

124 DENNY WAY MIXED-USE

124 Denny Way
Seattle, WA 98109

**Design Review
Recommendation Meeting**
March 25, 2015
DPD #3015549

prepared for Deutsch Partners



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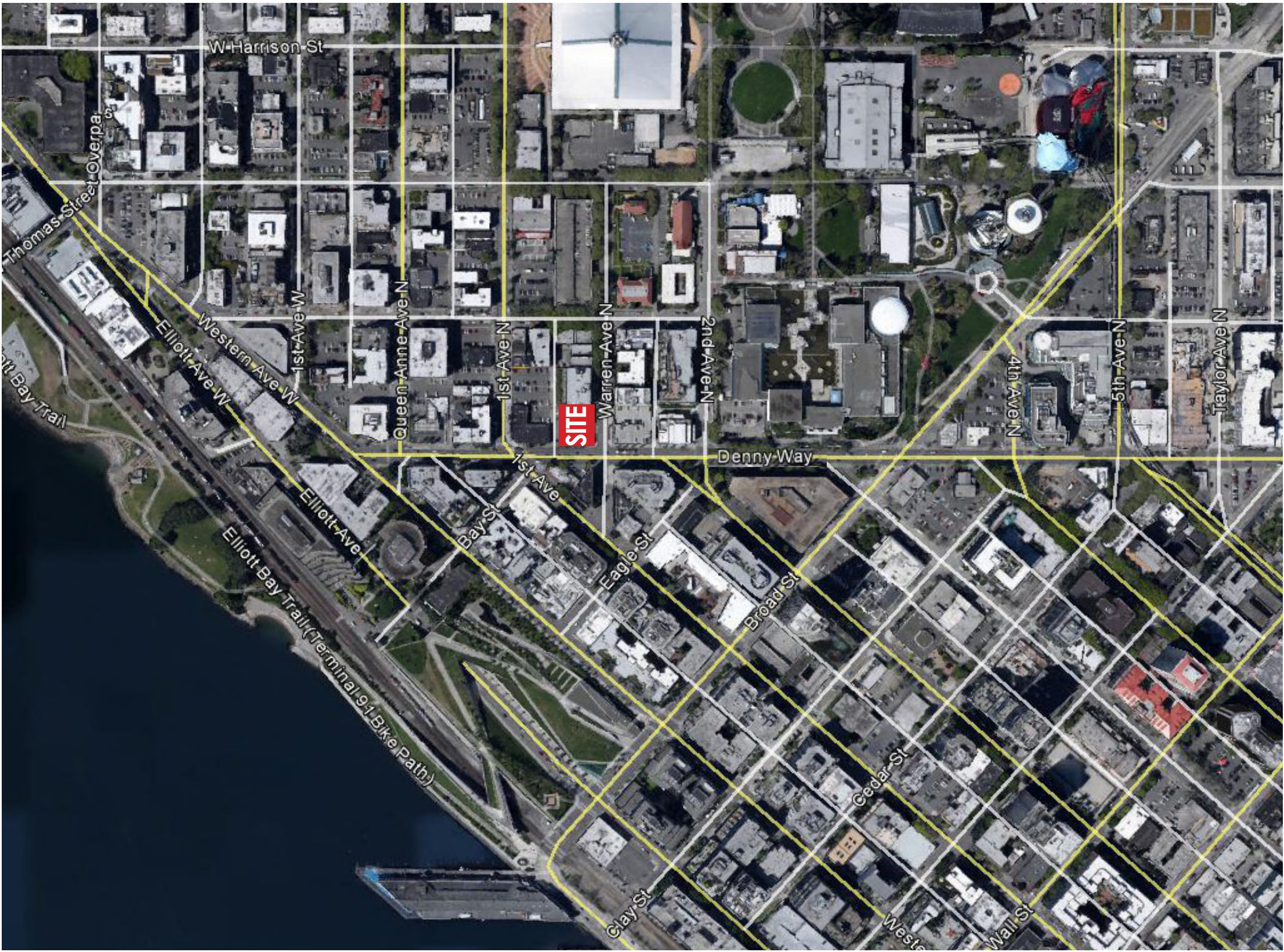
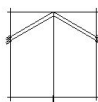
PROJECT PROPOSAL/GOALS

The applicant proposes to construct a 5 story wood frame residential building over one story concrete podium with residential units facing the street on Warren Ave. N and commercial space facing Denny Way.

The project will have approximately 75 apartment units and 6 live/work units, along with approximately 50 below grade parking stalls accessed from the alley, and 2,500 SF of street front commercial space.

The building strives to provide a conscientious interface with the surrounding neighborhood context. The height, bulk, and scale of the primary massing responds to the immediate site constraints and takes advantage of the scenic views afforded to three of its four sides.

The development will contribute to the neighborhood a more vibrant pedestrian and resident experience. Humaizing Denny Way and transitioning to the quieter neighborhood to the north will provide a tasteful urban experience to both those who live in the area, and to pedestrians passing through to downtown and the Seattle Center.



Site Location - Aerial Photo



Nearby Amenities Diagram

NEIGHBORHOOD

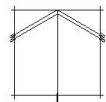
The site is conveniently located with access to multiple amenities in the neighborhood, and is close to downtown, the Olympic Sculpture Park, the waterfront and Seattle Center. Units throughout the building have views of the Puget Sound and the Space Needle.

Adjacency to Seattle Center allows for rooftop viewing of the fireworks on the 4th of July and New Years as well as close-by access to the IMAX theater and events at Key Arena.

The waterfront is just two blocks away and provides convenient access to jogging and biking trails along the waterfront.

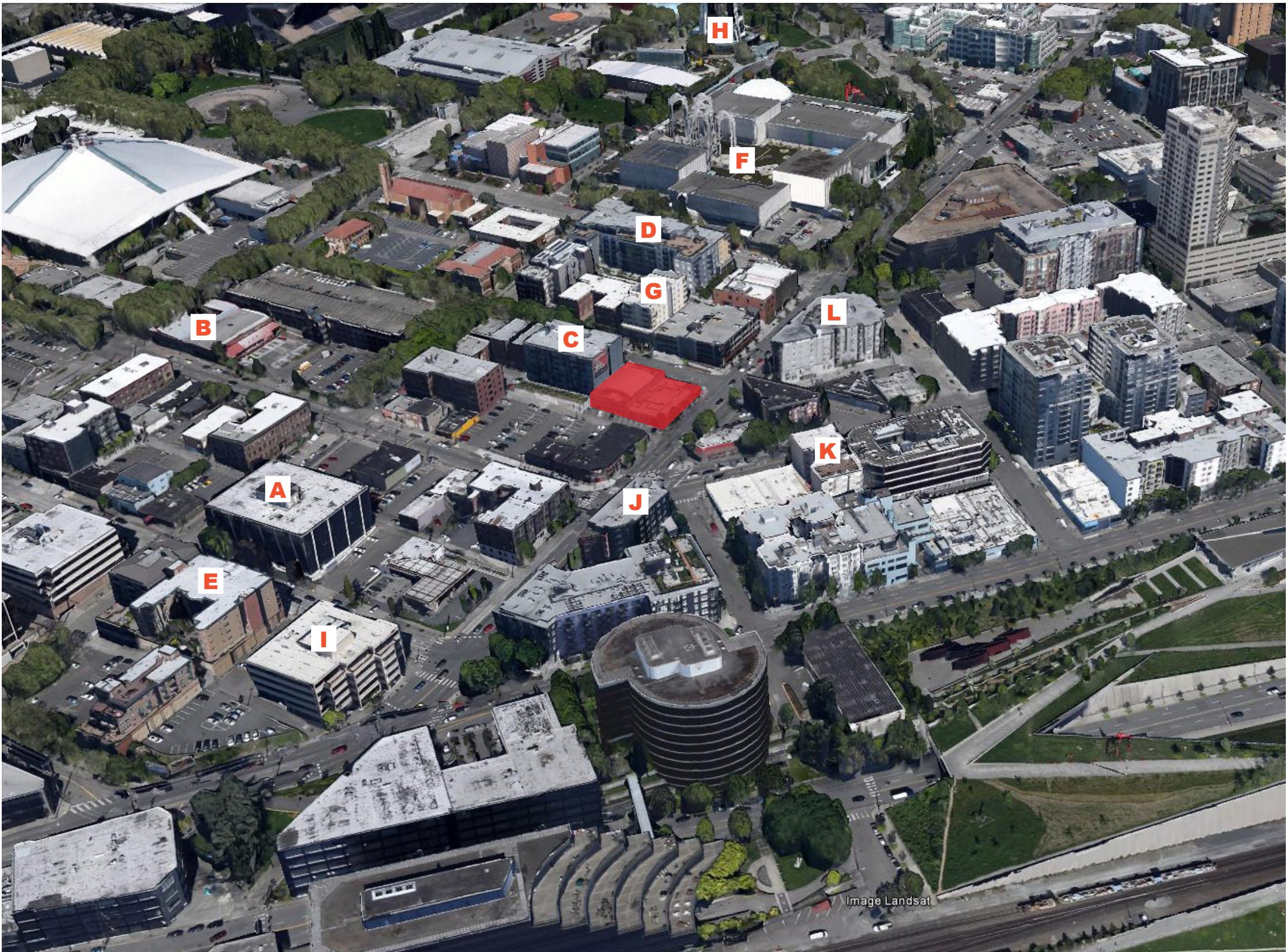
The Olympic Sculpture Park allows residents to tour the art gallery at their convenience as well as visit the park during summer events.

Major bus routes provide convenient access to South Lake Union, downtown, and beyond.



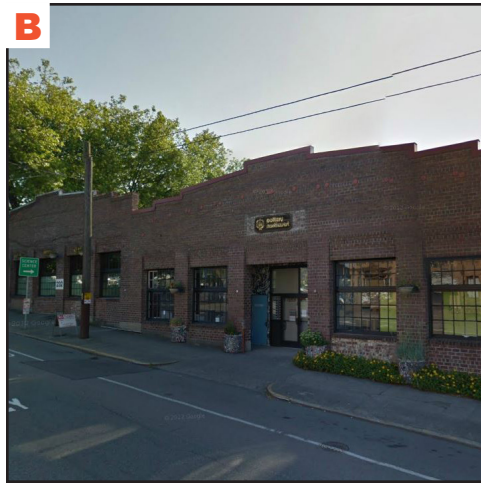
3D AXON VIEW

A neighborhood in transition - this view shows that the neighborhood still has significant areas of open parking lots and one story commercial structures. A few new multi-family and mixed-use projects are currently being planned in the immediate vicinity of the site. As the area transitions to a more residential zone this project will be a visible, high-quality addition to the architectural setting.

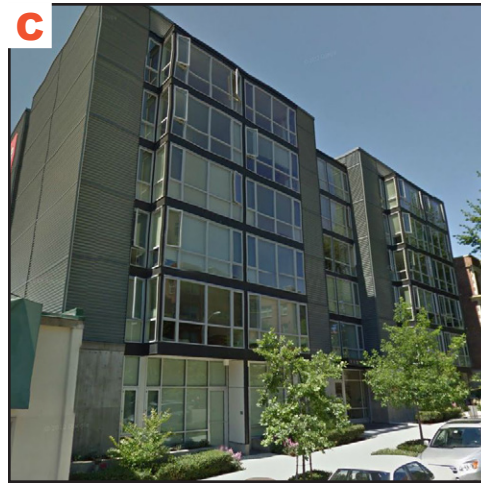




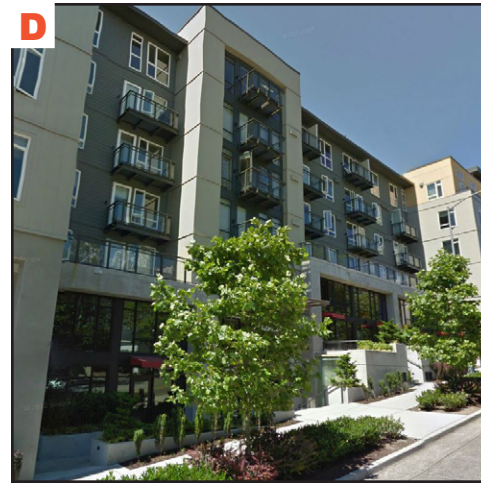
Seattle Housing Authority



Pottery Northwest



Bernard Apartments



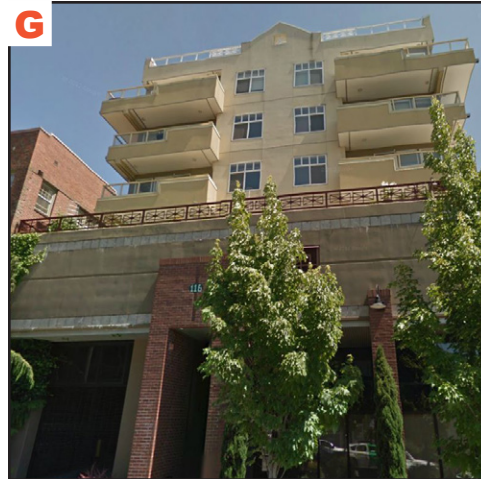
AXIS Apartments



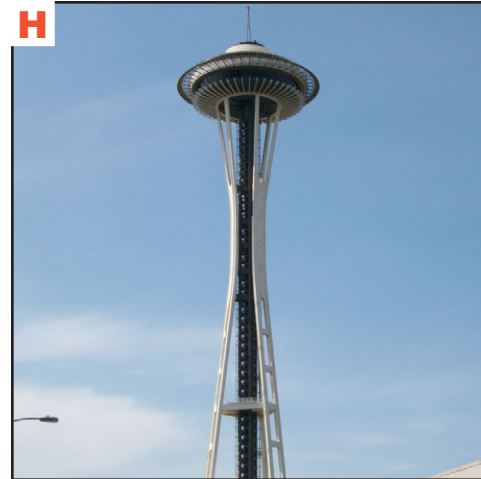
Seattle Furnished Apartments



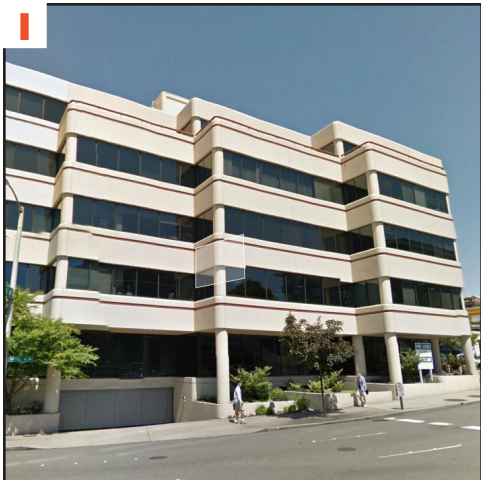
Pacific Science Center



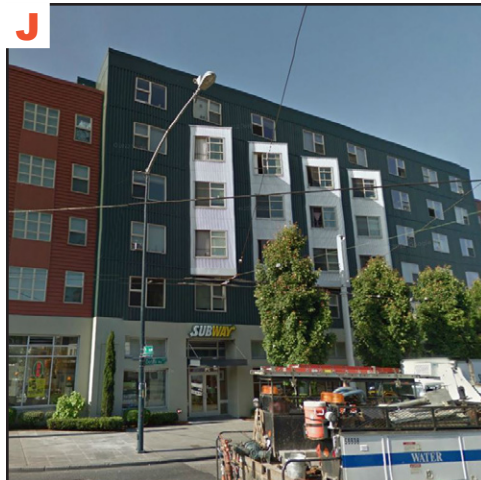
Center Court Apartments



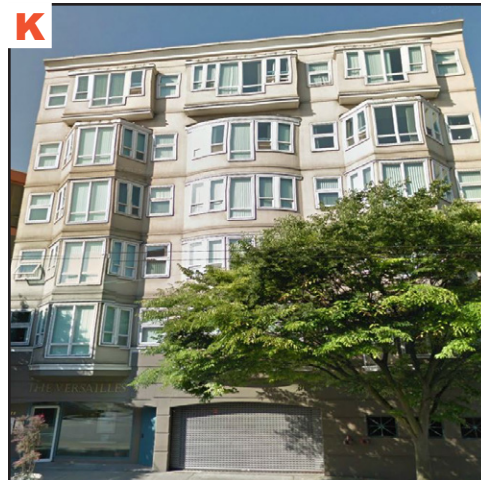
Space Needle



111 Queen Anne



Plymouth Place



The Versailles



Matae Belltown Condos

CONTEXT PHOTOS

The neighborhood is in transition, with open parking lots, single-story structures, and multi-story buildings from the 1970's and 1980's. Newer projects are sprinklered throughout the neighborhood, including the Bernard Apartments, immediately to the north of the project site.



