



CITY OF SEATTLE | APPLICATION FOR EARLY DESIGN GUIDANCE

PART I: CONTACT INFO

1.

Project Addresses:

1307, 1311, 1315, 1317, 1319, 1321, 1325 Harbor Avenue S.W.
1312 California Way
2.

DPD Project Numbers:

3015628, 6374794
3.

Owner / Lessee Name:

YMSA USA LLC
4.

Contact Person Name:

Margaret Sprug

Firm:

The Miller Hull Partnership, LLP

Mailing Address:

71 Columbia Street - 6th Floor

City State Zip:

Seattle, WA 98104

Phone:

206.254.2039

E-mail Address:

msprug@millerhull.com
5.

Applicant's Name:

Margaret Sprug

Relationship to Project

Architect
6.

Design Professional's Name:

Margaret Sprug

Firm:

The Miller Hull Partnership, LLP

Mailing Address:

71 Columbia Street - 6th Floor

City State Zip:

Seattle, WA 98104

Phone:

206.682.6837

E-mail Address:

msprug@millerhull.com
7.

Applicant's Signature:

Date

*Only the contact person will receive notice of the meeting. The contact person is responsible for informing other pertinent parties.

PART II: SITE AND DEVELOPMENT INFO

1.

Please describe the existing site, including location, existing uses and/or structures, topographical or other physical features.

The site is located in West Seattle on a parcel that fronts both Harbor Avenue SW to the east and California Way SW to the west, just north of the Water Taxi terminal. Part of the project site includes the site of the former Alki Tavern. The buildings on the site consist of 1-2 story wood frame commercial and multi-family structures that are in poor to fair condition. The site is relatively flat along the Harbor Avenue SW frontage at an elevation of approximately 14.0' and is bordered to the east by a steep forested slope up to California Way SW. Across California Way SW to the east, the sloping hillside continues to an elevation of approximately 250.0'. There is one single family structure built in the 1920's that fronts California Way SW. Half of this existing structure is built in the right of way.
2.

Please indicate the site's zoning and any other overlay designations, including applicable Neighborhood-Specific Guidelines

The project site is comprised of multiple parcels of land. The three northern lots fall within a MR Multi-family zone and the five lots to the south fall within a NC2-65 zone. There is a small sliver of land at the northwest corner of the site that falls within the SF 7200 Single Family zone. The MR zoned lots fall under the Alki Area Parking Overlay (23.54.015B). The project does not fall within any specific neighborhood design guidelines so will follow the newly adopted city-wide guidelines. The east edge of the site falls with in an archeological buffer zone. The east edge of the site falls within a designated Environmental Critical Area for steep slopes. California Way SW and Harbor Avenue SW are designated scenic routes.

3.

Please describe neighboring development and uses, including adjacent zoning, physical features, existing architectural and siting patterns, views a, community landmarks, etc.

At the north end of Alki in the vicinity of this project, the developed uses remain largely residential with a smattering of mixed use and commercial uses. To the east, is the Don Armeni Boat Ramp, and recreational bike and pedestrian pathways that lead to Alki's beaches and commercial district farther to the east. Adjacent zoning is MR Multifamily Residential to the north, NC2-65 Neighborhood Commercial to the south and SF 7200 Single Family. There are significant city, water and mountain views to the northeast, east and southeast. Views to the west are largely of forested hillside.
4.

Please describe the applicant's development objectives, indicating types of desired uses, structure height (approx), number of residential units (approx.), amount of commercial square footage (approx.) and number of parking stalls (approx.). Please also include potential requests for departure from development standards.

Development Objectives:

- 79,250 square foot mixed-use structure containing approximately:
 - 21 residential apartments, totaling about 25,400 square feet
 - 11,800 square feet of commercial office space
 - 7,500 square feet of light manufacturing
 - 6,700 square feet of ground floor retail
 - 4,200 square feet of restaurant
 - 41 parking spaces below grade, totaling approximately 14,400 square feet

REQUESTED DEPARTURES

1.

Side Yard Setback - SMC 23.45.518.B

- The access into our parking garage is along the north property line in the MR zone. Although the side yard setback requirement for this zone (for portions of the structure less than 42') is 7' average / 5' minimum, we believe that our north neighbors would benefit from screening the garage entrance with a wall along the property line with landscaping on the deck above. The wall would prevent the glare from headlights cars exiting up the ramp towards the north and towards the neighbors' bedroom windows.
2.

Front Yard Setback - SMC 23.45.518.B

- The project is proposing a gracious, 3 story high breezeway thru the entire structure front to back in lieu of the courtyard exception. The breezeway width is partially in the MR zone and partially in the NC2-65 zone. The breezeway is entirely open to the public and would provide a significant public amenity. The stairway thru the breezeway will provide views to and access up the hillside to the west. The stairway will also provide perches to enjoy the city and sound view to the east. The breezeway provides access to the upper level restaurant and is a permanent part of the project exiting scheme.
3.

Sight Triangle - SMC 23.54.030G

- The project is providing a 22'-0" wide two-way entry to the parking garage. The project proposes the use of a safety mirror in lieu of the required site triangle at the exit side of the ramp, similar to the exception used in Downtown, Industrial and Commercial 1& 2 zones. This would develop a more cohesive and enhanced street frontage for pedestrians along Harbor Avenue SW by de-emphasizing the drive lane width.
4.

Parking - Backing Distance SMC 23.54.030.C

- ECA site development restrictions significantly limits the width of the building and the space available to park vehicles requiring an exception to the 50' backing distance limit. Building tenants with familiarity of the garage will be able to negotiate a longer backing distance of approximately 100'. Private parking can be designated with signage. The limited number of required public parking stalls will not require excessive backing distance. A parking scheme on this tight site that meets the 50' backing restriction would require two levels of parking to meet the required number of parking stalls for the proposed program.

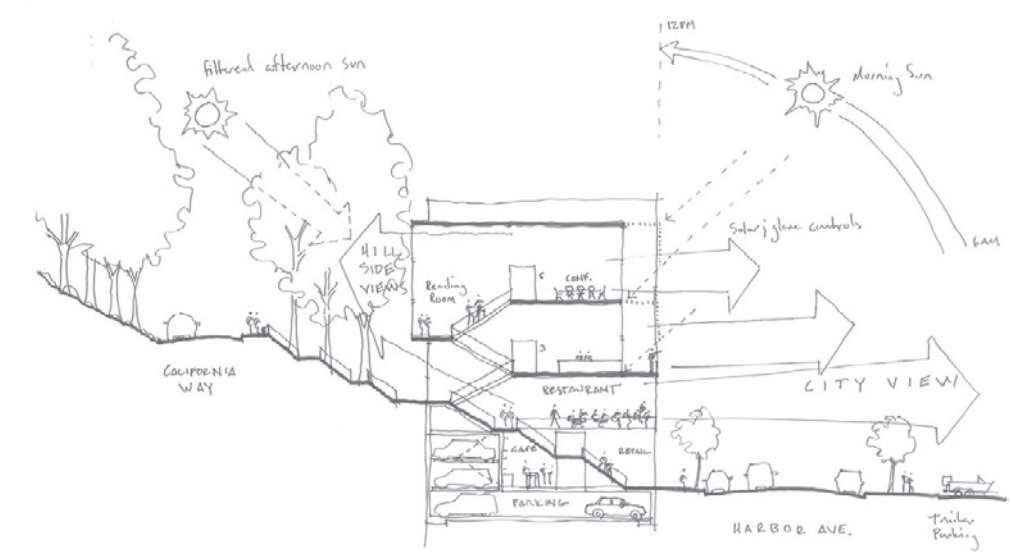
SITE ENVIRONMENT

The project site has a long north-south orientation, and the steep slope of the hillside limits the buildable area to a long, narrow strip along the eastern edge of the site. The resulting building shape is one with a long eastern exposure, capable of harnessing early morning sun during heating months while employing internal blinds to reflect heat gain in cooling months. The west façade of the building facing the hillside will benefit from the existing heavy deciduous and evergreen tree canopy which will block the majority of late afternoon solar exposure in summer. In winter, when the nearby trees have lost their leaves, the project will benefit from afternoon sun.

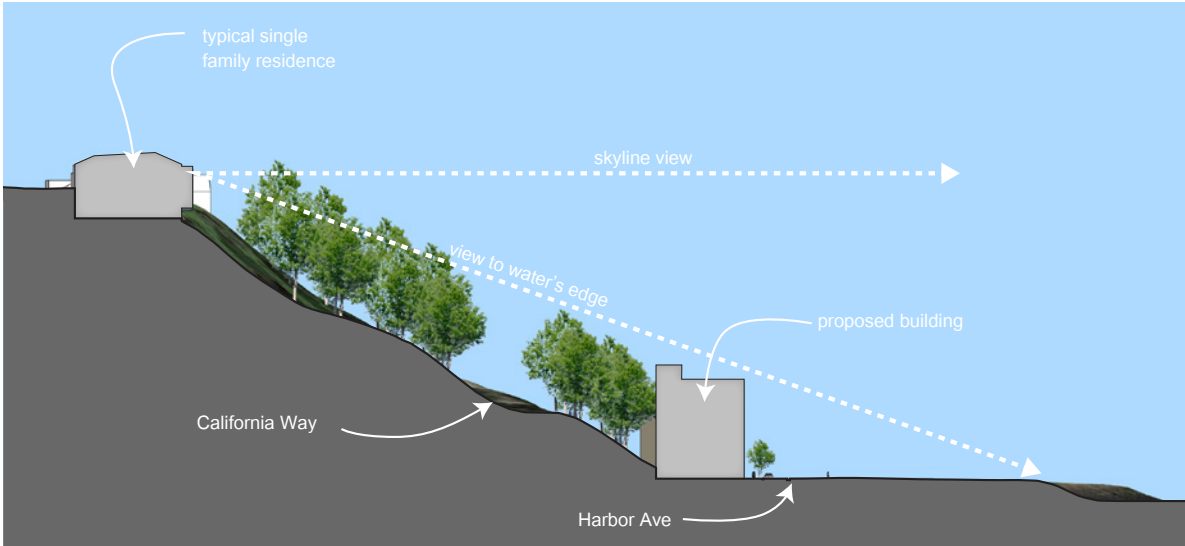
By limiting the development on the hillside, existing trees and habitats will be preserved on the undeveloped west portion of the property. Both landscape and building design will integrate with existing on-site natural habitats along the by incorporating similar elements such as plantings and boulders. The project proposes a two-story water feature which will be a continuous waterfall that will mute the sounds of street traffic and create a natural northwest forest stream ambience.

Due to the project’s proximity to Elliott Bay, there is the possibility that natural through ventilation could be used to cool parts of the building. All occupiable spaces in the building could have operable windows and large door openings on both the east and west facades to promote passive ventilation when appropriate.

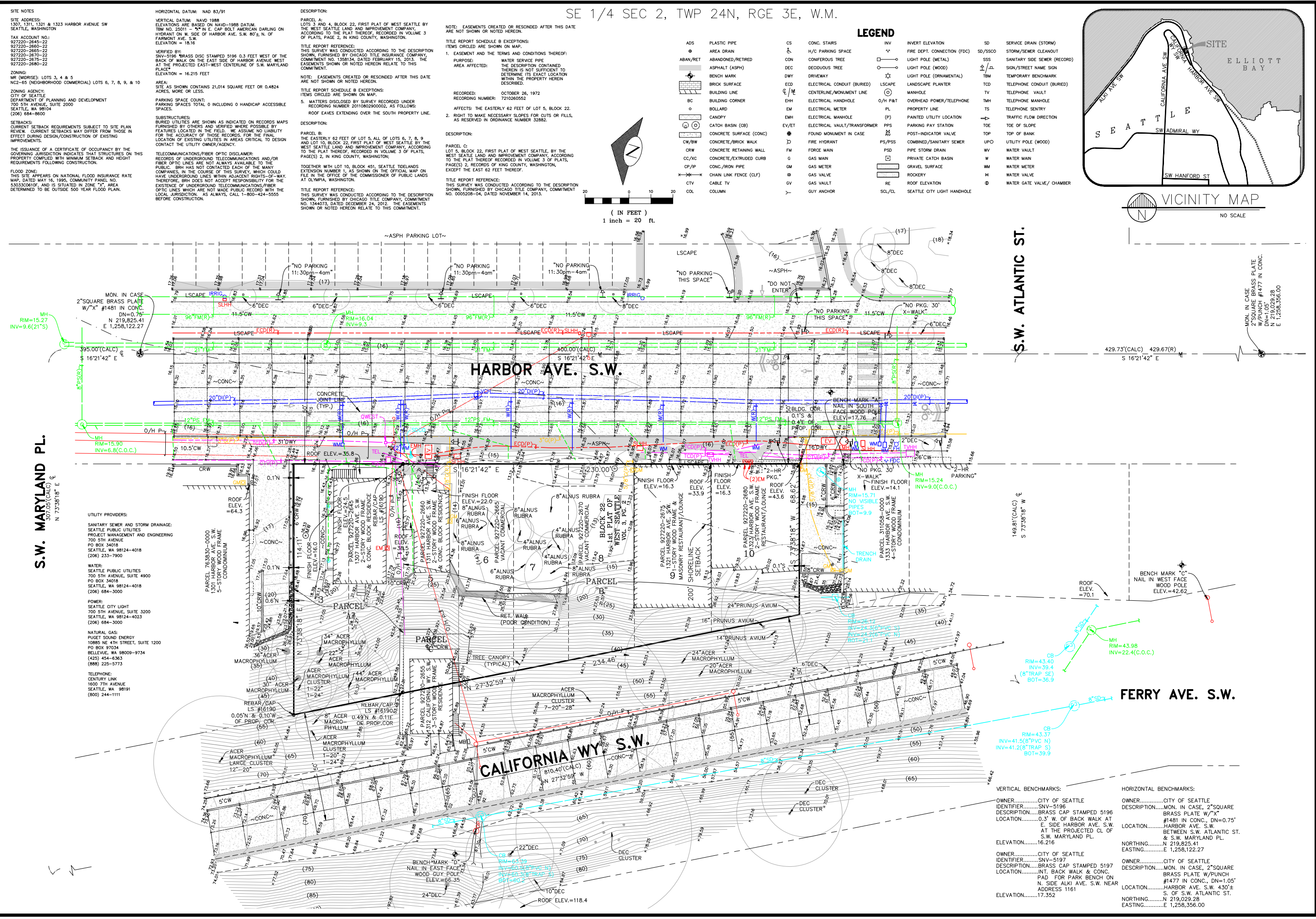
The site’s unique location also affords the possibility of linking California Way SW with Harbor Avenue SW via a public stairway. The existing single-family residence (which currently encroaches into the public right of way on California Way SW) will be demolished. This previously developed uphill lot will be restored with native habitat and a publicly accessible route will be created down the hillside, bringing the public through the forested hillside site and giving access to the waterfront and potential new ground level commercial uses.



EARLY SECTIONAL CONCEPT SKETCH



SITE SECTION



BUSH, ROED & HITCHINGS, INC.
CIVIL ENGINEERS & LAND SURVEYORS
2009, MINOR AVE. EAST
SEATTLE, WA 98102-3510
PHONE: (206) 333-4144
FAX: (206) 333-7135
WEBSITE: BRH-INC.COM

RHODES ARCHITECTURE & LIGHT
HARBOR AVE. S.W. & CALIFORNIA WY. S.W.
SEATTLE
KING COUNTY
WASHINGTON

NO	REVISION	DATE
3	UPDATED TREE INFORMATION	3/24/14
2	ADDED PARCEL C	2/20/14
1	TREE SPECIES	9/4/13

drawn by
JAL/MWH

checked by
JUM

scale
1"=20'

date
9/04/13

job no.
2013025.00

sheet
1 of 1

SITE CONSTRAINTS

The property is 230 feet in length from the north end to the south and tapers from approximately 114 feet wide on the north end to 68 feet wide on the south end. There are a variety of buildings currently on the site; three single-family residences, each between 2 and 3 stories tall and 2 commercial buildings, between 1 and 2 stories tall. All of these buildings will be demolished. Adjacent to the project site are two condominiums: the Harbor Landing Condominiums (4 stories, 10 units) to the south and the Sea Bird Condominiums (4 stories, 50 units) to the north.

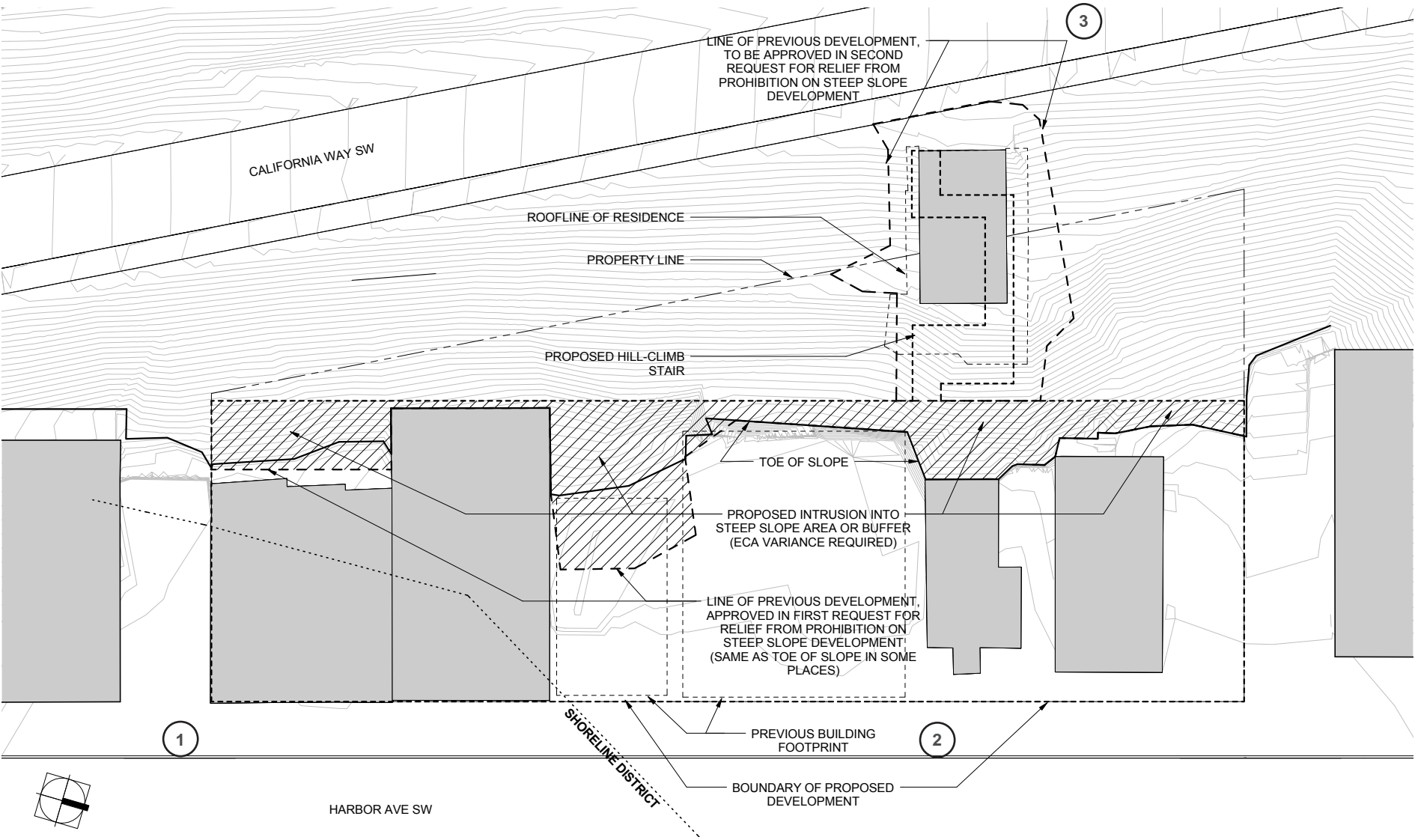
SHORELINE DISTRICT

The southern three lots fall partially within the boundary of the Shoreline District, which is designated Urban Stable (US) in this location. The maximum building height in the US Environment is 30 feet. Therefore, portions of the building above 30 feet in height must be set back outside the Shoreline District. In addition, a view corridor of 35% of the width of the portion of the lots within the Shoreline District is also required, unless modified under SMC 23.60.162.

ECA STEEP SLOPE

Much of the western portion of the site is an ECA steep slope. We will be requesting a variance to encroach into not more than 30% of the slope. This will allow for a 62 foot wide building footprint with 5 feet of buffer behind the building for construction of the site wall and in order to establish proper drainage. This is the minimum viable building footprint that will allow for functional below grade parking. Existing retaining walls on site will be removed, as they are in poor condition and not up to the standards of the current code.

The line of previous development was determined through a Request for Relief from Prohibition on Steep Slope Development in September of 2013. Site development without a steep slope variance must be east of this line. We will be requesting a second Request for Relief which will propose an additional line of previous development along the hillside where the existing single-family residence has been constructed.



