

DOWNTOWN DESIGN REVIEW BOARD

EARLY DESIGN GUIDANCE MEETING #2

07 JAN 2014

600 WALL STREET

DPD# 3015251

LACONIA 
DEVELOPMENT LLC

SEATTLE, WASHINGTON

 KwanHenmi

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Property Owner and Project Applicant

600 Wall Street Development LLC
1981 N Broadway, Suite 415
Walnut Creek, CA 94596

Submittal prepared by

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San Francisco, CA 94104



SITE INFORMATION



DEVELOPMENT OBJECTIVES

The proposal is to apply for a Master Use Permit (MUP) and Building Permit for an approximately 400 ft tall highrise multifamily development on the parcel bounded by 6th Ave. Wall St. and Denny Way in the Denny Triangle Urban Village.

Downtown Seattle is a world class city comprised of iconic high rise buildings that create an architecturally diverse and aesthetically pleasing skyline. In addition, Seattle has paid attention to the human scale environment, resulting in a lively and vibrant city center. In the last few years, visionary city planners and developers have begun to extend the urban cityscape along 6th Avenue toward the Denny Triangle. This extension includes the Via6 mixed-use community, the Amazon headquarters development and Bosa’s Insignia Towers. Older, utilitarian, low and mid-rise buildings are giving way to stunning new developments in an urban renaissance.

The elegant, slender tower at 600 Wall will read as the architectural exclamation point to this junction of the Denny Triangle, Bell Town and South Lake Union, Seattle’s “coolest” areas of urban renewal. At the street level, architectural details will enhance and energize the pedestrian experience.

The design team has been inspired by downtown Seattle and by the astonishing redevelopment taking place in the immediate vicinity of our site. The resulting design has not been created in a vacuum. It is not a design that could be successfully dropped onto any parcel of land in Minneapolis or San Diego or Portland. Instead, the design has been strongly influenced by its specific environment and is intended to coordinate with, to complement and enhance the revitalization of 6th Avenue.

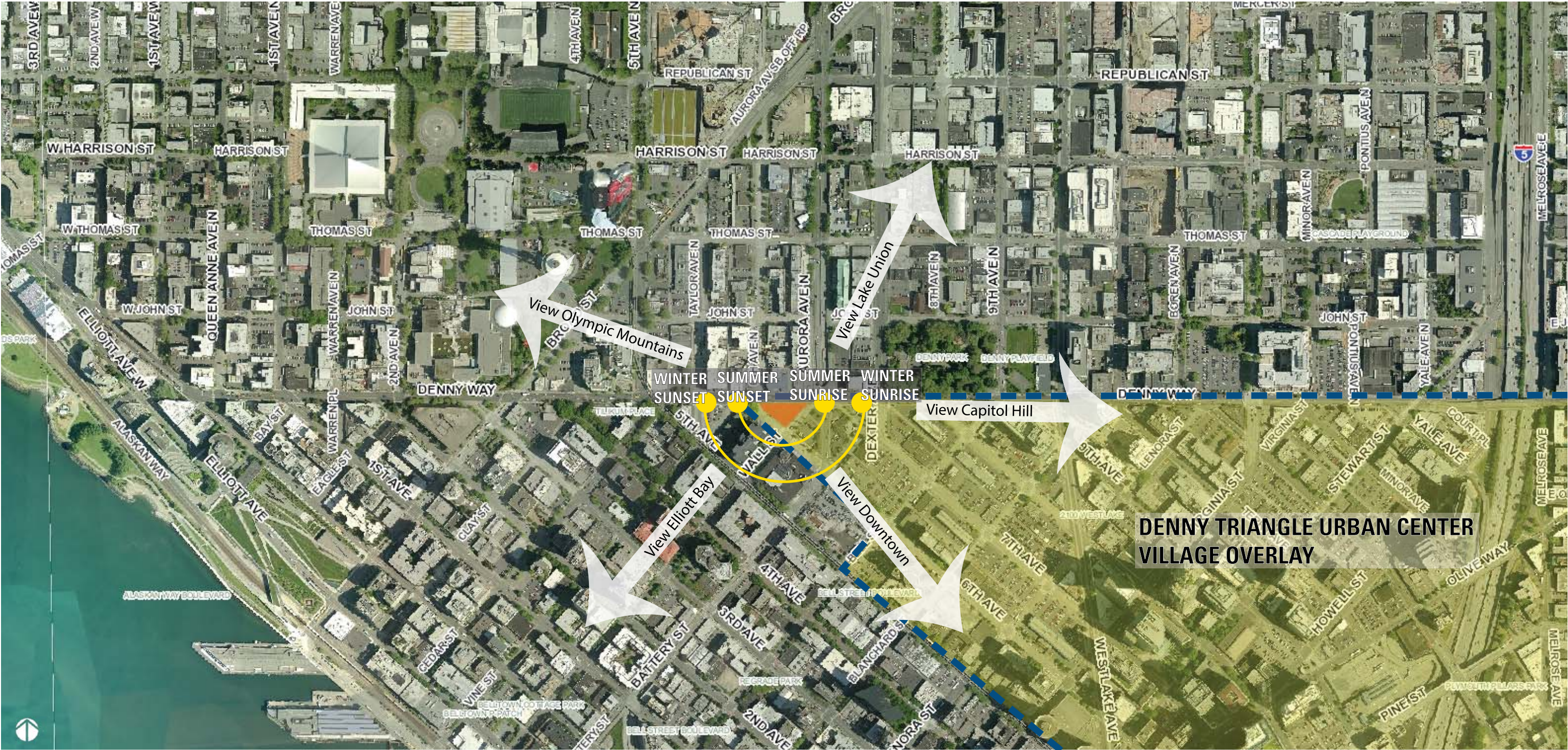
Summary of Responses:

- Street level plan changes
- Podium design
- Podium level plan revisions
- Street level activation
- Denny Way activation at street and podium levels

Project Data	
Project Data:	DPD Project #3015251
Building Address:	600 Wall Street Seattle, WA 98109
Legal Description:	Lots 1, 2 and 3, Block X, Bell's 6th Addition to the City of Seattle, according to the plat thereof recorded in vol. 2 of alley as vacated under Ord. No. 65236 and portion of Wall St. and Denny Way as vacated under Ord. No. 91279. Except that portion thereof condemned in King County Superior Court Cause No. 193437 for widening 6th Ave, as provided under City of Seattle Ord. No. 50890 and except those portions conveyed to the City of Seattle by deeds recorded under recording nos. 2848083 and 7408080115.
Assessor's Tax Parcel Number:	069700-0370-00
Current Zoning:	DMC 240/290-400
Gross Lot Area:	10,665 Square Feet
Project Description:	The project will consist of 39 floors above grade and 6 floors of below grade parking. Above grade levels will be comprised of: <ul style="list-style-type: none">- 1 Floor of Lobby/Retail/Parking Ramps- 5 Floors of Parking- 31 Floors of Residential- 2 Floors of Amenities
Project Unit Count:	310 Residential Units
Commercial Space:	1,800 Square Feet
Parking Count:	284 Stalls

SITE ANALYSIS

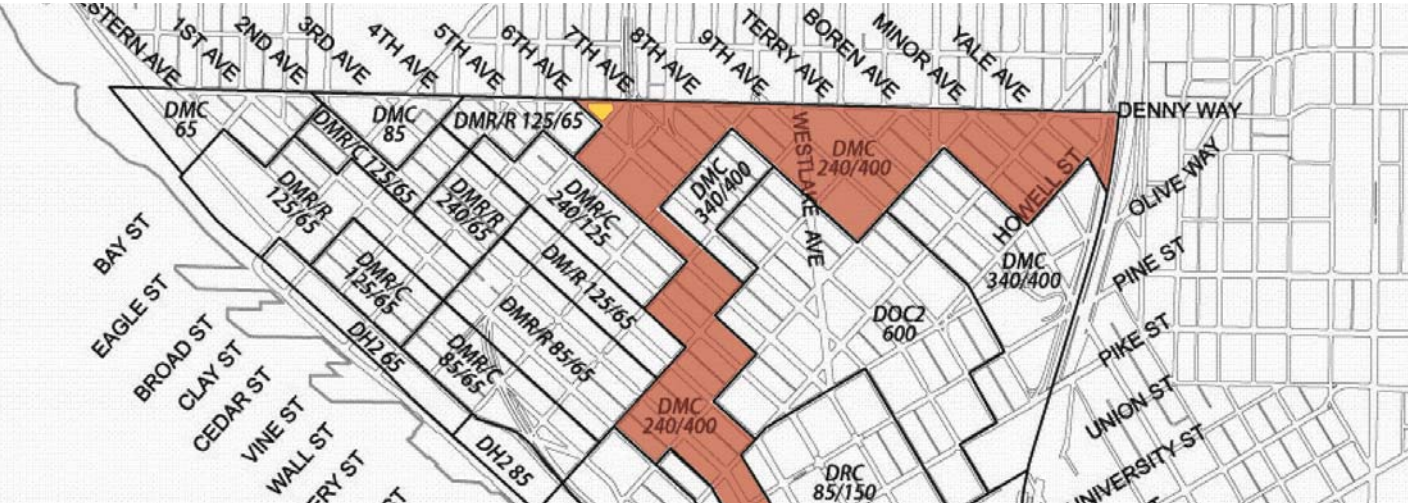
LARGER VICINITY MAP AND VIEWS





TOP Northwest view from 20th
 floor of proposed building
 ABOVE Southwest view from 20th
 floor of proposed building
 RIGHT Site footprint as seen from
 the Space Needle

SMC TITLE 23



23.49 Map 1A: Zoning

Code Section	Requirements	Proposed
23.49 Map 1 A Zoning	DMC 240/290-400 - Downtown Mixed Commercial	
23.49.008 Structure Height	400'	396'
23.49.010 B Common Recreation Area	<p>Common recreation area is required for all new development with more than twenty 20 dwelling units. Required common recreation area shall meet the following standards:</p> <ol style="list-style-type: none"> 1. 5 of gross residential area 302,040 15,102 sf. Lot area 10,665 sf. n no instance shall the amount of required common recreation area exceed the area of the lot. The common recreation area shall be available to all residents and may be provided at or above ground level. 2. A maximum of fifty 50 percent of the common recreation area may be enclosed. Maximum nterior Space Allowed 5,332 sf 3. The minimum hori ontal dimension for required common recreation areas shall be fifteen 15 feet, except for open space provided as landscaped setback area at street level, which shall have a minimum hori ontal dimension of ten 10 feet. No required common recreation area shall be less than two hundred twenty-five 225 square feet. 	<p>4,883 sf Open Space 7,239 sf nterior Recreation Floor Area 12 122 ot</p>

Code Section	Requirements	Proposed
23.49.011 FAR	Base: 5 Max: 7	62,300 sf
23.49.018 Overhead Weather Protection	Continuous - minimum 8' or extend within 2' of curb - whichever is less. 10' minimum and maximum 15' above sidewalk	Continuous Canopy 15' above side walk elevation
23.49.020 Demonstration of LEED Silver	If condition of permit.	TBD
23.49.024 View Corridor Requirements		Not Applicable - Map 1D
23.49.056 Façade, landscaping, Setback Requirements	<p>Minimum Façade Heights</p> <p>6th Ave. - 15' Wall St. - 15' Denny Way - 15'</p> <p>Setback Limits</p> <p>Setback Limits for Property Line Facades. The following setback limits apply to all streets designated on Map 1H as requiring property line facades.</p> <p>a. The facades of structures 15 feet or less in height shall be located within 2 feet of the street lot line.</p> <p>b. Structures greater than 15 feet in height are governed by the following criteria:</p> <p>1) No setback limits apply up to an elevation of 15 feet above sidewalk grade.</p> <p>2) Between the elevations of 15 and 35 feet above sidewalk grade, the facade shall be located within 2 feet of the street lot line, except that:</p> <p>a) Any exterior public open space that satisfies the Downtown Amenity Standards, whether it receives a bonus or not, and any outdoor common recreation area required for residential uses, is not considered part of the setback.</p> <p>b) iv. The facade of the structure shall return to within 2 feet of the street lot line between each setback area for a minimum of 10 feet. Balcony railings and other nonstructural features or walls are not considered the facade of the structure.</p>	<p>15' 15' 15'</p> <p>NA - Map 1H NA</p>

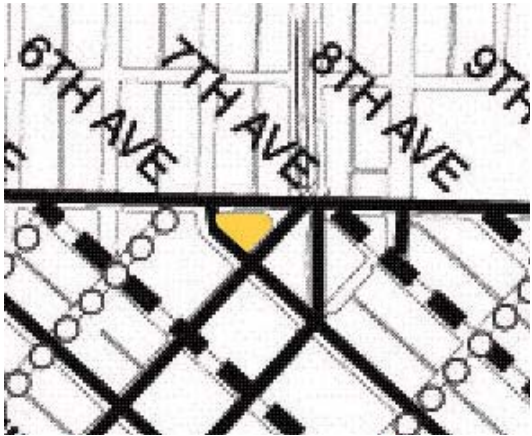
Code Section	Requirements	Proposed	Code Section	Requirements	Proposed
	Façade Transparency Facade transparency requirements apply to the area of the facade between 2 feet and 8 feet above the sidewalk, except that if the slope along the street frontage of the facade exceeds 7.5 percent, the transparency requirements apply to the area of the facade between 4 feet and 8 feet above sidewalk grade. Only clear or lightly tinted glass in windows, doors, and display windows is considered to be transparent. Transparent areas shall allow views into the structure or into display windows from the outside. Facade transparency requirements do not apply to portions of structures in residential use			d. All plant material shall be planted directly in the ground or in permanently installed planters where planting in the ground is not feasible. A minimum of 50 percent of the plant material shall be perennial.	
	Blank Façade Limits Blank facade limits apply to the area of the facade between 2 feet and 8 feet above the sidewalk, except that where the slope along the street frontage of the facade exceeds 7.5 percent, blank facade limits apply to the area of the facade between 4 feet and 8 feet above sidewalk grade. Any portion of a facade that is not transparent shall be considered to be a blank facade. Blank facade limits do not apply to portions of structures in residential use	There will not be any blank facades exceeding 25' in length	23.49.058 Table 23.49.058 D Maximum gross residential floor area	Development Standards Average gross residential floor area: 10,000 sf Maximum Façade Width* Maximum Residential Floor area on any level: 11,500 sf In DMC zones, the maximum facade width for portions of a building above eighty-five (85) feet along the general north/south axis of a site (parallel to the Avenues) shall be one hundred twenty (120) feet or eighty (80) percent of the width of the lot measured on the Avenue.	9,640 sf 6th Ave.:144'
	Setback and Landscaping requirements within the Denny Triangle Urban Village Landscaping in the Street Right-of-Way for All Streets Other Than Those With Green Street Plans Approved by Director's Rule. All new development in DMC zones in the Denny Triangle Urban Village, as shown on Exhibit F for 23.49.056, shall provide landscaping in the sidewalk area of the street right-of-way, except on streets with a green street plan approved by Director's Rule. The square footage of landscaped area provided shall be at least 1.5 times the length of the street lot line (in linear feet). The following standards apply to the required landscaped area: a. The landscaped area shall be at least 18 inches wide and shall be located in the public right-of-way along the entire length of the street lot line, except for building entrances, vehicular access or other connections between the sidewalk and the lot, provided that the exceptions may not exceed 50 percent of the total length of the street lot line(s). b. As an alternative to locating the landscaping at the street lot line, all or a portion of the required landscaped area may be provided in the sidewalk area within 5 feet of the curb line. c. Landscaping provided within 5 feet of the curb line shall be located and designed in relation to the required street tree planting and be compatible with use of the curb line for parking and loading.	See Landscape Plans See Landscape Plans See Landscape Plans See Landscape Plans	23.54.015, 23.54.019, 23.54.030 Parking 23.49.01.2 Parking* Table 23.49.019 A Bicycle Parking	No parking, either long-term or short-term, is required for uses on lots in Downtown zones. (2) Parking above the third story of a structure shall be separated from the street by another use for a minimum of thirty (30) percent of each street frontage of the structure. For structures on lots located at street intersections, the separation by another use shall be provided at the corner portion(s) of the structure. (3) The perimeter of each story of parking above the first story of the structure shall have an opaque screen at least three and one-half (3 1/2) feet high where the parking is not separated from the street by another use. 1 space / 2 dwelling units - 155 required	284 automated parking spaces Departure requested for separation of uses 155

* - See Anticipated Departure Requests

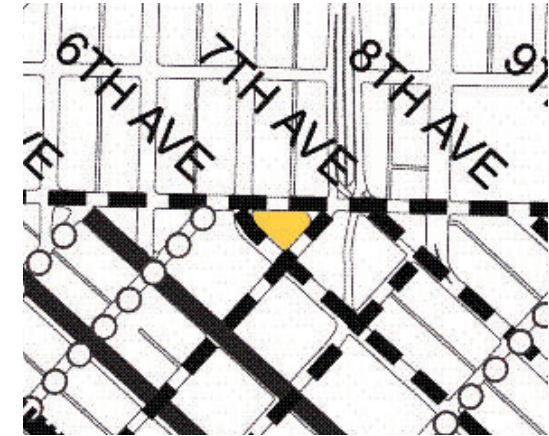
Code Section	Requirements	Proposed
23.54.040 Solid Waste & Recyclable Materials	<p>Parking spaces required by Section 23.54.015, and required barrier-free parking, shall meet the standards of this Section 23.54.030. Parking for residential uses provided in excess of the quantity required by Section 23.54.015 is exempt from the requirements of subsections 23.54.030.A and 23.54.030.B.</p> <p>Provide 575 SF for first 100 units + 4sf for each additional unit above 100 + 50% of storage for non residential space less 15% for minimum 20' horizontal dimension. 575sf + (210x4sf + 82 sfx 50%) less 15%= 1202 SF.</p>	1,220 sf

23.49 Map 1B Street Classifications	Principal Arterial	All three streets are prinicpal arterial
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23.49 Map 1F Pedestrian Classification	Class II	<p>Residential</p> <p>Street Level Uses Overhead weather protection All three streets are Class II Pedestrian</p>
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23.49 Map 1B



23.49 Map 1F

Access Street

Principal Arterial
(other than Principal Transit Street)

Minor Arterial
(other than Principal Transit Street)

Green Street

Principal Transit Street

Access to parking regulated by Special Review or Historic District regulations. See Map D for 23.66.170 and Map B for 23.66.326 for street classifications in South Downtown.

Class I

Class II

Green Street

Streets with pedestrian street classifications within the Pioneer Square Preservation District and International Special Review District are identified on Map D for 23.66.170 and Map B for 23.66.326.

Pedestrian Street Classifications may be addressed by Special Review, Historic District or other provisions

NEIGHBORHOOD CONTEXT

URBAN NETWORK



- Bus Stop
- Existing Trees
- One Way Street
- Two Way Street
- Elevated Monorail
- Tunnel
- Bike Lane

Approximate building heights are shown on building footprints.

