



EARLY DESIGN GUIDANCE

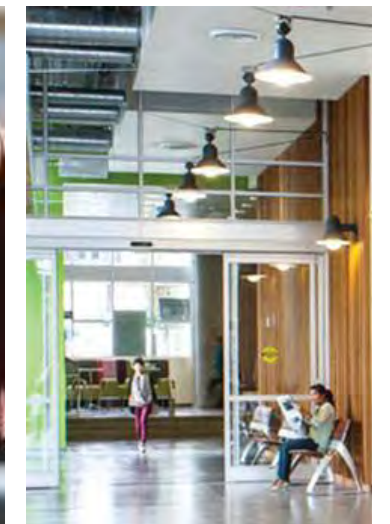
AMERICAN CAMPUS COMMUNITIES

4555 ROOSEVELT WAY NE
SEATTLE, WA 98126

DPD NUMBER: 30155133

VISION

- To create a unique off campus living environment for UW students that provides a variety of apartment choices and amenities.
- To provide a more substantial building on the site of an existing parking garage, Trader Joe's, and office building that respects the design guidelines set forth by the University District and the City of Seattle.
- To strengthen the urban nature of the site by creating street edges, and residential housing provides new life and vitality to the area, making the transition to the adjacent neighborhood more residential and safer.
- To strengthen the designated "Gateway" intersection of Roosevelt Avenue and 45th Street.
- To provide a common entry plaza for Trader Joe's, the existing parking garage and the proposed residential housing on Roosevelt Avenue.
- To capture the views of the Seattle skyline to the south and the views to UW to the east.



TEAM

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PROJECT

The existing site is located at 4555 Roosevelt Way NE. It is a half-block site and is bound by 9th Ave NE on the west, North east 47th Street on the North, Roosevelt Way NE on the east and the Landmarks Metro Cinema Complex to the south. There is currently a 32,000 sf 2 story office/retail building with a parking garage and at grade parking located on the site. The lower retail space is leased by Trader Joes and various other office uses exist in the remaining space. It is intended that the Trader Joes remain in operation during construction.

There are a variety of uses surrounding the site, including retail, restaurant, and both multi-and single-family residential. There are 3 newly planned multi-family developments that are expected to be completed within the next year; the AVA U-District development, the Marriot Hotel and the Bridges @ 11th housing development directly to the east.

Number of Residential Units: Approximately 168 Total

Number of Parking Spaces: Approximately 150 New, 110 Existing. 260 Total

Gross Area: Approximately 176,316 Square feet

TABLE OF CONTENTS

4

SITE ANALYSIS AND CONTEXT

University District Community
Site Environment
Aerial photo & Neighborhood
Architectural Context
Site Orientation & Views

16

LAND USE CONSIDERATIONS

Allowable Zoning Envelope
Zoning Summary
Design Guidelines

23

MASSING OPTIONS

Option 1
Option 2
Option 3 - Preferred

28

PREFERRED OPTION

Landscape Character
Architectural Character
Concept Sketches
Code Departures

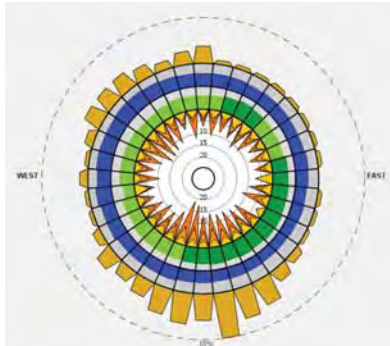
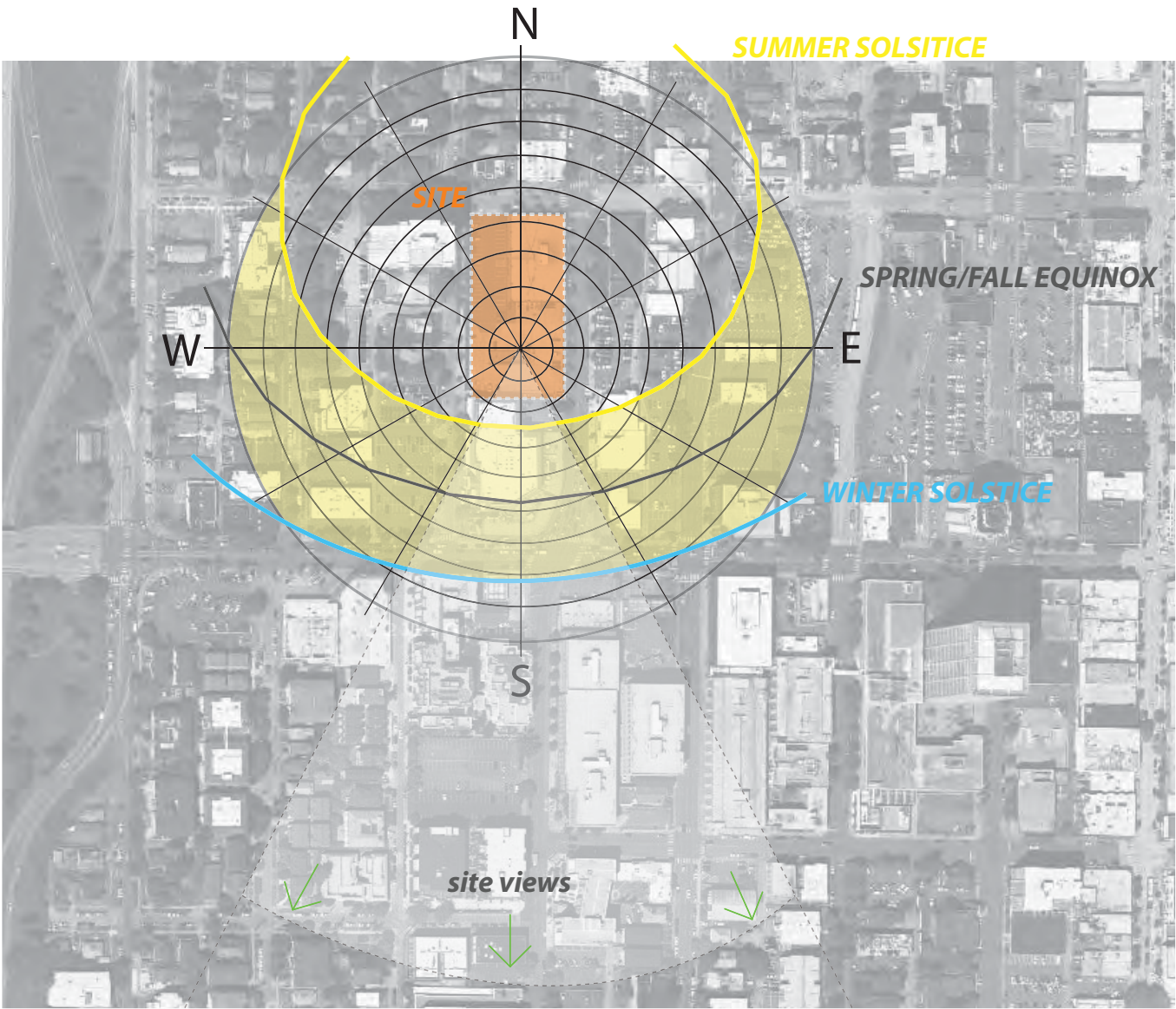
NEIGHBORHOOD CONNECTIVITY



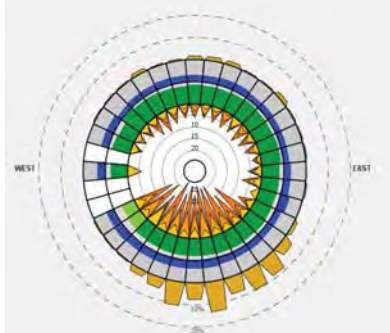
URBAN VILLAGES AND ZONING



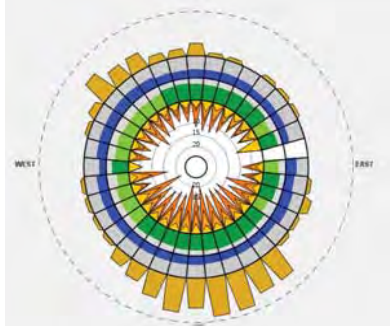
SITE ENVIRONMENT



JUNE WIND DIRECTION



DECEMBER WIND DIRECTION



MARCH WIND DIRECTION



Sun, Wind and Light

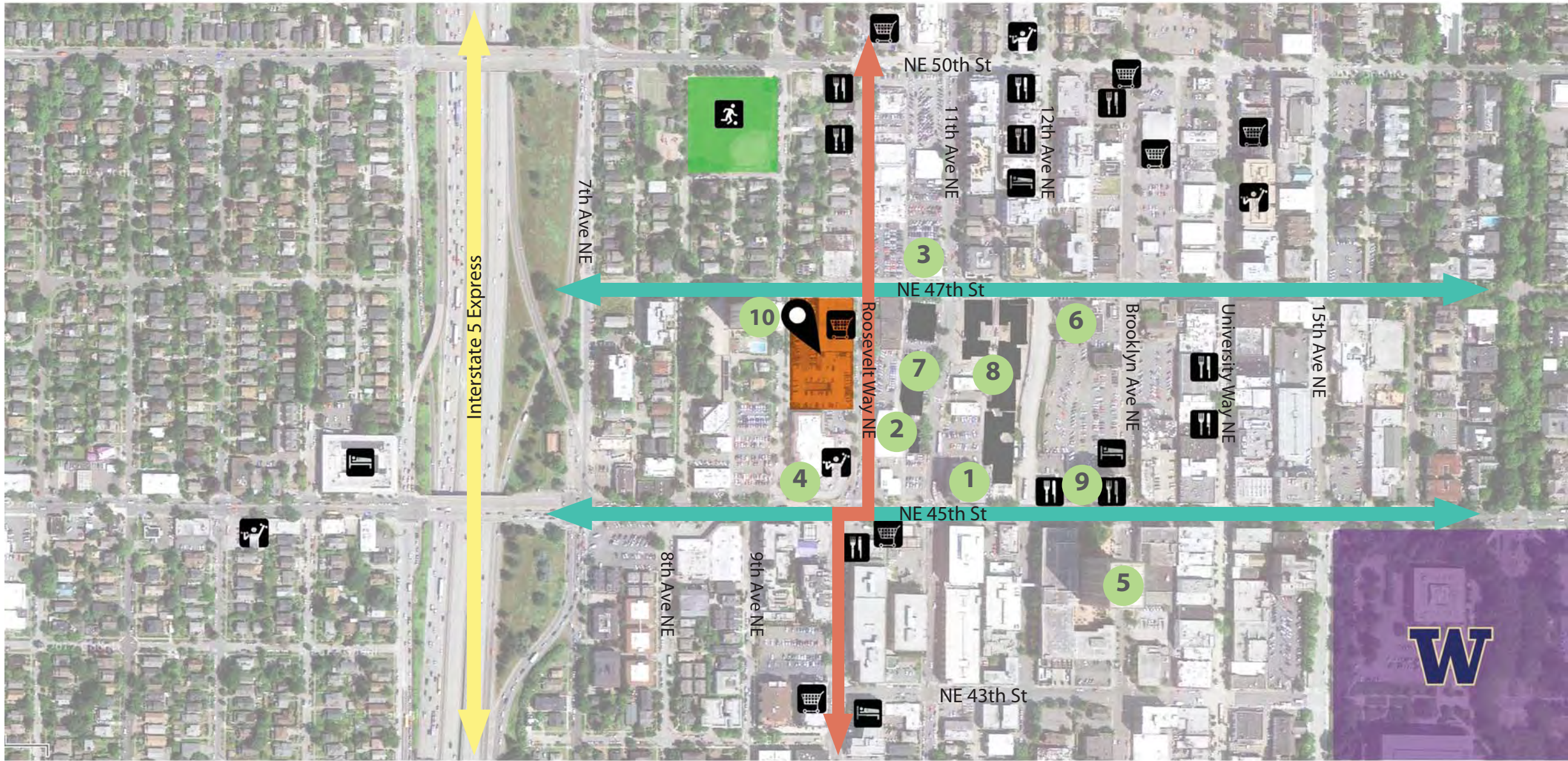
The site lies on predominantly a north/south axis which allows for full sun exposure on the east and the west facades throughout the year. Solar exposure is typical for greater Seattle region; predominantly overcast/cloudy skies with intermittent rainfall for much of the year except June through October. Daylight hours are long in summer and short in winter. Sunrise and sunset are north of the horizon in the summer and south of the horizon in the winter with higher sun angles in the summer. Adjacent properties, if fully developed in the future may cast shadows on the project site in the late afternoons.

The prevailing winds for the summer months are strongest coming from the southwest direction but more consistent from an almost direct southern direction and from the northwest direction. They provide dry cooling breezes typically. The prevailing winds in the winter months are strongest and most consistent coming from the south/southeastern. They tend to be associated with heavy rainfall.

Site, Slope, Tree Canopy

The site is pre-developed and slopes from an elevation of approximately 215.00' at the northwest corner, to 191.00' at the southeast corner. The existing concrete building structure is proposed to remain, along with most existing street trees and pedestrian paving in the ROW. There are several existing street trees that are dead or dying and will need to be replaced as part of the ROW street improvements. There are no steep slopes or any other critical areas on site.

AERIAL PHOTO & NEIGHBORHOOD



Site

Restaurant

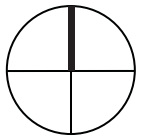
Grocery

Hotel

Fitness Center

Recreation

1 Architectural Context (see following page)



ARCHITECTURAL CONTEXT

The site is located on the corner of Roosevelt Way NE and NE 47th Street in the heart of the University District.

The site is surrounded by a variety of housing, retail, automotive, commercial and grocery establishments. The site is within walking distance to amenities and to the University of Washington. Although the site is located along the busy Roosevelt Way NE Street, the surrounding area is pedestrian friendly.

Future developments include “Bridges @ 11th”, “AVA University District” and “Marriott Hotel” which are located to the East of the site. The surrounding context provides a mixture of low and high rise buildings that allow for a pedestrian oriented environment along the street level. The American Campus Community project fits in well with the surrounding context.



