



W
C
O
L
L
I
N
S
E
R
M
A
N

URBAN UNION

**Master Use Permit
Recommendation Packet**

Project #3015947

March 26, 2014

URBAN UNION: 501 FAIRVIEW AVENUE NORTH, SEATTLE, WA 98109

OWNER

509 Fairview Partners, LLC
c/o Schnitzer West, LLC
818 Stewart Street, Suite 700
Seattle, WA 98101

Contact
NAME: Luis Adan
PH: 206.626.3700
EMAIL: ladan@schnitzerwest.com

ARCHITECT

CollinsWoerman
710 Second Ave Suite 1400
Seattle, WA 98104
PH: 206.245.2041
FX: 206.245.2101

Contact
NAME: Tim Bissmeyer, *Project Manager*
PH: 206.245.2047
EMAIL: tbissmeyer@collinswoerman.com

APPLICANT

Permit consultants NW
26456 Marine View Drive South
Des Moines, WA 98198
PH: 425.681.4718

Contact
NAME: Jodi Patterson
PH: 425.681.4718
EMAIL: jodi@permitcnw.com

PROJECT DESCRIPTION

Urban Union is a 12 story office building located at 501 Fairview Avenue north, at the northwest corner of Fairview Avenue and Republican Street in the South Lake Union neighborhood. The project includes a total of 278,328 square feet of Floor Area with parking for 392 cars in Five levels below grade. The project includes 6,138 sf of grade related retail located on Republican Street and Fairview Avenue, with landscaped plazas fronting both streets.

The design intent for Urban Union is to create a building that is a great place to work and an asset to the neighborhood. The immediate neighborhood is undergoing tremendous change with at least four new projects underway. Urban Union is designed to expand the network of open space and pedestrian movement from the Amazon campus west along Republican Street, crossing over Fairview Avenue to the new projects across the street.

Urban Union is organized at the street level to take advantage of the site topography and create connections to sunlight and viable retail in order to provide great public space for tenants, visitors and pedestrians. The building form and placement on the site respond to the evolving context, including residential underway across Republican Street, and biotech and office projects on the east side of Fairview Avenue. The setbacks and orientation are intended to preserve views toward the waterfront and back toward the city as well.



DESIGN GUIDELINES

Provide a brief description of how the proposal meets the intent of the applicable citywide and neighborhood design review guidelines. Identify design guidelines most relevant to the proposal:

SEATTLE DESIGN GUIDELINE	SOUTH LAKE UNION SUPPLEMENTAL GUIDANCE	DESIGN RESPONSE
<div>A-1. Responding to Site Characteristics</div> <div><p>“The siting of buildings should respond to specific site conditions and opportunities such as non-rectangular lots, locations on prominent intersections, unusual topography, significant vegetation, and view or other natural features.”</p></div> <div></div>	<div></div> <div><p>The board is willing to recommend departures if they are needed to achieve sustainable design.</p></div> <div></div>	<div><p>The building connects to the pedestrian neighborhood network on Republican Street as well as the planned projects across Fairview Avenue by providing open space at Republican Street towards the heart of the Amazon Campus and the entry lobby reaching out to the corner at Fairview Avenue and Republican Street. Due to the significant grade changes along both streets, the exterior open space steps to relate to both the high corner at Fairview Avenue and Republican Street and the lower sidewalk elevation near the alley along Republican Street. All of the parking is located below the levels of the sidewalk, with access from the alley.</p></div> <div></div>
<div>A-2. Streetscape Compatibility</div> <div><p>“The siting of buildings should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.”</p></div> <div></div>	<div></div> <div><p>Provide pedestrian-friendly streetscape amenities, such as: tree gates, benches, and lighting.</p><p>Encourage provision of spaces for street level uses that vary in size, width, and depth. Encourage the use of awnings and weather protection along street fronts to enhance the pedestrian environment.</p><p>Where appropriate, consider a reduction in the required amount of commercial and retail space at the ground level, such as in transition zones between commercial and residential zones. Place retail in areas that are conducive to the use and will be successful.</p></div> <div></div>	<div><p>In option 3, the preferred option, the primary outdoor plaza faces Republican Street, the quieter pedestrian friendly street, while the Fairview Avenue street frontage is effectively widened by setting the ground floor back from the street. In option one, the retail and office at the base is brought out to the sidewalk line to maximize the usable area and create a more defined edge at the sidewalk, thereby bringing the retail closer to the sidewalk. In option two, the primary outdoor plaza and entry are located at the intersection, where it is more visible, yet impacted by vehicular traffic on Fairview Avenue.</p></div> <div></div>

Provide a brief description of how the proposal meets the intent of the applicable citywide and neighborhood design review guidelines. Identify design guidelines most relevant to the proposal:

SEATTLE DESIGN GUIDELINE	SOUTH LAKE UNION SUPPLEMENTAL GUIDANCE	DESIGN RESPONSE
<div>A-4. Human Activity</div> <div><p>“New development should be sited and designed to encourage human activity on the street.”</p></div> <div></div>	<div><p>Create graceful transitions at the streetscape level between public and private uses.</p><p>Keep neighborhood connection open and discourage closed campuses.</p><p>Reinforce pedestrian connections both within the neighborhood and to other adjacent neighborhoods. Transportation infrastructure should be designed with adjacent sidewalks, as development occurs to enhance pedestrian connectivity.</p><p>Create business and community activity clusters through co-location of retail and pedestrian uses as well as other high pedestrian traffic opportunities.</p><p>Design for a network of safe and well-lit connections to encourage human activity and link existing high traffic areas.</p></div> <div></div>	<div><p>The public plaza at the southwest corner of the site on Republican Street reinforces the neighborhood pedestrian network, and extends to the intersection of Republican Street and Fairview Avenue to facilitate connection to new development planned across Fairview Avenue. This plaza will be activated by a retail use (most likely a restaurant) and will be separated from the alley by landscaping, and overlooked in the preferred option by the Great Room. Fairview Avenue will be activated with retail along a portion of the frontage, and will either be held close to the sidewalk edge, or set-back to provide outdoor plaza area.</p></div> <div></div>
<div>B-1. Height, Bulk, and Scale Compatibility</div> <div><p>“Projects should be compatible with the scale of the development anticipated by the applicable Land Use Policies for the surrounding area and should be sited and designed to provide a sensitive transition to nearby, less intensive zones. Projects on zone edges should be developed in a manner that creates a step in perceived height, bulk, and scale between anticipated development potential of adjacent zones.”</p></div> <div></div>	<div><p>Address both the pedestrian and auto experience through building placement, scale, and details, with specific attention to regional transportation corridors such as Mercer Street, Aurora Avenue, Fairview Avenue, and Westlake Avenue. These locations, pending changes in traffic patterns, may evolve with transportation improvements.</p><p>Relate proportions of buildings to the width and scale of the street.</p><p>Articulate the building facades vertically or horizontally in intervals that relate to the existing structures or existing pattern of development in the vicinity.</p><p>Consider using architectural features to reduce building scale such as: landscaping; trellis; complimentary materials; detailing; accent trim.</p></div> <div></div>	<div><p>The preferred building massing responds to the adjacent development by setting the building back from all property lines, including the alley, to provide more access to light and air. This presents the narrowest façade to Republican Street, and holds the corner at Fairview Avenue. The tower form is notched at the southwest corner above the plaza to set the building mass back from the 400 Boren Avenue residential project. The preferred massing also sets the tower back approximately 16 feet from the Fairview Avenue property line. 10 feet is required in order to enhance the view corridor to Lake Union, and the north façade will be modulated to break up the massing of the building as viewed from the north.</p></div> <div></div>

DESIGN GUIDELINES

Provide a brief description of how the proposal meets the intent of the applicable citywide and neighborhood design review guidelines. Identify design guidelines most relevant to the proposal:

SEATTLE DESIGN GUIDELINE

SOUTH LAKE UNION SUPPLEMENTAL GUIDANCE

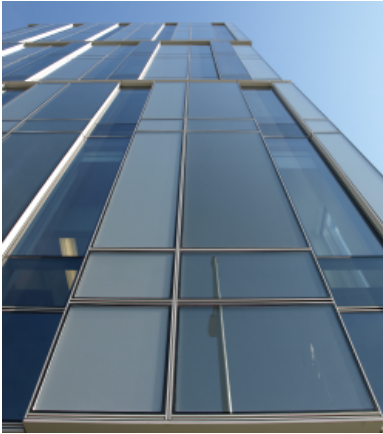
DESIGN RESPONSE

C-2. Architectural Concept and Consistency

“Building design elements, details and massing should create a well proportioned and unified building form and exhibit an overall architectural concept. Buildings should exhibit form and features identifying the functions within the building. In general, the roof line or top of the structure should be clearly distinguished from its facade walls.

Design the “fifth elevation” - the roofscape - in addition to the streetscape. As this area topographically is a valley, the roofs may be viewed from locations outside the neighborhood such as the freeway and Space Needle. Therefore, views from outside the area as well as from within the neighborhood should be considered, and rooftop elements should be organized to minimize view impacts from the freeway and elevated areas.

The notched south facade of the preferred option creates the impression of a more slender building and modulated north facade. The building will most likely incorporate floor by floor mechanical units to reduce the bulk of screened roof top mechanical units.



D-1. Pedestrian Open Spaces and Entrances

“Convenient and attractive access to building’s entry should be provided. To ensure comfort and security, paths and entry areas should be sufficiently lighted and entry areas should be protected from the weather. Opportunities for creating lively, pedestrian-oriented open space should be considered.”

New developments are encouraged to work with the Design Review Board and interested citizens to provide features that enhance the public realm, i.e. the transition zone between private property and the public right of way. The Board is generally willing to consider a departure in open space requirements if the project provides an acceptable plan for features such as: curb bulbs adjacent to active retail spaces where they are not interfering with primary corridors that are designated for high levels of traffic flow; pedestrian-oriented street lighting; street furniture.

The building entry will be accessed from the upper plaza and will be covered with a canopy that wraps the lobby element. The lower plaza provides access to retail and is built with ample landscaping, lighting, and seating. The retail entry will be recessed or covered with a canopy.



PROJECT CONTEXT



- A 400 Boren
- B 500 Fairview
- C Troy Block Development
- D 400 Fairview
- E Fiona Building

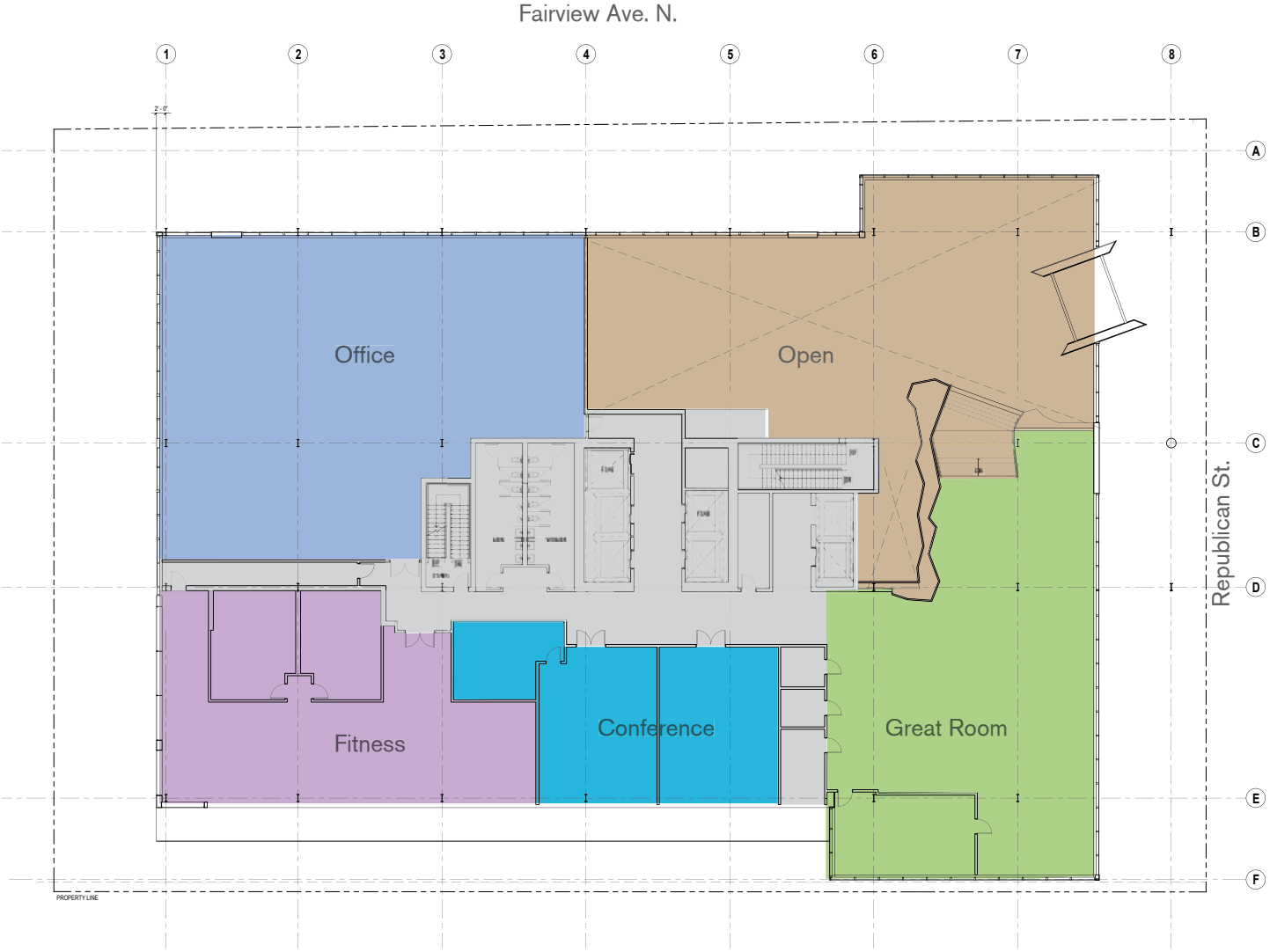
COMPOSITE SITE PLAN



CURRENT FLOOR PLANS



Level 1



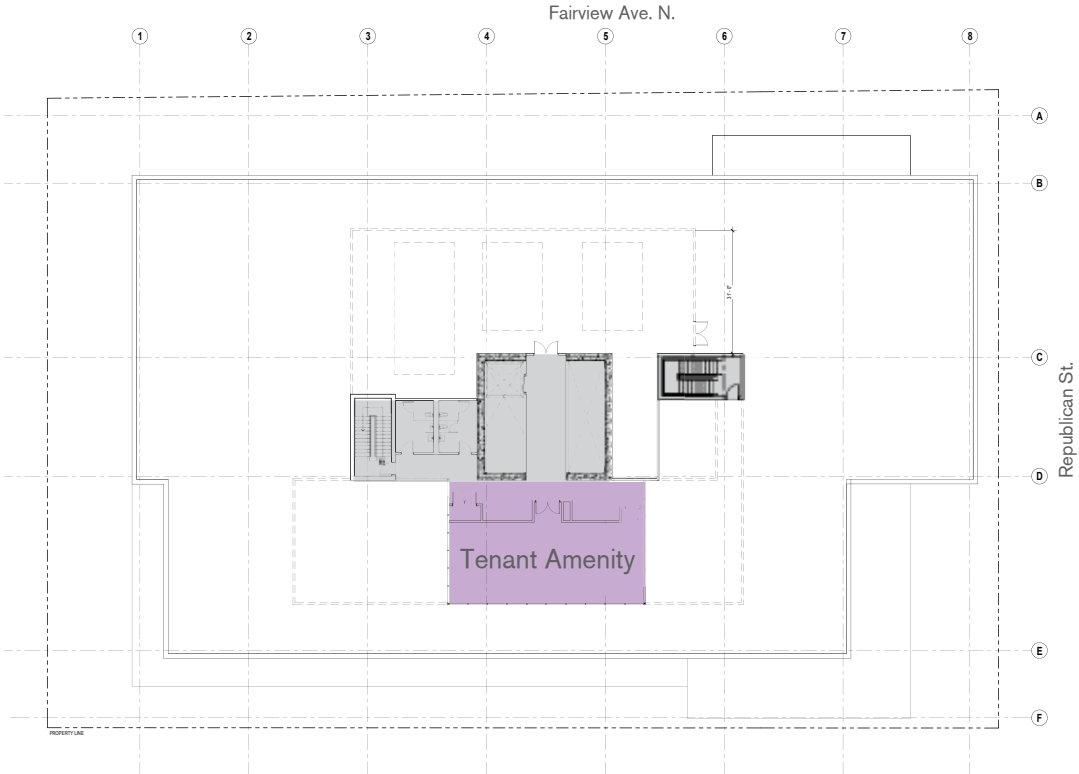
Level 2

CURRENT FLOOR PLANS

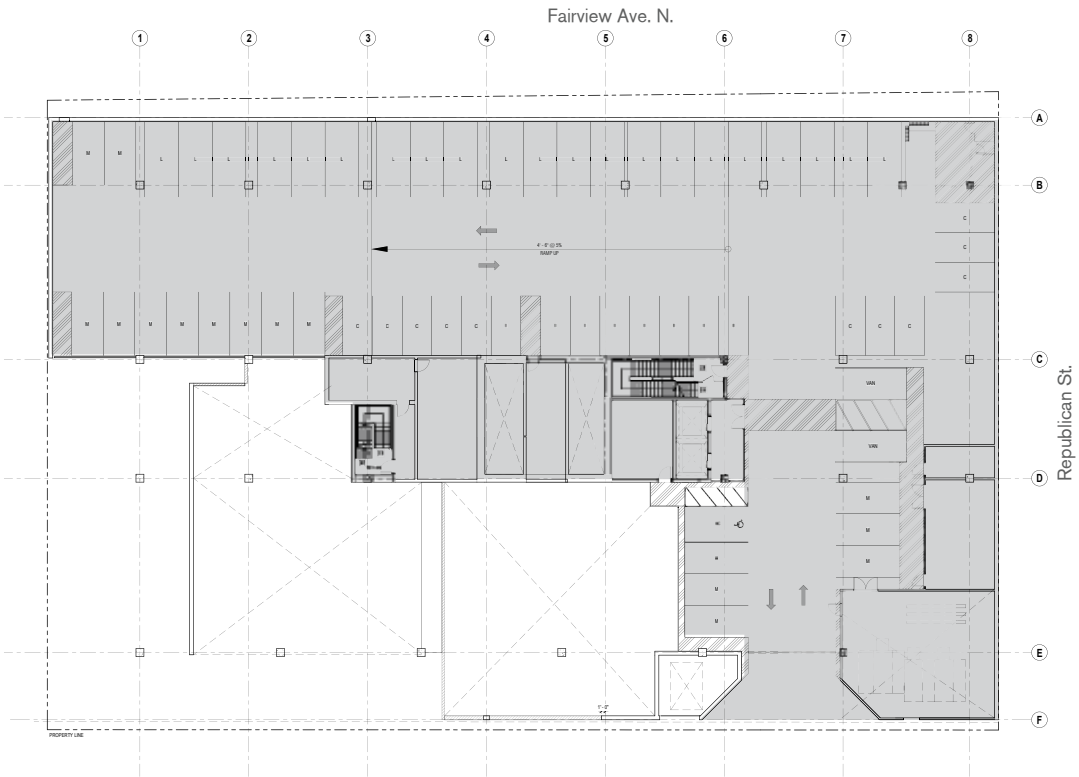
COLLINSWOERMAN | SCHNITZER WEST | URBAN UNION | MARCH 26, 2014