Looking north on Warren Ave N.



Looking west on Denny Way



VIEW OF SITE FROM ACROSS WARREN AVE

SITE



VIEW OF SITE FROM ACROSS DENNY WAY

SITE



VIEW FROM SITE ACROSS DENNY WAY

EXISTING SITE CONDITIONS

The site is located at 124 Denny Way, on the northwest corner of Denny Way and Warren Ave. N. The 14,400 SF (120'x 120') site is currently occupied by a 2-story brick building on the northern lot and a 1-story brick building on the southern lot.

The site slopes down approximately 12' from the northeast corner to the southwest corner. The northeast corner is located at an elevation of 95.0' and the southwest corner is located at an elevation of 82.5'. The site exposure on Denny way drops in elevation approximately 6'-6" from east to west. The site exposure on both the alley and Warren Ave. N. increase about 6'-6" from south to north.

Denny Way is a busy street with many uninteresting buildings. Bill boards and graffiti adorn the walls of structures and barren facades are frequent. The project will introduce a dynamic facade on Denny Way, and a reserved, elegant massing on Warren. Recessed units on the east and west sides reflect a residential scale while the southern face will help define the new architectural character on Denny Way.

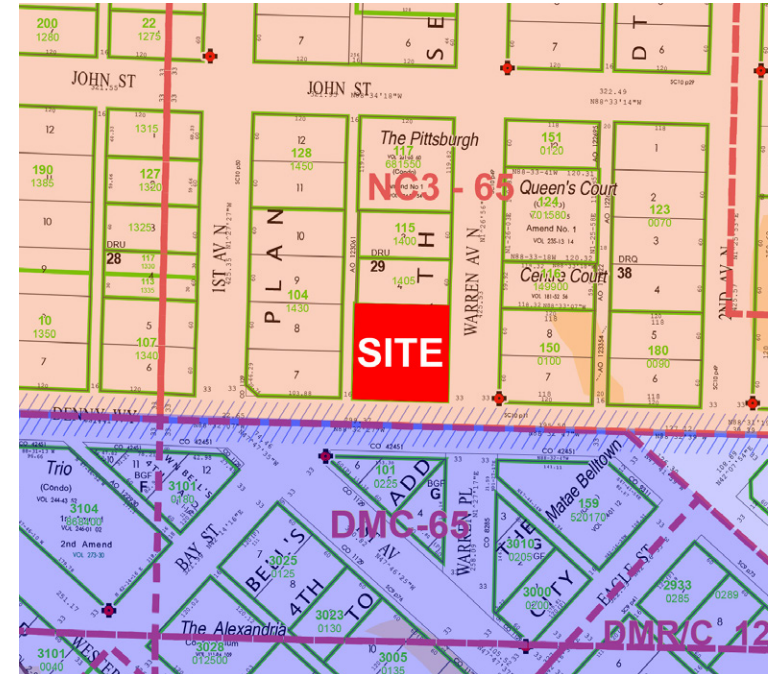
The adjacent building north of the site has a similar massing to the proposed project, but the neighborhood still has significant areas of open parking lots and one story commercial structures. As the area transitions to a more residential zone this project will be a visible, high-quality addition to the architectural setting.

ZONING DATA SUMMARY

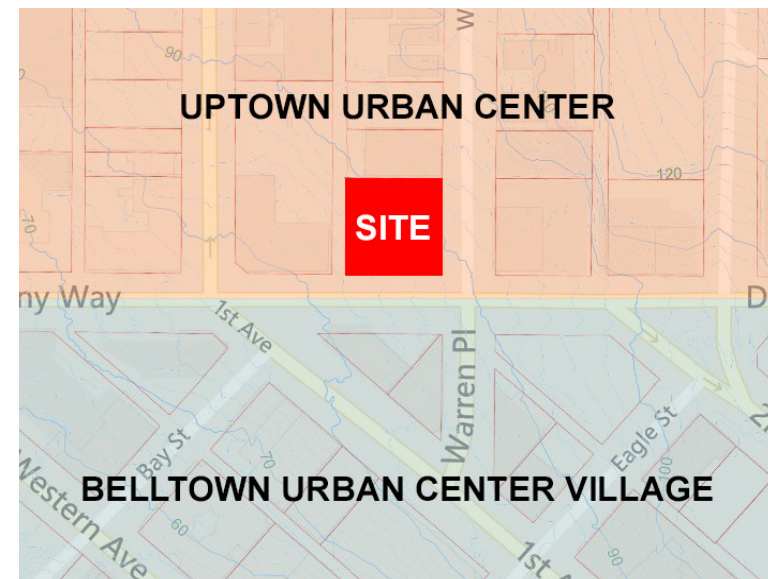
ZONING CODE: CITY OF SEATTLE LAND USE CODE
ZONE: NC3-65 - WITHIN THE UPTOWN URBAN CENTER
LOT AREAS: +/-14,400 SF

- 23.47A.005.C.1.G STREET LEVEL USES: 23.47A.005.C.1.G
In the arterial street frontage zone of Denny Way there is a 20% limit on the percentage of residential street frontage.
- 23.47A.008 STREET LEVEL DEVELOPMENT STANDARDS:
Blank segments of the street-facing street-level facade between 2' and 8' above the sidewalk may not exceed 20'
NONRESIDENTIAL STREET LEVEL REQUIREMENTS
60% of the street facing facade between 2' and 8' above the sidewalk shall be transparent, nonresidential use shall extend an average depth of at least 30' from the street facing facade, nonresidential use shall have a floor to floor height of at least 13'
RESIDENTIAL USES
At least one of the street level street facing facades containing a residential use shall have a visually prominent pedestrian entry - provided on the corner of Denny Way & Warren Ave. N. The floor of a dwelling unit located along the street-level street facing facade shall be at least 4' above or below sidewalk grade or be set back a minimum of 10' from the sidewalk
- 23.47A.012 STRUCTURE HEIGHT: 65' FOR BOTH LOTS
Open railings, parapets may extend 4' above height limit
Stair and elevator penthouses may extend 16' above height limit
- 23.47A.013 FLOOR AREA RATIO:
The FAR limit for each zone applies to the portion of the lot located in that zone
NC3-65:
Total for any single use within a mixed use structure = 4.25
Total all uses in a mixed use structure = 4.75

- 23.47A.016 LANDSCAPING REQUIREMENTS:
Landscaping that achieves a green factor of .30 is required. Existing street trees shall be retained unless the director of transportation approves their removal.
- 23.47A.024 AMENITY AREA:
Amenity area required equal to 5% of the total gross floor area in residential use. All residents shall have access to at least one common or private amenity space. Amenity areas shall not be enclosed. Common amenity areas shall have min dimension of 10' and be at least 250 SF. Private balconies and decks shall have min dimension of 6' and be at least 60 sf
- 23.47A.032 PARKING LOCATION AND ACCESS:
Access to parking shall be from the alley. Street level parking shall be separated from street-level, street facing facades by another permitted use.
- 23.47A.033 TRANSPORTATION CONCURRENCY:
Proposed uses shall meet the transportation concurrency level-of-service standards
- 23.54.015 REQUIRED PARKING:
No parking required for non-residential uses within Urban Centers.
No parking required for residential uses within Urban Centers.
No parking required for live-work under 1500 sf.
Bicycle parking for commercial = short term 1 space/4,000 sf
Bicycle parking for residential = long term 1 per 4 units
- 23.54.040 SOLID WASTE AND RECYCLABLE MATERIALS STORAGE AND ACCESS:
375 SF plus 4 SF for each unit above 50. 82 SF for 0-5,000 SF of non-residential use. Mixed use shall meet requirements for residential plus 50% of non-residential requirements.



Zoning Map - "NC3-65"



Urban Village Zone - Uptown Neighborhood, Uptown Urban Character Zone

ZONING AND EXISTING USE

The immediate surrounding buildings are mostly low-rise mixed use and multi-family residential buildings. Surface parking, commercial use are spread throughout the neighborhood, with denser structures south of Denny Way. Significant civic buildings are close by.

The site is at the boundary of the NC3-65 zone, with the DMC-65 zone to the south of Denny Way. The lot also is within the Uptown Urban Center area, with the Belltown Urban Center Village immediately to the south.

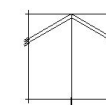
mixed use

civic building

commercial

multi family residential

parking and service

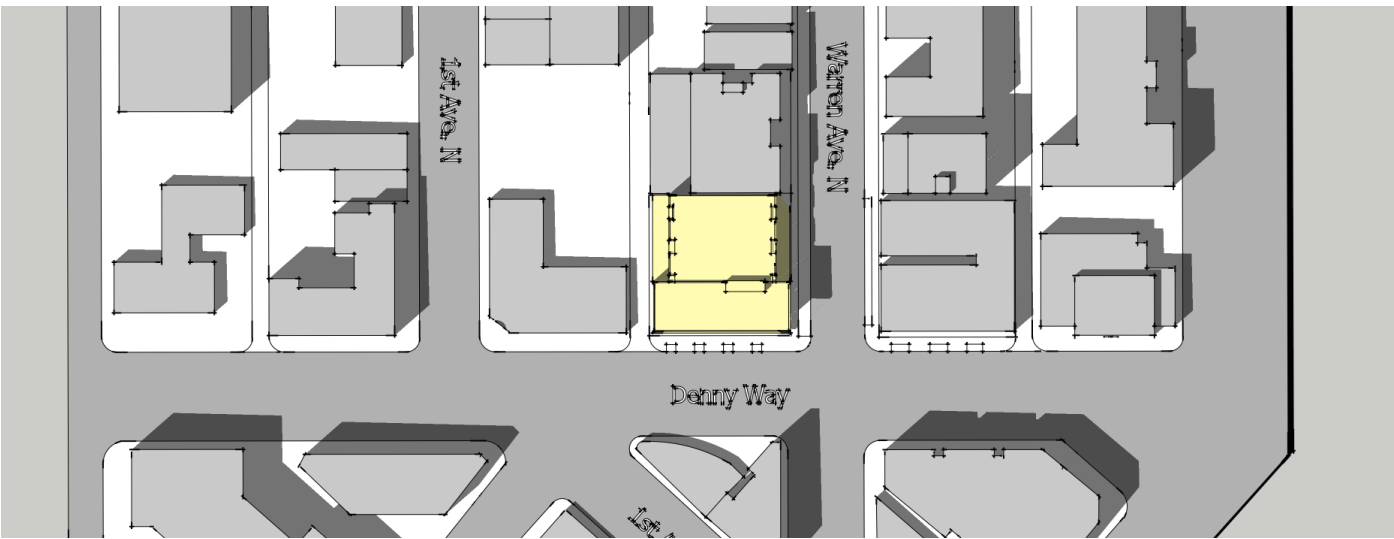
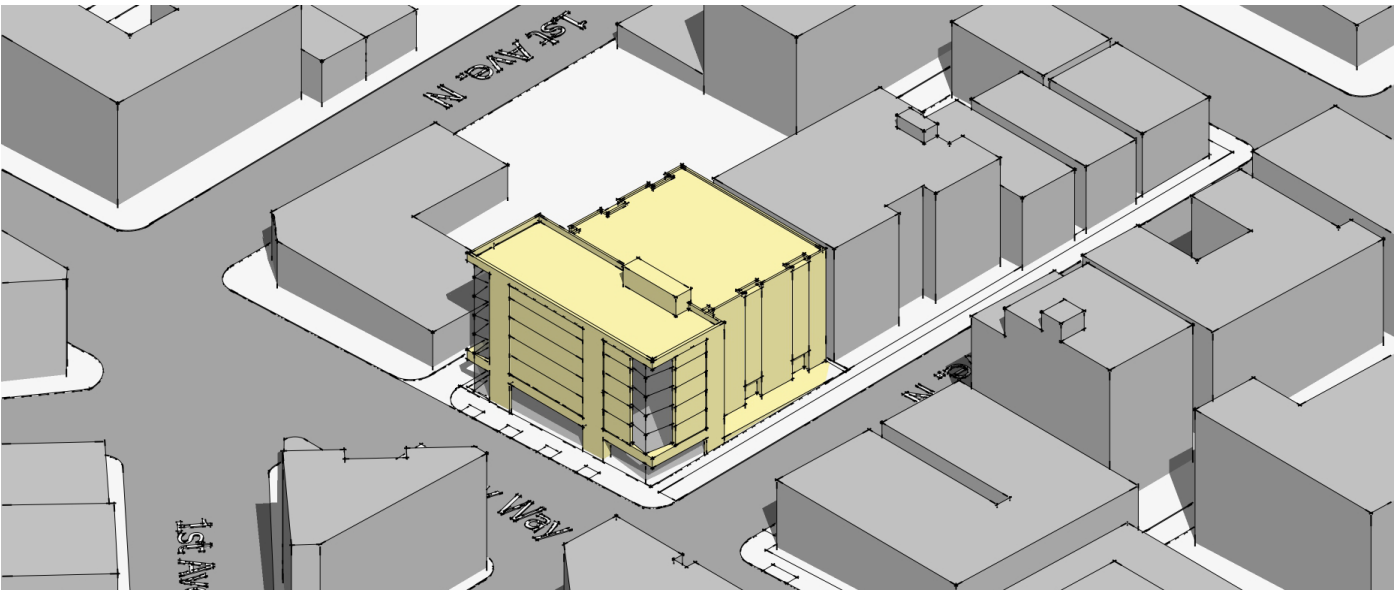


EARLY DESIGN GUIDANCE

March 2014

The Board directed the applicant to proceed with the preferred concept B.

PREFERRED CONCEPT B:
This concept uses two primary masses to form the overall structure. A “bar” form that orients itself toward Denny Way creates a presence on the main street, while a secondary “bar” centers itself perpendicular to Denny Way. This relationship creates recessed zones from the alley and Warren Ave. N., thus reinforcing the more private residential street.