1 EXISTING COMMERCIAL STRUCTURES ON SITE



2 CAVED-IN SIDEWALK ALONG HARBOR AVE



3 UPHILL SINGLE-FAMILY RESIDENCE

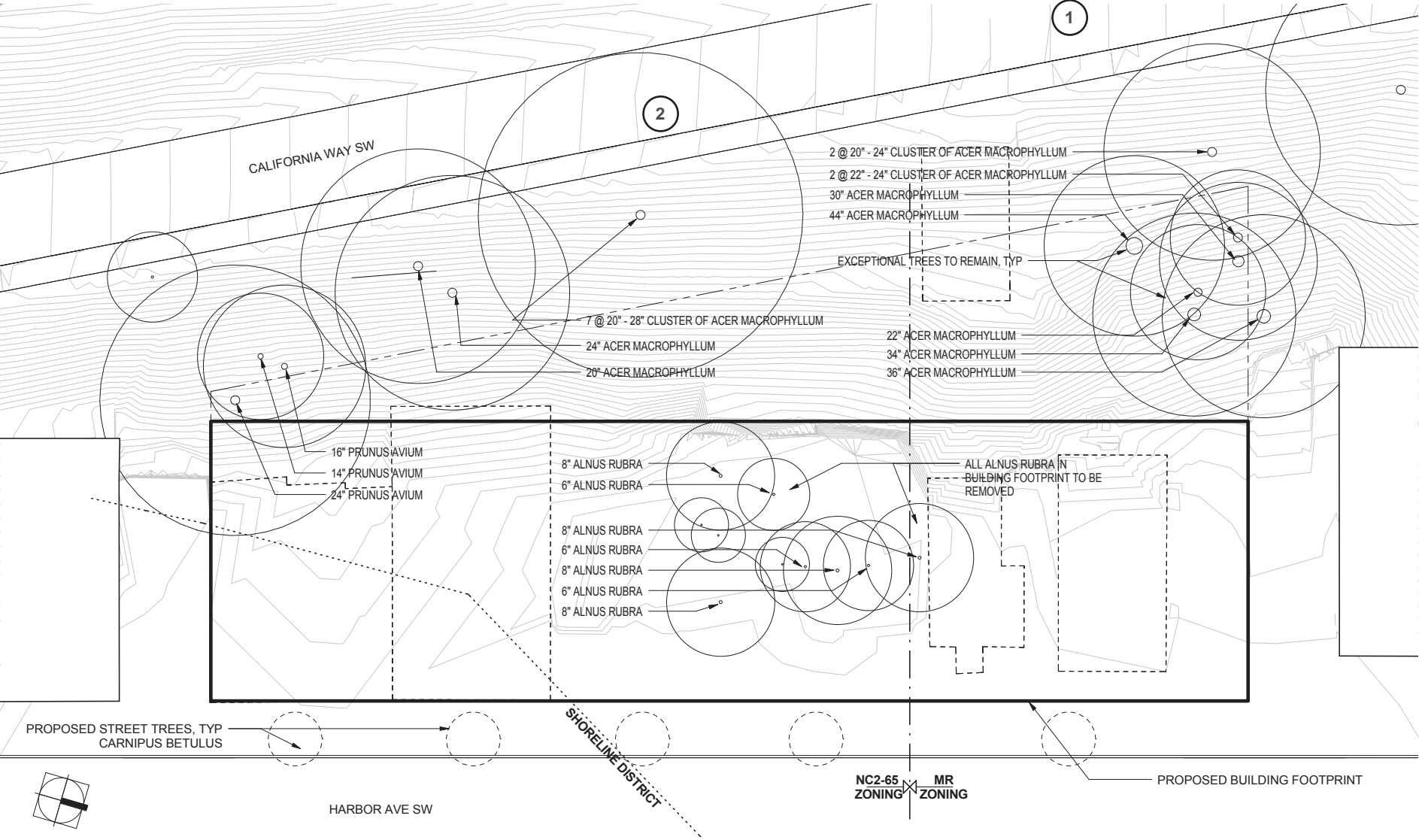
TREES, LANDSCAPE, AND OPEN SPACE

The project does not propose to remove any exceptional tree clusters. There are numerous tree species on the project site and within the right of way along California Way SW. The hillside is predominantly populated by groupings of tall, dense Bigleaf Maple trees (*Acer macrophyllum*) with a few Wild Cherry trees (*Prunus avium*) existing along the southern portion of the site. The sheer size of these trees and the density of their canopies allow them to effectively screen the roof of the proposed building from residences further up the hill.

There are also a number of much smaller Red Alder trees (*Alnus rubra*) in the lower, flatter part of the site. These fall within the building footprint and will be removed. Pyramidal European Hornbeams (*Carpinus betulus*) will be planted along Harbor Avenue SW between the sidewalk and the curb and will be accompanied by other, smaller plantings.



AERIAL VIEW FROM HILLTOP RESIDENCE
The project's roofline is concealed from above by the dense mass of trees which cover the steep slope of the hillside. This is view 5 on the map on page 11.



1 DENSE TREE GROWTH UPHILL OF SITE



2 TREES GROWING IN BASEMENT OF FORMER EMBERS NIGHTCLUB

FAR AND BUILDING ENVELOPE

To establish the allowable height of the proposed structure, the ‘average grade level’ per SMC 23.60.952 was determined, as depicted in the adjacent plan diagram. This establishes the base elevation at the 19’-9” datum, approximately 3’-6” above the sidewalk elevation, as depicted in the elevation diagram.

Per SMC 23.45.514.G.3, the base height limit of 60’-0” in the MR zones may be increased five feet to match the adjacent NC2-65 zone (ref elevation diagram).

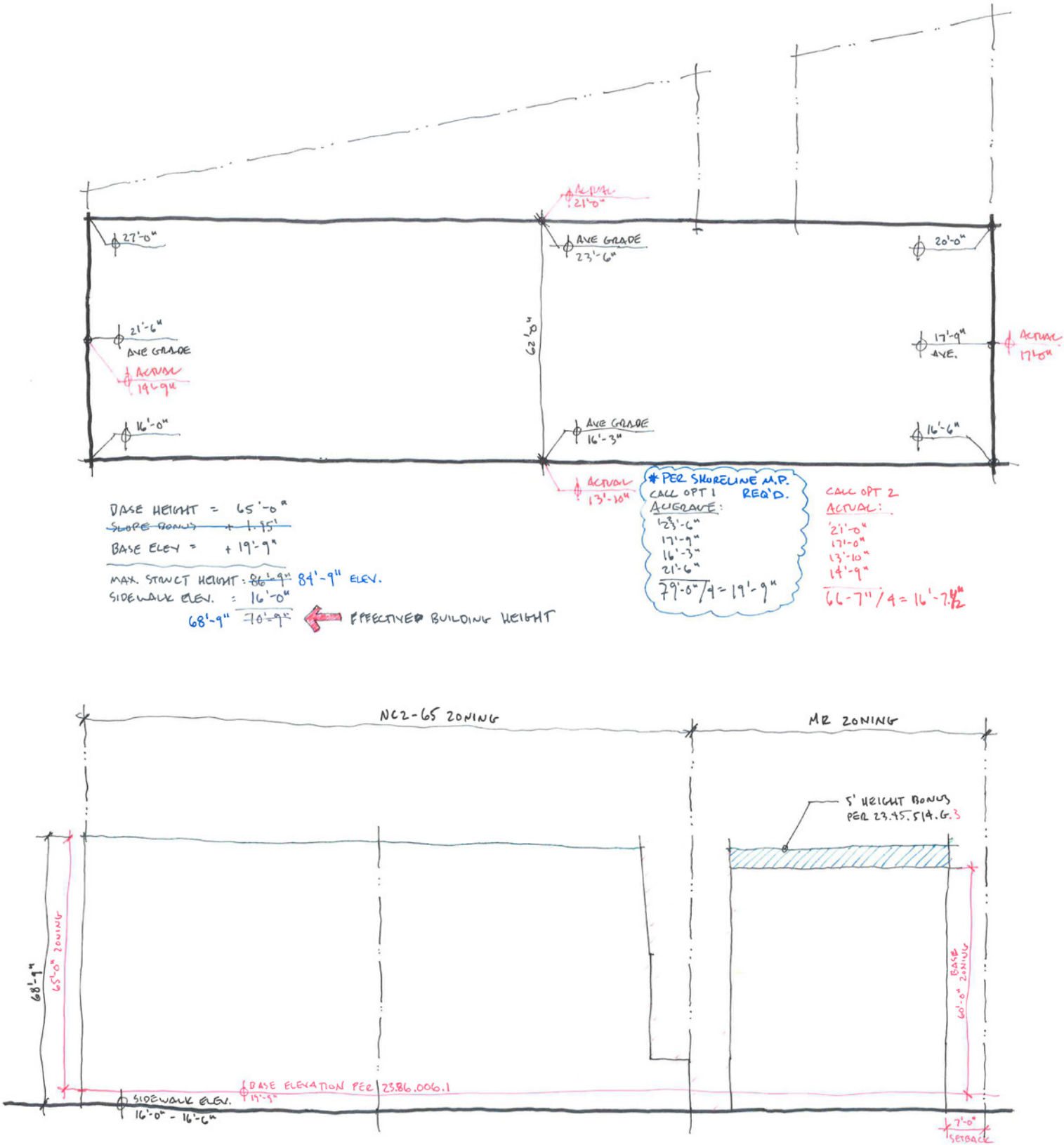
In combination, these provisions allow for a building with an effective height of approximately 70’-3” above sidewalk elevation on Harbor Avenue SW.

The split site zoning for this site requires two different floor area ratio (FAR) calculations. For the southern portion of the site in the commercial NC2-65 zone, the allowable FAR is 4.75. For the northern portion of the site in the residential MR zone, the FAR is required to be lower at 3.2.

In none of the schemes developed for Early Design Guidance review were the allowable FARs achievable. The western portion of the site is impacted by the ECA steep slope; the southeastern portion, by the Shoreline District.

The totally code compliant Option A had both achievable FARs significantly below the allowable FAR with the commercial zone FAR = 2.8 and the residential zone = 2.5. With the low FARs, high skin-to-volume ratio and the car elevator required to make this option work, it would be unlikely that this option would be financially viable.

With an ECA variance for steep slope encroachment, a functional below grade parking garage can be developed. Options B-D work on this assumption resulting in somewhat higher FARs but still significantly below the allowable FARs. For these three options, the actual FARs are fairly similar. The preferred Option D, has an actual commercial FAR = 3.3 and a residential FAR = 2.8, slightly below the other two options. Please see the Massing Alternatives - section 6.0 for more information.





PARKS FORESTS & BEACHES

The project is located near the north tip of West Seattle, close to the east end of the Alki Beach area. The string of local public amenities begins with Luna Park Beach at the north which blends into Seacrest Park at the Don Armeni Boat Ramp directly across from the project site. This long stretch of parks and beaches along the waterfront attracts walkers, runners, fishers, and kayakers, making the area a highly visited destination for residents and tourists alike, especially in summer months. The project, given its adjacency to these public destinations, has the opportunity to support passers-by and park-goers by providing robust ground level commercial uses like retail shops and dining establishments.

There is also a significant amount of wooded area within the vicinity of the project site, primarily along the steep hillside which wraps around West Seattle. The western portion of the project site, starting at the toe of the slope, is similarly forested, providing opportunities for a design concept that integrates with the existing landscape. A little further west of the site is California Way SW, which bisects the hillside as it winds its way to the toward Hamilton Viewpoint Park. At the top edge of the hill are a handful of single-family houses, at roughly 200.0' above sea level, well above the grade of the project site.



DON ARMENI BOAT LAUNCH



HAMILTON PARK



HAMILTON PARK



SEACREST PARK

TRANSIT & ACCESS

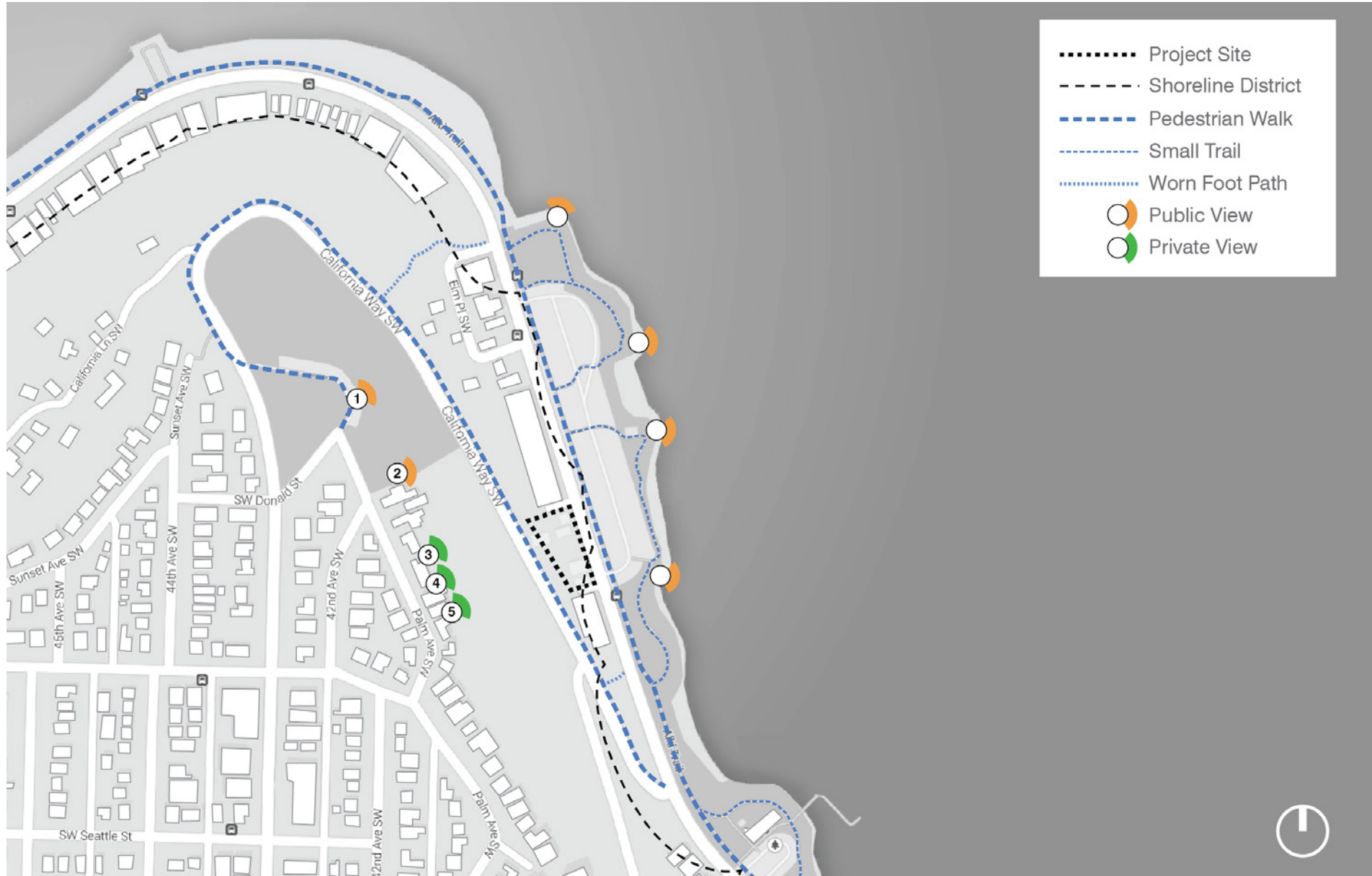
The project site is directly adjacent to a Metro Bus stop near the southeast corner. A couple of “blocks” to the south is the water taxi dock.

California Way SW and Harbor Avenue SW are designated Scenic Routes. The upper portion of California Way SW that wraps Hamilton Viewpoint Park is the portion of the roadway that opens up to territorial views over the treetops. Near the project site, the lower portion of California Way SW is largely bordered by tall trees with only peek-a-boo slots to the city and water view beyond.



WATER TAXI





VIEW FROM HAMILTON PARK
This is view 1 from the map above.



ALKI TRAIL



WORN FOOTPATH

PEDESTRIAN ROUTES & PUBLIC LIFE

The area surrounding the project site has the potential to offer plenty to pedestrians. Currently, the site has a walk score of 32, OK - Car Dependent. The Alki Trail rounds the tip of West Seattle, connecting Seacrest Park with Luna Park and beyond. A number of smaller trails exist throughout Seacrest Park, tying together a handful of scenic overlooks. California Way SW provides a less formalized route for walkers and cyclists looking to move between the waterfront and the upper residential area of West Seattle. At the top of the hill, Hamilton Park provides a wide-angle view of the Seattle skyline, Elliot Bay, and the Olympic Mountains beyond.

SHORTCUTS

For pedestrians looking to shortcut California Way's gradual slope down the hillside, there exist two worn foot paths down the hillside:

- a short foot path exists at the southern end of California Way SW, where walkers have cut the corner of the hillside's large switchback and
- a much steeper, longer trail has been carved through a vegetated part of the slope further north, near Marshall Reserve.

These foot paths are quite steep, and, under inclement weather conditions, can be somewhat treacherous. While they are decidedly not pedestrian friendly, they do seem to suggest that a more accessible connection, such as an outdoor public stairway between California Way SW and Harbor Avenue SW, would be desirable.

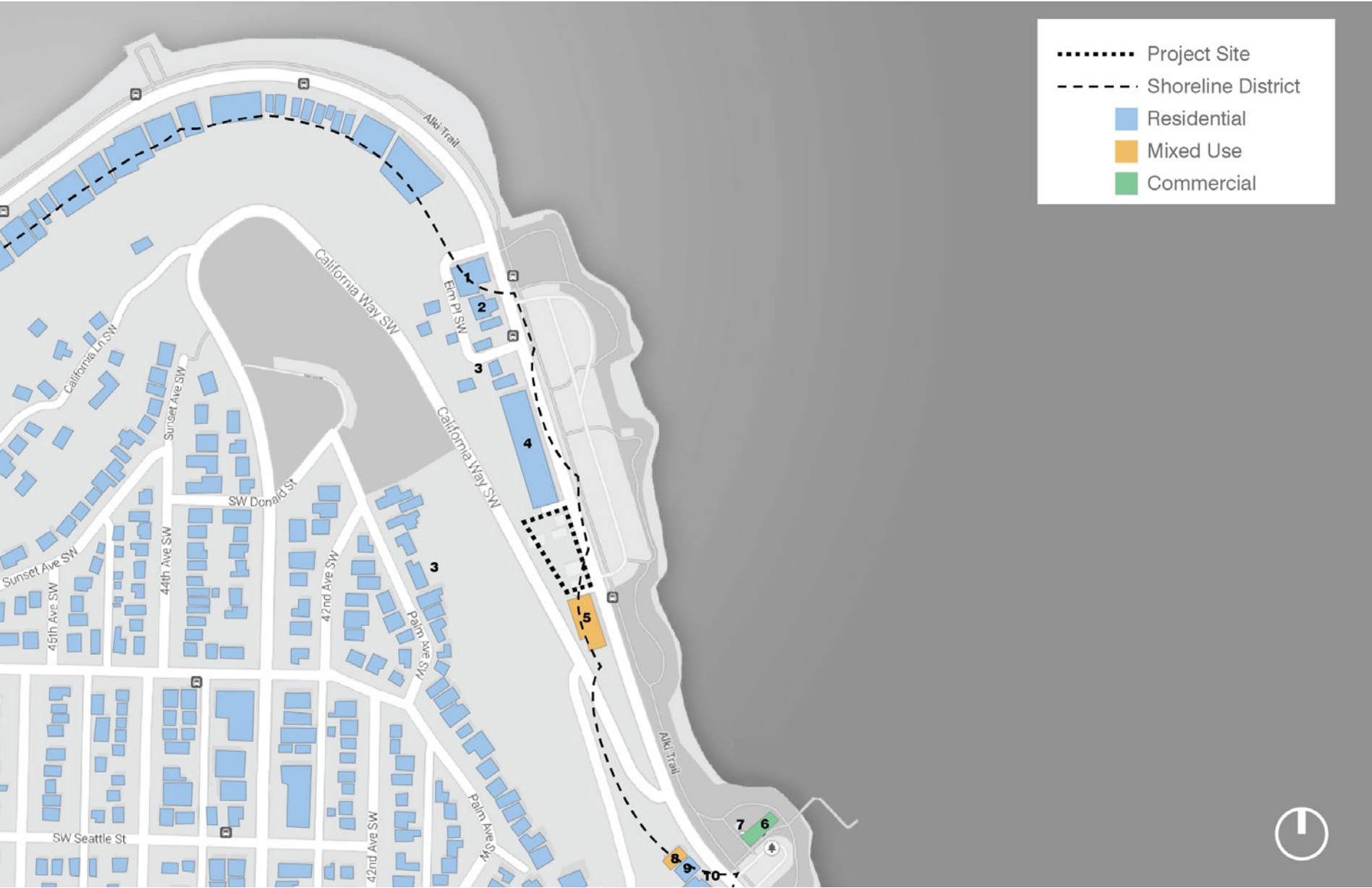
GROUND LEVEL COMMERCIAL

A few commercial amenities exist at ground level for pedestrians travelling along Harbor Avenue SW, but for the most part the west side of the road with its back against the hill is home to large multi-family housing blocks. Ground floor retail and restaurants, with appropriately scaled commercial bases, could make the area more walkable and accommodating for pedestrians.

SURROUNDING USES

At the north end of Alki, the developed uses in the vicinity of the project site remain largely residential with a smattering of mixed use and commercial uses. The waterfront side of Harbor Avenue SW is home to community uses such as beaches, parks, and public spaces like the Don Armeni Boat Ramp. The upland side of Harbor Avenue SW is predominately lined with multi-family residences with very few commercial uses at ground level. There are a handful of small eating establishments further south along Harbor Avenue SW, but for the most part the northern end of the peninsula is without commercial uses. The location of the site for this project is ideally suited for a mixed-use structure, and the proposed building will contain a large retail space (or spaces) as well as a two-story restaurant.