Level 5 (Typical levels 4-11)

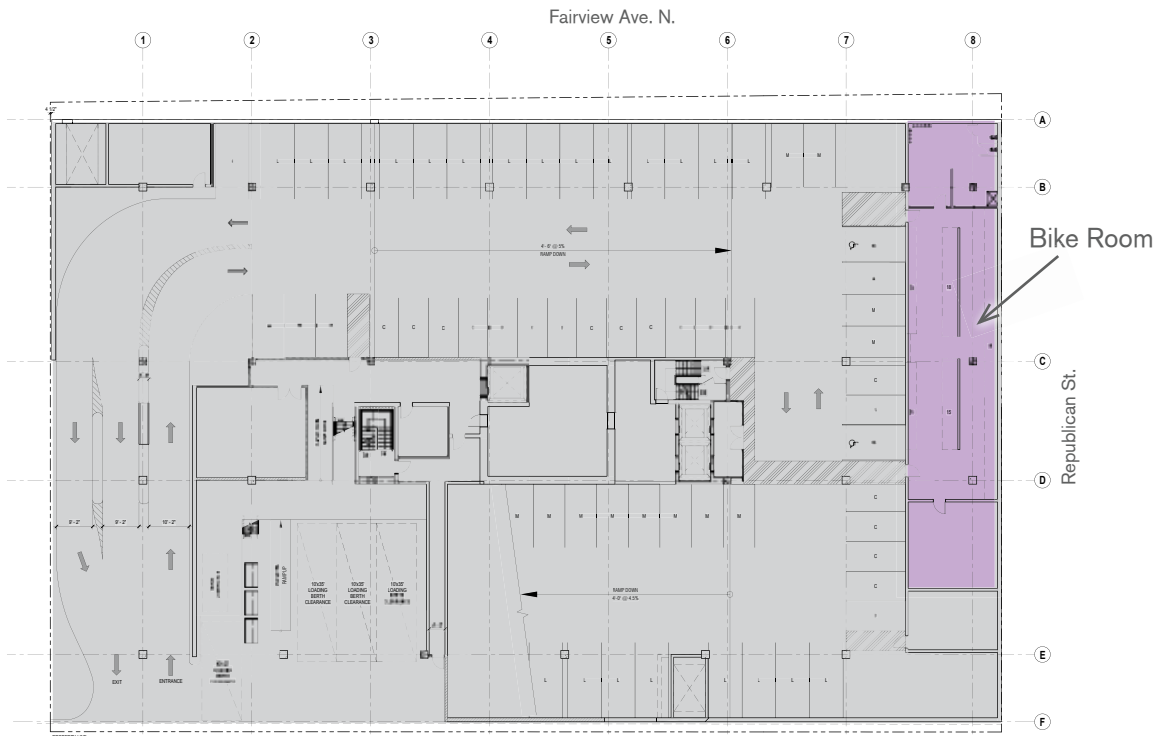


Roof

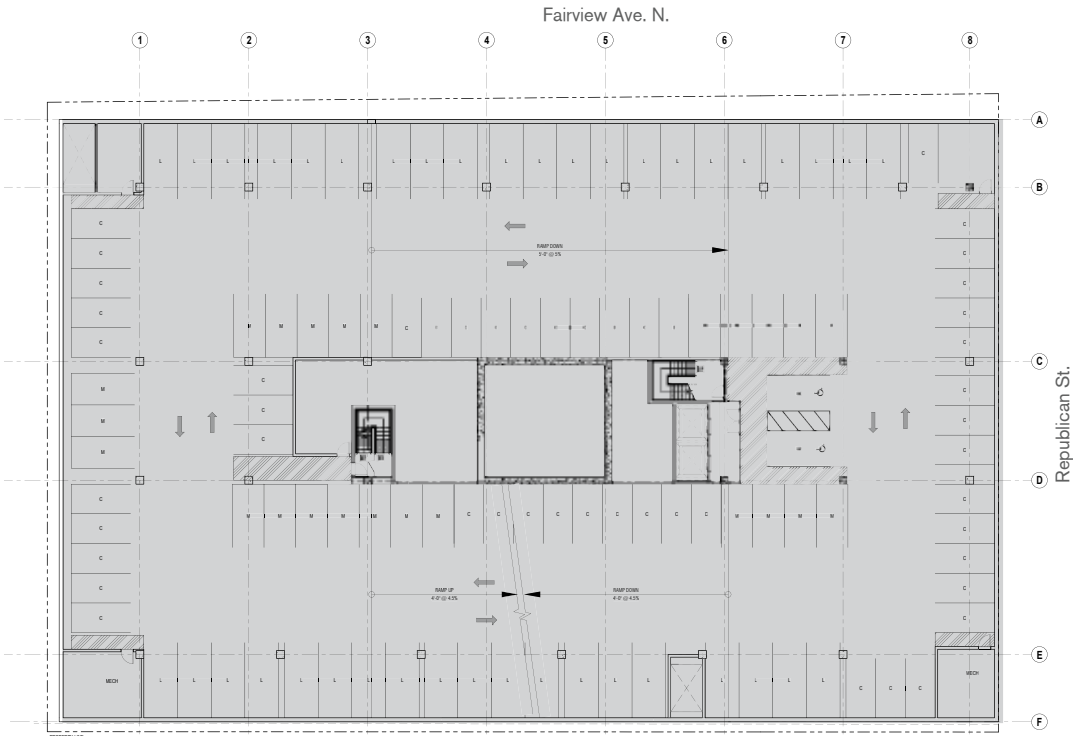


Level P1 Parking

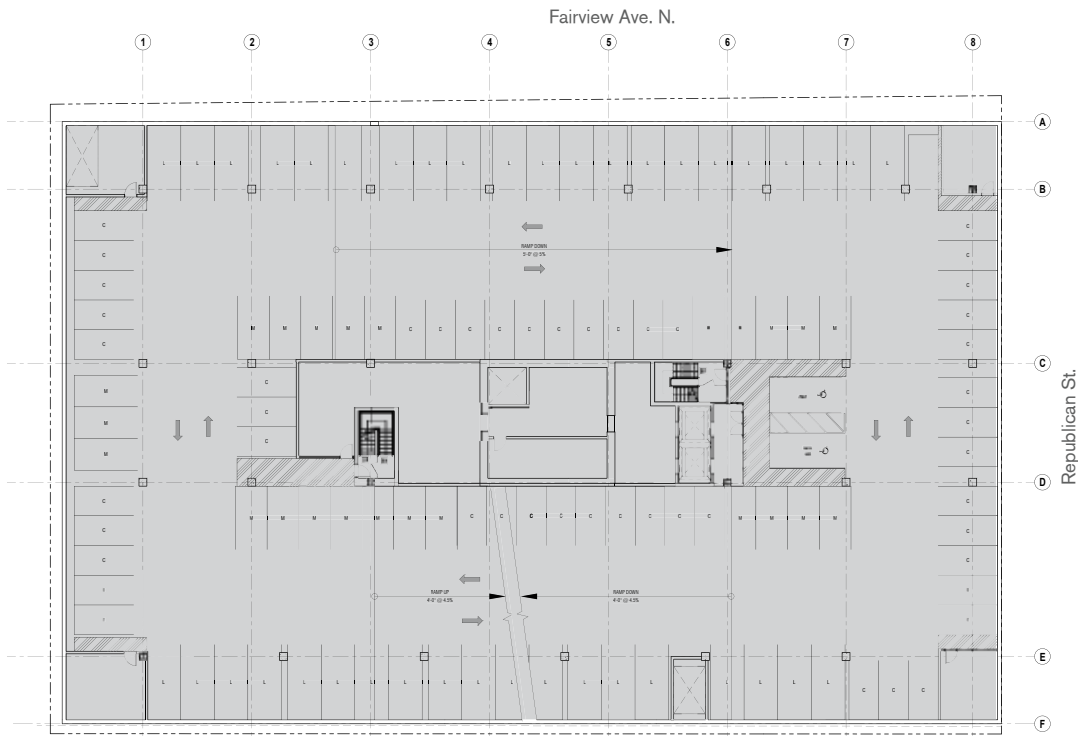
CURRENT FLOOR PLANS



Level P2 Parking



Level P4 Parking



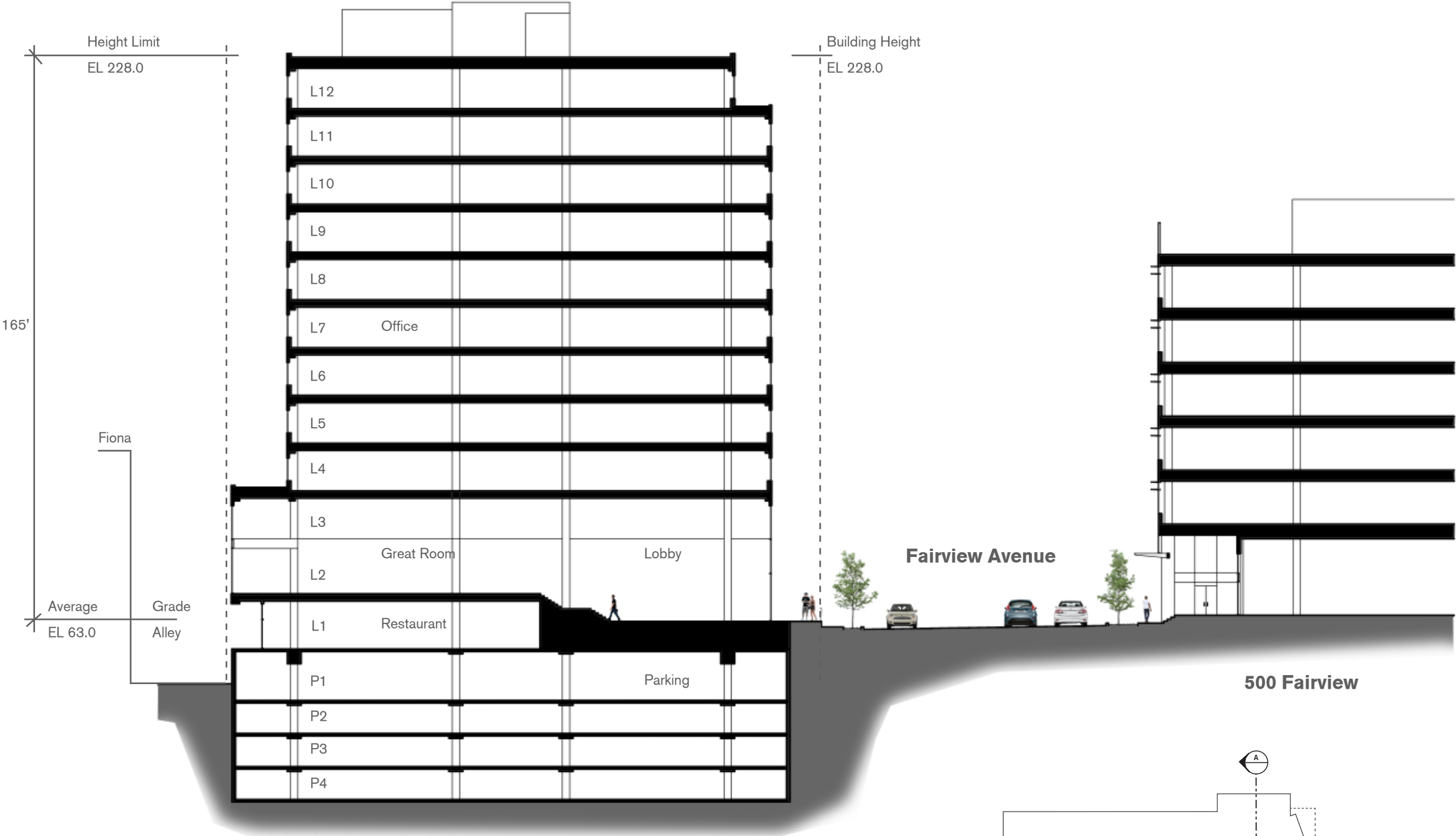
Level P3 Parking

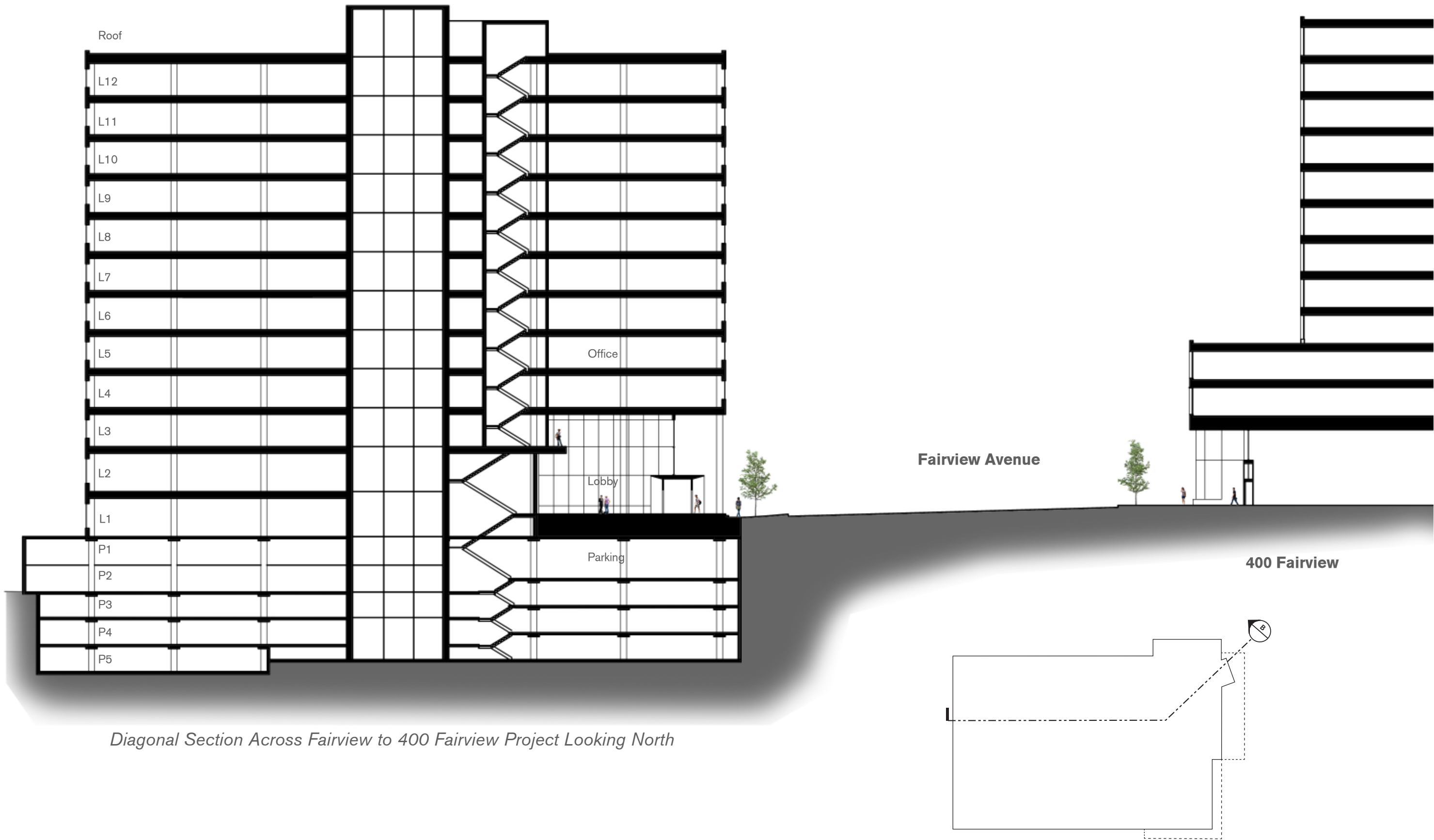


Level P5 Parking

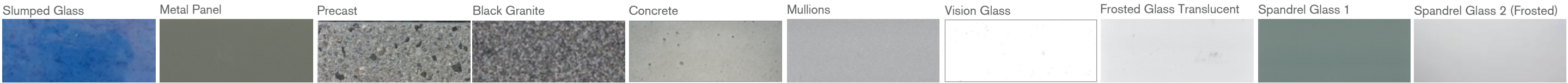
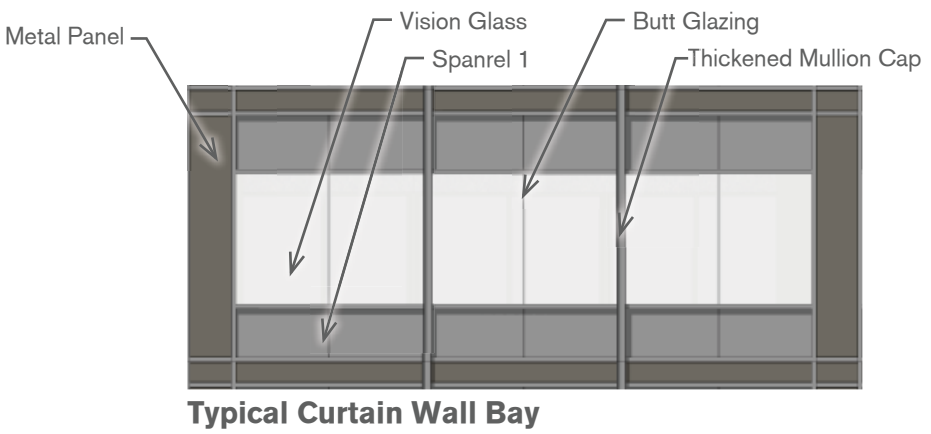
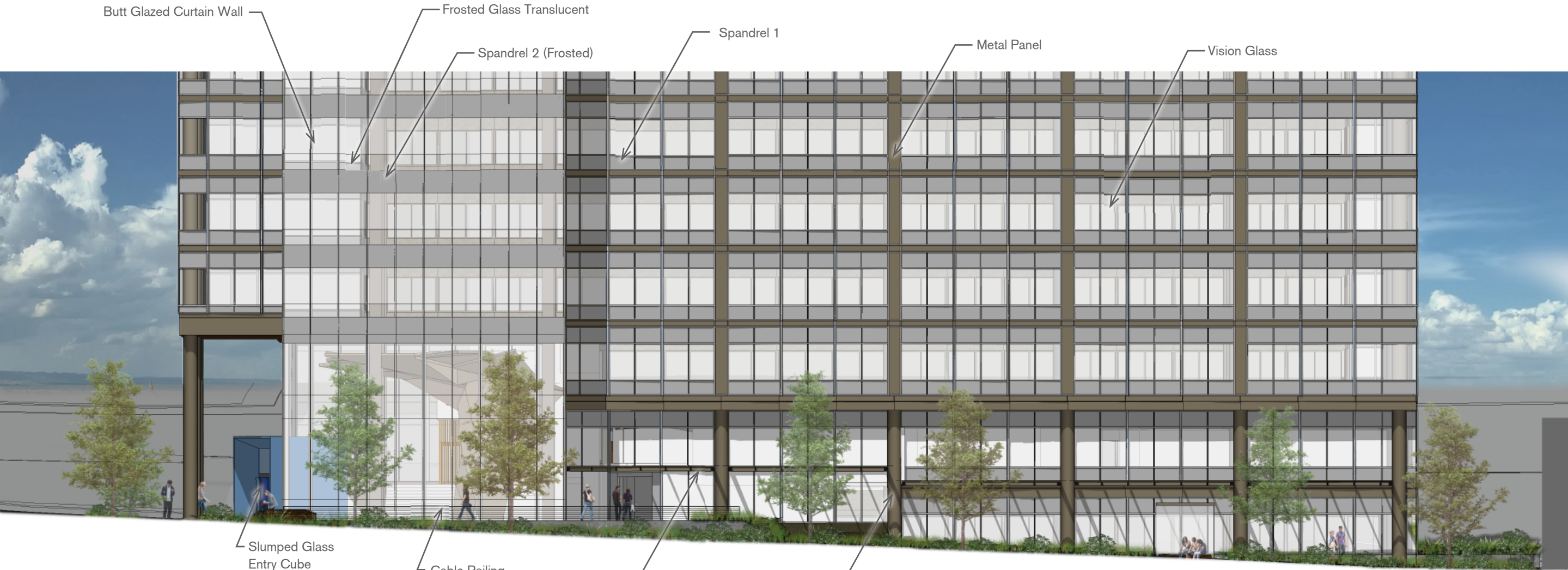


SECTIONS

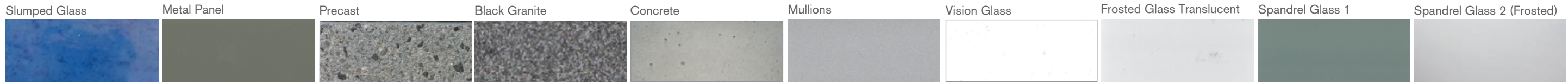
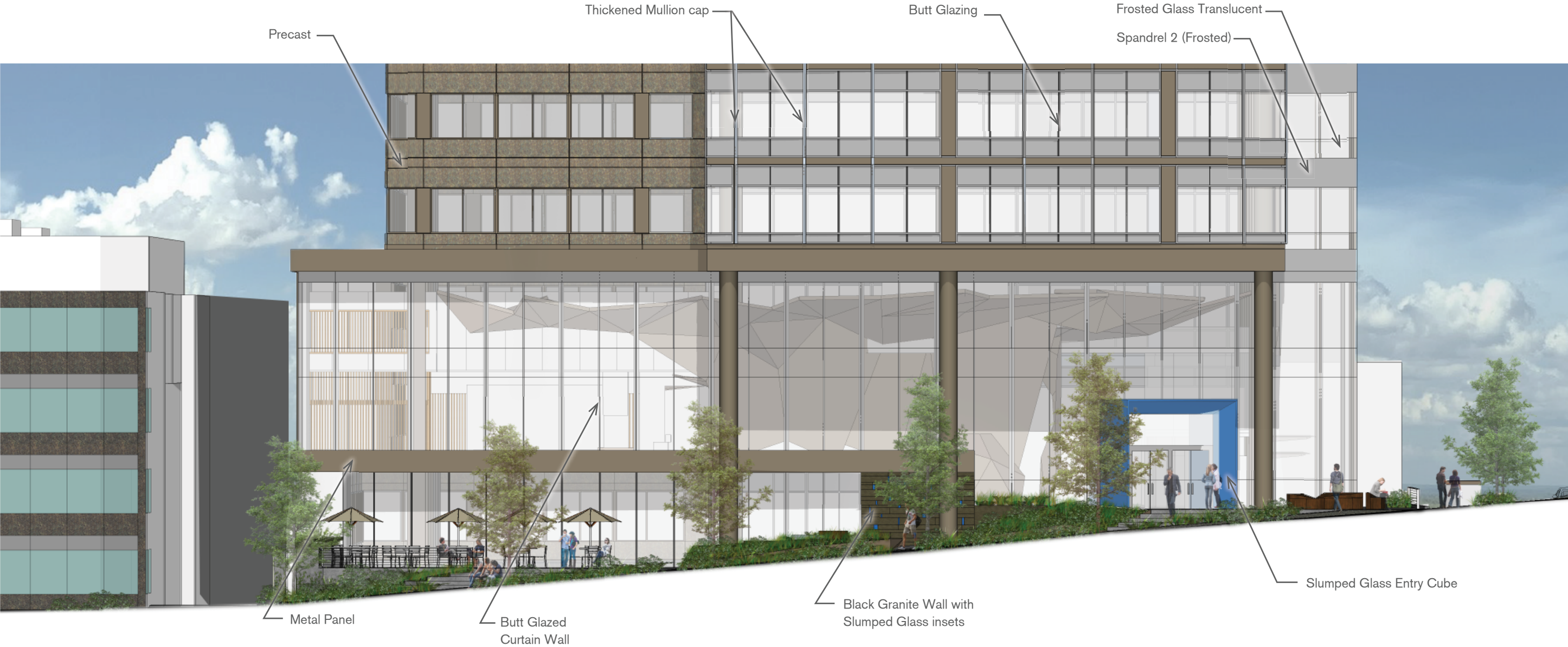




ENLARGED ELEVATIONS : EAST



ENLARGED ELEVATIONS : SOUTH

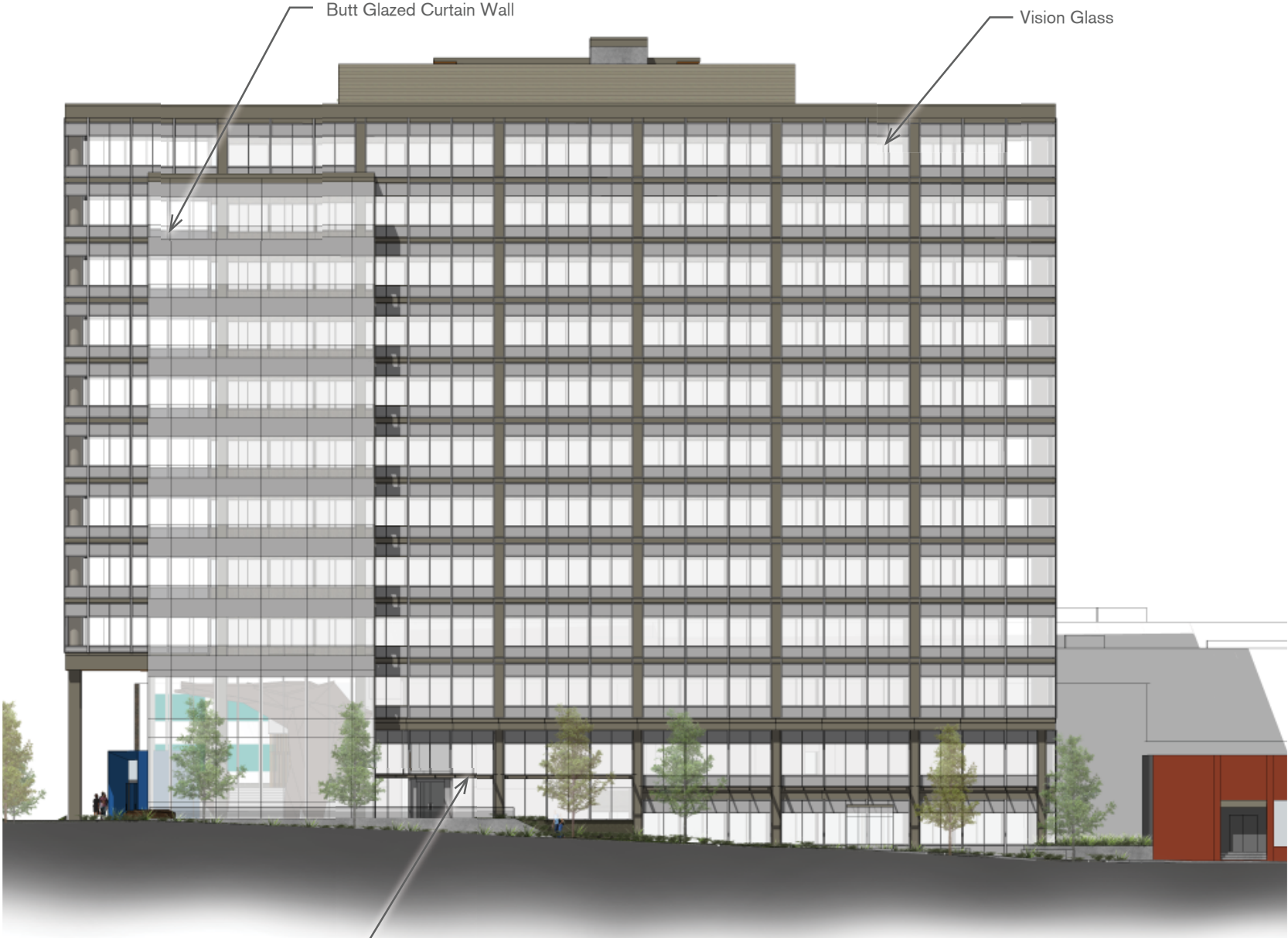


BUILDING ELEVATIONS

COLLINSWOERMAN | SCHNITZER WEST | URBAN UNION | MARCH 26, 2014



North Elevation



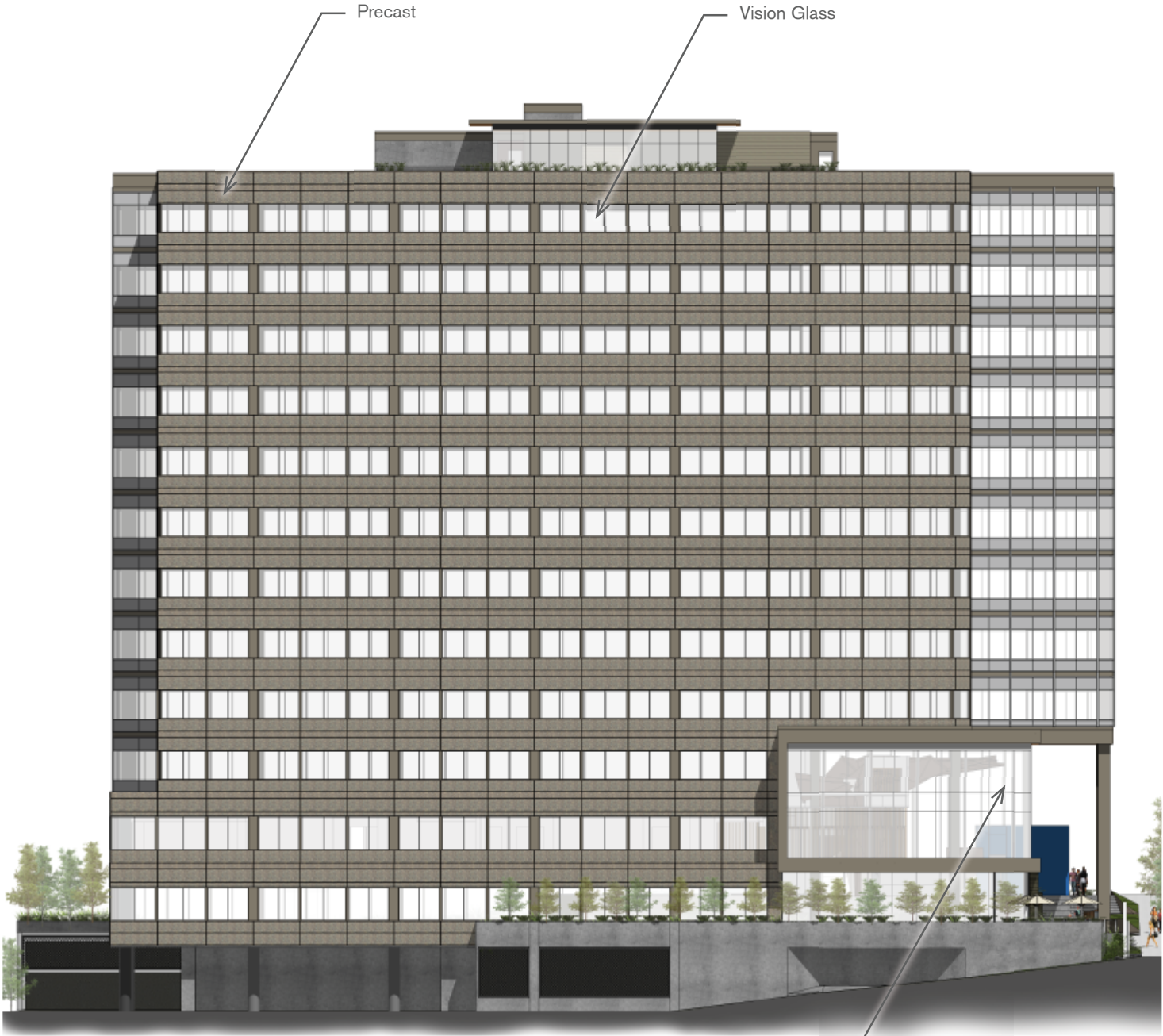
East Elevation

Slumped Glass	Metal Panel	Precast	Black Granite	Concrete	Mullions	Vision Glass	Frosted Glass Translucent	Spandrel Glass 1	Spandrel Glass 2 (Frosted)

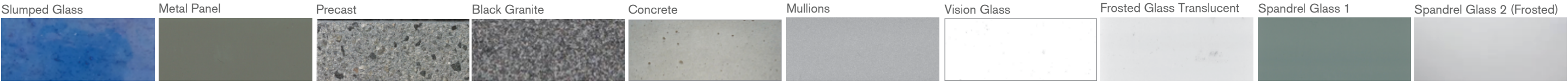
BUILDING ELEVATIONS



South Elevation



West Elevation



EARLY DESIGN GUIDANCE: DRB COMMENTS

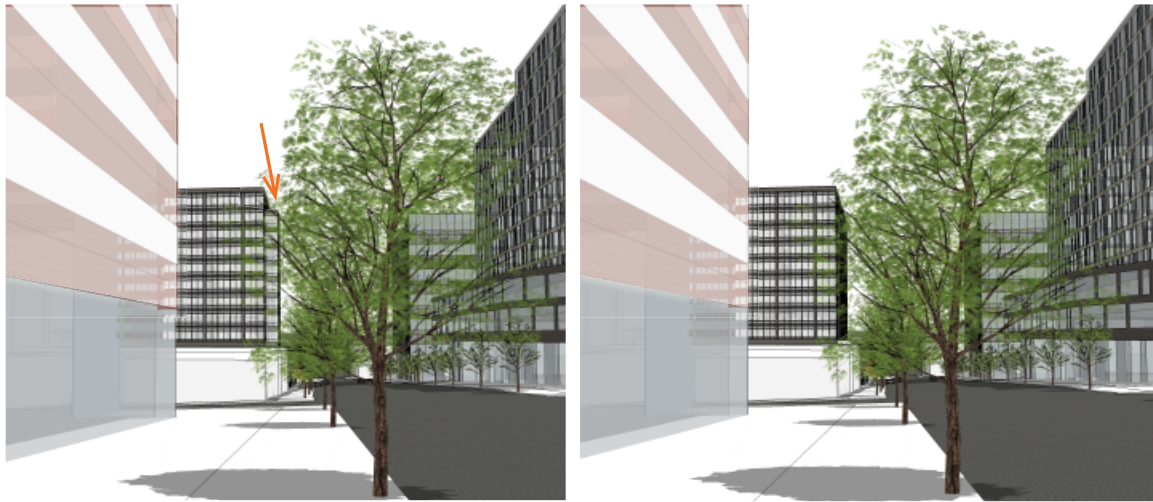
COLLINSWOERMAN | SCHNITZER WEST | URBAN UNION | MARCH 26, 2014



1. Southwest Plaza Relationship to the Sidewalk and Public Realm



2. Fairview Ave N. Street Level



3. Fairview Ave N. Upper Building Mass



4. Preferred Massing Option

DRB RESPONSE COMMENTS

1. Southwest Plaza Relationship to the Sidewalk and Public Realm. The southwest plaza and south edge should be designed to visually and physically relate to human activity at the sidewalk on Republican Street. (A-2, A-4, C-3, D-2, D-3, E-2)

- a. The proposed plaza will provide opportunity for an outdoor restaurant space, but the corner of this plaza includes a 5' retaining wall. The edge of the plaza needs to be treated to create a better visual transition to the sidewalk. Possible strategies include landscaping, water features, boulders, etc. (A-2, C-3, D-1, D-2, D-3, E-2)

RESPONSE: The wall at the southwest corner of the plaza has been moved 8 feet north away from the sidewalk with a landscape transition zone, reducing its impact from the sidewalk. Angled steps create a wider, friendlier entry allowing more visibility into the plaza and a more natural transition from the sidewalk. Refer to the landscape plan, sheet L1.00 and the Republican Street perspective located on page 6 of the MUP supplemental package.

