cafes, mixed use and multifamily uses. One block east and one block west you will find single family homes with neatly manicured yards. The street boasts a variety of scales, styles, and materials.

PROJECT DESCRIPTION:

The proposed development will create an urban mixed use building with 83 dwelling units, 7 live-work units, and around 3,000 square feet of commercial and office spaces. 48 Parking stalls will be provided within the building on the ground level. The structure will follow the slope of the site and is divided into smaller sections including courts, live-work, a residential entrance lobby, and small commercial spaces. Amenity space for the residents will be located on the roof deck for entertaining and relaxing and providing views of the city to the south.

DEVELOPMENT OBJECTIVES



EXISTING SITE PLAN

DEVELOPMENT STATISTICS SUMMARY:

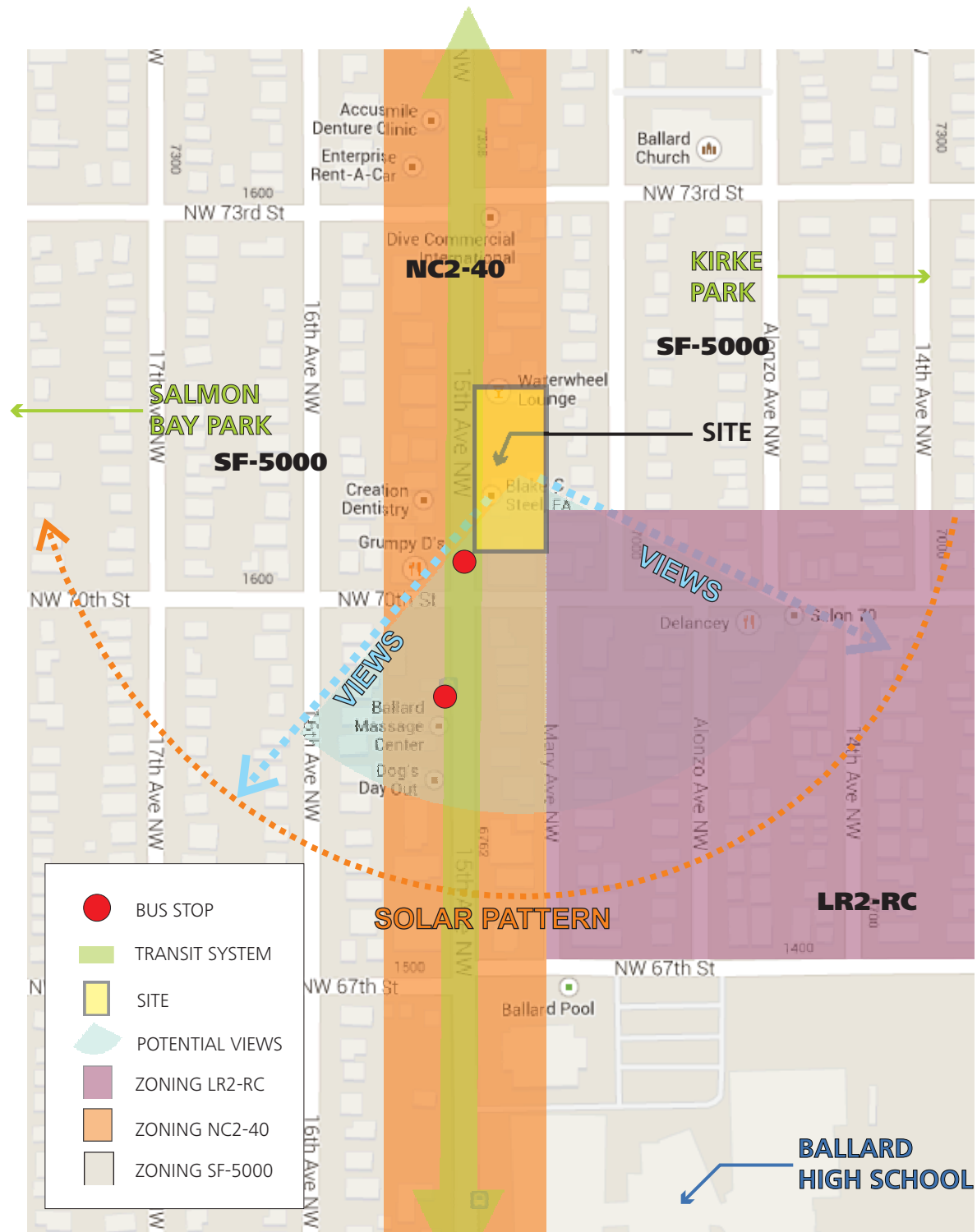
- Lot Size	23,375 SF
- FAR	3.25
- Allowable FAR	75,969 SF
- Proposed FAR	75,913 SF
- Parking Stalls	47 Stalls

Floor Area Summary

	FAR SF	Units
Roof	134	
4th Level	13,240	18
3rd Level	17,135	25
2nd Level	17,135	25
1st Level	17,040	15
P1/0 Level	11,285	5 (Live Work)
Total	75,913 sf	88 units

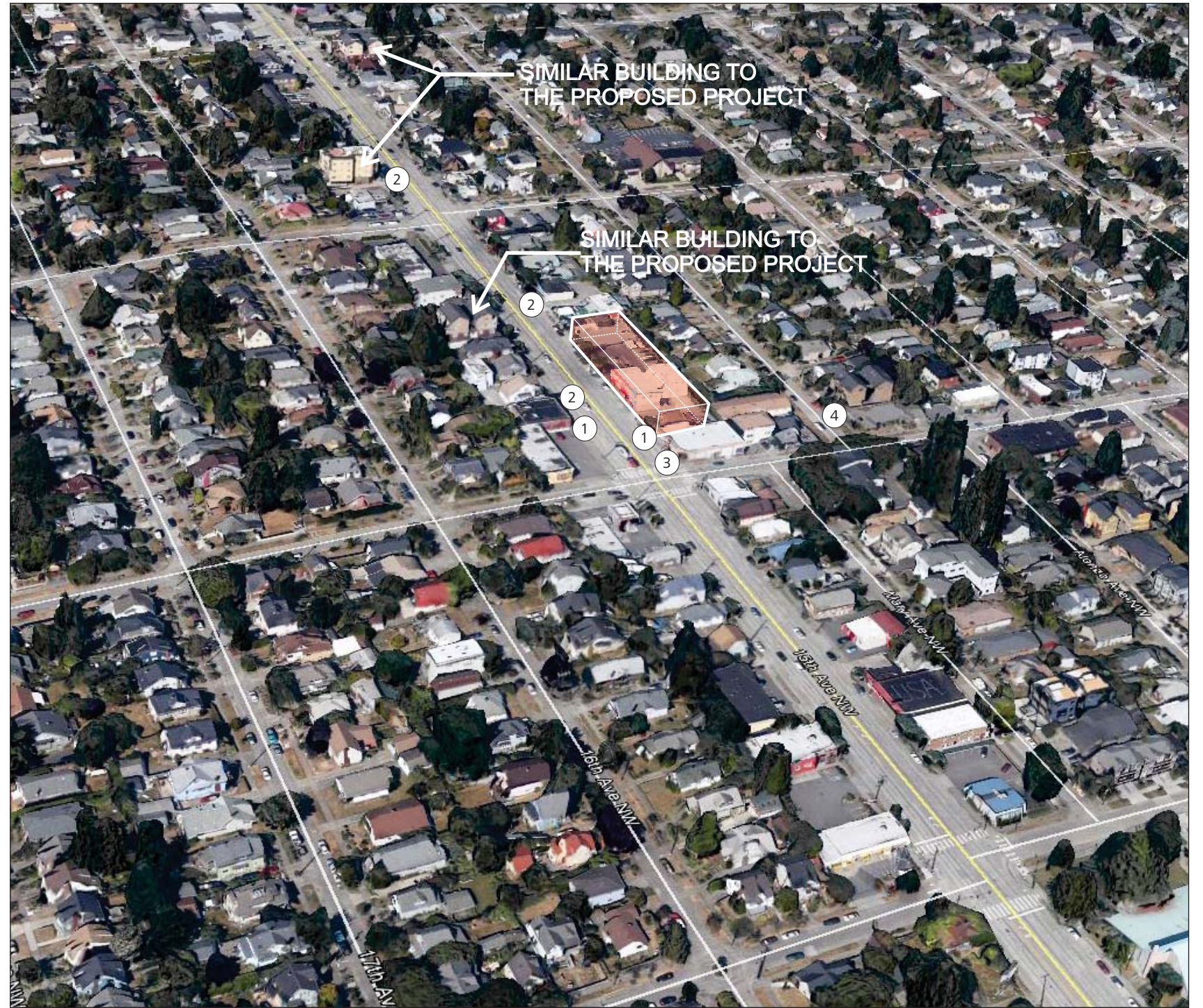
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SITE AERIAL

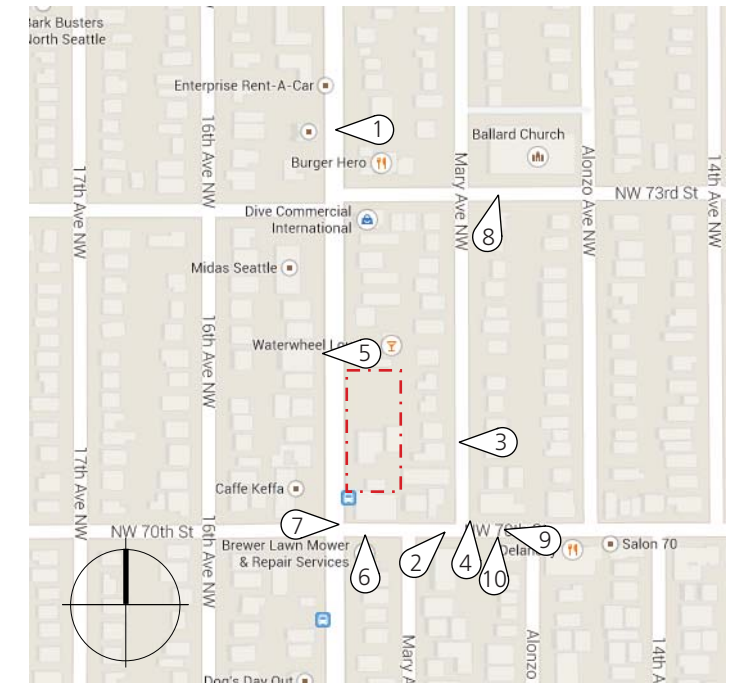
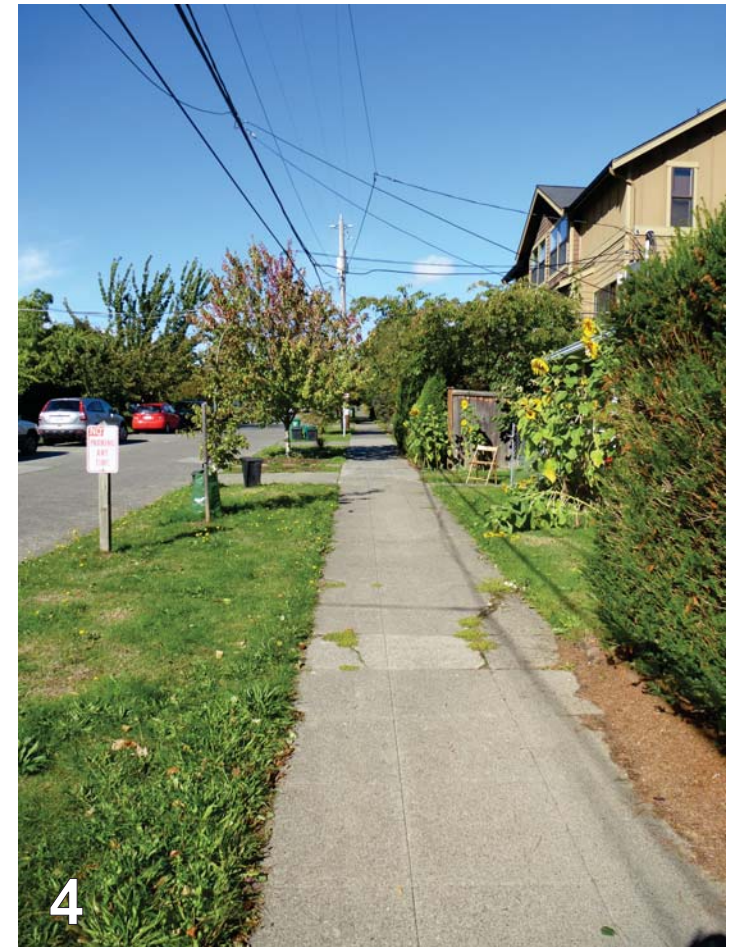
CONTEXT ANALYSIS



SITE AERIAL

Neighborhood design cues:

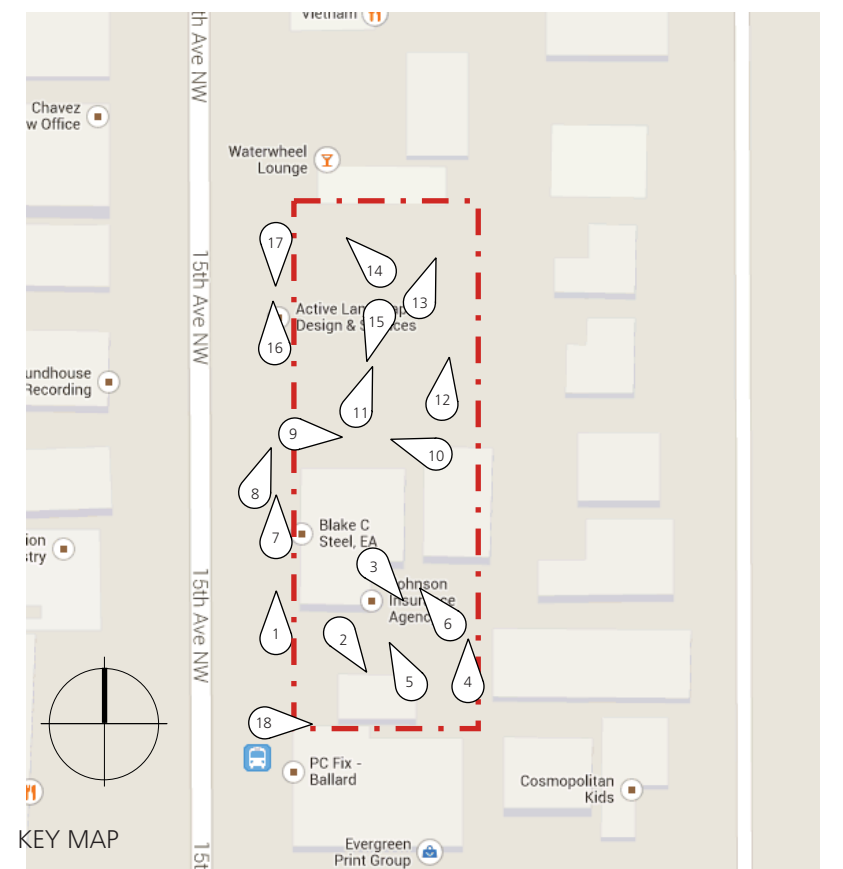
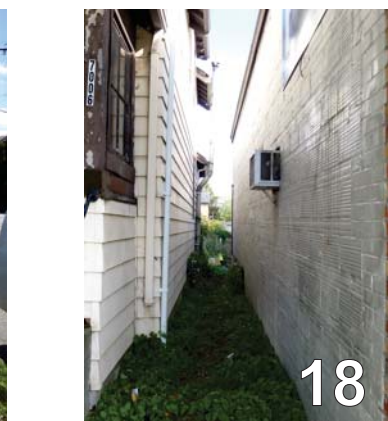
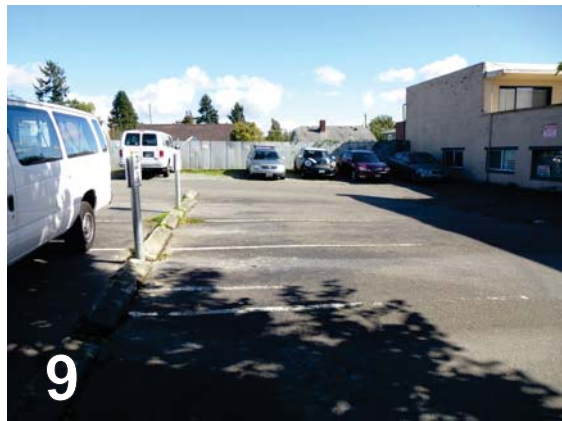
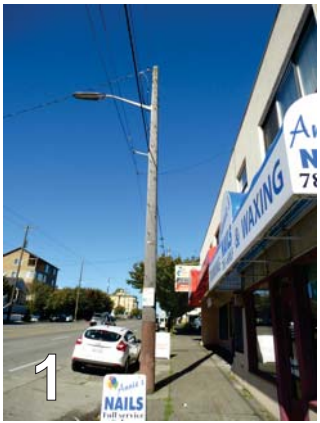
The surrounding built fabric is a mix of single family homes, mix-uses, commercials and retail spaces. (1) Brick veneer, (2) flat roofs, (3) storefront and fiber cement panel siding are some of the materials used in the area. Massing varies from 1 story building to 44' (maximum allowed per zoning, 40' base + bonus height) which translate into a 4 stories building.



NEIGHBORHOOD PHOTOS

7.14.2014 RECOMMENDATION PACKAGE
7016 15TH NW DPD PROJECT #:3016160

washington 2505 3rd avenue*suite 300C , Seattle, WA 98121 • california 1404 Broad Street, San Luis Obispo, CA 93401 • www.caronarchitecture.com



SITE PHOTOS



1. 15th LOOKING EAST



2. 15th LOOKING WEST

STREET CONTEXT

SMC SECTION	REQUIREMENT	PROVIDED
USE 23.47A 23.47A	The proposed use of mix-used (residential and commercial) in this NC (Neighborhood Commercial) are allowed outright by the Seattle land use & zoning code. A Master Use Permit (MUP) is required, including full design review per Tip 208	(83) Residential Units of difference sizes to include Studios, 1 Bedrooms, 1 Open bedrooms and 2 Bedrooms. And 5 Live Work Spaces, 1 Office space, 2 Commercial space and Services.
HEIGHT 23.47A.012 23.47A.012.A.1.c 23.47A.012.C.4.f	Base height limit is 40-ft. Height is measured from the average grade level per director's rule 4-2012 and smc 23.86.006.a. 4-ft is added as a bonus if a floor-to-floor height of 13 feet or more is provided for non-residential uses at street level. Elevator/stair penthouses may extend up to 16-ft. Above height limit.	Proposed height is 44'-0" to top of roof at the rooftop deck and within the height limit allowed under SMC 23.47A.012. Elevator/stair penthouse is under the allowed height limit
FAR 23.47A.013 Table 23.47A.013	If the use is only residential or only commercial the FAR is 3 but if mix-used the maximum permitted FAR is 3.25 and with a lot of 23,375 sf. the total FAR allowed is 75,968.75 sf.	Proposed FAR : 75,913 sf
SETBACK 23.47A.014	Front setback: None required Rear setback: 15 ft. Minimum from any Residential zoning + 2 foot setback over 40' above grade. Side setback: None Required	Front setback: Varies between 0 ft and 5 ft. Rear setback: 15 ft. < 40 feet above grade > +2 feet South side setback: not required.
LANDSCAPE 23.47A.016	Green factor score of 0.3 or greater is required for any lot with development containing more than one dwelling unit. Street trees are required when any development is proposed. Existing street trees shall be retained unless the Director of Transportation approves their removal. The Director, in consultation with the Director of Transportation, will determine the number, type and placement of street trees to be provided:	A green factor of 0.3 or greater is provided. Street trees have been provided.
PARKING 23.47A.030 23.54.015 TABLE 23.54.020.F.2	A 50% parking reduction is allowed for all uses per proximity to frequent transit. With a ratio of 1 to 1 at 83 units total, needed is 83 parking stalls. Commercial/Livework under 1,500 sf not needed. Office Space will require 1 stall based on the 1/1000 over 1,500 sf rule and Service use will require 1 stall based on the 1/500 over 1,500 sf rule. Total of 86 stalls with the 50% rule is reduced to 43 parking stalls. Bicycle parking is required 1 to 4 for residential, 1 to 4,000 for Office, 1/12,000S for Service., 1 to 4 for Live Work ad Commercial and 1 to 4 for residential. For a total of 25 bicycle parkings.	48 on-site parking spaces have been provided 25 bicycle spaces are provided.
SOLID WASTE 23.54.040	-375 sf, plus 4 sf each additional unit over 50. -Non-residential use area of 5001 - 15000 sf = 125 req'd/2 = 63 sf. -Office & retail recycling must be separate from residential sf.	Following discussion, review and approval by Liz Kane, a 390 sf space for trash and recycling is provided.

ZONING CODE ANALYSIS

A. Site Planning

A-1: Responding to site characteristics

The project site is sloping from south to north with an overall gain of approximately 8 ft. Residential lots to the east step down a few feet from the project's east lot line. The proposed design scheme incorporates modulation to step the building from south to north to accommodate the site's existing slope. Stepping of the structure provides opportunity to use a portion of the roof as outdoor amenity space with great southern views and solar exposure.

A-2: Streetscape compatibility

Design elements along the street level of the building reinforce the pedestrian oriented businesses and activity along 15th Ave. NW.

A-3: Entrances visible from the street. Residential entry to the building is highly visible from the right-of-way, and commercial uses are easily accessed from the sidewalk. The proposed design incorporates the use of glass storefronts for transparency and architectural elements such as canopies and signage to further identify each entrance. In addition, the residential entrance is architecturally marked with an elevator/stair tower that is set back from the street creating an entry court.

A-4: Human activity.

The proposed entry court promotes pedestrian interaction in the semi-public space—see preferred design option C. Canopies and landscape beds are located at the street level to protect pedestrians from weather and provide visual interest. Open space on the roof deck is provided as common amenity space for the residents.

A-5: Respect for adjacent sites.

Building modulation steps down in elevation at the south end to minimize the vertical impact to the existing 1 story structure to the south. The proposed building is set back 15' from the rear lot line abutting residential uses and the roof deck amenity space includes a landscaped buffer to improve privacy of the residential lots to the east. Building modulation including a court along the east façade helps to break up the massing and reduce the vertical impact on the residential lots. Parking at grade will be screened at the lot line.

A-7: Residential open space.

Open space is incorporated into the proposed scheme through the use of courts on the east and west facades and a roof deck providing outdoor open space for resident's use.

B. Bulk, Height and Scale

B-1: Height, bulk and scale compatibility.

The proposed design utilizes building modulation to break up the perceived scale and bulk within the context of adjacent sites and dissimilar zoning and uses. Modulation steps down along the street in elevation to reduce the height in relation to the lower structure to the south. The east façade is set back from the lot line where there is an existing retaining wall separating the lot from adjacent single family zoning.

C. Architectural Elements and Materials

C-2: Architectural concept and consistency. The proposed design scheme is modulated to create smaller proportioning which compliments the existing neighborhood scale. Modulation of the architecture has employed repetition to create a language that unifies the design while still providing smaller scale elements. The architectural features that divide the facades are used to delineate commercial space at the street level from the residential floors above.

C-3: Human scale.

Commercial and live-work space at the street level are set back from the right-of-way and include landscaping to create a human scale space. Canopies at the edge of the right-of-way help to break up the height of the façade and bring the scale of the space on the street down to the pedestrian level.

C-5: Structured parking entrances.

The parking garage entry of the proposed design is located at the lowest elevation of the site to reduce its prominence. Focus on the commercial and residential entrances is achieved by setting the entrances back from the sidewalk, and including architectural design elements that appropriately highlight each type of entry.

D. Pedestrian Environment

D-1: Pedestrian open space and entrances.

An entry court is proposed for the residential entrance to the building to create a separate entry for the residential portion of the project and to provide for an attractive and semi-public space off the sidewalk. The entry is protected from weather with a canopy and the use of glazing, openness, and lighting add security.

D-7: Personal safety and security.

Numerous openings and storefront on the street-facing façade provide transparency with lends to greater visibility. Open spaces are designed to be open and well lit in order to aid in security. Clear lines of sight are provided for at the parking garage entrance.

D-9: Commercial signage.

The design intent is to have commercial signage on the storefront canopies. Commercial signage will be unique to each business and be well lit. Residential signage will be clearly visible and located at the residential entry.

D-11: Commercial transparency.

Live-work and business storefronts will utilize large transparent glazing systems along the street level. Combined use of lighting at exterior and interior of storefronts will improve safety at the right-of-way. D-12: Residential entries and transitions. Security at the residential entrance to the building is provided at the street level by way of an open entry court that is well lit, set back from the sidewalk, and includes appropriate signage. Design elements that create a transition from the public right-of-way to the semi-public residential entrance are: attractive paving, landscaping, site furniture such as benches, transparency at the building entrance which promotes a secure and inviting entry, residential signage and address, and a canopy for weather protection.

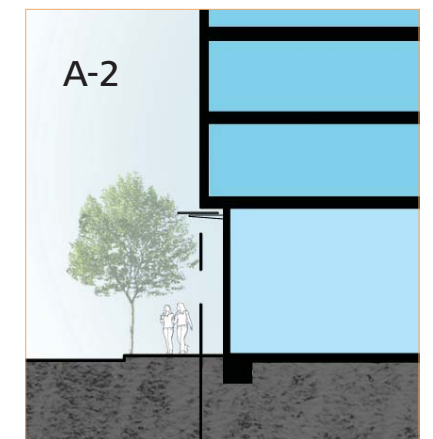
E. Landscaping

E-2: Landscaping to enhance the building or site.

Landscaping is proposed to be located at building entrances. The residential entry court will be landscaped to create an inviting and attractive semi-public transition space to the residential entrance lobby. Landscape boxes will be located adjacent to the live-work units to enhance the entrances and may incorporate decorative paving. Landscaping including green roof, grasses and small shrubs, and trees will be part of the roof deck amenity design for improved outdoor space.

E-3. Landscape design to address special site conditions.

Landscaped courts on the west and east facades will be used as a design elements that aids in building modulation. Landscape boxes will be used as transition elements along the street right-of-way to mitigate the existing slope at each live-work entrance.