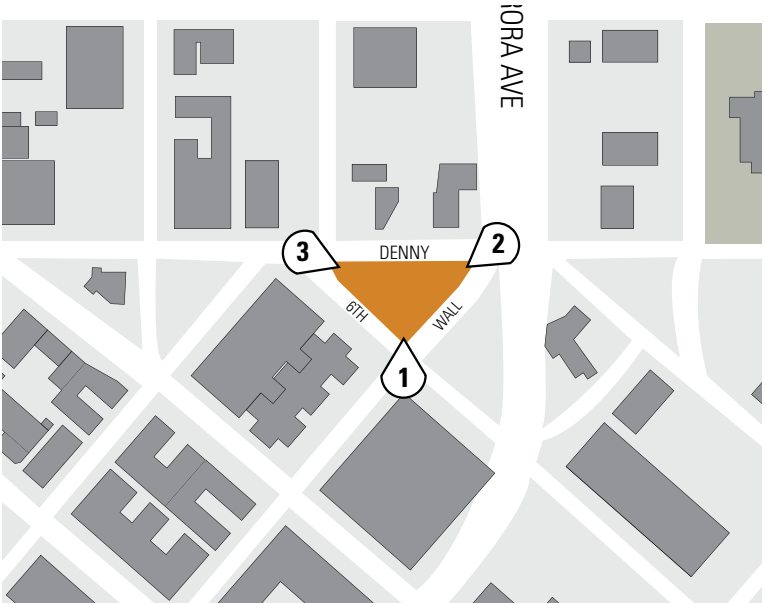
LANDMARKS



- A. EMP Museum
- B. Space Needle
- C. Pacific Science Center
- D. Taylor 28 Apartments
- E. Archstone Belltown Apartments
- F. City University of Seattle
- G. Hyatt Place
- H. Shell Gas Station
- I. Elephant Super Car Wash
- J. Denny Park

SITE CONDITIONS

STREETSCAPE



② Wall and Denny



① 6th and Wall



③ Denny and 6th

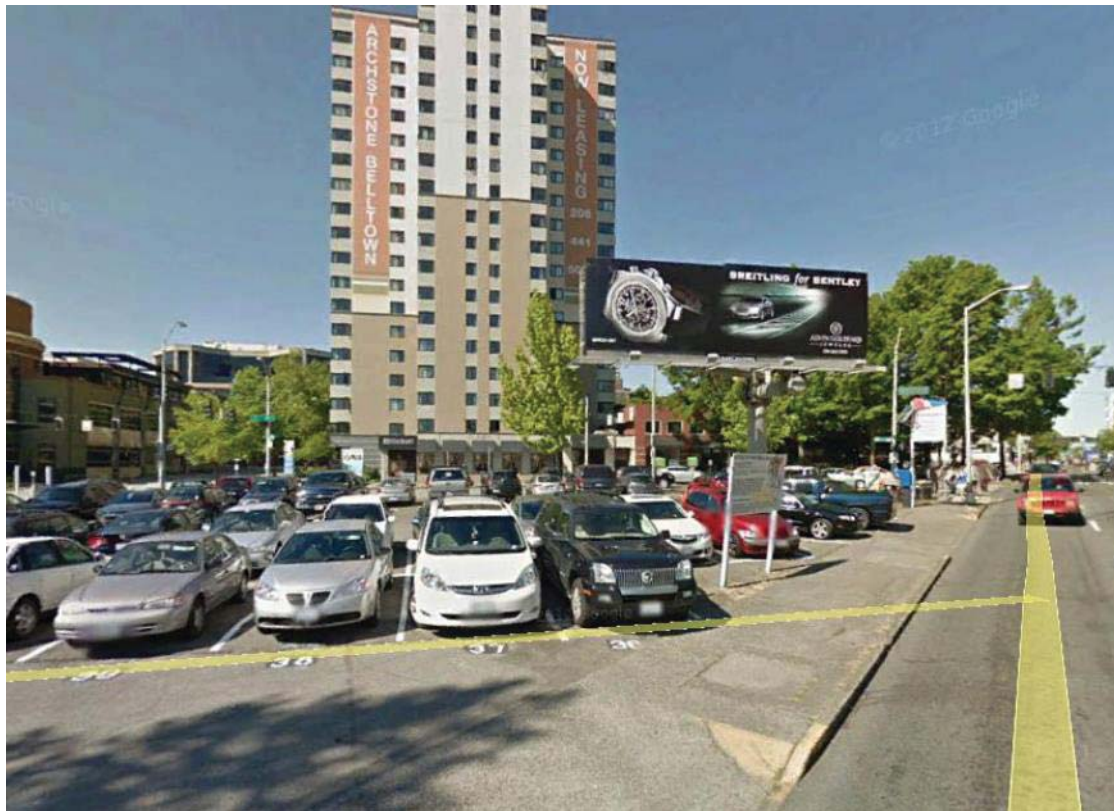
ADDITIONAL VIEWS OF SITE



(left) South on 6th Avenue
(right) West on Wall Street



(left) East from 6th Avenue and Wall Street
(right) Denny and Wall Street



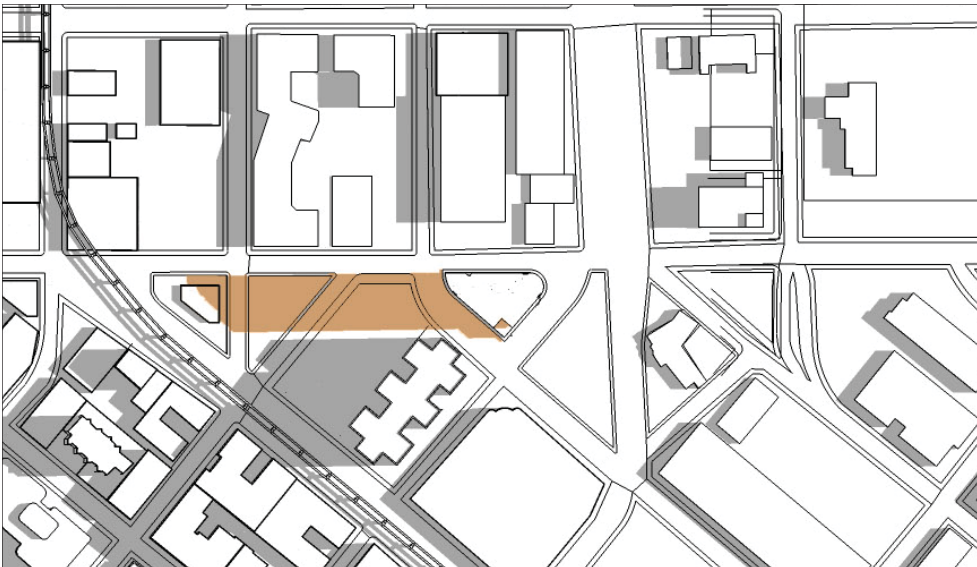
(left) Site from Denny Way
(right) West on Wall Street



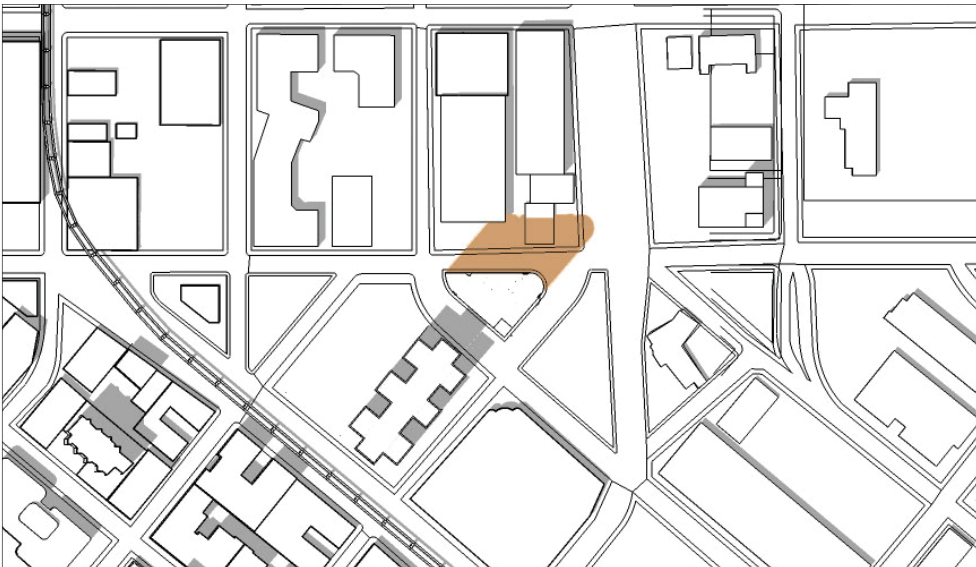
(left) Denny and 6th Avenue
(right) Downtown from Denny and Wall Street

SHADOW STUDY

SUMMER AND WINTER SOLSTICES



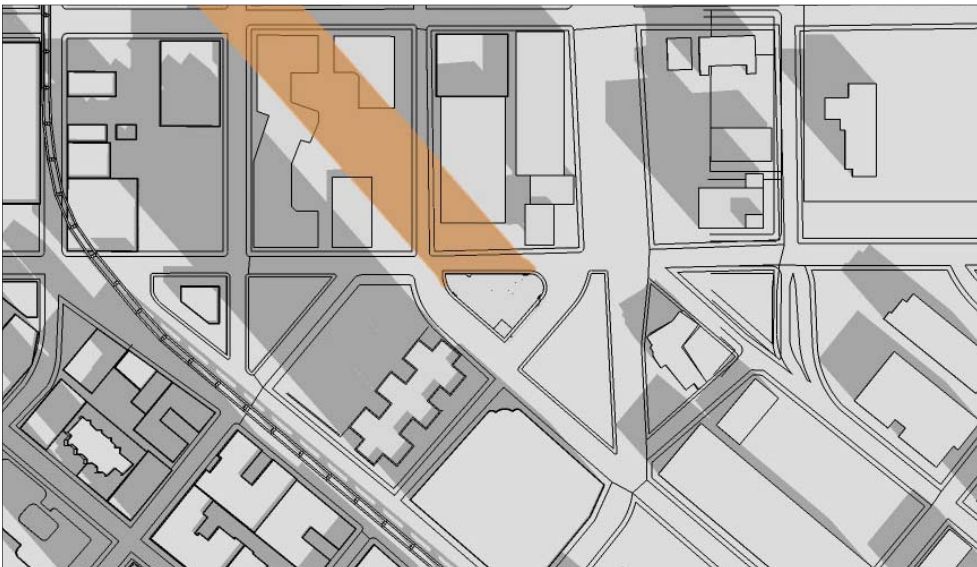
JUNE 8AM



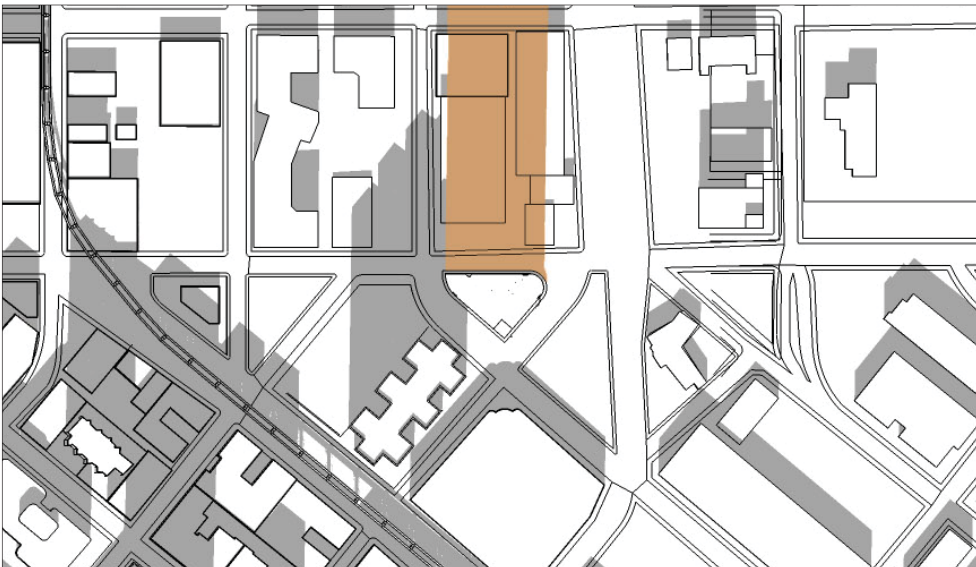
JUNE 1PM



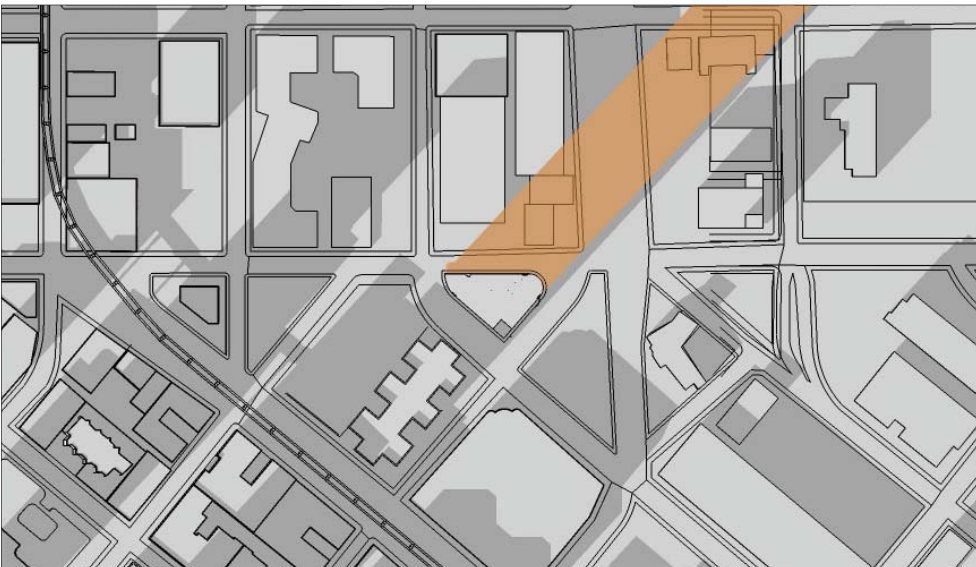
JUNE 6PM



DECEMBER 9AM



DECEMBER 12PM



DECEMBER 3PM





DESIGN GUIDANCE

RESPONSES TO DRB

A. Site Planning & Massing

Responding to the Larger Context

A-1 Respond to the Physical Environment. Develop an architectural concept and compose the building’s massing in response to geographic conditions and patterns of urban form found beyond the immediate context of the building site.

At the Early Design Guidance Meeting, the Board discussed how this strategic site, while small and constrained, offers exceptional visibility and prominence in the city, and thus deserves a persuasive design concept that responds to that unusual site and its context. The Board requested more understanding of how the context and street level patterns inform the lower levels and tower massing, beyond a pure extrusion. The acute corners will be very prominent at proximate and distant viewpoints, so they require special analysis, and may be different expressions based on the specific conditions of this ‘pivot’ between grids. Also see comments under B-2 about the podium scale.

Response:

- Base of tower has been redesigned to create a stronger building base and podium design element.
- Corner elements and uses have been refined to anchor the tower to the street.
- The facade at the parking levels creatively showcases the automobile to the general public.
- The Denny Way podium base has a very distinct vocabulary and scale to emphasize the amenity spaces behind the façade. It is intended to act somewhat as a stage framed by a terra cotta surround.

A-2 Enhance the Skyline. Design the upper portion of the building to promote visual interest and variety in the downtown skyline.

At the Early Design Guidance Meeting, the Board discussed how the building, especially the top, would be visible from many locations and distances, including the sizable traffic flowing east and west on

Denny Way, north-south on Aurora, and the Mercer Street off-ramp. To fully test the tower top and profile on the skyline, future meetings should include multiple perspective simulations from key locations.

Response:

- Multiple perspectives and close up views are provided from different vantage points.

Relating to the Neighborhood Context

B-2 Create a Transition in Bulk & Scale. Compose the massing of the building to create a transition to the height, bulk, and scale of development in neighboring or nearby less intensive zones.

At the Early Design Guidance Meeting, the Board commented that the immediate context is diverse, but exhibits an important ‘urban’ podium datum of 5-7 stories, which is currently not expressed in the proposal. This height also corresponds with the proposed parking/studios above grade, which should be expressed more authentically, especially the two-story studios at the corners. To confirm the relationship of uses to facades, future meetings should include large scale cross sections of the first 7 floors at several locations, including the streetscape to curb, and corresponding façade treatments side-by-side with the scale sections.

Response:

- The redesigned base creates a podium vocabulary which is more consistent with parapet and roof heights of many of the adjacent neighborhood buildings.
- A podium height of approximately 58’ is consistent with building heights of 5 to 6 stories.
- We have included building sections and landscape sections showing streetscape to curb and landscape planter widths.

B-4 Design a Well-Proportioned & Unified Building. Compose the massing and organize the publicly accessible interior and exterior spaces to create a well-proportioned building that exhibits a coherent architectural concept. Design the architectural elements and finish details to create a unified building, so that all components appear

integral to the whole.

At the Early Design Guidance Meeting, the Board discussed the parking program and functions of the lower section of the tower at length. While supportive of the efficiencies of the proposed robotic parking, the Board agreed any above grade parking is undesirable, since it replaces truly active facades and human uses. To fully understand the constraints, the Board requested the following explanations at future meetings:

- Sections to better understand why the proposed system is not used deeper than B6 on the south and east robotic banks, opposite the tunnel constraint (to reduce/eliminate the amount of parking above-grade); description of why the storage lockers and mechanical rooms on levels 26 cannot be replaced with more activating studios;
- Diagrams of ramps and precise car movements on B1; and convenient provision of bikes and car-share (on B-1 or 1) to reduce/minimize the high parking ratio in this dense urban location.

The Board also discussed the facades of the parking/studio floors at length, and was concerned they display a confusing design logic. The Board agreed the 2 story studios add an important double scale, and should be expressed as 2 stories, while the parking portions between (if retained) should not be disguised like glass residential units. The large framing elements above these floors create scale, but they should not simply emerge from the ground, and maybe they should be different on 1 or 2 sides of the triangle. There was particular concern about the 160 ft. width and absence of deep modulation along the Denny wall (see departure #1 comments).

Response:

- A sectional studying the additional depth of parking would result in further encroachment into the WASHDOT tunnel zone.
- We have been in discussions with car share programs, preliminary indications are that potential needs are substantial due to it’s strategic location to some future major employment developments in the area and the demand for car share facilities

ties could be significant.

- The podium facades and floor plans have been redesigned to emphasize a base for the tower.
- The mix of street levels and podium uses will help enhance the pedestrian experience.
- The mechanical and storage spaces have been relocated to below grade levels. The above grade uses along Denny are now a mix of multilevel amenity spaces including spaces for wine bars, art studios and some building management support offices.
- The podium design takes a cue from the larger vertical frames on the tower. The frames in a horizontal orientation at the base reinforces the notion of a podium and the different uses within this zone.

Creating the Pedestrian Environment

C-1 Promote Pedestrian Interaction. Spaces for street level uses should be designed to engage pedestrians with the activities occurring within them. Sidewalk-related spaces should be open to the general public and appear safe and welcoming.

At the Early Design Guidance Meeting, the Board supported the corner bulb-outs and expanded streetscape proposed, but was concerned the ground level uses and design treatment do not support the positive streetscape created by these dimensions, in particular at the enlarged northeast and southwest bulb-outs. The two commercial spaces appear shallow and hardly viable, and the Board requested more commercial uses along the Wall Street frontage. The Board requested the walkable sidewalk on Denny Way be widened, and/or the storefront be set back more.

Response:

- Redesign of the street level floor has resulted in larger and more functional commercial spaces at street level and at all corners.
- Retail space has been added at the corner of Wall St and 6th Ave. with the primary frontage on Wall. The commercial space will be designed to open into the residential lobby so that the

lobby will be further activated as a semipublic space.

- The building storefront is set back 2' from the property line resulting in a 7' wide sidewalk along Denny Way.

C-2 Design Facades of Many Scales. Design architectural features, fenestration patterns, and materials compositions that refer to the scale of human activities contained within. Building facades should be composed of elements scaled to promote pedestrian comfort, safety, and orientation.

At the Early Design Guidance Meeting, the Board agreed the entire podium and ground floor facades require a design concept independent of the extruded tower above, incorporating maximized active uses at grade and the special opportunity of a triangular site seen fully from all sides (eg not boxed into a typical block).

Response:

- The base of the building has been redesigned to create a podium element. The podium design elements are now responsive to the uses that they are tied into.
- The concept of vertical frame elements on the tower facades have been reinterpreted into horizontal frames to define a base and podium.
- The frames are used to define the building uses and to change scales at the building base.

C-3 Provide Active—Not Blank—Facades. Buildings should not have large blank walls facing the street, especially near sidewalks.

At the Early Design Guidance Meeting, the Board supported the small parking portals and their basic two locations, and agreed transparent glass showing the operating car lifts would provide positive activation at the street and podium levels. The Board was concerned about the large square footage of residential 'support spaces' at grade, beyond a typical lobby and internal mailroom, and requested an explanation why the support spaces could not be located above the compressed and valuable street level.

Response:

- Street Level plan has been redesigned resulting in an increase of commercial uses along the street frontages.
- The relocation of the transformer access area has resulted in a more functional retail space at the corner of Denny and Wall.
- Added a retail space at Wall and 6th Ave. — this retail space will also tie into the residential lobby area so that it is a semi-public space similar to a hotel lobby.

Enhancing the Streetscape & Open Space

D-1 Provide Inviting & Usable Open Space. Design public open spaces to promote a visually pleasing, safe, and active environment for workers, residents, and visitors. Views and solar access from the principal area of the open space should be especially emphasized.

At the Early Design Guidance Meeting, the Board supported the expanded sidewalks and bulb-outs, and requested more detailed information on the plant species, pavers, furnishings, lighting and other landscape details at future meetings.

Response:

- We have revised the landscaping plan to provide additional pedestrian buffer along Denny Way as well as to provide additional pedestrian amenity for the retail space at Wall and 6th Ave.

D-3 Provide Elements that Define the Place. Provide special elements on the facades, within public open spaces, or on the sidewalk to create a distinct, attractive, and memorable "sense of place" associated with the building.

At the Early Design Guidance Meeting, the Board agreed the bulb-outs, working in conjunction with the adjacent building edges and commercial uses, are the prime opportunity for public place-making on this site, and to repair an existing dead zone with no pedestrian amenity. This distinctive triangular site at the grid-shift, provides cues for a memorable site-specific landscape design of these bulb —

outs, expressing the transition between neighborhoods.

Response:

- The landscape design has been revised. Hardscape paving design accentuates plazas and building entrances. Vertical design elements along Denny help create a buffer between the street and sidewalk.

Minimizing the Adverse Impacts

E-2 Integrate Parking Facilities. Minimize the visual impact of parking by integrating parking facilities with surrounding development. Incorporate architectural treatments or suitable landscaping to provide for the safety and comfort of people using the facility as well as those walking by.

At the Early Design Guidance Meeting, the Board understood the parking portal locations are possibly optimum, but requested detailed and dimensioned sections to better understand the curving position and height of the ramps to level B1. If possible, the ramps should be re-positioned to increase the size, depth and viability of all commercial spaces.

Response:

- We have provided ramp and building sections to demonstrate the functional circulation of the ramps, this along with study of the sidewalk grades we have been able to substantially increase commercial depths along Denny (ranging from a minimum of 15’ to 20’ in depth.

E-3 Minimize the Presence of Service Areas. Locate service areas for trash dumpsters, loading docks, mechanical equipment, and the like away from the street front where possible. Screen from view those elements which for programmatic reasons cannot be located away from the street front.

At the Early Design Guidance Meeting, the Board was very concerned that services such as transformer, storage, fire pump and similar are not activating to the street front, and requested explana-

tions why these cannot be located on another level, so the ground floor active use can be maximized.