- c. The retaining wall and any handrails on the south edge of the plaza should be treated for human scale and visual interest. (C-3, D-2, D-3)

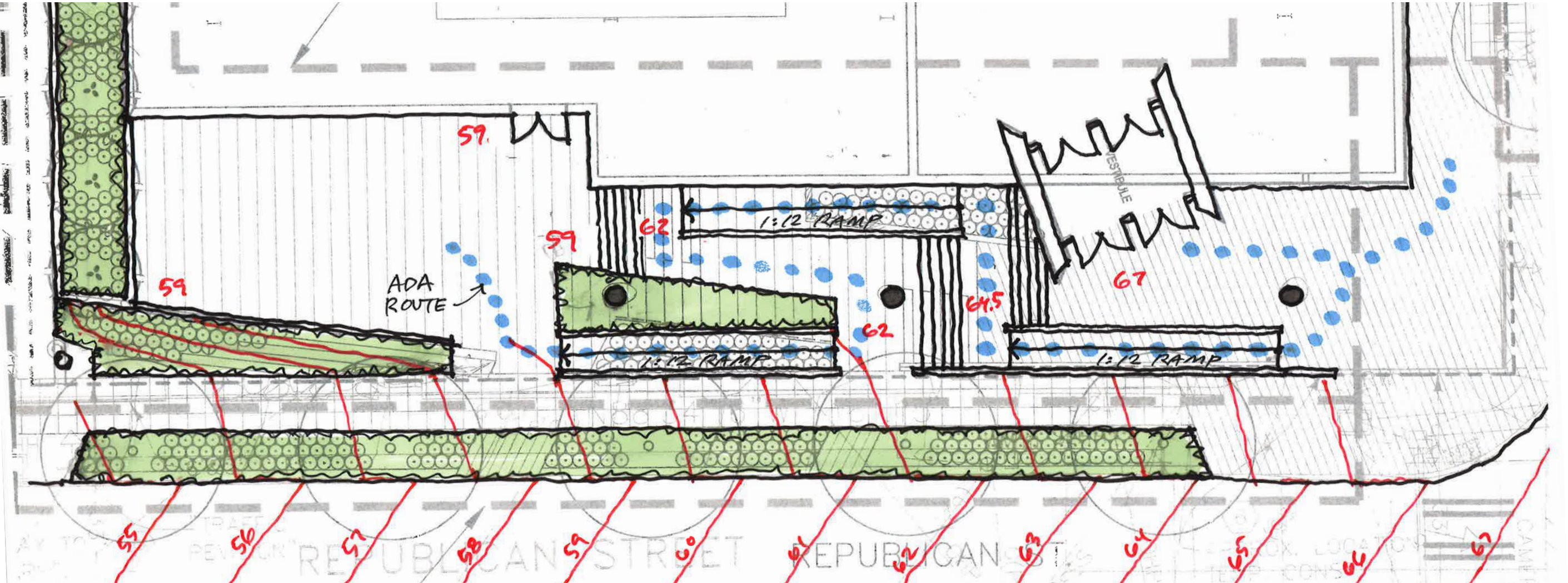
RESPONSE: The handrail will be an open metal railing, allowing visibility into the plaza. As noted in item 1a above, the westerly portion of the retaining wall has been moved to the north, setting back from the sidewalk, with landscaping and angled steps. The mid-block walls have been reduced in height by introducing an intermediate plaza. The retaining walls will be integrated with the landscape to reduce their visual impact.

- d. The southwest corner of the site should be designed to provide clear sight lines between pedestrians and vehicles at the alley intersection. (D-7, D-8)

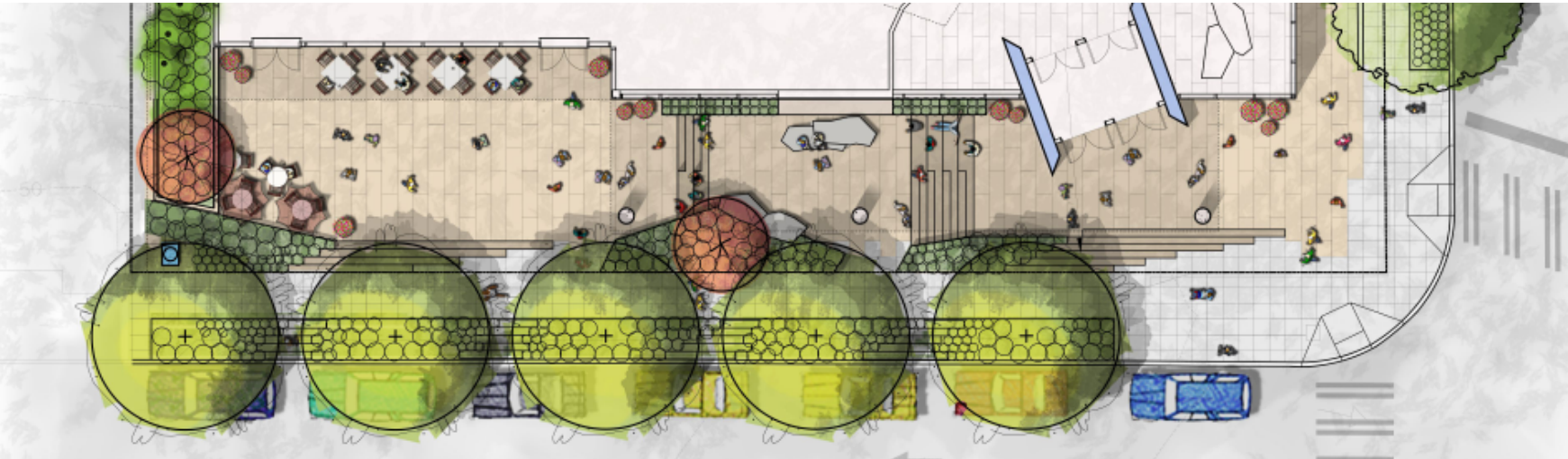
RESPONSE: As noted in item 1a, the southwest corner of the plaza has been moved north away from the sidewalk 8 feet, which opens up a clear sight line from sidewalk into the alley. Low planting will create a buffer, and allow visibility for both pedestrians and cars. This widened area will act as a node for pedestrians walking on Republican Street.



SW Corner Perspective



ADA Ramp Study



Republican St. Street Level Plan

DRB RESPONSE COMMENTS

1. (continued)

- b. If possible, an ADA ramp/stair system should be provided between the plaza and the sidewalk on Republican St. (A-1, A-2, A-4)

RESPONSE: An ADA ramp was investigated, but the length of the ramp alone would be between 96 feet and 160 feet not counting intermediate landings, significantly reducing the available plaza space, and creating a very complicated switchback pathway from upper to lower plaza. The introduction of a feasible ADA ramp and required landings would create a redundant pathway of virtually the same slope as the adjoining sidewalk (8.0 % vs. 8.1%) that would take up nearly half of the Republican frontage, usurping most of the usable space between the sidewalk and the building along its length. As an alternative, we are proposing an intermediate plaza level that connects to the sidewalk, creating an extra landing area and entry point, and reducing the distance traveled between plazas by half.

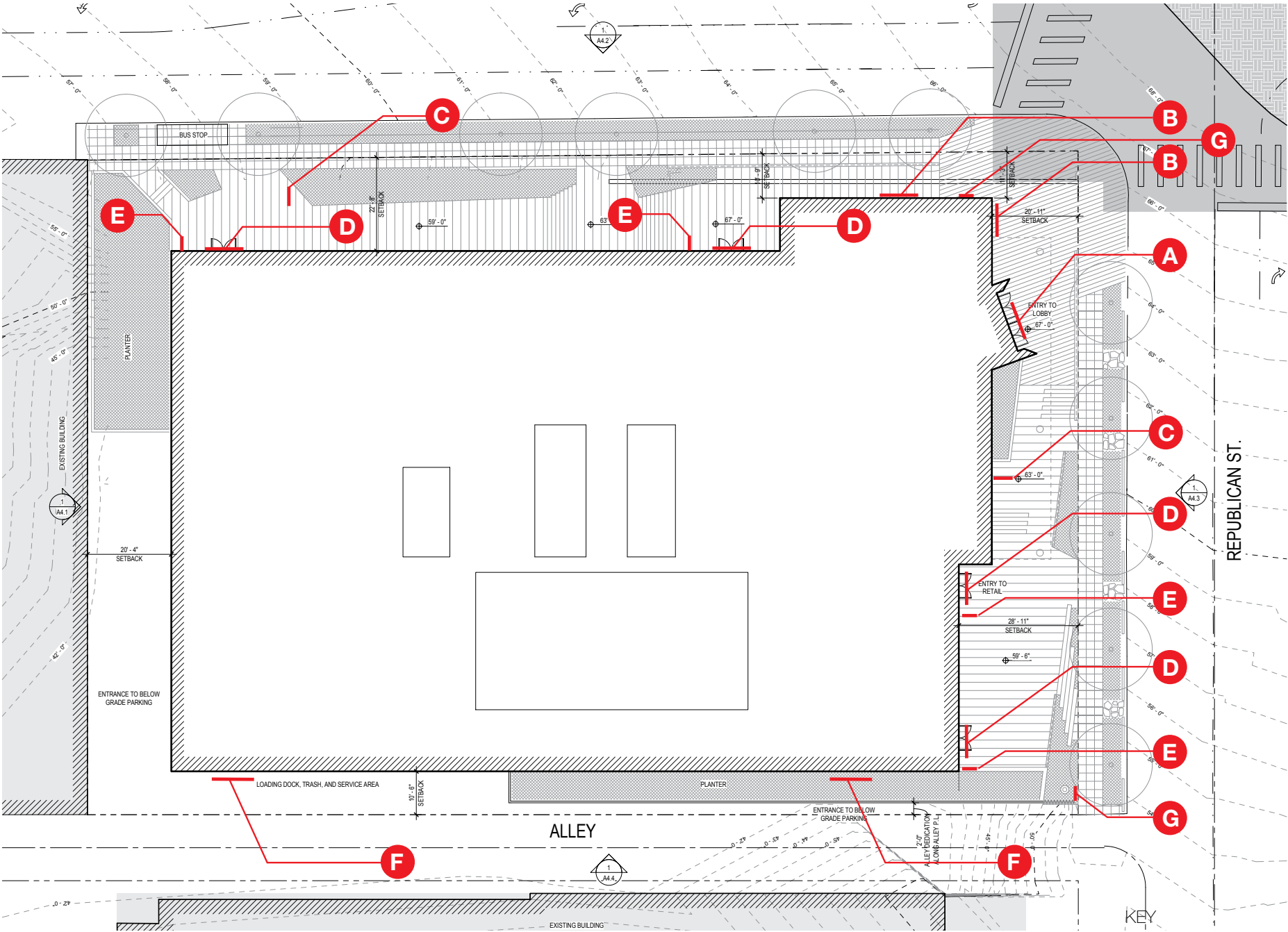
DRB RESPONSE COMMENTS

1. (continued)

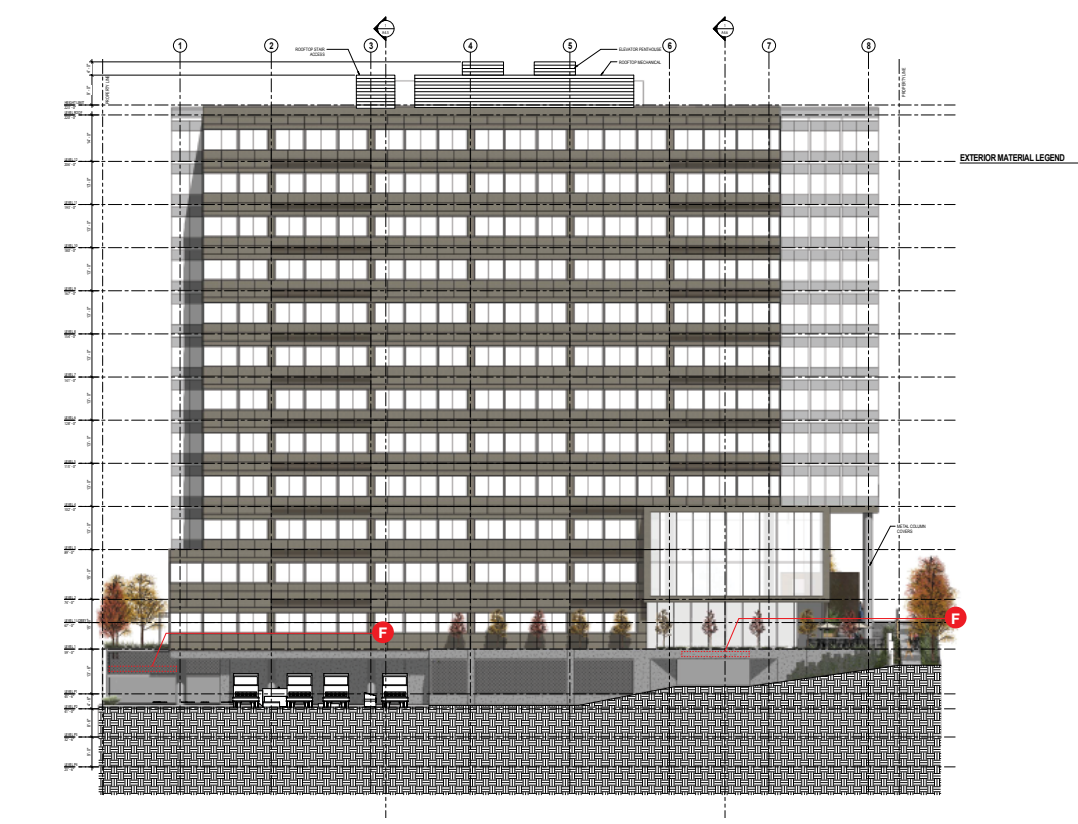
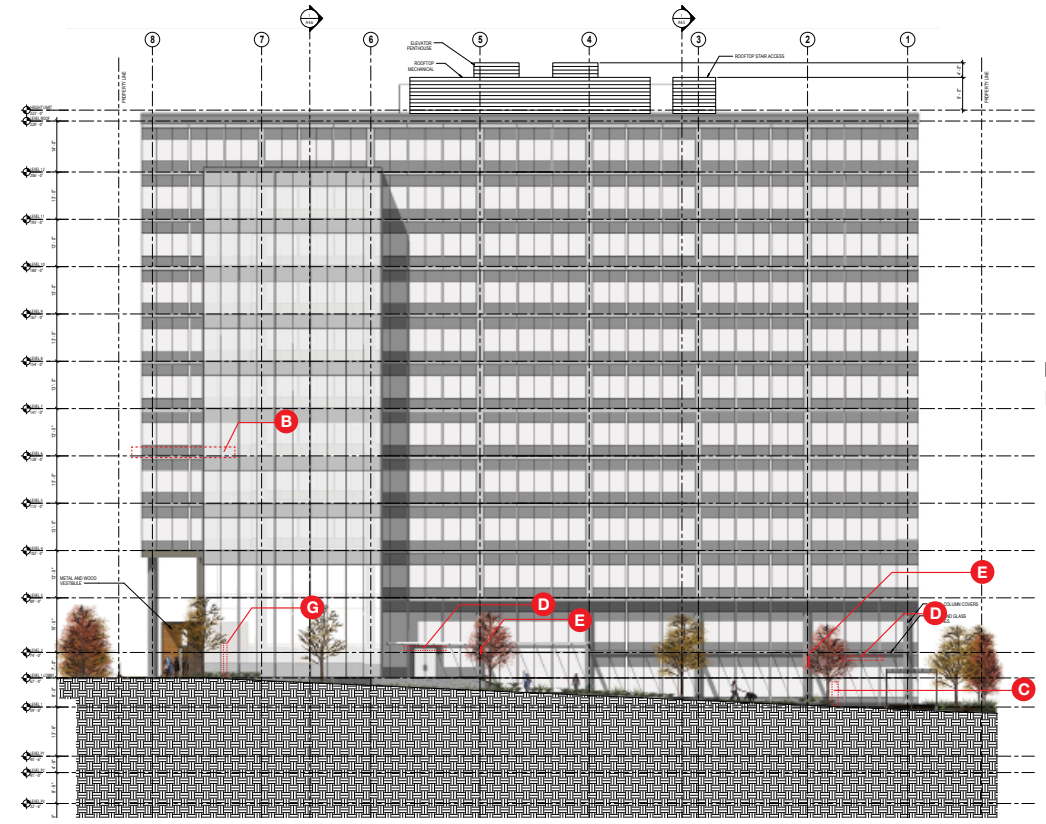
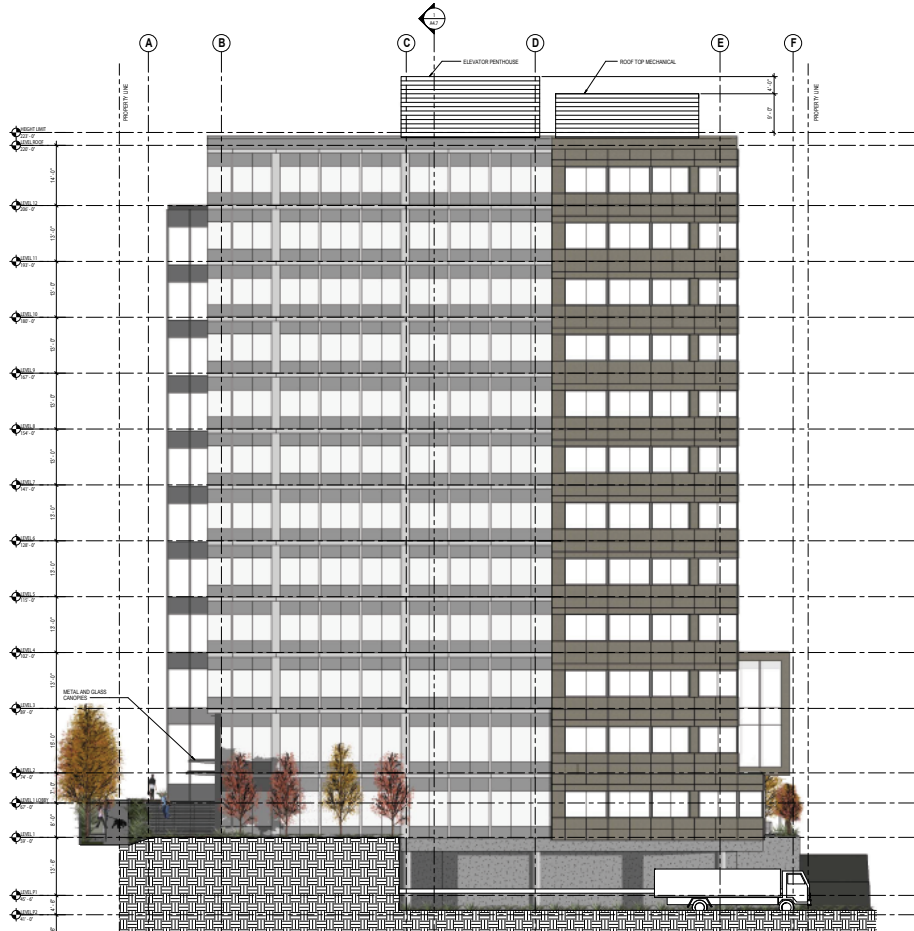
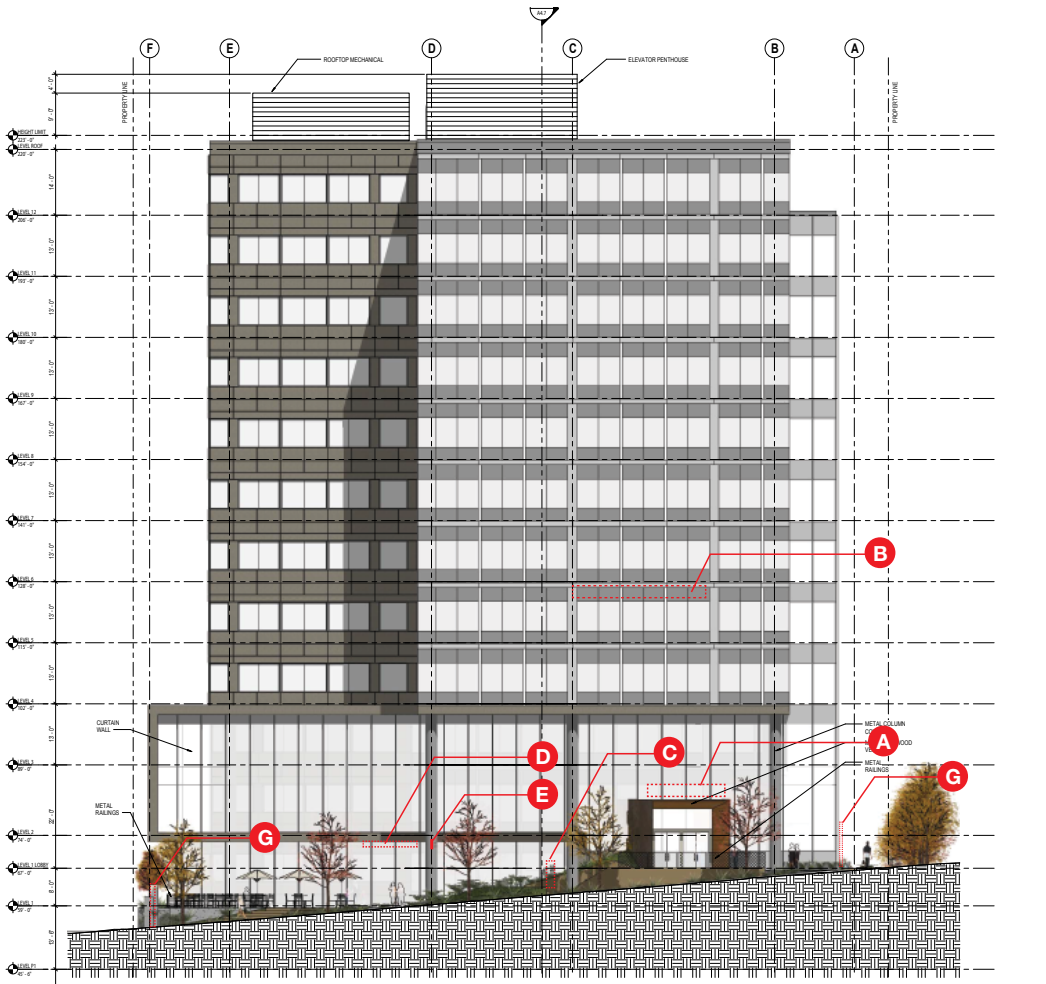
e. Signage opportunities should be provided on the façade, especially near the south-west corner (to avoid the need for sandwich boards in the sidewalk). (D-9)