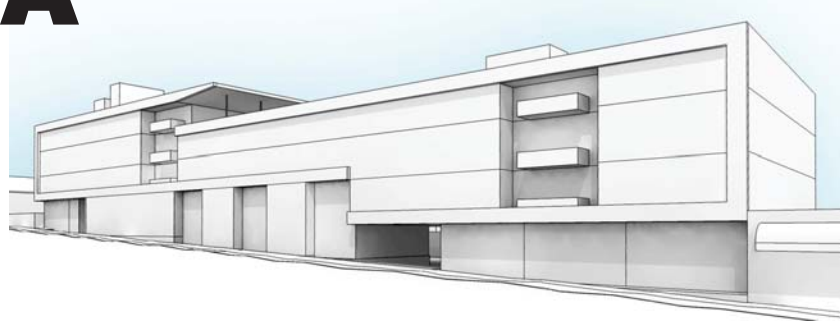


RELEVANT DESIGN GUIDELINES

EDG Recommendation Direction: The board expressed that the applicant should move forward with the design using the massing shown in either Option A or C.		Architect's Response
A-7 Residential Open Space		
The Board expressed some skepticism of the viability of the street facing courtyard as an amenity space.	Elevations, Level 1 Plan,	Based on the board comments regarding the viability of the west courtyard located on 15th, we decided to change the space use to the North of the courtyard from Office to Commercial. This will promote the use of the courtyard for activities promoting a more semi-public space in association to the Commercial Space which could be a coffee shop or similar.
B-1 Height, Bulk, and Scale Compatibility		
The Board noted that the project responds well to the constraints of the site except for the East side. The applicant should consider softening up the massing in response to the adjacent residential uses. It was expressed that the East side of Option A had a more residential scale.	Sections, Elevations	The east elevation massing was softened to relate better to the adjacent residential scale by cutting off 2 feet of level 4 and by breaking the façade in 3 segments.
C-4 Exterior Finish Materials		
Materials: The Board encouraged the project to use the type and quality of materials shown in the examples on page 9 of the EDG packet. The applicant should show what materials are being proposed for the blank north and south elevations. Consider pulling the south façade back from the property line and providing windows as the adjacent single-story property was recently renovated. The guidance that I am concerned has not been adequately addressed has been highlighted. The Board expressed that the intention of what was shown on page 9 of the EDG packet should determine materials and color. It appears that most of the images shown are high quality materials such as metal, stone, glass or mesh panels. As well, almost every building shown has multiple plane changes that create visual interest and appeal, these are not flat buildings. Provide higher quality materials and shifting of elevation elements to create a design more like the examples shown to the Board.	Elevations, Perspectives Plans Level 0, Level 1, Level 2, Level 3 and level 4 Elevations, Perspectives Plans Level 0, Level 1, Level 2, Level 3 and level 4 Elevations, Perspectives Plans Level 0, Level 1, Level 2, Level 3 and level 4	Material selections on the elevations are reflecting some of the elements presented on the referred EDG package, page 9. The design team decided to move back a section of the South facade based on the design board's recommendation. We looked at moving back the whole facade but by doing so the West facade would have been negatively impacted creating an unbalanced elevation. The massing being broken down into smaller segments will create plane variations which will provide a medium or a base to establish different texture/color schemes. As an example the courtyards and bays material selections will be using contrasting color schemes on composite panel and/or fiber cement panels creating variations between the facades and the planes.
D-1 Pedestrian Open Spaces and Entrances		
The Board expressed that the design of the street level along 15th Ave NW should take its cues from the commercial storefronts along NW 70th St. The applicant should provide a more generous setback from the property line at the live/work units. If needed, decrease the depth of the Live/Work units to provide this area. If provided, the entry courtyard should be designed to be a usable space for the tenants and discourage unwanted public use.	Site plan, Level 1 Plan, Landscape Plan, Perspectives Site plan, Level 1 Plan, Landscape Plan, Perspectives Site plan, Level 1 Plan, Landscape Plan, Perspectives	The abundant use of storefront type glazing and brick veneer on 15th, main project façade, will promote continuity and strong relation to 70th which follow the Board comments and vision. Regarding the Board desire to see a more substantial buffer between the Live/work and the public sidewalk, we believe the increase would impact the parking layout in a way that the required parking stalls would be compromised. The narrowness of the site combined with the need of adequate live/work spaces and a double loaded parking layout to meet requirements, make the expansion of the buffer not viable. See A-7 note for the courtyard comments.
D-2 Blank Walls		
Buildings should avoid large blank walls facing the street, especially near sidewalks. Where blank walls are unavoidable they should receive design treatment to increase pedestrian comfort and interest. Consider providing windows in the units flanking the street facing courtyard to add visual interest to the elevations.	Perspectives, Elevations, Level 0 and Level 1 plans Perspectives, Elevations, Level 0 and Level 1 plans	Blank facades, South and North, will get color variation treatments to diminish the blank wall appeal. Windows in units facing courtyards will be added to reduce blank façade.

REC #1 DIRECTION AND ITEMIZED RESPONSE

A ALTERNATIVE OPTION



FAR: **3.17**
 UNIT COUNTS: **83**
 LIVE WORK/COMMERCIAL: **8**
 PARKING STALL: **58**
 TOTAL SQUARE FOOTAGE: **56,520**

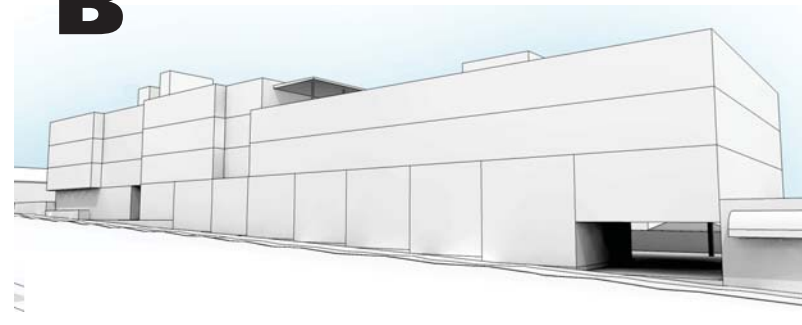
PROS:

- This option propose a more cutting edge design which re?ect Ballard's eclectic built environment.
- Setback from street at the live work levels creates a more pleasant pedestrian experience.
- A distinct entrance for the residential is clearly identi?ed.
- towards the neighborhood.
- Good number of residential units and live work spaces.

CONS:

- Massing on East side more cumbersome.
- Asking for departure on the minimum depth required for live work/commercial spaces.

B CODE COMPLIANT OPTION



FAR: **3.21**
 UNIT COUNTS: **86**
 LIVE WORK/COMMERCIAL: **7**
 PARKING STALL: **57**
 TOTAL SQUARE FOOTAGE: **57,850**

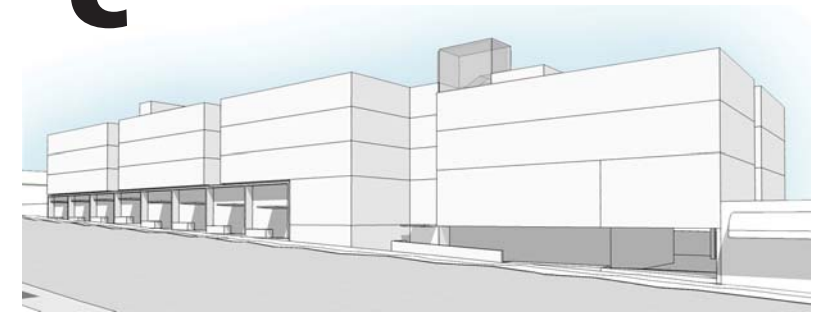
PROS:

- Code compliant.
- Good outdoor public spaces and amenities.
- The highest square footage of all options for residential usage.

CONS:

- Limited modulation on the West side (street side)
- No release at sidewalk from Live Work, Commercial and Of?ce spaces; building built up to property line.
- Residential lobby entrance not clearly separated.

C PREFERRED OPTION



FAR: **3.25**
 UNIT COUNTS: **83**
 LIVE WORK/COMMERCIAL: **7**
 PARKING STALL: **47**
 TOTAL SQUARE FOOTAGE: **75,913**

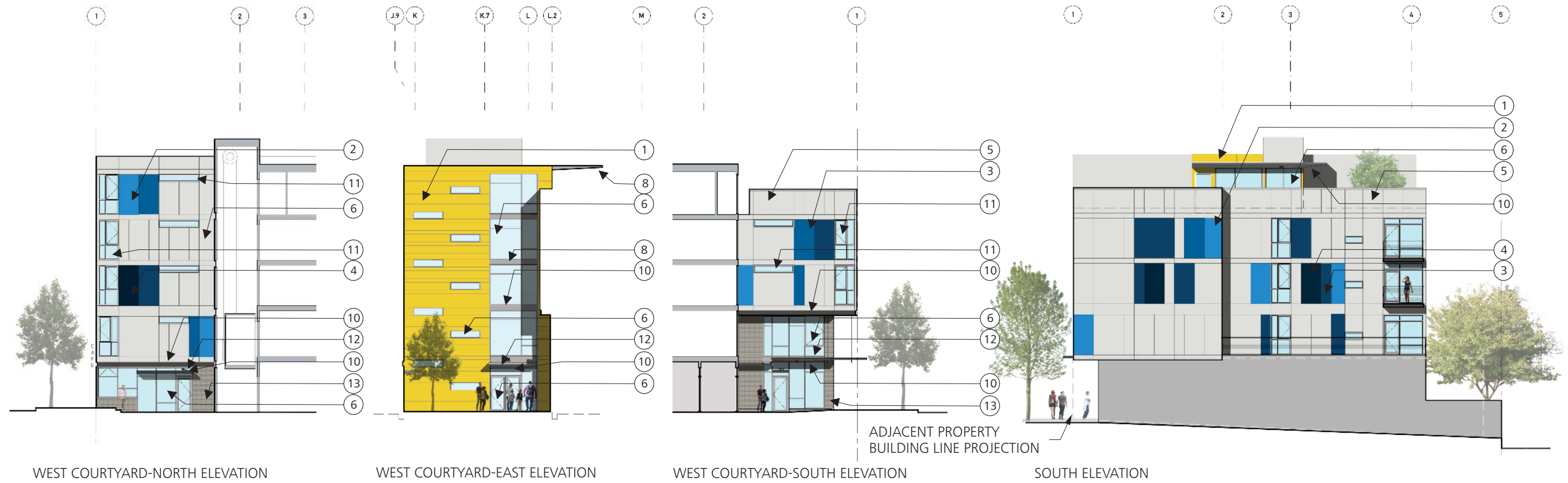
PROS:

- Good pedestrian experience a street/sidewalk and entries.
- Good modulation at street and at rear diminishing massing impact on neighborhood.
- Good separation between residential and live work/commercial entries.
- Good outdoor/green spaces and landscaping areas.
- Good modulation at street side re?ecting a better integration with existing neighborhoods scale and conditions.

CONS:

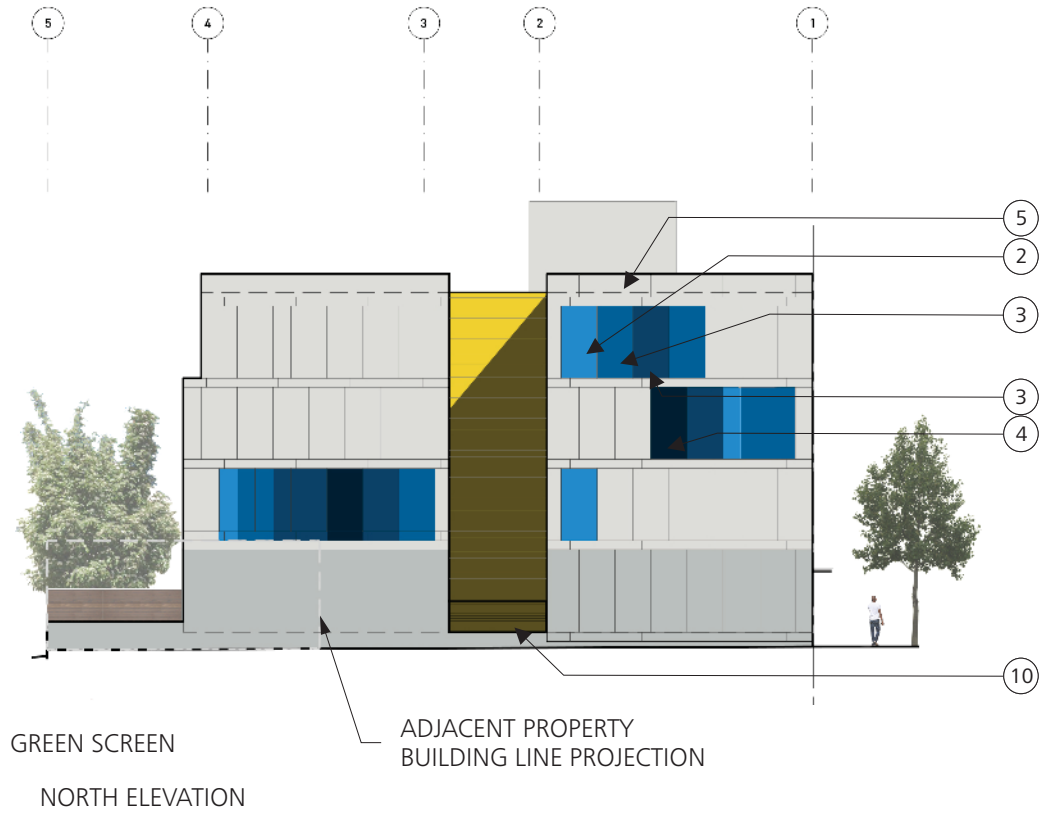
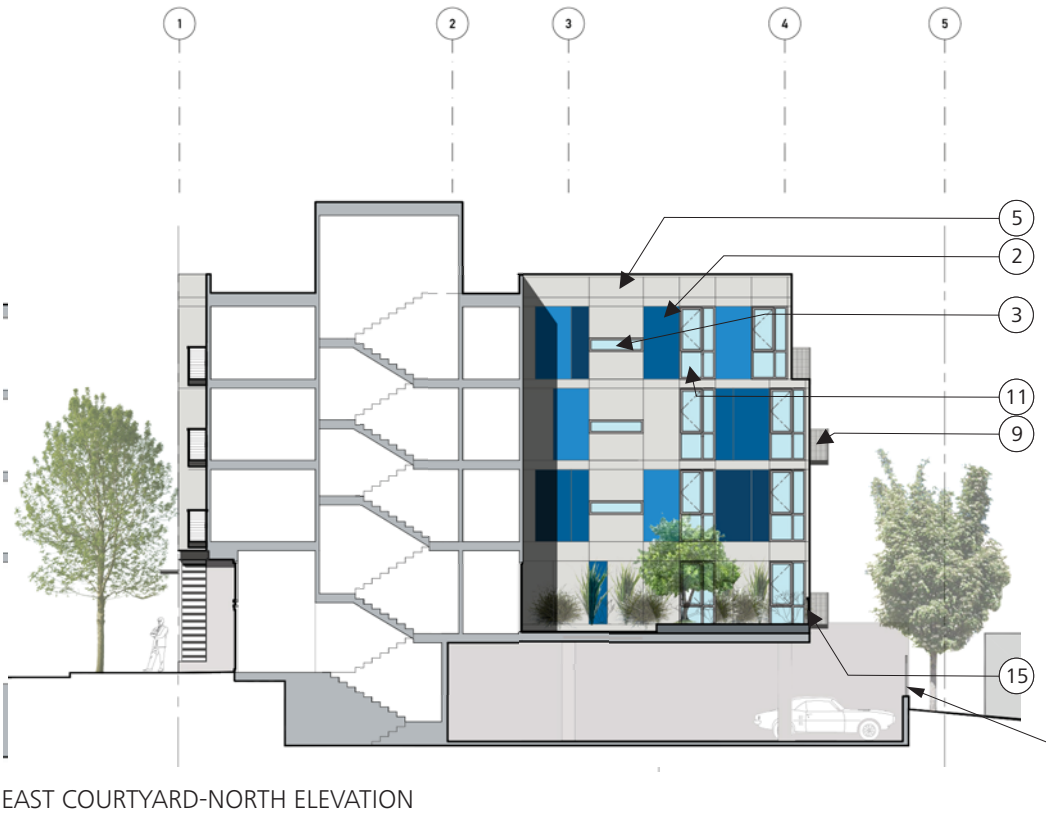
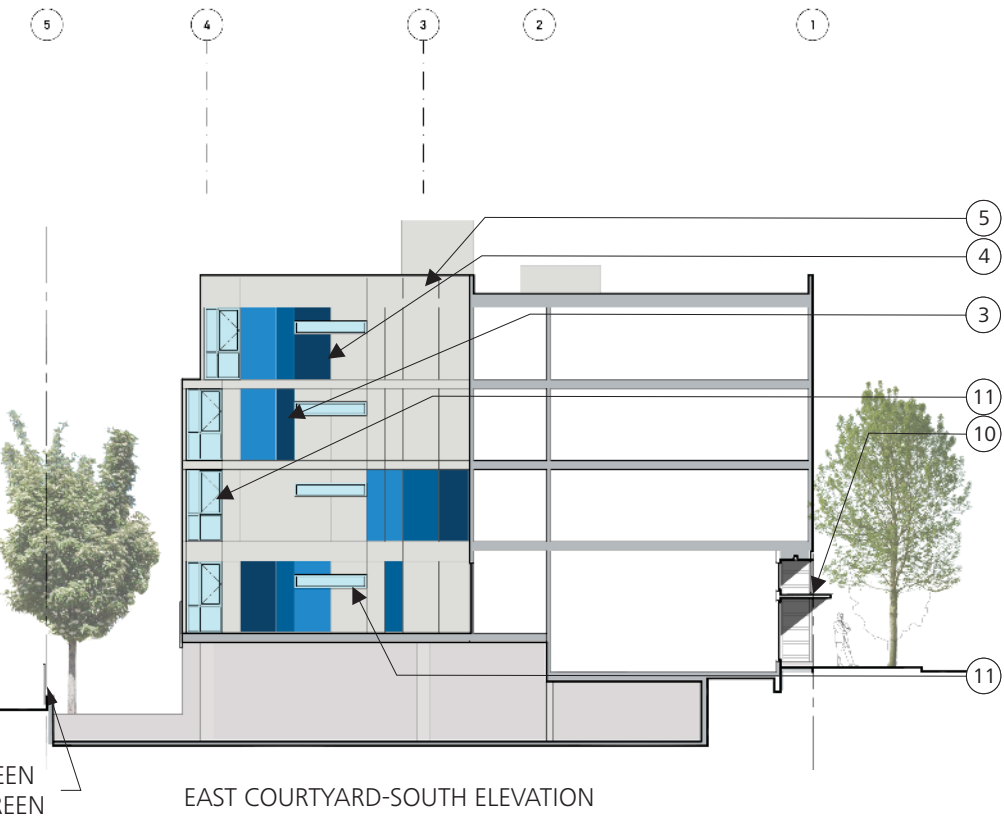
- Needing departures on the minimum depth requirement for livework/commercial spaces and at rear parking landscaping strategies.