BOARD SPECIFIC GUIDANCE

March 2014

Massing, Modulation and Facade Composition: The Board was pleased with the proportions of the project and directed the applicant to proceed with the preferred Concept B. The Board noted the preferred option massing was 'broken' into a northern section with setbacks from the street and alley, and a southern section along Denny Way. The Board gave the following guidance. (CS2.III.i, DC2.B.1, CS2.D.1)

- a. The massing of the preferred option is appropriate and relates well to the residential structure to the north. (CS2.D.1)
- b. Keep the modulation as shown. The north portion of the structure seems more modulated for residential use, the south 'piece' has less modulation. (CS2.IV.iii)
- c. The Board noted that given the shifting of the street grid at Denny the alley corner will be prominent, especially from Bay St. The corner treatment is very important and does not need to be treated the same as the corner at Warren Ave N. (CS2.A.2, CS2.C.1, CS2.III.i)
- d. Maintain the setbacks of the north section of the structure. (DC3.A.1)
- e. Work out how the facades of the two sections will relate to each other using datum lines and accent panels. (DC2.B.1)

Street Level and Entries: The Board expressed that the connection between the street and the structure needs to be carefully considered. (CS2.B.2, PL2.II.i-iv, PL3.A.4)

- a. Maintain the proposed retail use on Denny Way. (CS2.I.i, PL2.I.ii)
- b. The retail façade treatment should turn the corner and extend into the alley. (CS2.C.1, PL3.C.1)
- c. The residential lobby entry needs further study and design. (CS2.III.i, PL2.II.i)
- d. Provide patios for the residential units off of Warren Ave N. that will be primarily private. (PL2.II.ii)
- e. Maintain the 12' setback and use landscaping along Warren Ave N. to provide a sense of protection and privacy for the residential entries. (PL2.II.iii)
- f. Work with SDOT on providing street trees that will complement the design. (PL1.II.ii)

Alley Treatment: The Board supported and noted that the alley facing residential units and patios located at the first level will activate the alley. (DC1.VI.ii)

- a. The Board encouraged the design of the patio units above the alley provide security. (PL2.B)
- b. Provide a staging area on the north side of the driveway for solid waste on collection day that will work for SPU and building staff. (DC1.C.4)
- c. Design the driveway for safety of pedestrians and vehicles. Consider narrowing the width of the driveway. (DC1.C.4)
- d. The retail façade treatment should turn the corner and extend into the alley. (CS2.C.1, PL3.C.1)

EDG RESPONSE:

MASSING, MODULATION AND FACADE COMPOSITION:

BOARD SPECIFIC GUIDANCE: The massing of the preferred option is appropriate and relates well to the residential structure to the north.

RESPONSE: The proposed building creates a transition between the activity on Denny Way (an arterial) and the residential scale of Warren Ave, a shaded, tree-lined, residential street. The primary mass on Denny Way follows the property line, with the commercial facade reinforcing the same mass above. The design complements the height, bulk, and scale of the adjacent mixed-use structure to the north. A gracious setback along Warren reflects the residential use, preserves existing views, and signifies the transition to the residential neighborhood north of the site. Above grade decks on Warren help signify the residential use. It is anticipated that future buildings to the west and south will be of similar bulk, given current zoning restrictions.

BOARD SPECIFIC GUIDANCE: Keep the modulation as shown. The north portion of the structure seems more modulated for residential use, the south 'piece' has less modulation.

RESPONSE: The modulation has been retained. The building is broken down into two prominent masses. The consistent framework of the southern bar allows it to read as a whole, with smaller facade articulation providing interest and texture. The northern bar has a more residential, tripartite design with strong vertical elements broken down with the addition of decks, glazed guard rails, and a deeper exterior wall, creating shade and shadow and a more residential articulation of units.

BOARD SPECIFIC GUIDANCE: The Board noted that given the shifting of the street grid at Denny, the alley corner will be prominent, especially from Bay St. The corner treatment is very important and does not need to be treated the same as the corner at Warren Ave N.

RESPONSE: The high visibility of the corners of Denny Way and Warren Ave N is reflected in the prominent corner treatment and material placement, with extensive glazing on both corners of the project. The SW corner has wrap-around decks and window patterning that matches the south elevation, while the SE corner's prominence is emphasized by both wrap-around decks, facade articulation, and bold materials and textures. The residential entrance/lobby is located prominently on the SE building corner, signified by an overhead canopy, accent material above the entry, and floor to ceiling glazing.

BOARD SPECIFIC GUIDANCE: Maintain the setbacks of the north section of the structure.

RESPONSE: The setbacks of the north bar have been maintained. The east patio setback has been increased to 14' back from the sidewalk edge, while the west patios are 16' back from the alley. The patios adjacent to Warren are primarily private, with raised planters, steps and friendly gates. Landscaping provides screening for privacy.

STREET LEVEL AND ENTRIES:

BOARD SPECIFIC GUIDANCE: Maintain the proposed retail use on Denny Way.
The retail facade treatment should turn the corner and extend into the alley.

RESPONSE: The retail use has been maintained for the majority of the building facade on Denny Way. The retail facade glazing and materials turn the corner into the mouth of the alley.

STREET LEVEL AND ENTRIES CONT'D:

BOARD SPECIFIC GUIDANCE: The residential lobby entry needs further study and design.

RESPONSE: The residential lobby entrance is prominently located on the corner of Denny Way and Warren Ave. N. This location is highly identifiable, with the floor to ceiling glazing, overhead canopy, and pedestrian signage. It will have a strong connection to the street, providing an open and welcoming sense upon arrival. The retail entrance is on Denny Way, signified with an overhead canopy and pedestrian scaled signage. The space is visually connected to the street with large expanse of glazing and is provided with protection from the weather. Pedestrian entrances at the residential units on Warren are set back, providing protection, and humanly scaled with appropriately detailing. They are defined by landscaping, raised planters, and low gates. The patio space acts as a buffer between the public realm and the residential unit, creating a primarily private space along the street edge.

BOARD SPECIFIC GUIDANCE: Provide patios for the residential units off of Warren Ave. N. that will be primarily private.
Maintain the 12' setback and use landscaping along Warren Ave N. to provide a sense of protection and privacy for the residential entries.

RESPONSE: The east patio setback has been increased to 14' back from the sidewalk edge. The patios adjacent to Warren are primarily private. Steps, gates, and raised planters and landscaping provide a grade change and buffer between the sidewalk traffic and the residential units.

ALLEY TREATMENT:

BOARD SPECIFIC GUIDANCE: The Board encouraged the design of the patio units above the alley, provide security.

RESPONSE: The units facing the alley will help activate the alley. The raised patios are above the alley by an average of 4'-6". A 2' high planter on the west side of the patio softens the space, while a metal and wood screen provides both privacy and security. A locked access gate at the alley provides residents access to the building, and also allows a direct link to a secure bicycle storage room.

BOARD SPECIFIC GUIDANCE: Provide a staging area on the north side of the driveway for solid waste on collection day that will work for SPU and building staff.
Design the driveway for safety of pedestrians and vehicles. Consider narrowing the width of the driveway.

RESPONSE: Staging area is provided to the north of the driveway, adjacent to the garage entrance. Solid waste staging is located away from the sidewalk pedestrian zone. Operationally, there will be two pick-up days, with the container pushed out to the alley by building maintenance for pick-up by SPU. Garage access is provided off of the existing alley, minimizing conflicts between vehicles and pedestrians. The driveway width is designed for both vehicles and pedestrians, safety devices such as mirrors, and audible alerts, may be installed. The driveway is narrowed and proposed to be 16' (departure request). Multiple approaches towards safety at the alley will be considered, including security screening, lighting, and well-marked entries.

BOARD SPECIFIC GUIDANCE: The retail façade treatment should turn the corner and extend into the alley.

RESPONSE: The retail facade glazing and materials turn the corner into the mouth of the alley.

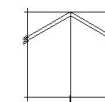




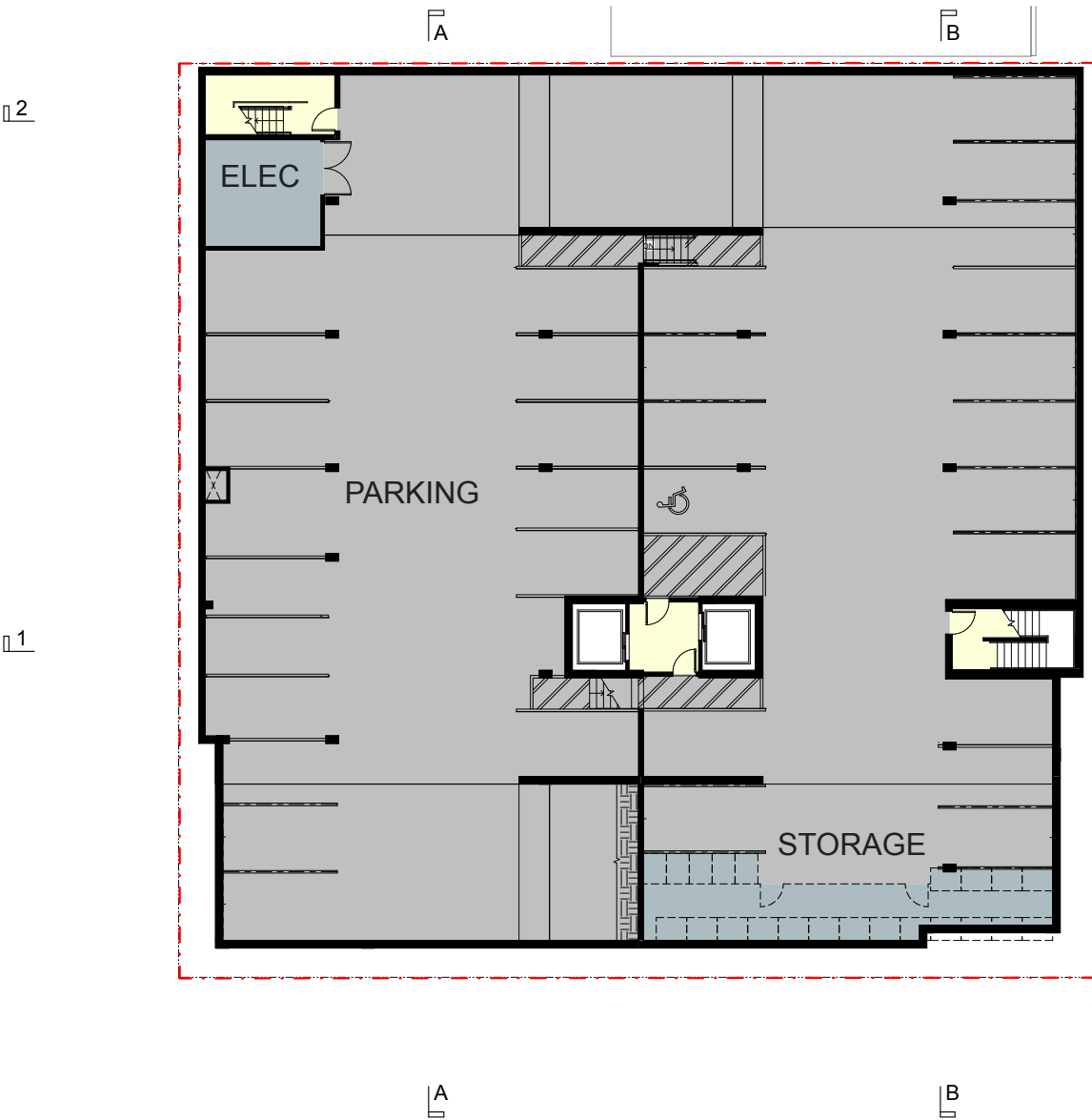
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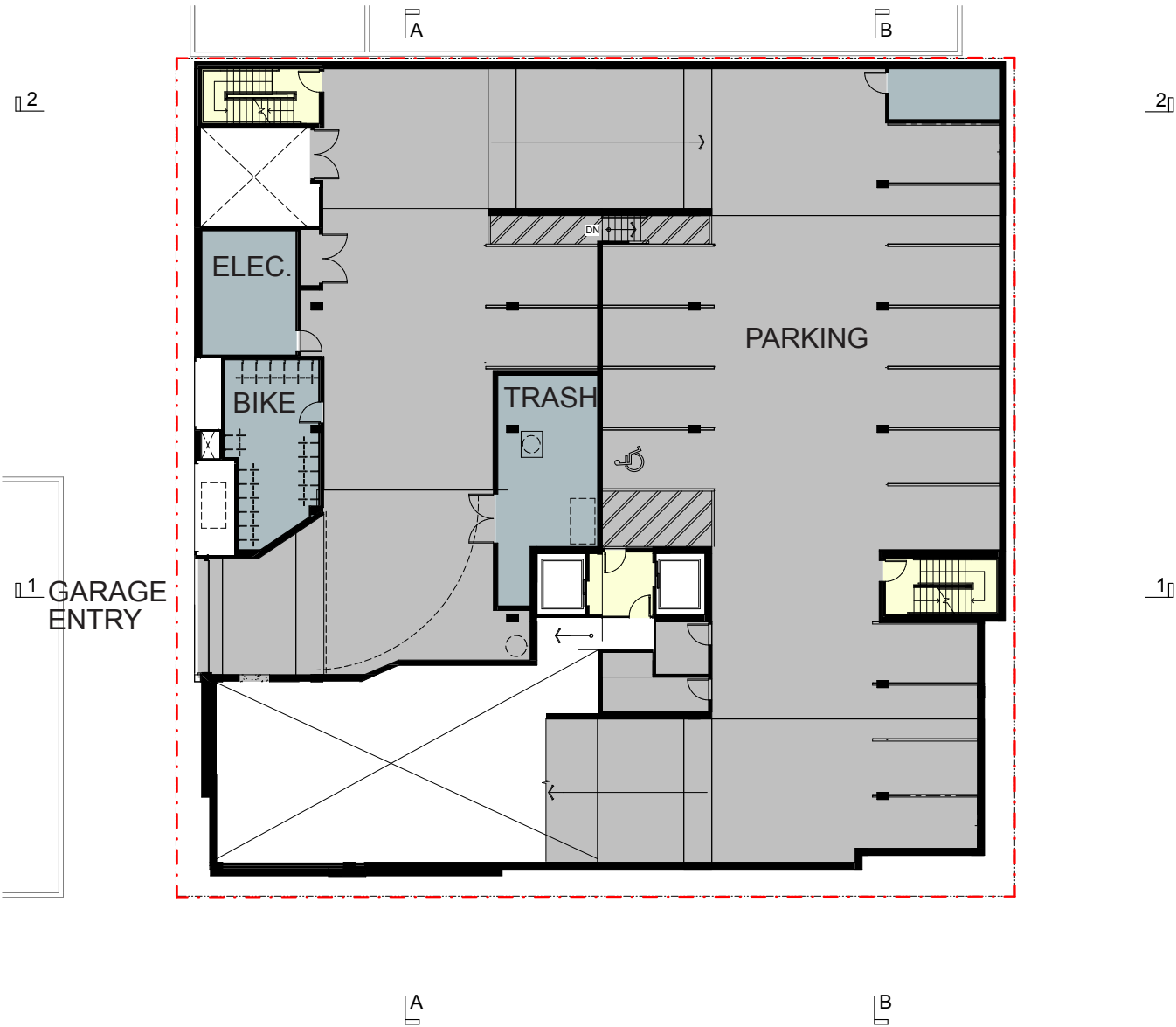
COMPOSITE SITE PLAN



BUILDING DRAWINGS:



PARKING LEVEL P-2



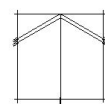
PARKING LEVEL P-1



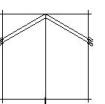




LEVEL 2

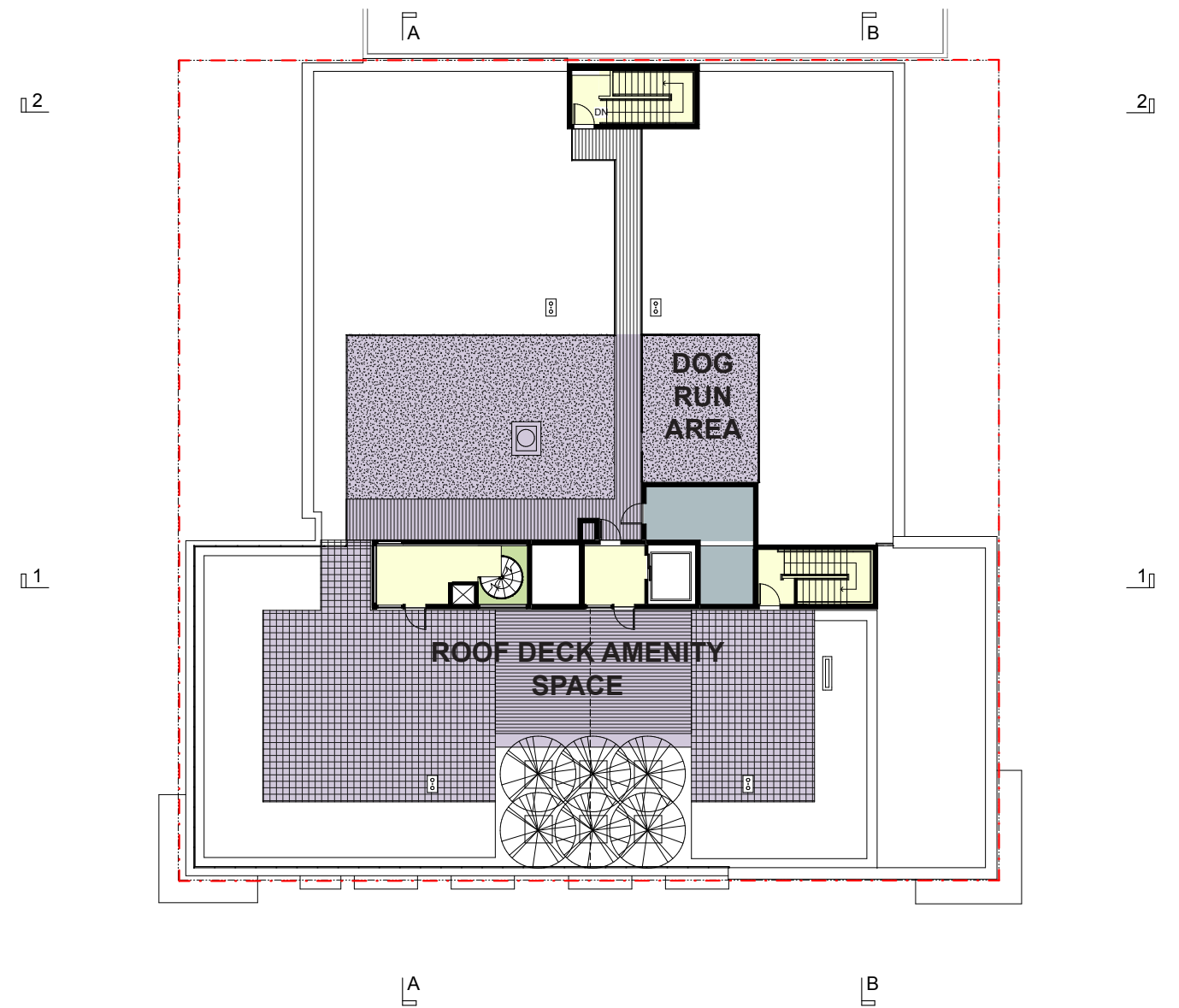
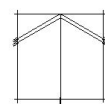


LEVELS 3, 4, 5

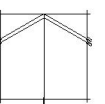




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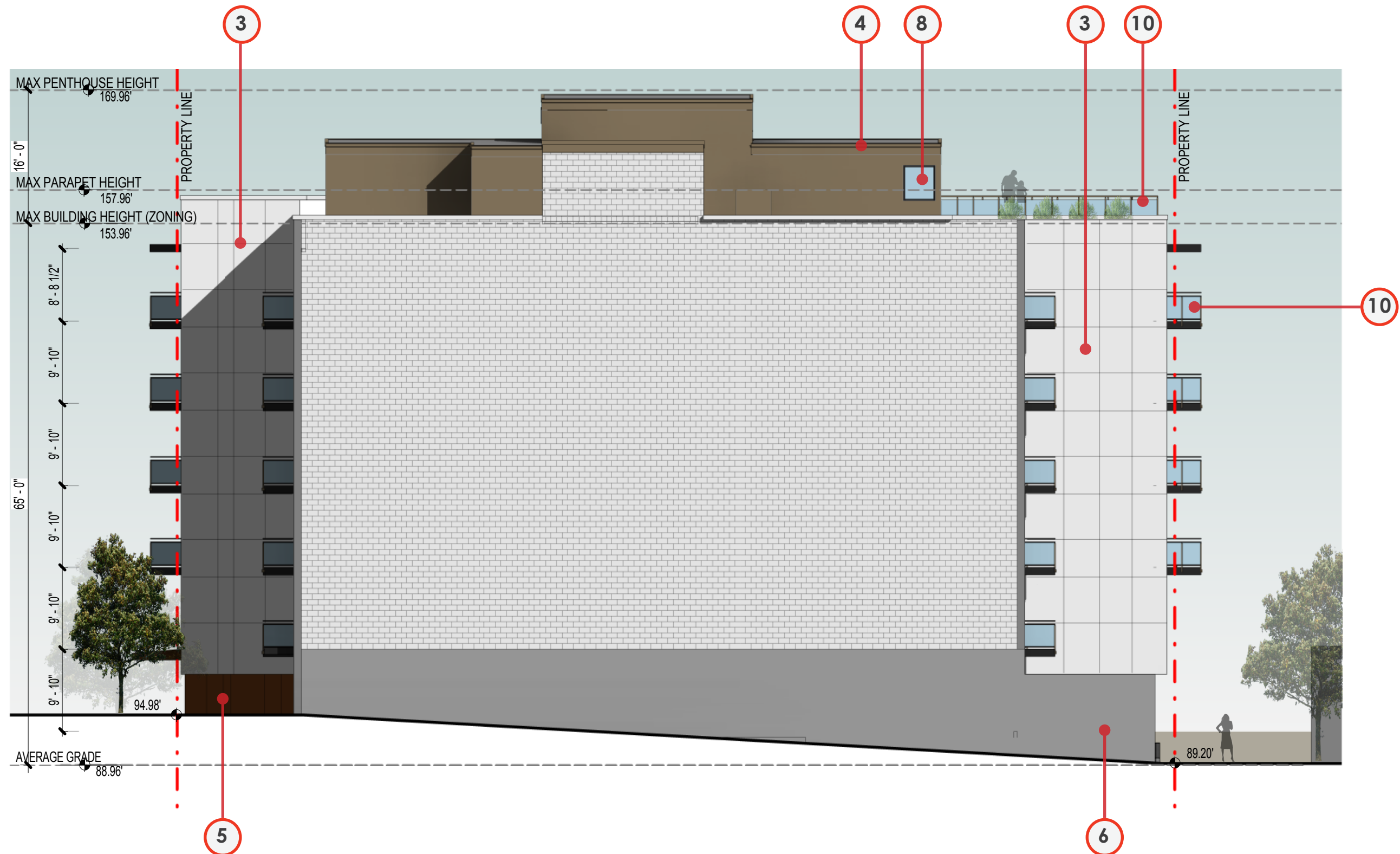


ROOF LEVEL





EAST ELEVATION



NORTH ELEVATION

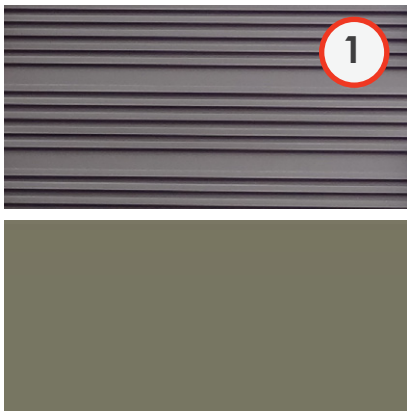


WEST ELEVATION



SOUTH ELEVATION

MATERIAL PALETTE



AEP SPAN Metal Panel
Installed View (top)
Color: Vintage (bottom)



AEP SPAN Metal Panel
Profiles (not for color)



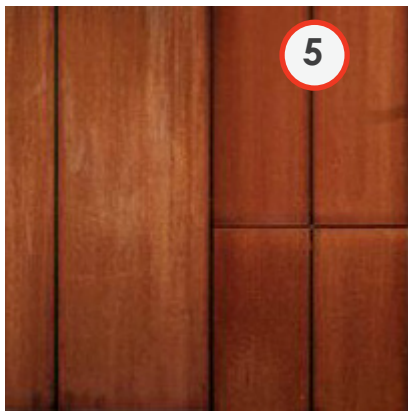
Pacific Clay
Dark Iron Spot



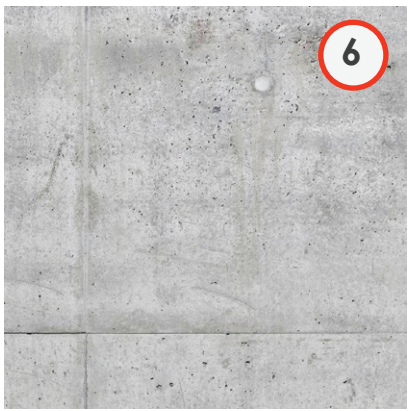
Benjamin Moore
858 Athena
Fiber Cement Panel



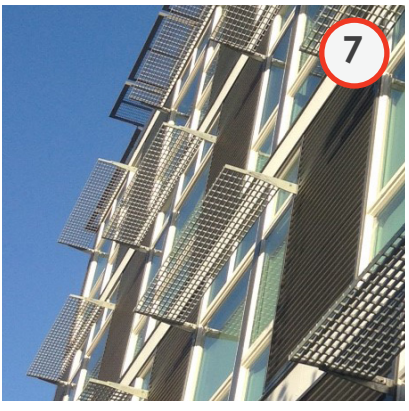
Benjamin Moore
HC 103 Cromwell Grey
Fiber Cement Panel



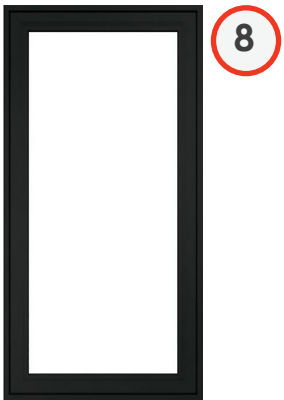
Weathering Steel



Exposed Concrete



Sunshades



Vinyl Windows
Black Frame



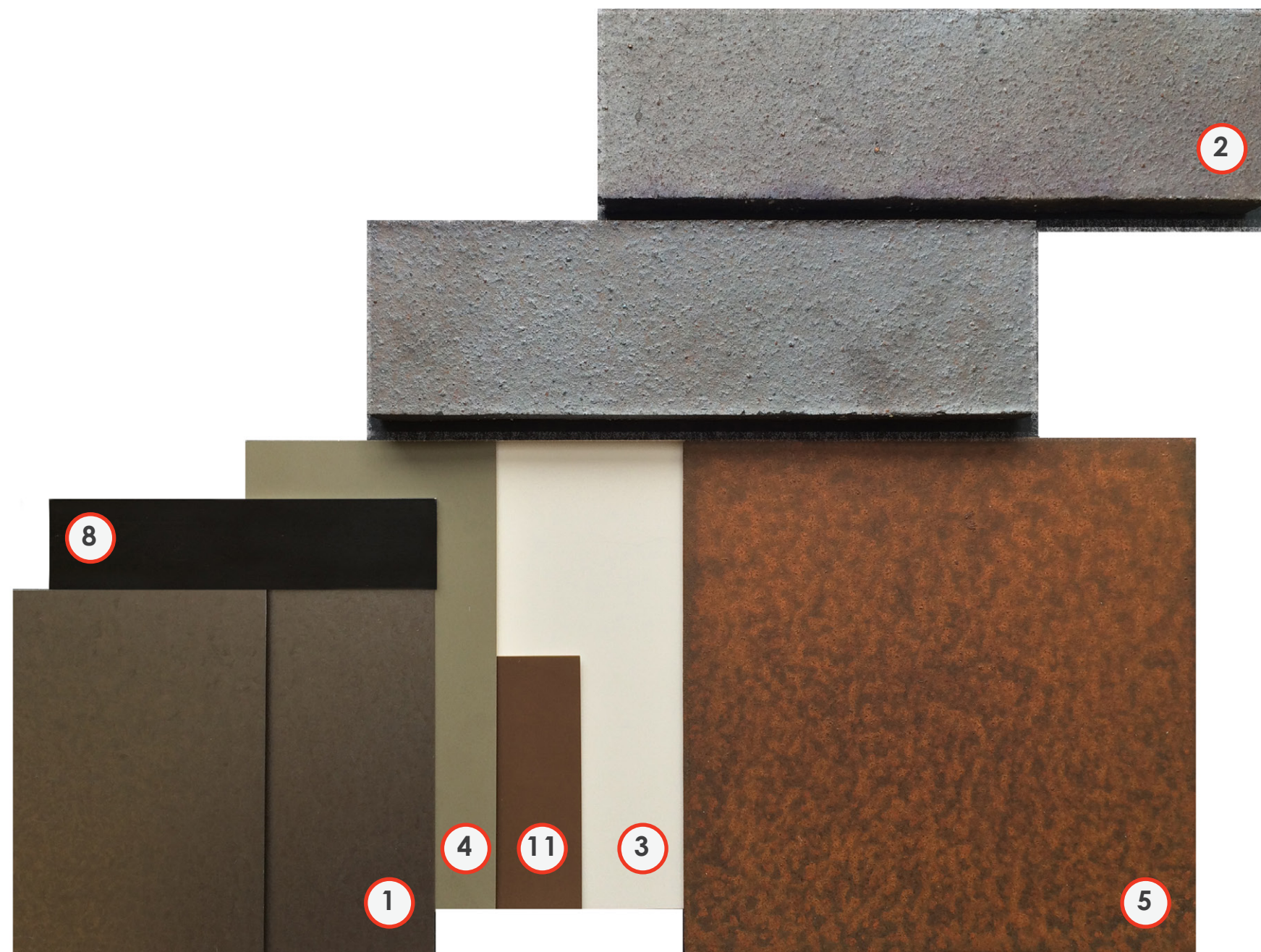
Aluminum Storefront
Black Anodized Frame