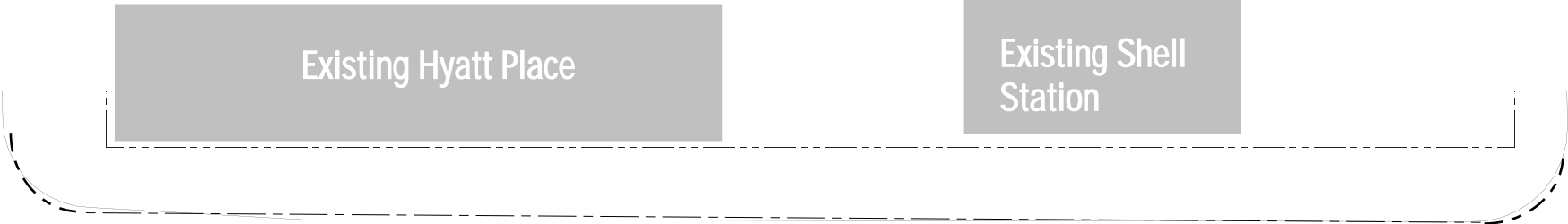
Response:

- The street level plan has been significantly redesigned to maximize street level uses.
- A retail component has been designed into the Wall and 6th ave. corner.
- Provided deeper retail commercial space along Denny by extending the floor over the vehicle ramps.
- The transformer access hatch (which is required) has been relocated so that the commercial space at Denny and Wall is more functional
- The notch in the building for the transformer access hatch would be designed to feature an art piece with architectural lighting – when access to the transformers (generally once in 15 years) is required, the art piece would be temporarily be relocated.

- Commercial
- Mechanical/Service
- Residential Lobby
- Amenities
- Parking

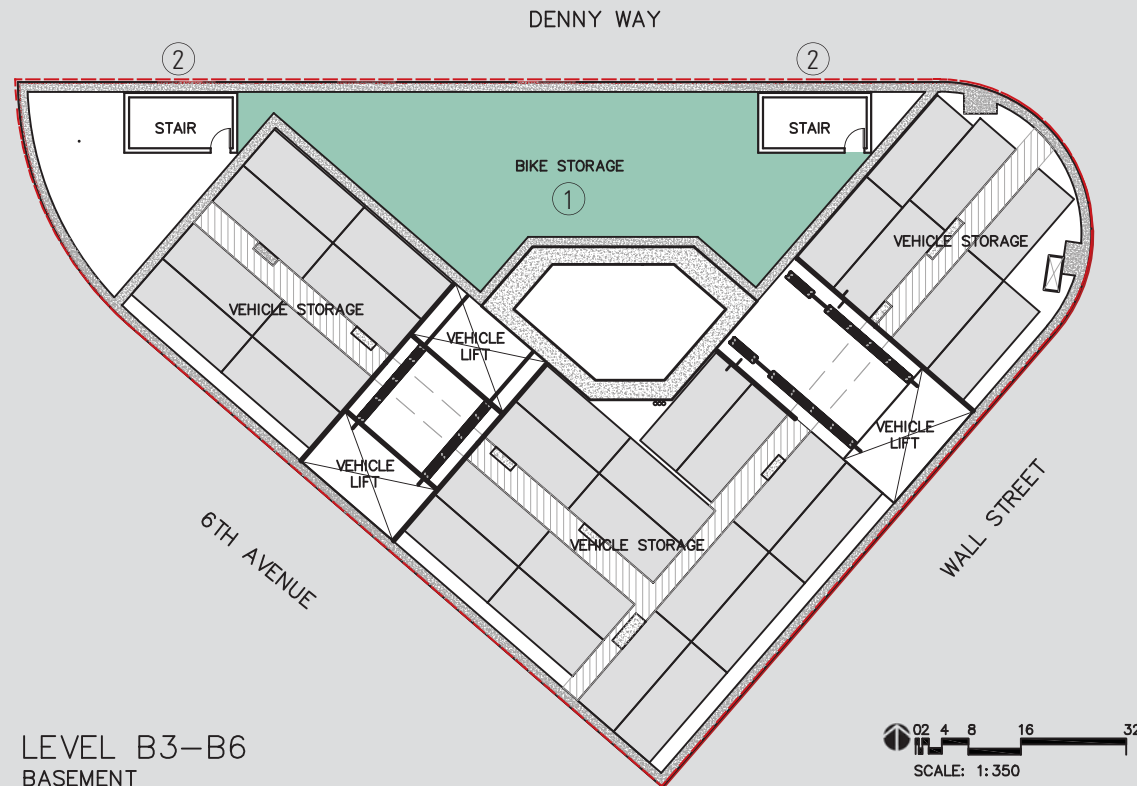
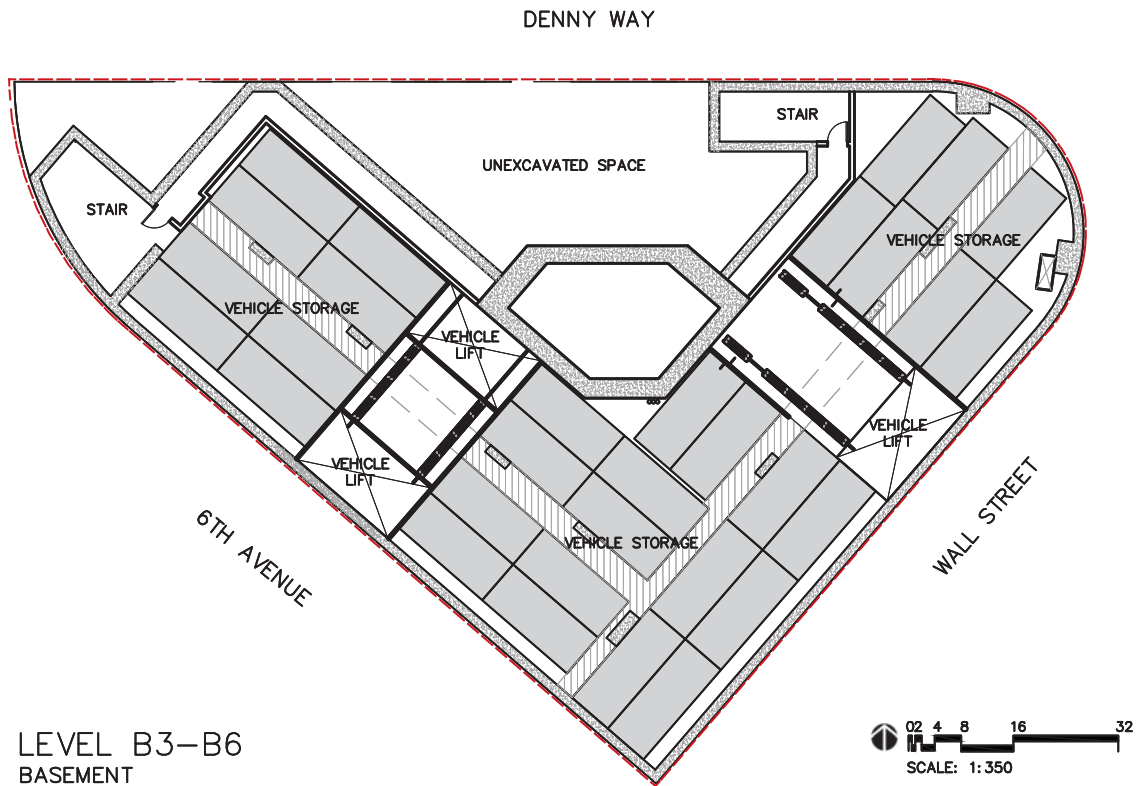


SITE PLAN
REVISED DESIGN



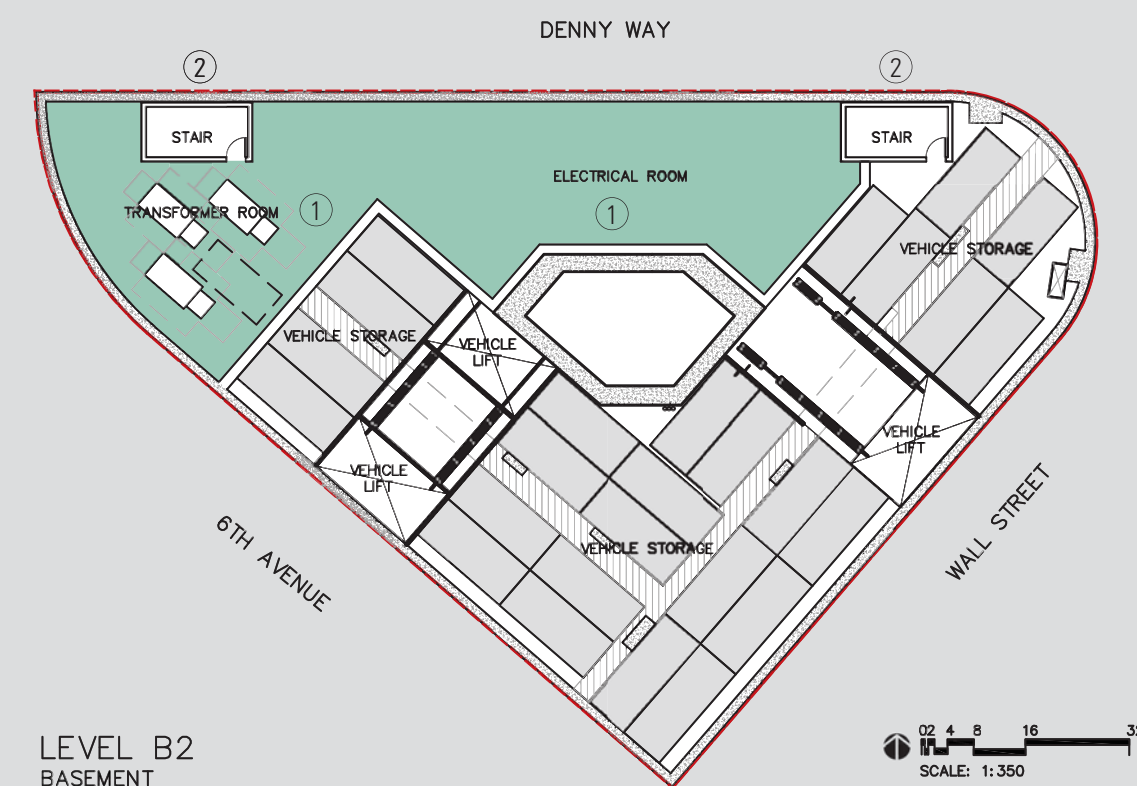
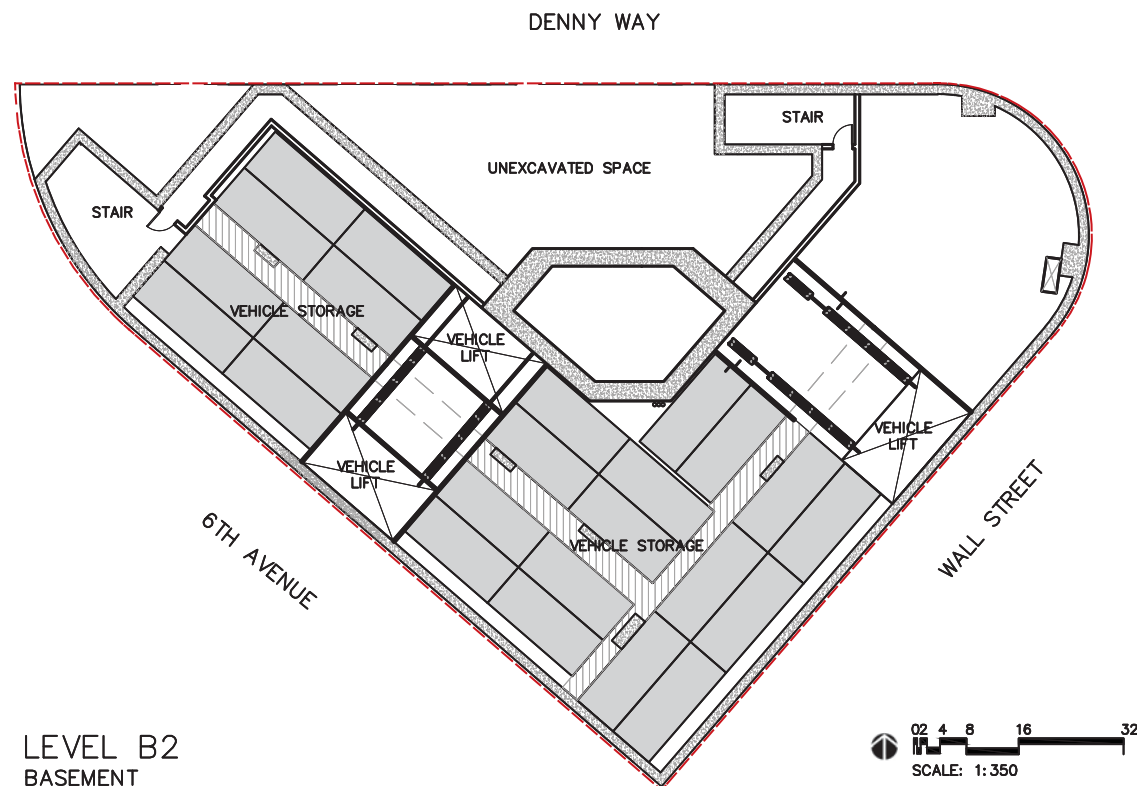
- SITE PLAN CHANGES
- 1. Revised Landscape Plan
 - 2. Revised curb layout per SDOT





LEVEL B3 - B7 CHANGES

1. Added bike storage rooms
2. Relocated stairs



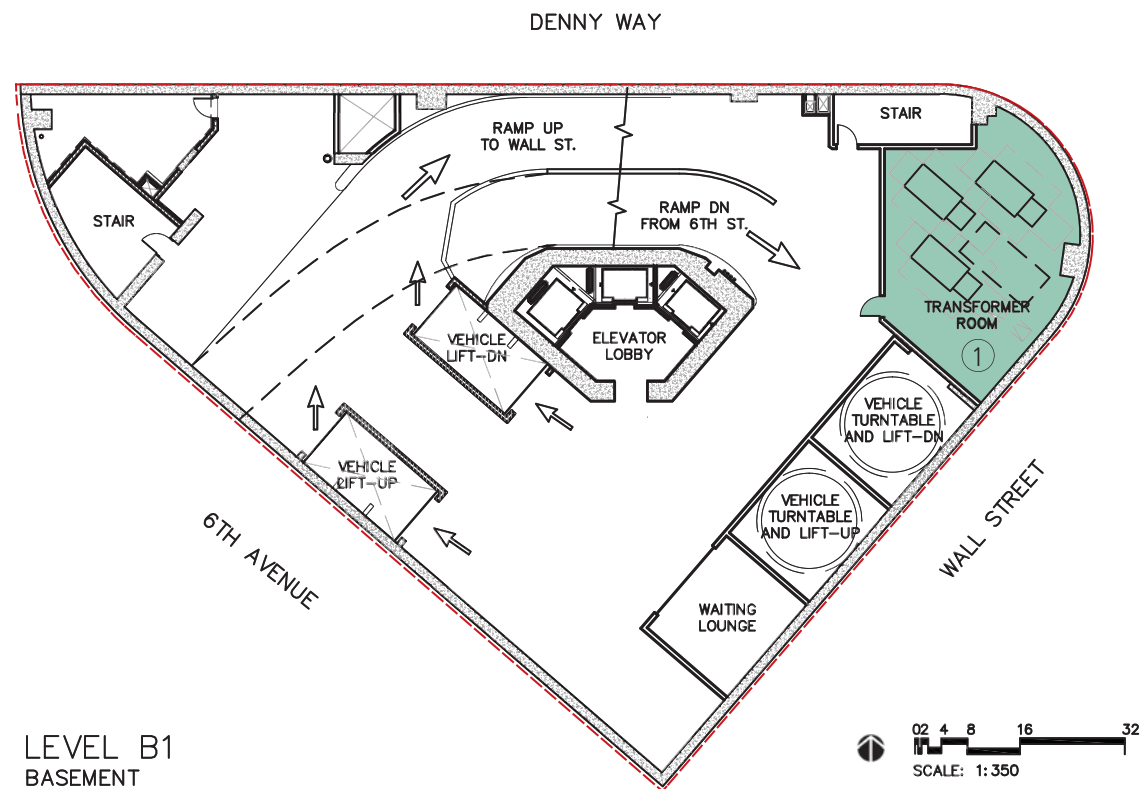
LEVEL B2 CHANGES

1. Relocated Transformer Room and Electrical Room
2. Relocated stairs

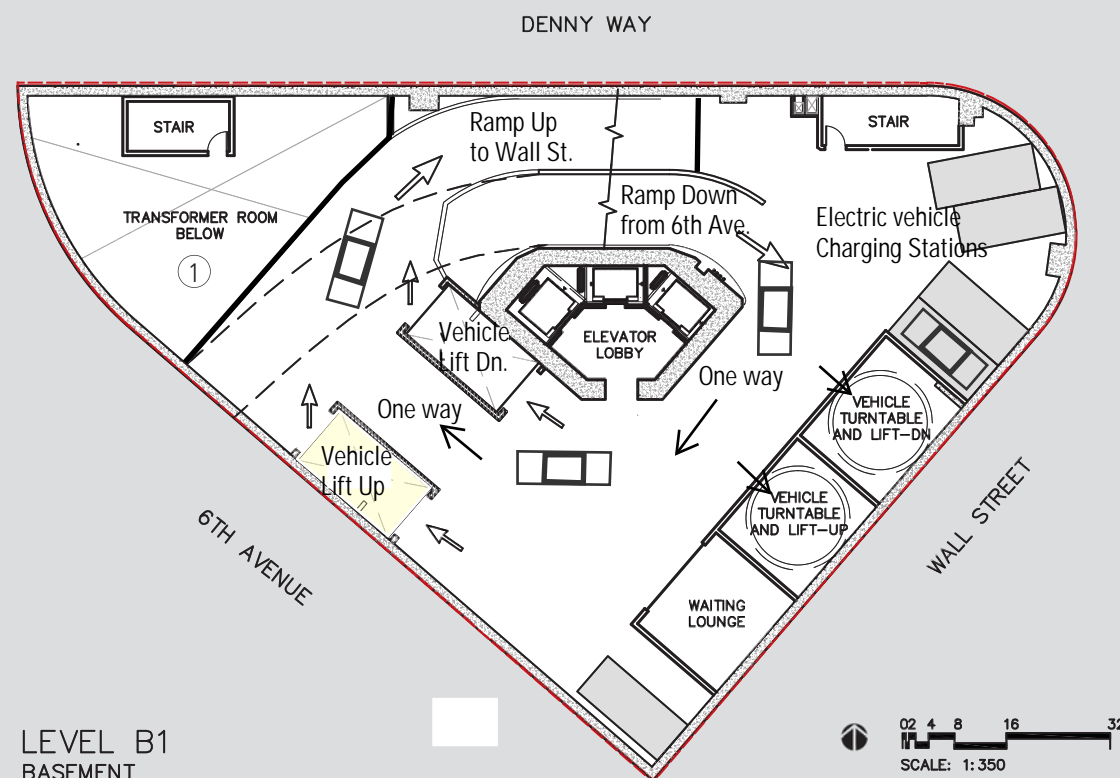
- Commercial
- Mechanical/Service
- Residential Lobby
- Amenities
- Parking