1 Office Building



2 Mazda Dealer



3 University Audi



4 Performance Bicycle



5 UW Tower



6 Mars Hill Church



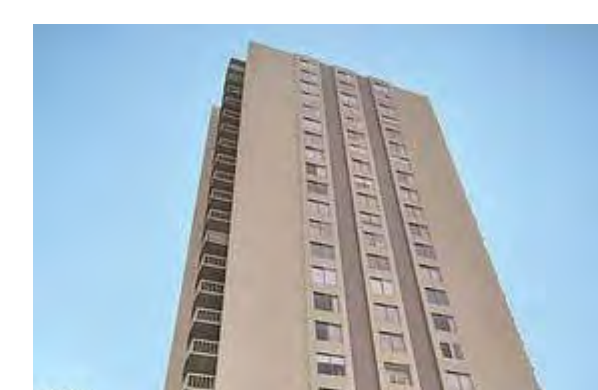
7 Housing- Bridges @ 11th



8 Housing- AVA U-District



9 Hotel Deca



10 Condo- Housing

CONTEXT MAP



 **EXISTING SITE**
(see following page)

1 4 STORY RESIDENTIAL PROJECT
PROJECT UNDER CONSTRUCTION

2 BRIDGES @ 11TH - GGLO PROJECT UNDER CONSTRUCTION

3 AVA UNIVERSITY DISTRICT
GGLO PROJECT UNDER CONSTRUCTION

EXISTING SITE PHOTOS



A : LOOKING SOUTH on Roosevelt Way



B : LOOKING SOUTH on 9th Ave



C : LOOKING NORTH on Roosevelt Way



D : LOOKING WEST on NE 45th St

STREET VIEW



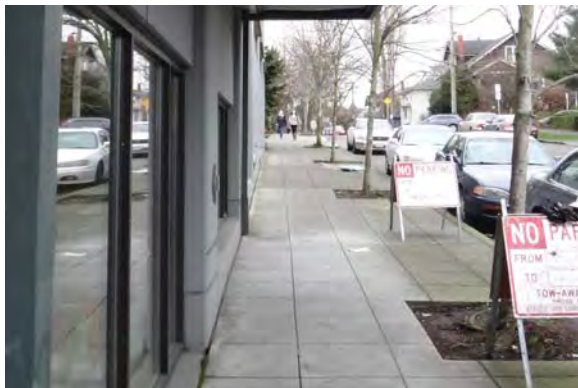
E : LOOKING SOUTH on Roosevelt Way



F : LOOKING NORTH on 9th Ave



G : LOOKING SOUTH on Roosevelt Way



H : LOOKING WEST on NE 45th St

DETAILS



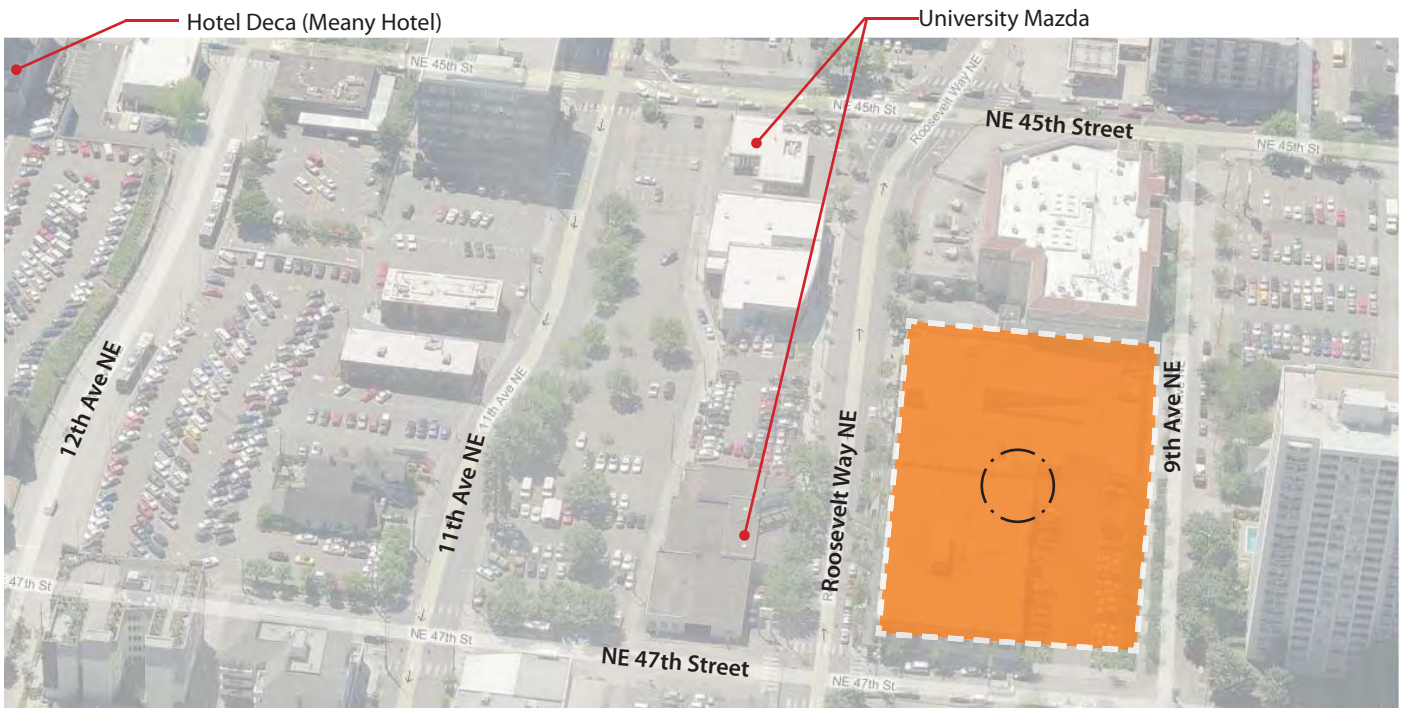
J : Parking Structure LOOKING WEST



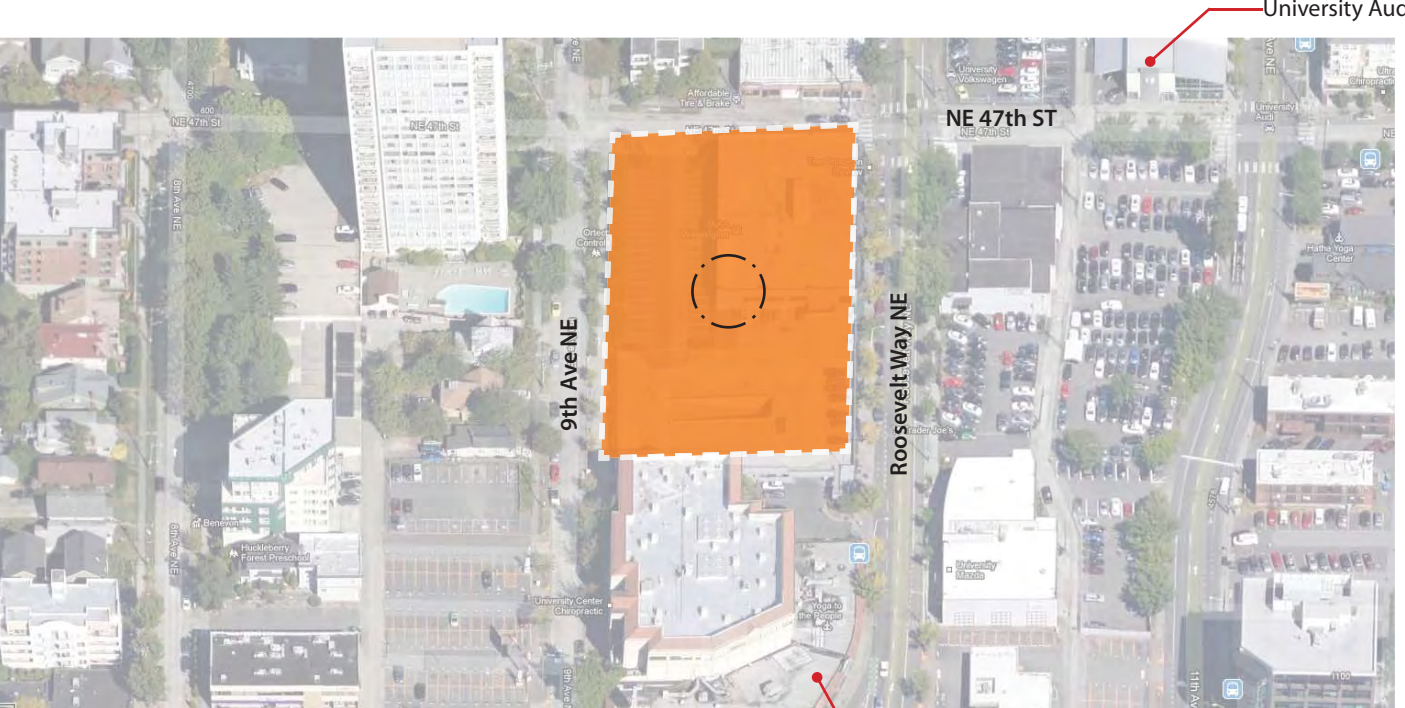
K : Parking Lot LOOKING NORTH

PARKING

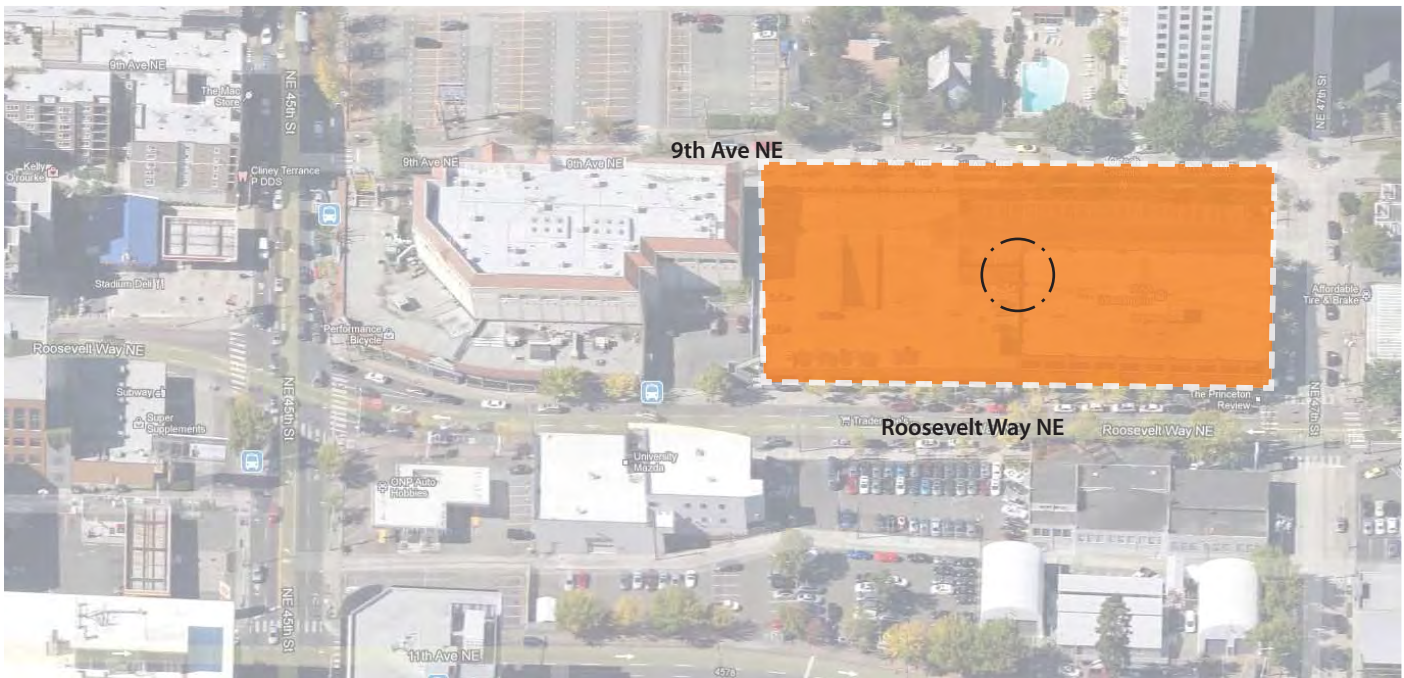
SITE ORIENTATION



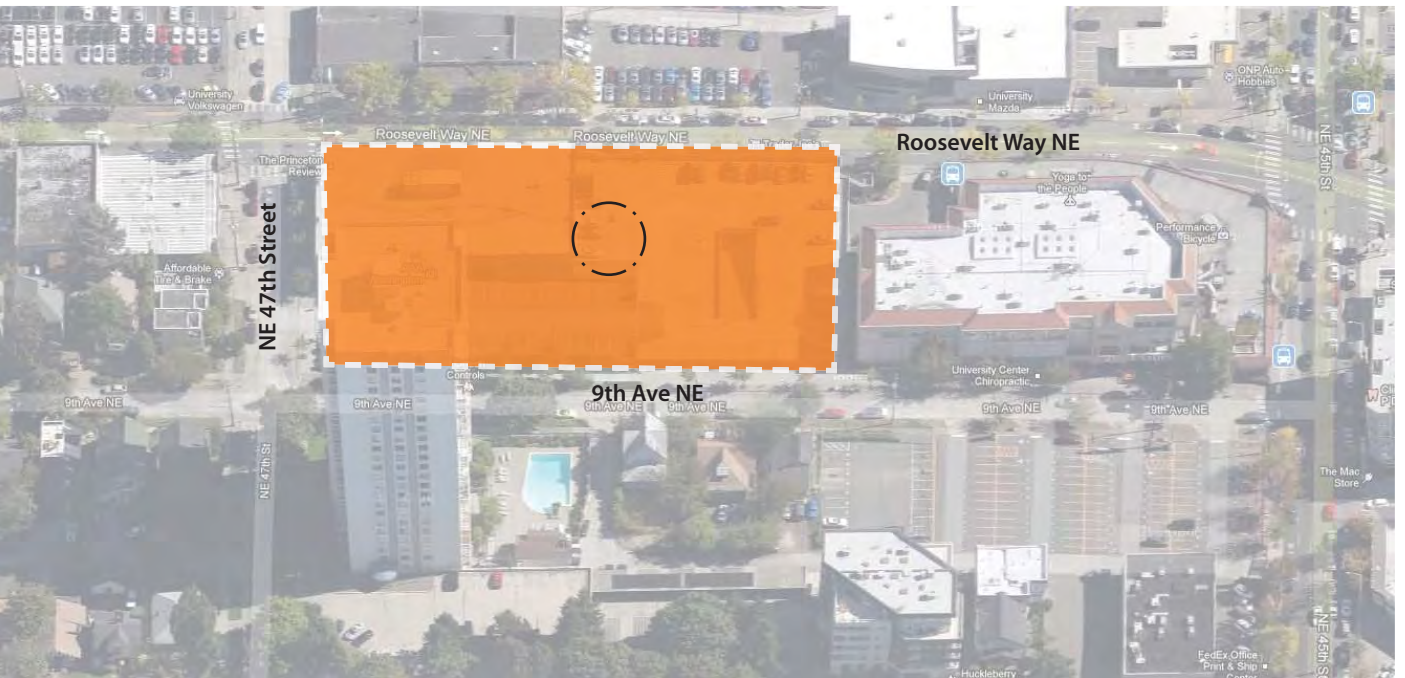
Lot #1
VIEW FROM NORTH



Lot #1
VIEW FROM SOUTH

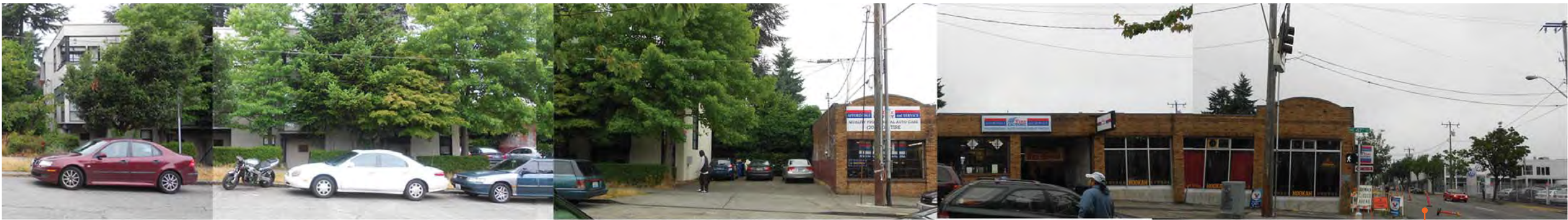


Lot #1
VIEW FROM EAST



Lot #1
VIEW FROM WEST

NE 47TH STREET ELEVATION



A : NE 47TH STREET LOOKING NORTH FROM SITE

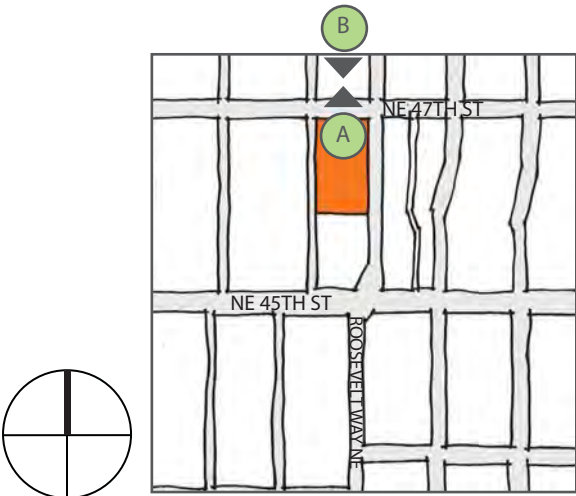
EXTENT OF NORTH FRONTAGE

NE 47TH STREET

ROOSEVELT WAY NE
THE UNIVERSITY PLAZA



B : NE 47TH STREET NORTH ELEVATION



KEY PLAN



ROOSEVELT WAY NE ELEVATION



A : ROOSEVELT WAY EAST ELEVATION

ROOSEVELT WAY NE

EXTENT OF EAST FRONTAGE

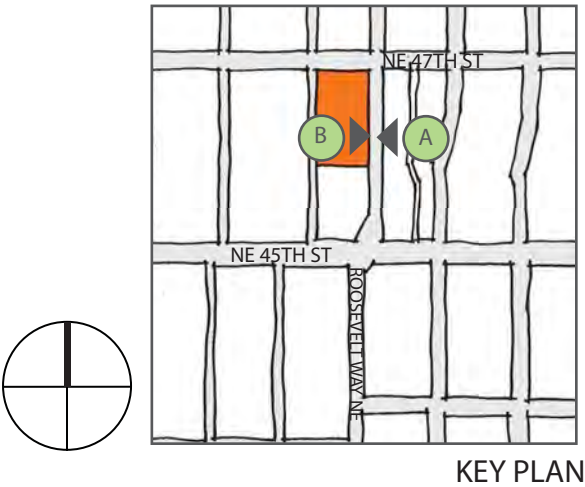


B : ROOSEVELT WAY LOOKING EAST FROM SITE

ROOSEVELT WAY NE ELEVATION



EXTENT OF EAST FRONTAGE



KEY PLAN

9TH AVE NE ELEVATION



A : 9TH AVE NE WEST ELEVATION

EXTENT OF WEST FRONTAGE



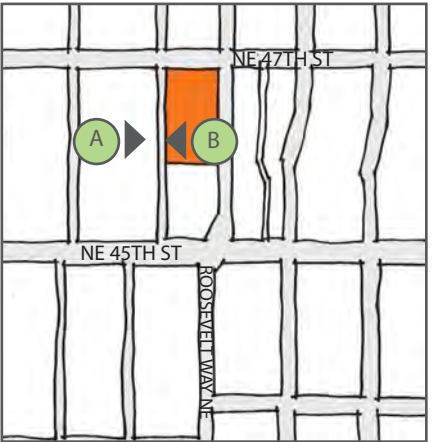
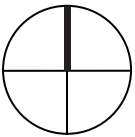
B : 9TH AVE NE LOOKING WEST FROM SITE

9TH AVE NE ELEVATION



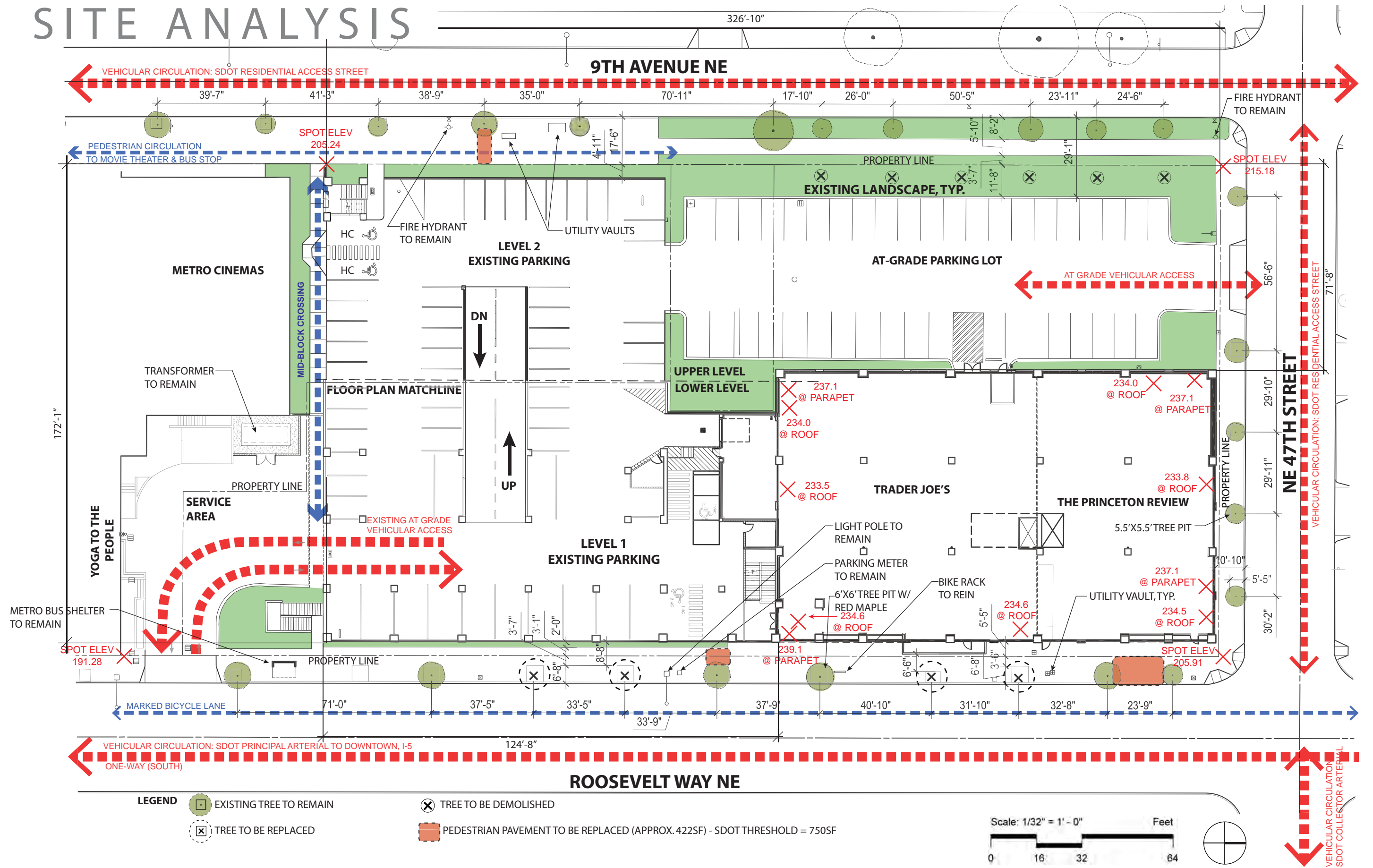
9TH AVE NE

NE 47TH STREET

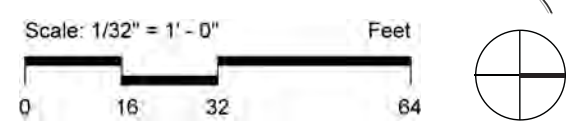


KEY PLAN

SITE ANALYSIS



- LEGEND**
- EXISTING TREE TO REMAIN
 - TREE TO BE DEMOLISHED
 - TREE TO BE REPLACED
 - PEDESTRIAN PAVEMENT TO BE REPLACED (APPROX. 422SF) - SDOT THRESHOLD = 750SF