CONCEPT SUMMARY: REFERENCE TO THE 3 OPTIONS PRESENTED AT THE EDG MEETNG



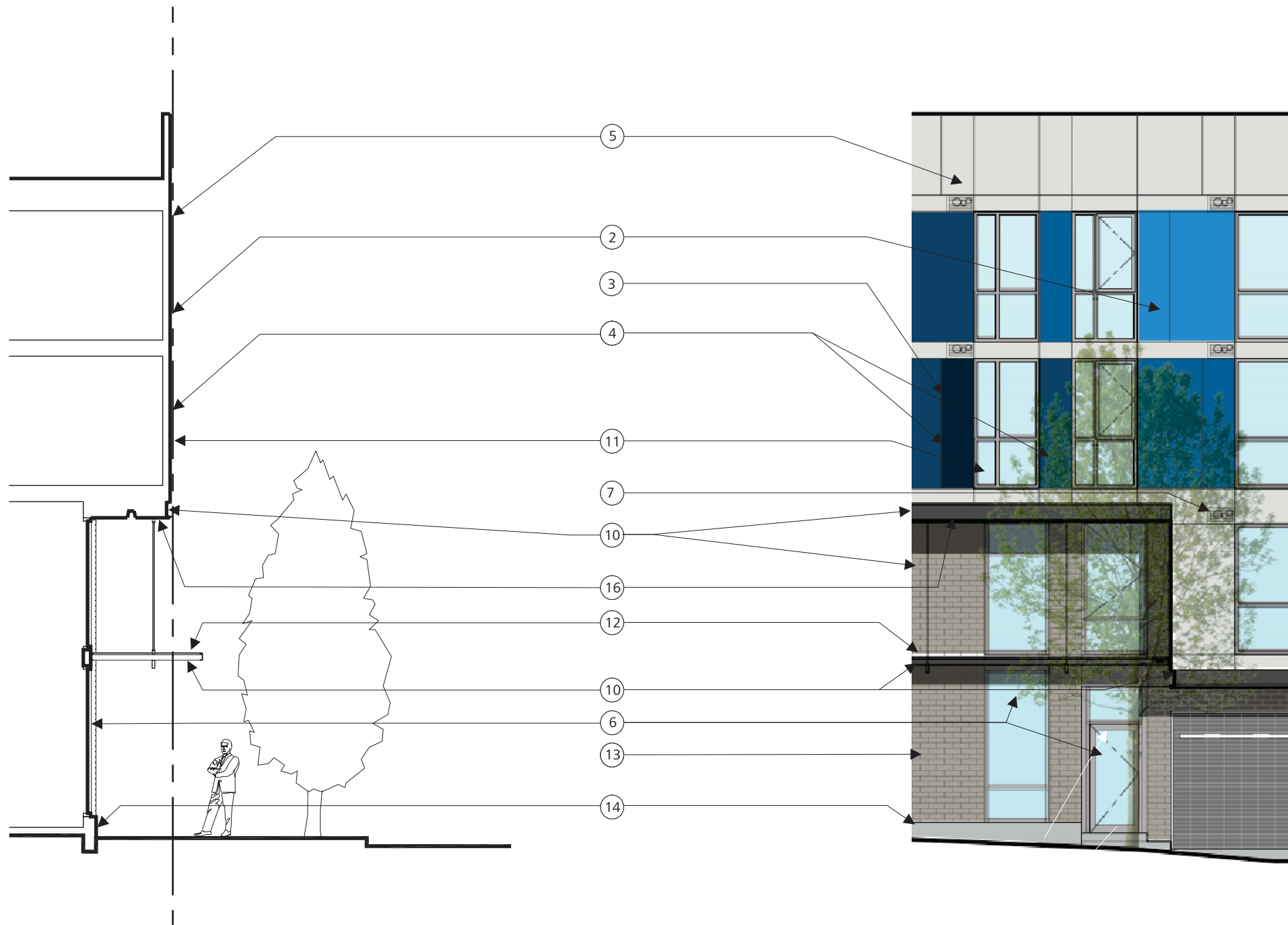
BUILDING ELEVATIONS

NOTE: SEE PAGE 13 FOR COLOR & MATERIAL CHART



BUILDING ELEVATIONS

NOTE: SEE PAGE 13 FOR COLOR & MATERIAL CHART



VIGNETTE A

BUILDING SECTION/ELEVATION DETAIL

	1	COMPOSITE PANEL: COLOR: ZINC YELLOW
	2	FIBER-CEMENT PANEL : SHERWIN WILLIAMS SW 6958 DYNAMIC BLUE
	3	FIBER-CEMENT PANEL : SHERWIN WILLIAMS SW 6964 PULSATING BLUE
	4	FIBER-CEMENT PANEL : SHERWIN WILLIAMS SW 6510 LOYAL BLUE
	5	FIBER-CEMENT PANEL : SHERWIN WILLIAMS SW 7063 NEBULOUS WHITE
	6	STOREFRONT DOOR/WINDOW*
	7	LOUVER/FLASHING*
	8	FASCIA/INFIELD PANEL*
	9	PREFABRICATED METAL*
	10	METAL TRELLIS/CANOPY/"C" CHANNEL*
		*COLOR TO MATCH: CHARCOAL
	11	VINYL WINDOW
	12	CLEAR TEMPERED GLASS
	13	MUTUAL MATERIALS BRICK : PEWTER COLOR
	14	CONCRETE : NATURAL COLOR
	15	CEDAR FENCE: NATURAL COLOR
	16	COMPOSITE PANEL: FINISH: SANTOS PALISANDER

COLOR & MATERIAL CHART

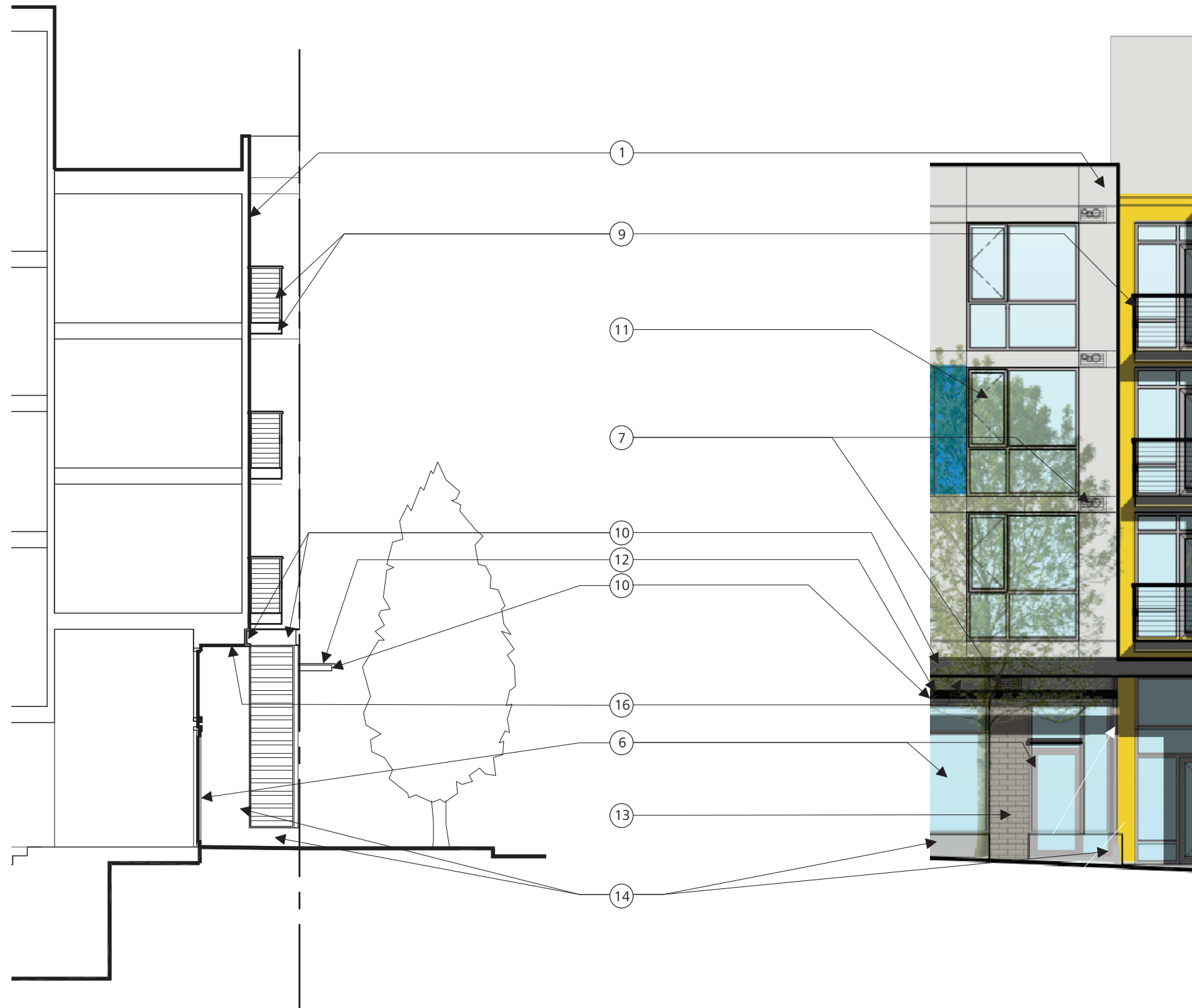


VIGNETTE B

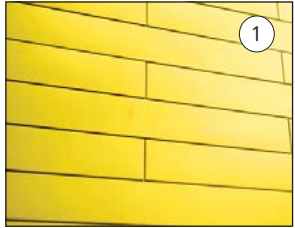


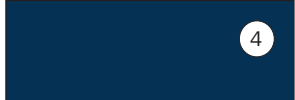

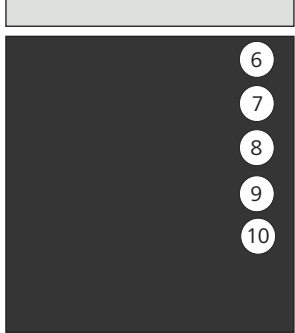
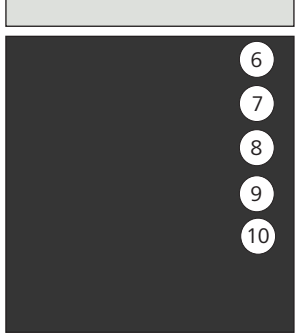
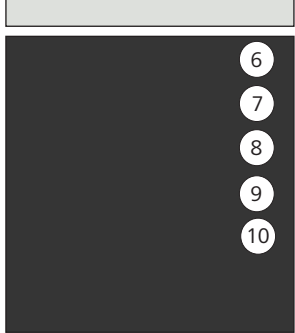
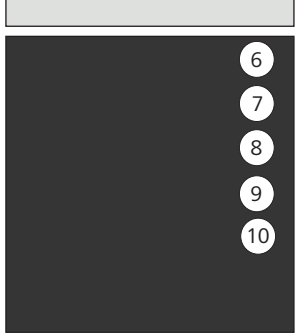
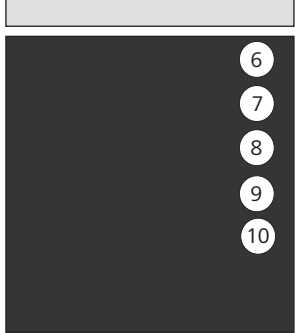