RESPONSE: See the attached Architectural Graphics Package prepared by Olive Design, illustrating opportunities for building mounted and free standing signage for building identification, tenant signage, and parking garage entries. Signage will be oriented for both pedestrian and vehicular orientation as viewed from Republican Street and Fairview Avenue as well as the alley.

- A** Building ID @ Main Entry / Building Address
size: sign to be of scale for entry, visibility to address pedestrian and vehicular orientation
- B** Anchor Tenant (Enterprise occupying 180,000+ sq ft)
2 per building based on vehicular visibility, located max height 65' above sidewalk
- C** Freestanding Building ID with Tenants
Size varies based on mounting location, 2 per building based on vehicular visibility
- D** Primary Retail Tenant with Entry ID sign parallel to street - vehicular orientation
- E** Primary Retail Tenant blade perpendicular to street - pedestrian orientation
- F** Garage Entry ID
- G** Building ID at Freestanding Pylon



SIGNAGE ELEVATIONS



EXTERIOR MATERIAL LEGEND



Looking South up Fairview Ave N.

DRB RESPONSE COMMENTS

2. Fairview Ave Street Level. The Fairview Ave N. walkways and street level facades should be designed for pedestrian and transit users, with efforts to enhance safety and comfort. (A-1, A-2, D-1, D-7)

- a. The west side of Fairview on this block will experience a high level of pedestrian traffic for people accessing the intersection to Lake Union on the north end of the block. The Fairview Ave N. façade should be treated for pedestrian comfort and should be designed to provide eyes on the street near the adjacent bus stop. (A- 2, A-4, D-1, D-

RESPONSE: The Fairview façade will be very transparent, and canopies will be provided at the building face. Planters with multiple openings will soften the ground plane and allow pedestrians to circulate freely between the sidewalk and the building face. The design encourages users to flow off the sidewalk and hug the building face under the canopy cover.

- b. Overhead weather protection should be of sufficient width to provide adequate protection for pedestrians on Fairview Ave N. (A-2, D-1)

RESPONSE: As noted in the item above, metal and glass canopies between six and eight feet in width will be provided along the majority of the Fairview Avenue façade.

4. Preferred Massing Option. The Board supported the preferred alternative, with the request for more information and the modifications described in this report. (A-1, B-1)

- a. At the Recommendation meeting, the Board would like to see graphics demonstrating the relationship between the proposed development and the developments to be built across Fairview Ave N. The Board noted that the graphics should describe the 'out-door room' that will be created at three corners of the intersection of Fairview Ave N. and Republican St. (A-1, A-2, C-2) The Board supported the proposed angled entry design at the southeast corner of the site. (A-1, A-2, A-3, A-10)

RESPONSE: Graphics showing the relationship of the proposed development and those to be built across Fairview will be provided at the recommendation meeting.

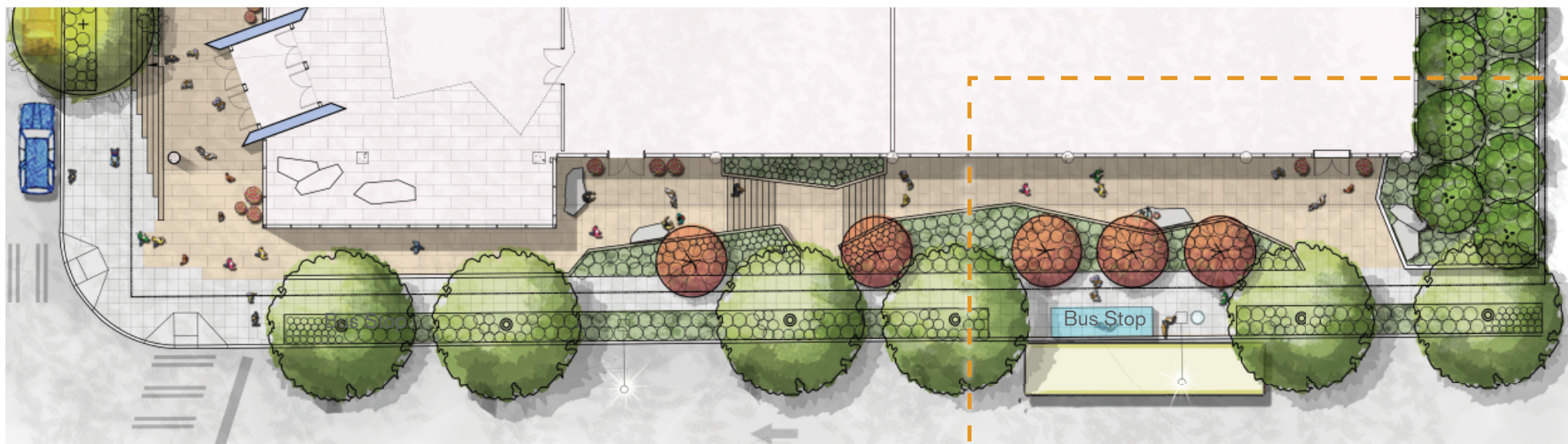
NORTHEAST CORNER CONDITION



Looking North along Fairview Ave N.



Looking Northwest from Fairview Ave N.



Fairview Ave N. Street Level Plan

DRB RESPONSE COMMENTS

2. (continued)

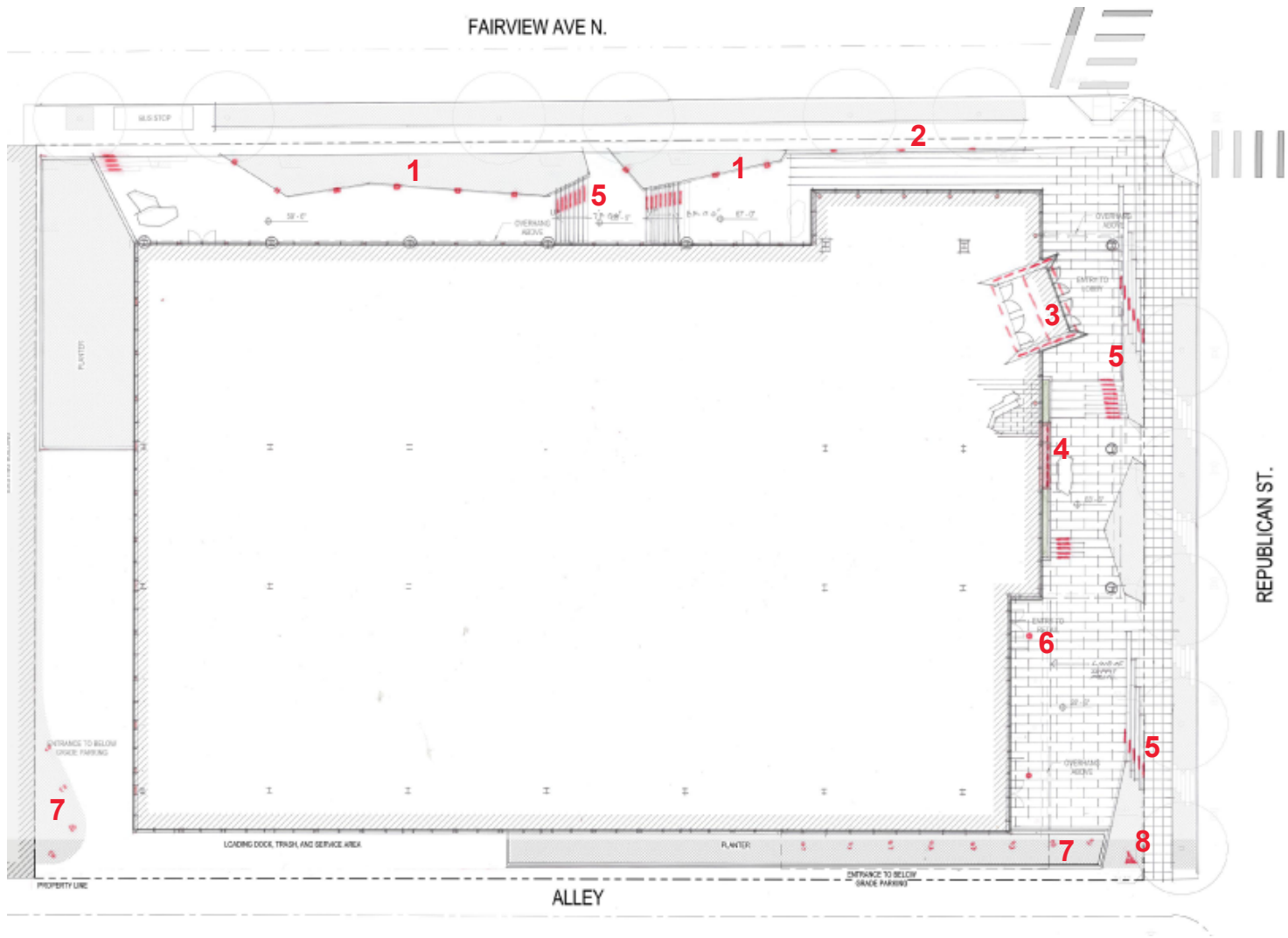
- c. The applicant should work with King County Metro to integrate the bus stop into the proposed development, possibly using leaning rails or seating and overhead weather protection on the building façade. (A-1, A-2, C-2, D-1)

RESPONSE: The bus shelter is proposed to be relocated in the right of way slightly north of its existing location to allow for a continuation of the street tree pattern on Fairview. Open walkways on either side will provide for ease of transition between the sidewalk and plaza area, and will reduce the chance of blocking access to the building face.

DRB RESPONSE COMMENTS

2. (continued)

- d. The north plaza should be treated for safety, including sufficient lighting. (D-7, D-10)
- RESPONSE:** To achieve the required green factor for the site, the north plaza will be comprised primarily of elevated planting beds and will not be occupied. However, the project will incorporate a variety of lighting strategies including in-wall landscape lighting, and building mounted exterior lighting for the outdoor plaza spaces. This area may also be protected with a transparent open fence and gate system.



- 1 Foot level LED bollards provide path light at landscaped edge
- 2 Wall recessed LED fixtures provide path light at low walls
- 3 Glowing walls and ceiling of linear LED fixtures inside architectural slumped glass frame the gateway entrance
- 4 Linear LED fixtures inside architectural wall cavity backlight slumped glass inserts within decorative stone wall
- 5 Stairway recessed LED step lights articulate rhythm of angled site geometry
- 6 Ceiling recessed or surface mount LED downlights provide ambient light at entry doors
- 7 Low level accent fixtures provide soft glow in lower landscaping
- 8 LED accent fixtures to uplight or backlight glowing columnar sculptural art/sign element

MINIMUM DEVELOPMENT SETBACK

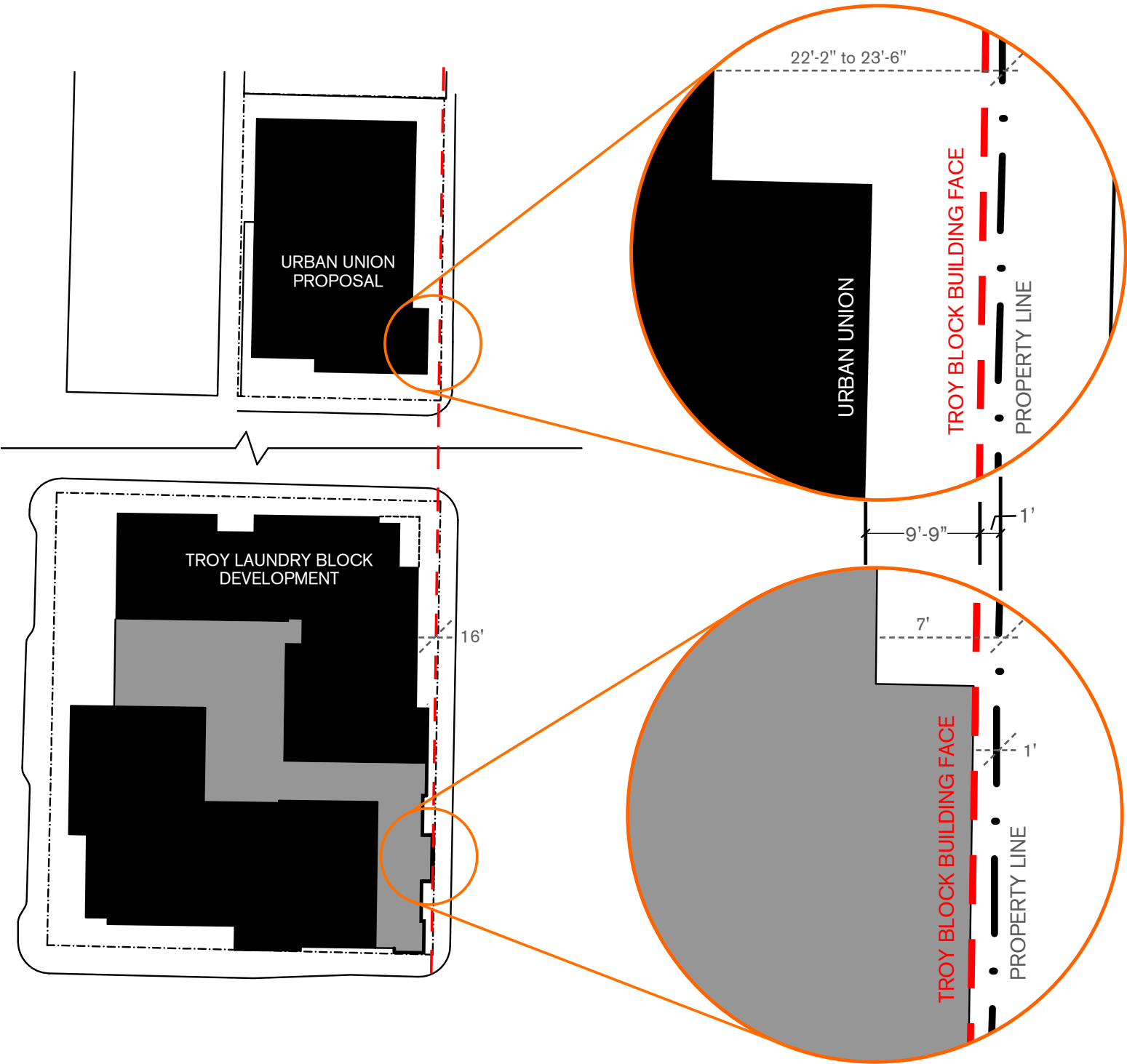
DRB RESPONSE COMMENTS

3. (continued)

b. The Board would like to see how the proposed east setback and façade relate to nearby proposed development on the west side of Fairview Ave N, including the Troy Block development (MUP 3012675). In response to nearby context and potential pedestrian views, the east façade of the proposed development should at least maintain the same setback as the upper levels of the Troy Block development. (A-1, A-2, C-1)

RESPONSE: See the attached illustrations on pages 4 and 5 of the MUP Supplemental Package showing the relationship of the proposed design to the proposed Troy Laundry block design. Because the podium levels of the two projects have the most effect on the pedestrian views, the illustrations compare the podium levels of the two buildings. The podium level of the Troy Laundry block is comprised of the retained historical façade, (55% of the length), and new construction (45%). At the historical façade one 38 foot portion extends to within one foot from the property line, with the remainder set back approximately seven feet from the property line. To the north, the predominant portion of the new façade of the podium is set back approximately 16 feet from the property line with the north most bay set back an additional 16 feet.