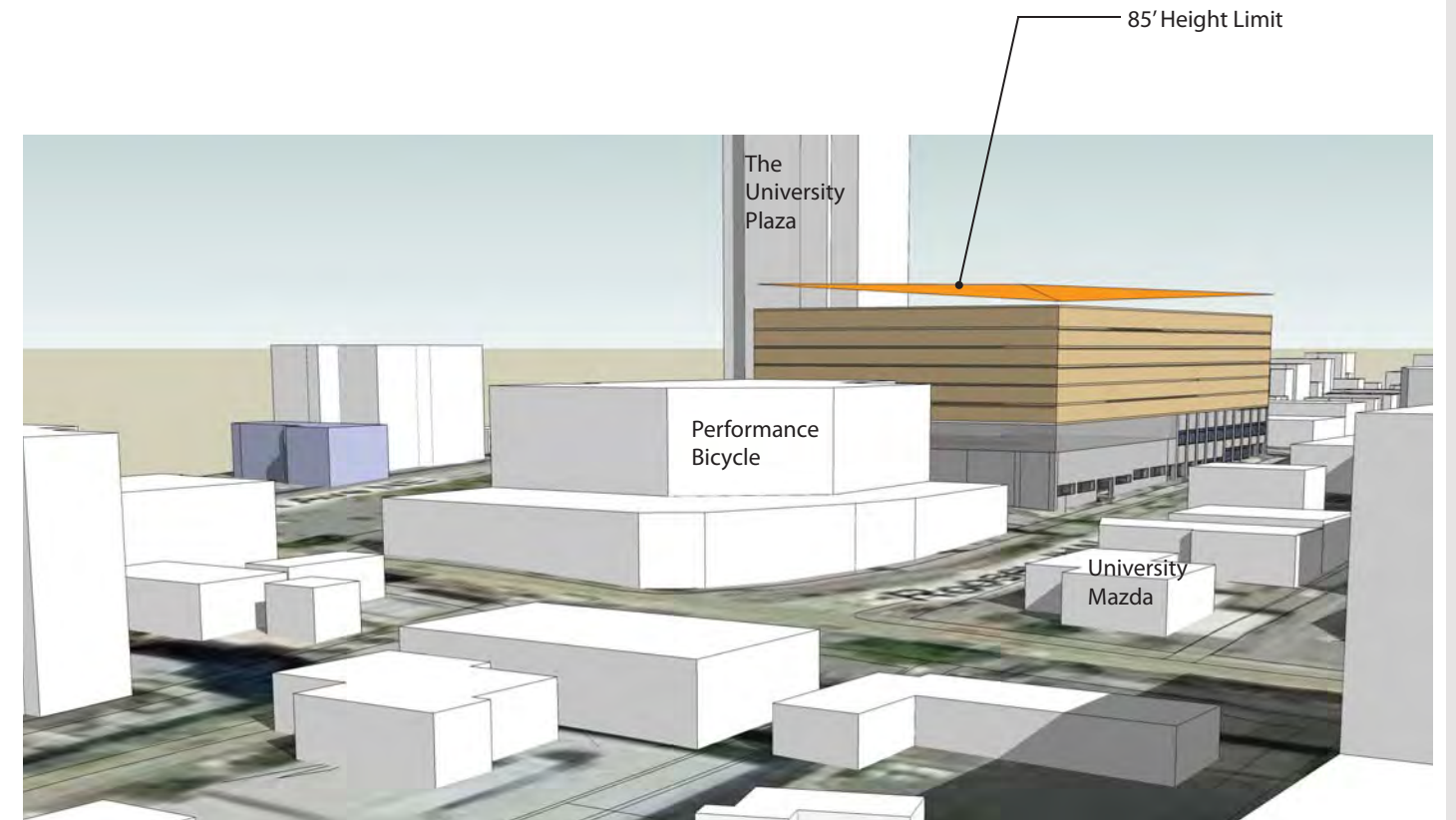
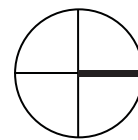


ALLOWABLE ZONING ENVELOPE

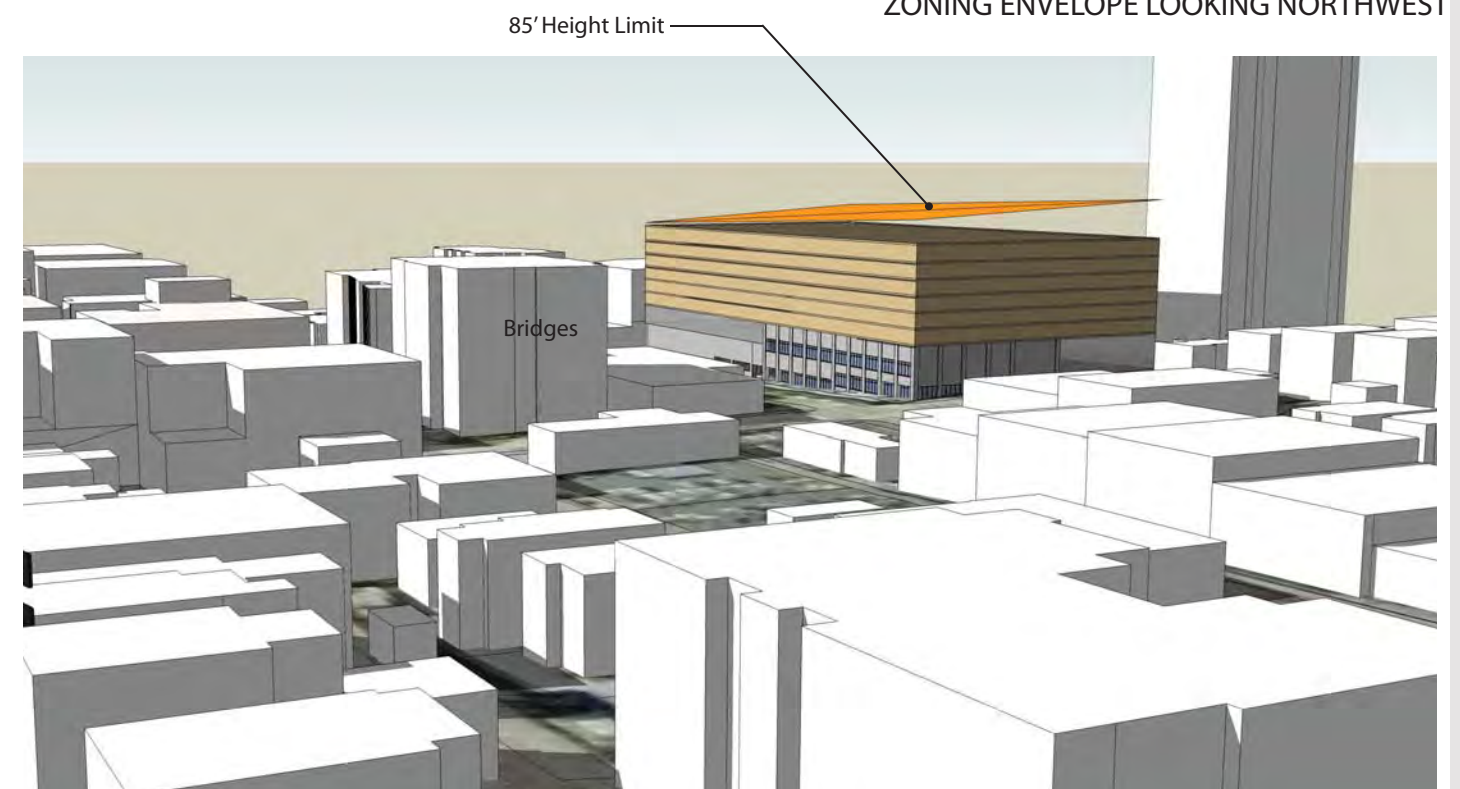


Maximum Allowable Height: 85 feet

Maximum Allowable FAR: Single-use = 4.5 / Mixed-use = 6



ZONING ENVELOPE LOOKING NORTHWEST



ZONING ENVELOPE LOOKING SOUTHWEST

LAND USE CONSIDERATIONS

ZONING SUMMARY

Zoning & Design Review

Base Zone	NC3 - 85
Adjacent Zones	NC3 - 65 North NE 47th Street. All other adjacent property zoned NC 3 - 85.
Overlay Zones	University District Northwest Urban Center Village. Light rail station overlay 45 (NE 45th Street).
Design Review Board	Northeast Board

Building Development

Lot Size	East-West 180'-0", North-South 404'-0"
Gross Floor Area	App. 58,270 SF
Allowable FAR	Single Use: 4.5 Max / Mixed Use: 6.0 Max Proposed: 2.26 Res. FAR / 3.03 Mix. Use FAR
Structure Height	85' Max

Street-Level Requirements

Blank Facade (23.47A.008)	40%
Landscaping & Screening Standards	5 FT landscape buffer / .30 Green Factor

Parking

Car	150 New Stalls + 110 Existing= 260 Total
Bicycle	35 New Long Term bike parking spots
Driveway Width	22' wide existing to remain
Driveway Slope	15% Max **
Curb Cuts	1 Existing Curb to remain
Access (23.47A.024)	New Res. Parking to be below/Above existing commercial parking. Existing ground level parking and access to remain

** SDOT Variance

23.47A.004 Permitted and Prohibited uses

Residential and Parking- Permitted	Response: Parking is permitted
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23.47A.008 Street- level development standards

Blank segments of the street- facing facade between 2 feet and 9 feet above the sidewalk may not exceed 20 feet in width. The total of all blank facade segments may not exceed 40 percent of the width of the facade of the structure along the street. Street- level street- facing facades shall be located within 10 feet of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided.	60 % of non- residential facades shall be transparent. We anticipate this will be achievable given the proposed design options.
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23.47A.012 Structure height

85 feet. Except as provided below, the following rooftop features may extend up to 15 feet above the applicable height limit, as long as the combined total coverage of all features gaining additional height listed in this subsection 23.47A.012.C.4 does not exceed 20 percent of the roof area, or 25 percent of the roof area if the total includes stair or elevator penthouses or screened mechanical equipment.	Design options maximize the 85' max height.
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23.47A.013 Floor area ratio

Maximum FAR of 4.5 if all structures are mixed residential/non-residential. For gross floor area below grade is not counted.	Design options meet but do not exceed maximum FAR 4.5.
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23.47A.016 Landscaping and screening standards

Landscaping that achieves a Green factor score of .30 or greater, pursuant to section 23.86.019 is required. Street trees shall be retained unless the Director of Transportation approves their removal. The Director, in consultation with the Director of Transportation, will determine the number, type and placement of street trees to be provided.	Design options address the Green factor and screening.
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23.47A.022 Light and glare standards

Exterior lighting must be shielded and directed away from adjacent uses. Interior lighting in parking garages must be shielded to minimize nighttime glare affecting nearby uses. To prevent vehicle lights from affecting adjacent properties, driveways and parking areas for more than two (2) vehicles shall be screened from adjacent properties by a fence or wall between five (5) feet and six (6) feet in height. If the elevation of the lot line is different from the finished elevation may substitute for a portion of the required height of the screen so long as the screen itself is a minimum of three (3) feet in height. The director may waive the requirement for the screening if it is not needed due to changes in topography, agreements to maintain an existing fence, or the nature and location of adjacent uses. Exterior lighting on poles is permitted up to a maximum height of thirty (30) feet from finished grade. In zones with forty (40) foot or greater height limit, exterior lighting on poles is permitted up to a height of forty (40) feet from finished grade, provided that the ratio of watts to area is at least twenty (20) percent below the maximum exterior lighting level permitted by the Energy Code.	Design options address the Light and glare standards.
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23.47A.024 Amenity area

<p>A. Amenity areas are required in an amount equal to 5 percent of the total gross floor area in residential use, except as otherwise specifically provided in this Chapter 23.47A. Gross floor area, for the purposes of this subsection, excludes areas used for mechanical equipment and accessory parking.</p> <p>B. Required amenity areas shall meet the following standards, as applicable:</p> <ol style="list-style-type: none">1. All residents shall have access to at least one common or private amenity area;2. Amenity areas shall not be enclosed;3. Parking areas, vehicular access easements, and driveways do not count as amenity areas, except that a woonerf may provide a maximum of 50 percent of the amenity area if the design of the woonerf is approved through a design review process pursuant to Chapter 23.41.4. Common amenity areas shall have a minimum horizontal dimension of 10 feet, and no common amenity area shall be less than 250 square feet in size;5. Private balconies and decks shall have a minimum area of 60 square feet, and no horizontal dimension shall be less than 6 feet.6. Rooftop areas excluded because they are near minor communication utilities and accessory communication devices, pursuant to Section 23.57.012.C.1.d, do not qualify as amenity areas.	Design options address the Amenity area.
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LAND USE CONSIDERATIONS

23.47A.030 Required Parking and loading.
23.54.015 Required Parking

Response:

Bicycle Parking: 1 per 4 residential units. Non- residential uses such as eating & drinking establishments and retail require 1 space / 2,000 SF of short term parking and 1/ 12,000SF of long term parking. Long- term parking for bicycles shall be for bicycles parking four (4) hours or more. Short- term parking for bicycles shall be for bicycles parked less than four (4) hours. Bicycle parking required for residential uses must be located on- site. Bicycle parking facilities shared by more than one use are encouraged.	Design options address the bicycle parking requirement.
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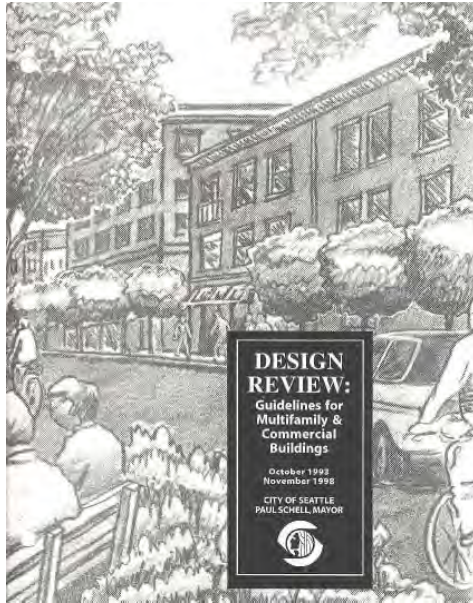
23.54.035 Loading berth requirements and space standards

Multipurpose convenience stores, sales, service and rental of major durables, and speciality food stores may be required by the Director to increase the length of required loading berths; however, these uses shall not be required to provide loading berths in excess of fifty- five (55) feet. The review of loading berth length requirements for these uses shall focus on the size of vehicles that frequently serve the business and the frequency of loading activity that will extend beyond the lot line during daytime hours (six (6:00) a.m to six (6:00) p.m.). Large- truck loading occurring on a daily basis shall generally require longer loading berths; when such activity occurs on at least a weekly basis, it will be evaluated regarding the amount of traffic disruption and safety problems potentially created; such activity occurring on less than a weekly basis shall generally not require longer loading berths.	Design options address the loading berth requirements.
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23.47A.032 Parking location and access

Off- street parking may be located anywhere on a lot in C1 and C2 zones, except that structures with residential uses in C zones and structures in C zones across the street from residential zones shall meet the requirements for parking location for NC zones as provided in subsection 23.47A.032.B.1, except that if a lot in a C zone is bordered by streets on all sides, then parking may be provided between a street and a structure, but only on sides facing other commercially- zoned lots. When an existing building is proposed to be expanded, the Director shall determine the location of parking and access with the goal of minimizing the impact of parking and access along the front lot line. Parking shall be screened according to the provisions of Section 23.47A.016.	Design options address the parking requirements.
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DESIGN GUIDELINES FOR MULTI-FAMILY AND COMMERCIAL BUILDINGS



Guideline A-1 : Responding to Site Characteristics

The siting of buildings should respond to specific site conditions and opportunities such as non-rectangular lots, location on prominent intersections, unusual topography, significant vegetation and views or other natural features.

Response:

- All options incorporate the existing parking garage and Commercial/Office building.
- All options are built out to the property lines creating defined street edges.
- The design responds to the varied nature of both sides of the site. Roosevelt Way is a more active, pedestrian and vehicular street. This will be the main entry location. The quieter more residential 9th Ave NE will be the location of the townhomes entries.

Guideline A-2 : Streetscape Compatibility

The siting of buildings should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.

Response:

- Street wall along Roosevelt Way defines and strengthens the street edge.
- All options improve the entry to Trader Joe's by providing an enlarged common plaza.

Guideline A-3 : Entrances Visible from the Street

Entries should be clearly identifiable and visible from the street.

Response:

- The main common entry to the residential, Trader Joe's and parking are midblock and are part of a common entry plaza recessed from the street edge.
- The main entry to the upper level office space will be accessed from NE 47th St
- The townhomes will have separate entries off of 9th Ave NE.

Guideline A-4: Human Activity

New development should be sited and designed to encourage human activity on the street.

Response:

- Fifty percent of the street facade facing Roosevelt Way and NE 47th St is ground floor retail.
- Pedestrian traffic will be higher on Roosevelt Way and NE 47th St with residents going to and from UW, the new transit station and the commercial areas.
- Townhome entries along 9th Ave NE will provide resident use along a street edge.
- Upper level office access is provided off NE 47th St.