BUILDING SECTION/ELEVATION DETAIL

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	6 STOREFRONT DOOR/WINDOW*
	7 LOUVER/FLASHING*
	8 FASCIA/INFIELD PANEL*
	9 PREFABRICATED METAL*
	10 METAL TREILLIS/CANOPY/"C" CHANNEL* *COLOR TO MATCH: CHARCOAL
	11 VINYL WINDOW
	12 CLEAR TEMPERED GLASS
	13 MUTUAL MATERIALS BRICK : PEWTER COLOR
	14 CONCRETE : NATURAL COLOR
	15 CEDAR FENCE: NATURAL COLOR
	16 COMPOSITE PANEL: FINISH: SANTOS PALISANDER

COLOR & MATERIAL CHART



VIGNETTE C

	1	COMPOSITE PANEL: COLOR: ZINC YELLOW
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	5	FIBER-CEMENT PANEL : SHERWIN WILLIAMS SW 7063 NEBULOUS WHITE
	6	STOREFRONT DOOR/WINDOW*
	7	LOUVER/FLASHING*
	8	FASCIA/INFIELD PANEL*
	9	PREFABRICATED METAL*
	10	METAL TRELLIS/CANOPY/"C" CHANNEL*
	11	VINYL WINDOW
	12	CLEAR TEMPERED GLASS
	13	MUTUAL MATERIALS BRICK : PEWTER COLOR
	14	CONCRETE : NATURAL COLOR
	15	CEDAR FENCE: NATURAL COLOR
	16	COMPOSITE PANEL: FINISH: SANTOS PALISANDER

COLOR & MATERIAL CHART



MAIN LOBBY ENTRANCE/WEST COURTYARD-EYE LEVEL VIEW LOOKING EAST

SELECTED RENDERINGS

7.14.2014 RECOMMENDATION PACKAGE
7016 15TH NW DPD PROJECT #:3016160

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WEST ELEVATION RENDERING



SOUTH/EAST CORNER-EYE LEVEL VIEW LOOKING NORTH



GREEN SCREEN VIEW FROM SOUTH/EAST CORNER



SOUTHWEST CORNER-EYE LEVEL VIEW LOOKING NORTH/EAST



SOUTHWEST CORNER-BIRD VIEW LOOKING NORTH/EAST



SOUTHWEST CORNER-EYE LEVEL VIEW LOOKING NORTH



MAIN LOBBY VIEW- EYE LEVEL



NORTH/WEST CORNER-EYE LEVEL VIEW LOOKING SOUTH/EAST

SELECTED RENDERINGS

7.14.2014 RECOMMENDATION PACKAGE
7016 15TH NW DPD PROJECT #:3016160



NORTH/EAST CORNER-BIRD VIEW LOOKING SOUTH/WEST



SOUTH/EAST BIRD VIEW LOOKING NORTH/WEST

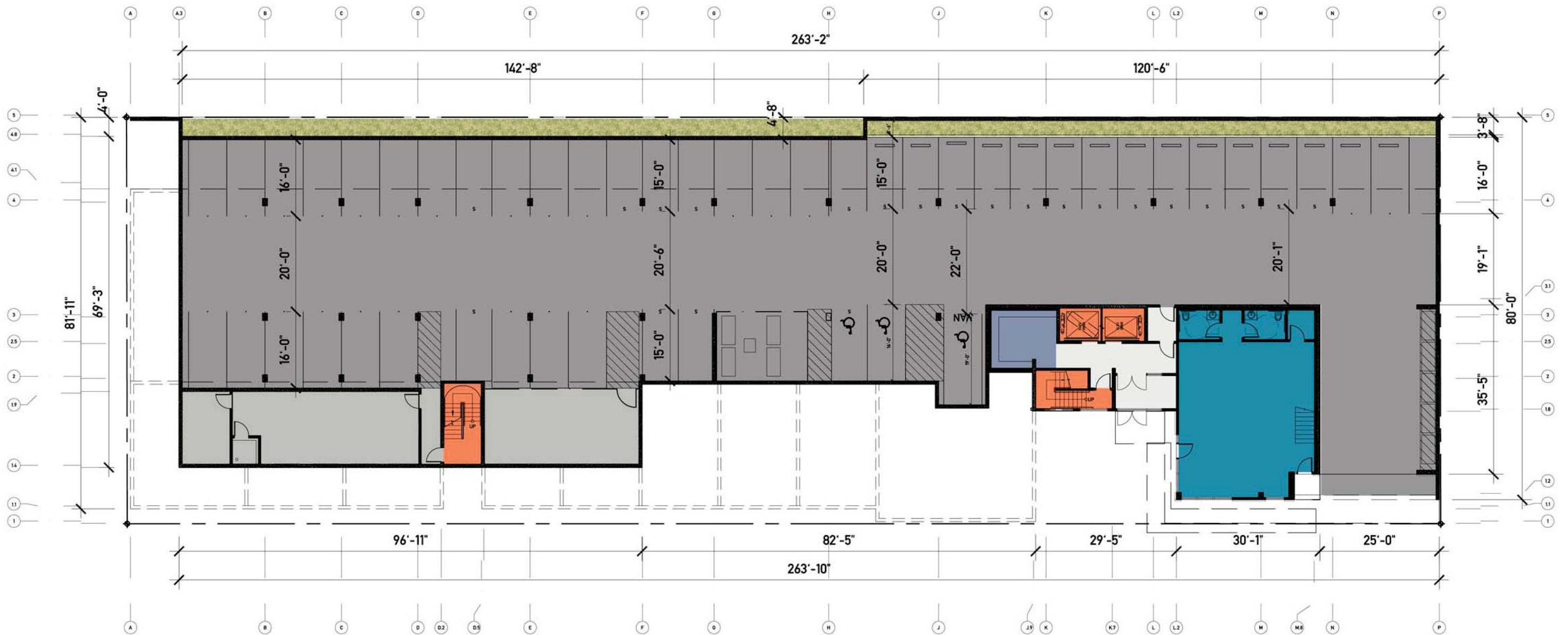
SELECTED RENDERINGS

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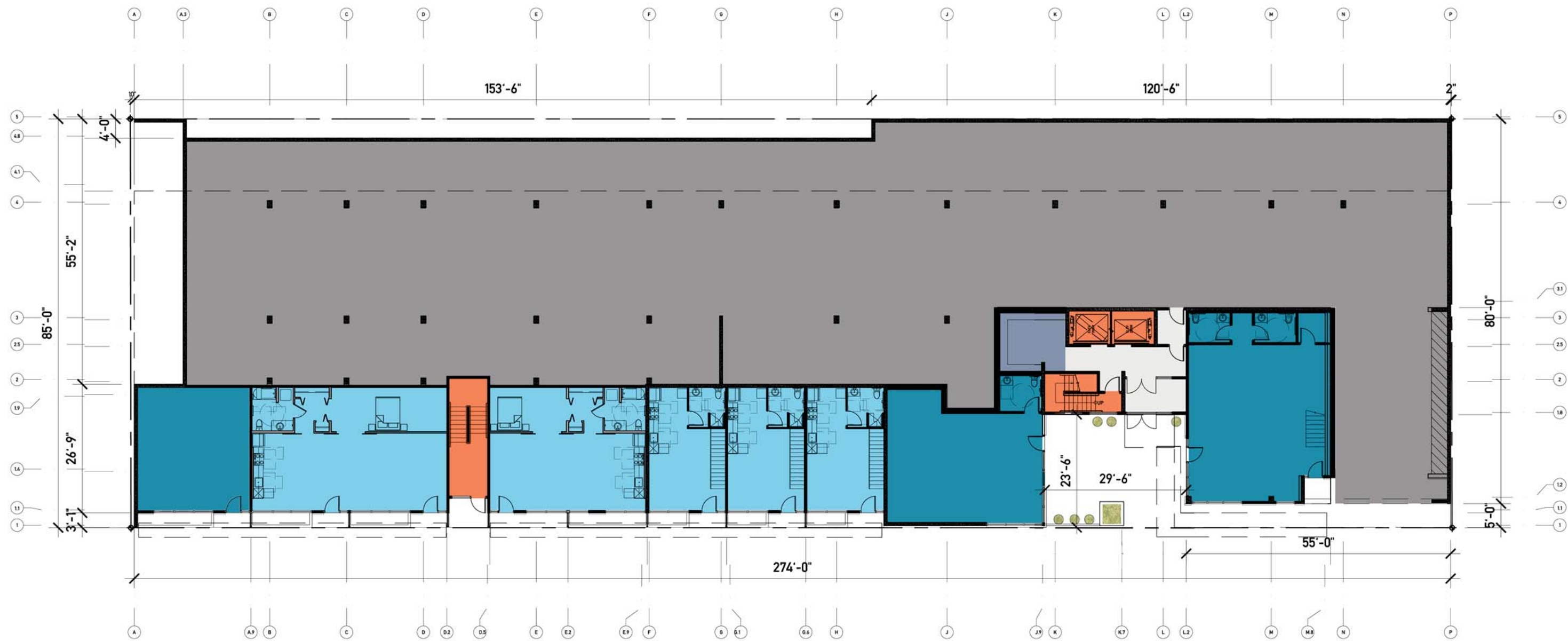


SITE PLAN



- RESIDENTIAL UNITS
- LIVE-WORK UNITS
- COMMERCIAL
- MAILROOM
- CIRCULATION
- PARKING
- BOH/STORAGE
- CORRIDOR

LEVEL P1-PARKING FLOOR PLAN

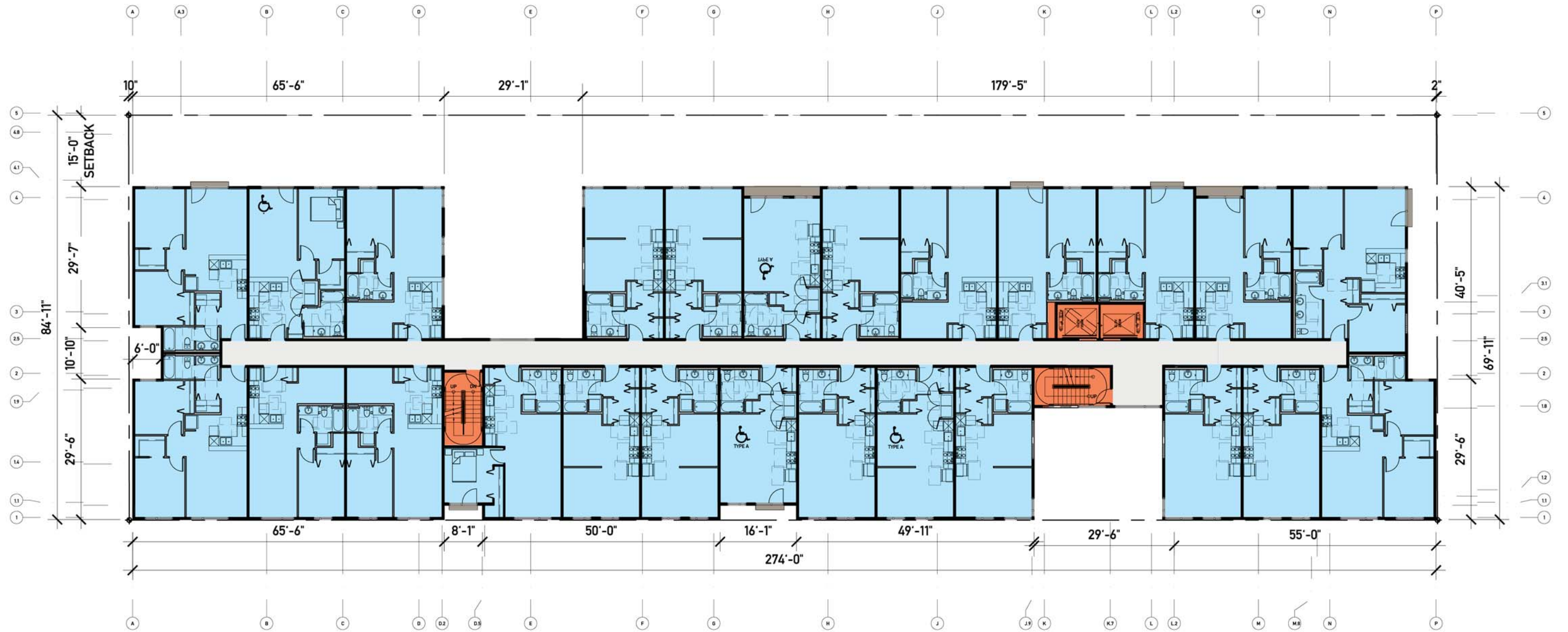


LEVEL 0-LIVE WORK/BASEMENT FLOOR PLAN

- RESIDENTIAL UNITS ■
- LIVE-WORK UNITS ■
- COMMERCIAL ■
- MAILROOM ■
- CIRCULATION ■
- PARKING ■
- BOH/STORAGE ■
- CORRIDOR ■