INITIAL DESIGN

REVISED DESIGN



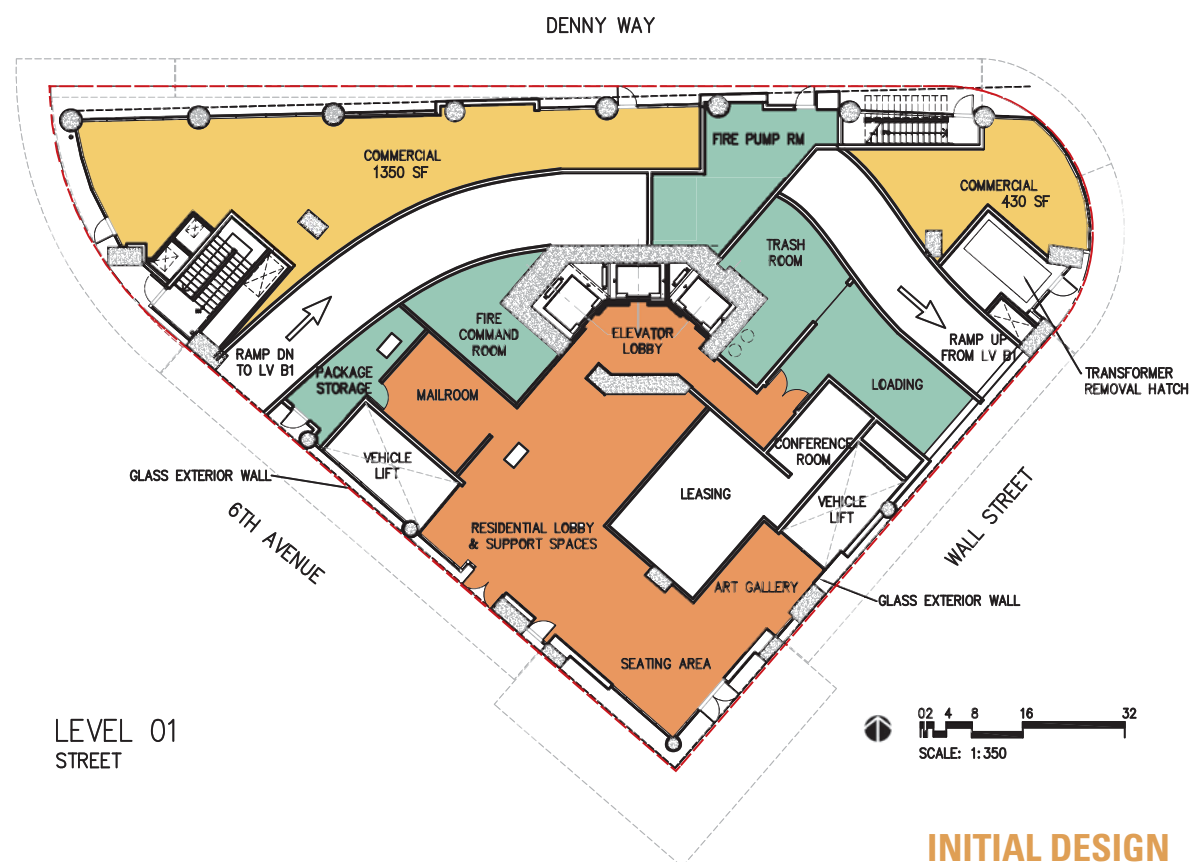
LEVEL B1
BASEMENT



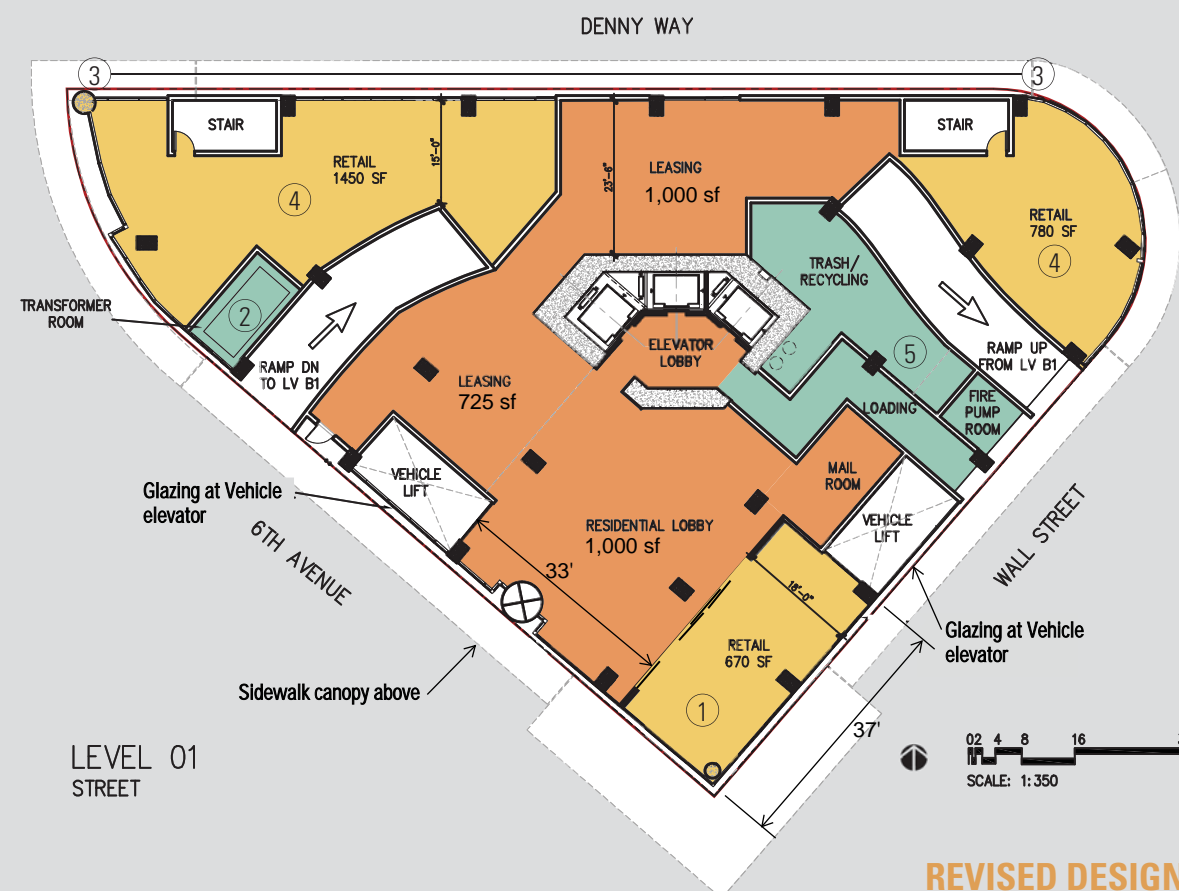
LEVEL B1
BASEMENT

LEVEL B1 CHANGES

1. Relocated Transformer Room



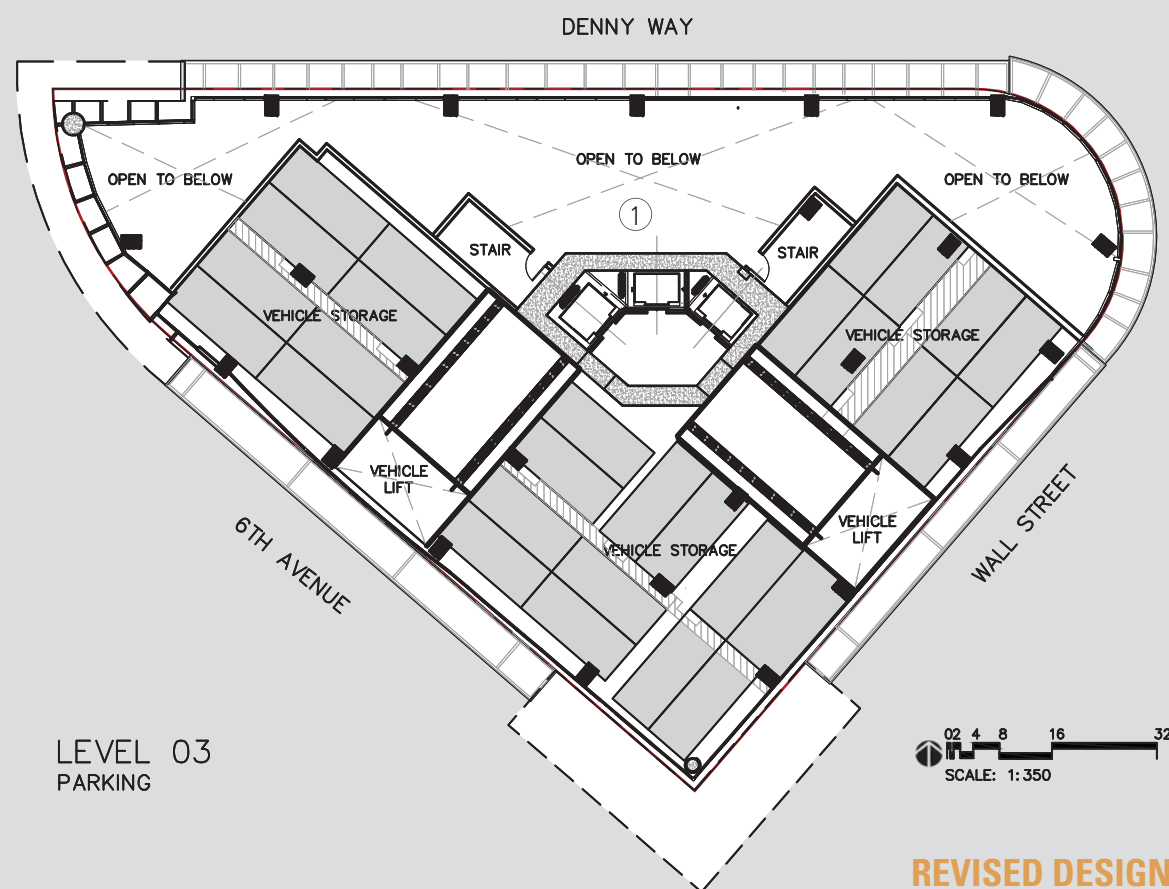
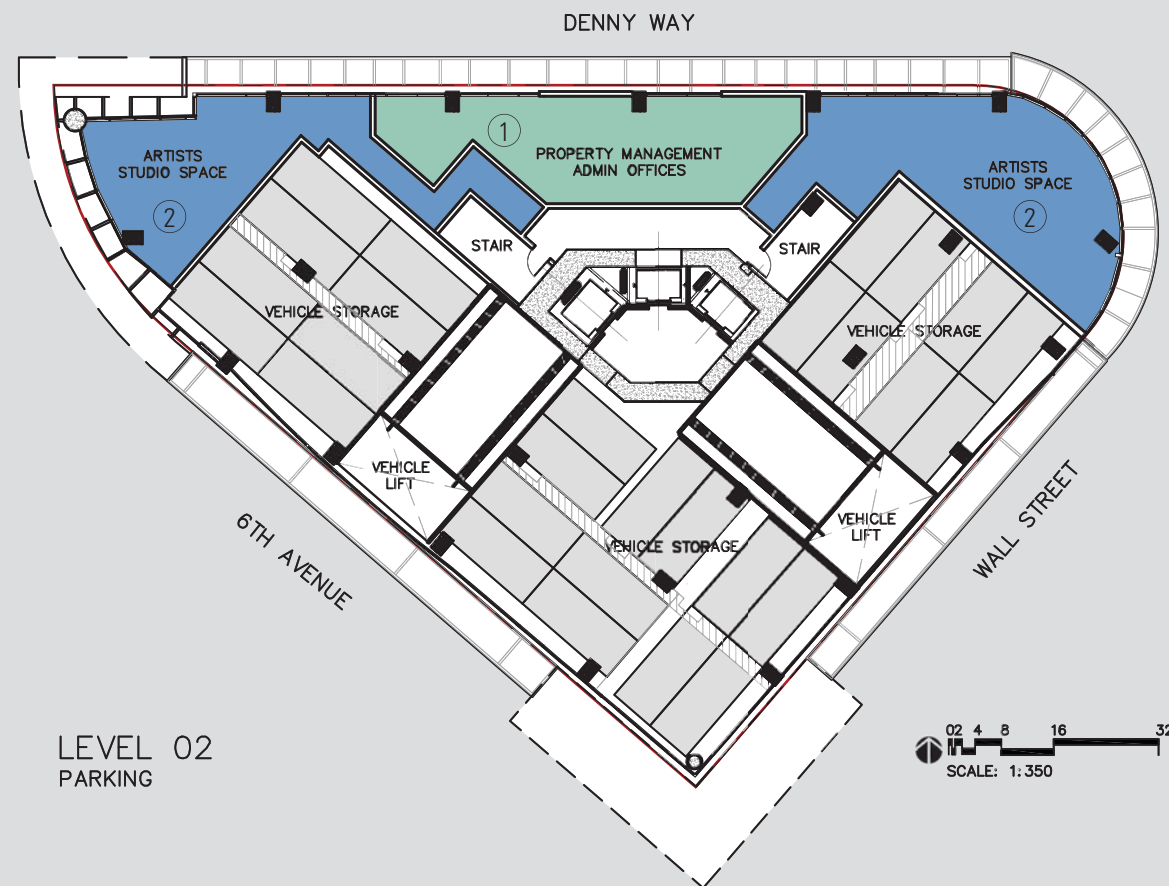
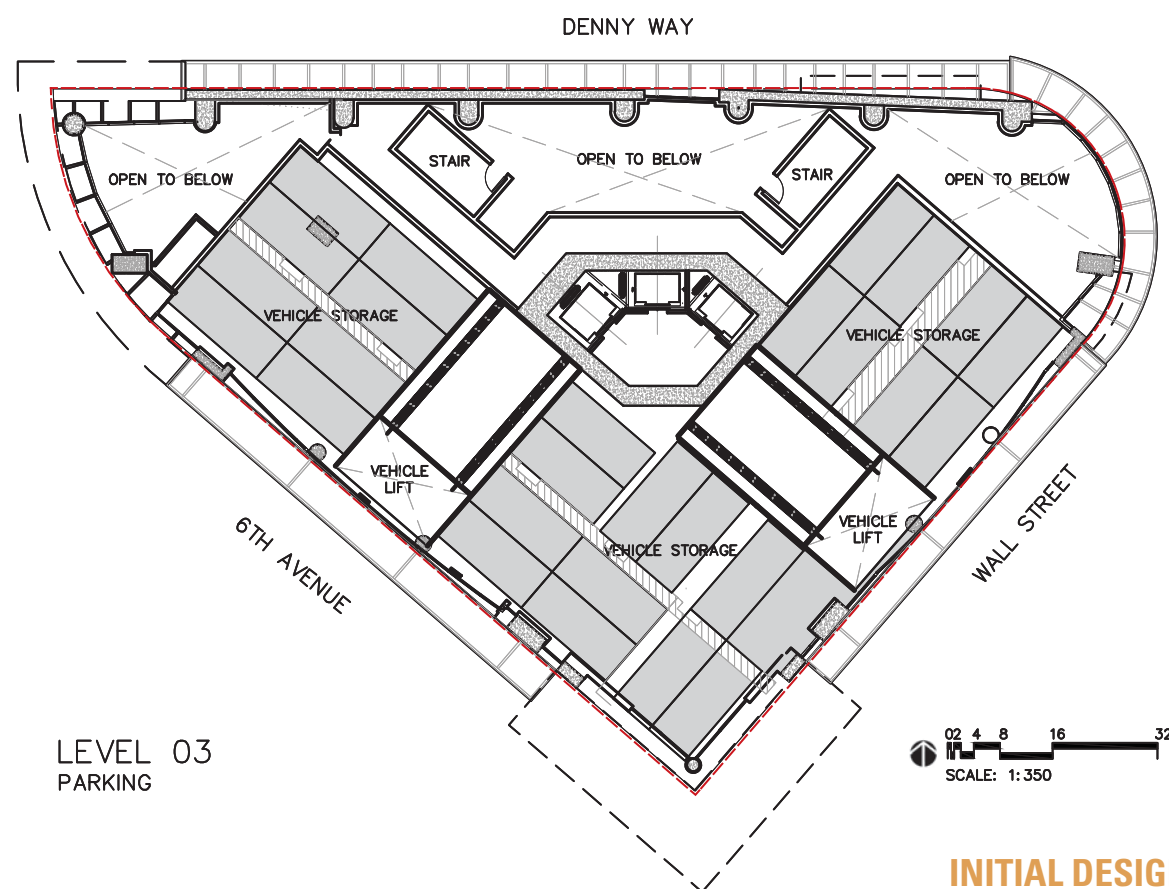
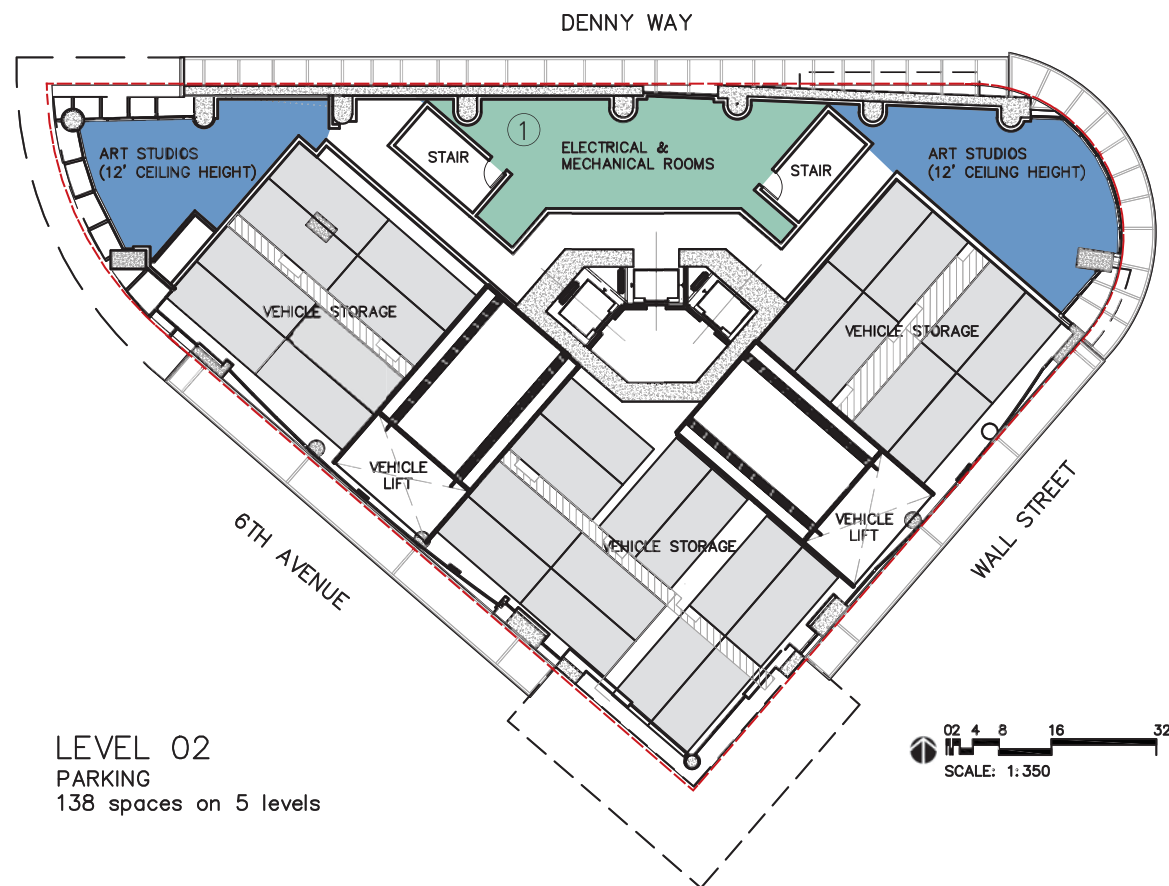
LEVEL 01
STREET



LEVEL 01
STREET

LEVEL 1 CHANGES

1. Added retail at Wall Street and 6th Avenue
2. Relocated transformer removal hatch to 6th Avenue
3. All street level uses on Denny Way are commercial/retail - thet spaces
4. Commercial spaces at corners have been reconfigured
5. Back of house support space has been reconfigured to minimize street frontage



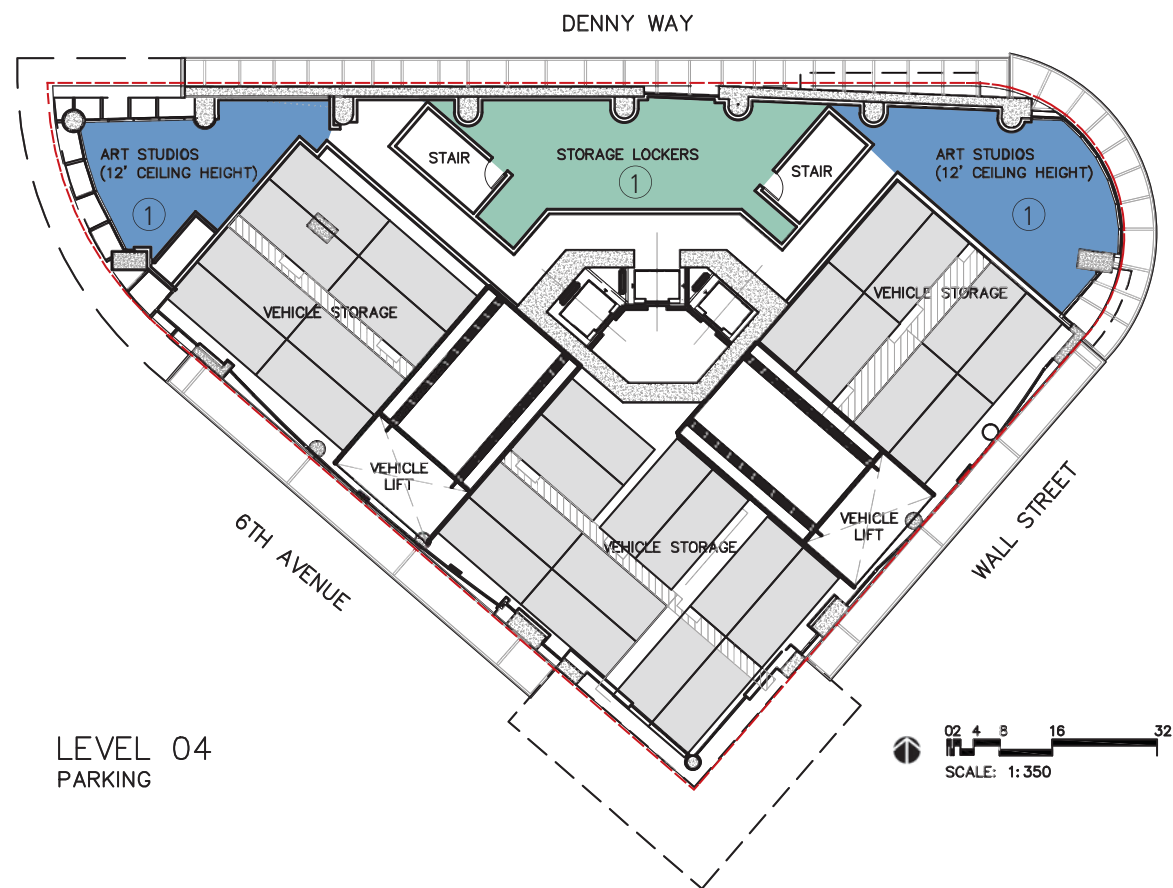
LEVEL 2 CHANGES

1. Electrical Room relocated to basement level - replaced with office space
2. Art studio spaces have been increased in floor area