Guideline A-5 : Respect for Adjacent Sites

The massing of the building responds to the massing of adjacent buildings and views from adjacent units, providing greater setbacks where needed to maintain daylight for units in neighboring buildings.

Response:

- Common spaces of high activity are oriented toward Roosevelt Way.
- Open courtyards in (preferred option) overlook Roosevelt Way.
- Facade facing the adjacent residential neighborhood to the NW of the site will be treated differently than the Roosevelt Way facade.
- The scale of the building is in line with neighboring projects of similar density.

Guideline A-6 : Transition between Residence and Street

For residential projects, the space between the building and the sidewalk should provide security and privacy for residents and encourage social interaction among residents and neighbors.

Response:

- Entries will be designed to be recessed to provide protection or will use canopies where sidewalk allows.
- Third floor courtyard will provide an open architectural window to Roosevelt Way and will be a transition from the Mixed-use corridor "Room" in the housing and common space.
- Third floor courtyard and fourth floor green deck will provide relief, transition and privacy.

Guideline A-7 : Residential Open Space

Residential projects should be sited to maximize opportunities for creating usable, attractive, well-integrated open space.

Response:

- Major open space for all 3 options is located within the courtyards to the center of the project and provides for central outdoor protected common space for the residential population.
- Common entrance plaza for Trader Joe's, parking garage and the proposed residence located at the center of the block on Roosevelt Way provides a transition from the public realm to the private realm.
- Streetscape including street trees, planting strips and furniture will be provided in each option in conformance with the design guidelines.

Guideline A-9 : Location of Parking on Commercial Street Fronts

Parking on a commercial street front should be minimized and where possible should be located behind a building.

Response:

- All parking is enclosed within a garage except a small number of spaces that will remain at the Roosevelt Way entrance location.

Guideline A-10 : Corner Lots

Buildings on corner lots should be oriented to the corner and public street fronts. Parking and automobile access should be located away from the corners.

Response:

- The northeast corner will continue to be enhanced by storefront along facade. The northwest corner will be residential with all vehicular access located at existing curb cuts at the mid block.

Guideline B-1 : Height and Scale

Projects should be compatible with the scale of development anticipated by the applicable Land-Use Policies for the surrounding area and should be sited and designed to provide a sensitive to near-by, less-intensive zones. Projects on zone edges should be developed in a manner that creates a step in perceived height, bulk and scale between the anticipated development potential of the adjacent zones.

Response:

- Proposed build is oriented to the corners and public street fronts.
- Scale of projects is similar to that allowed by zoning on adjacent properties.

Guideline C-1 : Architectural Context

New buildings proposed for existing neighborhoods with a well-defined and desirable character should be compatible with or complement the architectural character and siting pattern of neighboring buildings.

Response:

- See response to University Community Guidelines.

DESIGN GUIDELINES FOR MULTI-FAMILY AND COMMERCIAL BUILDINGS

Guideline C-2 : Architectural Concept and Consistency

Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept.

Response:

- Building massing will respond to the adjacent commercial and residential building
- The facade of the existing parking garage and Trader Joe's will be improved to create a more unified architectural statement that will enhance the ground plane retail vocabulary that exists within the University District.
- Architectural lighting will be provided, vertical bay windows and strategically placed building lighting.
- Selective openings in the parking garage will be enlarged to reduce the existing massive concrete facade of the garage.
- The existing concrete base of the wall will be integrated into the architectural vocabulary of the residential above.

Guideline C-3 : Human Scale

The design of new buildings should incorporate architectural features, elements and details to achieve a good human scale.

Response:

The building will incorporate items such as modulation, signage, lighting, fenestration that addresses the human scale along each adjacency.

Guideline C-4: Exterior Finish Materials

Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.

Response:

- See response to University Community Guidelines.

Guideline C-5: Structured Parking Entrances

The presence and appearance of garage entrances should be minimized so that they do not dominate the street frontage of a building.

Response:

- The Roosevelt Way garage entrance is perpendicular to the street and remain is. The entrance off NE 47th St will maintain the existing curb cut as ingress/egress but will be integrated into a garage entrance facade that will transition the architecture of the existing office and the new townhomes in .

Guideline D-1: Pedestrian Open Space and Entrances

Convenient and attractive access to the building's entry should be provided. To ensure comfort and security, paths and entry areas should be sufficiently lighted and entry areas should be protected from the weather. Opportunities for creating lively, pedestrian-oriented open spaces should be considered.

Response:

Response:

- The main entry plaza to residential, Trader Joes and parking are midblock and are part of a common entry court recessed from the street edge.
- The mid block pedestrian access will be lit and maintained for neighborhood access.
- Entrance to the office building will be along NE 47th Street and will be well articulated.

Guideline D-2: Blank Walls and Retaining Walls

Buildings should avoid large blank walls facing the street, especially near sidewalks. Where blank walls are avoidable , they should receive design treatment to increase pedestrian comfort and interest.

Response:

- The walls adjacent to any pedestrian way will be activated with either green wall, landscape, art or architectural elements

Guideline D-4: Design of Parking Lots near Sidewalks

Parking lots near sidewalks should provide adequate security and lighting, avoid encroachment of vehicles onto the sidewalk, and minimize the visual clutter of parking signs and equipment.

Response:

- All site parking will be within enclosures

Guideline D-5: Visual Impacts of Parking Structures

The visibility of all at-grade parking structures or accessory parking garages should be minimized. The parking portion of a structure should be architecturally compatible with the rest of the structure and streetscape. Open parking spaces and carports should be screened from the street and adjacent properties.

Response:

- The facades of all at-grade parking will be screened to be sensitive to the other ground floor storefronts on neighboring properties.

Guideline D-6: Screening of Dumpsters, Utilities and Service Areas

Building sites should locate service elements like trash dumpsters , loading docks and mechanical equipment away from the street front where possible. When it is not possible to locate these elements away from the street front, they should be screened from view using high-quality and compatible materials and should not be located in the pedestrian right-of-way.

Response:

- Service areas will be located within the building footprint

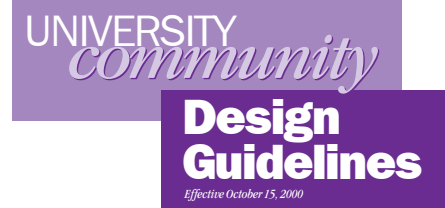
Guideline D-7: Personal Safety and Security

Project design should consider opportunities for enhancing personal safety and security in the environment under review.

Response:

- Project design to feature ample storefronts and upper level windows facing the streets and courtyard/courtyards. Residences will maintain 24 hour security in the residential lobby.
- Lighting to be provided at all entries, entry plaza, pedestrian link to the north of the existing theatre building and south of our project.
- Main entrances to Trader Joe's, parking and the

UNIVERSITY COMMUNITY DESIGN GUIDELINES



Guideline A-1 : Responding to Site Characteristics

The siting of buildings should respond to specific site conditions and opportunities such as non-rectangular lots, location on prominent intersections, unusual topography, significant vegetation and views or other natural features.

Response:

- All options incorporate the existing parking garage and Commercial/Office building.
- All options are built out to the property lines creating defined street edges.
- The design responds to the varied nature of both sides of the site. Roosevelt Way is a more active, pedestrian and vehicular street. This will be the main entry location. The quieter more residential 9th Ave NE will be the location of the townhomes entries.

Guideline A-2 : Streetscape Compatibility

The siting of buildings should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.

Response:

- Street wall along Roosevelt Way defines and strengthens the street edge.
- All options improve the entry to Trader Joes by providing an enlarged common plaza.

Guideline A-3 : Entrances Visible from the Street

Entries should be clearly identifiable and visible from the street.

Response:

- The main common entry to the residential, Trader Joe's and parking are midblock and are part of a common entry plaza recessed from the street edge.

Guideline A-4: Human Activity

New development should be sited and designed to encourage human activity on the street.

Response:

- Fifty percent of the street facade facing Roosevelt Way and NE 47th St is ground floor retail.
- Pedestrian traffic will be higher on Roosevelt Way and NE 47th St with residents going to and from UW, the new transit station and the commercial areas.
- Townhome entries along 9th Ave NE will provide resident use along a street edge.
- Upper level office access is provided off NE 47th

Guideline A-5 : Respect for Adjacent Sites

The massing of the building responds to the massing of adjacent buildings and views from adjacent units, providing greater setbacks where needed to maintain daylight for units in neighboring buildings.

Response:

- Common spaces of high activity are oriented toward Roosevelt Way.
- Open courtyards in (preferred option) overlook Roosevelt Way.
- Facade facing the adjacent residential neighborhood to the NW of the site will be treated differently than the Roosevelt Way facade.
- The scale of the building is in line with neighboring projects of similar density.

Guideline A-7 : Residential Open Space

Residential projects should be sited to maximize opportunities for creating usable, attractive, well-integrated open space.

Response:

- Major open space for all 3 options is located within the courtyards to the center of the project provides for central outdoor protected common space for the residential population.
- Common entrance plaza for Trader Joe's, parking garage and the proposed residence located at the center of the block on Roosevelt Way provides a transition from the public realm to the private realm.
- Streetscape including street trees, planting strips and furniture will be provided in each option in conformance with the design guidelines.

Guideline A-10 : Corner Lots

Buildings on corner lots should be oriented to the corner and public street fronts. Parking and automobile access should be located away from the corners.

Response:

- The northeast corner will continue to be enhanced by storefront along facade. The northwest corner will be residential with all vehicular access located at existing curb cuts at the mid block.

Guideline B-1 : Height and Scale

Projects should be compatible with the scale of development anticipated by the applicable Land-Use Policies for the surrounding area and should be sited and designed to provide a sensitive to near-by, less-intensive zones. Projects on zone edges should be developed in a manner that creates a step in perceived height, bulk and scale between the anticipated development potential of the adjacent zones.

Response:

- Proposed build is oriented to the corners and public street fronts.
- Scale of projects is similar to that allowed by zoning on adjacent properties.

Guideline C-1 : Architectural Context

New buildings proposed for existing neighborhoods with a well-defined and desirable character should be compatible with or complement the architectural character and siting pattern of neighboring buildings.

Response:

- See response to University Community Guidelines.

Guideline C-4: Exterior Finish Materials

Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.

Response:

- All exterior materials will be of similar quality, durability and maintenance as all new developments within the neighborhood.

Guideline D-1: Pedestrian Open Space and Entrances

Convenient and attractive access to the building's entry should be provided. To ensure comfort and security, paths and entry areas should be sufficiently lighted and entry areas should be protected from the weather. Opportunities for creating lively, pedestrian-oriented open spaces should be considered.

Response:

- The main entry plaza to residential, Trader Joes and parking are midblock and are part of a common entry court recessed from the street edge.
- The mid block pedestrian access will be lit and maintained for neighborhood access.
- Entrance to the office building will be along NE 47th Street and will be well articulated

DESIGN: MASSING OPTIONS

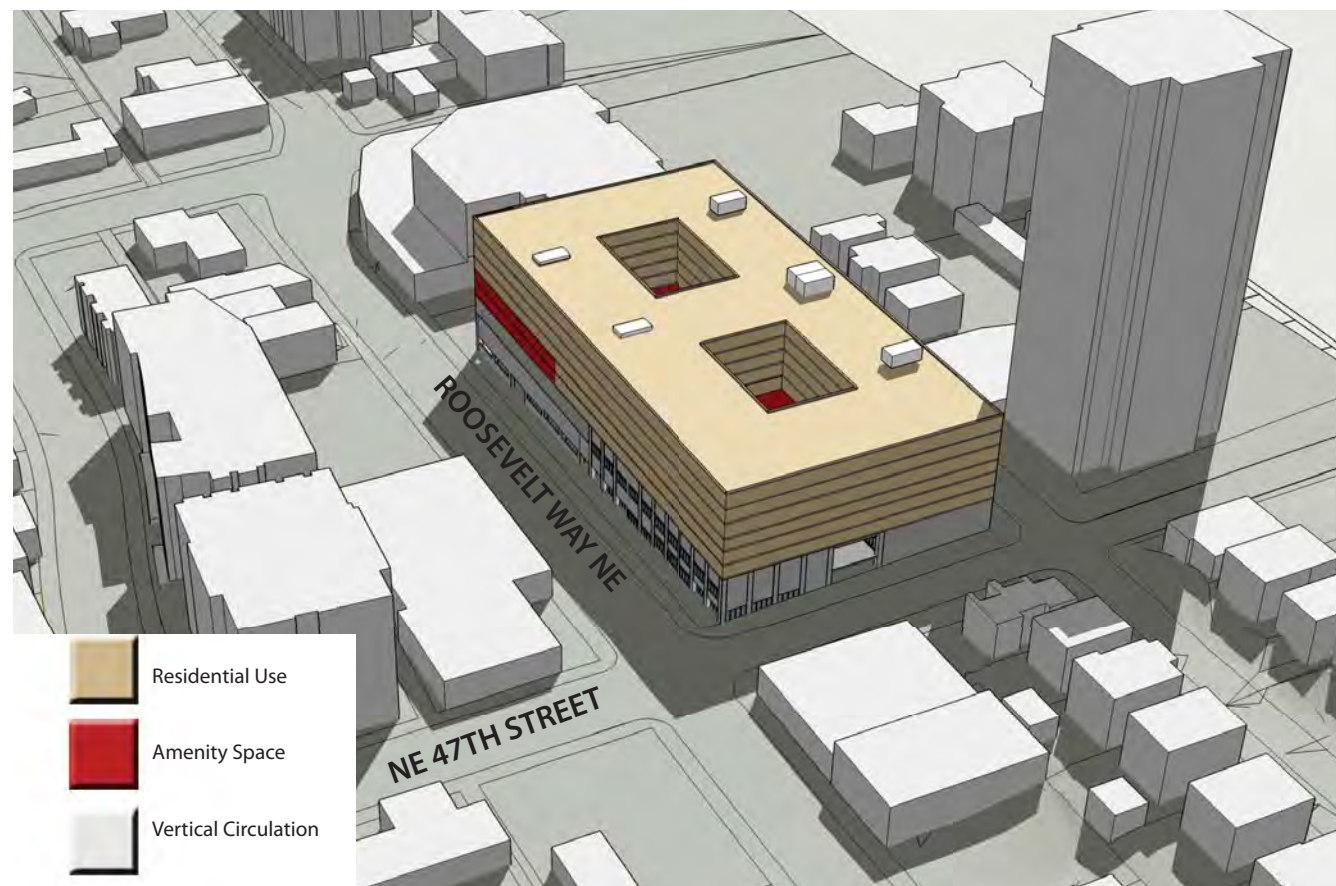
MASSING: OPTION #1

ADVANTAGES

- Largest number of student beds.
- Single courtyard allows for maximum amount of internal south light.
- Provides stacked glazed lounges at southeast corner of building. These common spaces have commanding views to the south and east towards the Seattle skyline and UW campus.
- Defines street edges.
- Larger number of student beds allows for sculpting of north west corner of building facing adjacent residential neighborhood.

DRAWBACKS

- Double courtyards reduce amount of internal south light.
- Most massive. Requires creative modulation of Roosevelt Avenue Facade.



VIEW OF NORTHEAST CORNER



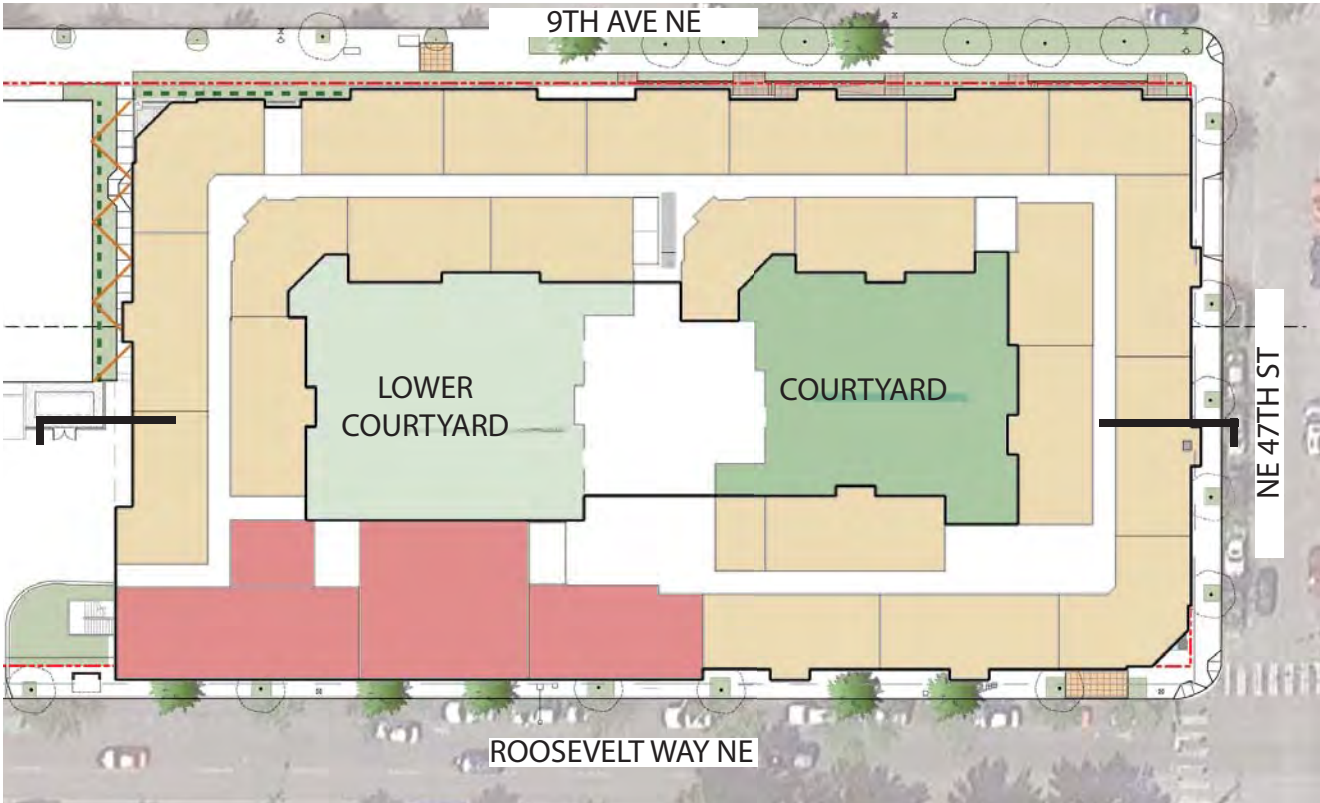
VIEW OF SOUTHEAST CORNER

MASSING: OPTION #1



- Residential Use
- Amenity Space
- Vertical Circulation

VIEW OF NORTHEAST CORNER



OPTION 1 SCHEMATIC
4TH FLOOR PLAN



SECTION: SCALE 1" = 40' - 0"