Metal and Glass Railing
Black



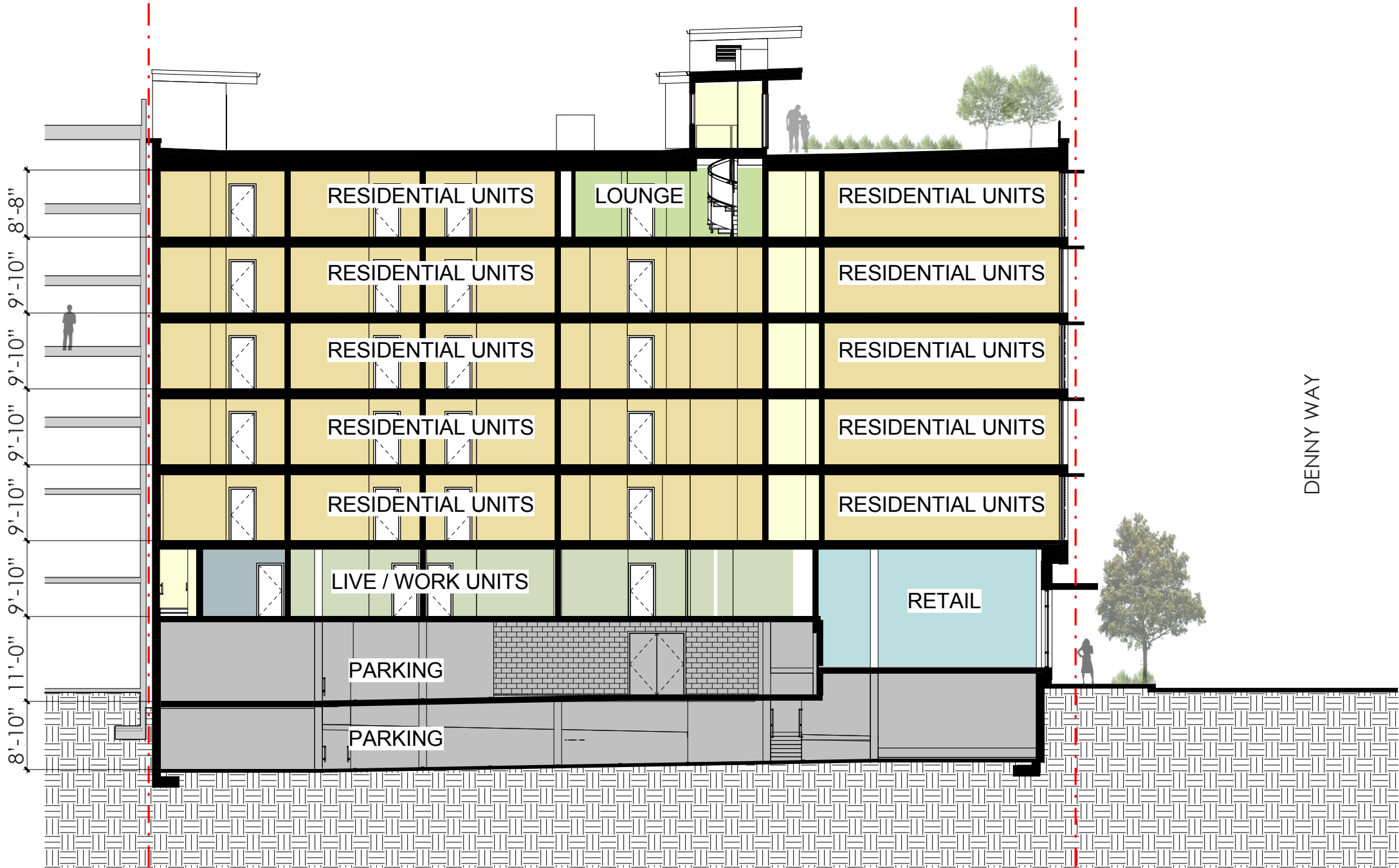
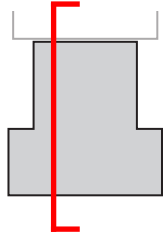
Benjamin Moore
1238 Falcon Brown
Fiber Cement Panel

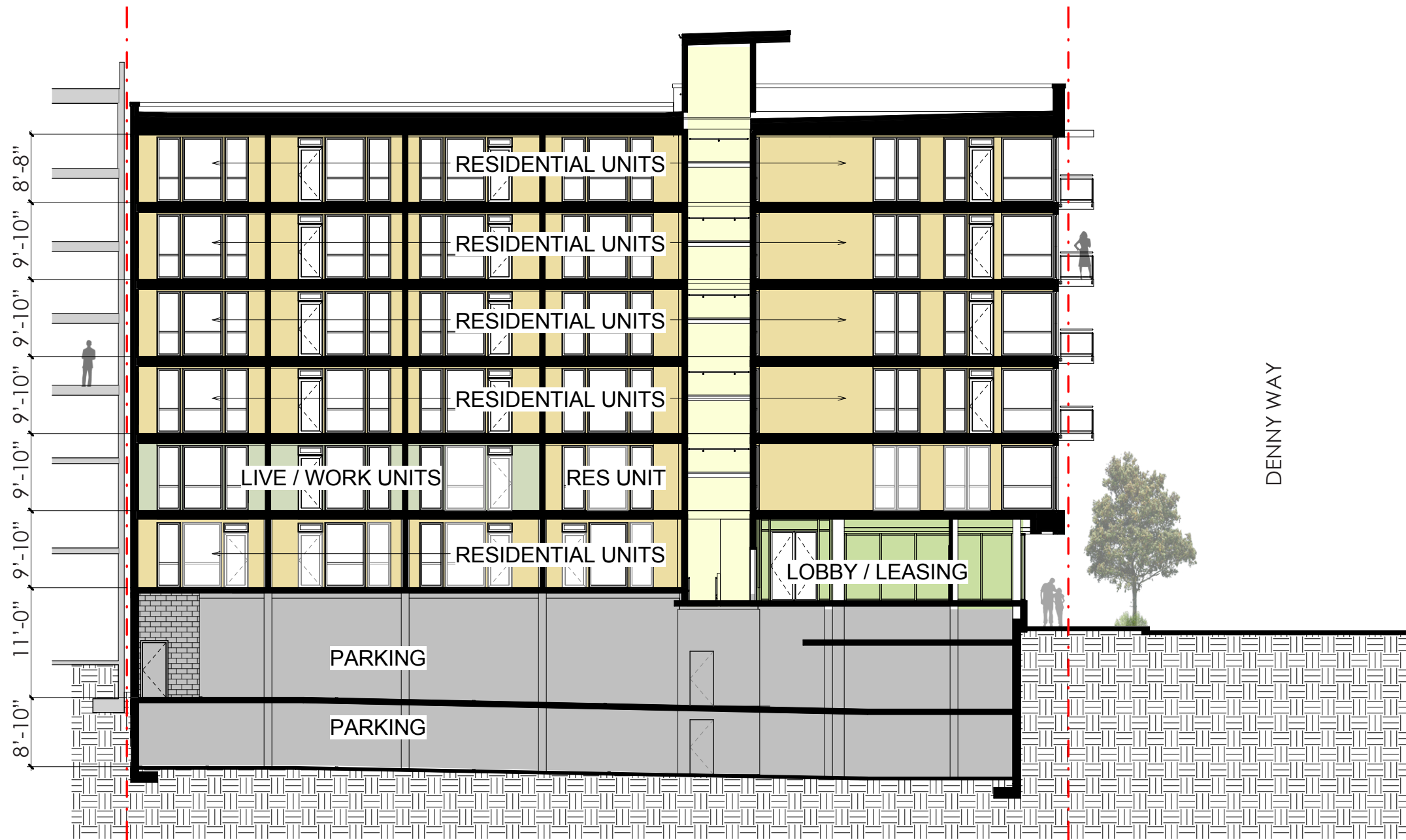


MATERIAL BOARD IMAGE

SECTION N-S

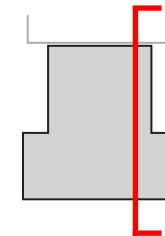
- residential
- parking
- mech/storage/elec
- live/work
- amenity
- retail
- common space





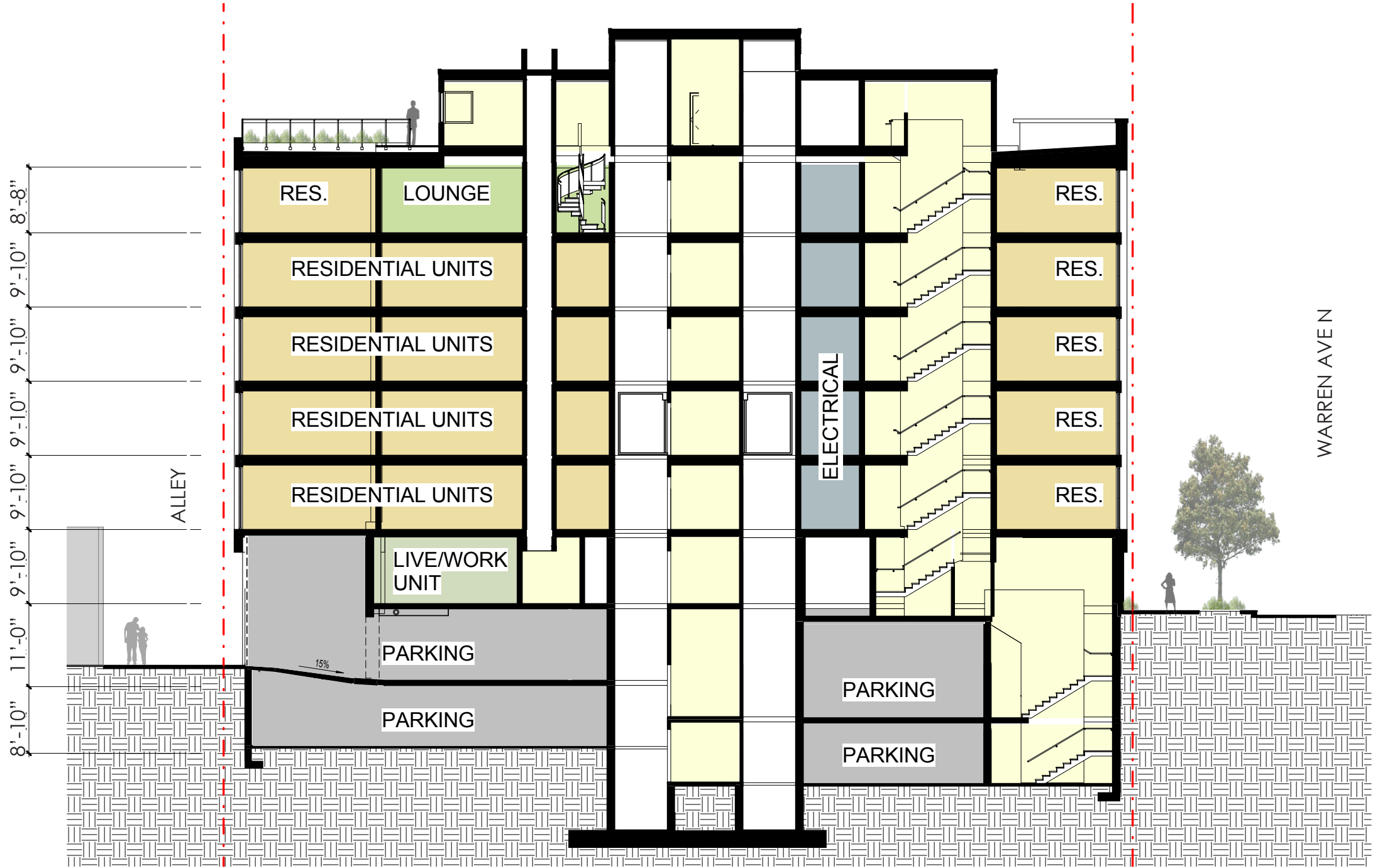
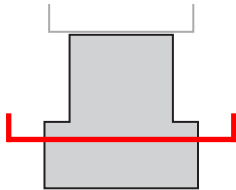
SECTION N-S

residential
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amenity
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common space



SECTION E-W

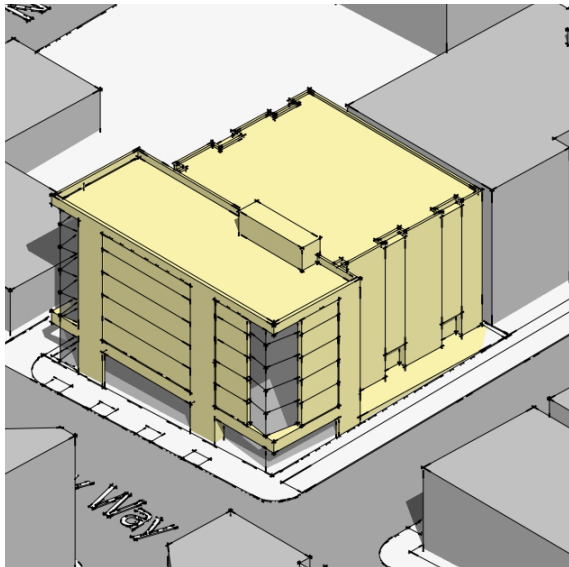
- residential
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- retail
- common space



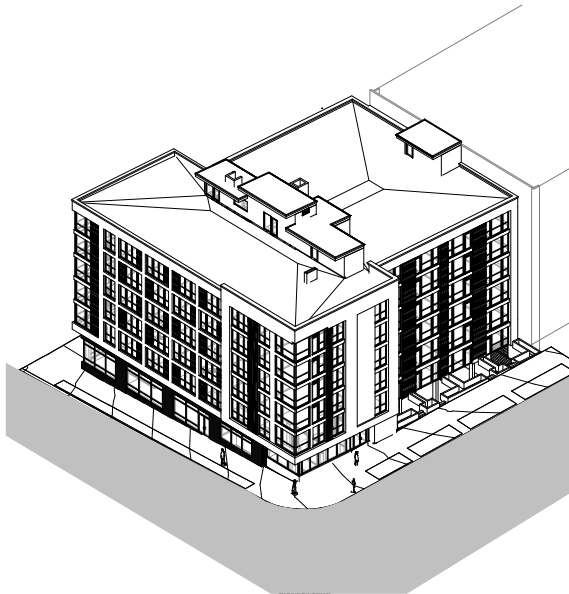
GUIDANCE:

BOARD SPECIFIC GUIDANCE: Keep the modulation as shown. The north portion of the structure seems more modulated for residential use.

1



EDG MASSING

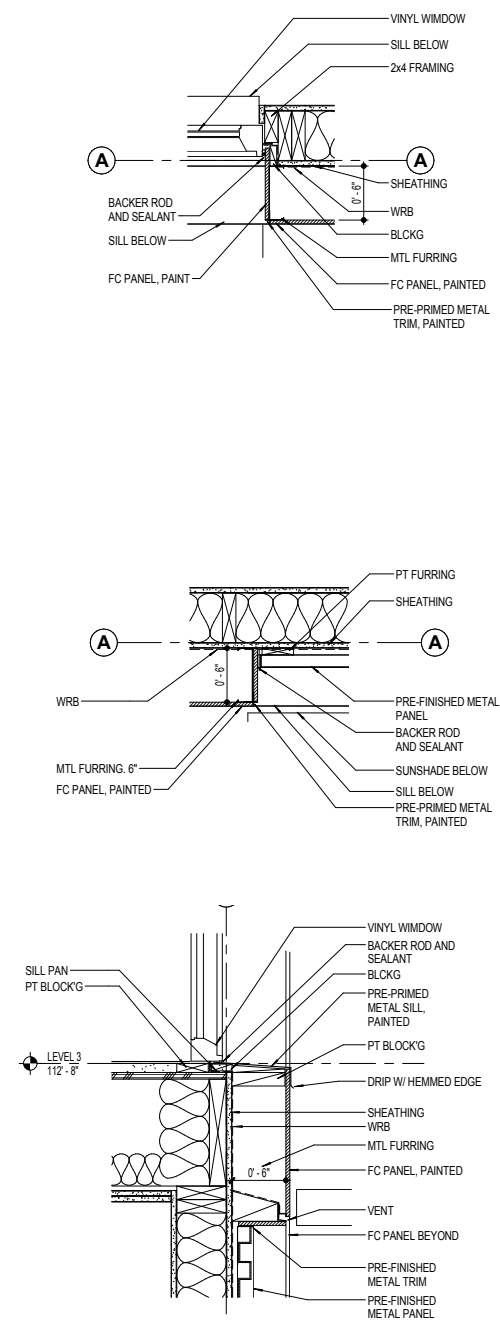


CURRENT MASSING

CONTEXT AND SITE: MASSING IN THE UPTOWN URBAN CHARACTER AREA



EAST ELEVATION



FACADE CONCEPT DETAILS



GUIDANCE:

BOARD SPECIFIC GUIDANCE: The massing of the preferred option is appropriate and relates well to the residential structure to the north.