- 1 Alii-Kai Condominiums - 16 units, 5 stories
- 2 Wahkiakum Condominiums - 7 units, 8 stories
- 3 Single Family
- 4 Sea-Bird Condominiums - 50 units, 4 stories
- 5 Mixed Use
 - Harbor Landing Condominium, 10 units, upper 3 stories
 - Alki Dental, ground floor
- 6 Marination Station Restaurant
- 7 Alki Kayak Tours
- 8 Mixed Use
 - Seacrest Place Condominiums - 3 units
 - Alki Juice & Java - Now closed, ground floor
 - Angel Nails - Nail salon, ground floor
- 9 Bayview Condominiums - 9 units
- 10 Markus Place Condominium - 5 units



4 SEA BIRD CONDOMINIUMS



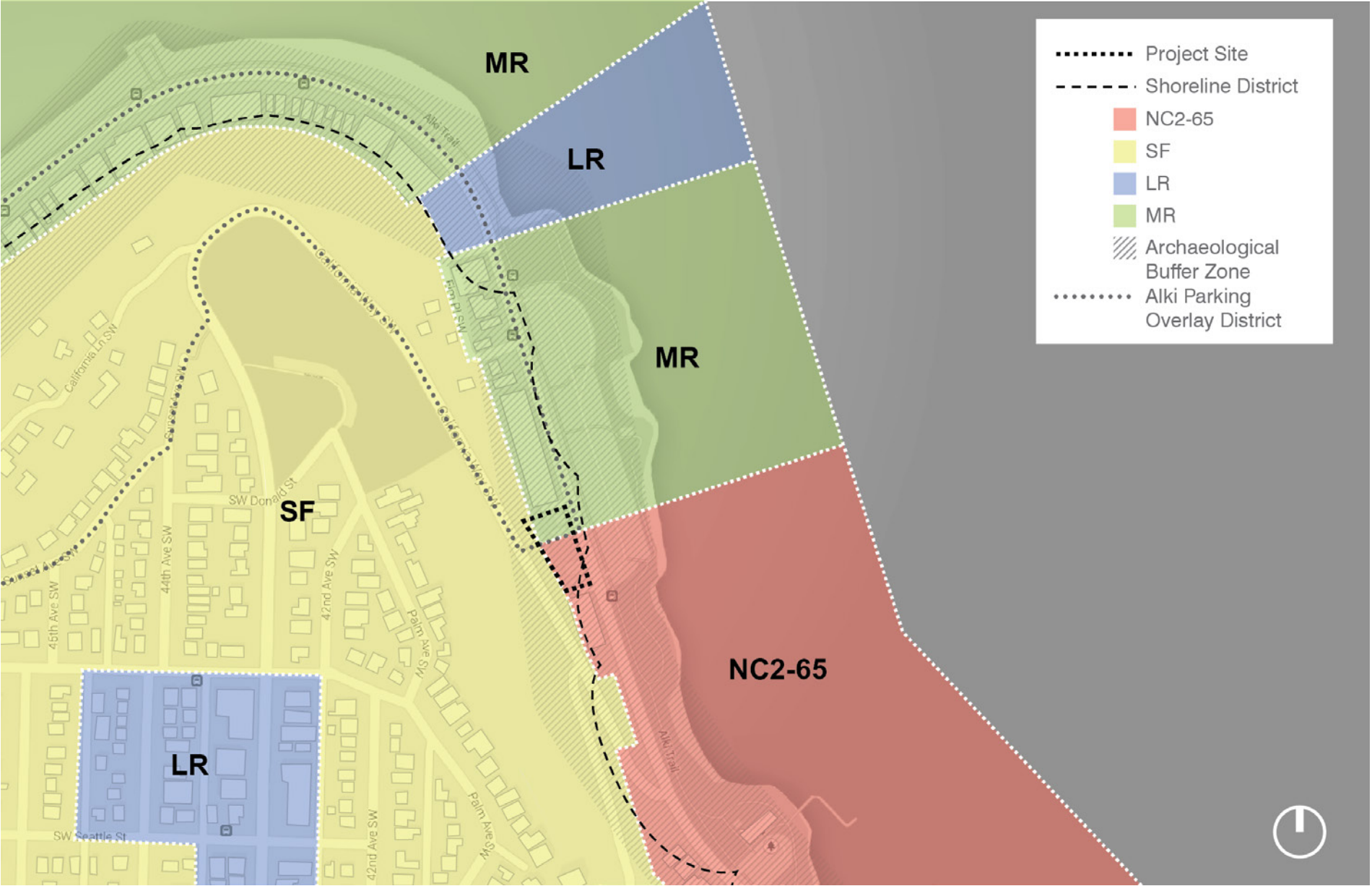
8/9 MIXED USE BUILDINGS



6 MARINATION MA KAI



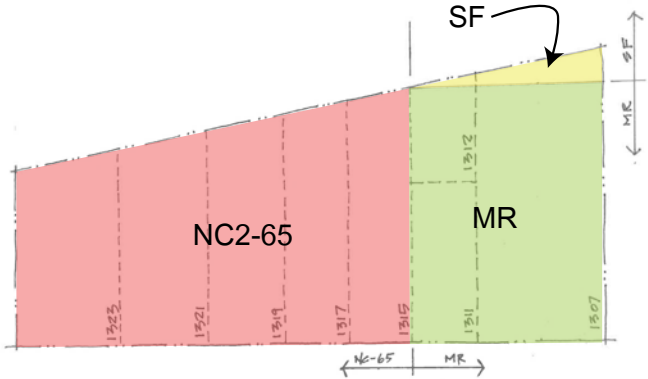
5 HARBOR LANDING CONDOMINIUMS



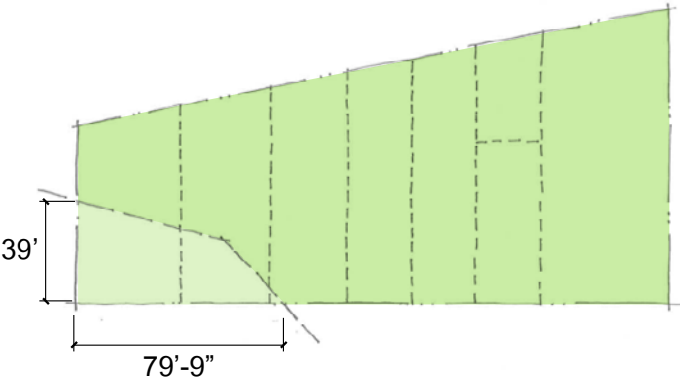
ZONING

The project site is comprised of multiple parcels of land. The three northern lots fall within a MR Multi-family zone and the five lots to the south fall within a NC2-65 zone. There is a small sliver of land at the northwest corner of the site that falls within the SF 7200 Single Family zone. The southeast corner of the site is in the city's shoreline district and the MR portion of the site falls under the Alki Area Parking Overlay (23.54.015B). The project does not fall within any specific neighborhood design guidelines so will follow the newly adopted city-wide guidelines. The site falls within an archaeological buffer zone. The west edge of the site falls within a designated Environmental Critical Area for steep slopes. California Way SW and Harbor Avenue SW are designated scenic routes.

At the north end of Alki, the developed uses in the vicinity of this project remain largely residential with a smattering of mixed use and commercial uses. Adjacent zoning is MR Multi-family Residential to the north, NC2-65 Neighborhood Commercial to the south and SF 7200 Single Family to the west.



SITE ZONING AREAS
NC2-65 = 13,012 SF
MR = 7,445 SF
SF 7200 = 565 SF



SHORELINE DISTRICT

ZONING CODE ANALYSIS

YMSA

MH project #1321

Seattle Zoning Code Analysis

March 21, 2014

DPD Project Nos. : 3015628, 6374794
Project Addresses: 1315, 1317, 1319, 1321, 1323 Harbor Avenue S.W.
Zoning: NC2-65' (Neighborhood Commercial)
Total Lot Area: 13,012 square feet

Project Address: 1311Harbor Avenue S.W.
Zoning: MR (Multi-family)
Total Lot Area: 1550 square feet

Project Addresses: 1307 Harbor Avenue S.W.
1312 California Way S.W.
Zoning: Split Zoning: MR (Multi-family) / SF7200 (Single Family)
Total Lot Area: MR=5895 square feet, SF7200 = 565 Square feet

general		
zoning adjacent to site	north east south west	MR (Multi-family) N/A (Puget Sound) NC2-65' (Neighborhood Commercial) SF7200 (Single Family)
zoning overlays		Alki Area Parking Overlay Archeological Buffer Area California Way SW and Harbor Avenue SW - designated Scenic Routes
critical areas		Shoreline Setback ECA - Steep Slope / Liquefaction Zone
code section	subject	code language
23.45	Multi-Family (MR)	
23.45.504	Permitted and prohibited uses	Residential, permitted Ground Floor Commercial, permitted subject to 23.45.504.E
23.45.504.E.2	Ground Floor Commercial use	The following uses are permitted per 23.45.532: Business support services; General sales and services; Offices; Restaurants, etc.
23.45.510	Floor area ratio (FAR) limits	Base FAR = 3.2 <ul style="list-style-type: none">Allowable = Site area in MR, 7445sf x 3.2 = 23,824sfProposed = 19,435sf, FAR = 2.6
23.45.514.B	Structure Height	Base height limit = 60 feet
23.45.514.G.3	Height increase	In MR zones, the base height limit is increased by 5 feet if the number of stories...does not exceed six, and...(3.) the lot is split between a MR zone and an NC zone, and the base structure height allowed in the NC-zoned portion is 65 feet or more.
23.45.514.J.5	Rooftop features	The following may extend 15 feet above the applicable height limit...if the combined total coverage does not exceed 20 percent of the roof area, or 25 percent of the roof area if the total includes screened mechanical equipment: Stair penthouses;

		mechanical equipment; penthouse pavilions for the common use of residents; greenhouses and solariums...
23.45.514.J.8	Protect solar access	Locate rooftop features at least 10 feet from the north edge of the roof, or provide shadow diagrams....
23.45.518.B	Setbacks and Separations	Front and side setback from street lot lines: <ul style="list-style-type: none">7 foot average setback; 5 foot minimum setback No setback is required if a courtyard abuts the street and the courtyard has: <ul style="list-style-type: none">A minimum width equal to 30 percent of the width of the abutting street frontage or 20 feet, whichever is greater;A minimum depth of 20 feet...from the lot line Side Setback from interior lot line: <ul style="list-style-type: none">Portion of structure 42 feet or less: 7 foot average, 5 foot minimumAbove 42 feet: 10 foot average setback; 7 foot minimum
23.45.518.J	Structures in required setbacks	4. underground structures are permitted in any setback
23.45.522.C	Amenity Area	Amount of amenity area in MR is equal to 5 percent of the total gross floor area of a structure in residential use.
23.45.522.D	Amenity Area	2. Enclosed amenity area. <ul style="list-style-type: none">In MR zones no more than 50 percent of the amenity area may be enclosed, and this enclosed area shall be provided as common amenity area.
23.45.522.D	General Requirements	1. All units shall have access to a common or private amenity area 2. no more than 50 percent of the amenity area may be enclosed 5. no common amenity area shall be less than 250 square feet in area, minimum horizontal dimension of 10 feet and shall have seating, exterior lighting, weather protection, etc. . . .
23.45.524	Landscaping standards	A.2. Green Factor of 0.5 or better required... B.1. Street trees required...
23.45.532.A.1	Standards for ground floor commercial uses in MR and HR zones	On sloping lots, the commercial use may be located at more than one level within the structure as long as the floor area in commercial use does not exceed the area of the structure's footprint.
23.45.532.A.2	Ground floor Commercial	The maximum size of use of any one business establishment is 4000 square feet...
23.45.532.A.3	Ground floor Commercial	Vents for venting of odors...and other similar devises, shall be at least 10 feet above finished sidewalk grade, and directed away fromfrom residential uses within 50 feet of the vent.
23.45.534	Light and glare standards	To prevent vehicle lights from affecting adjacent properties, driveways and parking areas for more than two vehicles shall be screened...by a fence or wall between 5 feet and 6 feet in height.
23.45.536A	Parking location, access, and screening	Off-street parking spaces are required per Chapter 23.54, Quantity and design standards for access and off-street parking.
23.47A	Commercial	
23.47A.004	Permitted and prohibited uses	C.2.b Restaurants = permitted, limited to 25k sf C.5 Laboratories, Research and development, permitted, 25k sf C.8 Offices, permitted to 25k sf C.10 Sales and Services, general, permitted to 25k sf

		G.1 Manufacturing, light, permitted to 10k sf J.1 Residential, permitted
23.47A.008	Street-level development standards	Blank segments of the street-facing facade may not exceed 20' 60% of the façade shall be transparent Non-residential use @ street level. Minimum of 13'-0" floor to floor.
23.47A.012	Structure height	Base height limit = 65 feet
23.47A.012.C.4	Rooftop features	...may extend up to 15 feet above the applicable height limit, as long as the combined total coverage of all features gaining additional height does not exceed 20 percent of the roof area, or 25 percent...if the total includes stair or elevator penthouses or screened mechanical equipment: b. mechanical equipment f. stair and elevator penthouses
23.47A.012.C.7	Setbacks	Rooftop features listed here shall be located at least 10 feet from the north edge of roof... b. planters; c. clerestories; d. greenhouses and solariums; g. play equipment
23.47A.013	Floor area ratio	Per Table A, total permitted for all uses within a mixed-use structure: Base FAR = 4.75 <ul style="list-style-type: none"> Allowable FAR = 13,012sf * 4.75 = 61,807sf Proposed = 44,006sf, FAR = 3.4
23.47A.015	View Corridors	On lots that are partially within the Shoreline District, a view corridor shall be required for the entire lot if the portion of the lot in the Shoreline District is required to provide a view corridor under the Seattle Shoreline Master Program.
23.47A.016	Landscaping and screening standards	Green factor of 0.30 or greater required. Street trees required.
23.47A.022	Light and glare standards	Exterior Lighting must be shielded and directed away from adjacent uses.
23.47A.024	Amenity area	Required in an amount equal to 5 percent of total gross floor area in residential use. <ul style="list-style-type: none"> Residential area = 13,995sf * 0.05 = 700sf Amenity Areas shall meet the following standards: 2. amenity areas shall not be enclosed 4. 10 foot min dim and total area not less than 250sf
23.47A.030	Required parking and loading	Parking per 23.54.015 and loading per 23.54.035.
23.54.030	Parking space standards	
23.54.030.B.1.b	Residential Uses	When more than five parking spaces are provided, a minimum of 60 percent must be striped for medium vehicles. 40 percent may be striped for any size...
23.54.030.B.2.c	Nonresidential uses	When 20 or more parking spaces are provided, a minimum of 35 percent shall be striped for small vehicles...maximum of 65 percent shall be striped for small...minimum of 35 percent shall be striped for large vehicles.
23.54.030.B.2.d	Minimum vehicle clearance	Shall be at least 6 feet 9 inches on at least one floor...
23.54.030.C	Backing distances	Ingres and egress from all parking spaces shall be provided without backing more than 50 feet.
23.54.030.D	Driveways	Driveway requirements for residential and nonresidential uses are described below. When a driveway is used for both residential and nonresidential parking, it shall meet the standards for nonresidential uses described in subsection 23.54.030.D.2
23.54.030.D.1.c	Width	Driveways of any length that serve more than 30 parking spaces shall be at least 10 feet wide for one-way traffic and at least 20 feet wide for two-way traffic.

23.54.030.D.2.a.2	Width	For one-way is 12 feet, two way 22 feet, maximum width shall be 25 feet.
	Slope	Not to exceed 15%
23.54.030.E	Aisles	90 degrees to spaces, stall length 19 = 24 foot aisle; 16 foot stall length = 22 foot aisle; 15 foot stall length = 20 foot aisle.
23.54.030.F	Curb cuts	We would be allowed up to 3 curb cuts per table A
	Max width	23 feet on arterials
23.54.030.G	Site triangle	Driveways less than 22 feet wide, triangle required on both sides.....driveways more than 22 feet wide, triangle required on exit side only.
23.54.040	Solid waste and recyclable materials storage and access	
23.54.040.B	Storage Requirement	Mixed use development that contains both residential and nonresidential uses shall meet the storage space requirements shown in Table A for 23.54.040 for residential development, plus 50 percent of the requirement for nonresidential development. In mixed use developments, storage space for garbage may be shared between residential and nonresidential uses, but separate spaces for recycling shall be provided.
23.60	Shoreline Master Plan	
23.60.022	Compliance – Partial Shoreline Lot	The use and development standards of this chapter apply only to that part of the development that occurs within the Shoreline District unless the underlying zoning requires the entire development to comply with all or part of this chapter.
23.60.162	View Corridors	<p>23.60.162 View corridors.</p> <p>A. View corridors shall be provided for uses and developments in the Shoreline District as required in the development standards of the environment in which the use or development is located.</p> <p>B. When a view corridor is required the following provisions shall apply:</p> <ol style="list-style-type: none"> A view corridor or corridors of not less than the percentage of the width of the lot indicated in the development standards for the applicable shoreline environment shall be provided and maintained. Structures may be located in view corridors if the slope of the lot permits full, unobstructed view of the water over the structures. Unless provided otherwise in this chapter, parking for motor vehicles shall not be located in view corridors except when: <ol style="list-style-type: none"> The parking is required parking for a water-dependent or a water-related use and no reasonable alternative exists; or The area of the lot where the parking would be located is four (4) or more feet below street level. Removal of existing landscaping shall not be required. <p>C. The Director may waive or modify the view corridor requirements if it is determined that the intent to preserve views cannot be met by a strict application of the requirements or one (1) of the following conditions applies:</p> <ol style="list-style-type: none"> There is no available clear view of the water from the street; Existing development or topography effectively blocks any possible views from the street; or The shape of the lot or topography is unusual or irregular. <p>D. In making the determination of whether to modify the requirement, the Director shall consider the following factors:</p> <ol style="list-style-type: none"> The direction of predominant views of the water; The extent of existing public view corridors, such as parks or street ends in the immediate vicinity;

		<div><div>3. The availability of actual views of the water and the potential of the lot for providing those views from the street;</div><div>4. The percent of the lot which would be devoted to view corridor if the requirements were strictly applied;</div><div>5. Extreme irregularity in the shape of the lot or the shoreline topography which precludes effective application of the requirements; and</div><div>6. The purpose of the shoreline environment in which the development is located, to determine whether the primary objective of the environment is water-dependent uses or public access views.</div></div>
23.60.636	View Corridors	<div>A. A view corridor or corridors of not less than thirty-five (35) percent of the width of the lot shall be provided... 79’9” frontage * 35% = 27’11” required view corridor.</div>
23.60.632	Height	<div>A. Maximum Height. The maximum heights in the US Environment shall be as follows, as modified in subsections B through E of this section:<div><div>1. The maximum height on upland lots along Harbor Avenue Southwest between California Way Southwest and Southwest Bronson Way shall be sixty-five (65) feet.</div></div></div> <div>D. Rooftop Features.<div><div>1. Open rails, planters, skylights, clerestories, monitors, greenhouses, parapets, and firewalls may extend four (4) feet above the maximum height limit with unlimited rooftop coverage.</div><div>2. Stair and elevator penthouses and mechanical equipment may extend up to fifteen (15) feet above the maximum height limit, so long as the combined total coverage of all features listed in this subsection does not exceed twenty (20) percent of the roof area or twenty-five (25) percent of the roof area if the total includes screened mechanical equipment:</div></div></div>
23.60.954	View Corridor Measurement	<div>When a view corridor is required, it shall be provided according to the development standards set forth in Section 23.60.162 using the following measurement techniques:<div><div>A. The width of the view corridor or corridors shall be determined by calculating the required percent of the width of the lot at the street or upland lot line;</div><div>B. The view corridor or corridors shall be in the direction of the predominant view of the water and, when topographically possible, generally parallel to existing view corridors;</div><div>C. When a lot is bounded by more than one (1) street, the Director shall determine which street front shall be used for the view corridor calculation; the determination shall be based on consideration of the relative amounts of traffic on each of the streets, the direction of the predominant view of the water and the availability of actual views of the water.</div></div></div>



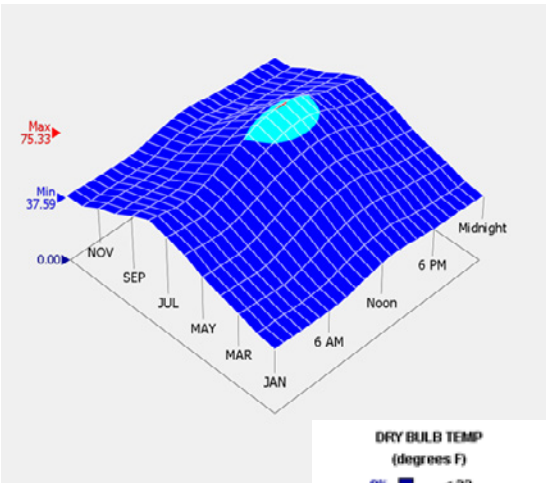
SITE OPPORTUNITIES AND CONSTRAINTS

OPPORTUNITIES & ADVANTAGES

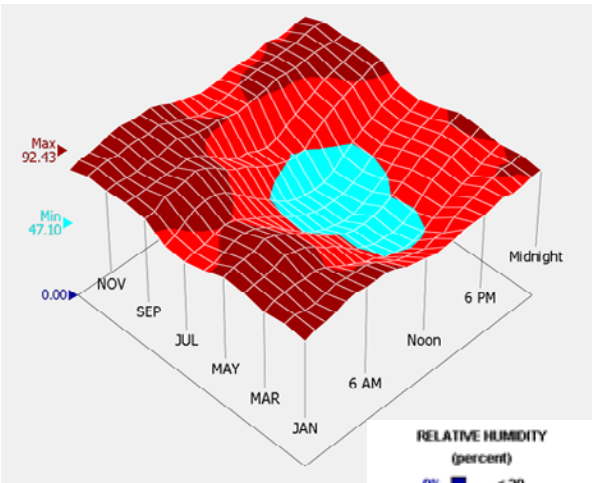
- Upper building levels will have significant eastern views out over the water and western views of forested hillside.
- Narrow site enables many spaces to enjoy both east and west views.
- Good solar access to the east. Harness morning sun to warm the building during heating months.
- Limited heat gain from narrow south elevation and shaded west elevation.
- Existing residence in public right of way along California Avenue to be removed. Native landscape to be restored.
- Potential for the development of a significant public stair climb amenity thru the site connecting Harbor Avenue SW and California Way SW
- Easy access to pedestrian / bike pathways to and around Alki beaches
- Project retail and restaurants will enliven the pedestrian experience along Harbor Avenue SW.
- Good proximity to the water taxi dock and bus stop.
- Shoreline setback creates view and deck opportunities at the SE corner of the building

CONSTRAINTS

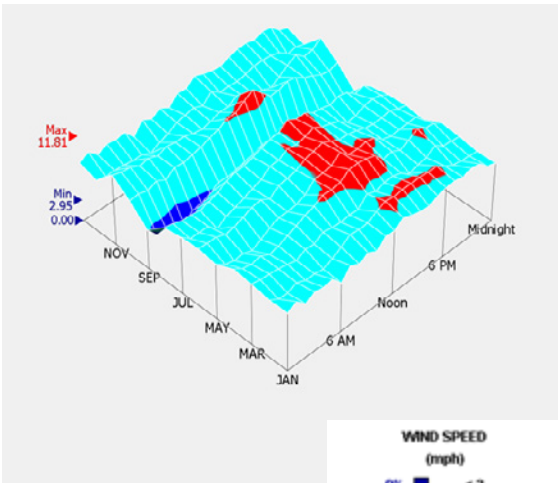
- Steep slope to the west limits buildable area.
- Parking at Don Armeni Boat Ramp detracts from views
- Shoreline setback limits maximizing allowable F.A.R.
- Harbor Avenue SW is busy thoroughfare that creates noise
- California Way SW is a busy street favoring vehicles over pedestrians.