MASSING: OPTION #2

ADVANTAGES

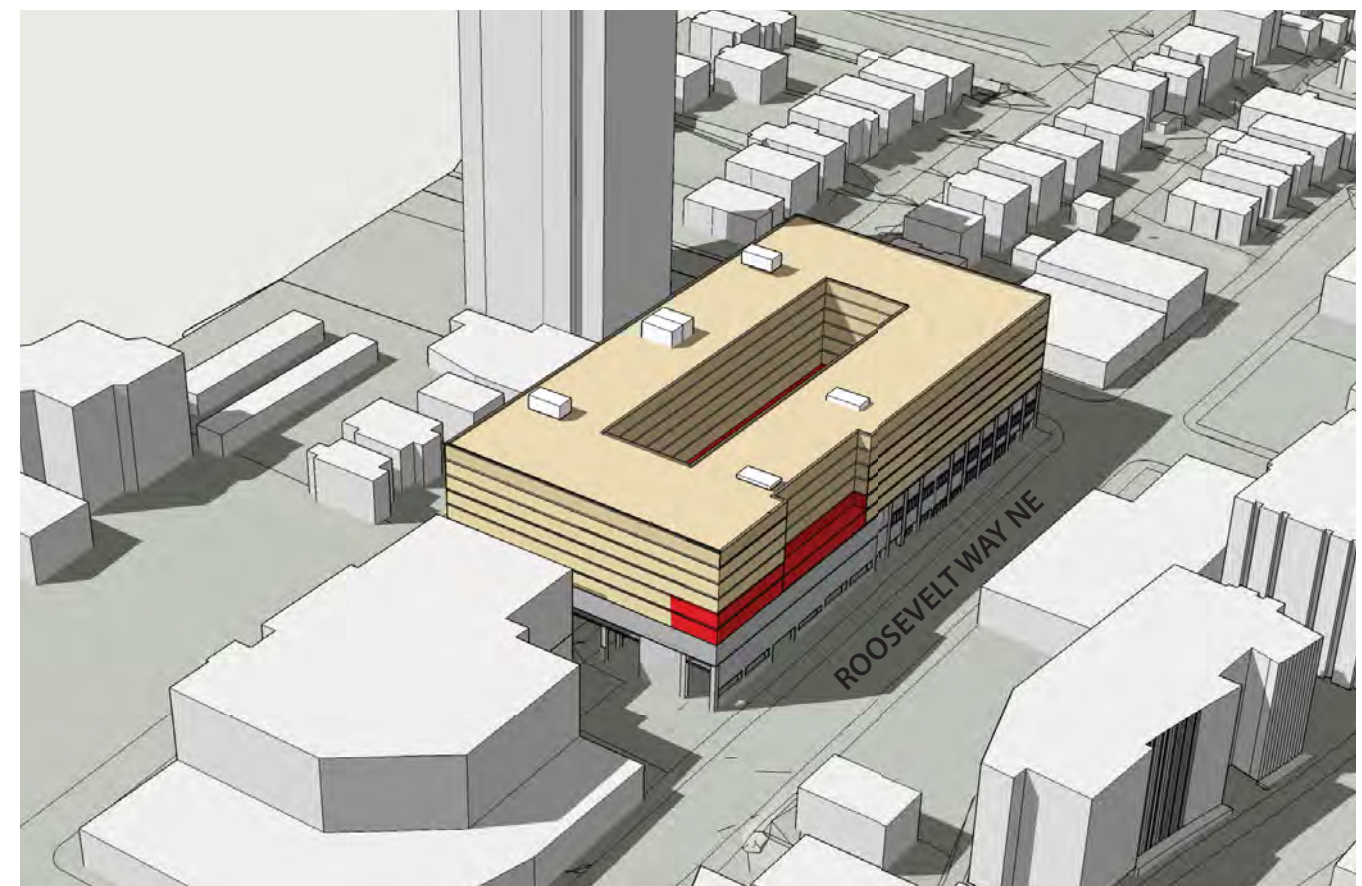
- Single courtyard allows for maximum amount of internal south light.
- Provides stacked glazed lounges at southeast corner of building. These common spaces have commanding views to the south and east towards the Seattle skyline and UW campus.
- Glazed corner provides a “Beacon” enhancing the intersection of Roosevelt Avenue and 45th Street as a designated “Gateway”.
- Defines street edges.

DRAWBACKS

- No modulation along the east facade.
- Provides no opportunity for street level connection along Roosevelt Way NE.

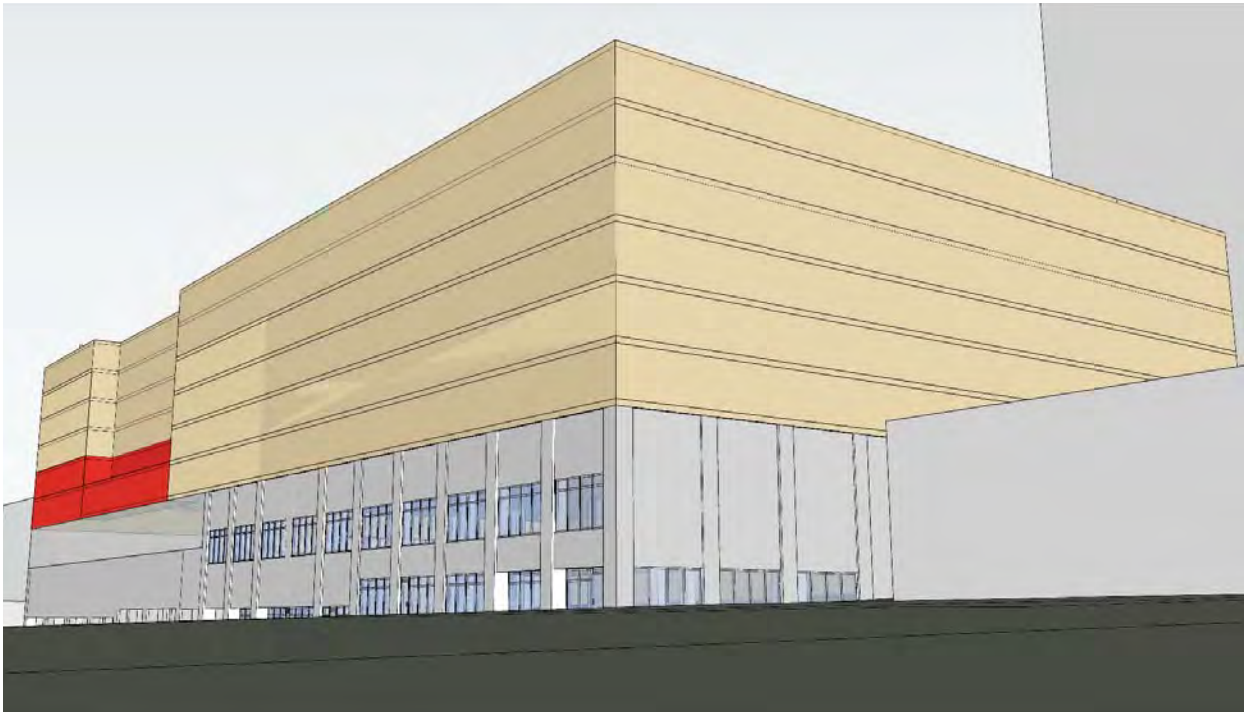


VIEW OF NORTHEAST CORNER



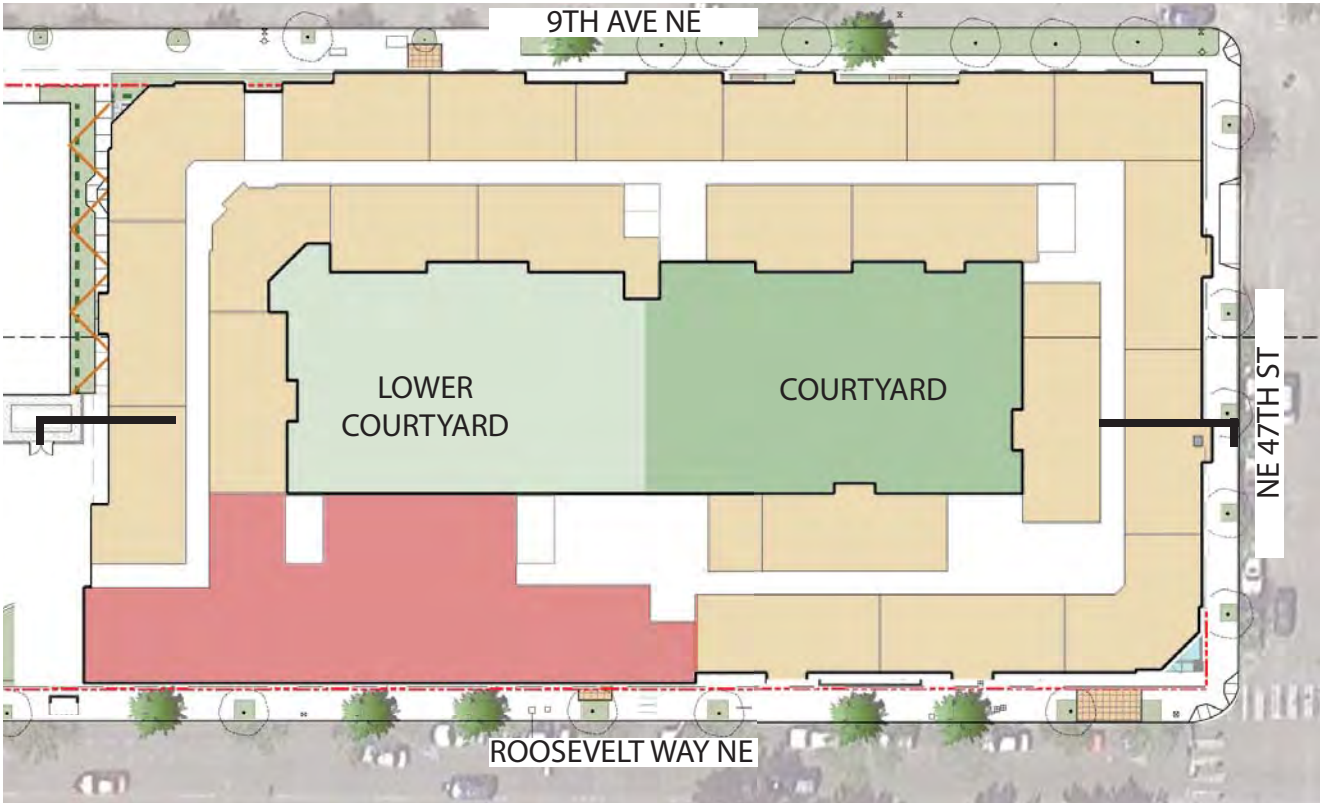
VIEW OF SOUTHEAST CORNER

MASSING: OPTION #2

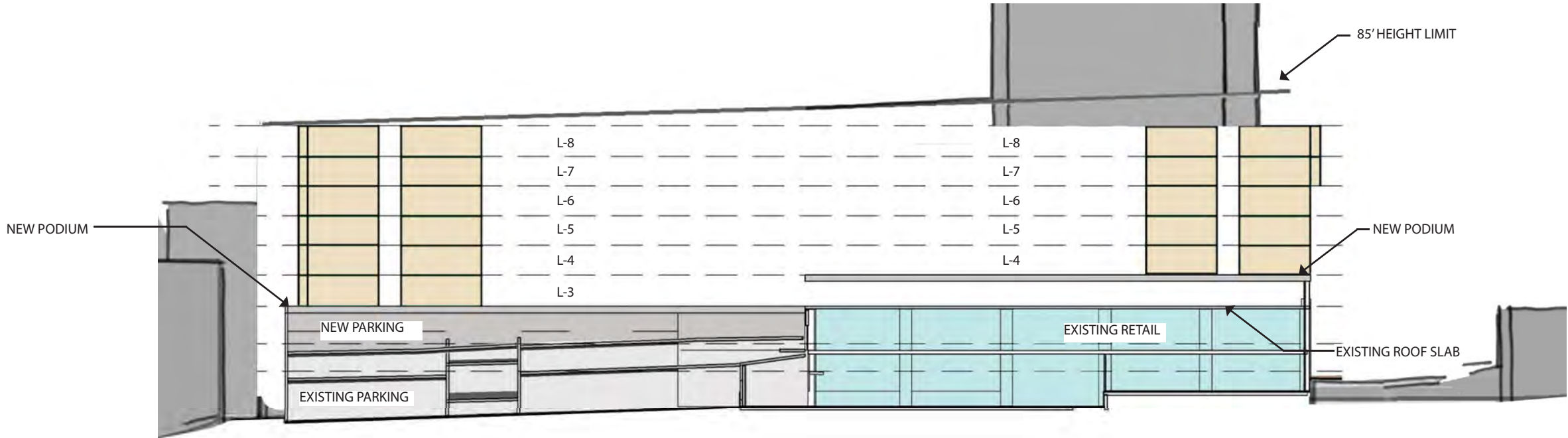


- Residential Use
- Amenity Space
- Vertical Circulation

VIEW OF NORTHEAST CORNER



OPTION 2 SCHEMATIC
4TH FLOOR PLAN



SECTION: SCALE 1" = 40' - 0"

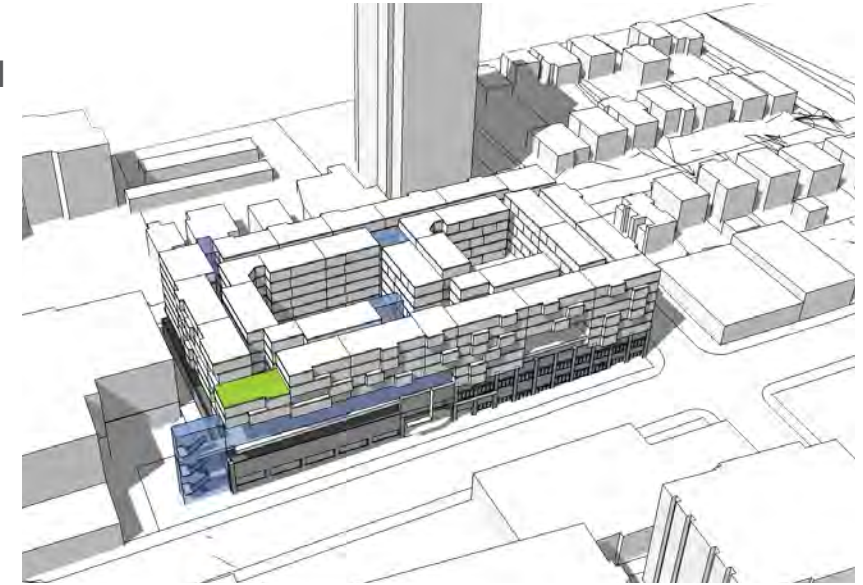
MASSING: Preferred Option (Code Compliant)

ADVANTAGES

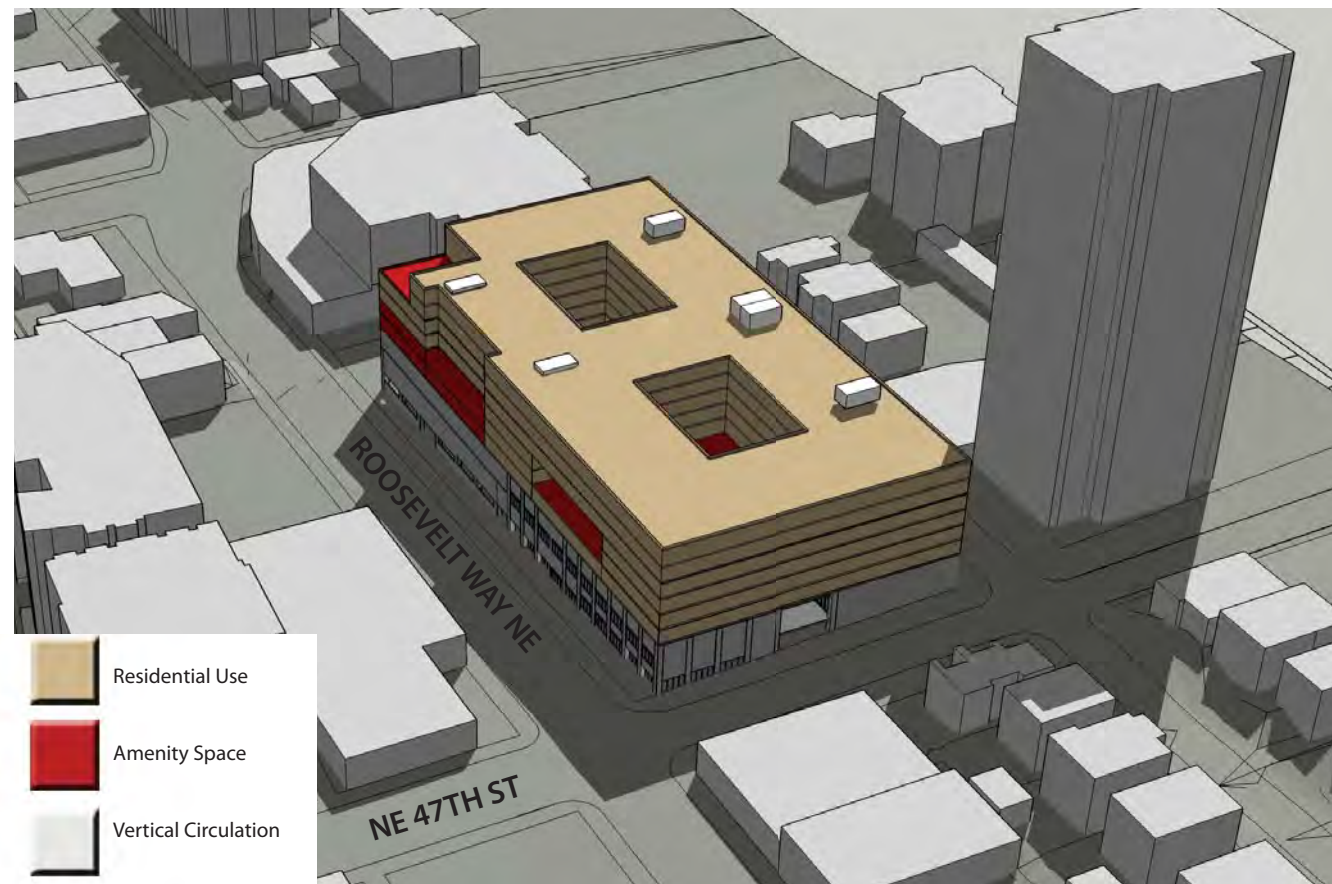
- Modulates Roosevelt Avenue facade to reduce apparent mass of building.
- Provides a terrace overlook and opens 1 courtyard to Roosevelt Avenue. Provides life and activity.
- Provides stacked glazed lounges at southeast corner of building. These common spaces have commanding views to the south and east towards the Seattle skyline and UW campus.
- Glazed corner provides a “Beacon” enhancing the intersection of
- Roosevelt Avenue and 45th street as a designated “Gateway”.
- Defines street edges.