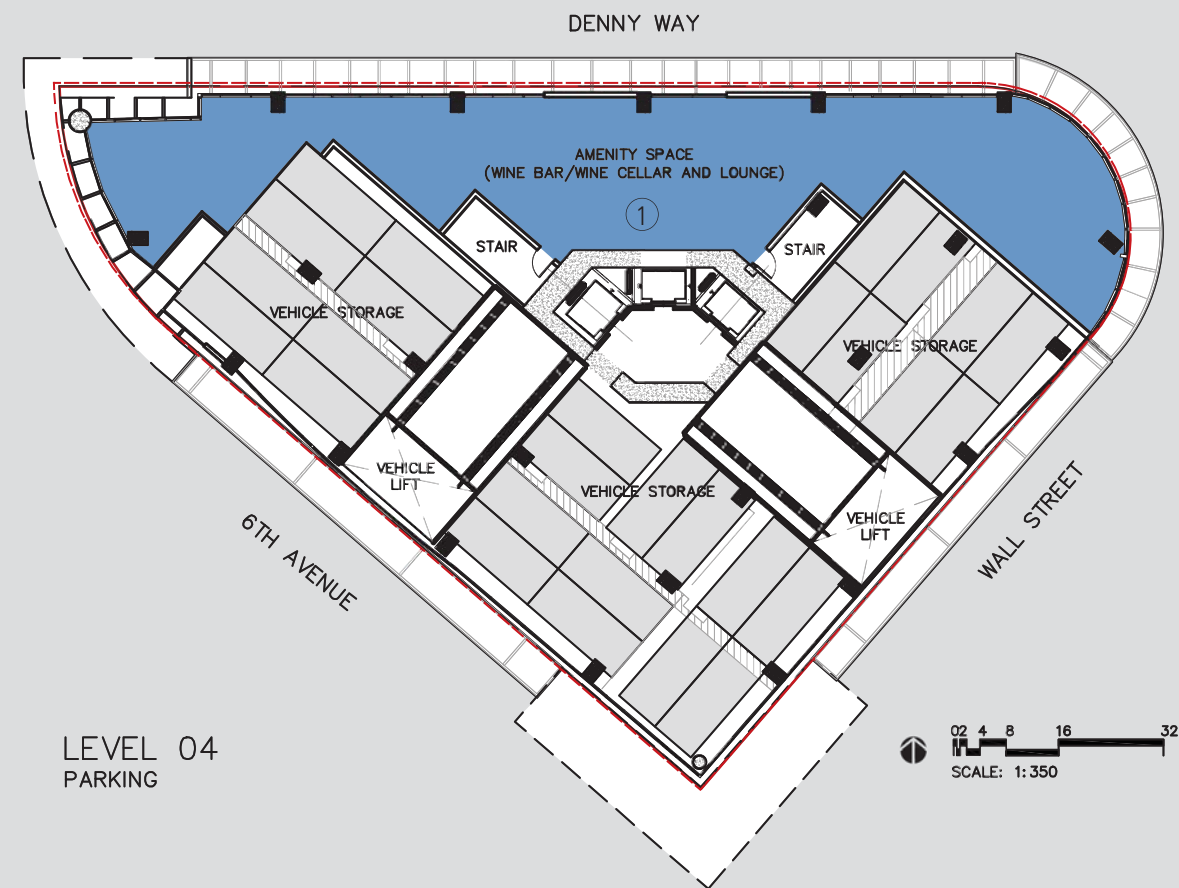
LEVEL 3 CHANGES

1. More double height space created for amenity space

- Commercial
- Mechanical/Service
- Residential Lobby
- Amenities
- Parking



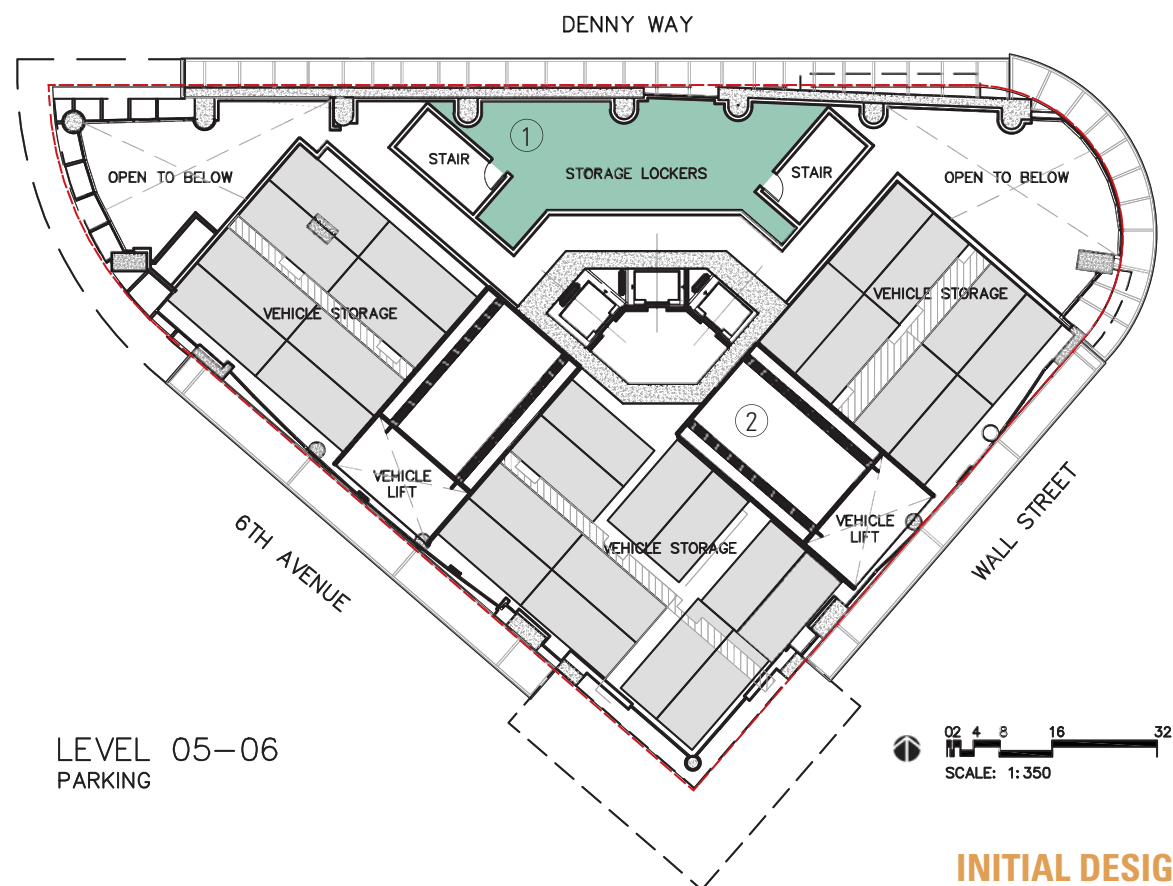
LEVEL 04
PARKING



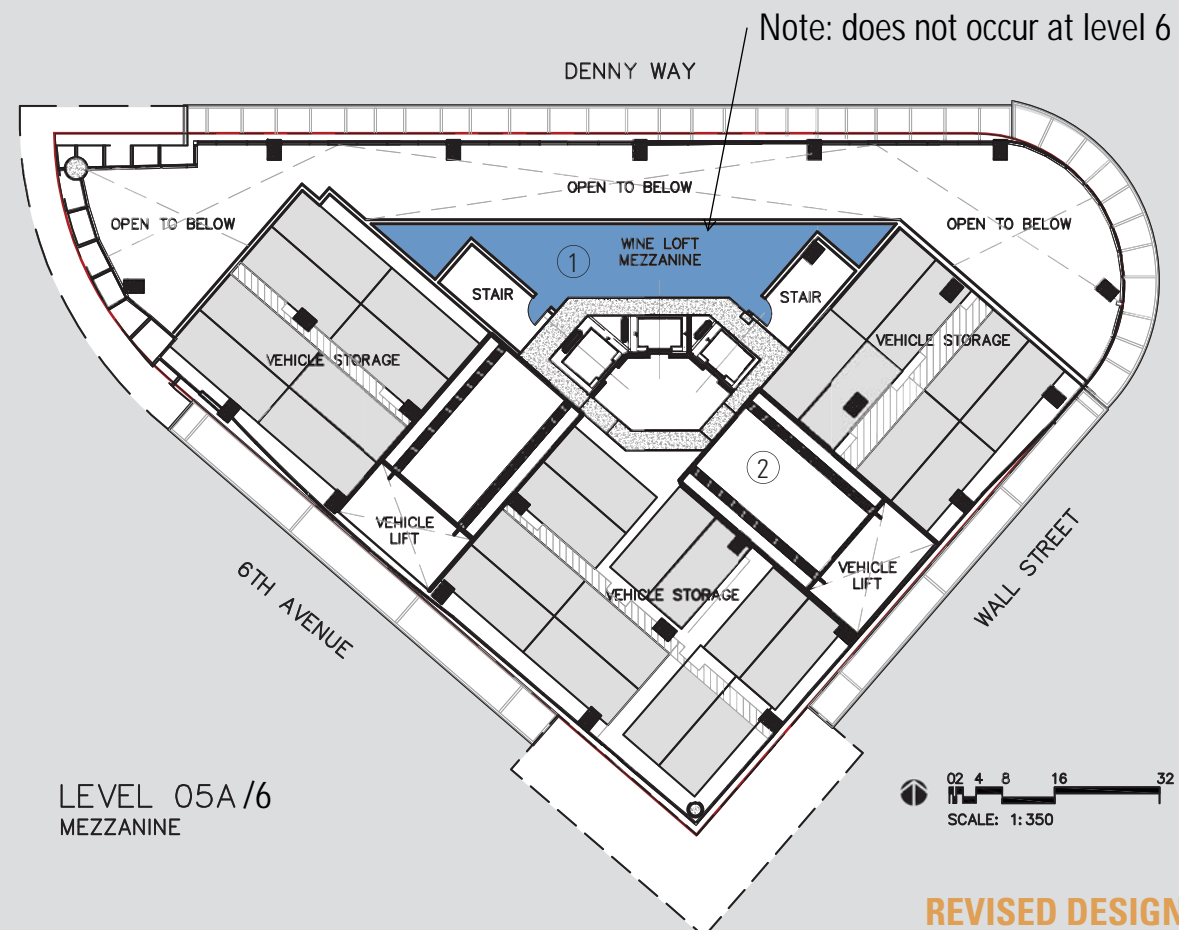
LEVEL 04
PARKING

LEVEL 4 CHANGES

1. Storage area and art studios are converted to a wine bar/cellar and tasting rooms/lounge areas



LEVEL 05-06
PARKING



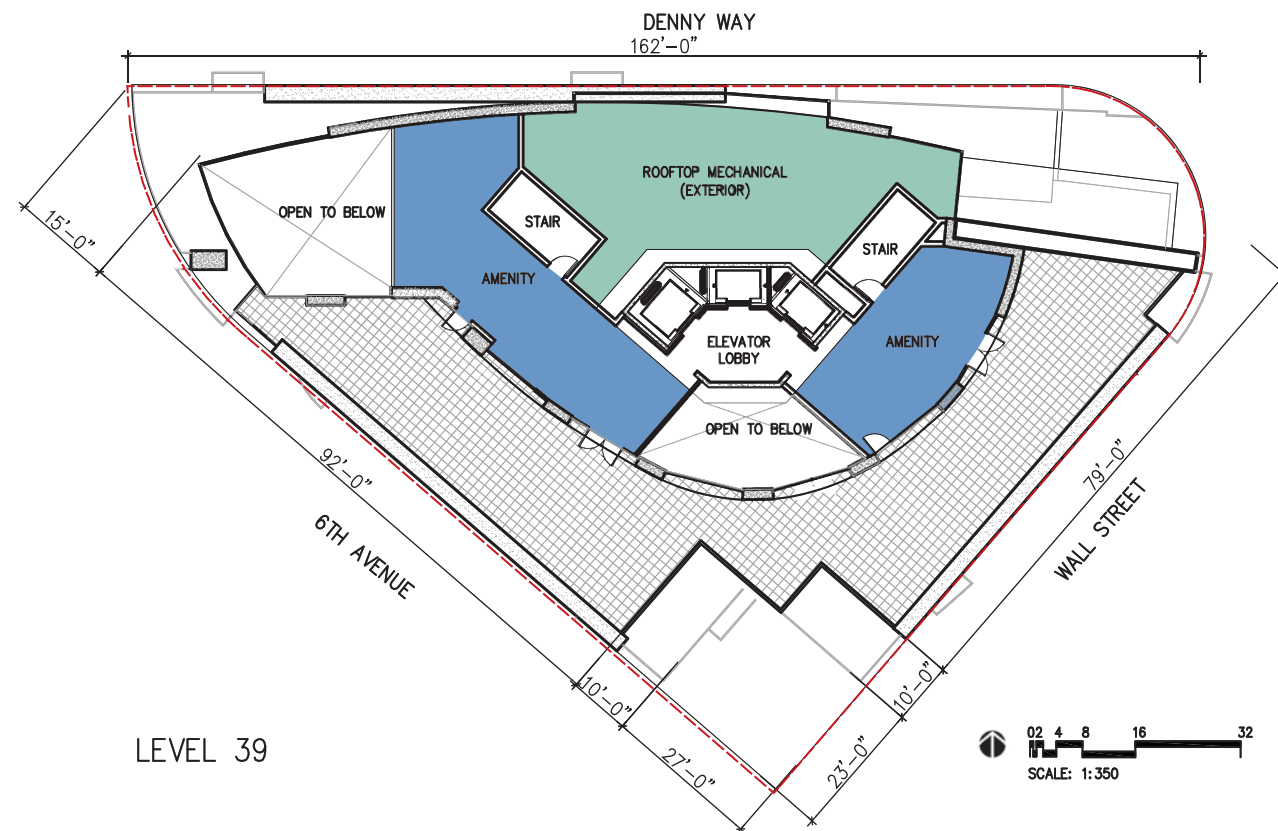
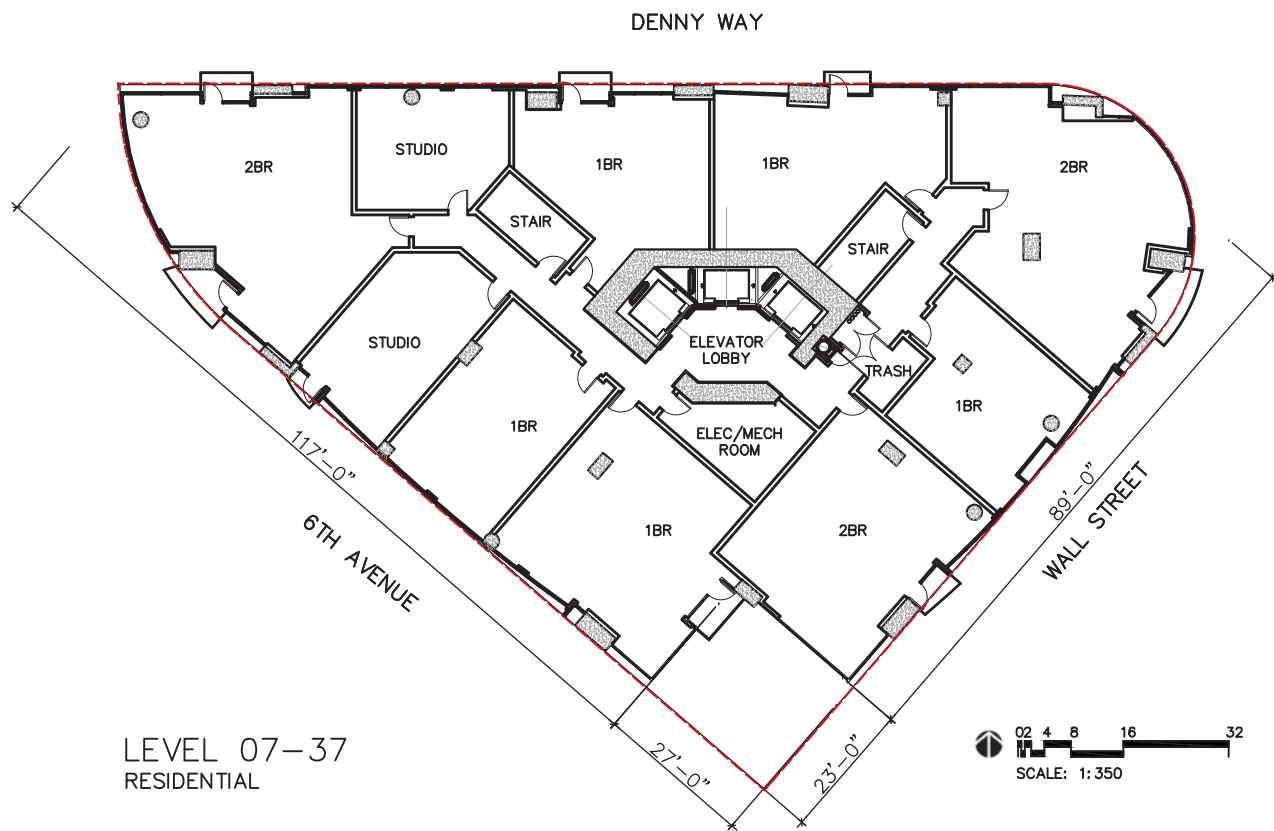
LEVEL 05A/6
MEZZANINE

LEVEL 5A/6 CHANGES

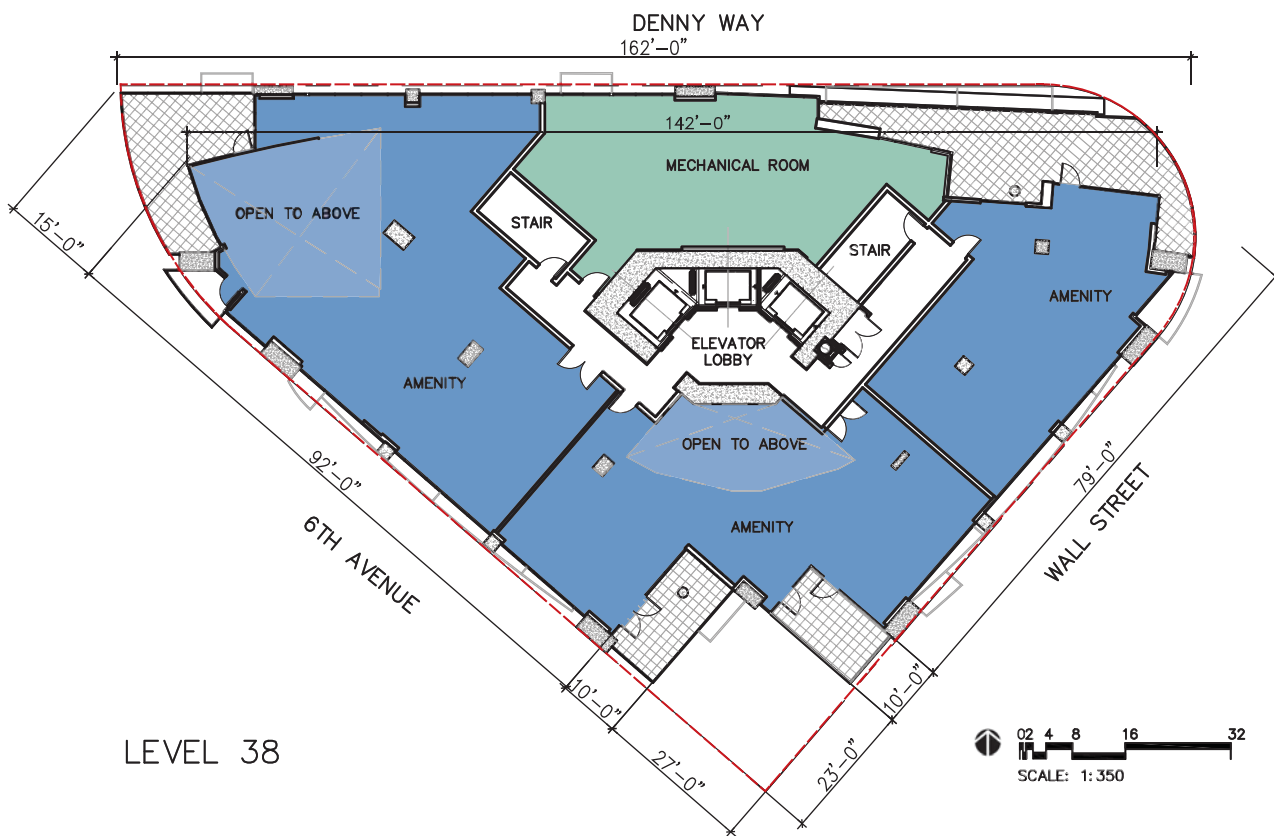
1. Private mezzanine area created in the Wine Loft area for private wine lockers
2. Levels 5 and 6 parking levels remain unchanged

INITIAL DESIGN

REVISED DESIGN



LEVELS 07 - 39
No changes



- Commercial
- Mechanical/Service
- Residential Lobby
- Amenities
- Parking

LANDSCAPE PLAN

INITIAL DESIGN

- 7

ACER RUBRUM 'SCARSEN'

SCARLET SENTINEL MAPLE

MIN. 2" CAL.

9

GINKGO BILOBA

MAIDENHAIR

MIN. 3" CAL.

6*

GLEDISTIA TRIACANTHOS INERMIS

THORNLESS HONEY LOCUST

MIN. 2" CAL.
- R.O.W. PLANTING

* ARCTOSTAPHYLOS UVA-URSI

* CISTUS SUNSET

* MISCANTHUS VARIETIES

* POLYSTICHUM MUNITUM

* EUONYMUS F.