TEMPERATURE



HUMIDITY



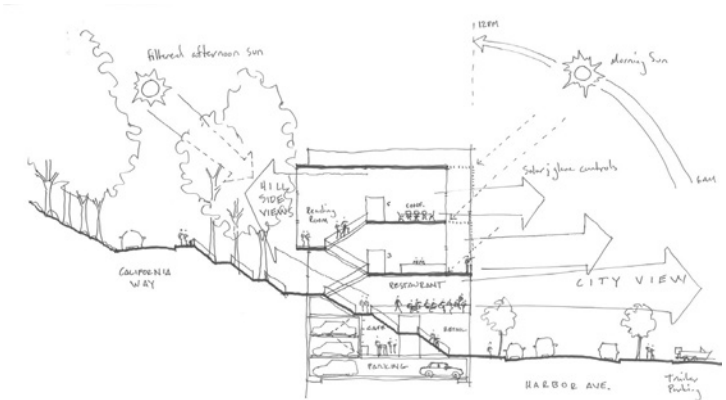
WIND SPEED

SEATTLE WEATHER DATA

- Afternoons, June thru October most favorable for natural ventilation
- Peak seasonal temperatures tend to occur in August late in the afternoon.
- Part of the reason for Seattle's moderate climate is because the highest humidity levels occur in the cooler months. The hotter months tend to have lower humidity.



SKYLINE AND HILLSIDE
Creating connections between the two, both for building occupants and passers-by, is a major focus of the design concept.



EARLY CONCEPT SKETCH



EXISTING BUILDINGS ON HARBOR AVENUE SW
The pedestrian experience along Harbor Avenue SW will be improved with the addition of retail storefronts, repaved sidewalk, and new street plantings.

CONTEXT AND SITE

CSI. NATURAL SYSTEMS AND SITE FEATURES

B. SUNLIGHT AND NATURAL VENTILATION

- The long north south orientation of the site and steeply sloping hillside encroachment from the west limit the buildable area to a long north south bar with eastern exposure.
- The project will utilize this eastern exposure to its benefit and harness early morning sun to warm the building in heating months. In cooling months internal blinds will be employed to reflect heat gain.
- The west façade of the building will benefit from the existing heavy deciduous tree canopy which will block the majority of late afternoon solar exposure in summer. In winter, with the leaves down the project will benefit from any afternoon sun.
- To take advantage of the project’s proximity to Elliott Bay the design recognizes the potential for natural through ventilation. All occupiable spaces in the building will have operable windows and large door openings on both the east and west facades to promote passive ventilation when appropriate.

C. TOPOGRAPHY

- The project is embracing the hillside and includes a publicly accessible stair-climb connecting California Way SW to Harbor Avenue SW through a large breezeway through the building. To take advantage of topography, a broad publicly accessible stair climbs up through the breezeway providing access to second floor commercial space.

D. PLANTS AND HABITAT

- Preserving existing trees and habitat on the undeveloped west portion of the property.
- In limited areas, previously disturbed by construction, plantings will be restored with native species.

E. WATER

- A two-story high water feature in the breezeway will be a continuous waterfall which will mute the sounds of street traffic and act as a natural northwest forest stream ambiance

CS2. URBAN PATTERN AND FORM

A. LOCATION IN THE CITY AND THE NEIGHBORHOOD

I. SENSE OF PLACE

- The design team early on recognized the importance of the lush, verdant hillside as a compliment to broad city skyline views. The design team believes that in order to establish a true sense of place on this site, one should be able to appreciate both the forest and city simultaneously. To that end, all office and residential uses have a dual exposure. All public spaces have a strong to connection to both hillside and skyline as well. The breezeway and stair-climb literally bring the hillside—and the public experience of the hillside—right through the building.
- The process of descending the stair and moving through the building with ever expanding skyline views provides a dramatic and memorable passage from the forest to the water’s edge.

- The design team wanted the project to be an experience not just an object.
- The owner wanted to welcome the neighbors, not exclude them.

2. ARCHITECTURAL PRESENCE

- The design team recognized the potential for a unique architectural presence on this site. The street is lined with buildings on one side only, with an undeniable orientation towards the city skyline. The city block does not exist in its typical form here: the neighborhood is more a string of pearls along the water’s edge than a gridded urban or suburban layout.
- The project strives to maintain a strong street edge and urban character on either side of the breezeway hill climb. Upper floor restaurant and office spaces are setback from the primary façade to allow for landscape screening and facade modulation.
- The project will also utilize high quality materials. Cast in place concrete walls with an architectural level of finish and aluminum storefront are proposed wherever the lower three floors of the project engage with the street edge. The upper level block of apartments will be clad in prefinished metal panel and aluminum window system.

B. ADJACENT SITES, STREETS, AND OPEN SPACES

I. SITE CHARACTERISTICS

- While the design team is not able to take advantage of the triangular shape of the site due to ECA steep slope regulations, we have been able to take advantage of the two hundred foot shoreline setback mandated by the Shoreline Master Plan. This setback applies to portions of buildings thirty feet or more above grade within the setback area. This setback pushes the upper four floors back approximately forty feet at the south property line and extends to the north approximately eighty feet. The triangular setback provides an opportunity for a large roof deck that takes advantage of the view and, when connected to the breezeway via a third floor setback, effectively splits the project massing into two corresponding yet offset bars. These bars remain flush to the property line while the rest of the building is set back.

2. CONNECTION TO THE STREET

- Harbor Avenue is a fairly high-speed arterial with one northbound and one southbound vehicular lane with bike ‘sharrows’ painted in both directions and parallel parking on both sides of the street. The Don Armeni Boat Ramp and Park are located east of Harbor Avenue and the typical sidewalk has been replaced with the ten foot wide Alki Trail bike and pedestrian path. A crosswalk is located just south of our site and the Dan Armeni parking lot where the park green space connects with the Alki Trail.
- While the Alki Trail and waterfront are considered by the design team to be attractive amenities, the boat ramp parking and high-speed nature of Harbor Drive are not. These two factors drove the desire to: one, lift the restaurant to the second floor, and two, provide the third floor landscaped setback for offices.
- The sidewalk in front of the project site was originally a six foot wide concrete surface separated from the property line by a two foot planting strip and the curb by a four foot planting strip. Subsequent to the fire and demolition of the ‘Embers’ nightclub in 2003, the sidewalk and supporting soils have slid into what was the basement of the building. For this reason a temporary asphalt sidewalk has been poured out at the face of curb and chain link fence built to keep pedestrians from falling into the site. The project proposes to

- restore the grade within the right of way and restore the sidewalk per SDOT standards in its intended location.
- While retail storefronts provide a strong connection to the street for the majority of the ground plane, the strongest connection to the street occurs where the breezeway breaks the building massing, bringing the hillside and pedestrian hill climb through to the sidewalk.
- There are numerous landscaped decks on multiple levels from which occupants can engage the street and the view.

3. CHARACTER OF OPEN SPACE

- We have two primary types of open space on the project. Public open space is within and adjacent to the breezeway hill climb. These spaces are heavily influenced by the character of the forested hillside and transition into restaurant outdoor open spaces which are very much oriented toward the skyline views. The second type of open space in the project are private residential and office amenity spaces. These areas are carefully screened and separated from the public areas with a combination of parapets, screen walls and landscaping.

C. RELATIONSHIP TO THE BLOCK

2. MID-BLOCK SITES

- The project draws inspiration and proportion from the long linear character of adjacent buildings but then reduces the scale and composes the resulting forms, or ‘bars,’ in a dynamic way.

D. HEIGHT, BULK, AND SCALE

1. EXISTING DEVELOPMENT AND ZONING

- Review of the height, bulk and scale of neighboring buildings reveals a real variety of scales, from numerous single family bungalows and beach shacks, to large apartment,condominium and multi-use buildings.
- The one unifying characteristic, due no doubt to the extraordinary views and proximity to water, is a ubiquity of decks.
- The hillside behind rises approximately two hundred feet, so zoning heights are well below the height necessary to impact views from the single-family zoned neighborhood above.

2. EXISTING SITE FEATURES

- The site is split into a relatively flat, lower rectangular area of approximately 230 feet in length and 62 feet in width (corresponding to the proposed footprint of the project) and an uplands portion of the site which is in a triangular configuration extending up the hill to the California Way SW right of way. The grades on the lower portion are at a relatively level elevation of 16 feet along the Harbor Avenue SW property line and up about one story to an elevation of approximately 24 feet against the hillside. The steeply sloping upland triangular portion of the site slopes at about a 1:1 slope to elevation 32 at the southwest corner and elevation 53 feet at the northwest corner.
- Due to limitations regarding building on the ECA steep slope portion of the site, the entirety of the massing has been located on the lower flat portion of the site. The heavily wooded hillside helps to screen the lowlands development from the residential above.
- The flat portion of the site has two single family bungalows at the north end of the site and two two-story commercial structures at the south end. Additionally, there is a single family nonconforming structure that is built about half into the upland portion of the site and half

into the California Way SW right of way. There are a small number of related retaining walls in disrepair. All structures will be demolished.

3. ZONE TRANSITIONS

- The project site is comprised of multiple parcels of land. The four northern parcels fall within a MR Multi-family zone and the five parcels to the south fall within a NC-2 65 zone. The most significant difference between zones is with regard to required yards. In the MR zone front, side and rear yards are required while the NC-2 zone does not. Allowable height in the MR zone is 60 feet but can be extended to 65 feet per 23.45.514.G.3, which makes it equal to the NC zone.
- The result is that, with the exception of allowable use and required yards, the zoning is effectively the same. Front yard exceptions are provided for the code and will be described further below.

4. MASSING CHOICES

- In the MR zone we are providing a 10 foot side yard setback (3 feet wider than the required 7 foot minimum below 42'-0"). We are requesting a departure from the provision that ‘no structures can be built in the required yard’ in order to provide enclosure of the garage access ramp. This will additionally allow us to structurally carry the upper floors and to provide a landscaped roof garden to visually screen the garage ramp and buffer the sounds vehicles of entering and exiting the garage. While the neighboring building to the north does have windows on the upper floors, the ground floor yard is utilized only for a driveway and garage door.

5. RESPECT FOR ADJACENT SITES

- We are not proposing any windows or balconies other than for bedroom uses on the north façade, and are proposing a landscaped roof planted to screen the garage ramp and access to the trash and recycling dumpster area.
- The proposed relationship to the south neighbor is simply a 30-foot high wall,approximately the existing condition, with the bulk of the height held to rear third of the property line.
- The proposed 30-foot high podium picks up on and carries the cornice line of the building to our south through the site.
- The rhythm of the storefront bays has been derived from the bay spacing on the neighbor to the south.

CS3. ARCHITECTURAL CHARACTER AND CONTEXT

A. EMPHASIZING POSITIVE NEIGHBORHOOD ATTRIBUTES

1. FITTING OLD AND NEW TOGETHER

- Existing structures on site—two flat roof commercial buildings to the south and three pitched roofs, at approximately 4:12 pitch, to the north.

2. CONTEMPORARY DESIGN

- The design team early on placed a priority on contemporary design that drew formal influence and a sense of scale from the context.

3. ESTABLISHED NEIGHBORHOODS

- The design team undertook a thorough analysis of the neighborhood context, including existing structures, uses, businesses, public transportation, zoning, and more. That



PUBLIC STAIR

A grand public stair with integrated seating will be a place for building passers-by to stop for a break, for patrons of the building’s restaurant to sit, and for building occupants and residents to get together.



EARLY BREEZEWAY CONCEPT SKETCH

A breezeway and public stair connecting California Way SW to Harbor Avenue SW will create pedestrian-friendly open spaces and allow for new views of both skyline and forested hillside.



FORMER ALKI TAVERN



HILLSIDE VIEWING PLATFORM

Along the stair which will ascend the hillside an enlarged landing or platform will be created where pedestrians can stop and take in new perspectives of the surrounding hillside and views back to the sound.

research (much of which is presented in this document) has played a significant role in shaping the proposed building and its strategy for integrating into the local community.

4. EVOLVING NEIGHBORHOODS

- The downtown Seattle waterfront is undergoing unparalleled change and reinvention. Seattle is poised to become one of the great waterfronts cities in the world. We see the West Seattle waterfront becoming an increasingly important component of the greater Seattle waterfront renaissance. To that end, the buildings there need to reflect the international stature that Seattle represents in the Pacific Rim of nations.

B. LOCAL HISTORY AND CULTURE

I. PLACEMAKING

- Careful research into the history of the site yielded very little beyond the popularity of the Alki Tavern.

PUBLIC LIFE

PLI. OPEN SPACE CONNECTIVITY

A. NETWORK OF OPEN SPACES

I. ENHANCING OPEN SPACE

- The design team came to focus on the need for a through block connection.
- By allowing pedestrians that want to walk from the top of West Seattle to Alki point to shortcut through the proposed breezeway and hill-climb, 1600 feet of this journey, or about 5 minutes walk time, are shaved from taking the long way around on the street.
- Better connection between Hamilton Viewpoint Park and the Alki Trail and Duwamish Head overlook pier.
- Just south of the Harbor Landing Condominiums can be found a well-worn footpath that pedestrians use to shortcut the last few hundred feet of California Way SW, but it is steep and slippery when wet.

2. ADDING TO PUBLIC LIFE

- The design team decided early on that we did not want to build an impenetrable fortress but rather a building that welcomed the public in, a building that was receptive and engaging, providing public uses such as retail, restaurants and coffee shops that would make the building an attractive place to be not only for the tenants and occupants of the building but the community as a whole.
- The breezeway hill climb adds to Seattle’s rich network of 85 outdoor public stairways that people seek out for adventure and exercise.
- This project strives to be a contributing member of the community.

B. WALKWAYS AND CONNECTIONS

I. PEDESTRIAN INFRASTRUCTURE

- See discussion above.

2. PEDESTRIAN VOLUMES

- We are proposing a 6 foot wide stair along the upper portion of the hill climb, equal to the sidewalk along California Way SW, and a much wider stair incorporated through the breezeway space with seating steps incorporated into it and circulation steps equal to the

width of the sidewalk on Harbor Avenue SW.

3. PEDESTRIAN AMENITIES

- We have carefully separated the service entry points of the building from the public pedestrian breezeway hill-climb. Office uses on the upper levels will require more deliveries. This entrance has been pushed to south end of the building so that service calls do not detract from the quality of the pedestrian environment. The vehicular entry has been separated from the breezeway by a coffee shop retail space. The trash and recycling dumpster path has been pushed as far from the breezeway as possible.
- The breezeway hill-climb will have carefully designed lighting to ensure safety and visibility at night, and a water feature to buffer road noise and careful landscape integration with the hillside forest.

PL2. WALKABILITY

A. ACCESSIBILITY

2. ACCESS CHALLENGES

- There are a number of worn footpaths along the hillside between the two roads, evidence of a desire to shortcut the long switchback of California Way SW.
- The public breezeway and hillside stair have the potential to create a major pedestrian-friendly through-block connection between California Way SW and Harbor Avenue SW.
- A public passage through the building and up the hillside would not only provide ample seating, covered areas, and unique views of hillside and skyline, but would also contribute to Seattle’s growing network of grand, publicly accessible waterfront stairs.

B. SAFETY AND SECURITY

I. EYES ON THE STREET

- The design team decided early on that the building would have a public-oriented street presence. Ground level uses include a large retail establishment, a cafe, and a restaurant. The entrances to these uses are all adjacent to the breezeway, while the office entrance and parking garage door are pushed to the north and south ends of the site.
- Aggregating most of the commercial uses and public entries at the breezeway creates a public zone in that area and enhances a sense of shared space that is watched over by the commercial tenants and their patrons.

2. LIGHTING FOR SAFETY

- The breezeway and public hillside stair will be illuminated to designate the entry into the building, to allow for safe egress in case of fire, and to create a safe environment for pedestrians.

3. STREET LEVEL TRANSPARENCY

- Large glass storefronts along Harbor Avenue SW wrap into the breezeway, providing views into retail and restaurant spaces while also creating a safe environment by allowing for views in the opposite direction.

C. WEATHER PROTECTION

I. LOCATIONS AND COVERAGE

- Covered entries are provided at all commercial uses as well as at apartment and office lobbies.
- The breezeway is unconditioned and open to the elements, but nevertheless provides a

sizable amount of covered space that is freely accessible by the public.

DESIGN CONCEPT

DC1. PROJECT USES AND ACTIVITIES

A. ARRANGEMENT OF INTERIOR USES

I. VISIBILITY

- The program components have been arranged throughout the building so that those dependent on public access are located along the sidewalk or vertically distributed along the breezeway. Along the Harbor Avenue sidewalk we have commercial retail spaces. At the breezeway, the south half remains at grade and serves as the apartment lobby and elevator access. The north half of the breezeway is a monumental stair that provides access to the restaurant space at level two and to the hill climb stair which leads up to California way beyond. The monumental stair is split evenly between circulation stair and seating steps to promote lingering and help define the breezeway as ‘a place’ and outdoor room to serve the interior adjacent spaces.
- North of the breezeway we have located another small retail storefront that is envisioned as a coffee shop. Breezeway seating steps wrap around become a resource for the coffee patrons. The stair was located on the north side of the breezeway to receive the maximum amount of sun.

2. GATHERING PLACES

- As discussed above, the breezeway serves as the primary gathering space for the project and offers a great alternative shortcut to the California Way SW switchback path.
- All commercial spaces in the building will benefit from close adjacency to the Alki trail and the hikers, bikers, runners, strollers and walkers; the Don Armeni Boat ramp and associated fishing and boating traffic; the water taxi and the divers that frequent Seacrest Park. We see the project becoming an integral component of the outdoor, active lifestyle associated with the West Seattle waterfront offering support and services.

3. FLEXIBILITY

- The two overlying zones offer challenges with regard to commercial in a residential zone. A key goal of the applicant owner has been a flexible core and shell building that can be easily modified over time to accommodate different uses.

B. VEHICULAR ACCESS AND CIRCULATION

I. ACCESS LOCATION AND DESIGN

- Access to the parking garage from California Way was considered but eliminated due to the significant grade change. The next best location for vehicular entry was north of the breezeway. This placement would ensure that pedestrians descending from California Way through the breezeway hill climb would generally be heading south to the crosswalk just south of the building and connecting to the north end of Seacrest Park.
- We will undoubtedly have pedestrians heading north as well, though it is anticipated that the majority will head south.

2. FACILITIES FOR ALTERNATIVE TRANSPORTATION

- While shared vehicles were considered none are anticipated at this time. Charging stations could be added to the project in the future. Ample bicycle parking will be provided

in the below grade parking garage.

C. PARKING AND SERVICE USES

I. BELOW-GRADE PARKING

- All parking is located below grade.

2. VISUAL IMPACTS

- We have minimized the visual impact of the parking garage entry as much as possible.
- We are requesting a side yard setback departure in order to better screen views into the garage from neighbors to the north with a combination of structure and landscaped planters.
- The structure and planters will also serve to mitigate any noise and headlight glare associated with the garage and ramp.

4. SERVICE USES

- No loading docks are required per code.
- Trash and recycling dumpsters will be wheeled out through the parking garage entry to Harbor Avenue for collection. A solid panel on sliding barn door hardware will seal the dumpster area off from view when not in use. The door is located within the entry to the parking garage, so as not to be visible on the main street façade.

DC2. ARCHITECTURAL CONCEPT

A. MASSING

I. SITE CHARACTERISTICS AND USES

- Priority was given to the public hill-climb and breezeway connection through the site. The apartment block was therefore lifted up as high as possible to allow street and breezeway-related functions to take advantage of adjacencies with one another. The apartments furthermore,benefit from separation from street noise, boat launch and other commercial activities.
- Retail storefronts obviously want to be on the sidewalk and locating manufacturing and other office uses directly above provide the opportunity for internal connections if so desired in the future.

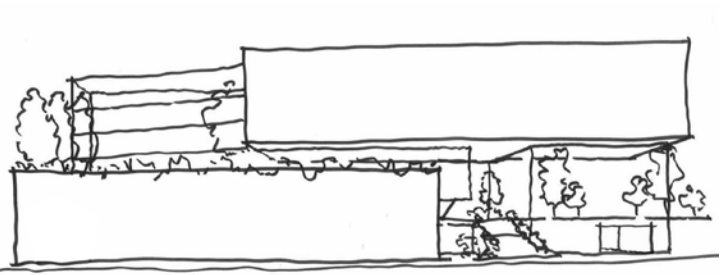
2. REDUCING PERCEIVED MASS

- The overall mass of the project has been reduced by breaking the overall volume into two dominant masses with a smaller third mass encompassing the parking entry and coffee shop. All other areas are setback seven feet or more.
- The third floor roof deck on the south end further accentuates this recess by opening to the sky while the breezeway in the center punches all the way through the volume opening to the earth and bringing the hill-climb and landscape elements through.

B. ARCHITECTURAL AND FAÇADE COMPOSITION

I. FAÇADE COMPOSITION

- As described above, the design team has worked to maintain a well-proportioned façade on the waterside of the project. But no less important are the west hillside façade and end elevations. The hillside elevation will be mostly screened when the deciduous trees are leafed out but will be mostly visible in winter. Cantilevered exterior circulation balconies stretch most of the length of the façade but plans were adjusted to keep the balconies just short of full length. Compositionally this allows the strong verticals to bookend the façade.