DRAWBACKS

- Double courtyards reduce the amount of internal south light.



CONCEPTUAL MASSING SKETCH

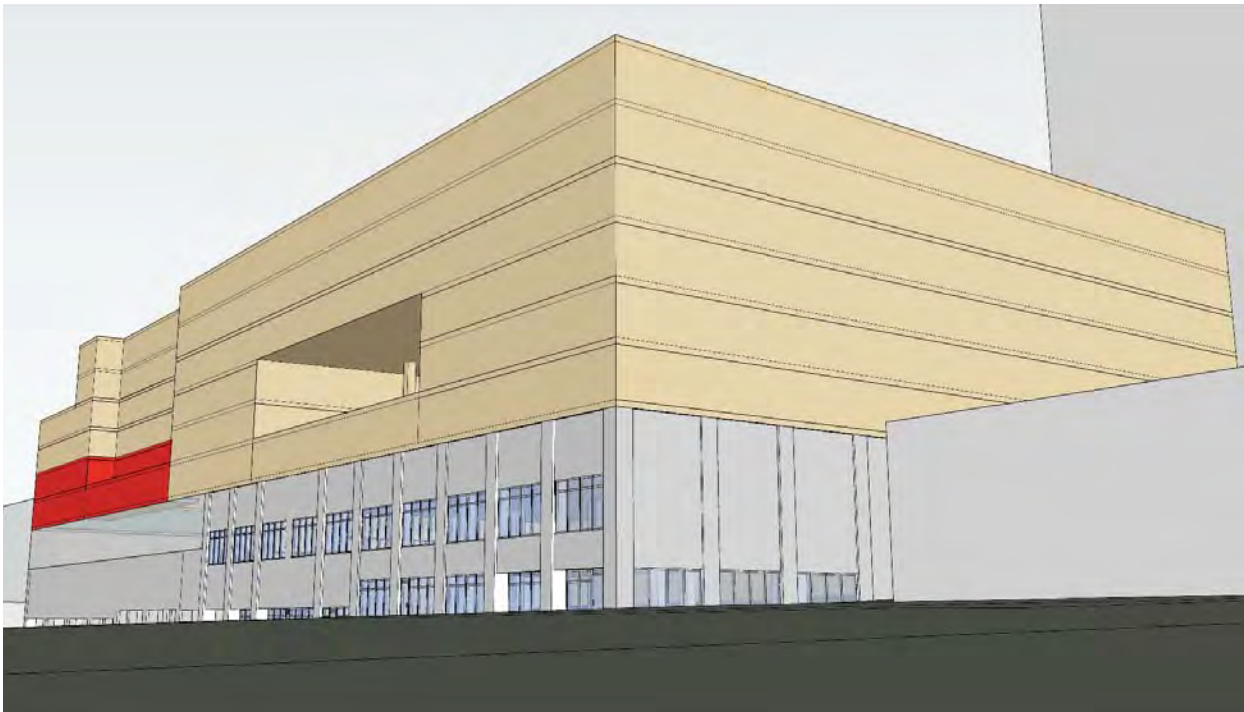


VIEW OF NORTHEAST CORNER



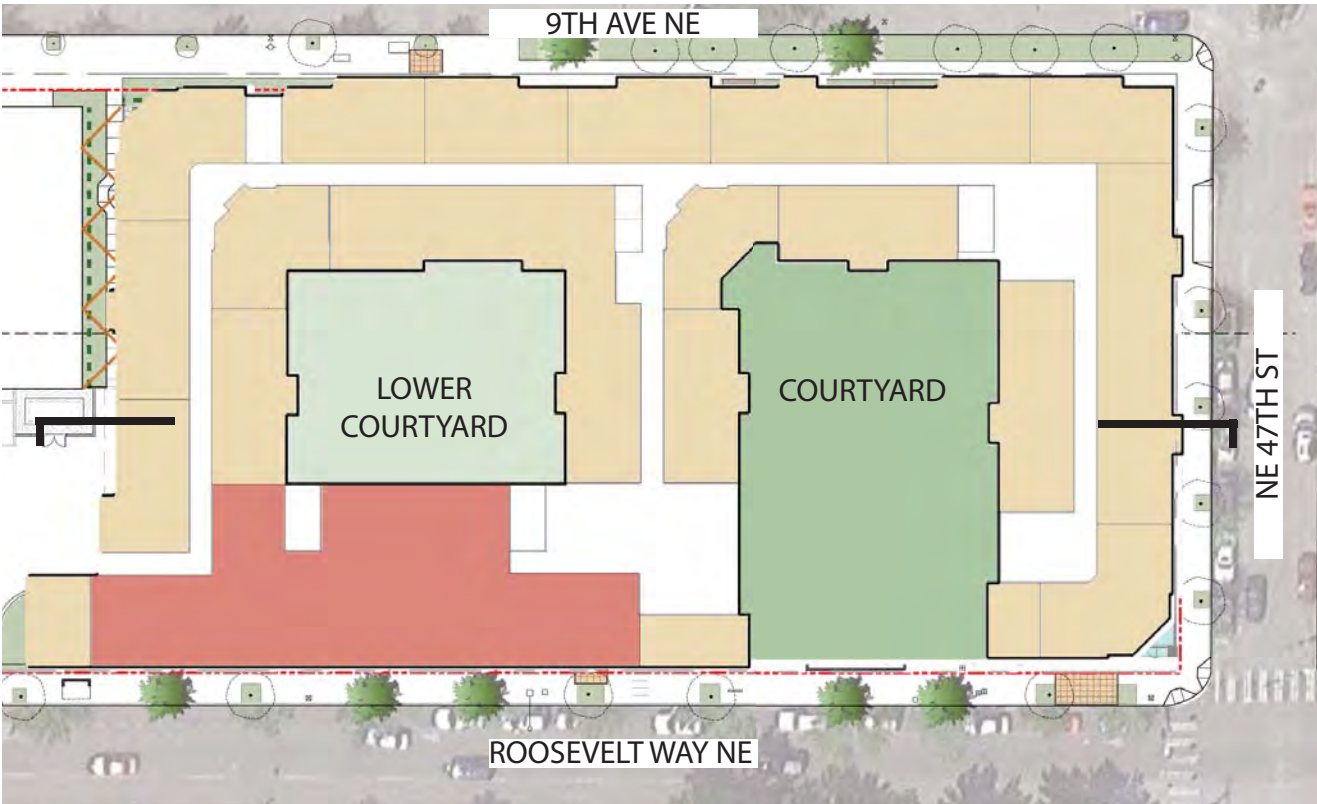
VIEW OF SOUTHEAST CORNER

MASSING: Preferred Option (Code Compliant)



- Residential Use
- Amenity Space
- Vertical Circulation

VIEW OF NORTHEAST CORNER



PREFERRED SCHEMATIC
4TH FLOOR PLAN

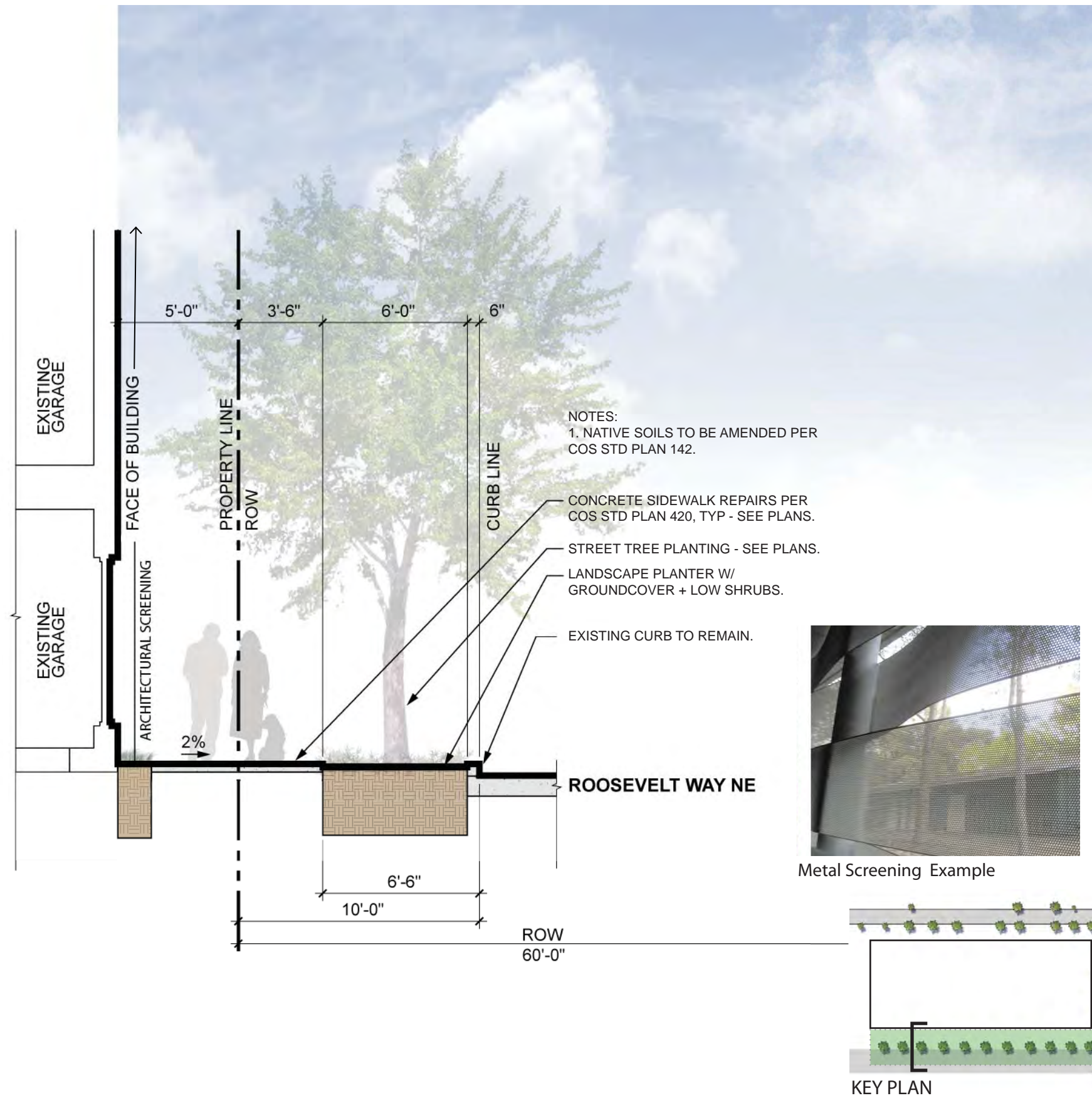


SECTION: SCALE 1" = 40' - 0"

LANDSCAPE CHARACTER



SITE PLAN

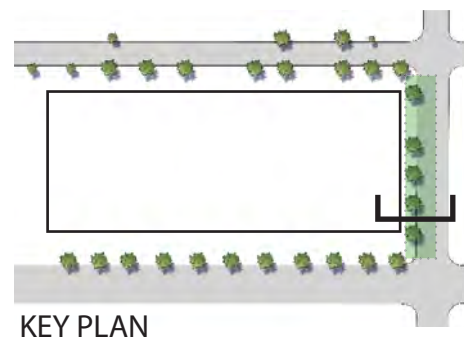
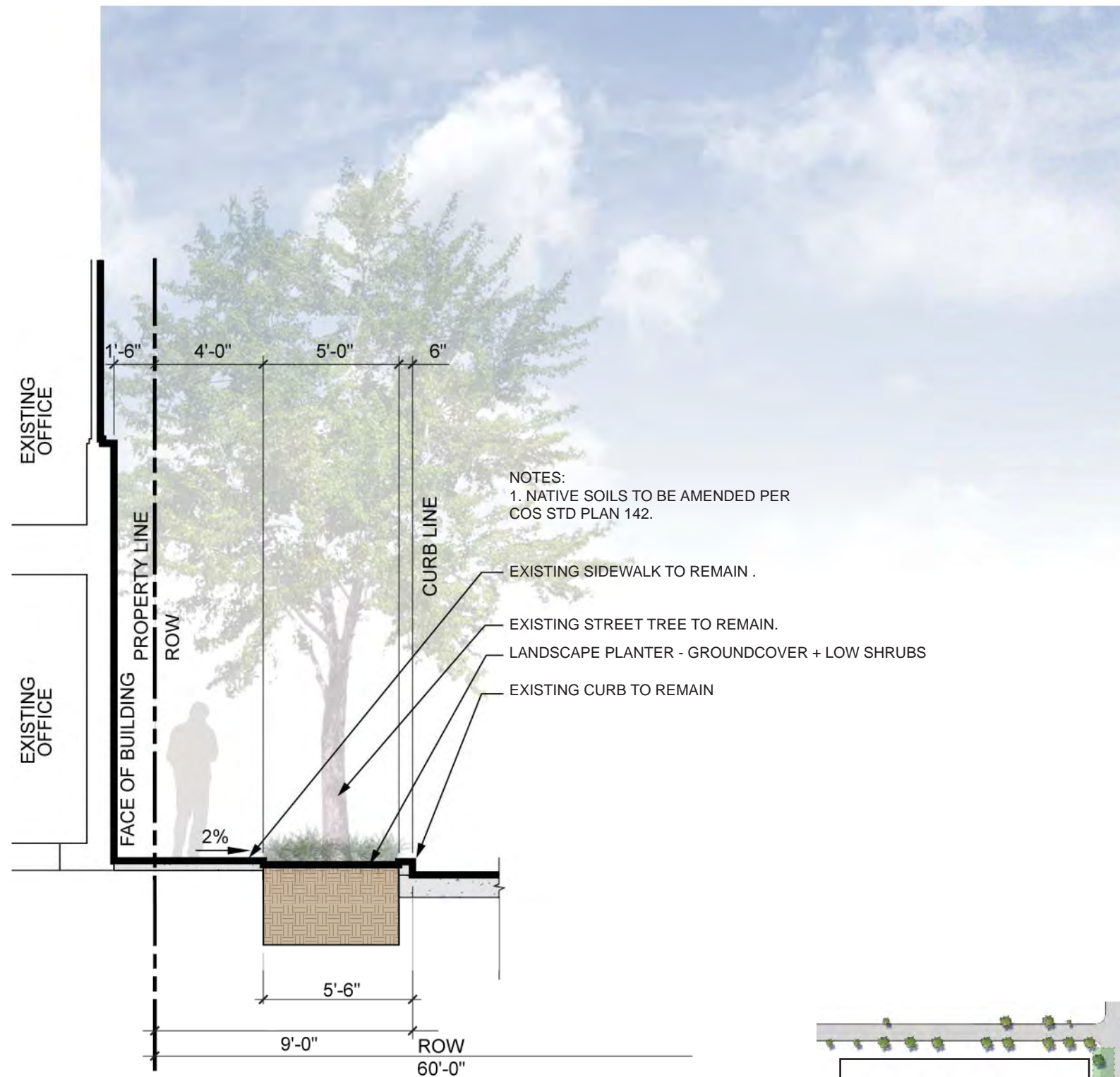


View looking South

Roosevelt Way NE Characteristics

The Roosevelt Way is a major southbound arterial with existing street trees (10) and standard 2'x2' cast-in-place concrete paving, that ranges in width from 6'-6" to 8'-6". Street tree planters are all approximately 6'-0" wide (meeting SDOT requirements) and vary in length. The proposed plan will preserve existing sidewalk widths, in an attempt to maximize the walk and preserve the fluid and heavy pedestrian foot traffic associated with Trader Joe's. Street trees will be replaced per recommendations of the city arborist. Small patches of cast in place concrete paving will be replaced per field conditions.