3

CONTEXT AND SITE: EXISTING DEVELOPMENT AND ZONING



PROPOSED MASSING, SETBACKS AND FACADE ARTICULATION



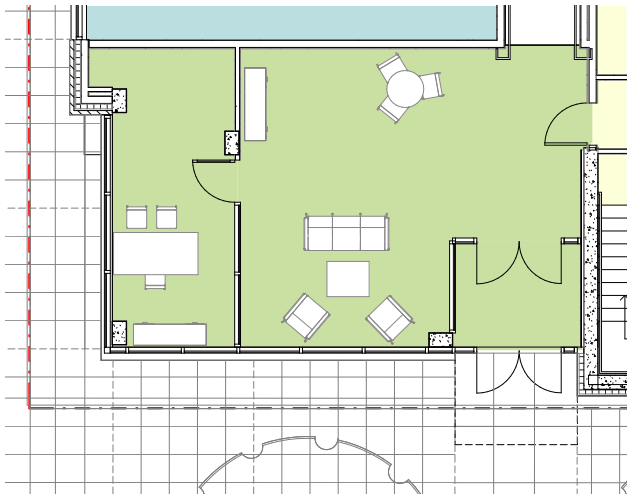
EXISTING BERNARD APARTMENTS

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GUIDANCE:

BOARD SPECIFIC GUIDANCE: The Board noted that given the shifting of the street grid at Denny the alley corner will be prominent, especially from Bay St. The corner treatment is very important and does not need to be treated the same as the corner at Warren Ave N.

3



SOUTHEAST CORNER - RESIDENTIAL ENTRY

CONTEXT AND SITE: CORNER SITES



SOUTHEAST CORNER - DENNY AND WARREN AVE N.



SOUTHWEST CORNER - DENNY AND ALLEY

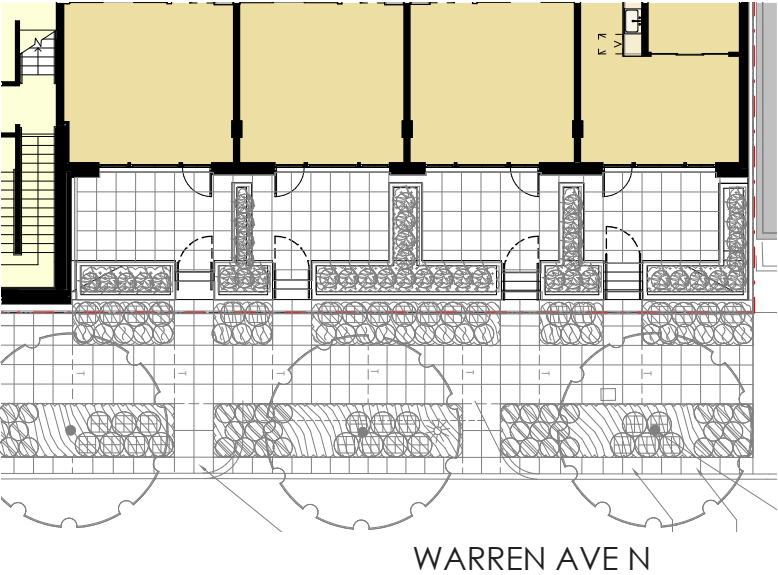
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GUIDANCE:

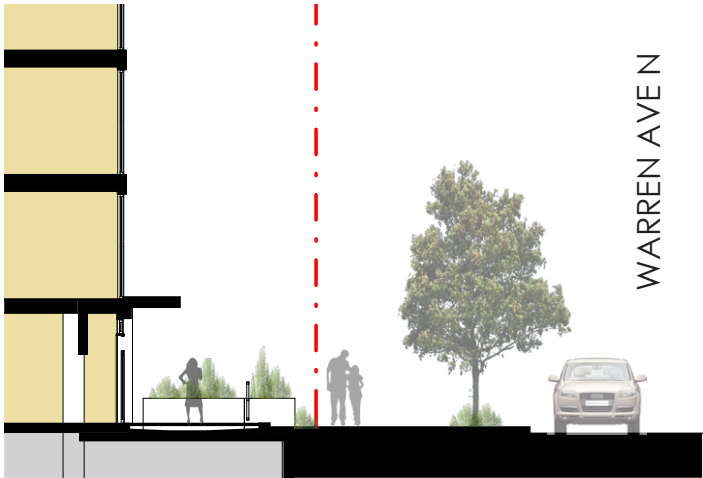
BOARD SPECIFIC GUIDANCE: Provide patios for the residential units off of Warren Ave. N. that will be primarily private. Maintain the 12' setback and use landscaping along Warren Ave N to provide a sense of protection and privacy for the residential entries.

4

PUBLIC LIFE: PEDESTRIAN FRIENDLY ENTRANCES / DEFENSIBLE SPACE



WARREN AVE N - PATIOS



WARREN AVE N - CANOPY AT RESIDENTIAL ENTRY



GATE AND LANDSCAPE AT WARREN PATIO



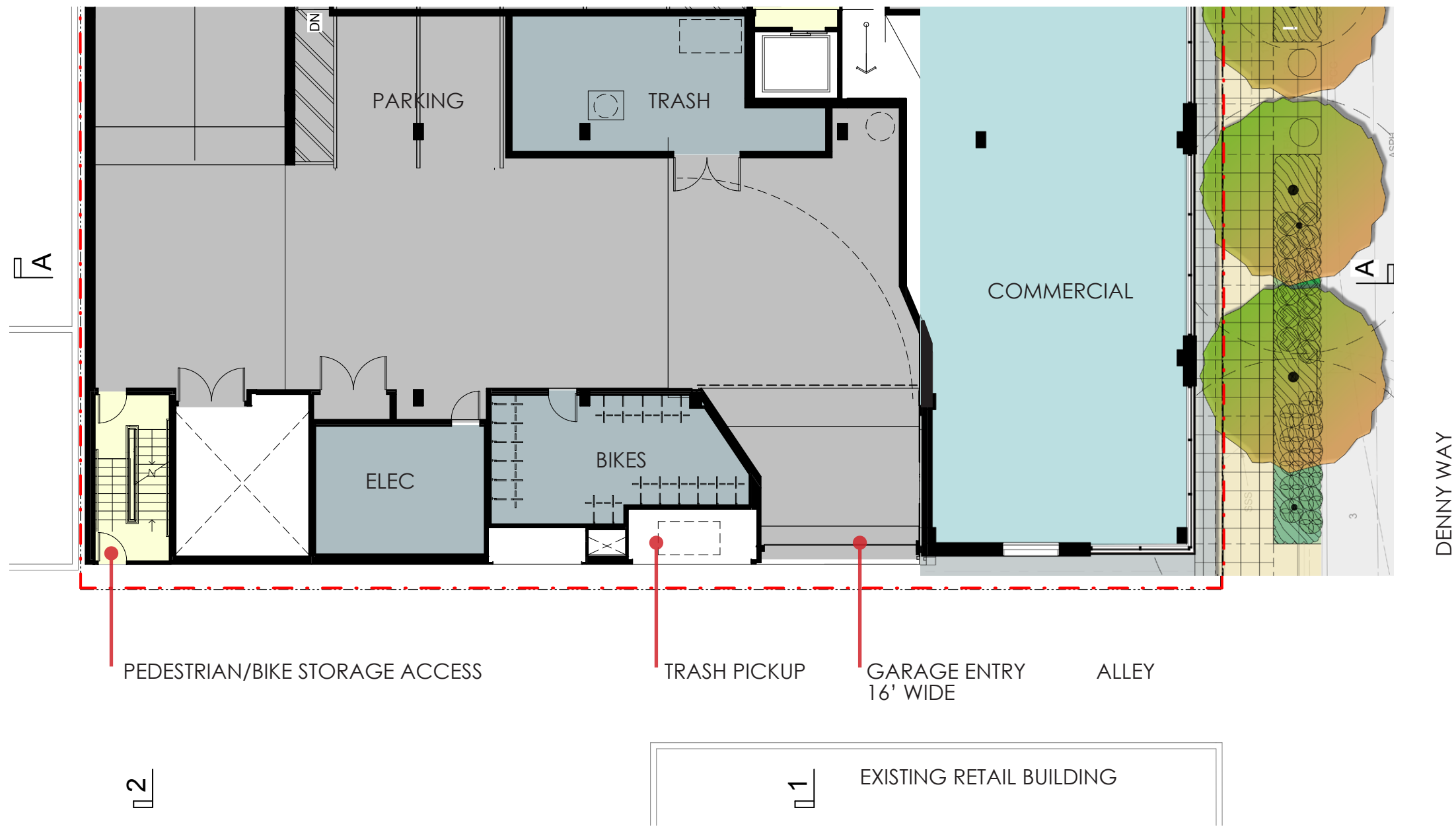
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GUIDANCE:

BOARD SPECIFIC GUIDANCE: Provide a staging area on the north side of the driveway for solid waste on collection day that will work for SPU and building staff. Design the driveway for safety of pedestrians and vehicles. Consider narrowing the width of the driveway. The Board encouraged the design of the patio units above the alley, provide security.