EARLY MASSING CONCEPT SKETCH

The building is composed of two primary volumes or “bars”. A residential bar above and to the north and a commercial bar below and to the south. The design of each bar picks up on the rhythm of the structural bay spacing of their respective neighbors.



RESTAURANT

The restaurant will be woody, warm and inviting, with views out to the water and accessible from the breezeway.



PROTOTYPING

The building will house offices and garment prototyping studios.



BUILDING TRANSPARENCY
The breezeway (along with other architectural moments throughout the building) can frame views out over the water by creating transparency at certain key moments.

- Keeping wind-driven rain from the balconies is accomplished with a perforated corrugated metal screen wall with a free open area of approximately 50%. This translucency will provide a veil-like quality. During daylight hours, the screen will appear somewhat opaque but at night, when lit from within, will glow and appear translucent. The corrugated profile brings a texture and grain to the screen wall breaking down the overall scale.
- The south façade of the project will actually be fairly visible when traveling north on California Way and has an elegant, slender profile due to the shoreline setback at the third floor roof terrace.
 - The north façade is the most restrained due to the fact it will be the least visible due to the angle of California Way and height and bulk of the condominium building to the north Windows further break down the scale and provide relief at the apartments. No windows are proposed into commercial spaces except at the NE corner of the restaurant. Landscaping is proposed to screen the parking garage access ramp from the upper levels of the building to the north and enliven the north elevation at level two.

2. BLANK WALLS

- As described above, the design team has worked to minimize the occurrence of blank walls.

C. SECONDARY ARCHITECTURAL FEATURES

I. VISUAL DEPTH AND INTEREST

- As described above, the breezeway, third floor roof deck and recessed portions of the façade are all working toward the goal of visual depth and interest.
- Additionally, surface textures are planned. The cast-in-place concrete walls will have carefully coordinated form tie holes and form lines that will add scale and detail to these portions of the façade. The storefront glazing will include exposed pressure plates and fasteners which add a much needed layer of interest and detail to the typical two-inch aluminum storefront. We will also be exploring the idea of accentuating the operable windows within the storefront with complimentary colors or finishes to the base storefront.
- A bridge at level three between the office area and the restaurant mezzanine serves to further activate the breezeway.
- Warm-colored materials such as wood, or finely detailed finish materials, will be included in key places to add visual interest and draw visitors thru the public areas. One potential location is the main entry wall at the second level restaurant.

3. FIT WITH NEIGHBORING BUILDINGS

- As described on the previous page, the horizontal bars are derived from the proportions of building to the north and south. See concept sketch on previous page.

DC3. OPEN SPACE CONCEPT

A. BUILDING-OPEN SPACE RELATIONSHIP

I. INTERIOR/EXTERIOR FIT

- One of our highest priorities on this project has been the strength of the interior-exterior relationship and an embrace of the site.

B. OPEN SPACE USES AND ACTIVITIES

I. MEETING USER NEEDS

- We are providing a broad stair, not only for circulation, but with integral seating steps so

that the breezeway becomes a place for people to gather.

- Along the hill-climb stair, landings and a view platform provide sequential views through the breezeway to the city skyline and Elliott Bay.
- Roof terraces will landscaped and provide seating and weather protection in certain areas.

2. MATCHING USES TO CONDITIONS

- The main stair in the breezeway was located on the north wall to maximize the amount of direct sun it will receive.

3. CONNECTIONS TO OTHER OPEN SPACE

- The hill climb stair and breezeway connect the waterfront parks and trails with the parks, trails and open space on top of the hill.
- The outdoor restaurant seating at level 2 was located adjacent to the breezeway to directly connect with the café and outdoor seating on level 1.

4. MULTIFAMILY OPEN SPACE

- The multifamily open space at the roof terrace will be located adjacent to an enclosed pavilion for year-round uses. A portion of the outdoor space will be partially covered for three season use and the remaining portion left uncovered to be used as weather permits. Planting boxes will provide residents with the opportunity to garden, BBQs and outdoor furniture provide opportunities to gather and relax.

C. DESIGN

2. AMENITIES AND FEATURES

- There will be a vertical water feature in the breezeway to help mitigate traffic sounds.

3. SUPPORT NATURAL AREAS

- The hill-climb and breezeway were envisioned so that building occupants, as well pedestrians, could better experience the natural hillside areas without harming the sensitive slope.

A. BUILDING MATERIALS

I. EXTERIOR FINISH MATERIALS

- Exterior materials are envisioned to be durable, authentic and attractive--cast-in-place concrete where the buildings comes out of the earth, aluminum storefront windows, metal panel siding, wood siding at focal walls, and varied paving patterns at street level walkways and the vehicle driveway.

2. AVOIDING GLARE

- We are requesting a side yard departure so that we can shield the adjacent multifamily building to the north from the glare from vehicles entering and exiting the garage.

SITE PANORAMAS



VIEW FROM HILLTOP RESIDENCE
This is view 3 on the map on page 11.



VIEW FROM HILLTOP RESIDENCE
This is view 4 on the map on page 11.

CALIFORNIA WAY SW



VIEW LOOKING EAST TOWARDS PROJECT SITE

PROJECT SITE



VIEW LOOKING WEST AWAY FROM PROJECT SITE

PROJECT SITE

HARBOR AVENUE SW



VIEW LOOKING WEST TOWARDS PROJECT SITE

PROJECT SITE



VIEW LOOKING EAST AWAY FROM PROJECT SITE

PROJECT SITE



NC2-65 ZONE
FAR Allowed = 4.75
Option A FAR = 2.8

MR ZONE
FAR Allowed = 3.2
Option A FAR = 2.5

Parking Required = 18
Parking Provided = 20

AREA
55,014 GSF

OPTION A

- PROS**
- Complies with land use code, no departures required.
 - Creates open space at southeast corner of the site.

CONS
PEDESTRIAN EXPERIENCE

- Building is separated from California Way SW by the steep slope. No pedestrian interface along this western edge of the site.
- The connection to the street is poor where the ground floor is setback outside the Shoreline District. Approximately 70' of street frontage has no opportunity to engage pedestrians.
- Building development outside of the Shoreline District to maximize the buildable envelope, restricts amenity development in the southeast open space.

SCALE / ARTICULATION

- Blank facades from two levels of parking above grade on the north end of the site.
- Development outside the Shoreline District does not necessitate a view corridor. the building is continuous from the north setback to the south property line.

FINANCIAL

- Skin to volume ratio is very high
- Odd building shape required to maximize development
- Actual FAR is far below the allowable FAR
- Providing adequate parking on narrow site requires car elevator

NOTES

- Building footprint is inside ECA line of previous development (refer to ground level floor plan). No ECA variance required.



NC2-65 ZONE
FAR Allowed = 4.75
Option B FAR = 3.4

MR ZONE
FAR Allowed = 3.2
Option B FAR = 2.9

Parking Required = 51
Parking Provided = 51

AREA
94,125 GSF

OPTION B

- PROS**
- Complies with land use code, no departures required.
 - Division of office and residential uses are clearly defined along zoning lines.
 - Residential front setback creates wider sidewalk or area for increased plantings

CONS
PEDESTRIAN EXPERIENCE

- Building is separated from California Way SW by the steep slope. No pedestrian interface along this western edge of the site.
- Two curb cuts / more driveway area fo navigate as a pedestrian.
- Fewer opportunities for pedestrian amenities than preferred Option D.

SCALE / ARTICULATION

- More monolithic massing at the northern half of the site.
- Strict application of commercial / residential zoning provides less visual interest.

SAFETY

- Larger commercial component means fewer eyes on the street after hours.

NEIGHBORS

- Parking garage ramp is exposed to residential neighbors to the north.
- Code compliant view corridor exposes more low roof to view from California Way SW.

FINANCIAL

- Two levels of below grade parking required.

NOTES

- Building footprint is outside ECA line of previous development (refer to ground level floor plan). Requires ECA Variance – up to 30% steep slope encroachment.



NC2-65 ZONE
FAR Allowed = 4.75
Option C FAR = 3.5

MR ZONE
FAR Allowed = 3.2
Option C FAR = 2.9

Parking Required = 39
Parking Provided = 39

AREA
81,428 GSF

OPTION C

PROS

- Complies with land use code, no departures required.
- Courtyard space at pedestrian level in MR zone.
- Improved facade modulation over Option B.
- Division of office and residential uses are clearly defined along zoning lines.
- Enhanced sense of entry in vicinity of the front courtyard.

CONS

PEDESTRIAN EXPERIENCE

- Building is separated from California Way SW by the steep slope. No pedestrian interface along this western edge of the site.
- Fewer opportunities for pedestrian amenities than preferred Option D.
- Thirty-two foot wide garage entrance that incorporates the required site triangle is less pedestrian friendly.

SCALE / ARTICULATION

- Lacks smaller scale facade articulation.
- Strict application of commercial / residential zoning provides less visual interest.
- Six story high courtyard is not pedestrian scale.

SAFETY

- Larger commercial component means fewer eyes on the street after hours.

NEIGHBORS

- Code compliant view corridor exposes more low roof to view from California Way SW.

FINANCIAL

- Courtyard impacts MR zone FAR

NOTES

- Building footprint is outside ECA line of previous development (refer to ground level floor plan). Requires ECA Variance – up to 30% steep slope encroachment.



NC2-65 ZONE
FAR Allowed = 4.75
Option D FAR = 3.3

MR ZONE
FAR Allowed = 3.2
Option D FAR = 2.8

Parking Required = 41
Parking Provided = 41

AREA
79,250 GSF

OPTION D (PREFERRED)

PROS

- Lower FAR and less perceived mass due to more articulation of facade
- Pedestrian-scaled commercial uses at ground level.
- Publicly accessible breezeway through building provides connection between Harbor Avenue SW & California Way SW and sequential views to the water and city beyond
- Monumental public stair and water feature create a series of outdoor spaces which invite pedestrians to pass thru and activate the area and to visit second floor commercial uses.
- Parking garage vehicular ramp is screened with a wall and densely landscaped roof that blocks headlight glare and view of expansive paving.
- Residential and commercial uses are interlaced across the zoning boundary adding visual interest to the facade.
- Garage entrance located at north side of property away from crosswalk near south property line.

CONS

REQUIRED DEPARTURES:

- Front and Side Yard Setbacks
- Sight Triangle
- Vehicle Backing Distance

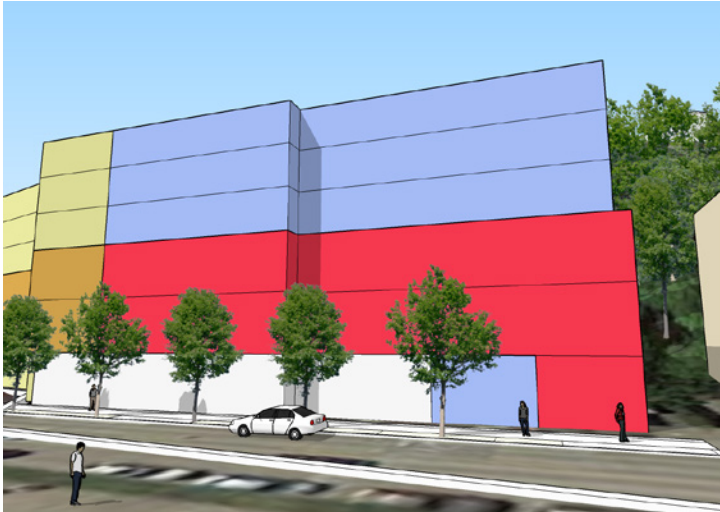
FINANCIAL:

- Breezeway impacts achievable FAR

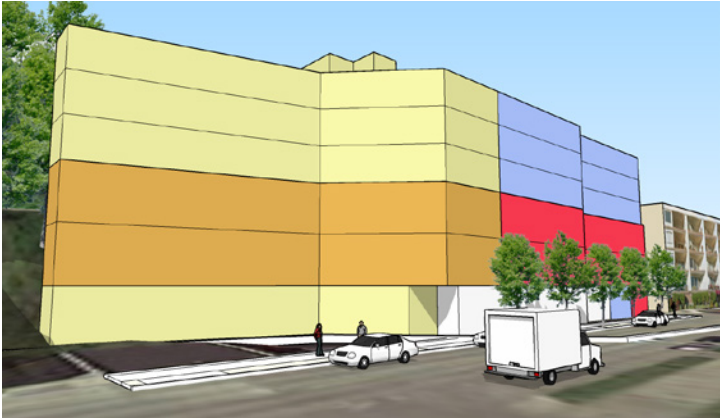
NOTES

- Building footprint is outside ECA line of previous development (refer to ground level floor plan). Requires ECA Variance – up to 30% steep slope encroachment.
- Potentially requires Director's Waiver for breezeway to meet portion of view corridor requirement.
- Requires SDOT approval for stair in ROW

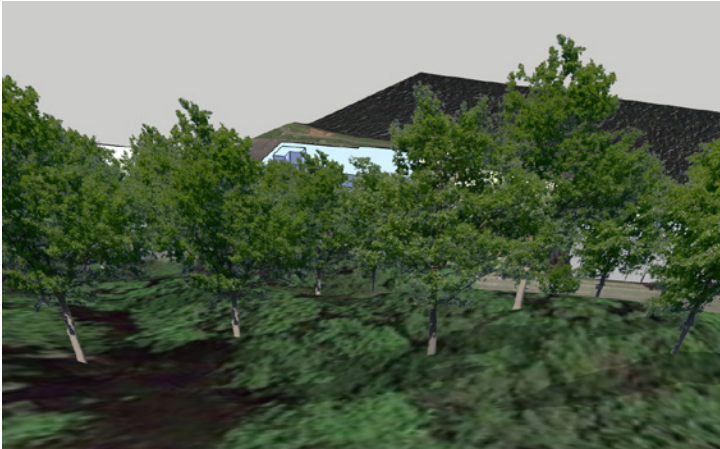
OPTION A



1 NORTHEAST CORNER, MR ZONE FROM ALKI TRAIL



2 SOUTHEAST CORNER OF BUILDING FROM HARBOR AVE



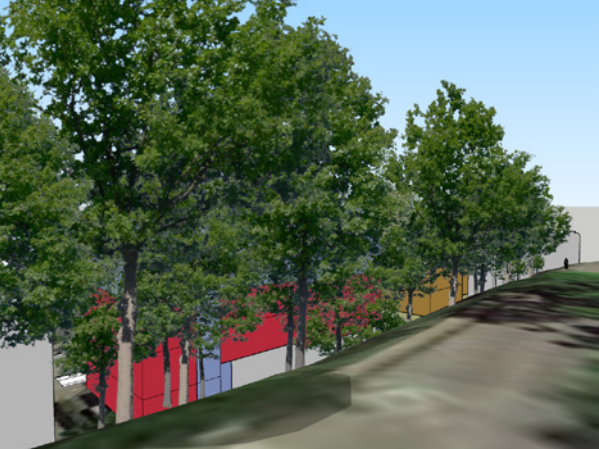
3 BUILDING FROM RESIDENCES ON PALM AVE SW



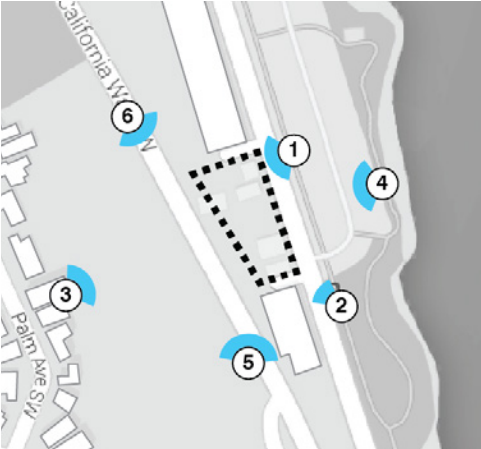
4 EAST ELEVATION FROM PARKING LOT ACROSS STREET

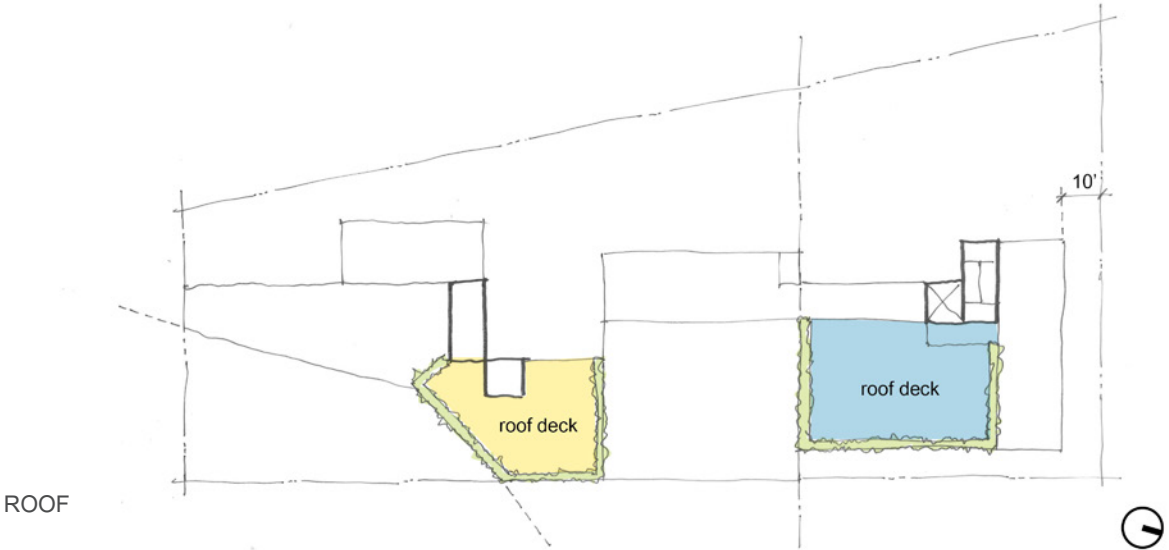
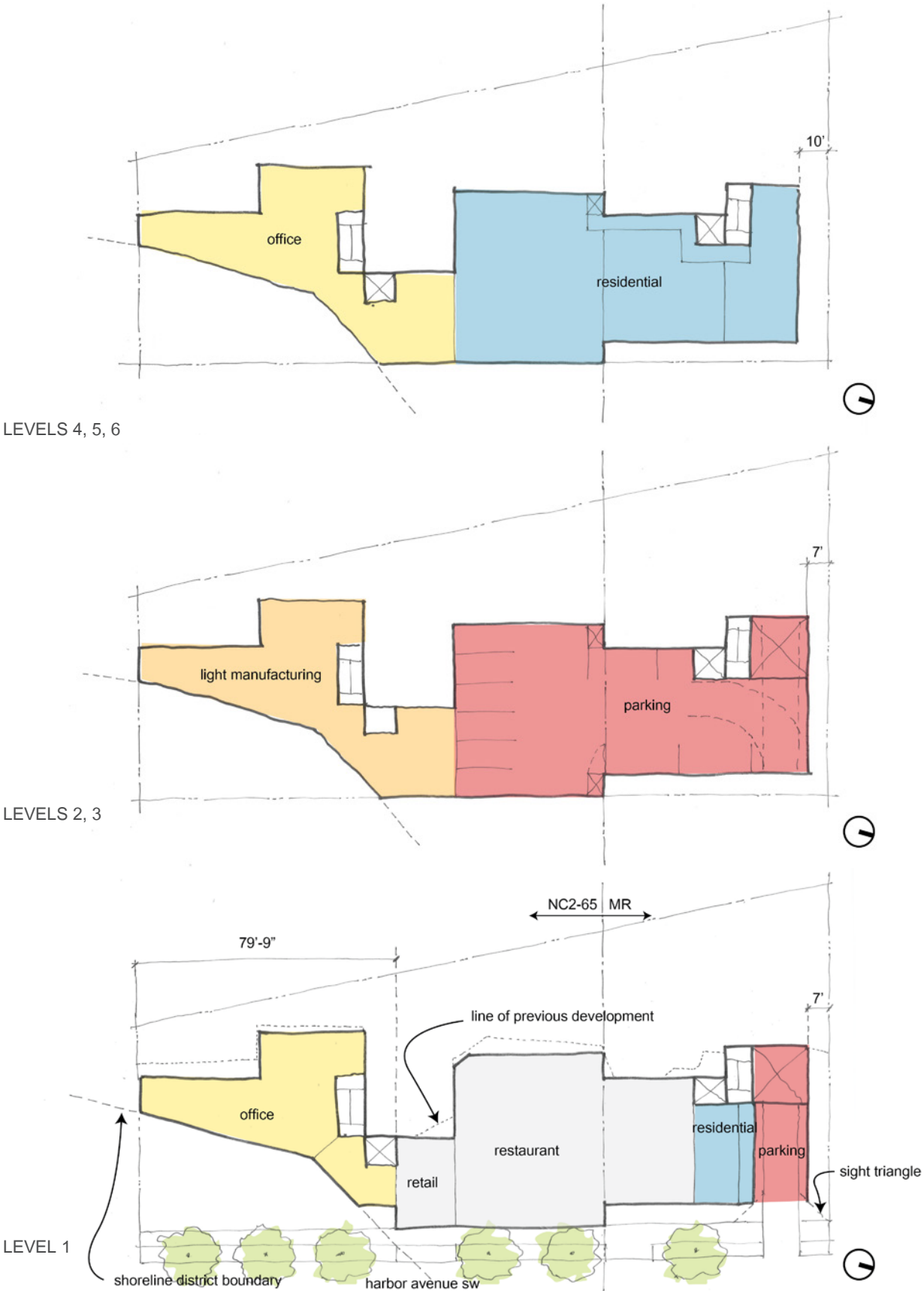