ROOSEVELT WAY NE street section

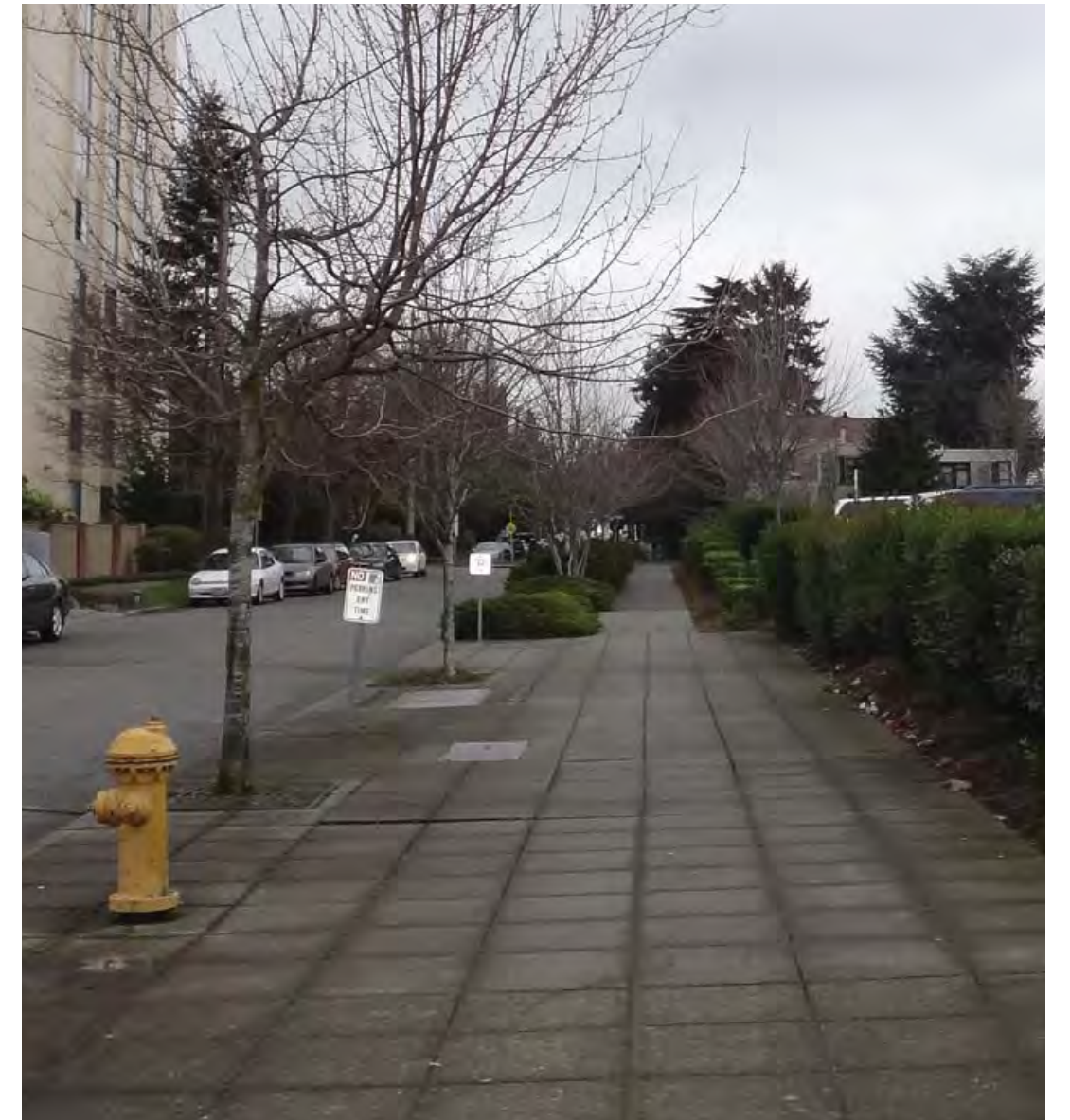
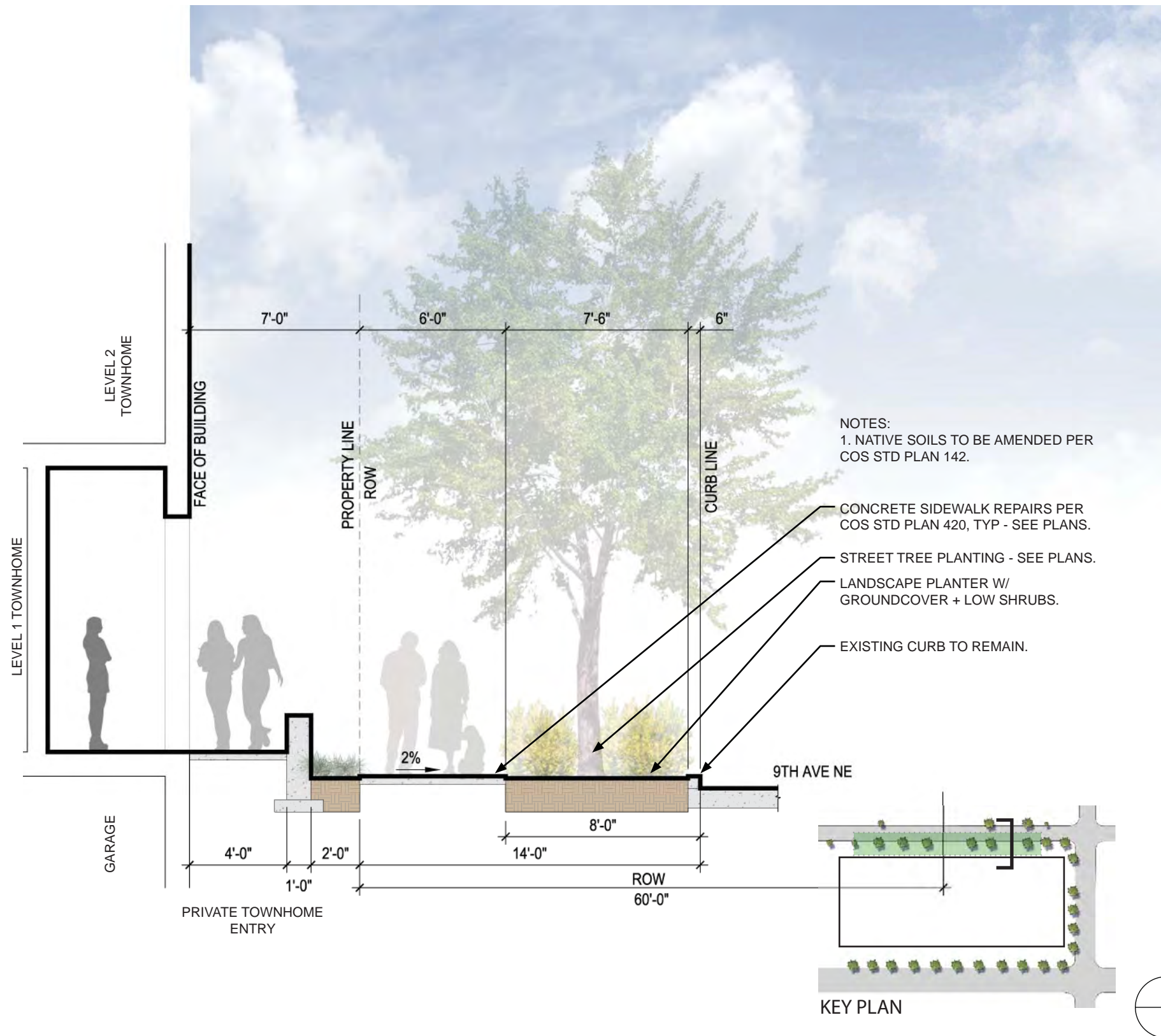


View looking West

NE 47TH Street Characteristics

NE 47th Street is a residential access street with parallel parking on both sides. This block acts as a transition between commercial and residential uses. The existing sidewalk ranges from 5'-5" (at the street tree planters) to 10'-0". With the existing structure to remain, sidewalk widths adjacent to the existing office use will not be compliant with SDOT standards. The existing sidewalk is in good condition and no concrete sidewalk improvements are planned for this block. Right of way improvements planned include soil amendments, per city arborist recommendations, and groundcover planting.

NE 47TH STREET street section



View looking North

9th Ave Characteristics

9th Avenue NE is a residential access street with street trees in good condition. The use of vegetated green screening at the future garage will enhance views for both drivers and pedestrians. The existing cast in place sidewalk ranges from 5'-10" to 21'-0" and is in good condition. Small patches of concrete may be replaced, per field conditions.



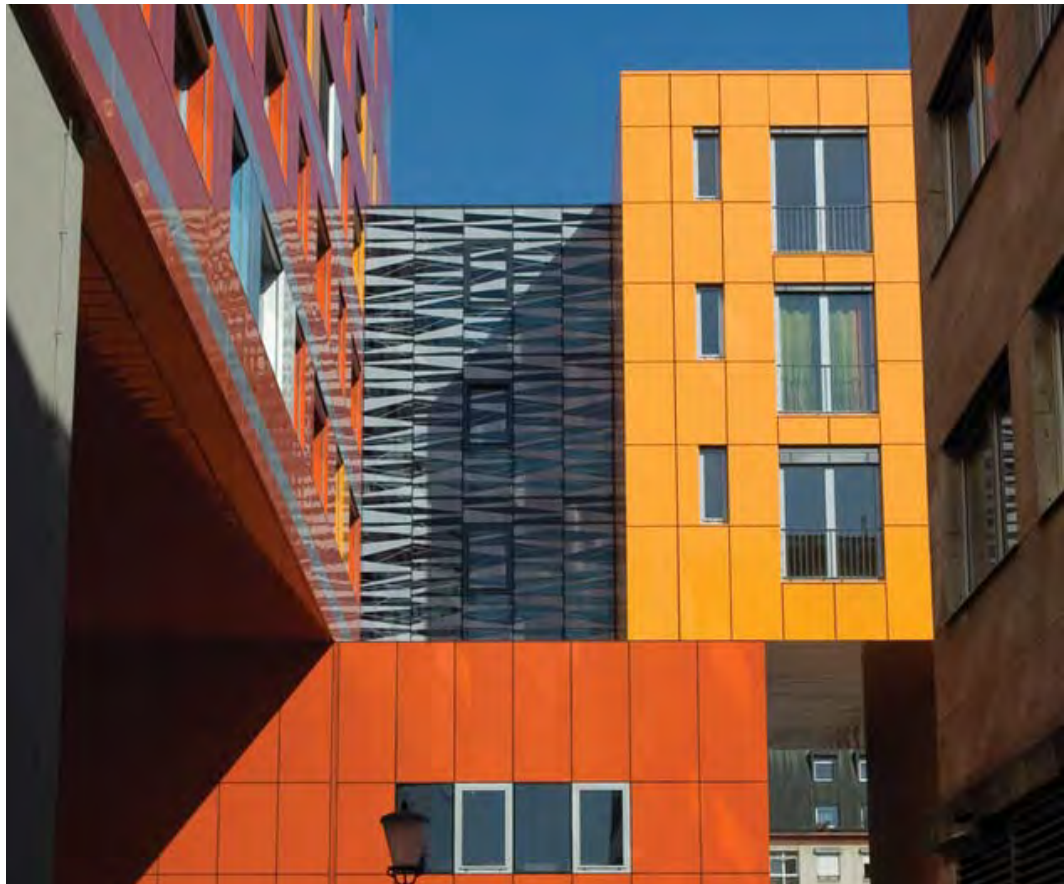
Greenscreen at Garage



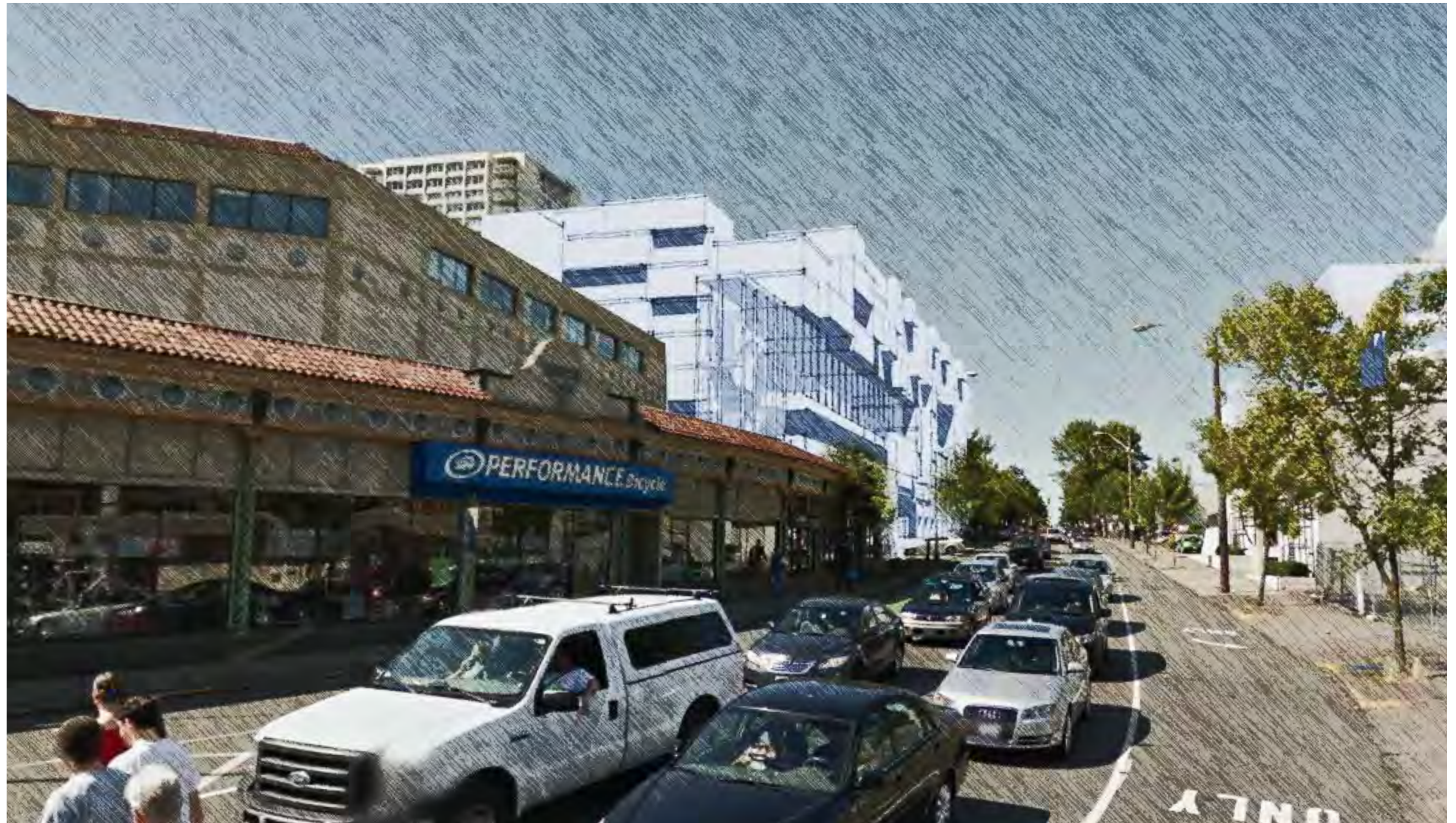
Townhome Entries

9TH AVENUE NE street section

ARCHITECTURAL CHARACTER



CONCEPT SKETCHES



SE CORNER LOOKING NORTH

CONCEPT SKETCHES



ENTRANCE COURTYARD FOR ALL OPTIONS

CONCEPT SKETCHES



NW CORNER LOOKING SOUTH

CONCEPT SKETCHES



VIEW OF COURTYARD ALL OPTIONS

CONCEPT SKETCHES



NE CORNER LOOKING SOUTH

CODE DEPARTURES

23.47A.008 Street-Level Development Standards

Blank segments of the street-facing facade between 2 feet and 8 feet above the sidewalk may not exceed 20 feet in width.

Response: On the North elevation of the existing building has a shear wall in which we can not penetrate. The East elevation we are proposing to cut openings into the panels and install perforated metal panels.

The total of all blank facade segments may not exceed 40 percent of the width of the facade of the structure along the street.

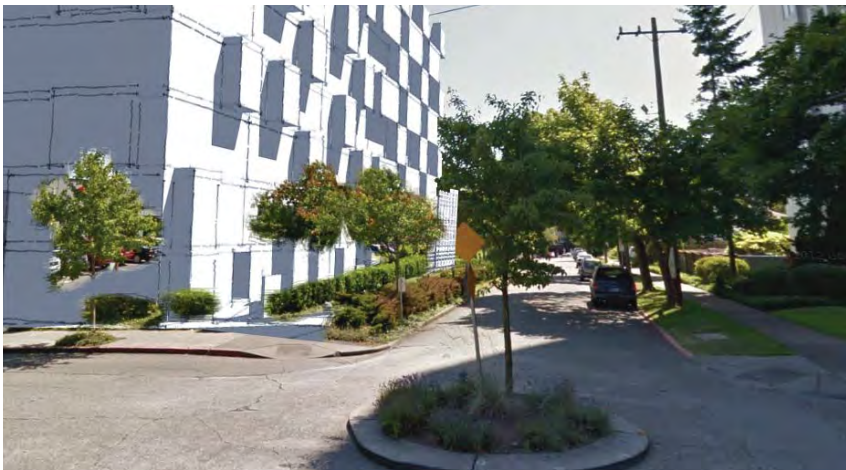
Response: On the North elevation of the existing building has a shear wall in which we can not penetrate. The East and partial West elevations we are proposing to cut openings into the panels and install perforated metal panels.

60% of nonresidential facade shall be transparent.

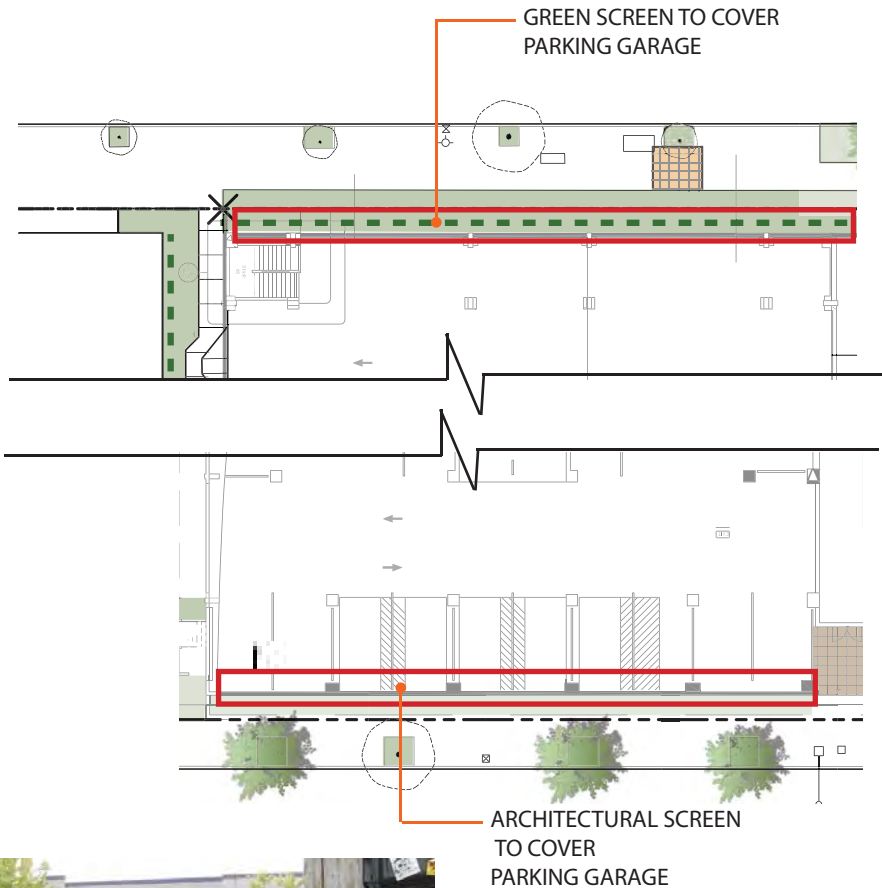
Response: No new openings will be created for the existing space.