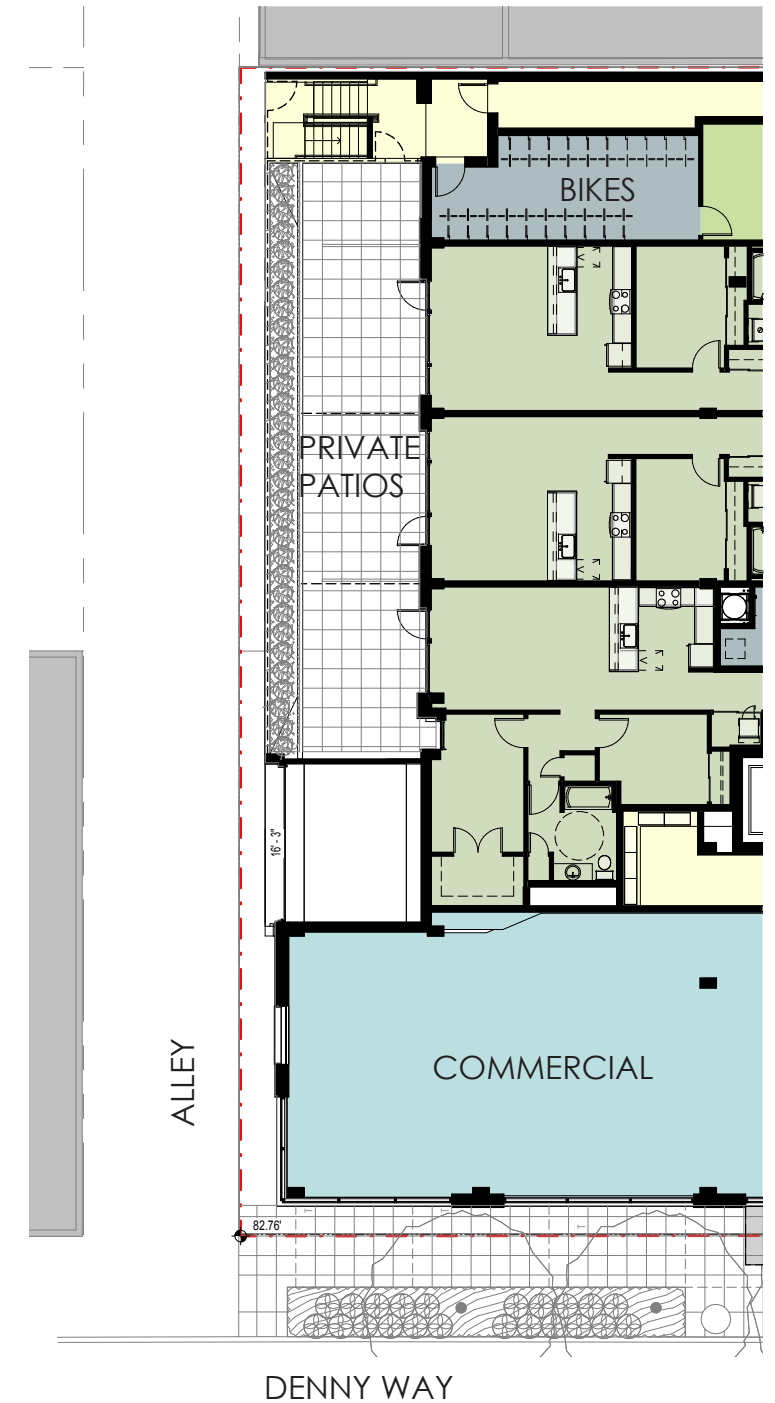
7

DESIGN CONCEPT: ACCESS LOCATION AND DESIGN / SERVICE USES





ALLEY VIEW FROM DENNY WAY



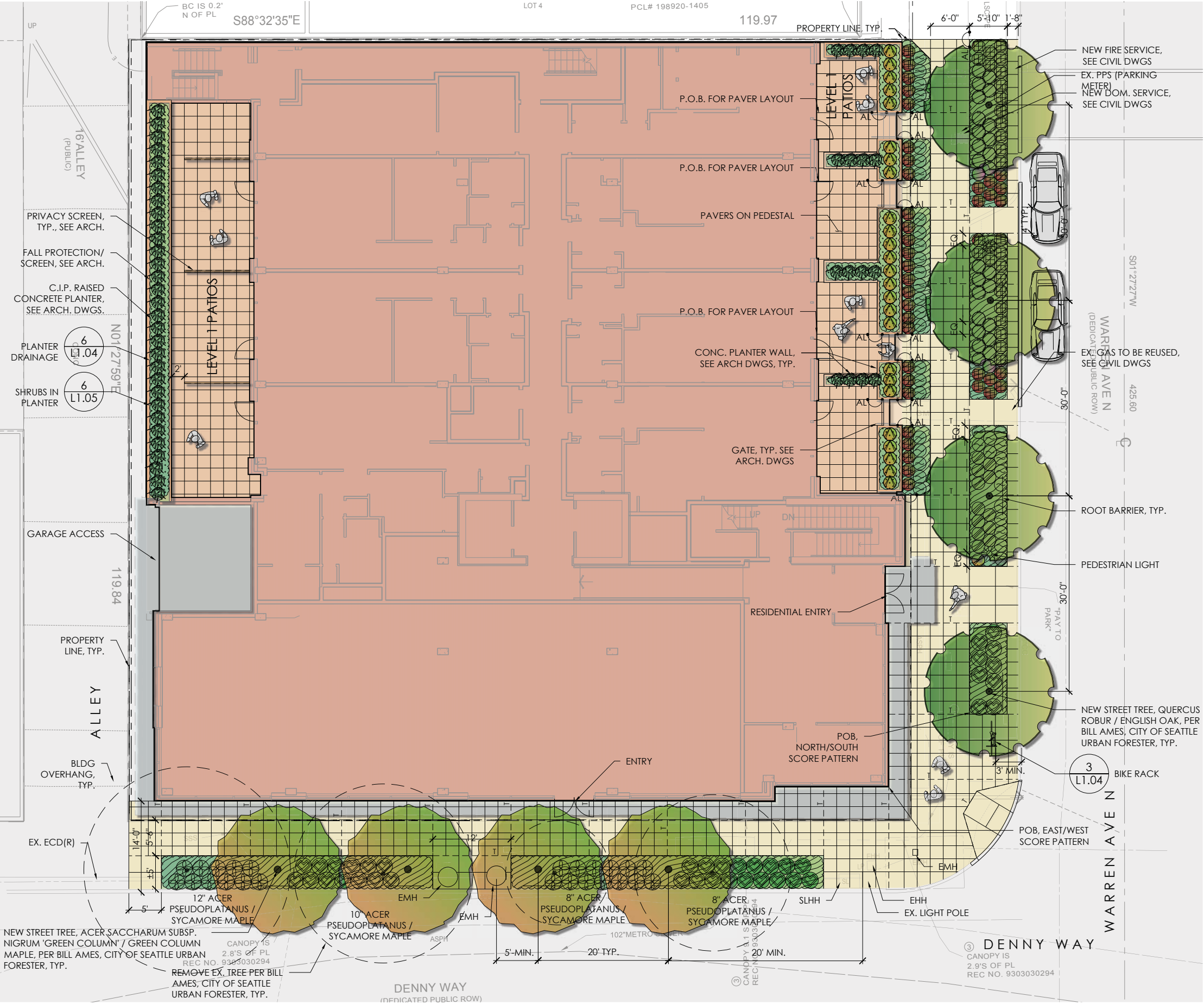
LANDSCAPE PLAN

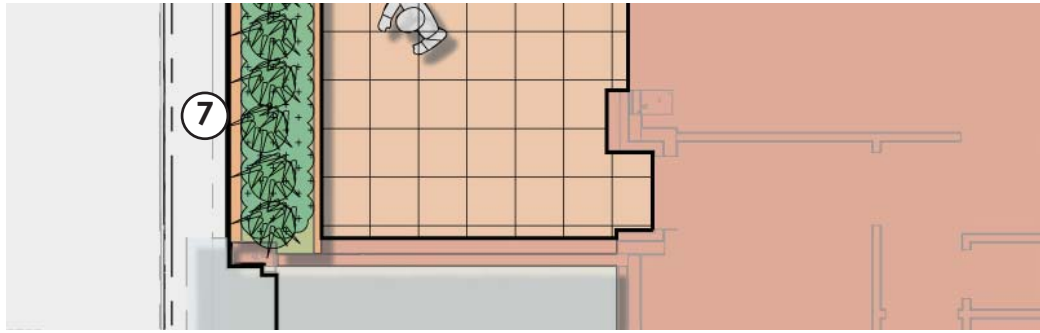
MATERIALS LIST - STREET LEVEL

- CONCRETE PAVING
PER COS STD. PLAN 420 W/ THE FOLLOWING EXCEPTIONS:
-SAND COATED EXPANSION JOINTS
-SAW CUT CONTROL JOINTS
- THROUGH JOINT

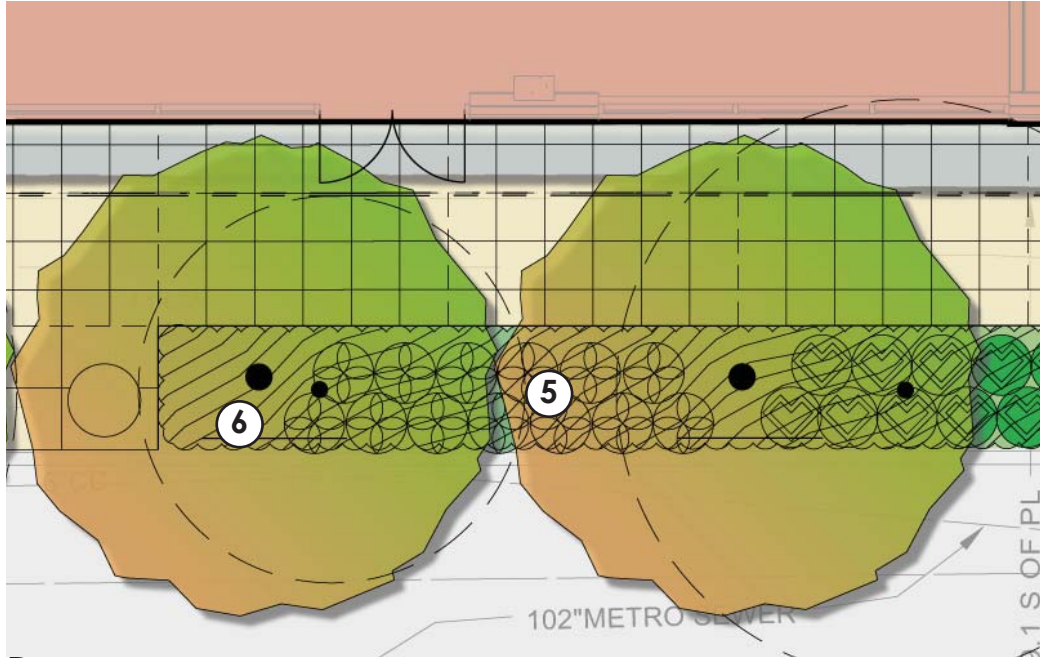
MATERIALS LIST - ON STRUCTURE

- CONCRETE PAVERS
24"x24" H1
W/APPIAN WAY PEDESTAL SYSTEM
COLOR/FINISH: TEXADA - NATURAL
BY ABBOTSFORD CONCRETE PRODUCTS 1.800.663.4091
- PEDESTAL SYSTEM
APPIAN WAY PEDESTAL SYSTEM
BY ABBOTSFORD CONCRETE PRODUCTS 1.800.663.4091

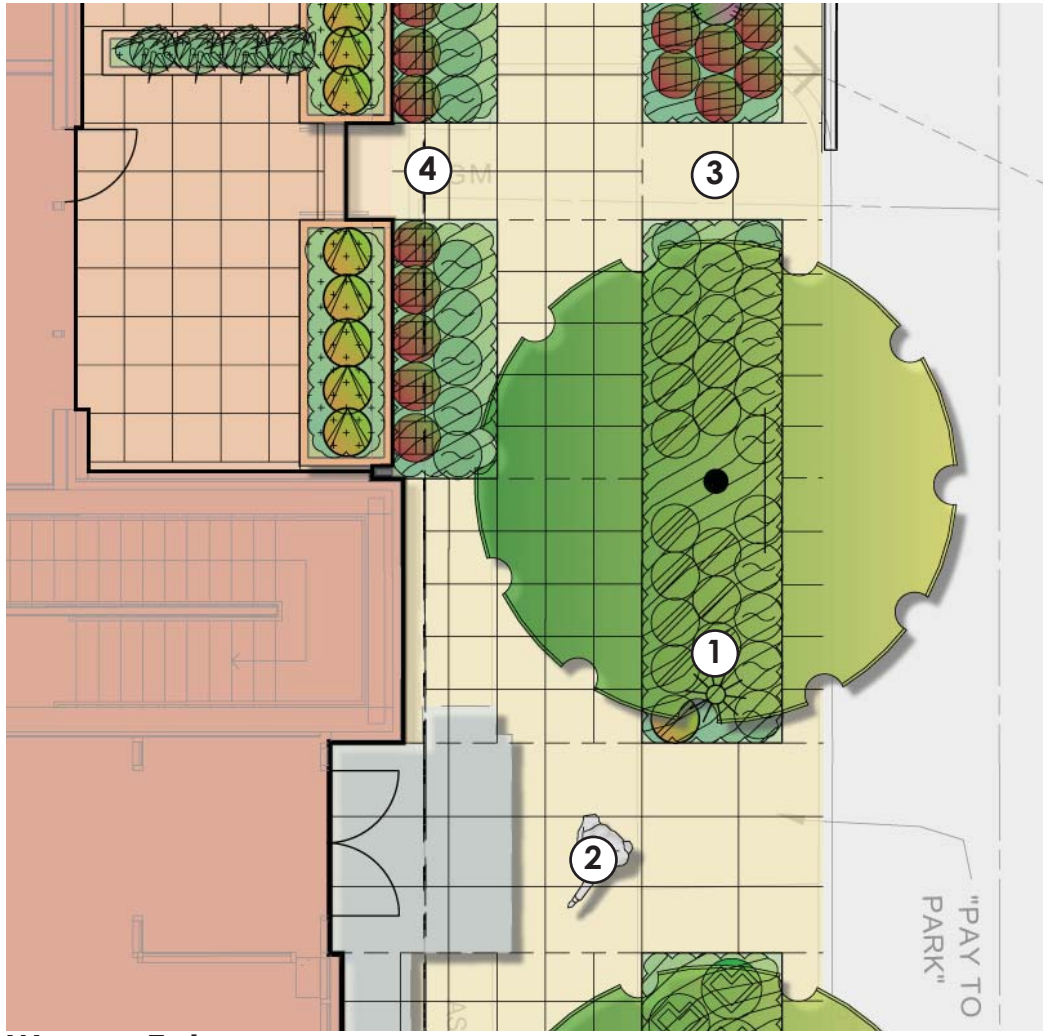




Alley



Denny



Warren Entry

DETAIL

Warren Entry

- 1 'Uptown' Pedestrian Light
- 2 Entry Pavement - 2x4 score pattern
- 3 Cut-Throughs
- 4 Gate, Steps and Planter

Denny

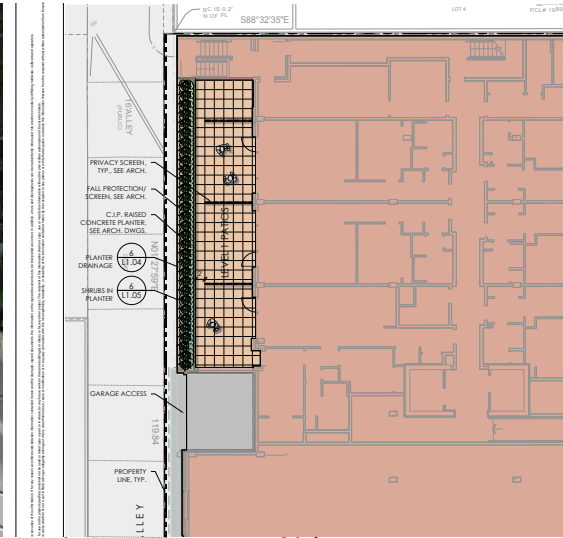
- 5 Lush Landscape Strip
- 6 Replace Street Trees

Alley

- 7 Wall, Landscape and Fence



Wall, Landscape and Fence



Lush Landscape Strip




Gate, Steps and Planter




'Uptown' Ped. Light

ROOF LANDSCAPE


MATERIALS LIST - ON STRUCTURE



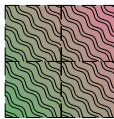
CONCRETE PAVERS
24"X24" HYDRAPRESSED SLABS
W/APPIAN WAY PEDESTAL SYSTEM
COLOR/FINISH: TEXADA - NATURAL
BY ABBOTSFORD CONCRETE PRODUCTS 1.800.663.4091




PEDESTAL SYSTEM
APPIAN WAY PEDESTAL SYSTEM
BY ABBOTSFORD CONCRETE PRODUCTS 1.800.663.4091




ROOF DECKING
WOOD: IPE




GREEN ROOF TRAYS
ADVANCED VEGETATIVE ROOF SYSTEM (AVRS)
24"X24" TRAYS BY COLUMBIA GREEN TECHNOLOGIES, INC. 503.684.9123.
INSTALL PER MFG. INSTRUCTION.




ME
METAL EDGING
GEOEDGE ALUMINUM RESTRAINT, AVAIL. FROM COLUMBIA GREEN TECHNOLOGIES, INC. 503.684.9123. INSTALL PER MFG. INSTRUCTION.




IC
IRRIGATION CONCEALMENT EDGING
AVRS EDGING - IRRIGATION CONCEALMENT, AVAIL. FROM COLUMBIA GREEN TECHNOLOGIES, INC. 503.684.9123. INSTALL PER MFG. INSTRUCTION.




ROOT BARRIER
18" H X 6' L
HIGH DENSITY POLYETHYLENE PLASTIC: 60-MIL THICK, WITH ABILITY TO PROVIDE ROOT BARRIER PROTECTION AT SEAMS, BY DEEPROOT, 800.458.7668, OR APPROVED EQUAL.



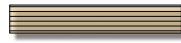
PEBBLE MULCH
7/8" WASHED DRAIN ROCK
AVAIL FROM MARENAKOS ROCK CENTER, 425.392.3313
2" MIN. DEPTH, 4" MAX. DEPTH, REF. LANDSCAPE DETAILS




PLANTERS
SQUARE PLANTER, FIBERGLASS
WILSHIRE COLLECTION,
48" L X 48" W X 36" HT
COLOR: FLAT BLACK, OR SUBMIT ALT. FOR OWNER APPROVAL BY TOURNESOL SITEWORKS 800.542.2282



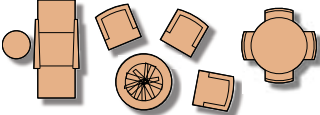
TROUGH PLANTER
8' L X 3' W X 24" MIN. H GALVANIZED STOCK TANK
W/ (3) 1/2" DIA. DRAINAGE HOLES IN BOTTOM



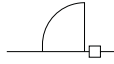
BENCH - ROOF
18" HT., FACE W/HORIZONTAL BOARDS,
MATERIAL: IPE




PEDESTRIAN LIGHT
TBD



SITE FURNITURE
OWNER FURNISHED



FENCE / GATE
SEE ARCH. DWGS.