LEVEL 1-RESIDENTIAL FLOOR PLAN



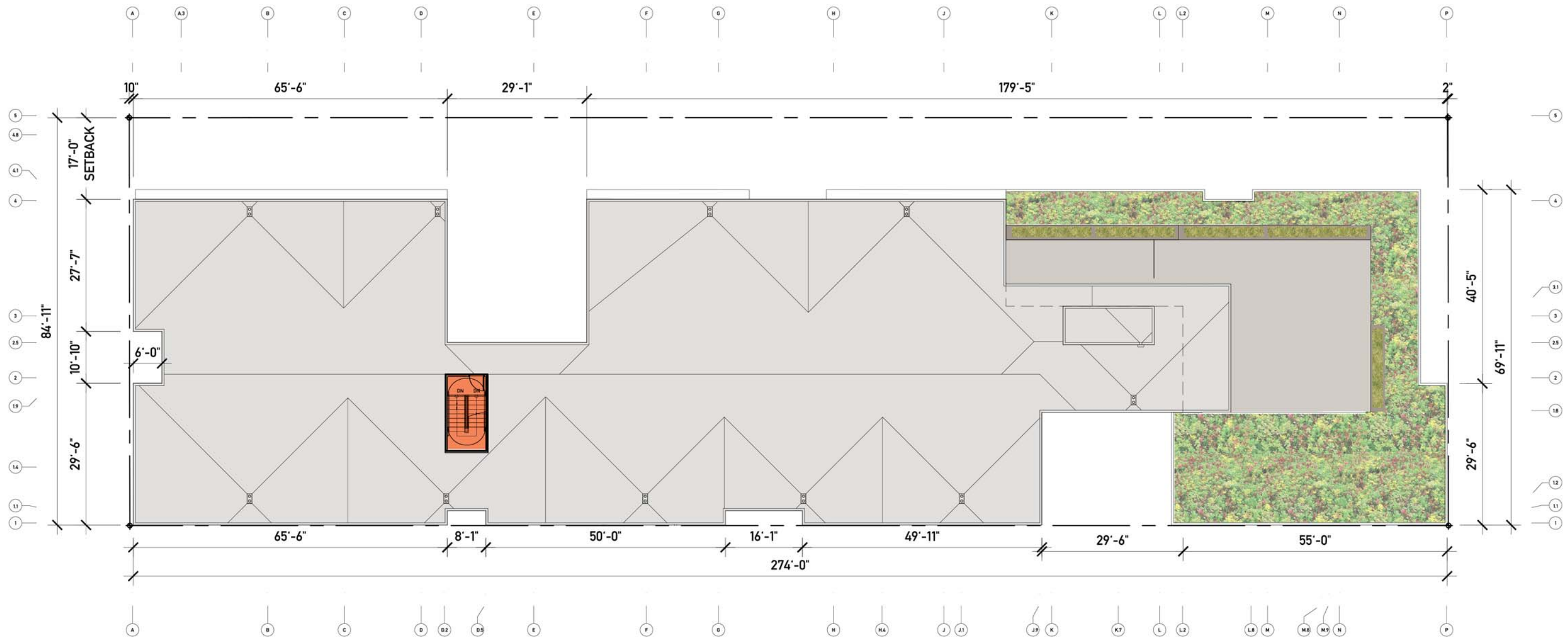
- RESIDENTIAL UNITS
- LIVE-WORK UNITS
- COMMERCIAL
- CIRCULATION
- PARKING
- BOH/STORAGE
- CORRIDOR

LEVEL 2 & 3-RESIDENTIAL FLOOR PLAN



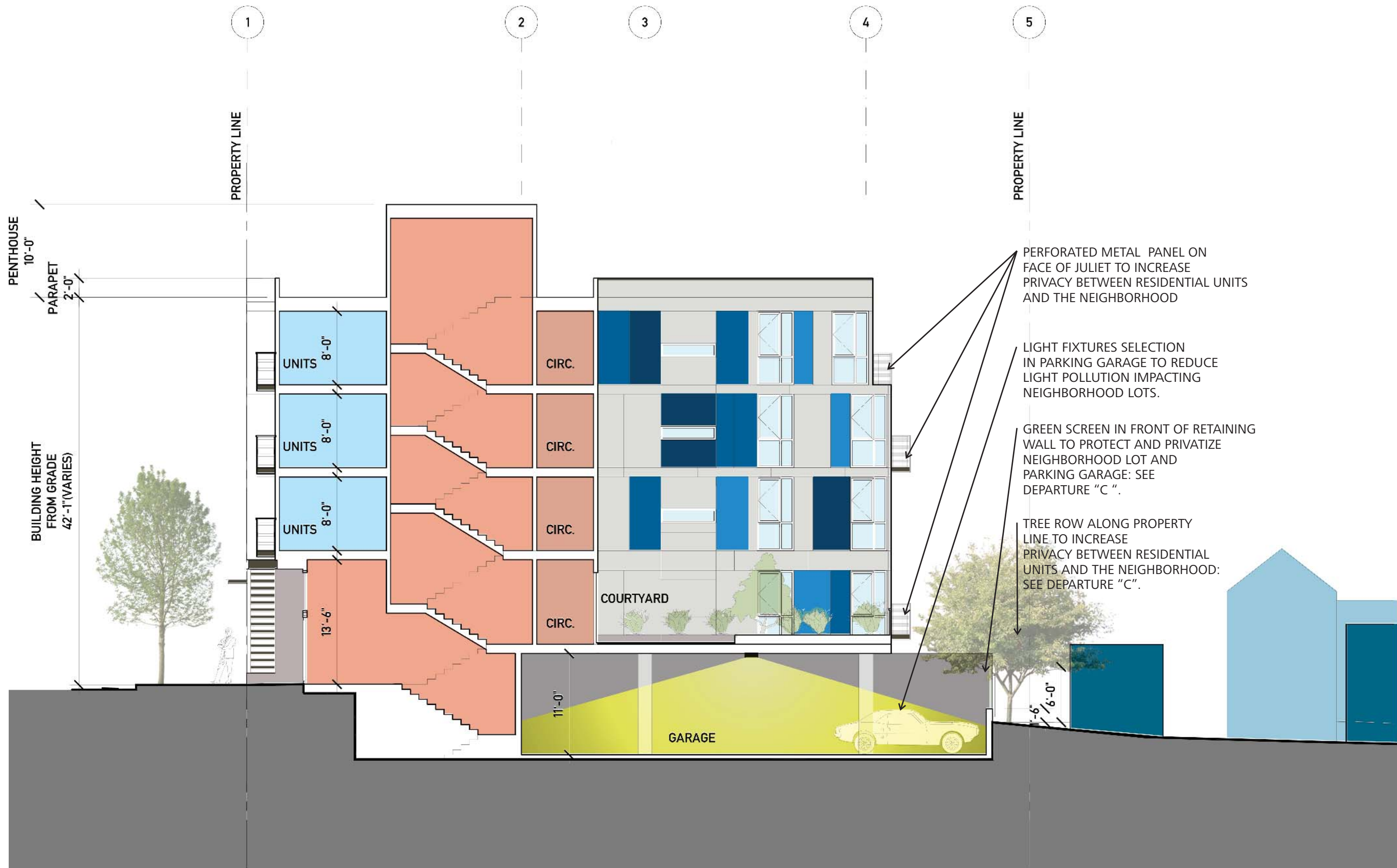
- RESIDENTIAL UNITS ■
- LIVE-WORK UNITS ■
- COMMERCIAL ■
- CIRCULATION ■
- PARKING ■
- BOH/STORAGE ■
- CORRIDOR ■

LEVEL 4-RESIDENTIAL UNITS/ROOF DECK FLOOR PLAN

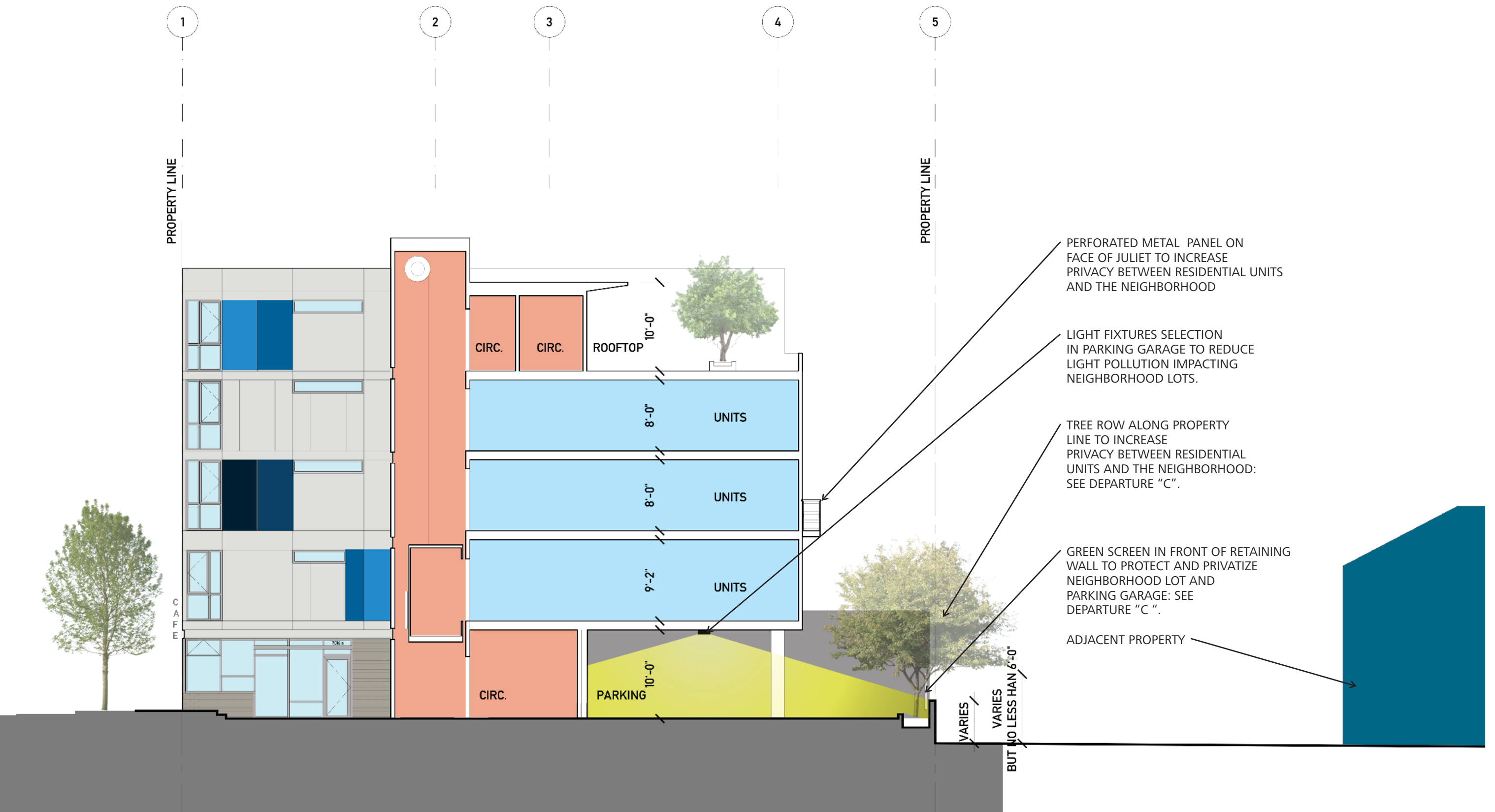


- RESIDENTIAL UNITS ■
- LIVE-WORK UNITS ■
- COMMERCIAL ■
- CIRCULATION ■
- PARKING ■
- BOH/STORAGE ■
- CORRIDOR ■

ROOF PLAN



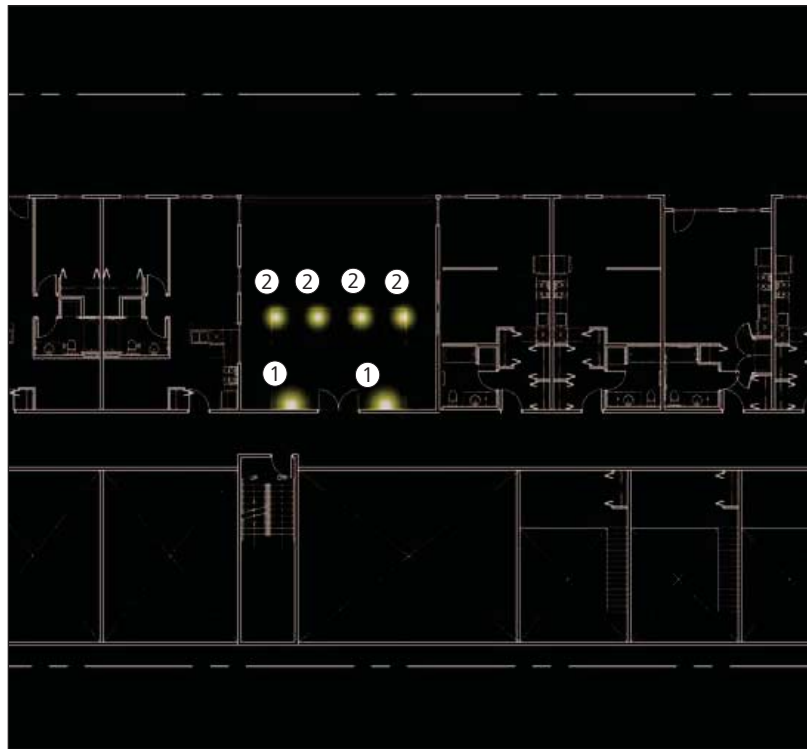
NORTH END-CROSS BUILDING/SITE SECTION



SOUTH END-CROSS BUILDING/SITE SECTION



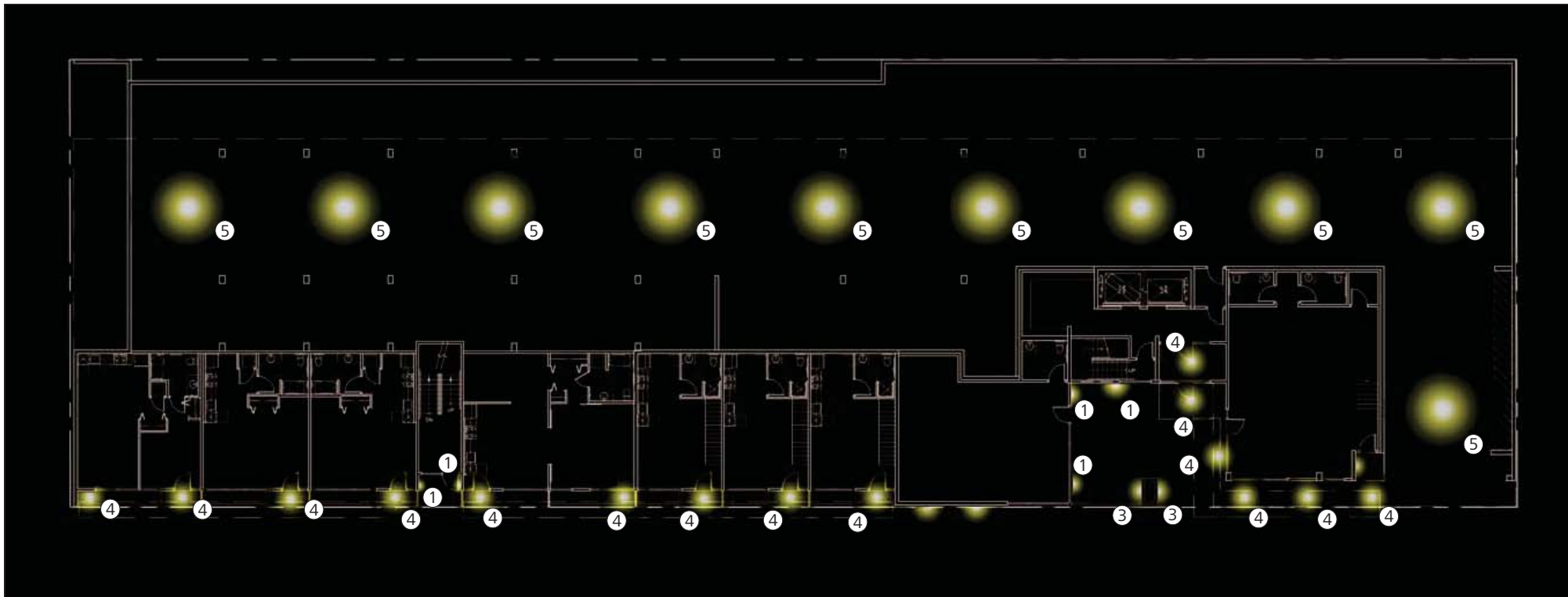
RENDERING ALONG 15TH AVE LOOKING SOUTH/EAST