VIEW ALONG NE 47TH ST



TOWNHOMES ALONG 9TH AVE NE



VIEW ALONG ROOSEVELT WAY NE

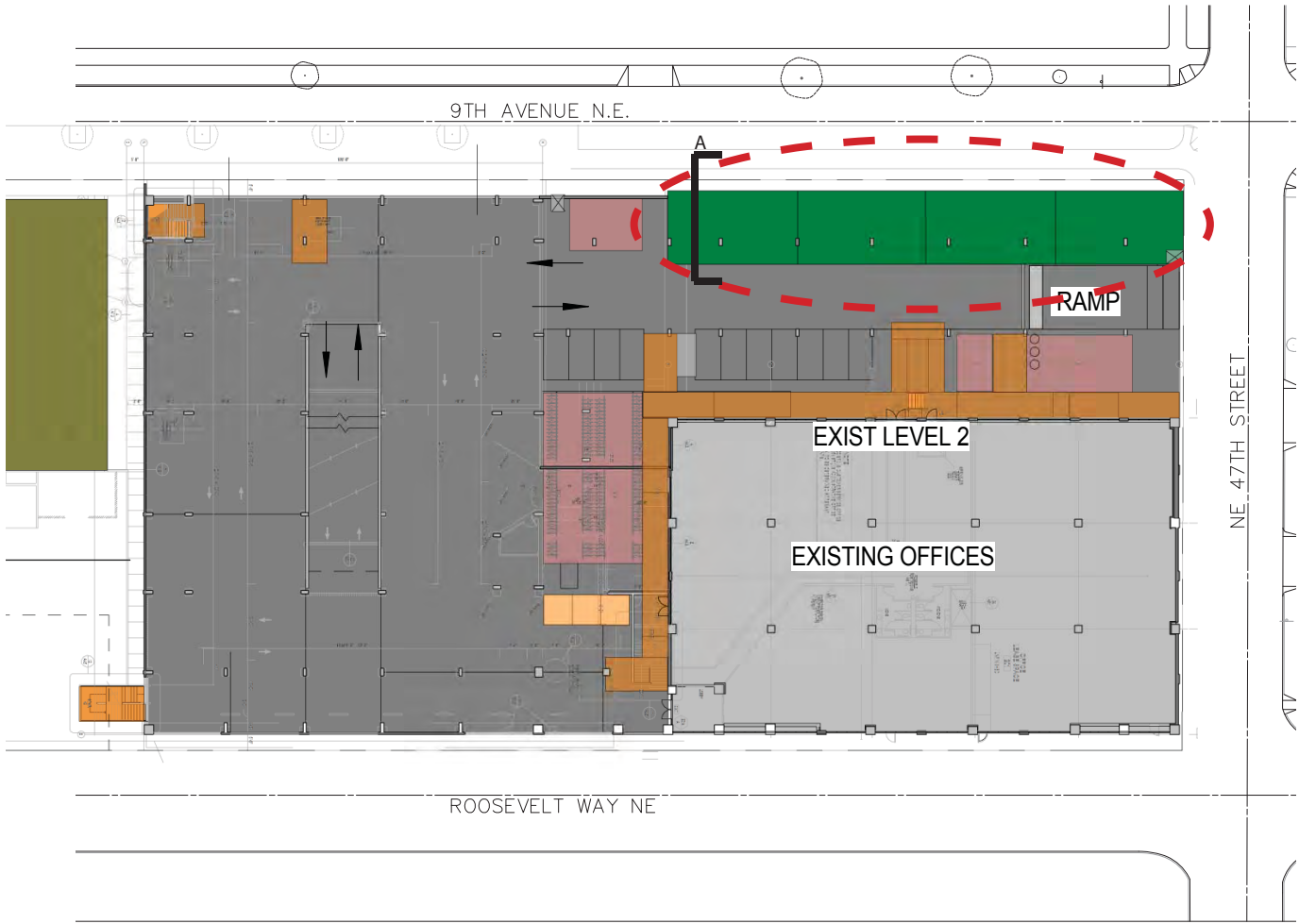


CODE DEPARTURES

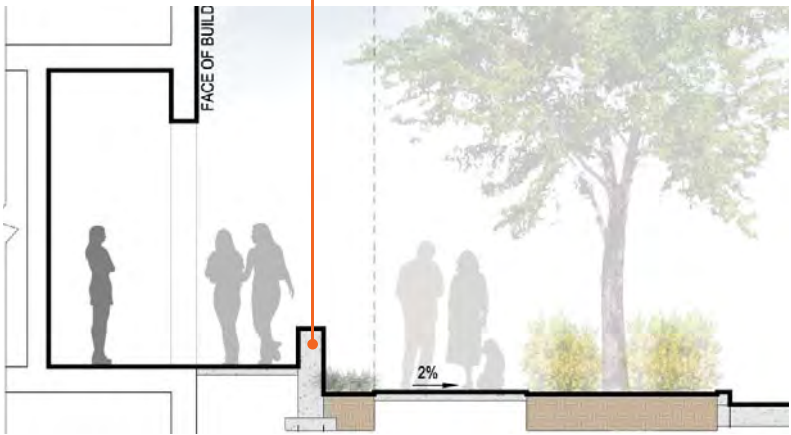
23.47A.008 Street-level Development Standards

Where Residential uses are located along a street-level-facing facade at least one of the facades containing a residential use shall have a visually prominent pedestrian entry and the floor of a dwelling unit located along the street-level street-facing facade shall be at least 4 feet above or 4 feet below sidewalk grade or be set back at least 10 feet from the sidewalk.

Response: To relate to the scale and the residential context along 9th Ave NE, the residential townhomes will be placed on sidewalk grade with a landscape buffer between the public sidewalk and the units.



ELEVATED TOWNHOMES WITH
PRIVACY WALL



SECTION A

CODE DEPARTURES

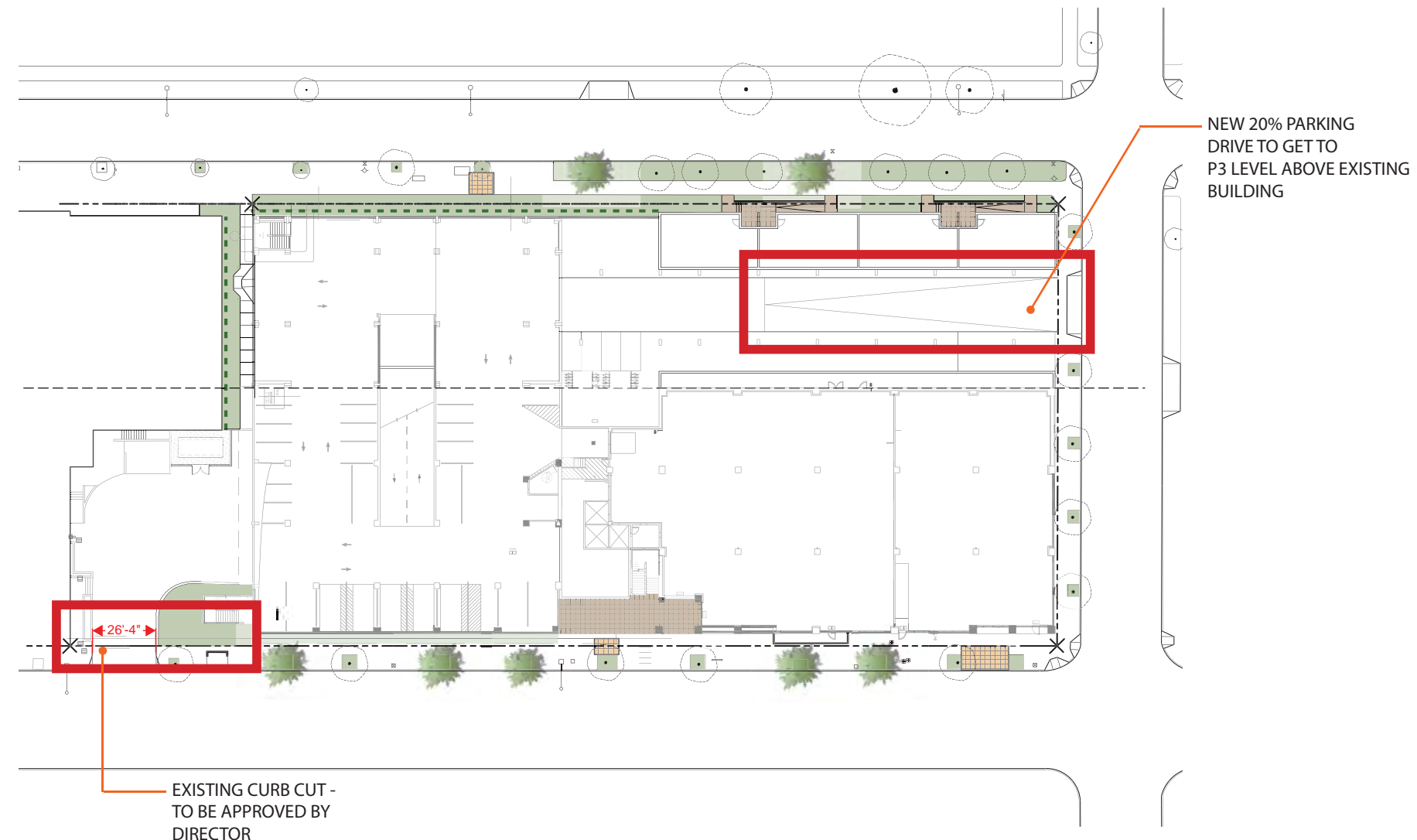
23.54.030 Parking Space Standards

Driveway slope for all uses shall not exceed a slope of 15 percent.

Response: Increasing the slope of the ramp off NE 47th Street to 20% will allow a straight run for vehicles to enter and exit the parking located on the existing roof structure above the office and Trader Joes. This minimizes the number of complicated vehicle turning movements required within the garage structure and improves safety and visibility.

For driveways that provide access to a solid waste management use the Director may allow both a maximum driveway width greater than the limits set in subsection 23.54.030.D.2.a and appropriate turning path radii, as determined necessary for truck maneuvering.

Response: Director to provide greater limit for driveway off of Roosevelt to allow for solid waste management.



CODE DEPARTURES

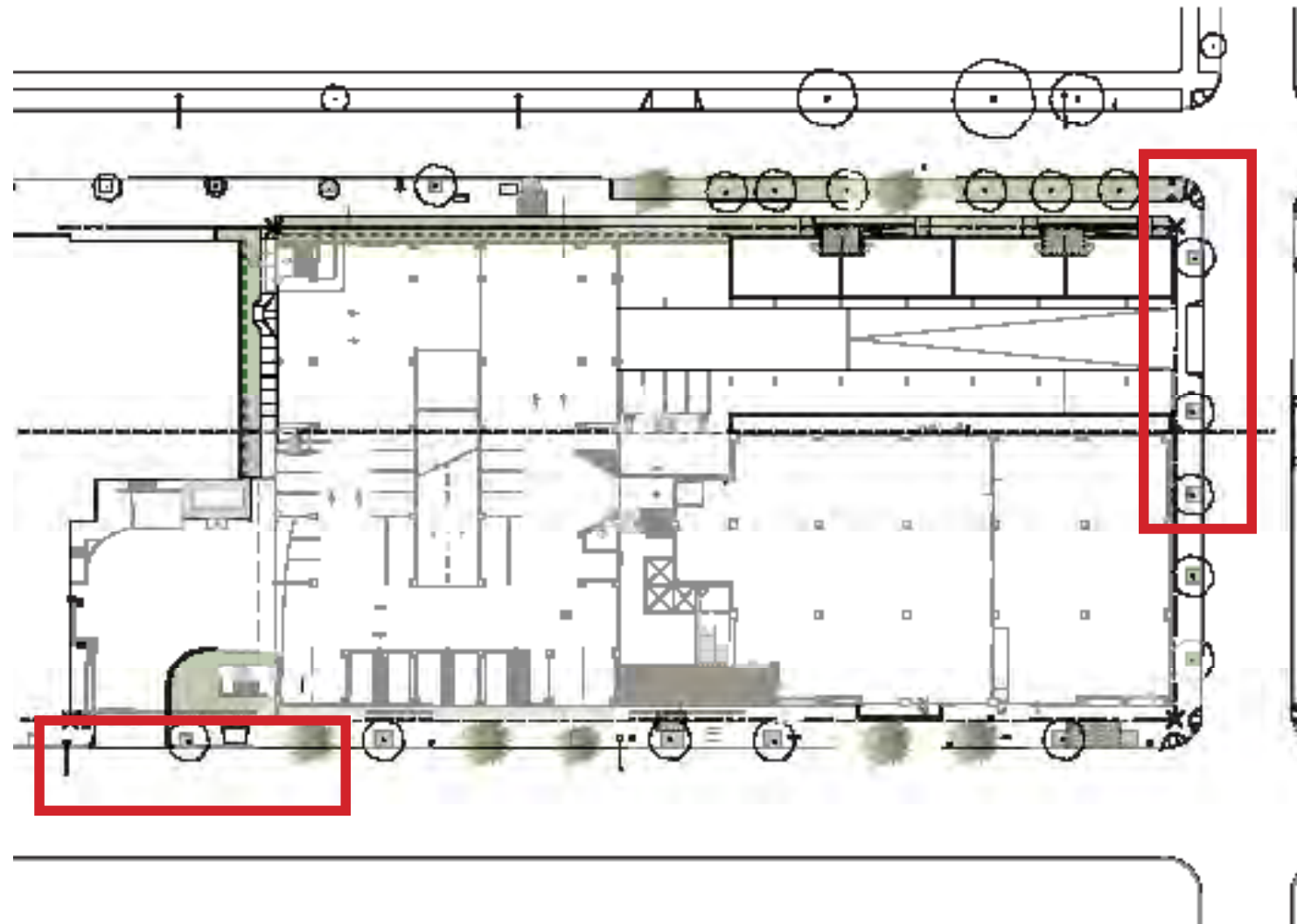
23.54.030 Continued

For two way driveways or easements 22 feet wide or more, a sight triangle on the side of the driveway used as an exit shall be provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection of the driveway or easement with a driveway, easement, sidewalk, or curb intersection if there is no sidewalk. The entrance and exit lanes shall be clearly identified.

Response: There are existing trees and light poles

The sight triangle shall also be kept clear of obstructions in the vertical spaces between 32 inches and 82 inches from the ground.

Response: There are existing site items.

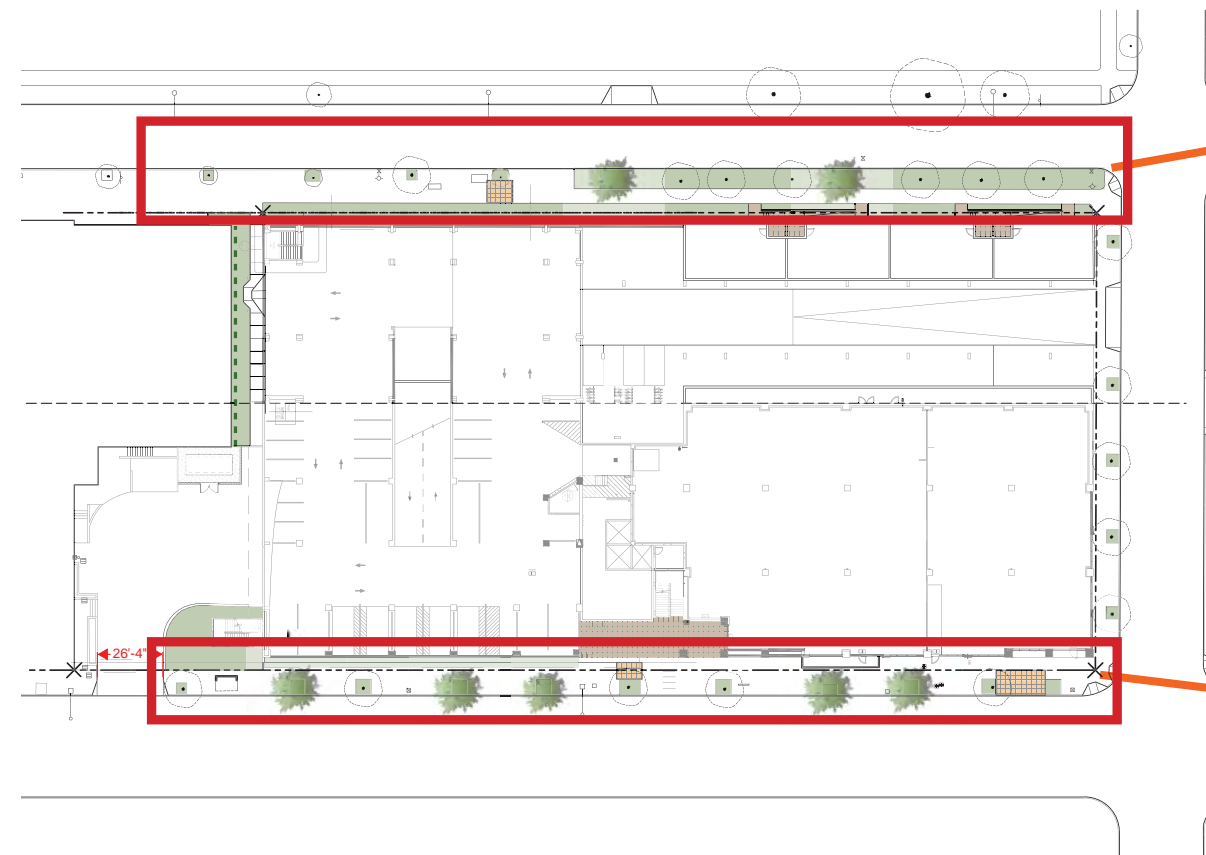


CODE DEPARTURES

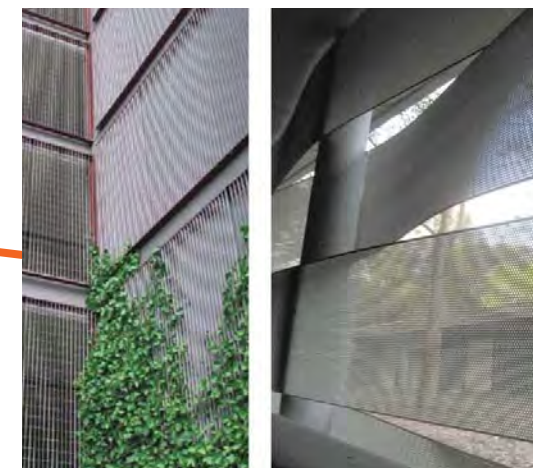
23.47A.022 Light and Glare Standards

To prevent vehicle lights from affecting adjacent properties, driveways and parking areas for more than two (2) vehicles shall be screened from adjacent properties by a fence or wall between five (5) feet and six (6) feet in height, or solid evergreen hedge or landscaped berm at least five (5) feet in height. If the elevation of the lot line is different from the finished elevation of the driveways or parking surface, the difference in elevation may substitute for a portion of the required height of the screen so long as the screen itself is a minimum of three (3) feet in height. The Director may waive the requirement for the screening if it is not needed due to changes in topography, agreements to maintain an existing fence, or the nature and location of adjacent uses.

Response: We intend on reusing the existing driveway and parking areas and adding green screens and perforated metal panels along the East and West Elevations.



GREEN SCREEN
PARKING SCREENING



ARCHITECTURAL
PARKING SCREENING

TEAM PROJECT EXAMPLES: ACC



TEAM PROJECT EXAMPLES: GGLO