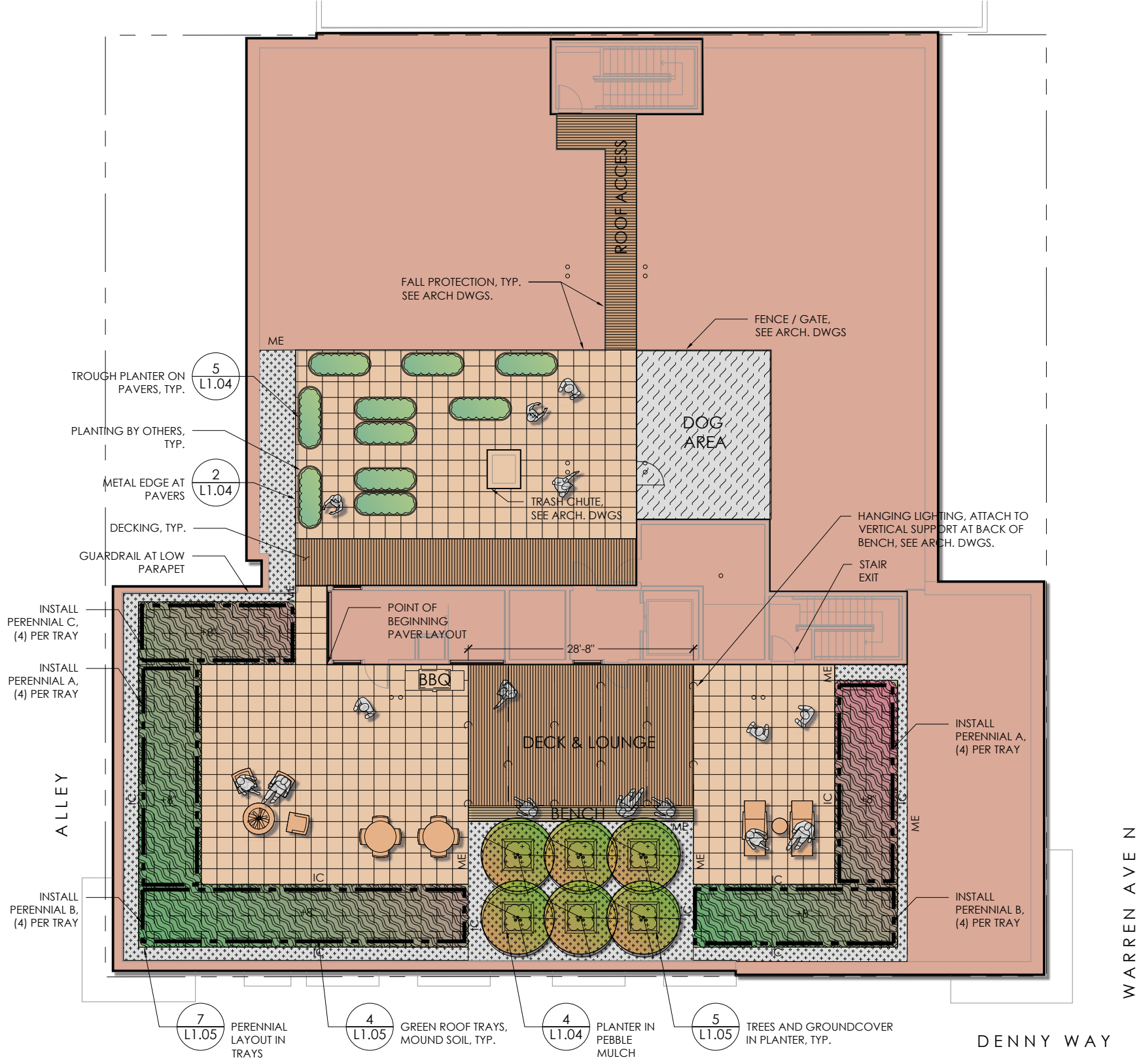


DOG AREA
SYNTHETIC TURF: K9 GRASS (CLASSIC) BY FOREVERLAWN 866.992.7876

DRAINAGE MAT: AIRDRAIN GEOCEL
BY AIRFIELD SYSTEMS LLC, 405.359.37

INSTALL SYNTHETIC TURF, OVER DRAIN PAVERS W/ WOOD SLEEPERS ATTACHED

HANGING LIGHTS



PLANTS



Green Column Maple
Acer 'Green Column'



English Oak
Quercus robur



Green Japanese Maple
Acer palmatum



Sweet box
Sarcococca confusa



Compact Japanese Holly
Ilex crenata 'Compacta'



Moon Bay Nandina
Nandina domestica 'Moon Bay'



'Cavatine' Pieris
Pieris 'Cavatine'



'Hino Crimson' Azalea
Rhododendron 'Hino Crimson'



Anthony Waterer Spirea
Spiraea 'Anthony Waterer'



David's Viburnum
Viburnum davidii



Creeping Lilyturf
Liriope spicata

PLANT LIST

SYMBOL	BOTANICAL NAME	COMMON NAME
STREET TREES		
	ACER SACCHARUM SUBSP. NIGRUM 'GREEN COLUMN' * (SPECIES APPROVED BY SDOT ARBORIST BILL AMES, 9.12.2014)	GREEN COLUMN MAPLE
	QUERCUS ROBUR * (SPECIES APPROVED BY SDOT ARBORIST BILL AMES, 9.12.2014)	ENGLISH OAK
ON-SITE TREES		
	ACER PALMATUM	JAPANESE MAPLE (GREEN)
SHRUBS		
	CORNUS STOLONIFERA 'KELSEYI' *	'KELSEYI' RED TWIG DOGWOOD
	ILEX CRENATA 'COMPACTA'	COMPACT JAPANESE HOLLY
	NANDINA 'MOON BAY'	'MOON BAY' HEAVENLY BAMBOO
	PHYLLOSTACHYS AUREA	GOLDEN BAMBOO
	PIERIS JAPONICA 'CAVATINE' **	'CAVATINE' JAPANESE PIERIS
	SARCOCOCCA CONFUSA	CONFUSED SARCOCOCCA
	SHIBATAE KUMASACA	SHIBATAE BAMBOO
	VIBURNUM DAVIDII	DAVID'S VIBURNUM
GROUND COVER MIXES		
	50% EPIMEDIUM x PERRALCHICUM 'WISLEY' **	50% 'WISLEY' EPIMEDIUM
	25% POLYSTICHUM MUNITUM **	25% SWORD FERN
	25% ASTILBE X AREDSII 'PEACH BLOSSOM'	25 'PEACH BLOSSOM' ASTILBE
	OPHIOPOGON P. 'NIGRESCENS' ** (PATIOS & ROOF)	BLACK MONDO GRASS
GREEN ROOF PERENNIALS		
	PERENNIAL A RUDBECKIA FULGIDA VAR. SULLIVANTI 'GOLDSTURM' **	BLACK-EYED SUSAN
	PERENNIAL B SEDUM 'AUTUMN JOY' **	STONECROP
	PERENNIAL C STIPA TENUISSIMA **	MEXICAN FEATHER GRASS

DESIGN DEPARTURE #1

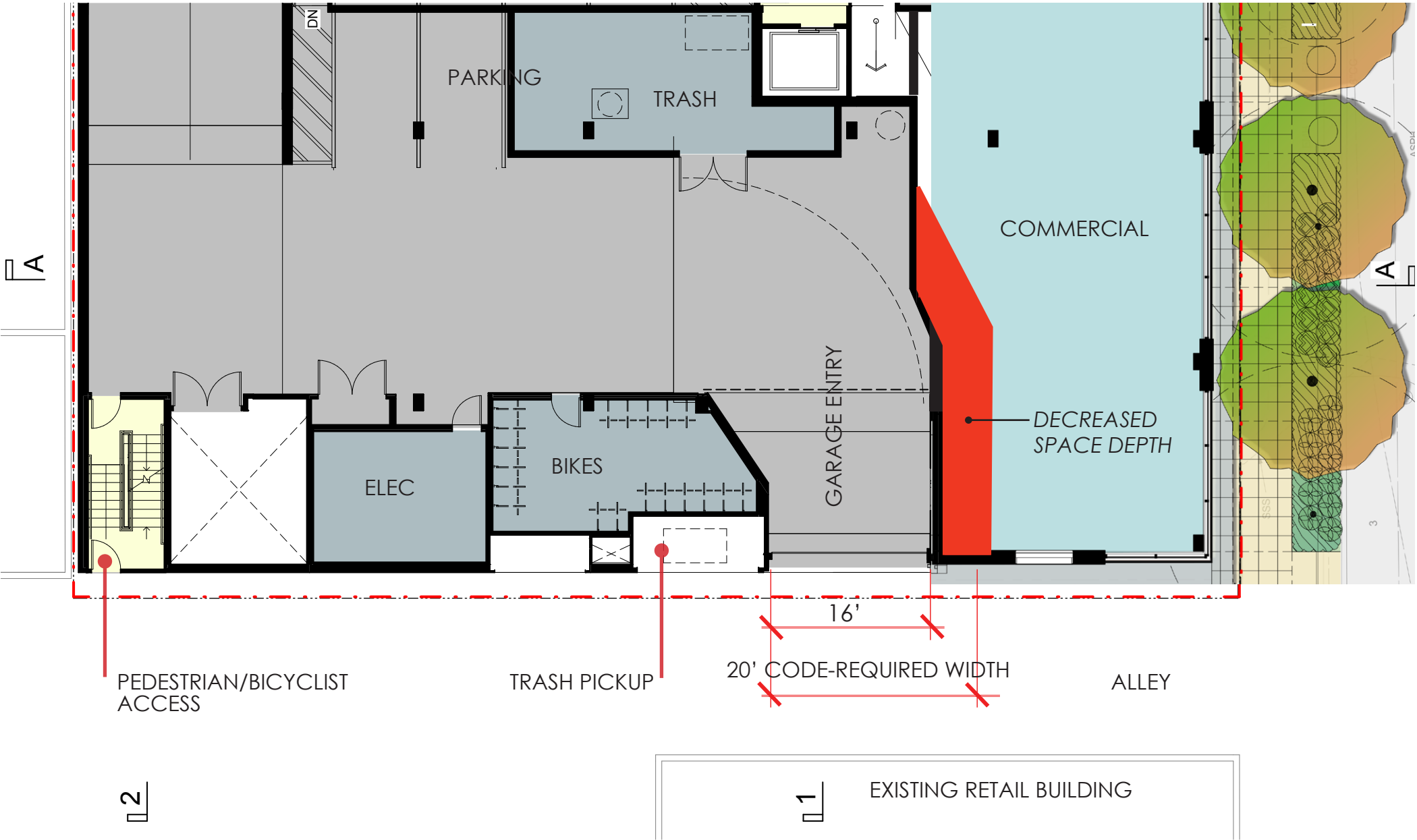
CODE CITATION/REQUIREMENT: 23.54.030.D.1.c - Driveways for residential use, of any length that serve more than 30 parking stalls shall be at least 10 feet wide for one-way traffic and at least 20 feet wide for two-way traffic.

PROPOSED DEPARTURE: Allow the parking entry ramp to decrease from the required 20' width to 16' width.

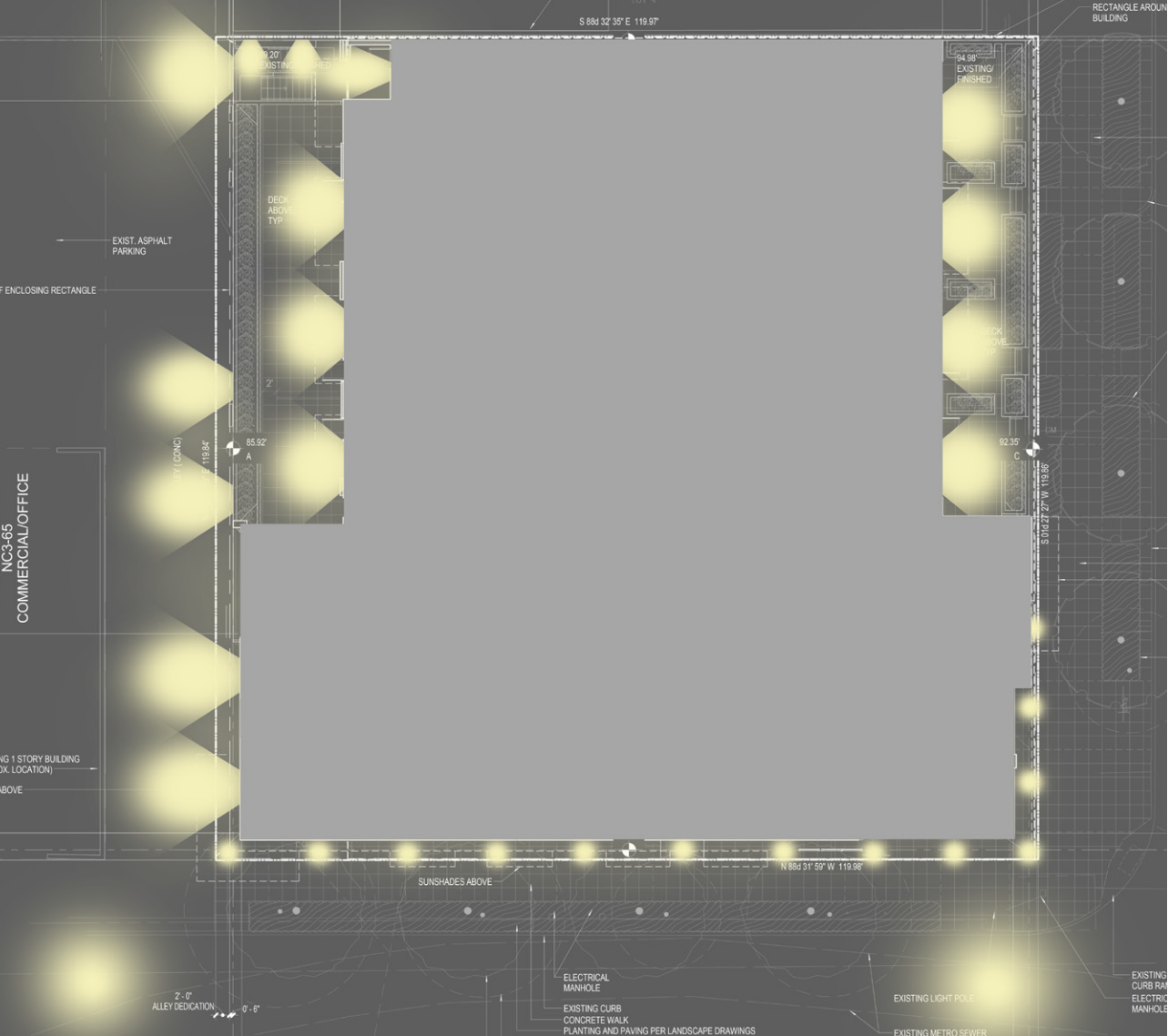
RATIONALE: Granting this departure keeps the massing concept of the north and south bars architecturally consistent. Widening the driveway would disrupt the composition of the west elevation, and create concept inconsistencies (DC2-B-1). Also, the retail space depth of an average of 30' is provided for with the reduced driveway width.

The site is located in the Uptown Urban Center, and is not required to provide commercial or residential parking. The parking is provided to support the project, and will decrease the amount of street parking utilized. A separate alley access has been provided for bicyclists and pedestrians which directly access the level 1 floor.

EDG COMMENTS:
The Board indicated they are inclined to grant this departure and noted a width of 10' may be supported to increase the depth of the retail space. By narrowing the driveway width the retail space off of Denny Way can have greater depth and will not need a departure. (PL2.I.ii)

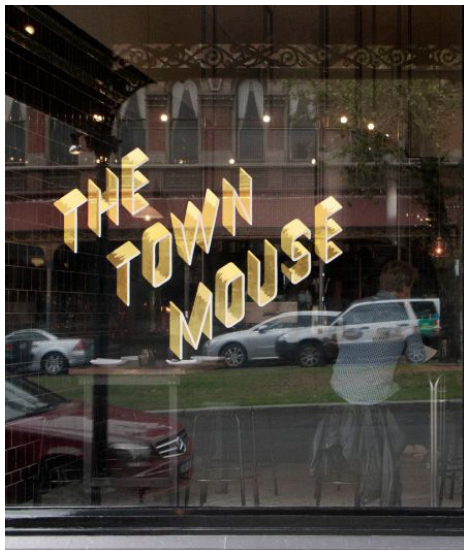


EXTERIOR LIGHTING PLAN
AND DESIGN ELEMENTS





SIGNAGE CONCEPTS



SHADOW STUDY

The project will not significantly impact the surrounding structures.