KINNIKINNICK

MAGENTA ROCK ROSE

MAIDEN GRASS

SWORD FERN

CREeping EUONYMUS
- FEATURE PLANTING

* FRAGARIA CHILOENSIS

* MAHONIA REPENS

* ROSEMARINUS OFFICINALIS 'IRENE'

* VINCA MINOR

* LAVANDULA ANGUSTIFOLIA

* LYCHNIS CORONARIA

* MALVA SYLVESTRIS

* LIRIOPE SPICATA

* LONICERA PILEATA

* POTENTILLA FRUTICOSA

BEACH STRAWBERRY

CREeping MAHONIA

ROSEMARY

PERIWINKLE

LAVENDER

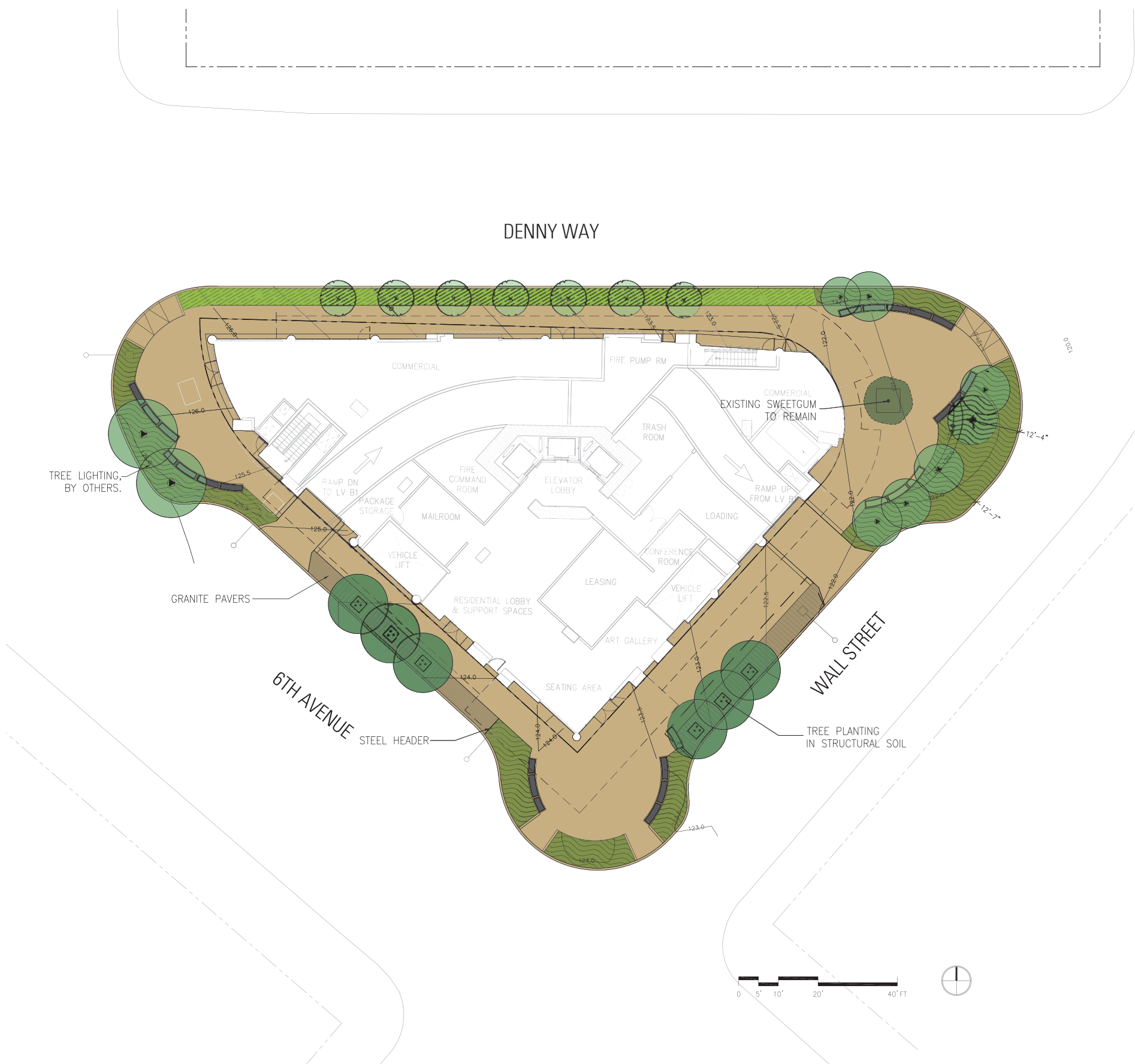
LYCHNIS

MALLOW

LILYTURF

PRIVET HONEYSUCKLE

BUSH CINQUEFOIL



LANDSCAPE PLAN

REVISED DESIGN

PLANTING SCHEDULE

SYMBOL	BOTANICAL NAME / COMMON NAME	QTY	SIZE
DENNY WAY PLANT LIST			
	ACER RUBRUM 'BOWHALL' / BOWHALL MAPLE	2"	2" CALIPER, B#B
	PRUNUS LAUROCERASUS 'OTTO LYKEN' / OTTO LYKEN LAUREL	3 GAL. CONT., MIN.	18"-24" HGT # SPD
	STIPA TENUISSIMA / MEXICAN FEATHER GRASS	1 GAL. CONT., MIN.	
	PENNESETUM A. 'HAMLYN' / FOUNTAIN GRASS	1 GAL. CONT., MIN.	
SIXTH AVENUE and WALL STREET PLANT LIST			
	CERCIS CANADENSIS / EASTERN REDBUD	10'	10' HT
	LAGERSTROEMIA 'TUSCARORA' / HYBRID CRAPE MYRTLE	10'	10' HT
	MUSCARI ARMENICUM / GRAPE HYACINTHS	BULBS	
	NEPETA NERVOSA 'FELIX' / CATMINT	1 GAL. CONT.	
	AGAPANTHUS / LILLY OF THE NILE	2 GAL. CONT.	
	EUONYMUS 'EMERALD 'N' GOLD'	2 GAL. CONT.	
	LIRIOPE MUSCARI / LILLY OF THE NILE	1 GAL. CONT.	
	CLETHRA ALNIFOLIA 'HUMMINGBIRD' / SWEET PEPPERBUSH	5 GAL. CONT.	
	HAKONECHLOA MACRA 'AUREA' / GOLDEN JAPANESE FOREST GRASS	1 GAL. CONT.	



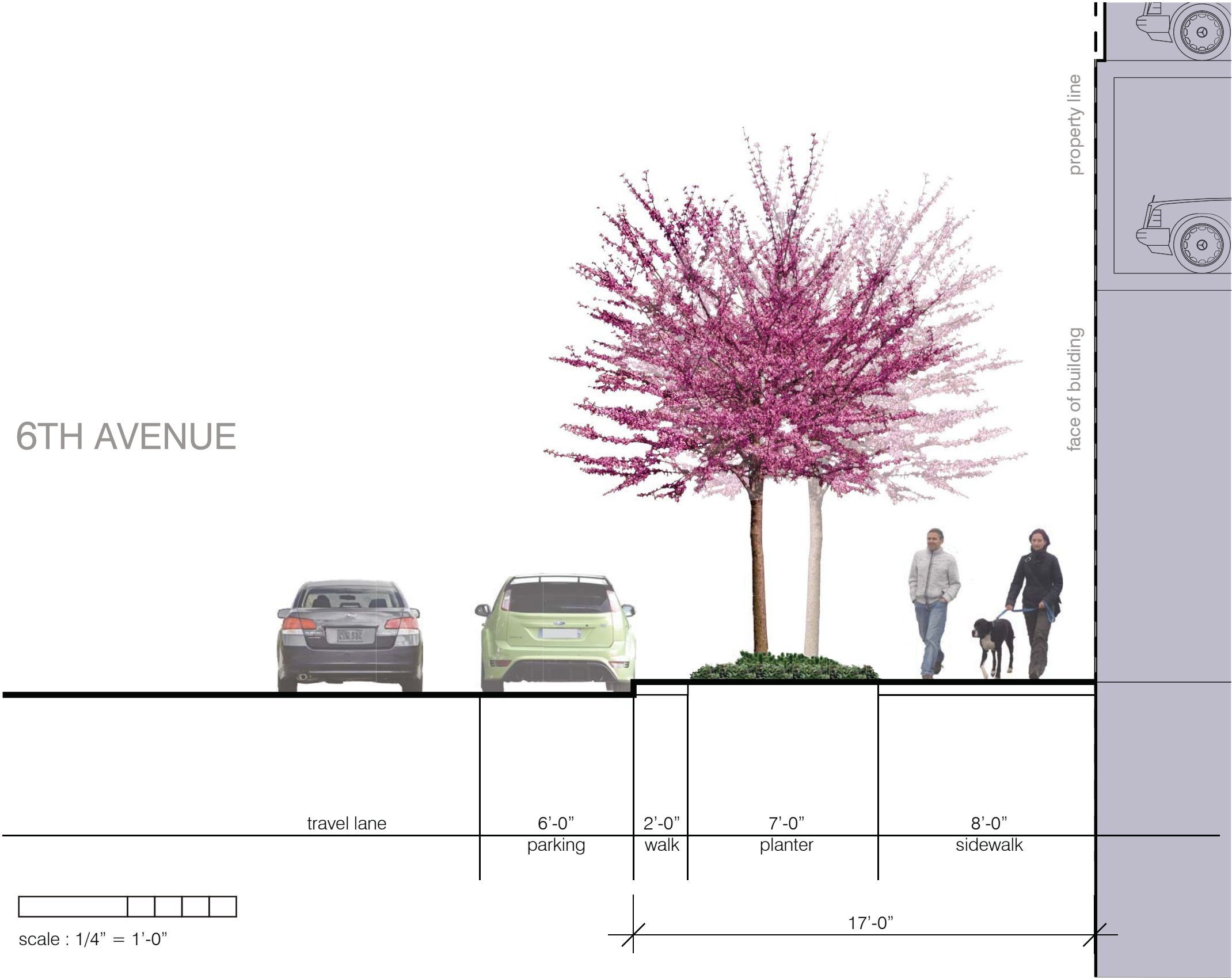
LANDSCAPE CHANGES

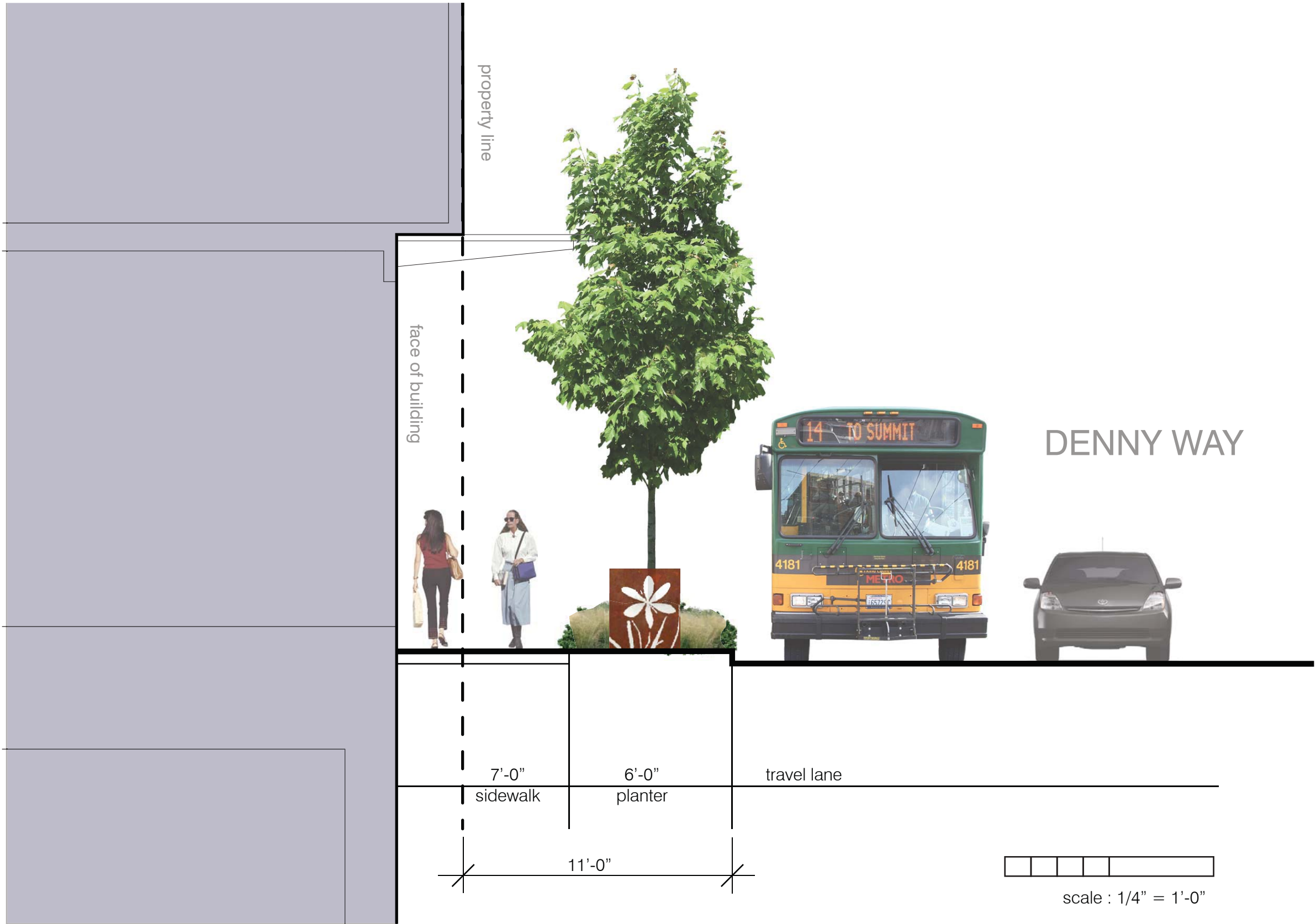
1. Paving accents at corners
2. Paving accent at residential lobby

SIDEWALK SECTIONS



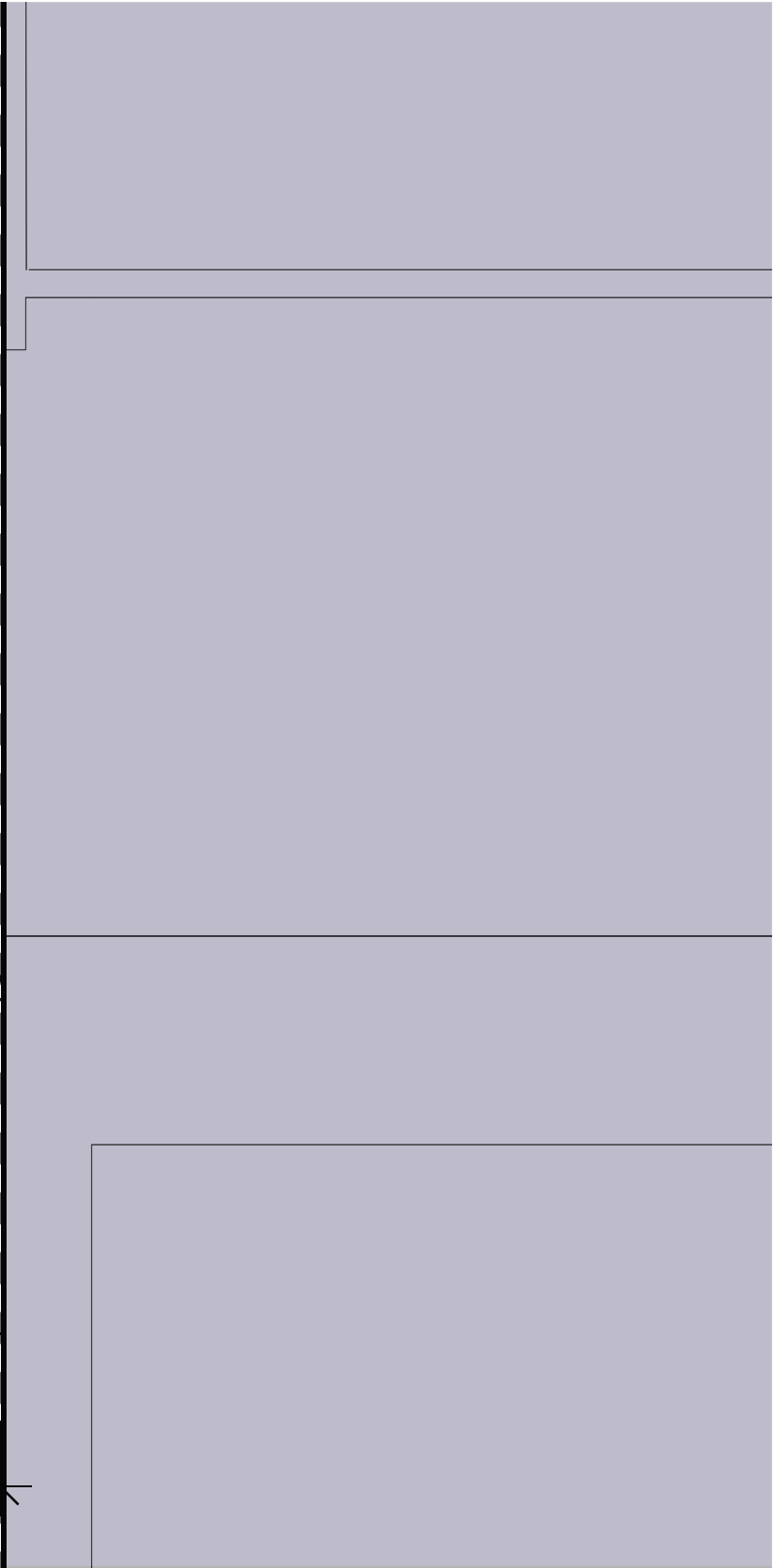
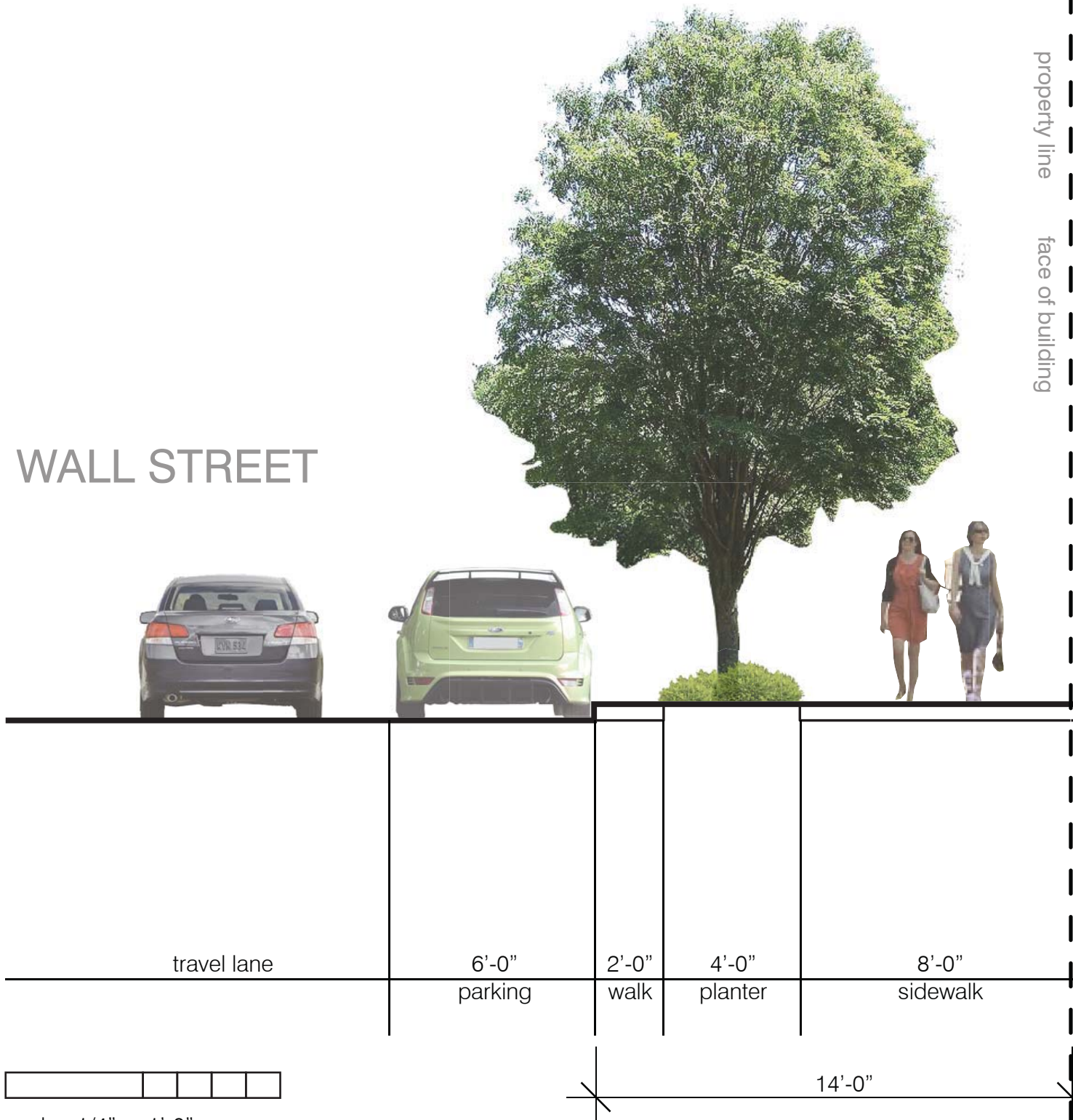
6TH AVENUE