AT EAST COURTYARD-LEVEL 1



AT ROOF DECK-LEVEL 4



AT GRADE-LEVEL 0



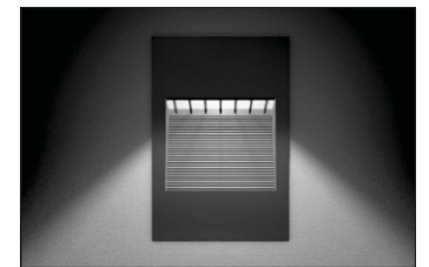
① Wall Mount Area LED Light



② LED Bollard Light



③ LED Light Fixture/Downlight



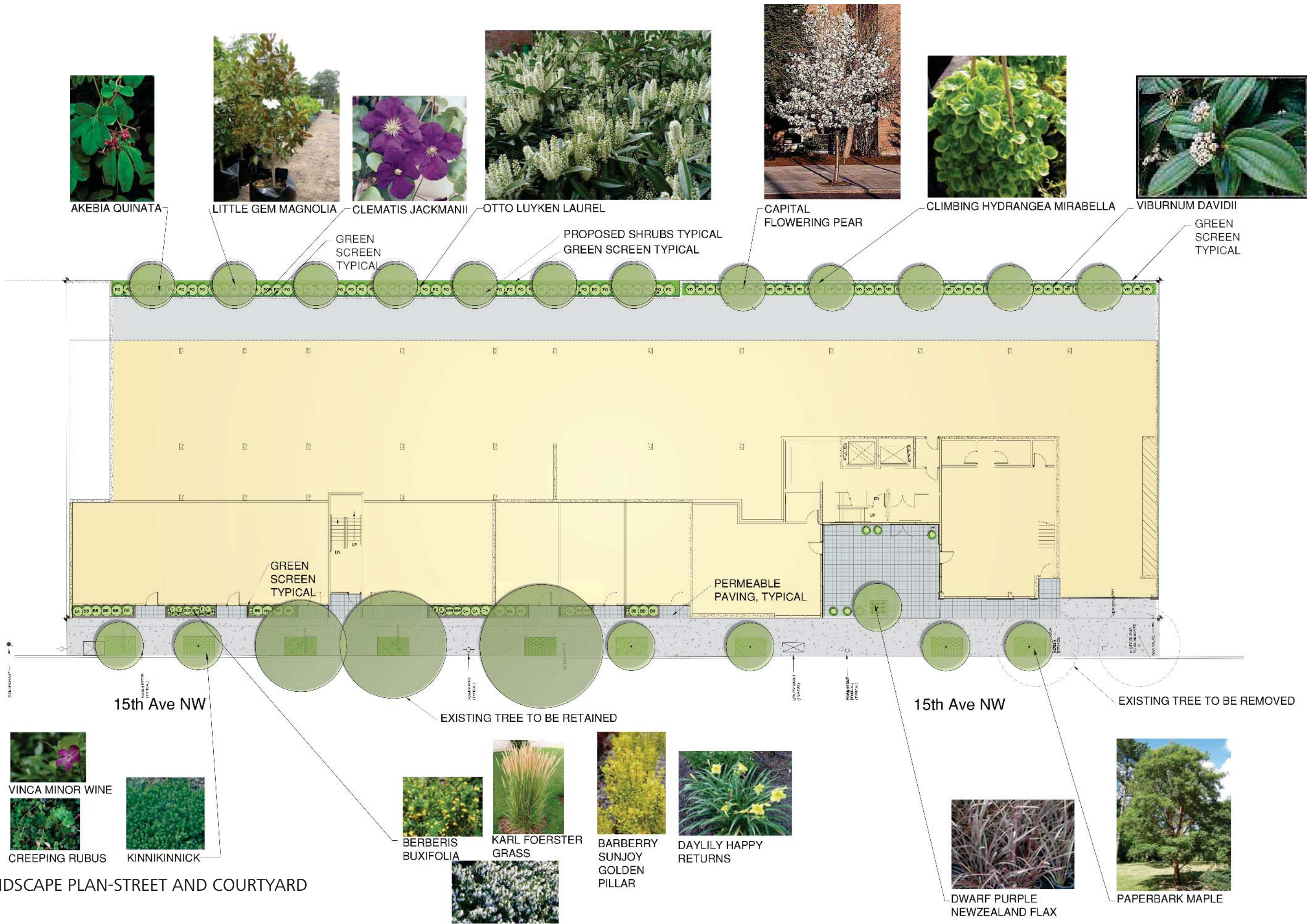
④ 4" Recessed LED Can Light



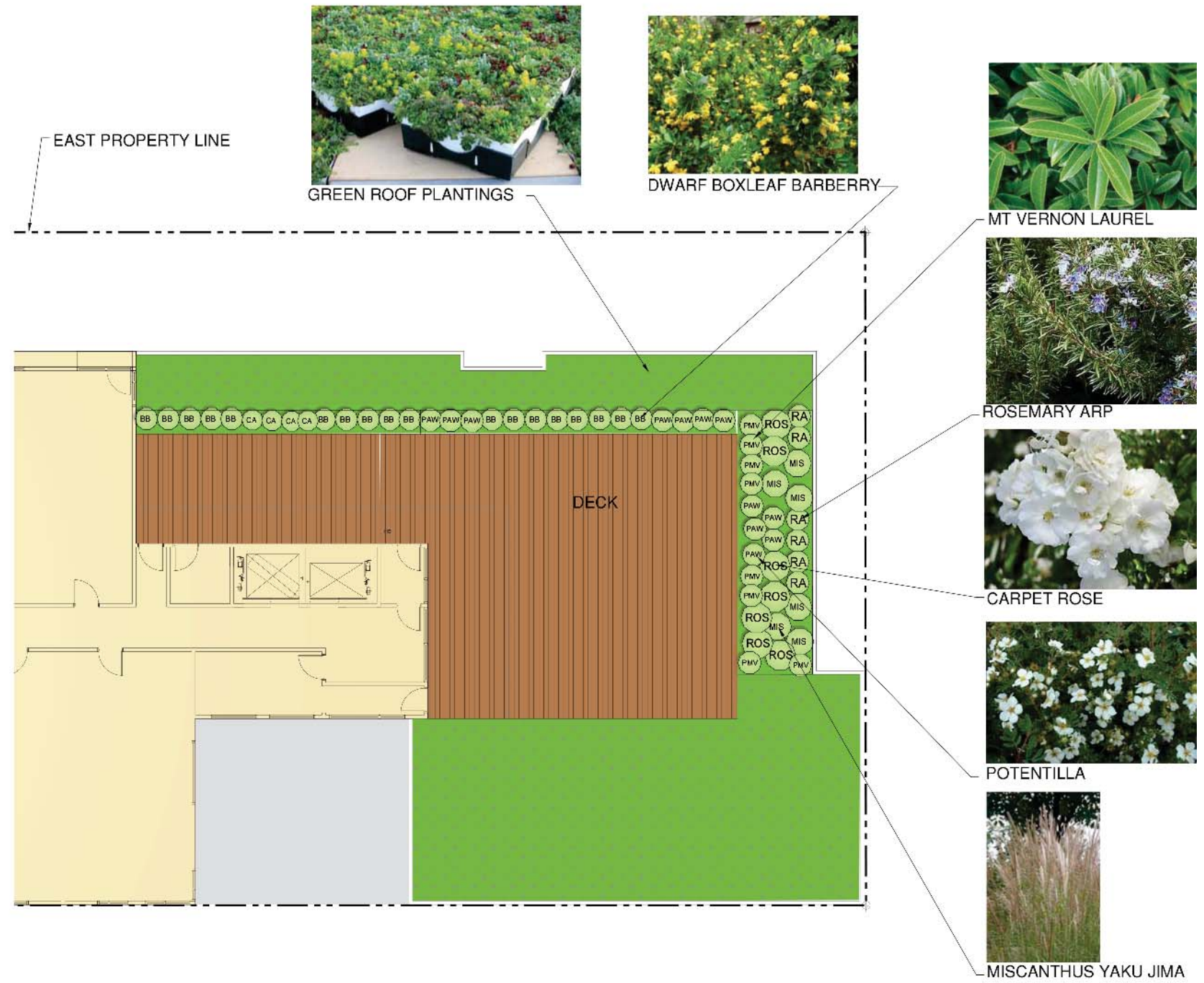
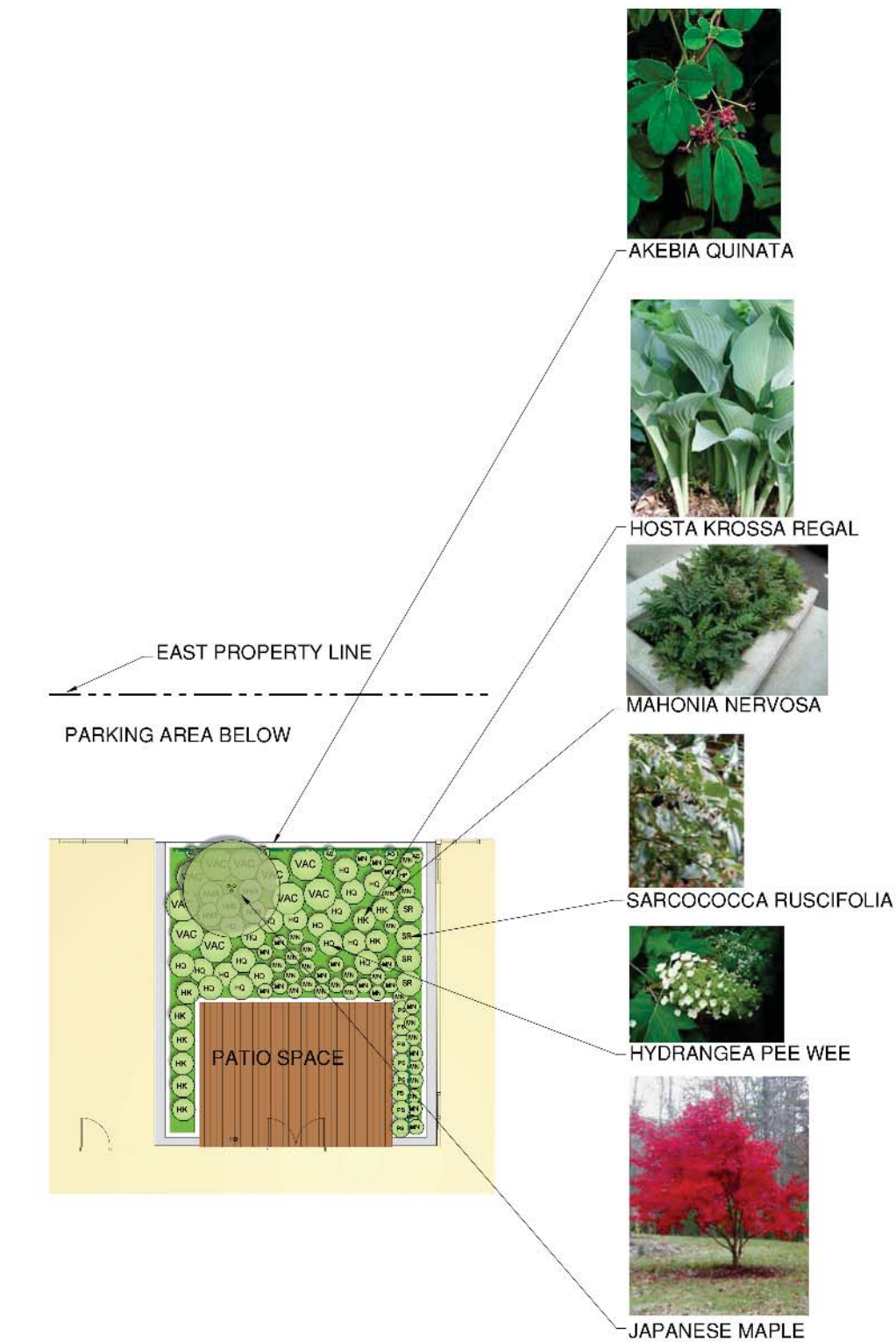
⑤ LED Parking light