As noted in item 3-a-ii above, the primary podium façade of our proposal is set back between 22'-2" and 23'-6" from the property line, and the furthest projection is 10'-9" from the property line, 9'-9" farther than the furthest projection of the podium level of the Troy Laundry project.



MINIMUM SETBACK ANALYSIS

The following exhibits demonstrate the relationship between the setbacks for the Urban Union proposal and the Troy Laundry Block development. As this diagram illustrates, the Urban Union proposal is set back 9'-9" further than the Troy Laundry Block development.

DRB RESPONSE COMMENTS

3. Fairview Ave Upper Building Mass. The Board directed the applicant to design the east façade with modulation, but also to preserve pedestrian level view potential along Fairview Ave N. (A-1, B-1, C-2)

- a. The Board noted that the extension of the building façade toward the east property line may intrude further into the pedestrian view potential along Fairview Ave. The Board recommended that the massing should be located to minimize impacts to potential pedestrian views along Fairview Ave N. (A-1, A-2, C-1)
- i. The Board suggested that the applicant could push the building further to the alley, in favor of setting back from Fairview Ave.

RESPONSE: The entire building has been moved several feet towards the alley, to the maximum amount possible to prevent the garage level from extending past the two foot alley set back. See item 3-a-ii, below.

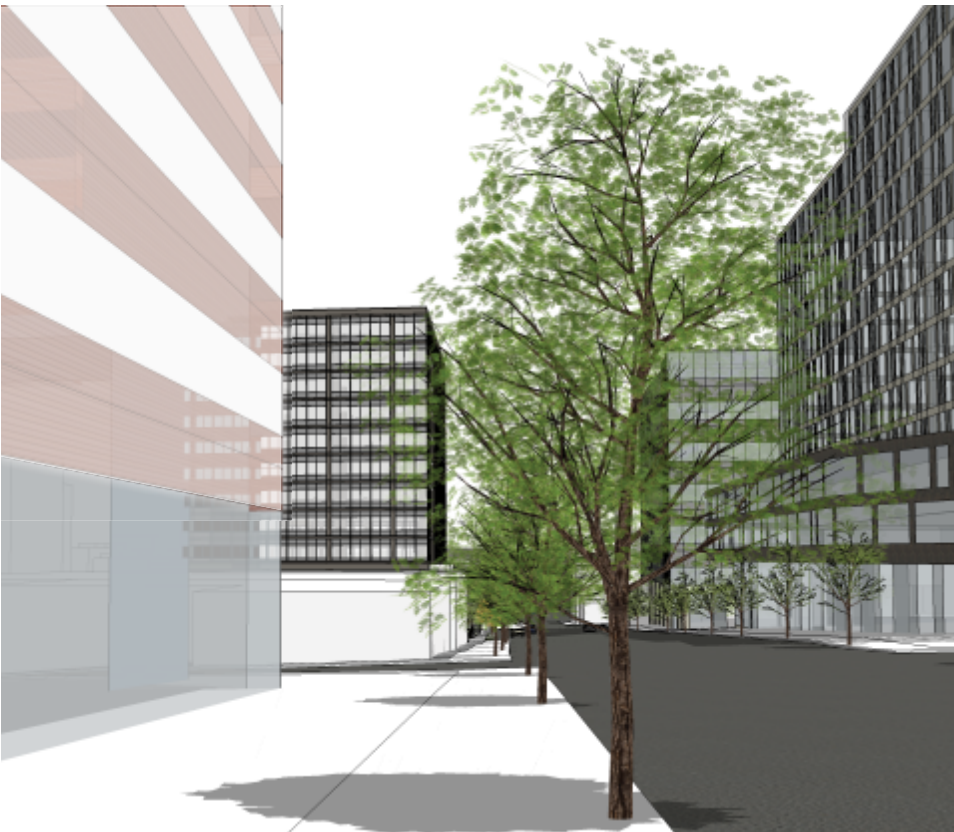
- ii. The Board also offered that the building bay could begin approximately 30' above grade, which might help to preserve pedestrian views along Fairview Ave N.

RESPONSE: The design team has investigated this option, and concluded that the integration of the lobby/great room expression with the vertical bay element is an important design expression that makes both elements stronger and more unified, and is central to the goal of achieving architectural concept and consistency (Design Guideline C-2). Therefore, we propose to keep the lower portion of the bay element, so it extends from the lobby level through level 11.

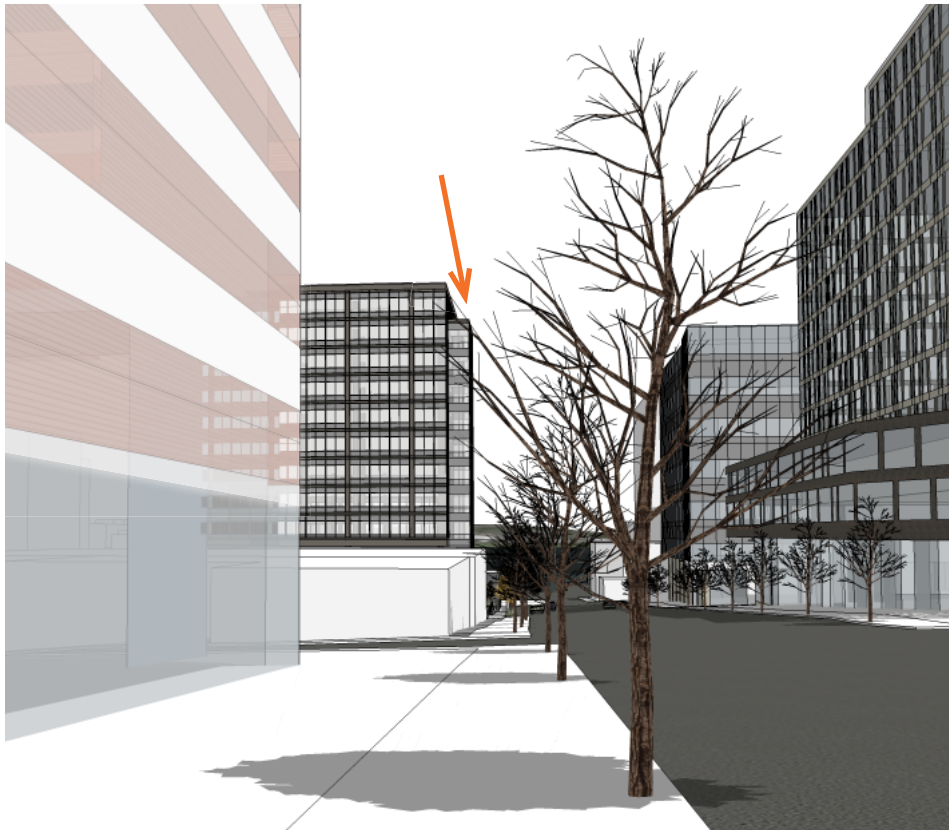
The predominant portion (75%) of the base of our proposal is set back between 22'-2" and 23'-6" from the property line, while the building bay is set back between 10'-9" and 11'-3" (due to the property line angle). The required set back per zoning at the podium level (up to 65 feet) is zero feet, with a 2.5 foot pedestrian easement at the sidewalk level. At the bay, the proposed design exceeds the most restrictive zoning set back by 8'-3", while maintaining a high degree of transparency.



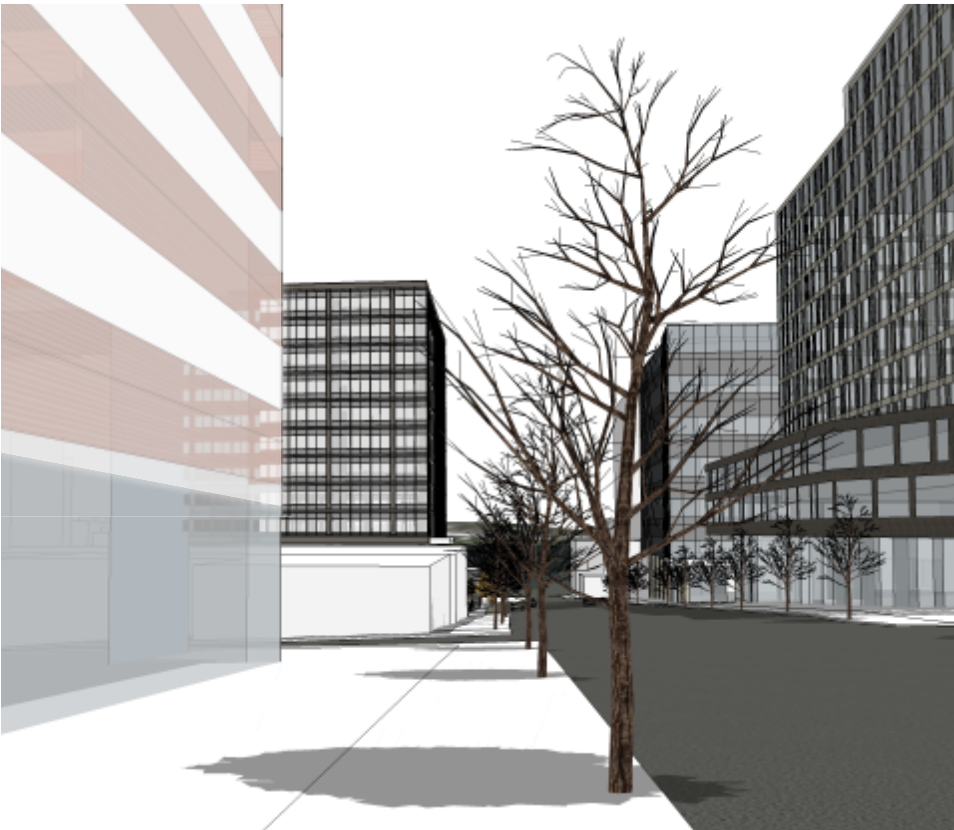
Summer view with vertical element



Summer view without vertical element



Winter view with vertical element



Winter view without vertical element

SOUTHEAST PERSPECTIVE



DRB RESPONSE COMMENTS

3. (continued)

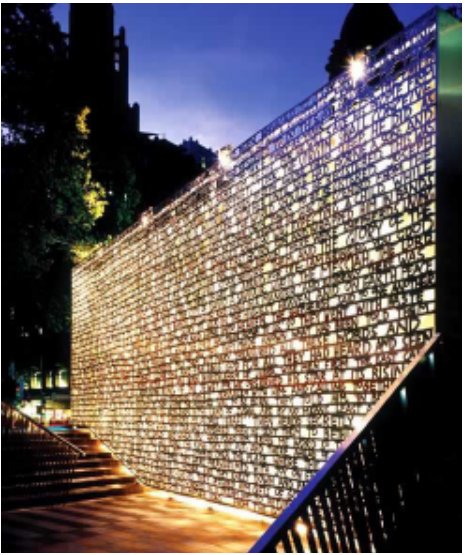
c. The Board supported the modulation/articulation of the east façade, and recommended that the east façade be modulated in a way that preserves potential pedestrian views along Fairview Ave N. (A-1, A-2, B-1, C-2)

RESPONSE: The location of the modulation (bay) to the south end of the building serves to lessen its impact on views to Lake Union as compared to a similar bay placed further to the north. As stated in item 3-a-ii, the podium levels of the bay exceed the required setback by over eight feet, and is constructed primarily of glass. It also exceeds the furthest projection of the podium at the Troy Laundry by 9'-9".

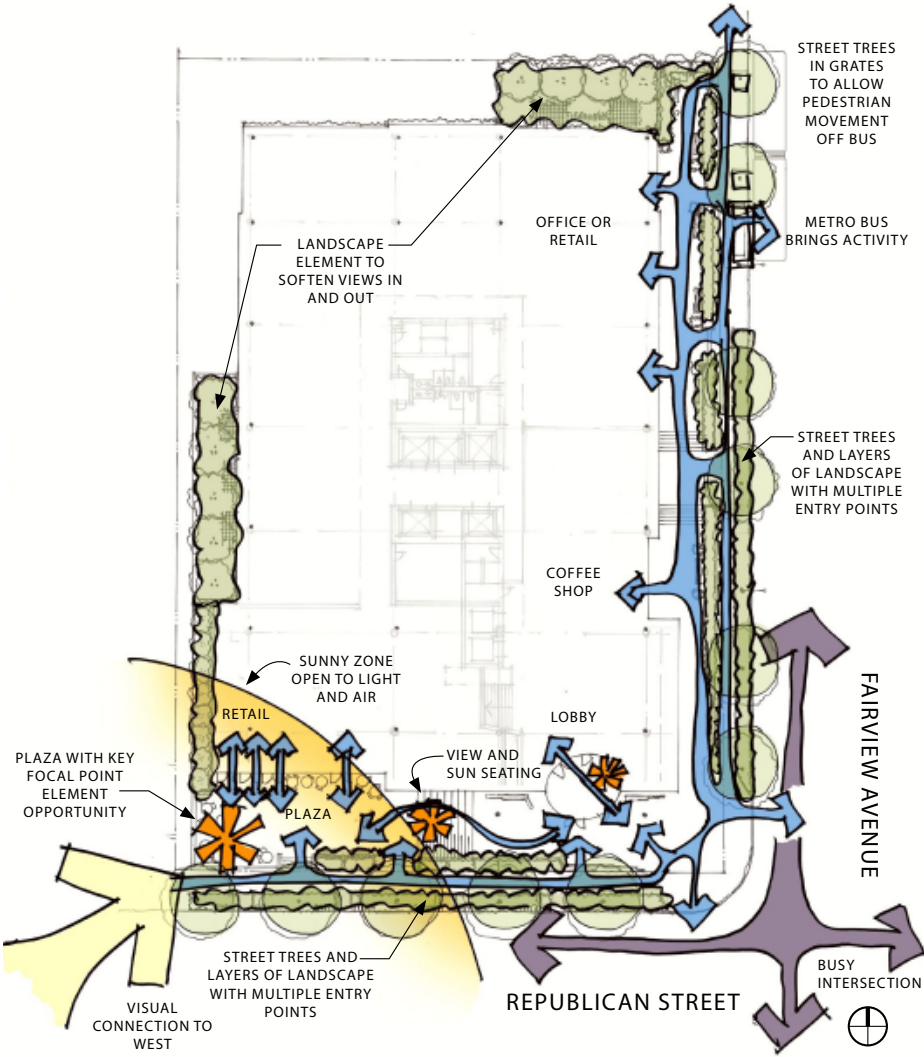
The building to the north of the project site is constructed to the Fairview Avenue property line to a height of approximately 20 feet above the sidewalk at the southern edge. Future

development on this block could be built to the property line to the allowed 85 foot podium height limit of that portion of the block.

In addition, city required street trees along the Fairview Avenue right of way will significantly obstruct pedestrian views to a greater extent than the proposed design, which exceeds the required setback at the podium level.