5 FROM SOUTHWEST ALONG CALIFORNIA WAY



6 FROM NORTHWEST ALONG CALIFORNIA WAY





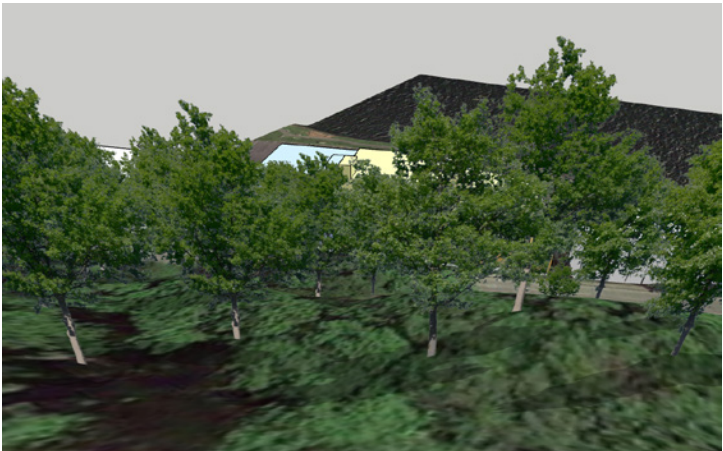
OPTION B



1 NORTHEAST CORNER, MR ZONE FROM ALKI TRAIL



2 SOUTHEAST CORNER OF BUILDING FROM HARBOR AVE



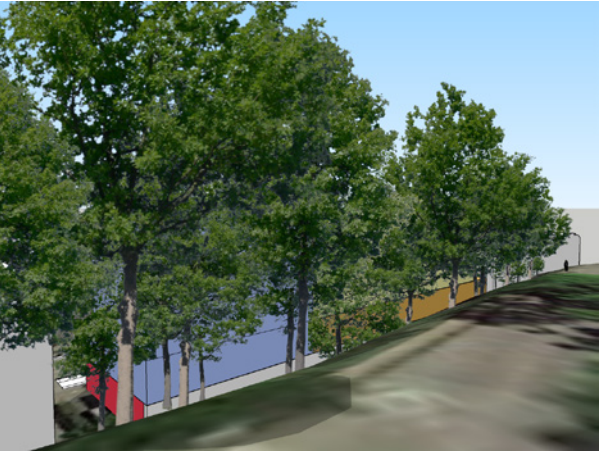
3 BUILDING FROM RESIDENCES ON PALM AVE SW



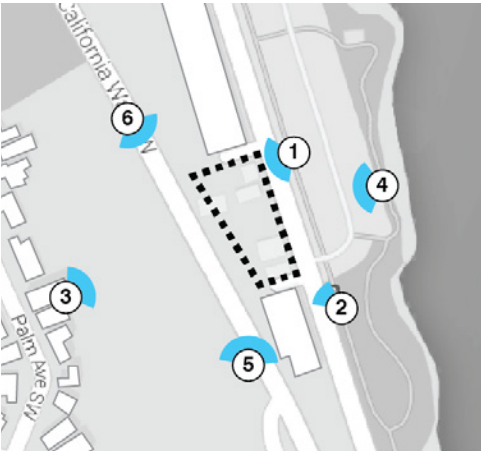
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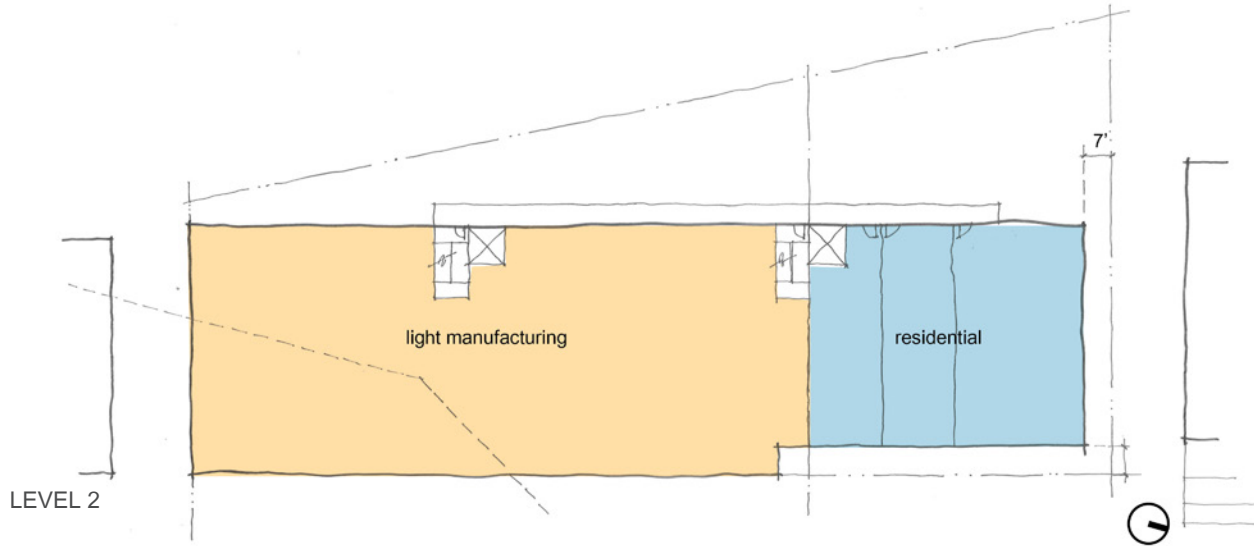
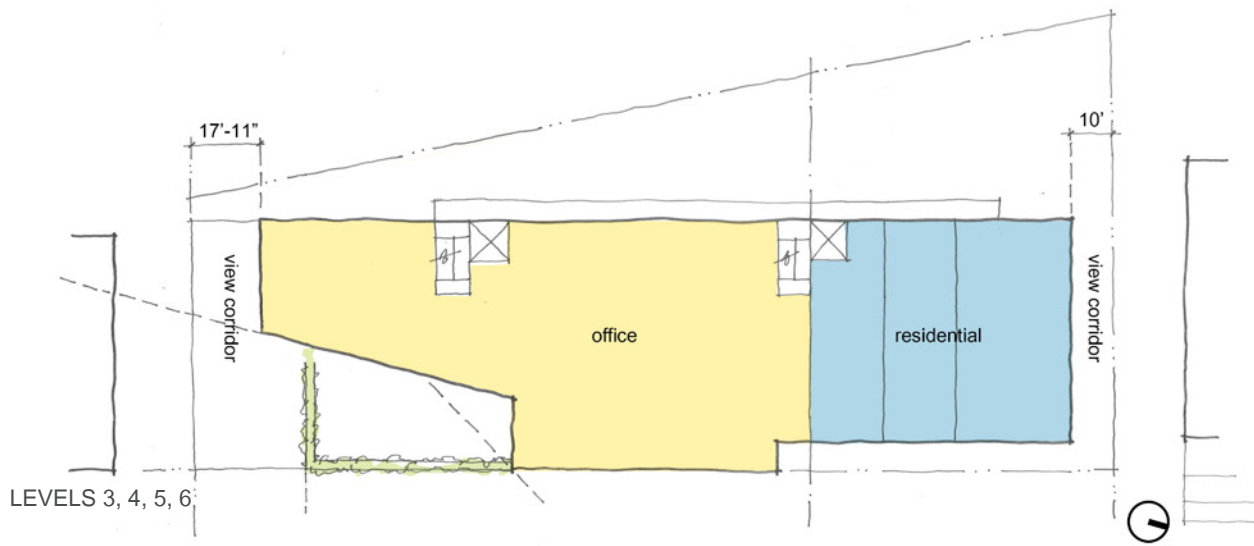
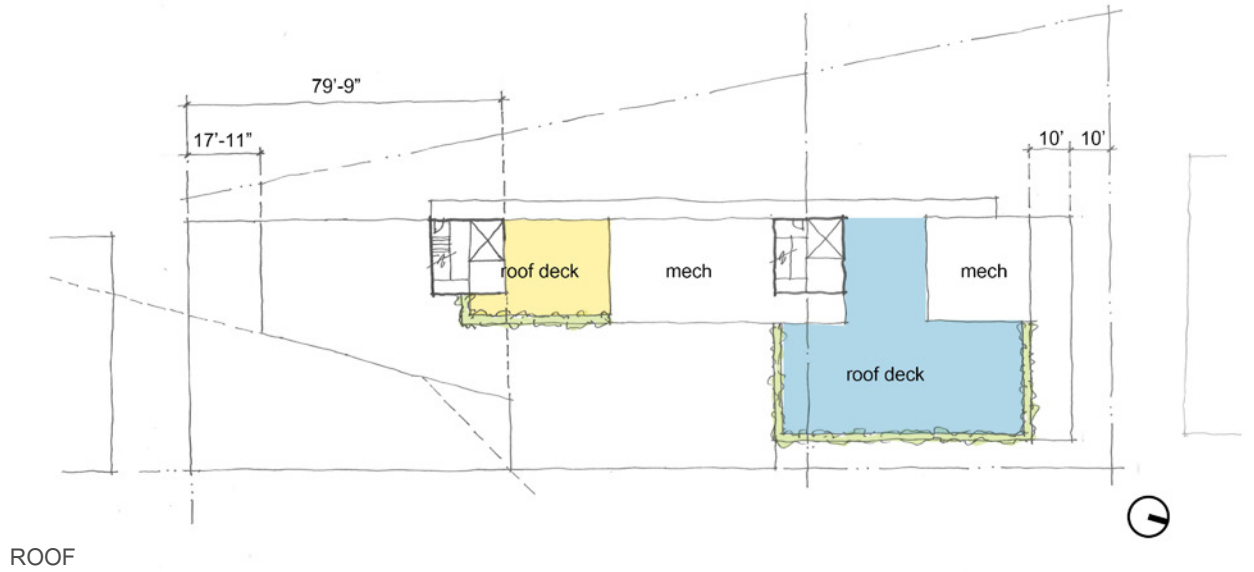
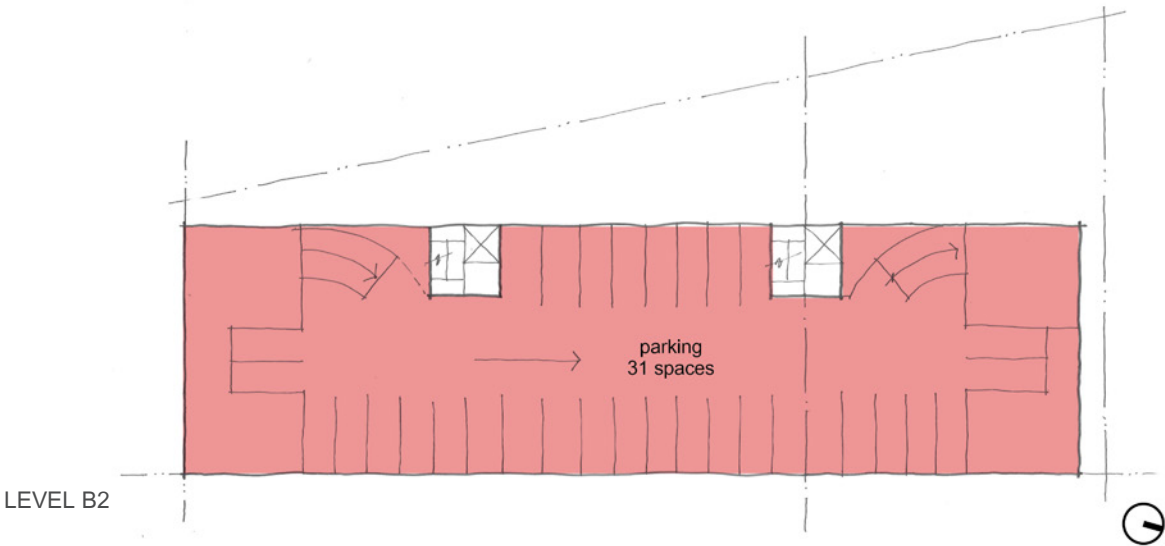
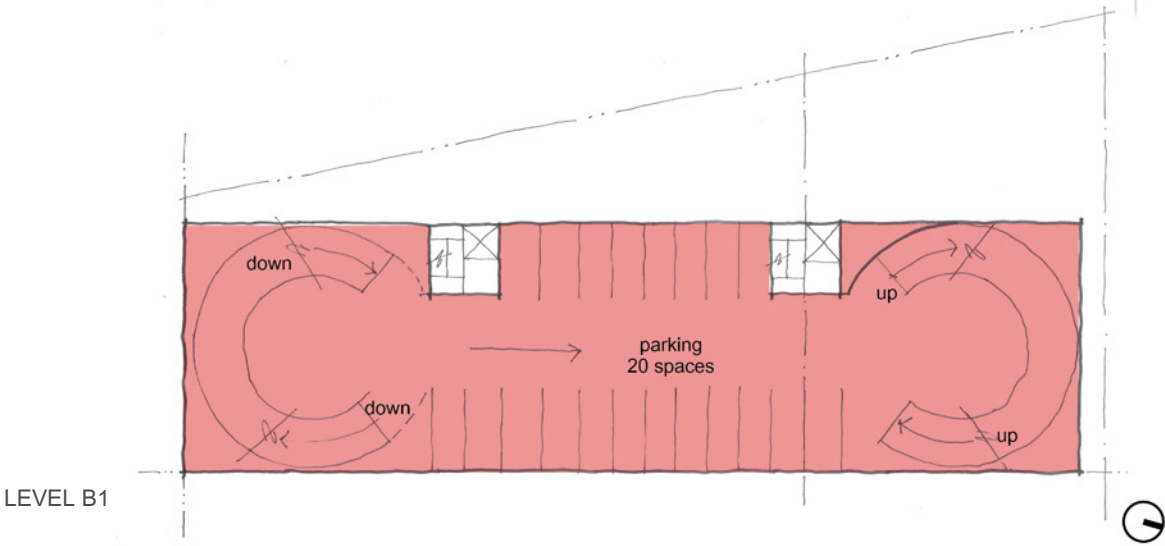
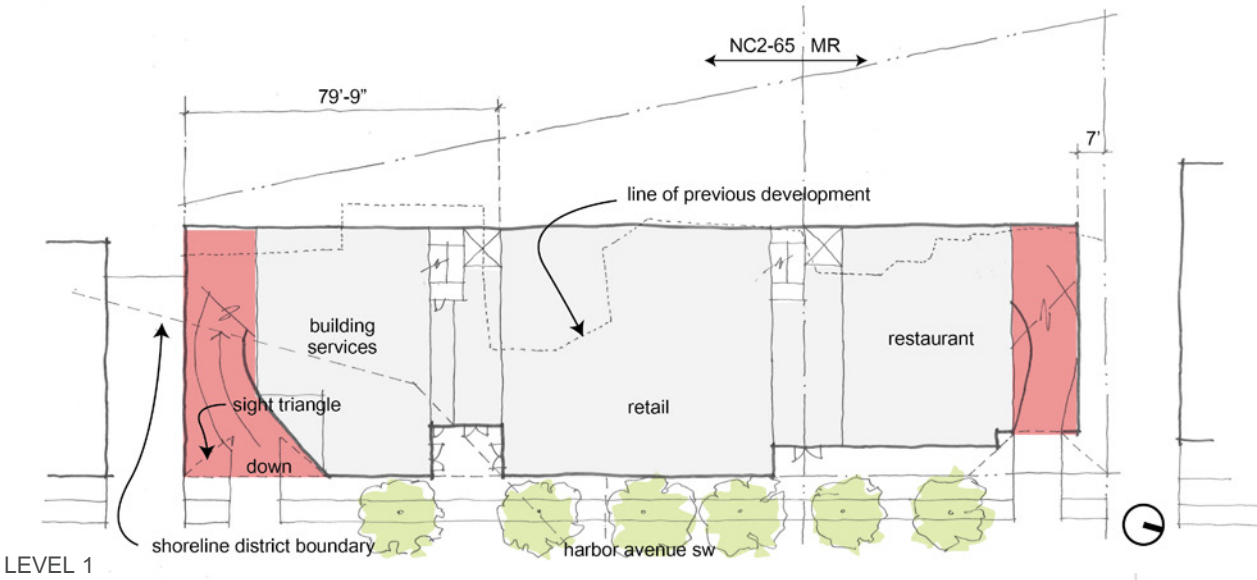


5 FROM SOUTHWEST ALONG CALIFORNIA WAY



6 FROM NORTHWEST ALONG CALIFORNIA WAY





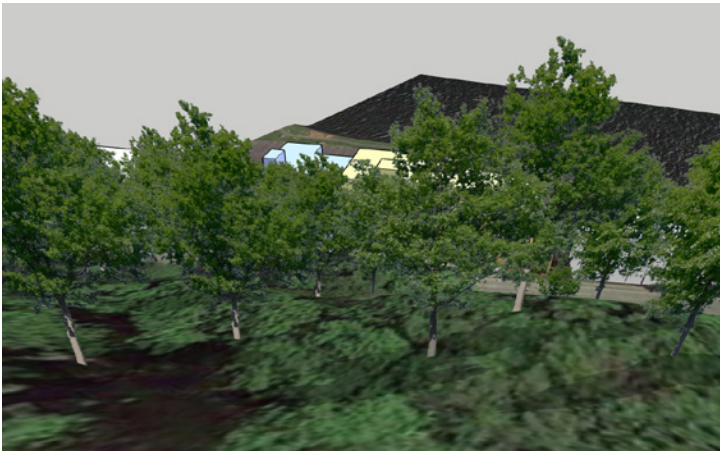
OPTION C



1 NORTHEAST CORNER, MR ZONE FROM ALKI TRAIL



2 SOUTHEAST CORNER OF BUILDING FROM HARBOR AVE



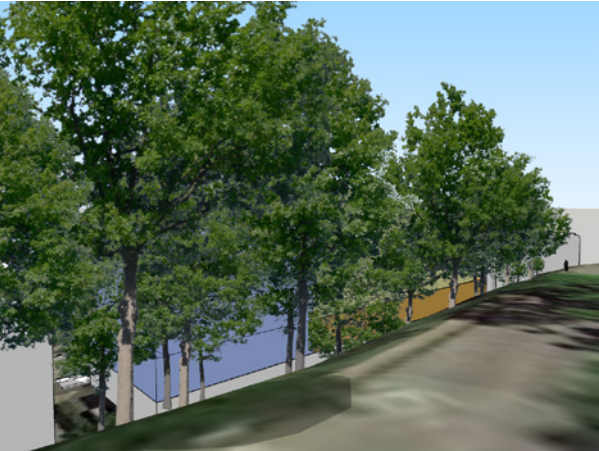
3 BUILDING FROM RESIDENCES ON PALM AVE SW



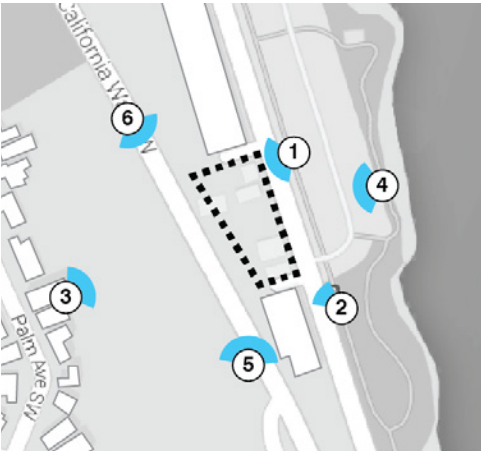
4 EAST ELEVATION FROM PARKING LOT ACROSS STREET