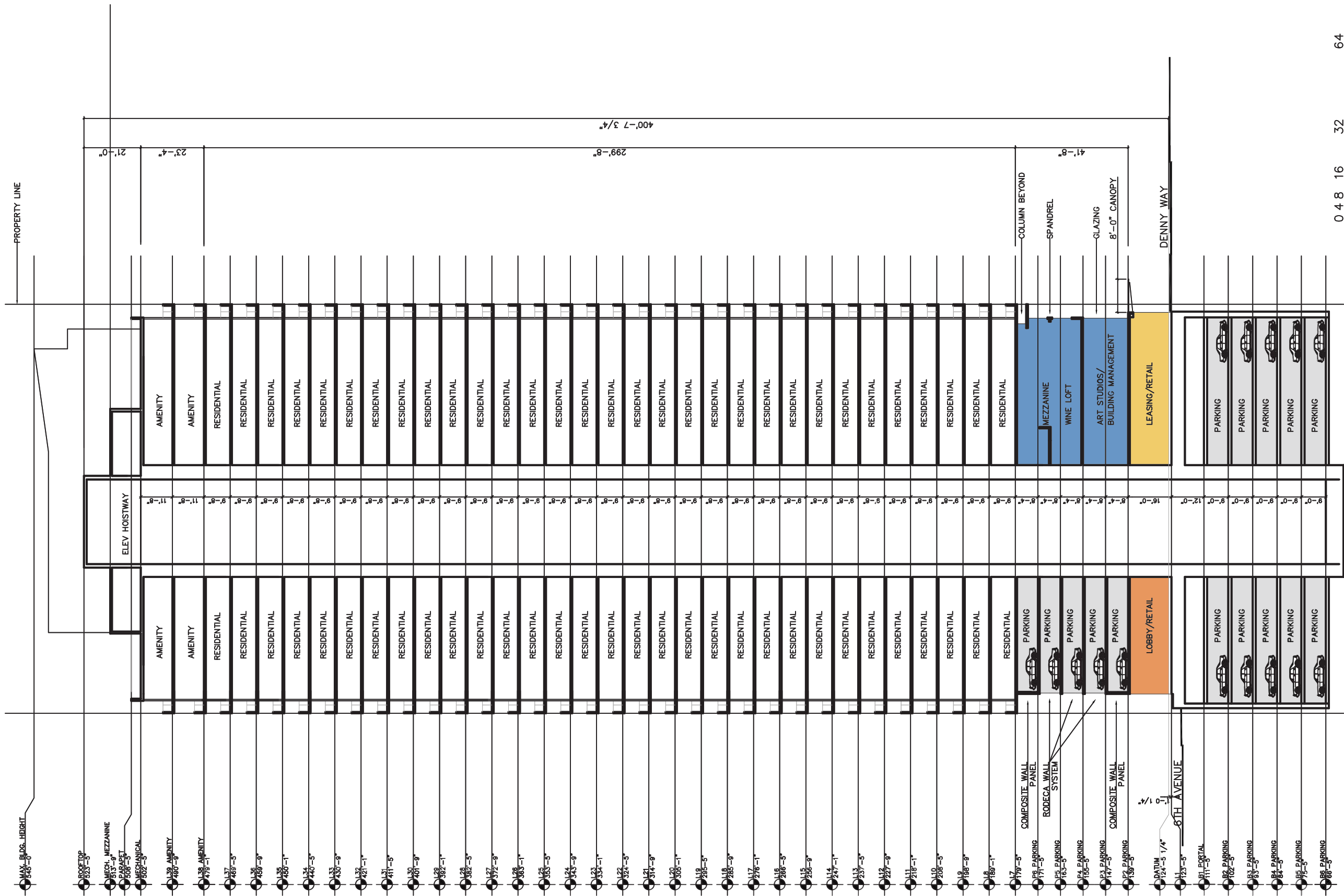




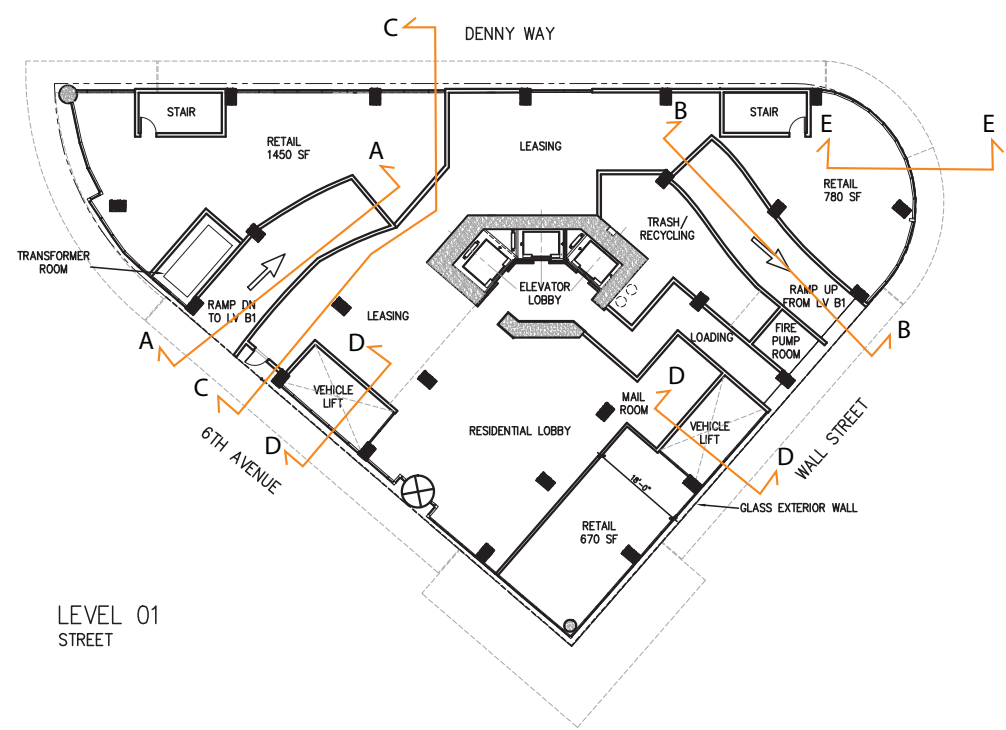
WALL STREET



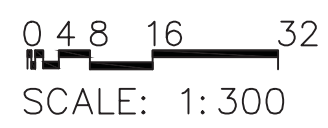
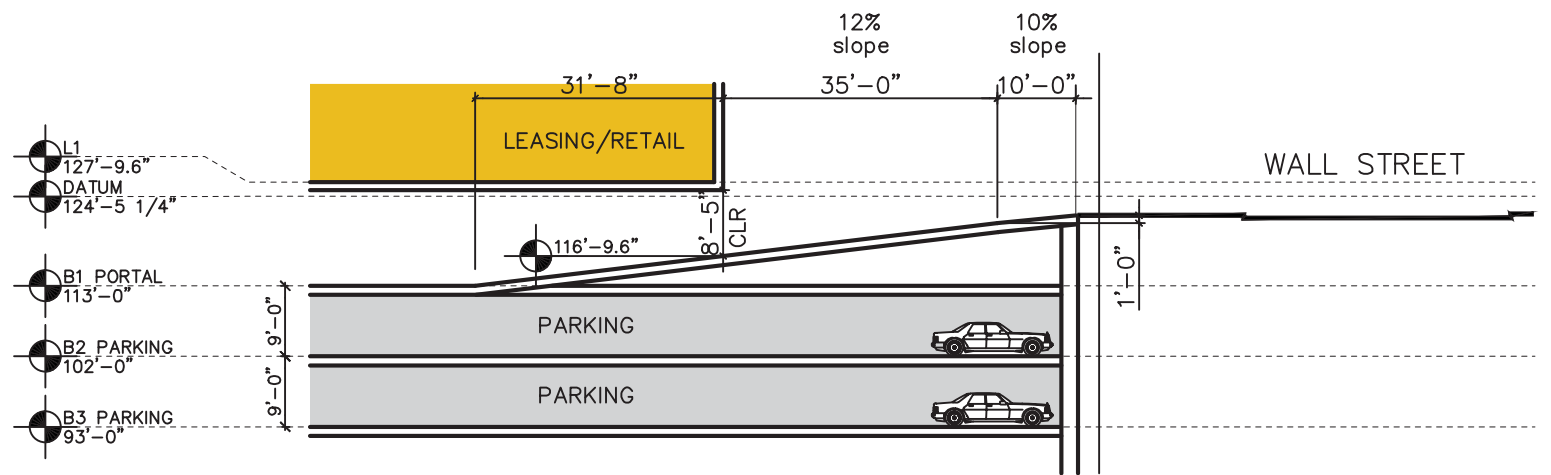
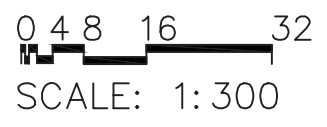
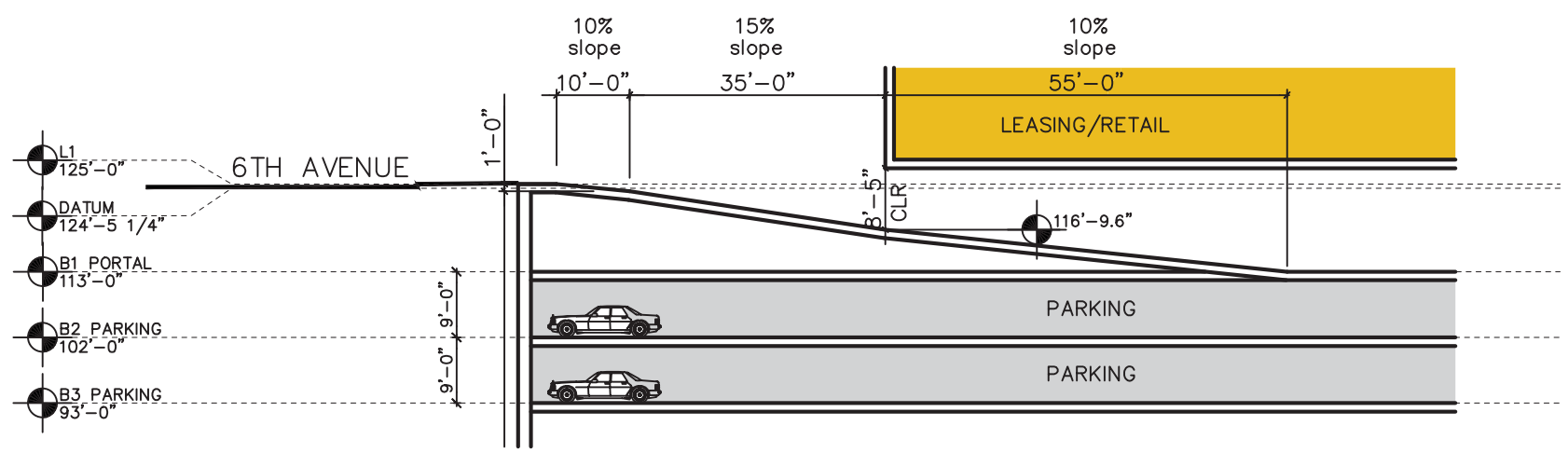
BUILDING SECTION

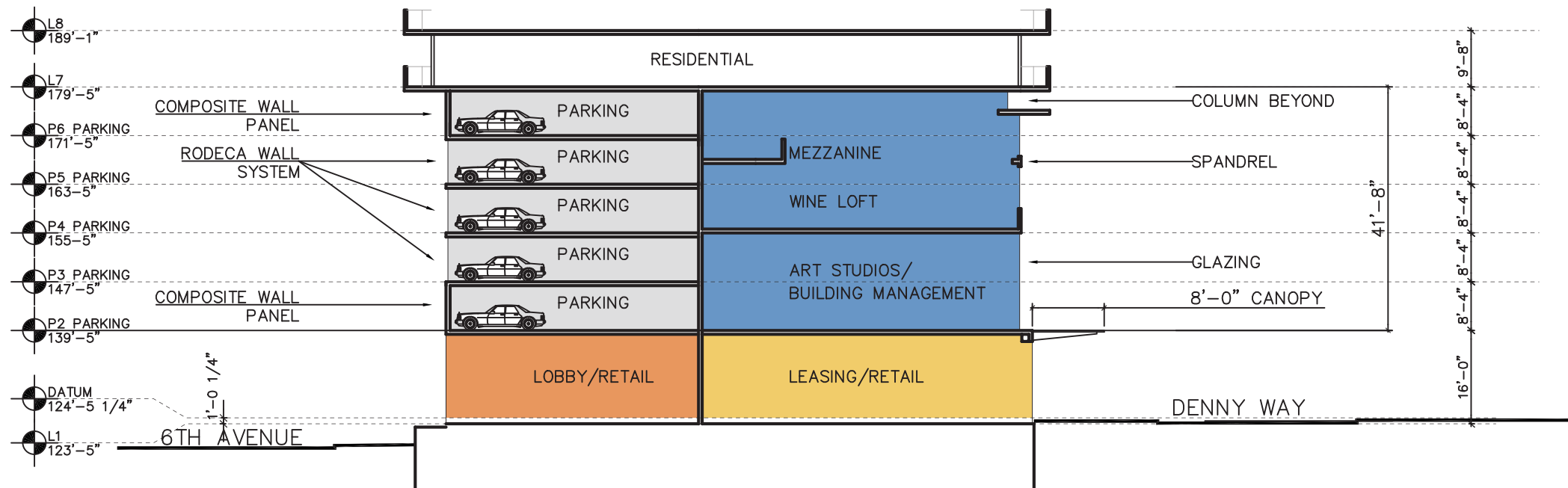


PARTIAL BUILDING SECTIONS



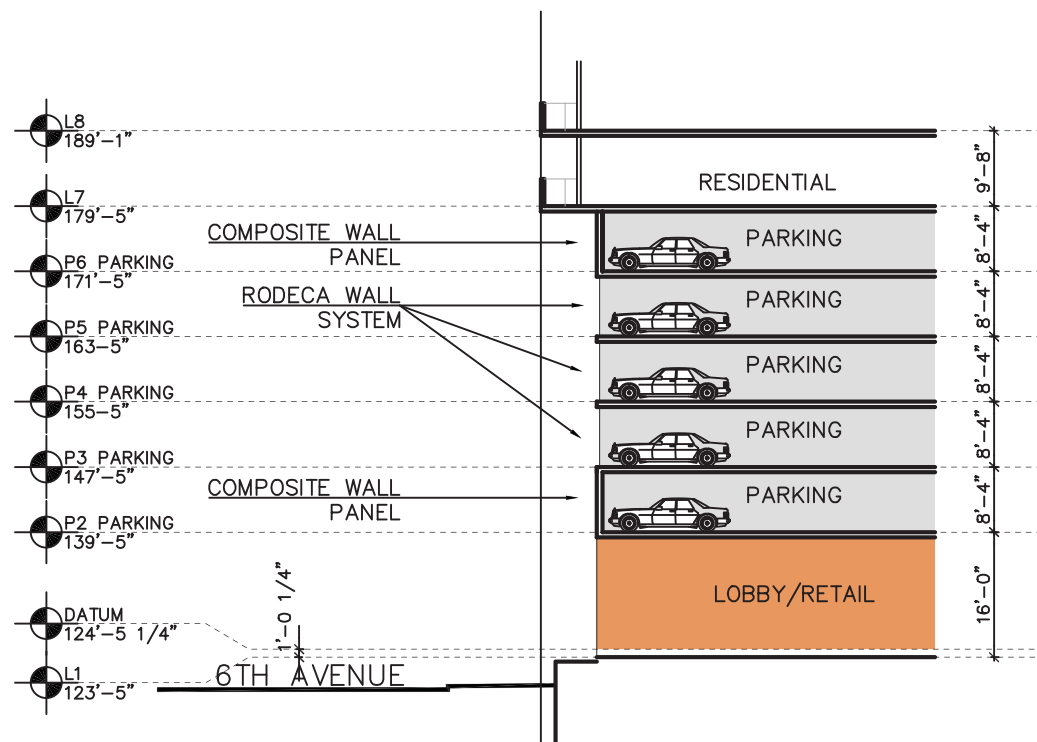
- Commercial
- Mechanical/Service
- Residential Lobby
- Amenities
- Parking





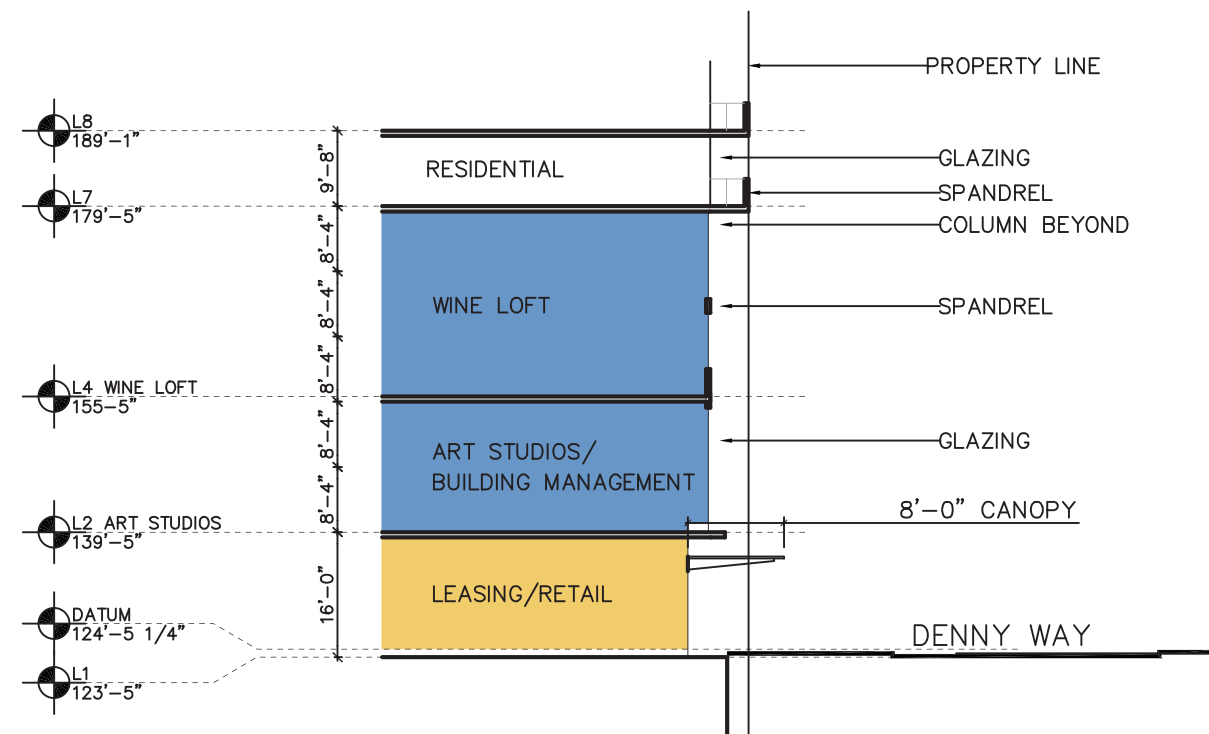
SECTION C

0 4 8 16 32
SCALE: 1:300



SECTION D
THROUGH CAR LIFTS

0 4 8 16 32
SCALE: 1:300



SECTION E
THROUGH ART STUDIOS (DENNY WAY)