EXTERIOR LIGHTING PLAN

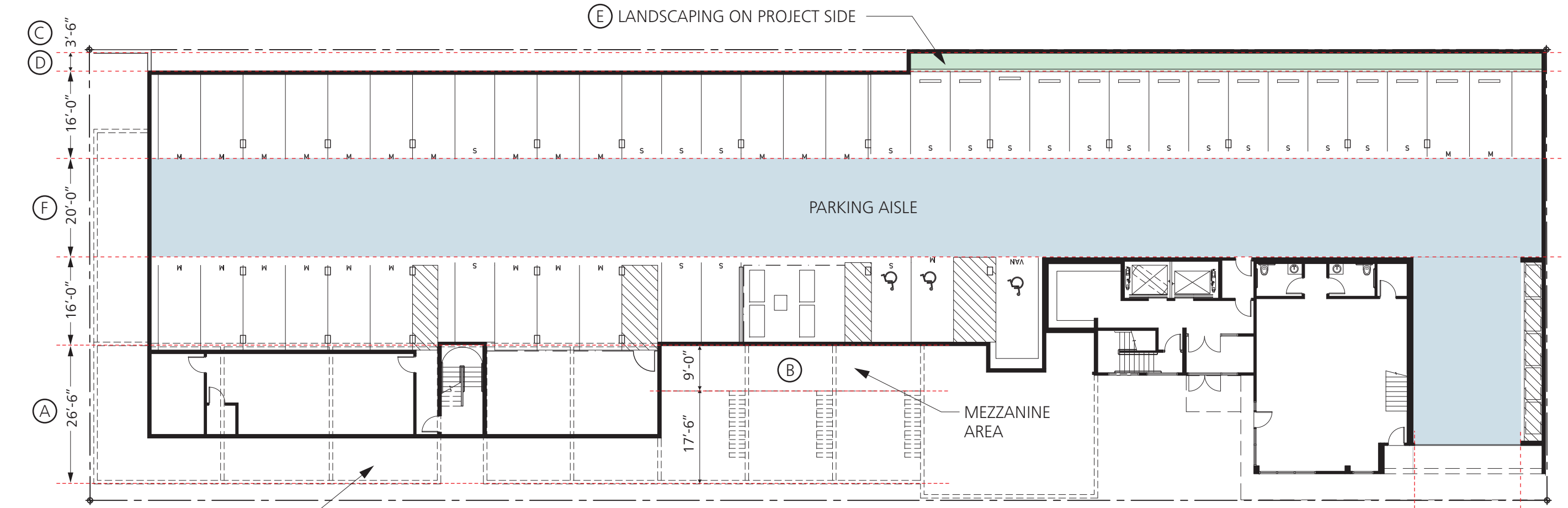


LANDSCAPE PLAN-STREET AND COURTYARD



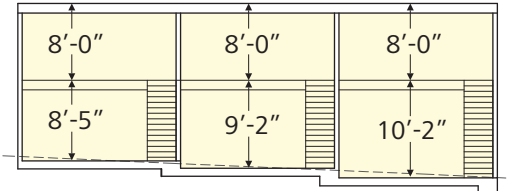
LANDSCAPE PLAN-EAST COURTYARD AND ROOFTOP

CODE REFERENCE/REQUIREMENT	DEPARTURE REQUEST	RATIONAL FOR DEPARTURE:
<p>A SMC.23.47A.008 Street-level Design development standards</p> <p>B.3. Height and depth provisions for new structures or new additions to existing structures. Non-residential uses shall extend an average depth of at least 30 feet and a minimum depth of 15 feet from the street-level street-facing facade. If the combination of the requirements of Sections 23.47A.005 or 23.47A.008 and this depth requirement would result in a requirement that an area greater than 50 percent of the structure's footprint be dedicated to non-residential use, the Director may modify the street-facing facade or depth requirements, or both, so that no more than 50 percent of the structure's footprint is required to be non-residential. Non-residential uses at street level shall have a floor-to-floor height of at least 13 feet.</p>	<p>Asking for depth reduction to 26'-6".</p>	<p>Reason for Departure: To allow for a wider right-of-way to the benefit of the pedestrian experience (Design Guidelines A4, C3, and D1) and to accommodate the required landscape buffer between parking and residential zone to the east (23.47A.016.D.). The wider sidewalk will provide a livelier and safer street level experience. Including a landscape buffer between the parking level and the adjacent residential uses will aid in softening the edge between the two uses and be more visually appealing.</p>
<p>B SMC.23.47A.008 Street-level Design development standards</p> <p>B.3. Height and depth provisions for new structures or new additions to existing structures. Non-residential uses shall extend an average depth of at least 30 feet and a minimum depth of 15 feet from the street-level street-facing facade. If the combination of the requirements of Sections 23.47A.005 or 23.47A.008 and this depth requirement would result in a requirement that an area greater than 50 percent of the structure's footprint be dedicated to non-residential use, the Director may modify the street-facing facade or depth requirements, or both, so that no more than 50 percent of the structure's footprint is required to be non-residential. Non-residential uses at street level shall have a floor-to-floor height of at least 13 feet.</p>	<p>Asking for reduction at some live/work units to 11'-0" and to allow, where mezzanine are located, a floor-to-floor height of 8'.</p>	<p>Project is on a site with R.O.W. slope of approximately 6% and base height limit of 40ft. In an effort to fit within these design constraints, the proposal includes some non-residential spaces that have a floor-to-floor height of min. 11'-0" at the street facing façade, and 3 live/work units include interior mezzanines that effectively reduce the floor-to-floor height to 8' at those mezzanines. The floor-to-floor heights of each space is different, based on location relative to the street slope and the average of all street level non-residential uses is 13.53 ft. The design meets the guidelines of A-1 by reflecting the existing topography of the site and stepping of the massing lends sensitivity to the massing of adjacent uses per B-1. The design also meets the intent of D-11 providing an appropriate amount of transparency.</p>
<p>C SMC23.47A.016 Landscaping and screening standards</p> <p>D. Screening and landscaping requirements for specific uses. When there is more than one use that requires screening or landscaping, the requirement that results in the greater amount applies.</p> <p>1. Surface parking areas.</p> <p>a. 18 sqft. of landscape is required for each parking space with min. dimension of 4 ft. Requesting reduction in landscape dimension and location.</p>	<p>Requesting reduction in landscape dimension and location.</p>	<p>Reason for Departure: In order to maintain minimum parking requirement and provide landscape buffer between parking and residential zone, landscaping area is located along east property line and planting dimension is reduced to 3'-0". (Design Guidelines B1, E2, and E3). The landscape buffer meets the intent of the code and addresses the relationship of the parking adjacency to the residential use to the east.</p>
<p>D SMC23.47A.016 Landscaping and screening standards</p> <p>D.1.c. Screening of surface parking areas.</p> <p>5 ft. wide landscape buffer inside a 6 ft high screen is required between surface parking and adjacent residential zone. Requesting reduction width from 5'-0" to 3'-0" of landscape buffer strip.</p>	<p>Requesting reduction width from 5'-0" to 3'-0" of landscape buffer strip.</p>	<p>Reason for departure: surface parking level will be either higher or lower than adjacent residential lots due to topography. A retaining wall will separate the parking from the neighbor lots at or near the lot line and the required screen will be a solid 3'-0" high concrete retaining wall. Design Review board has asked for trees to be planted in the landscape strip as part of the buffer to parking and as a method of softening the edge of the project and project massing. Proposal significantly screens the parking level from adjacent residential yards and softens and enhances the edge between the two. Trees and green screen aid in stepping the massing from lot line to structure beyond. (Design Guidelines A5, B1 and E2).</p>
<p>E SMC23.47A.016 Landscaping and screening standards</p> <p>D.1.c. Screening of surface parking areas.</p> <p>1) Three-foot-high screening is required along street lot lines.</p> <p>2) Surface parking abutting or across an alley from a lot in a residential zone must have 6-foot-high screening along the abutting lot line and a 5-foot-deep landscaped area inside the screening.</p>	<p>Requesting alternative arrangement of landscape strip and screen such that the screen can be placed on the project side of the landscaping.</p>	<p>Reason for departure: surface parking level will be either higher or lower than adjacent residential lots due to topography. A retaining wall will separate the parking from the neighbor lots at or near the lot line and the required screen will be a solid 3'-0"- 5'-0" high concrete retaining wall. Design Review board has asked for trees to be planted in the landscape strip as part of the buffer to parking and as a method of softening the edge of the project and project massing. For the south half of the parking area, landscape strip is located on the project side of the retaining wall and screen; on the north half of the parking area the landscape strip is located on the neighbor side of the retaining wall. (Design Guidelines A5, B1 and E2). Locating the green screen on the lot line helps to minimize the impact of the concrete retaining wall and view of the parking level as viewed from the east looking west.</p>
<p>F SMC 23.54.030 Parking space standards (Base on Exhibit "C") located at:</p> <p>E. Parking aisles</p> <p>1. Parking aisles shall be provided according to the requirements.</p> <p>-For medium stalls -aisle width required is 22 feet; for 90 degree small stalls it is 20 feet.</p>	<p>Requesting reduction in aisle width to 20 ft.</p>	<p>Reason for departure: reducing the parking aisle backing distance allows more space for landscaping at the east edge while providing 48 stalls, 8 above the 42 stalls required. Accommodating the landscape strip meets SMC 23.47A.016.D and the intent of design guidelines A5, B1 and E2 by respecting the adjacency of residential use and enhancing the project design at the lot line.</p>
<p>G SMC 23.54.030 Parking space standards</p> <p>D.2.a.2) Minimum Non-residential use driveways widths or 22 feet and the maximum width of 25 feet.</p>	<p>Requesting reduction in curb cut and driveway width to 20 ft.</p>	<p>Reason for departure: requesting departure for aisle width reduction to 20 ft. which coincides with driveway width. This reduction also helps to minimize the presence of the parking entrance on the street facing façade per design guideline D-5 by reducing the visual impact of entrance.</p>
<p>H SMC 23.47A.3.b Setback requirements</p> <p>A setback is required along any rear or side lot line that abuts a lot in a residential zone, as follows: b. For each portion of a structure above 40 feet in height, additional setback at the rate of 2 feet of setback for every 10 feet by which the height of such portion exceeds 40 feet.</p>	<p>Requesting a slight setback reduction</p>	<p>Reason for departure: We are providing a 2 foot setback above 40' for the northern part of the building and are requesting a departure for the southern part of the building which exceed the 40 foot limit by 1.08 foot due to the roof structure.</p>

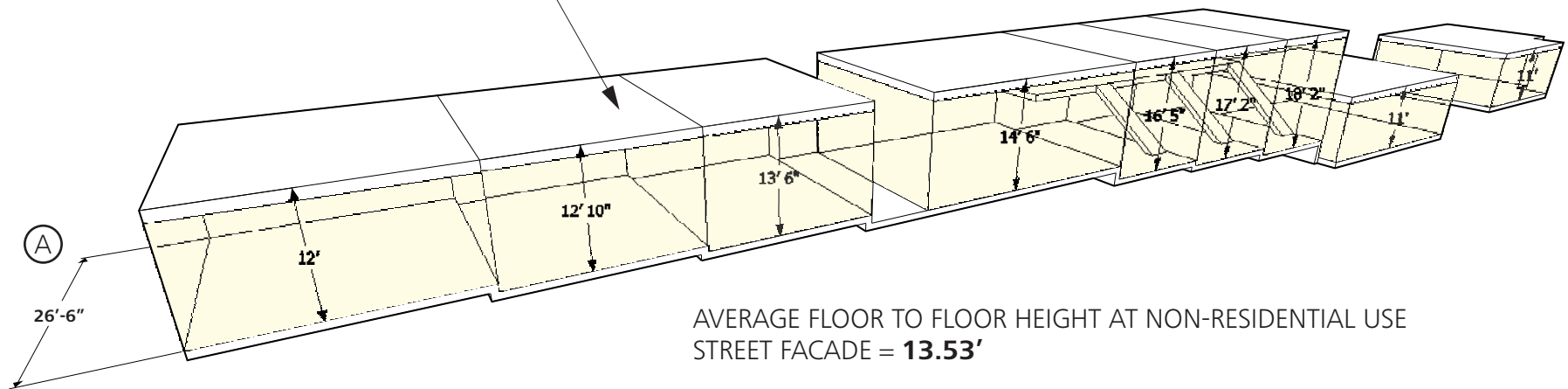


LIVE/WORK UNITS ABOVE GARAGE LEVEL

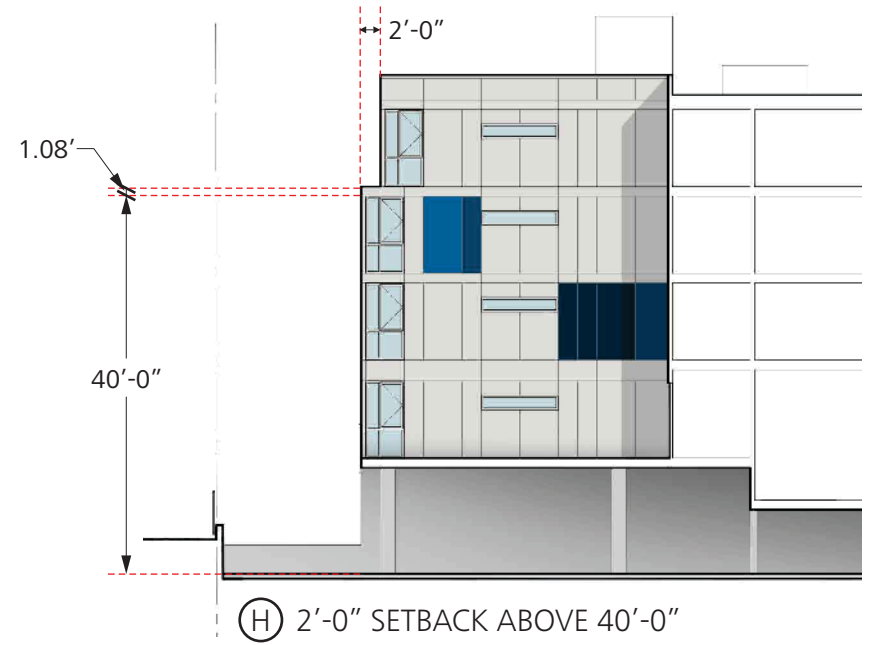
(B) MEZZANINE LEVEL
STREET LEVEL



(G) 20'-0" CURB CUT



AVERAGE FLOOR TO FLOOR HEIGHT AT NON-RESIDENTIAL USE STREET FACADE = 13.53'



DEPARTURE REQUEST



VOLTA



BREMERTON WATERFRONT CONDOMINIUMS



23RD AND MADISON



306 QUEEN ANNE AVE N



LEGACY AT PRATT PARK



MURIEL'S LANDING

OTHER PROJECTS BY THE TEAM

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7016 15TH NW DPD PROJECT #:3016160

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