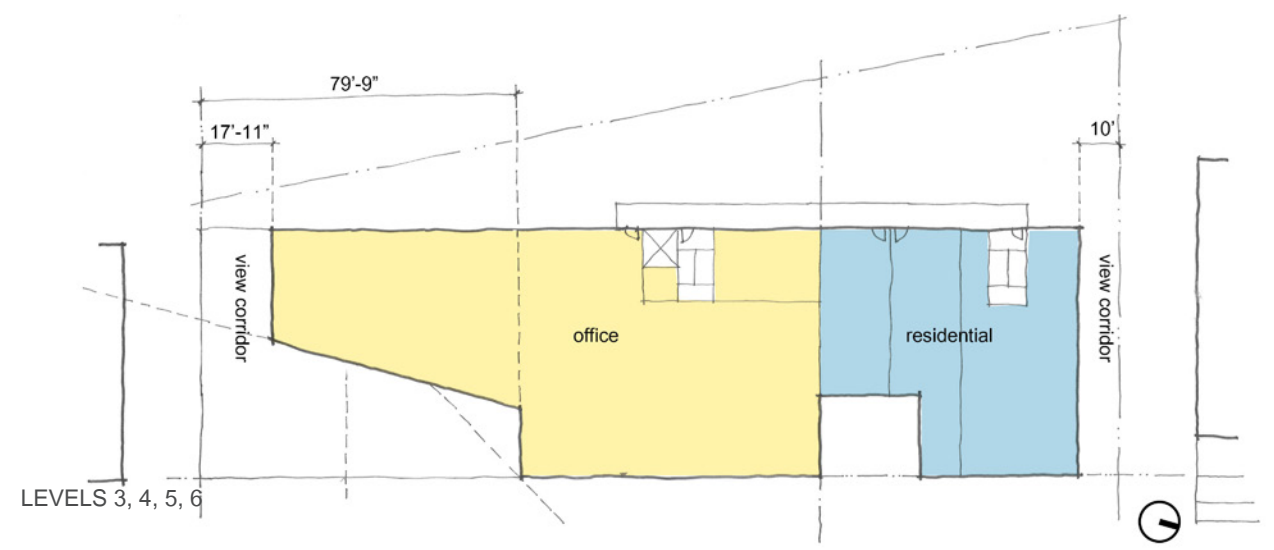
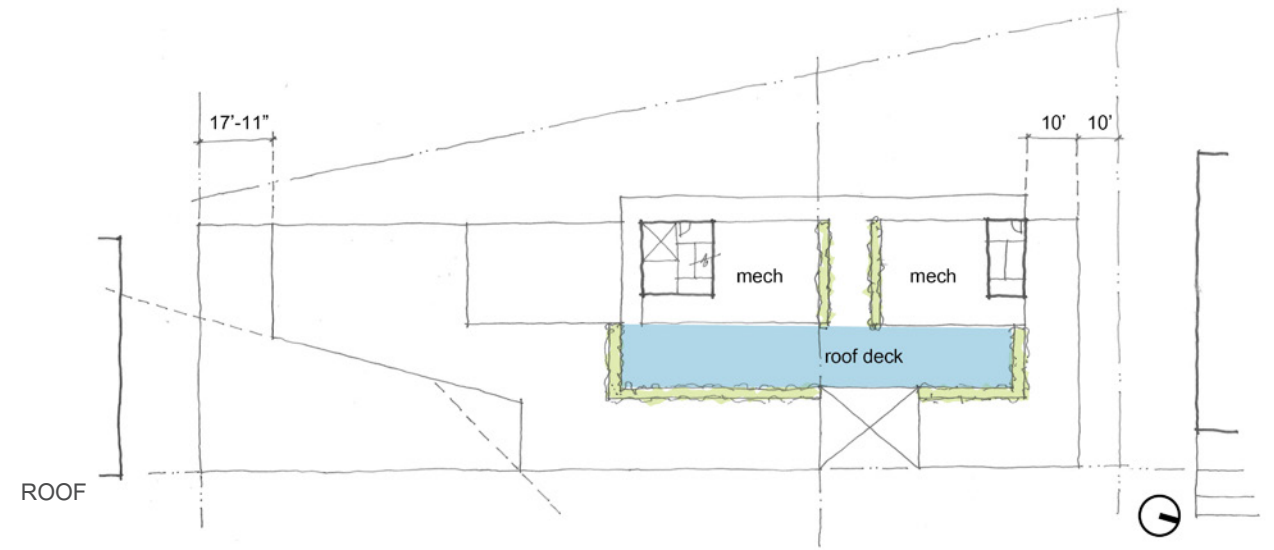
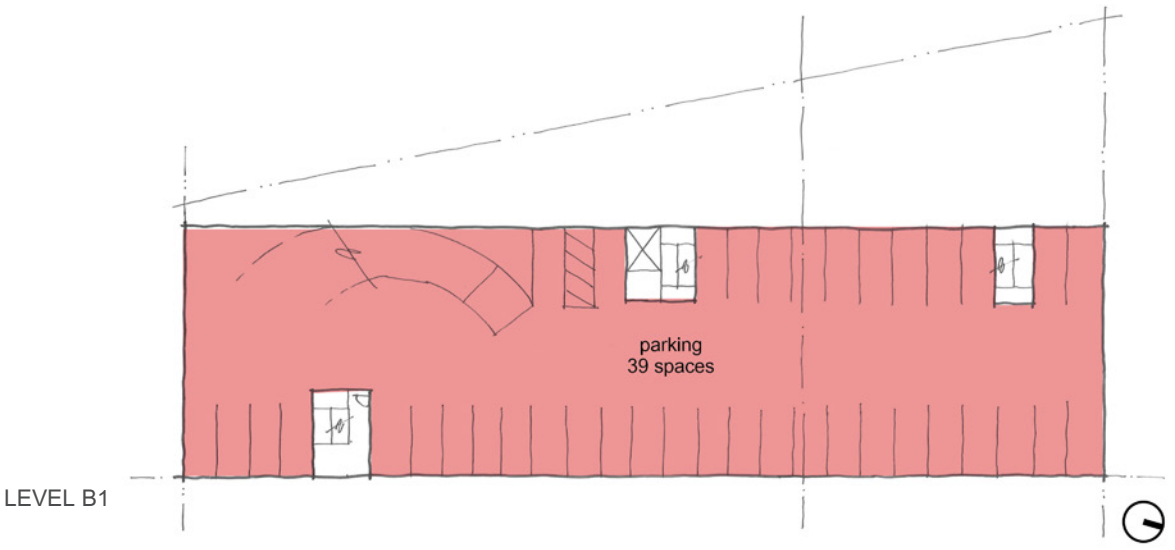
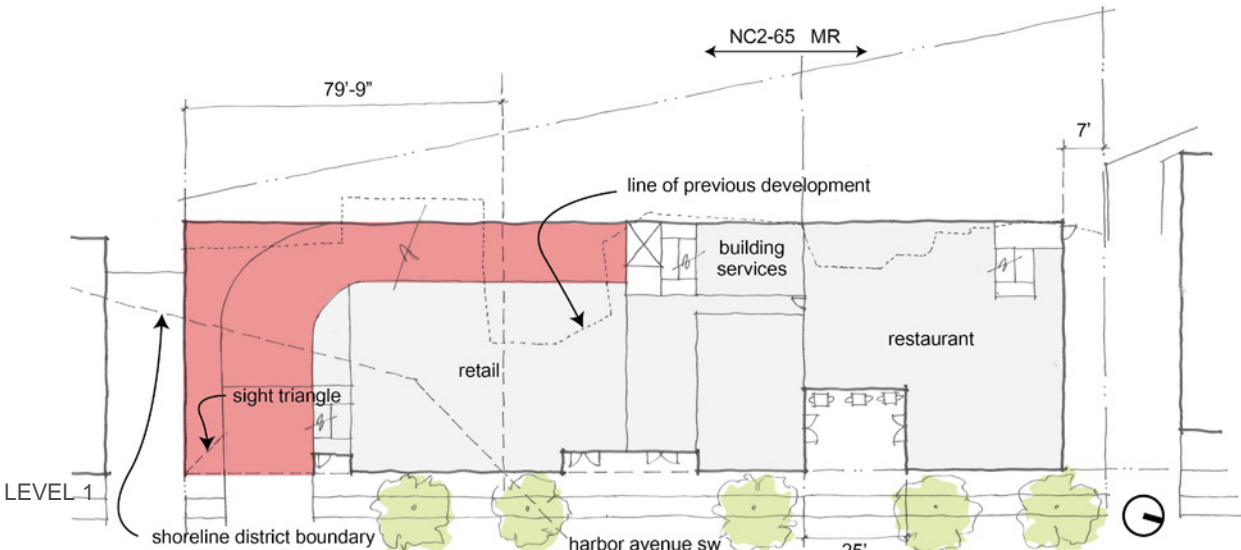
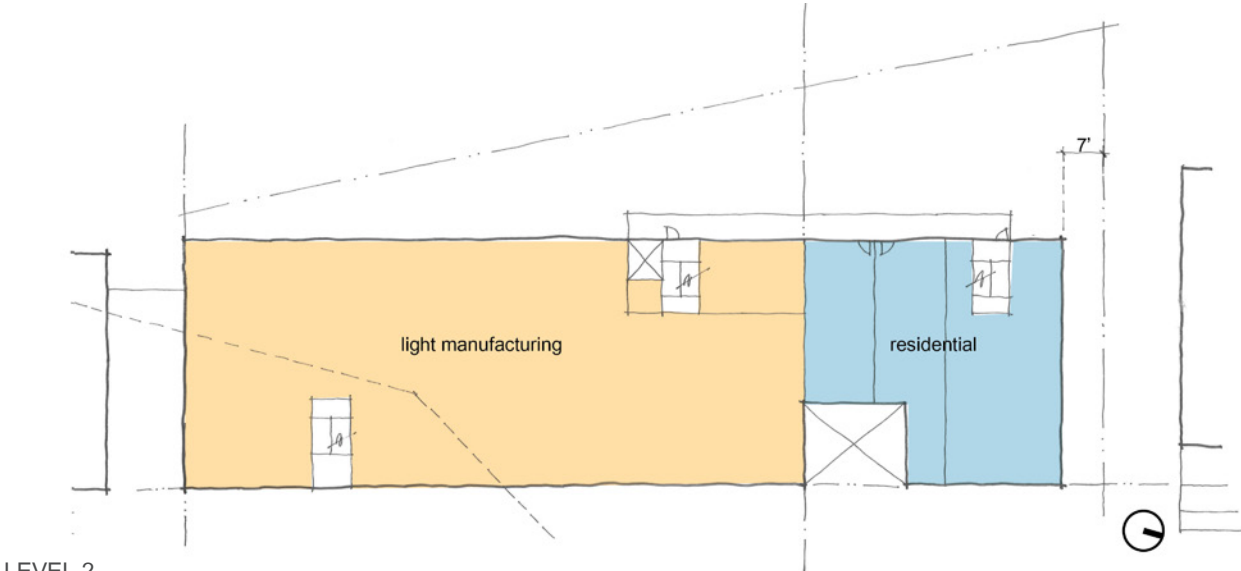


5 FROM SOUTHWEST ALONG CALIFORNIA WAY



6 FROM NORTHWEST ALONG CALIFORNIA WAY





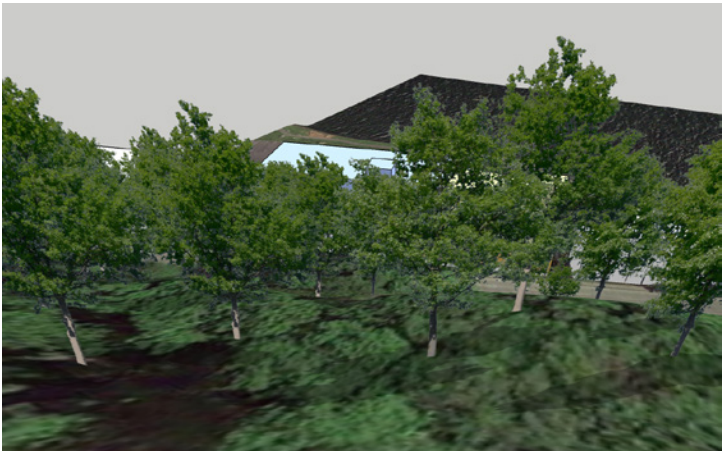
OPTION D (PREFERRED)



1 NORTHEAST CORNER, MR ZONE FROM ALKI TRAIL



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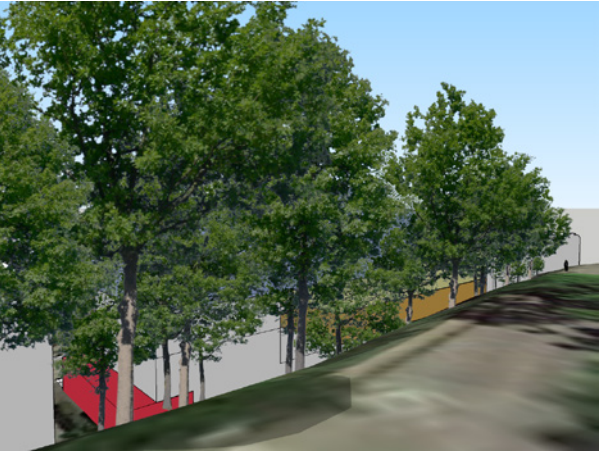
3 BUILDING FROM RESIDENCES ON PALM AVE SW



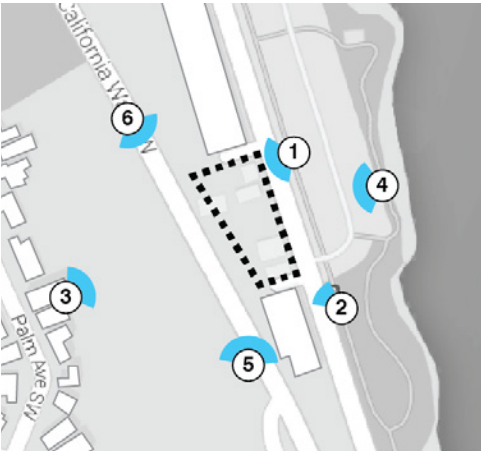
4 EAST ELEVATION FROM PARKING LOT ACROSS STREET

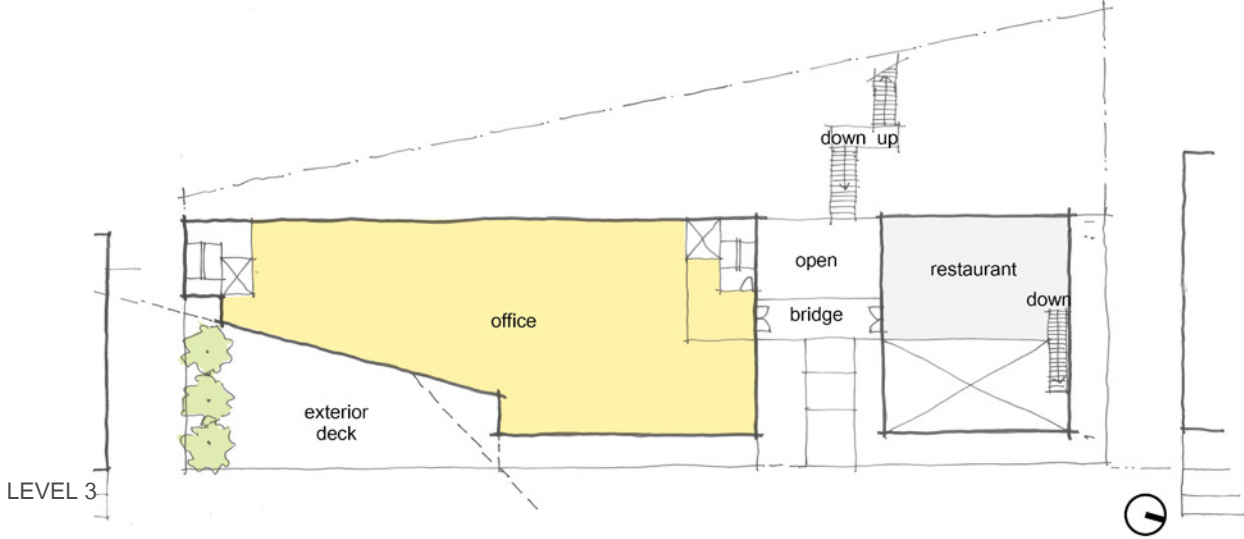
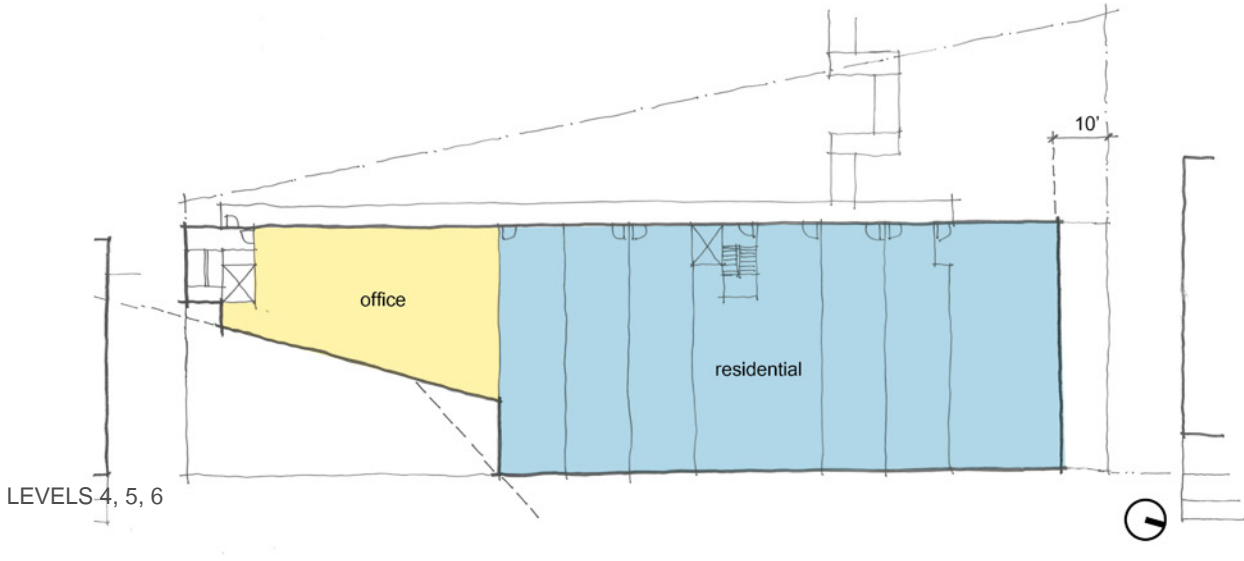
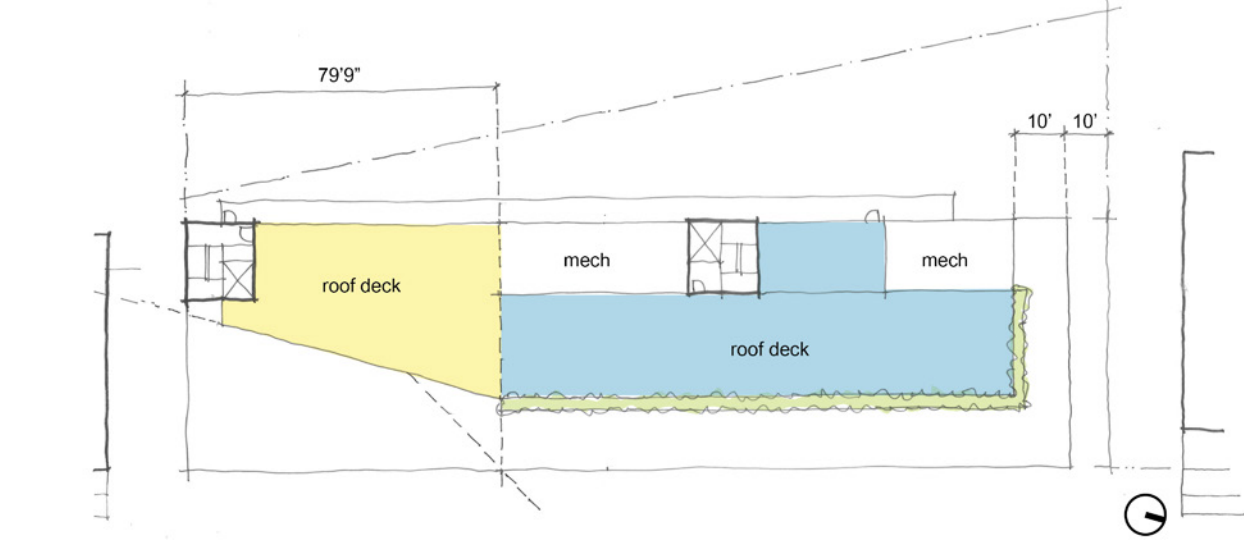
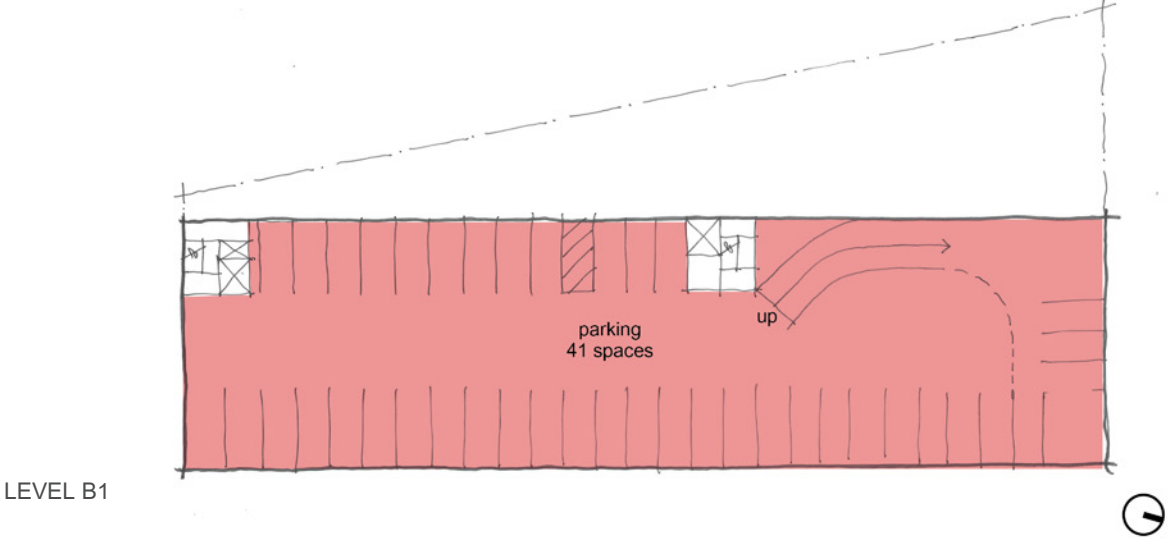
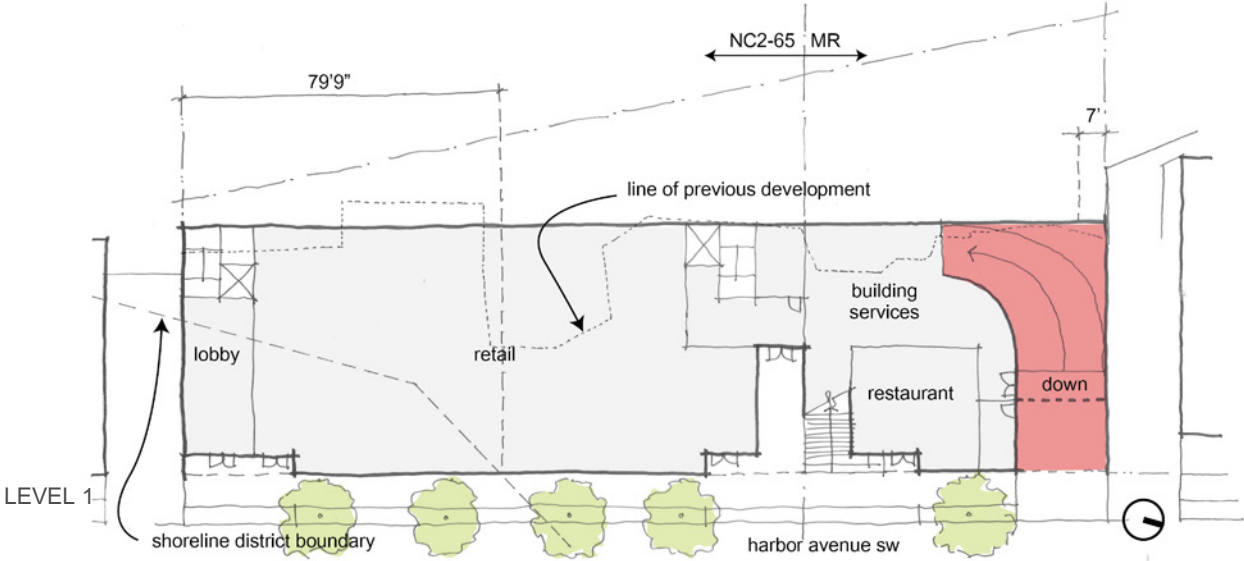
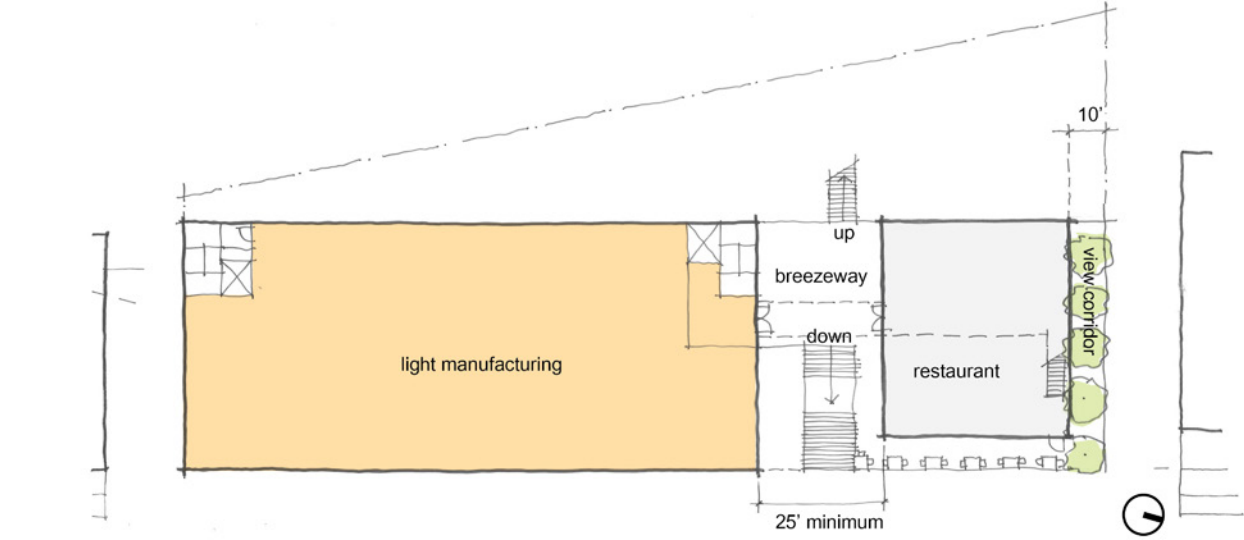