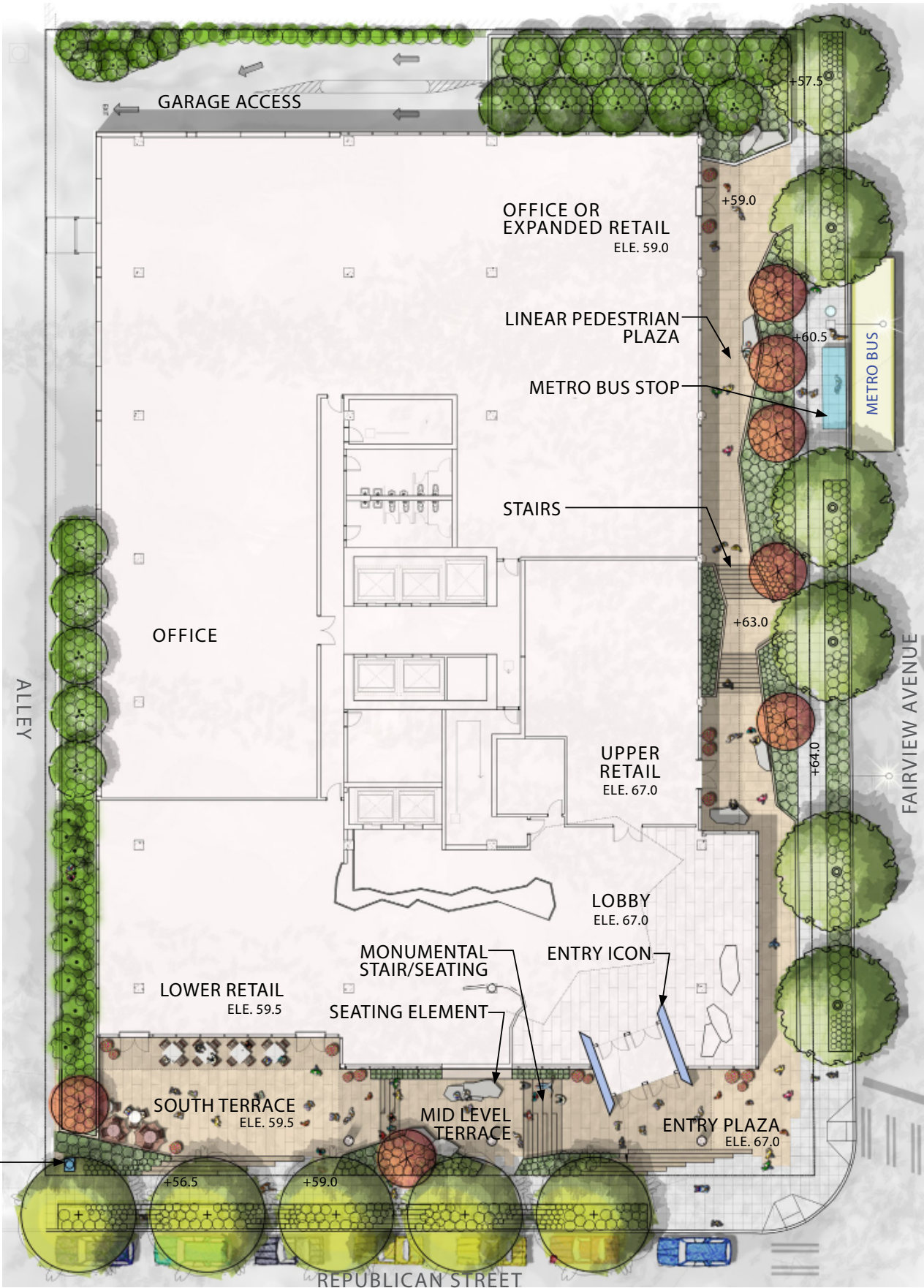


ALTERNATE ENTRY CUBE TREATMENT



LANDSCAPE DIAGRAM

ICONIC ART ELEMENT



LANDSCAPE DESIGN STRATEGY

The landscape concept for Urban Union takes advantage of its site in a way that offers welcoming and highly useable pedestrian spaces. To the east, the energy of busy Fairview Avenue is dissipated by setting the building back from the sidewalk, creating more open space with landscape options. Depending on how it is leased, the open space allows for either at-grade access to retail or an office outdoor courtyard. This open space is separated by a landscape strip at street level that embraces the bus stop, mediates connections to the plaza levels, and provides visual interest. Around the north side, a landscaped courtyard and plantings along the edge provide softening of views and screening.

At the intersection of Republican Street and Fairview Avenue, the main entry plaza reaches out to the street, connecting the lobby and interior concept to this important crossing. Sitting close to existing grade and covered high above by the upper floors of the building, this space offers protection from the elements while still allowing the southern light and action of the street to pass through.

The third major outdoor space is proposed at the southwest corner of the site. This plaza, open to the sky above and perched slightly above the alley, offers direct physical and visual connection to the bustling neighborhood west of it. This large, sunny space affords a place for more populous gatherings, al fresco dining, art installations and people watching. Tucked to the north is a more intimate garden space that buffers the alley and adjacent building, providing break out areas for meetings or quieter reflection.

Bridging the plazas at the SE and SW sides of Republican Street is a grand staircase that functions both as circulation and seating. Intended as a graceful transition between the upper and lower levels, the openness of the stair helps visually create the feeling of one larger space. Planters, built of high quality materials, will offer a soft green edge and mediate grade changes between the sloping sidewalk and the plazas.

LANDSCAPE PLANTINGS

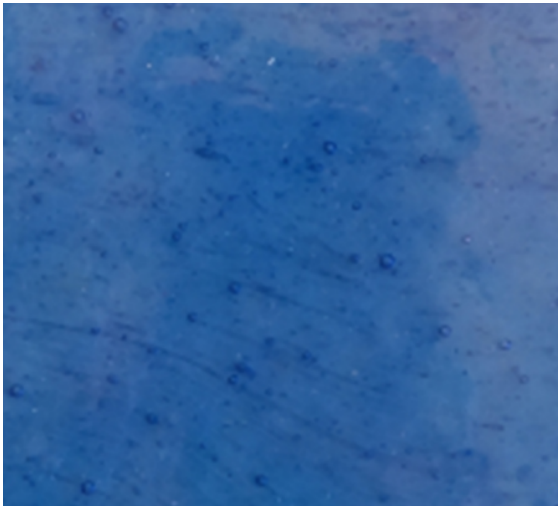


Plant Palette

- 1 Fraxinus pennsylvanica 'Summit'
Summit Ash
- 2 Gaultheria shallon
Salal
- 3 Pennisetum alopecuroides 'Hameln'
Fountain Grass
- 4 Epimedium x warleyense
Bishop's Hat
- 5 Liriope muscari
Lily Turf
- 6 Cornus stolonifera 'Kelsey'
Kelsey Redtwig Dogwood
- 7 Ginkgo biloba
Ginkgo
- 8 Parthenocissus tricuspidata
Boston Ivy
- 9 Hammamelis x intermedia 'Diane'
Hammamelis
- 10 Acer circinatum
Vine Maple
- 11 Ribes sanguineum
Red Flowering Currant
- 12 Rhamnus frangula 'Columnaris'
Tallhedge Buckthorn
- 13 Taxus baccata 'Repens'
Spreading Yew
- 14 Leucothoe catesbaei 'Nana'
Drooping Leucothoe
- 15 Hakonenchloa macrochaeta
Japanese Forest Grass
- 16 Mahonia aquifolium 'Compacta'
Compact Oregon Grape
- 17 Prunus laurocerasus 'Mount Vernon'
Mount Vernon Laurel



MATERIAL BOARD



Slumped glass at entry cube



Metal panel



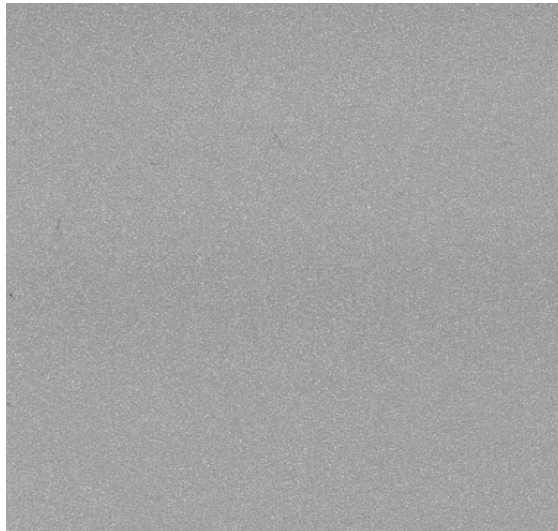
Precast



Absolute black granite accent walls



Concrete



Window mullion



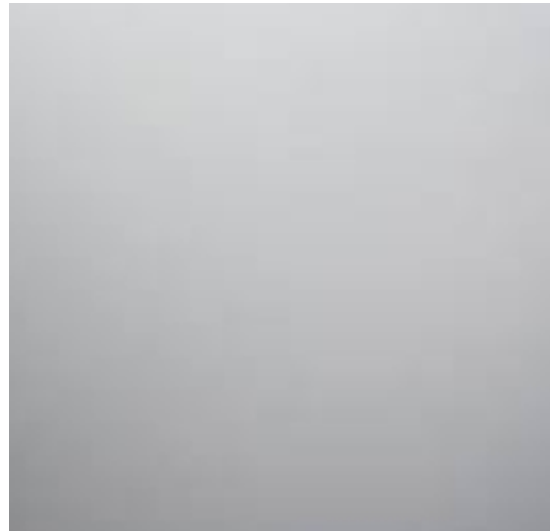
Vision glass



Frosted glass (translucent)

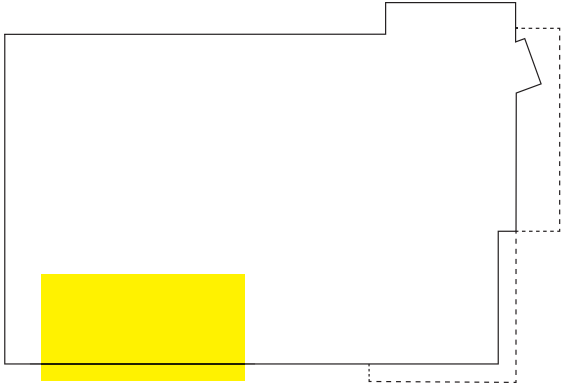
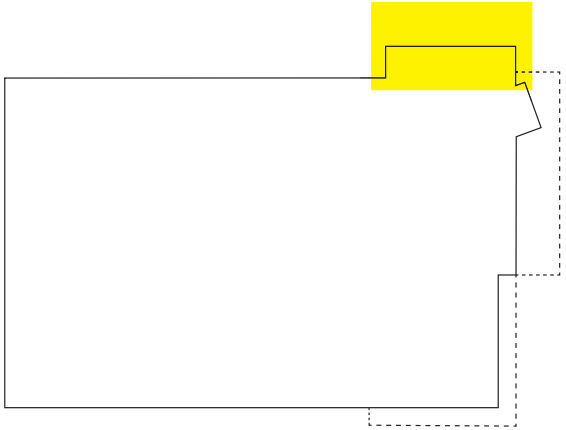


Spandrel glass 1



Spandrel glass 2 (frosted)

PROPOSED DESIGN DEPARTURES

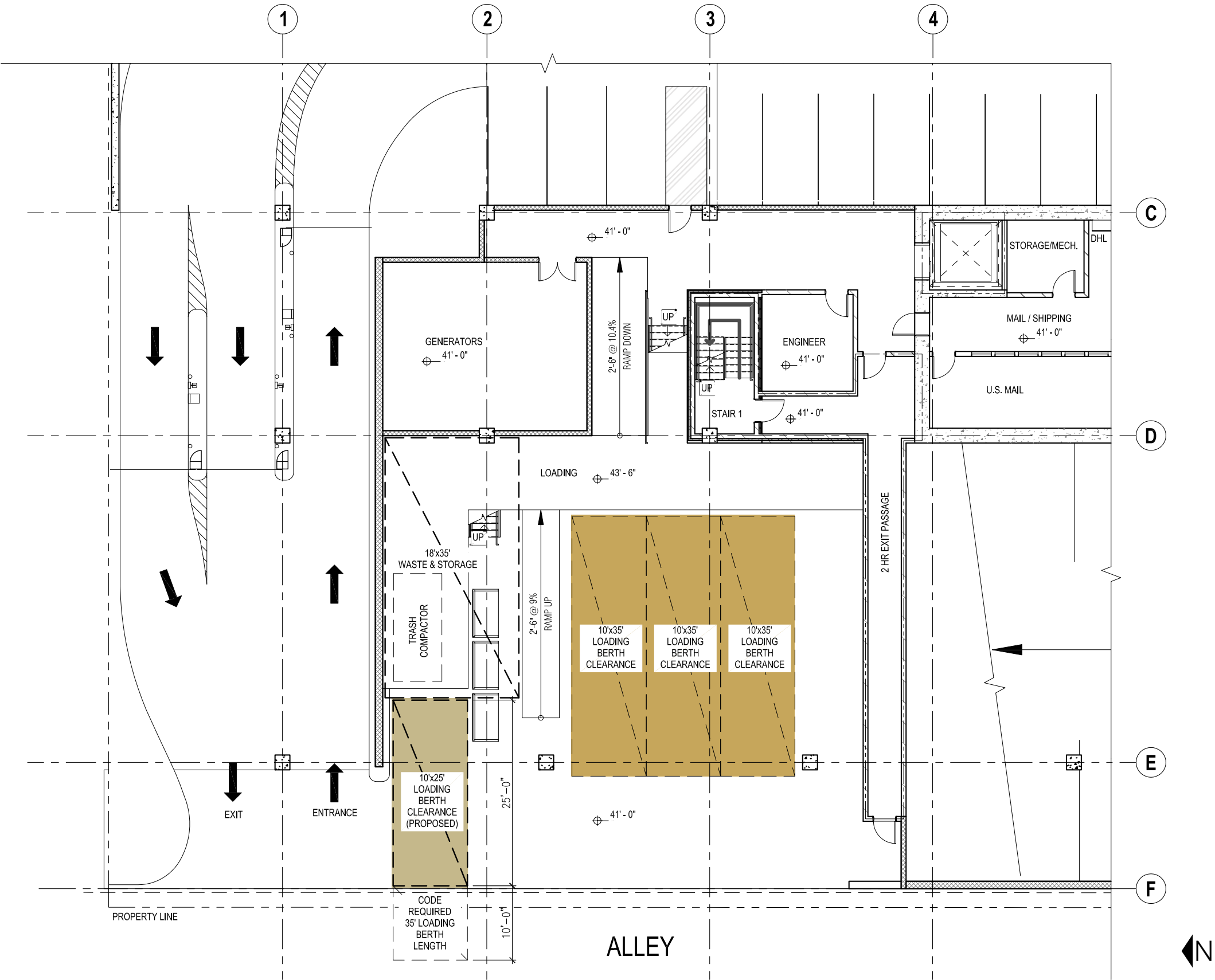
DEVELOPMENT STANDARD	DEPARTURE	DEPARTURE LOCATIONS
<div>23.54.035.c Standards for Loading Berths</div> <div><p>b. Low - and Medium - demand Uses. Each loading berth for low - and medium - demand uses, except those uses identified in subsection C2d, shall be a minimum of thirty-five (35) feet in length unless reduced by determination of the Director as provided at subsection C2c.</p><p>c. Exceptions to Loading Berth Length. Where the Director finds, after consulting with the property user, that site design and use of the property will not result in vehicles extending beyond the property line, loading berth lengths may be reduced to not less than the following:</p><p>(i) High-demand Uses. Thirty-five (35) feet when access is from a collector arterial or local access street; and forty-five (45) feet when access is from a principal or minor arterial street;</p><p>(ii) Low- and Medium-demand Uses. Twenty-five (25) feet.</p></div>	<div>Reduction of one loading bay from 35 feet to 25 feet in length (4 loading berths required - 3 loading bays will be 10 feet by 35 feet, and one compactor bay will be 10 feet by 25 feet).</div>	
<div>Setback : SMC 23.48.14.A.3.b</div> <div><p>Street-facing facade of a structure may be set back up to 12 feet from the street lot line, given it is landscaped according to 23.48.024 or provides required open space in accordance to 23.48.013.G.</p></div>	<div>The corner of Fairview Ave and Republican street has an eastward facing facade that is setback 10'-9" from the lot line, and does not provide landscaping or required open space.</div> <div>RATIONALE: <i>This space was not included in required open space calculations, as it reduces the average horizontal dimension of open space down to 19'-5", which is below the 20' horizontal width average requirement. The building has been set back from the lot line to provide open space, pedestrian access, and pedestrian site lines of Lake Union and Fairview Avenue, per Land Use Review.</i></div>	

23.54.035.c Standards for Loading Berths

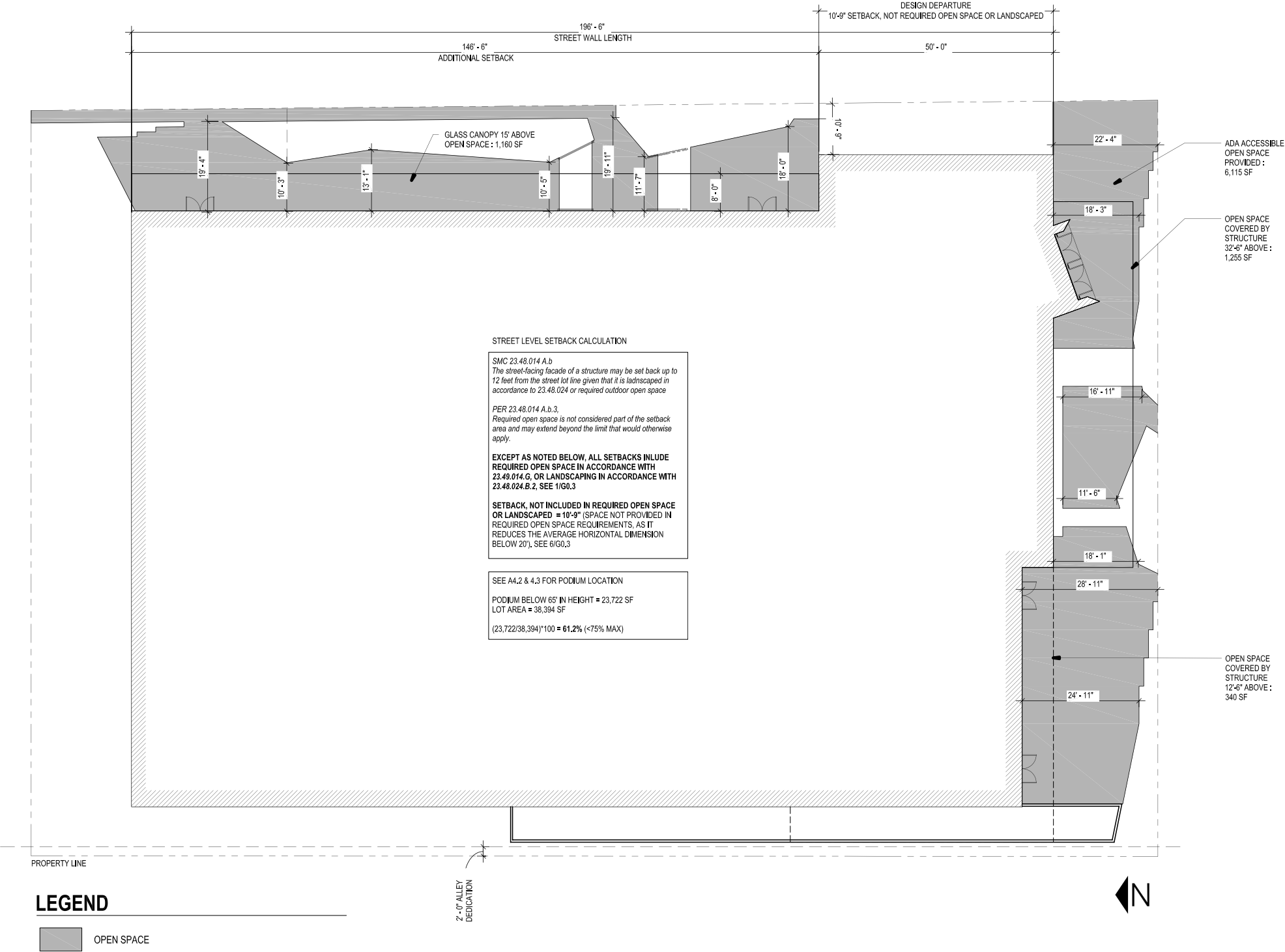
- b. Low - and Medium - demand Uses. Each loading berth for low - and medium - demand uses, except those uses identified in subsection C2d, shall be a minimum of thirty-five (35) feet in length unless reduced by determination of the Director as provided at subsection C2c.
- c. Exceptions to Loading Berth Length. Where the Director finds, after consulting with the property user, that site design and use of the property will not result in vehicles extending beyond the property line, loading berth lengths may be reduced to not less than the following:
 - (i) High-demand Uses. Thirty-five (35) feet when access is from a collector arterial or local access street; and forty-five (45) feet when access is from a principal or minor arterial street;
 - (ii) Low- and Medium-demand Uses. Twenty-five (25) feet.