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6 FROM NORTHWEST ALONG CALIFORNIA WAY



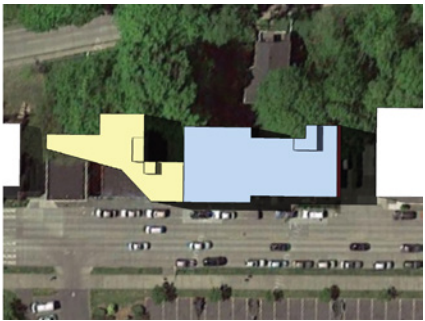


SHADOWS

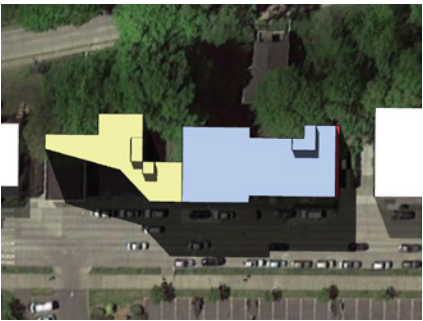
OPTION A



SUMMER SOLSTICE 10:00AM



SUMMER SOLSTICE 12:00AM



SUMMER SOLSTICE 2:00PM



EQUINOX 10:00AM



EQUINOX 12:00AM



EQUINOX 2:00PM



WINTER SOLSTICE 10:00AM



WINTER SOLSTICE 12:00AM



WINTER SOLSTICE 2:00PM

OPTION B



SUMMER SOLSTICE 10:00AM



SUMMER SOLSTICE 12:00AM



SUMMER SOLSTICE 2:00PM



EQUINOX 10:00AM



EQUINOX 12:00AM



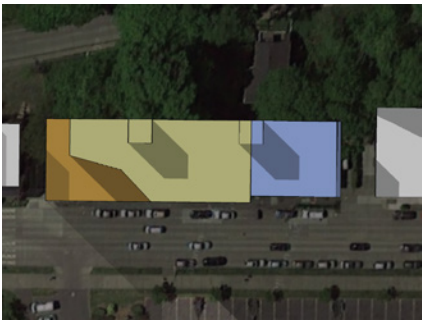
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WINTER SOLSTICE 10:00AM



WINTER SOLSTICE 12:00AM



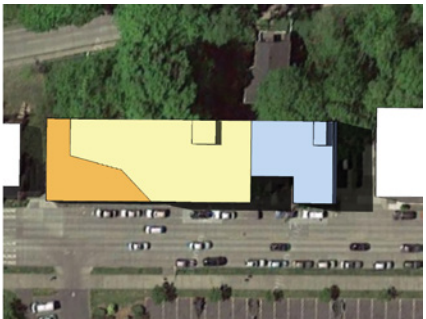
WINTER SOLSTICE 2:00PM

NOTE
Shadows of uphill trees not shown for clarity.

OPTION C



SUMMER SOLSTICE 10:00AM



SUMMER SOLSTICE 12:00AM



SUMMER SOLSTICE 2:00PM



EQUINOX 10:00AM



EQUINOX 12:00AM



EQUINOX 2:00PM



WINTER SOLSTICE 10:00AM



WINTER SOLSTICE 12:00AM



WINTER SOLSTICE 2:00PM

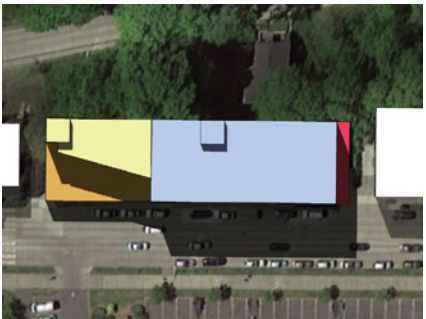
OPTION D



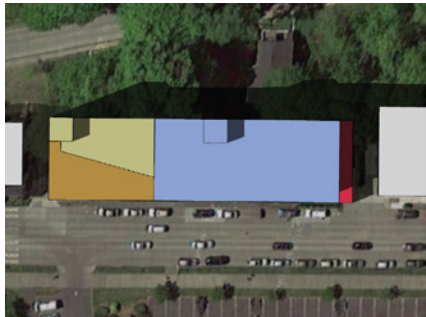
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SUMMER SOLSTICE 12:00AM



SUMMER SOLSTICE 2:00PM



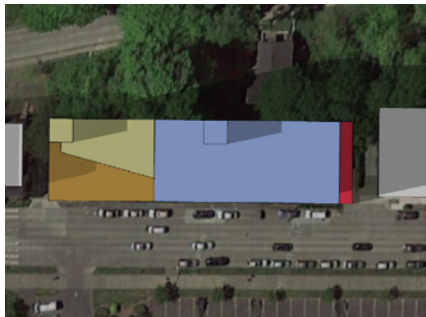
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EQUINOX 12:00AM



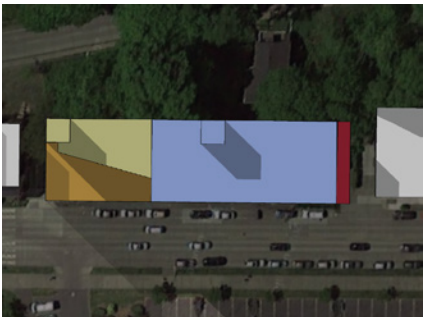
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WINTER SOLSTICE 10:00AM

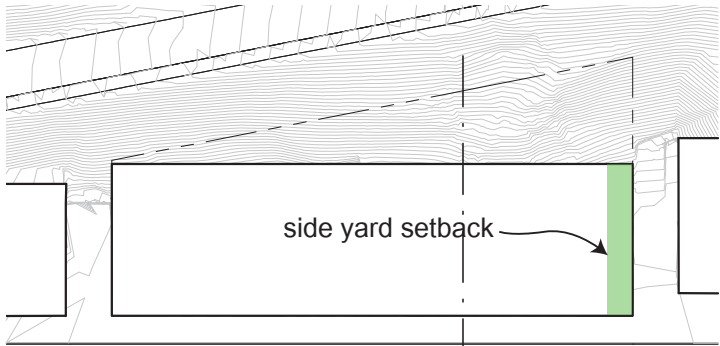


WINTER SOLSTICE 12:00AM



WINTER SOLSTICE 2:00PM

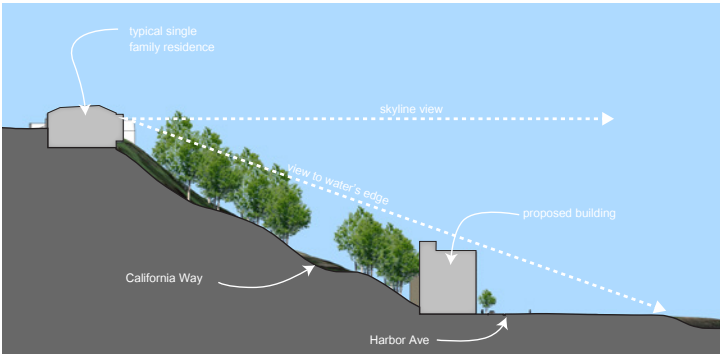
NOTE
Shadows of uphill trees not shown for clarity.



LOCATION OF SIDE YARD SETBACK



VIEW FROM HAMILTON VIEWPOINT PARK
The project's roofline is concealed from above by the dense mass of trees which cover the steep slope of the hillside. This is view 2 from the map on page 11. See also site panoramas on page 23.

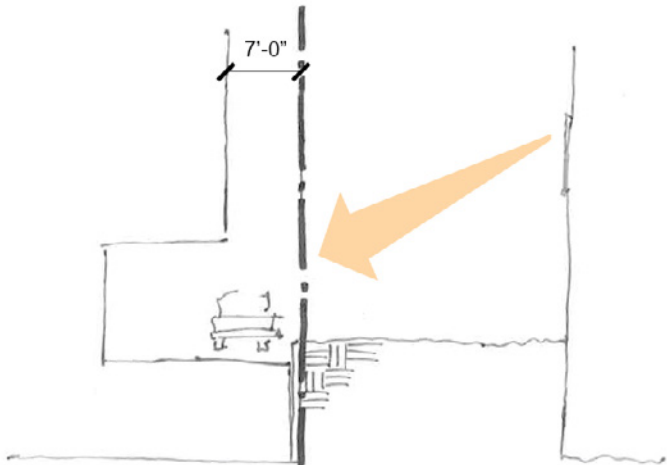


SITE SECTION (EAST WEST)
The far edge of the project roof line is shrouded to residences above by tree canopies along the hillside. If the roof is visible through the trees in winter months, it will not block views of the shore.

DEPARTURE 1

SIDE YARD SETBACK

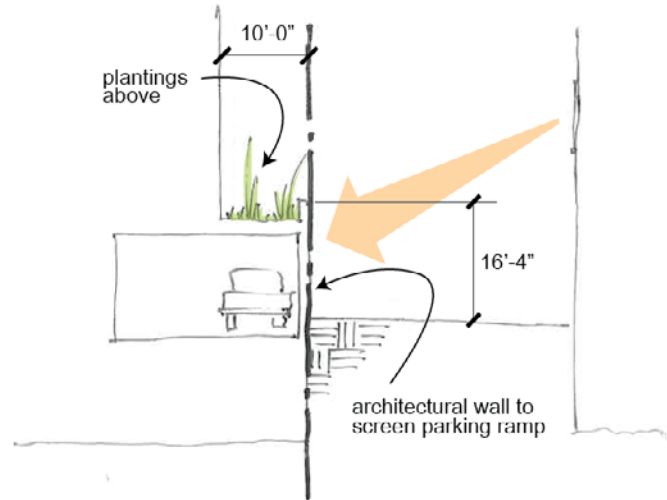
SMC 23.45.518.B MR Zones. Minimum setbacks per (Table B)
Setback Location: Side setback from interior lot line
Required Setback Amount: For portions of the structure 42 feet or less in height: 7 foot average setback; 5 foot minimum setback.



DESIRED DEPARTURE

Our north neighbors would benefit from screening the project parking garage entrance with a wall along the north property line and a roof deck over the drive aisle. The proposed departure would include landscaping along the roof deck for the residential units to look down upon. This landscaping would include some vertical plantings which would further enliven the project's north elevation for the neighbors.

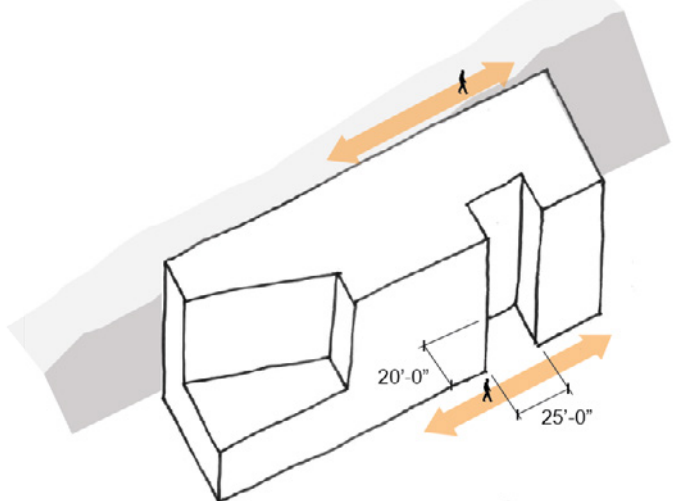
The wall would prevent the glare from headlights cars exiting up the ramp towards the north and towards the neighbors' bedroom windows. The neighboring property has a drive aisle along the north property line. Screening the parking drive aisle as proposed, would limit the area of paved surfaces to look down on from either property.



DEPARTURE 2

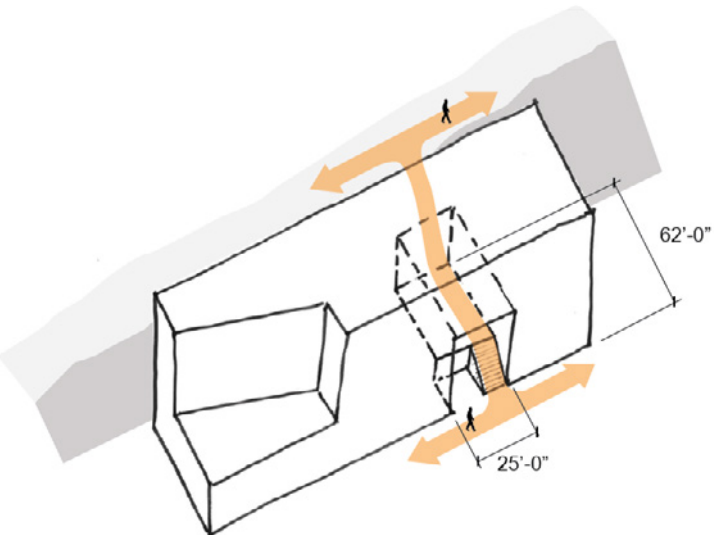
FRONT YARD SETBACK

SMC 23.45.518.B MR Zones. Minimum setbacks per (Table B)
Setback Location: Front setback from street lot line
Required Setback Amount: 7 foot average setback; 5 foot minimum setback. No setback is required if a courtyard abuts the street per Exhibit A for 23.45.518 and the courtyard has:
1. A minimum width equal to 30 percent of the width of the abutting street frontage or 20 feet, whichever is greater and
2. a minimum depth of 20 feet measured from the abutting street lot line.



DESIRED DEPARTURE

In lieu of a "courtyard " open to the sky, the project proposes to provide a three-story high public breezeway through the entire structure from east to west. This breezeway would provide visual and physical access for the public thru the site to the forested hillside beyond. The stairway thru the breezeway area would continue beyond the building up the hillside to California Way SW, west of the site, build on the rich language of urban public stairways in Seattle and become a significant public amenity. Currently there is very limited public access from California Way SW to Harbor Avenue SW.

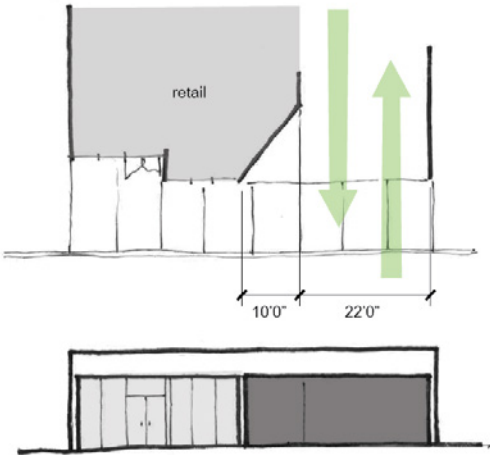


DEPARTURE 3

SIGHT TRIANGLE

SMC 23.54.030.G

For two-way driveways or easements 22 feet wide or more, a sight triangle on the side of the driveway used as an exit shall be provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection of the driveway with a sidewalk. The entrance and exit lanes shall be clearly identified.

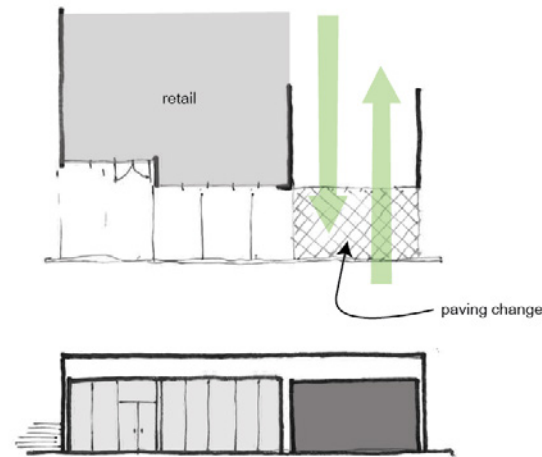


PLAN AND ELEVATION

Large parking entrance is less pedestrian friendly.

DESIRED DEPARTURE

Allowance to use a combination of safety features (including pedestrian and drive warning lights, safety mirrors, and paving changes at the driveway) in lieu of the required sight triangle at the exit side of the ramp, similar to the exception used in Downtown, Industrial and Commercial 1& 2 zones. This would develop a more cohesive and enhanced street frontage for pedestrians along Harbor Avenue SW by de-emphasizing the drive lane width.



PLAN AND ELEVATION

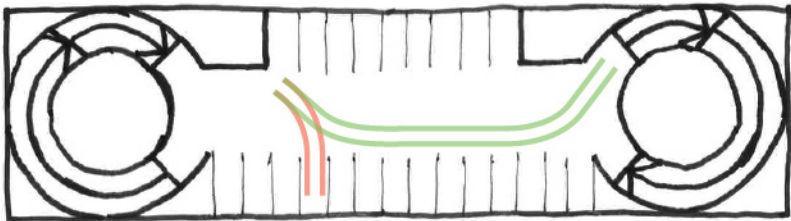
Parking entrance utilizing mirrors, warning lights, and paving change. More opportunities for pedestrians to engage with retail storefront.

DEPARTURE 4

VEHICLE BACKING DISTANCES

SMC 23.54.030.C.2

Except for lots with fewer than three parking spaces, ingress to and egress from all parking spaces shall be provided without requiring backing more than 50 feet.

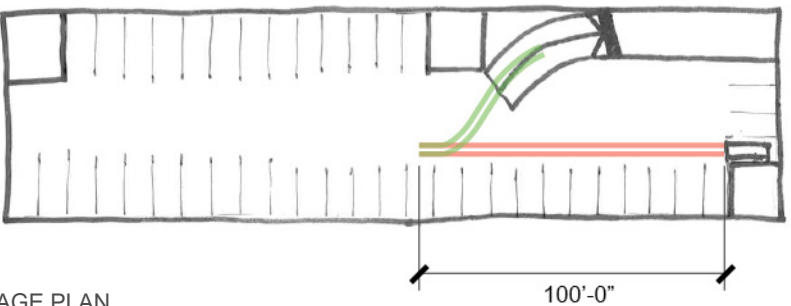


GARAGE PLAN

Two level garage with one way entrance and exit.

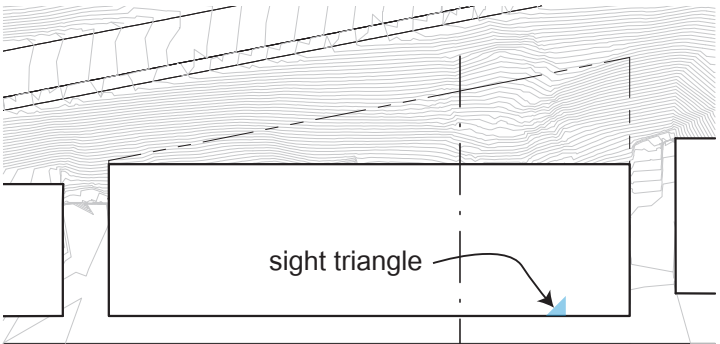
DESIRED DEPARTURE

ECA site development restrictions significantly limits the width of the building and the space available to park vehicles requiring an exception to the 50' backing distance limit. Building tenants with familiarity of the garage will be able to negotiate a longer backing distance of approximately 100'. Private parking can be designated with signage. The limited number of required public parking stalls will not require excessive backing distance. A parking scheme on this tight site that meets the 50' backing restriction would require two levels of parking to meet the required number of parking stalls for the proposed program. With steep slope, shoreline overlay and view corridor requirements, the limited achievable FAR for the site would not support the cost of a two level parking garage.



GARAGE PLAN

Single level garage with two way entrance and exit.



LOCATION OF SIGHT TRIANGLE