Departure

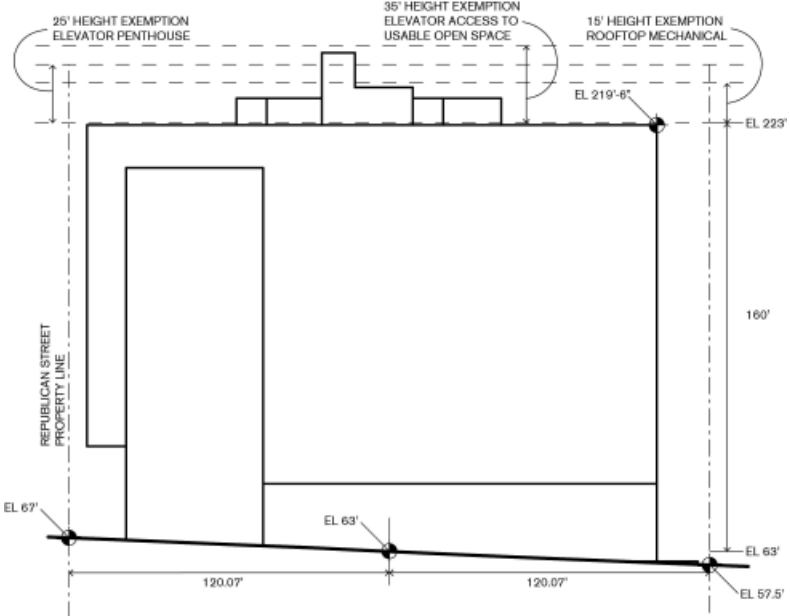
Reduction of one loading bay from 35 feet to 25 feet in length (4 loading berths required - 3 loading bays will be 10 feet by 35 feet, and one compactor bay will be 10 feet by 25 feet).



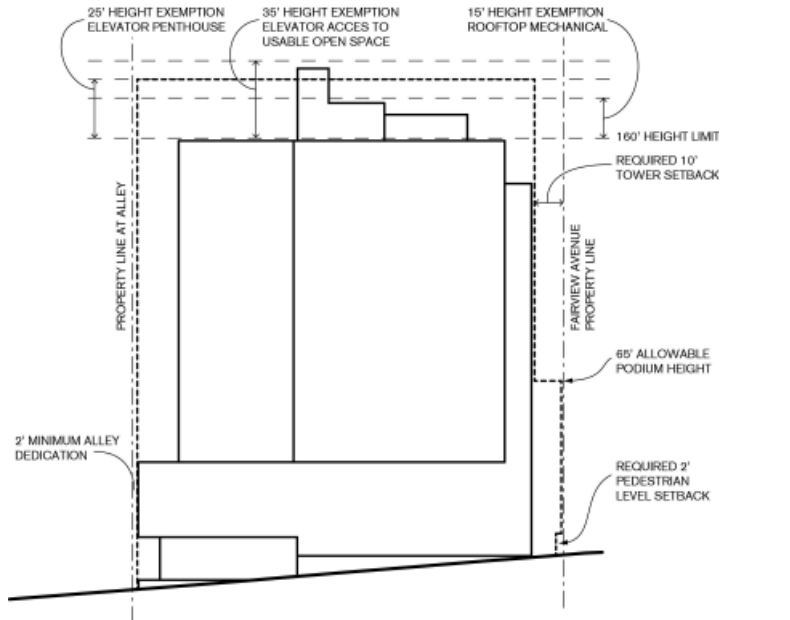
DESIGN DEPARTURE: OPEN SPACE



SETBACK AND HEIGHT LIMIT DIAGRAMS



East Elevation



South Elevation



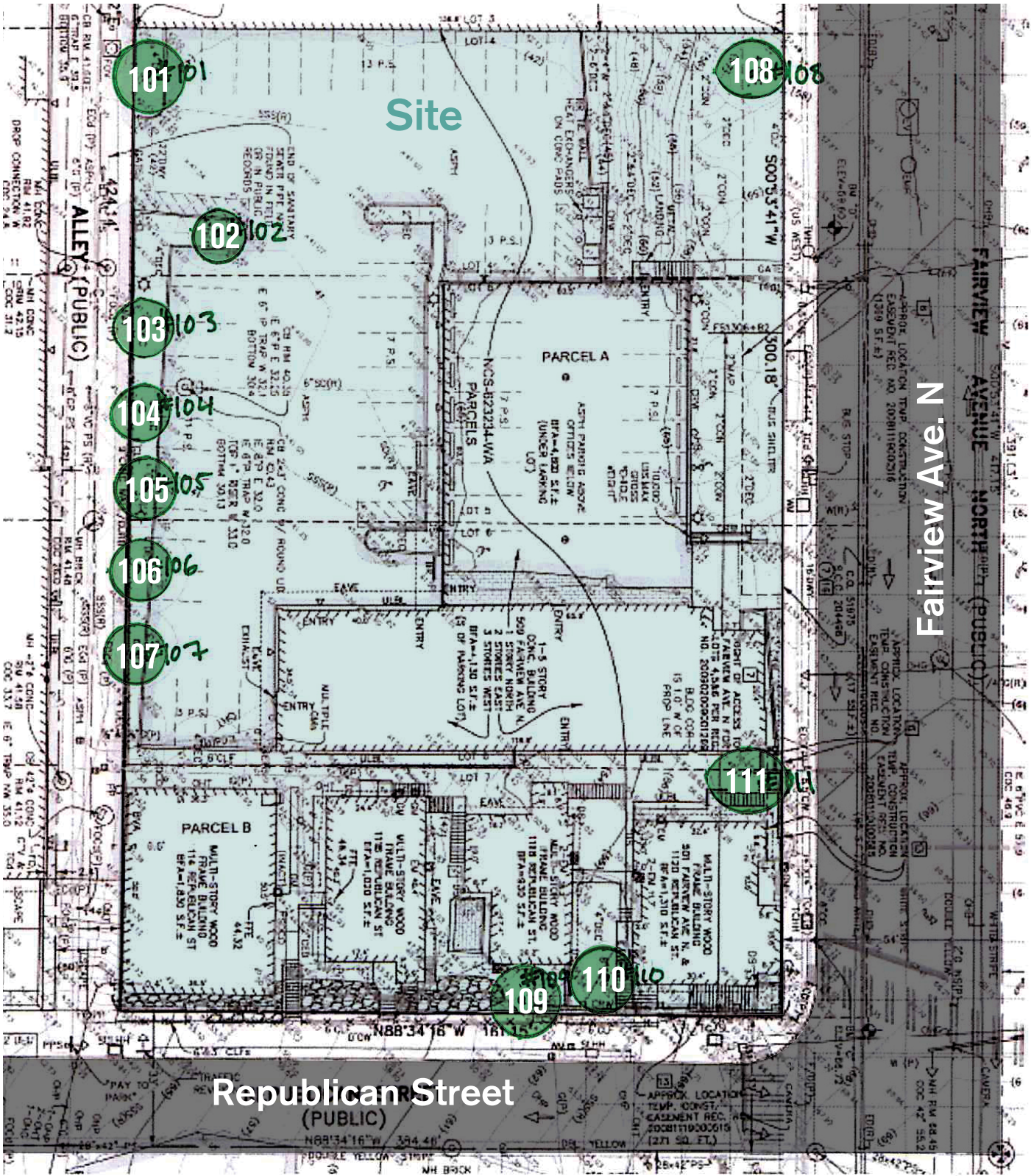
ADDITIONAL MATERIAL

TREE SURVEY AND SUMMARY TABLE

Tree Summary Table

Tree/Tag #	Species	Scientific Name	Native/ Planted/ Volunteer	DBH	Height	Drip-Line/Limits of Disturbance (feet)				Condition	Viability	Comments
						N	S	E	W			
101	Red Maple	Acer rubrum	Planted	6	30	10	12	10	10	good	viable	no concerns
102	Norway Maple	Acer platanoides	Planted	6	33	5	6	6	6	good	viable	no concerns
103	Red Maple	Acer rubrum	Planted	6	30	10	8	8	8	good	viable	no concerns
104	Red Maple	Acer rubrum	Planted	7	32	7	7	8	8	good	viable	scar on bole
105	Red Maple	Acer rubrum	Planted	6	30	8	8	10	10	good	viable	no concerns
106	Red Maple	Acer rubrum	Planted	6	30	7	8	8	8	good	viable	scar on lower bole
107	Red Maple	Acer rubrum	Planted	7	30	6	9	10	10	good	viable	forked stem, poor attachment
108	Red Maple	Acer rubrum	Planted	7	28	10	5	10	11	good	viable	no concerns
109	Grand fir	Abies grandis	Planted	10	12	8	10	9	9	fair	viable	overly pruned, poor form, small root zone
110	Highclere Holly	Ilex x altaclarensis	Planted	10	60	8	8	8	8	good	viable	no concerns
111	Ponderosa pine	Pinus ponderosa	Planted	8	50	9	11	4	12	fair - poor	viable	Slight lean north, trunk has a rope around it approximately 8' up inhibiting growth
Neighboring Trees												
None												

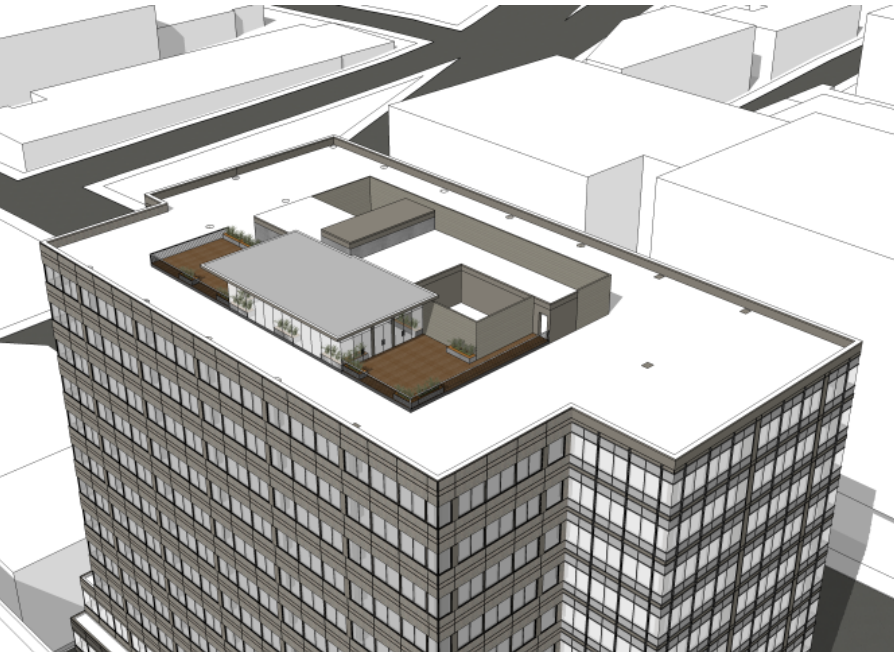
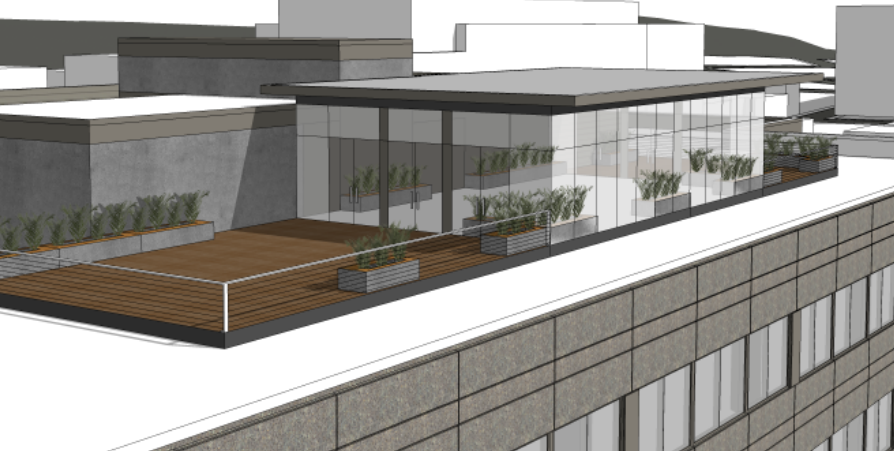
Drip-Line and Limits of Disturbance measurements from face of trunk
Trees on neighboring properties - Drip-line and Limits of Disturbance measurements from property lines



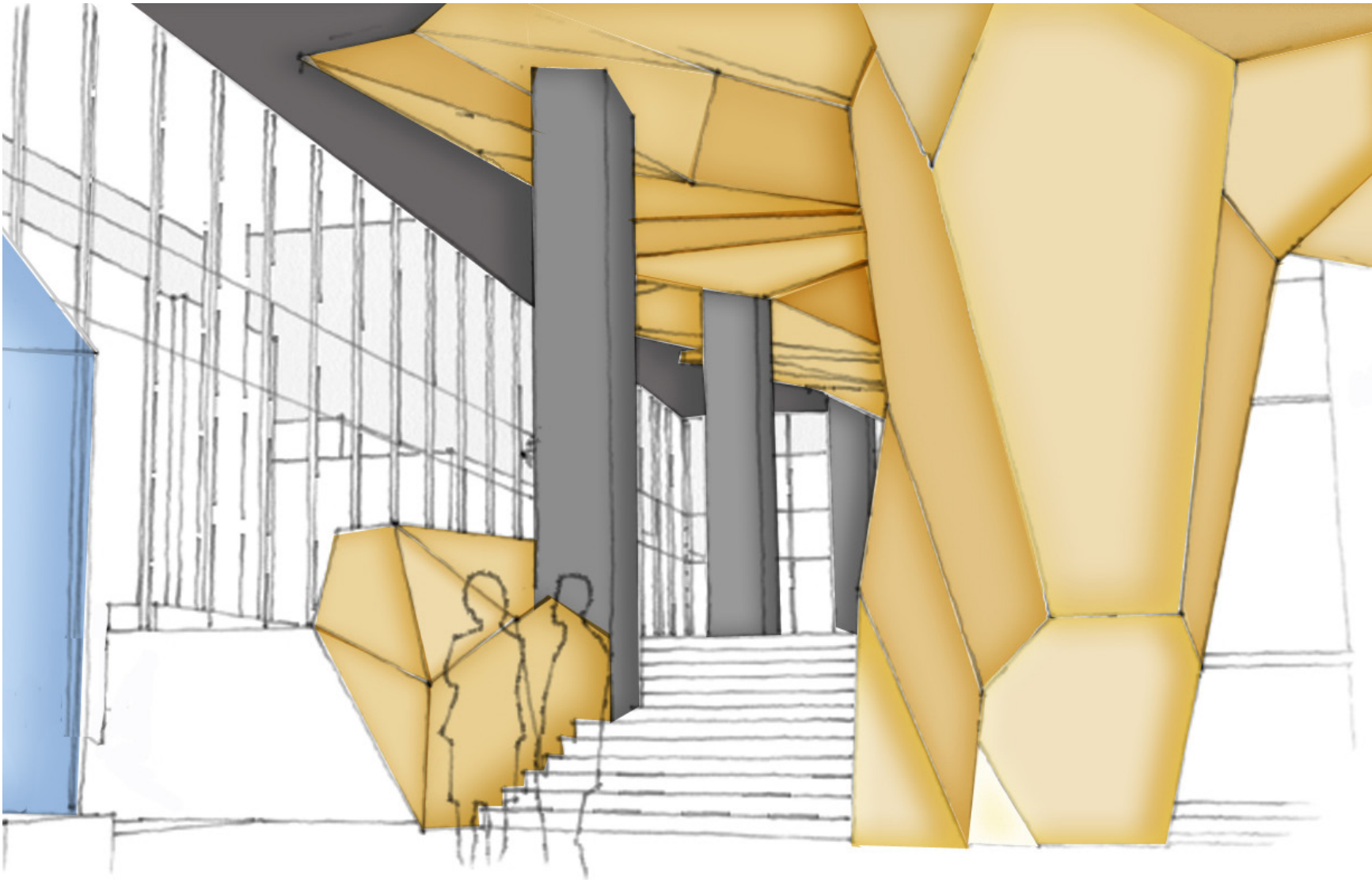
Tree Survey



ROOFTOP AMENITY SPACE



INTERIOR FEATURE WALL



View Standing in Lobby looking West



Sculptural architecture



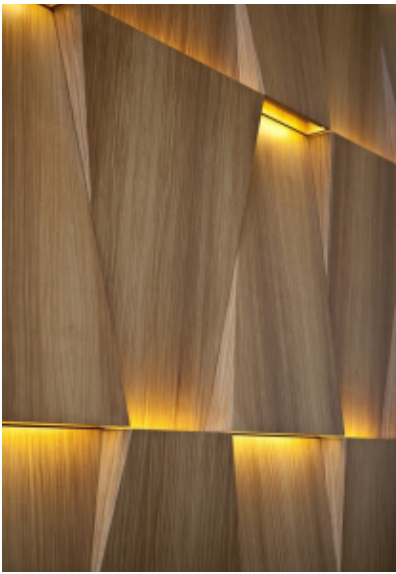
Abstract forms



Architectural lighting



Rustic wood planking



BLACK GRANITE ACCENT WALL