0 4 8 16 32
SCALE: 1:300



BUILDING VIEWS



SOUTH ON 6TH AVENUE



40 | 600 WALL STREET
SEATTLE, WASHINGTON



6TH AVENUE AND WALL STREET



DENNY WAY AND 6TH AVENUE



DENNY WAY AND WALL STREET



ALONG DENNY WAY



STREET LEVEL CONCEPTS

WALL STREET AND DENNY WAY



INITIAL DESIGN



KEY REVISIONS

- 1. Podium base
- 2. Landscape

REVISED DESIGN

6TH AVENUE AND WALL STREET



INITIAL DESIGN



KEY REVISIONS

1. Podium base
2. Landscape
3. Retail on Wall Street

REVISED DESIGN

DENNY WAY AND 6TH AVENUE



INITIAL DESIGN



- KEY REVISIONS
- 1. Podium base
 - 2. Landscape paving

REVISED DESIGN

BUILDING CLOSE-UPS

6TH AVENUE AND WALL STREET NIGHT VIEW



CORNER OF WALL STREET AND DENNY WAY



DENNY WAY STREET VIEW



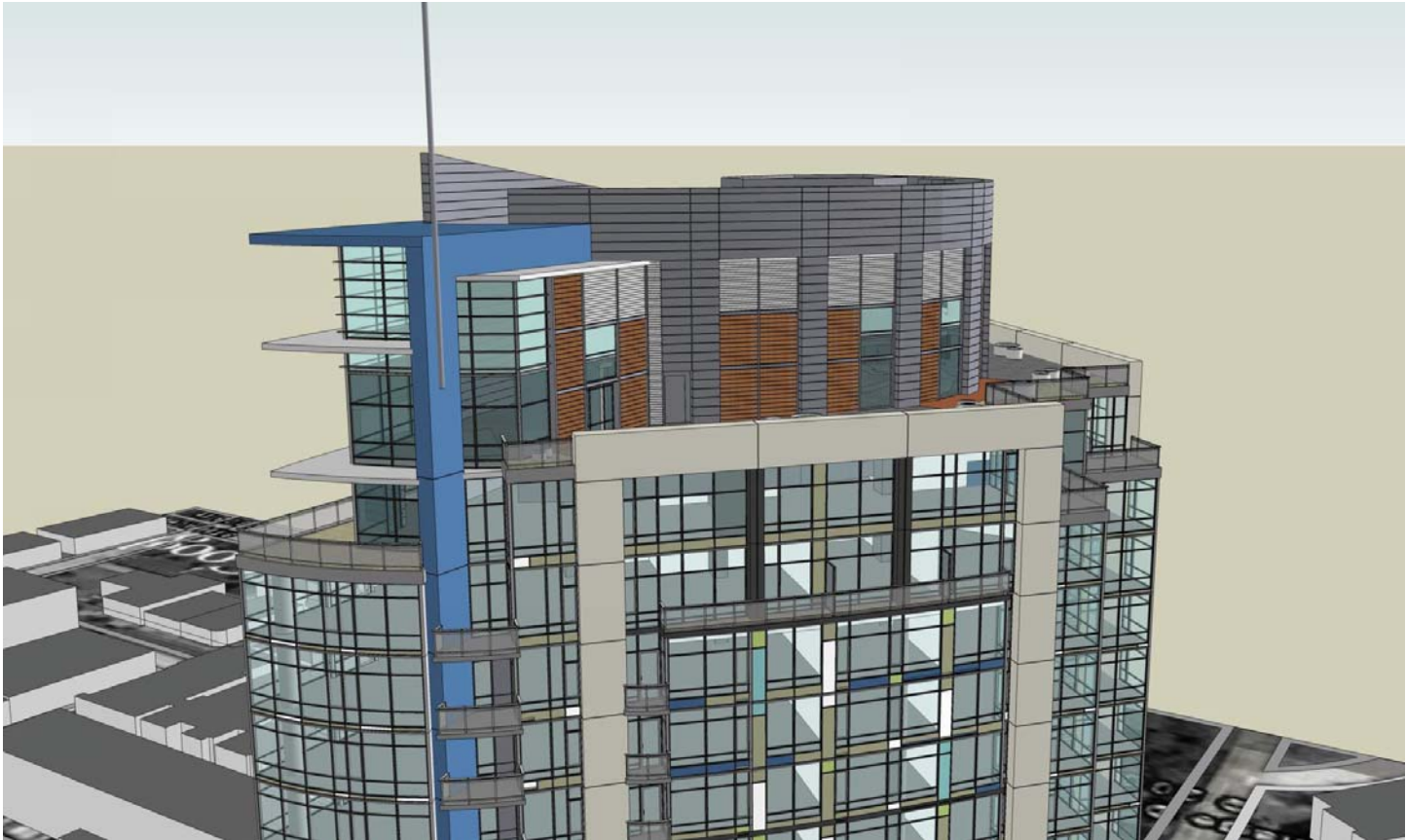
DENNY WAY AND 6TH AVENUE



TOP VIEW - DENNY WAY



TOP VIEW - 6TH AVENUE



TOP VIEW - 6TH AVENUE AND WALL STREET

PODIUM EXTERIOR FINISH MATERIALS



Swisspearl Composite Panel



Rodecda - Polycarbonate Panel





DEPARTURE
REQUESTS

DEPARTURE REQUESTS

DEPARTURE 1: FAÇADE WIDTH

DIAGRAM A
Property Extents

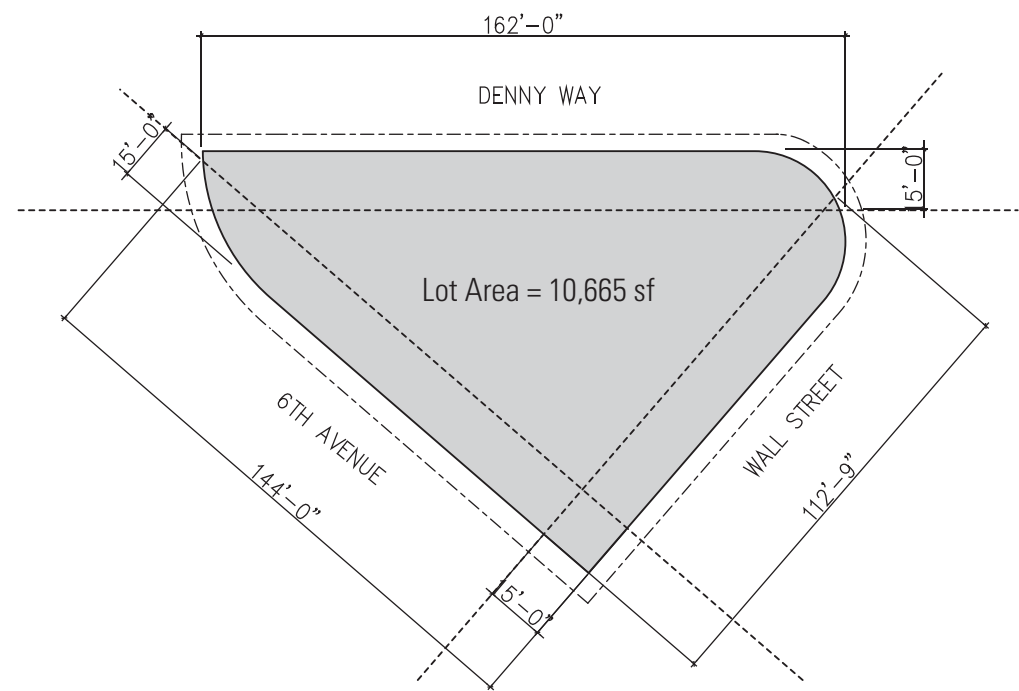


DIAGRAM B
Prescribed Façade Width

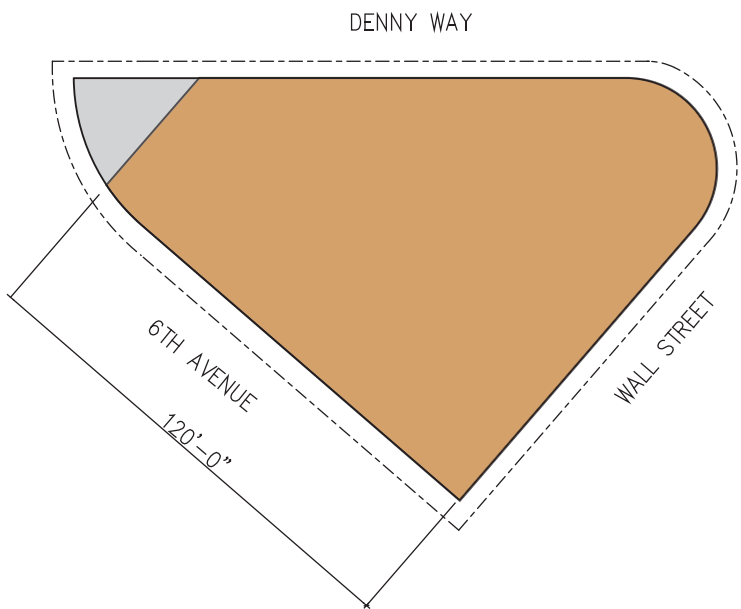
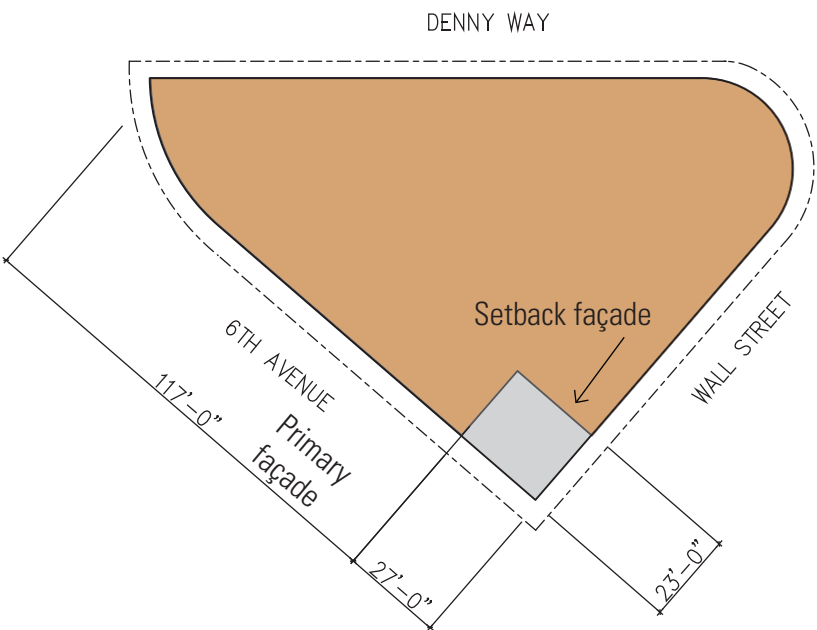
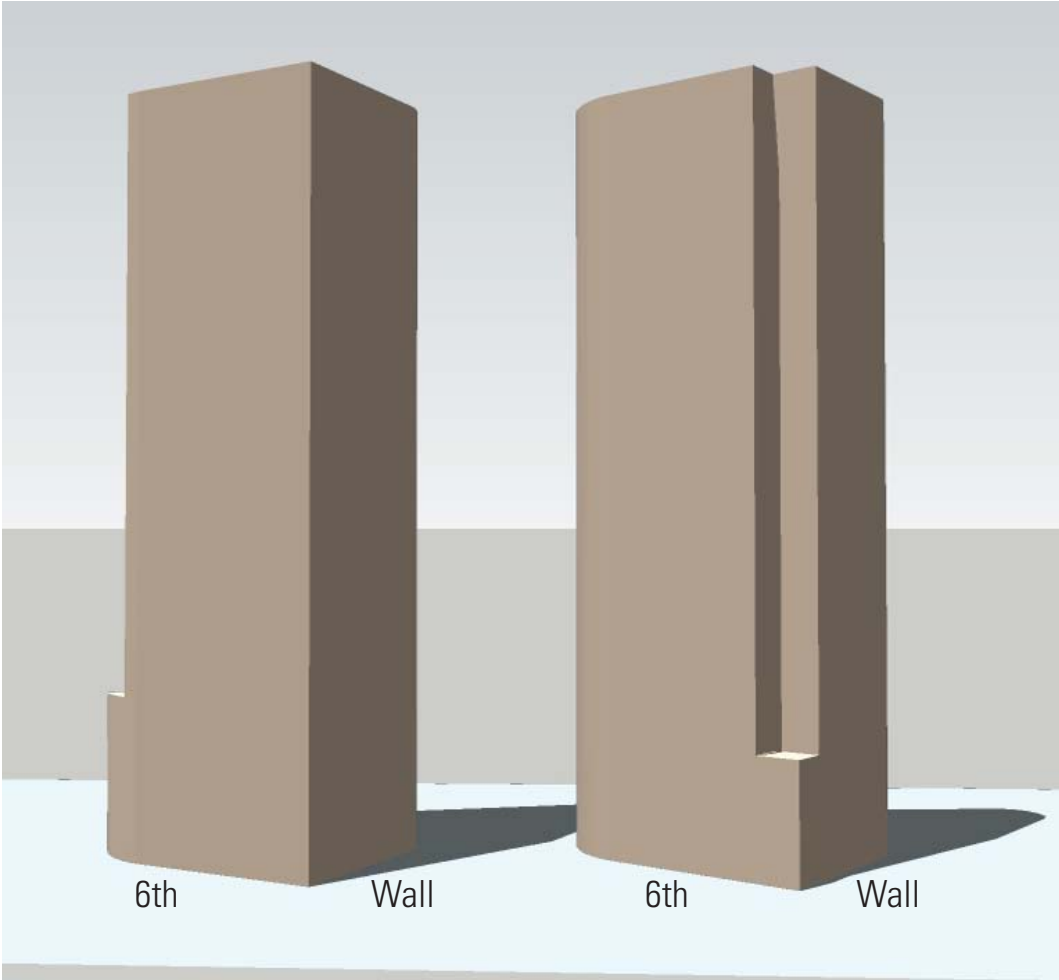


DIAGRAM C
Proposed Departure



Development Standard	B. Requirement	C. Proposed	Rationale
23.49.058D.2	In DMC zones, the maximum facade width for portions of a building above 85' along the general north/south axis (parallel to the avenues) shall be 120' or 80% of the width of the lot, which ever is smaller. On lots smaller than 10,700 sf the facade width shall be 120'	The proposed design along 6th Ave. would have a facade width of 144', of which 27' of the facade would be set back 23' from the property line. The result would be that 117' of the facade would be on the property line.	<p>A fully compliant fa ade width would result in a termination of the building approximately 24' from the corner of Denny and 6th. The resultant cut would be detrimental to the flatiron design opportunities that this triangular site provides. The proposed concept would result in a flat primary fa ade length of 117' with a 27' setback fa ade at Wall and 6th, which is set back 23' from the primary fa ade (see Diagram C).</p> <p>Also, a portion of the fa ade curves back from the flat plane towards Denny Way to reduce the visual impact (see Diagram C).</p> <p>The proposed building notch at the corner of 6th and Wall not only reduces the length of the flat fa ade along 6th Avenue, but also significantly reduces the fa ade length along Wall Street, thus resulting in a slimmer building profile.</p>

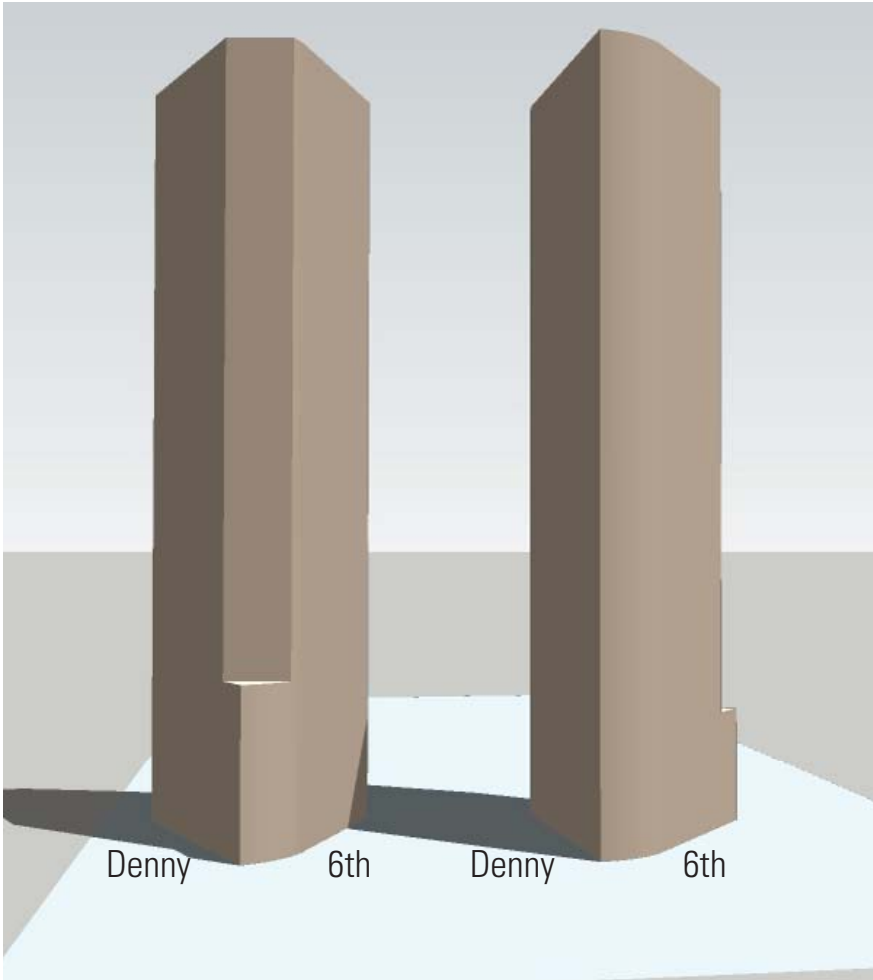
6th Avenue and Wall Street



Prescribed Massing

Proposed Massing

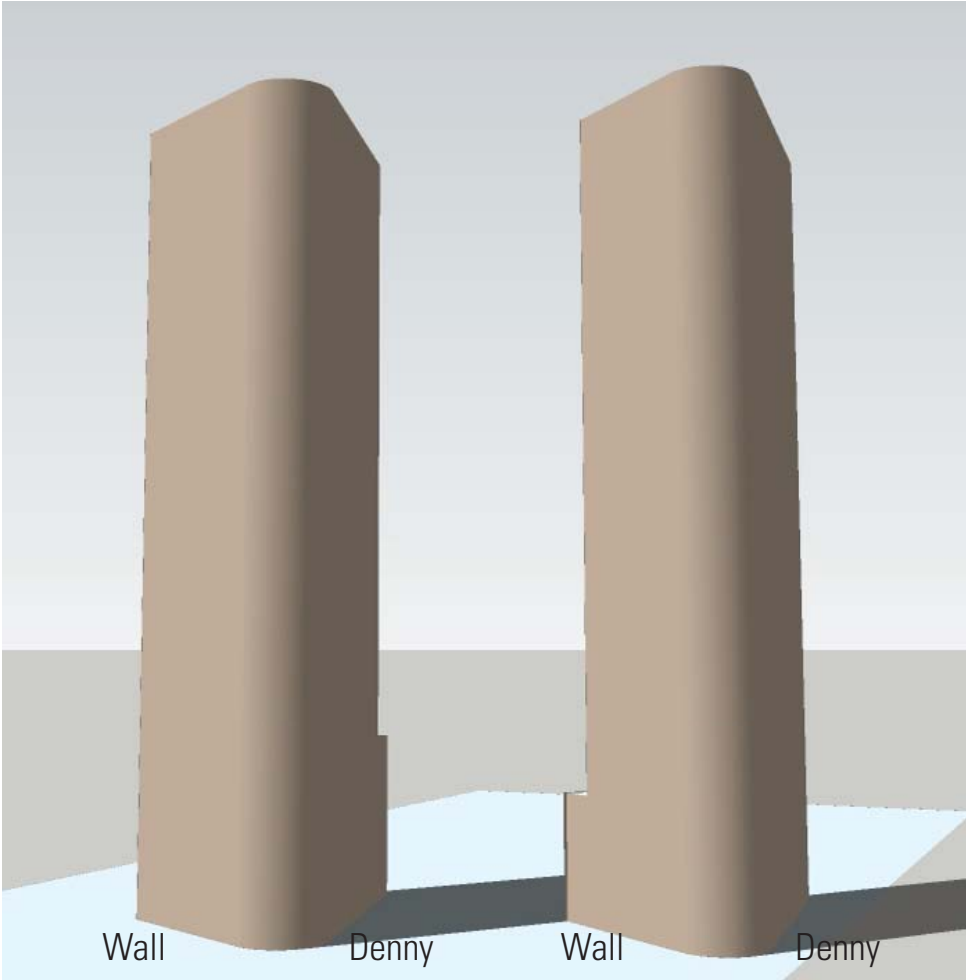
Denny Way and 6th Avenue



Prescribed Massing

Proposed Massing

Wall Street and Denny Way



Prescribed Massing

Proposed Massing

ALTERNATIVE DESIGN CONCEPTS

CONCEPT A

This alternative conforms with SMC facade width requirements. The 120' facade width along 6th Ave. would be achieved by notching the tower above 85' (see Alternative A plan and fig 1). Figure 2 is a view looking towards the corner of 6th Ave and Wall St. The Alternative building form illustrates the 120' facade width along 6th Ave., the preferred plan proposes to mitigate the facade width by notching the corner so that the facade on 6th. Ave is 117' and less than 90' along Wall St. Figure 3 illustrates a reduced street facade along Wall St. when viewed towards Wall and Denny.

Alternative A, while in conformance with SMC results in a less than satisfactory building expression at Denny and 6th Ave. The squared corner at 6th and Wall also increases the visual mass and does not reinforce the podium design element.

Note: This project in essentially the same form went through Downtown Design Review EDG and Recommendation in 2006, and had a DPD decision in April 2007. Both the 2006 DRB Recommendation and Decision endorsed the basic massing of Scheme C and similar departures presented in this booklet. This submittal is based on the scheme C massing.

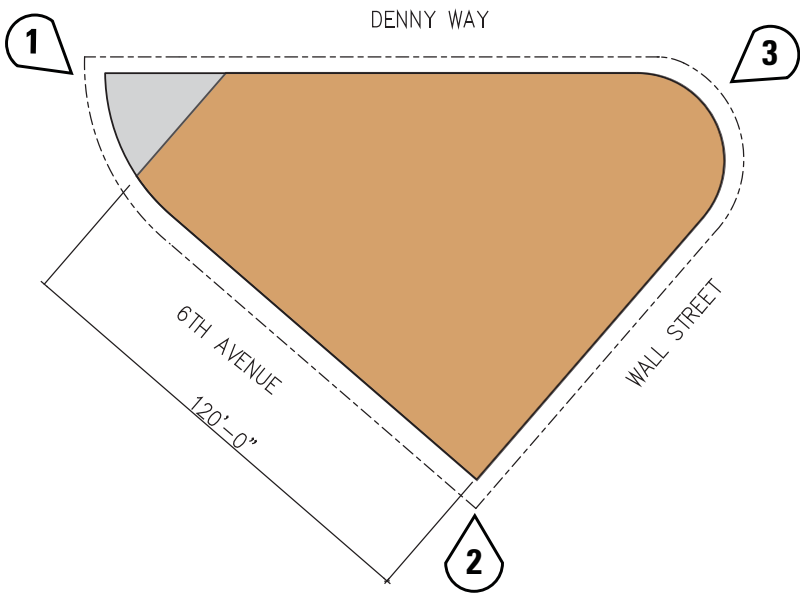
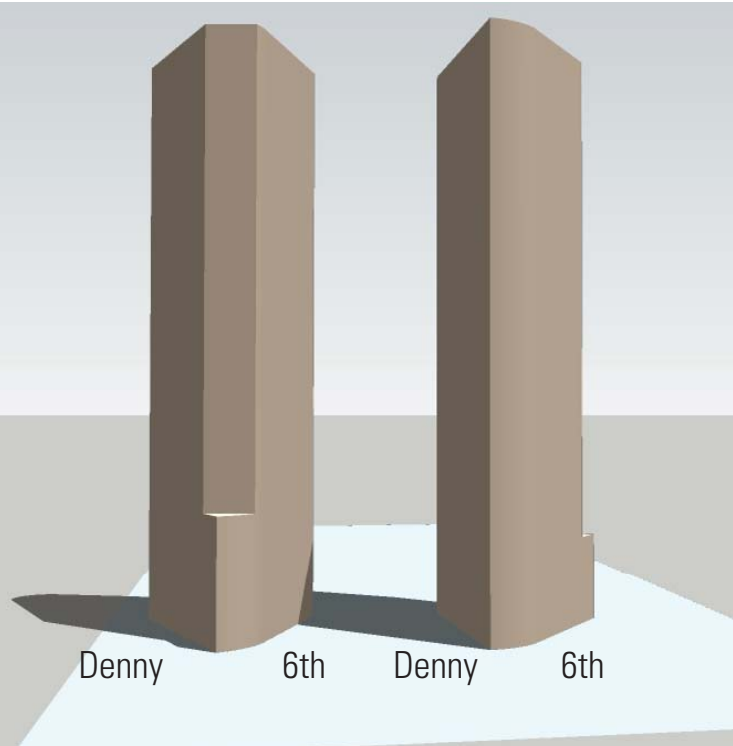
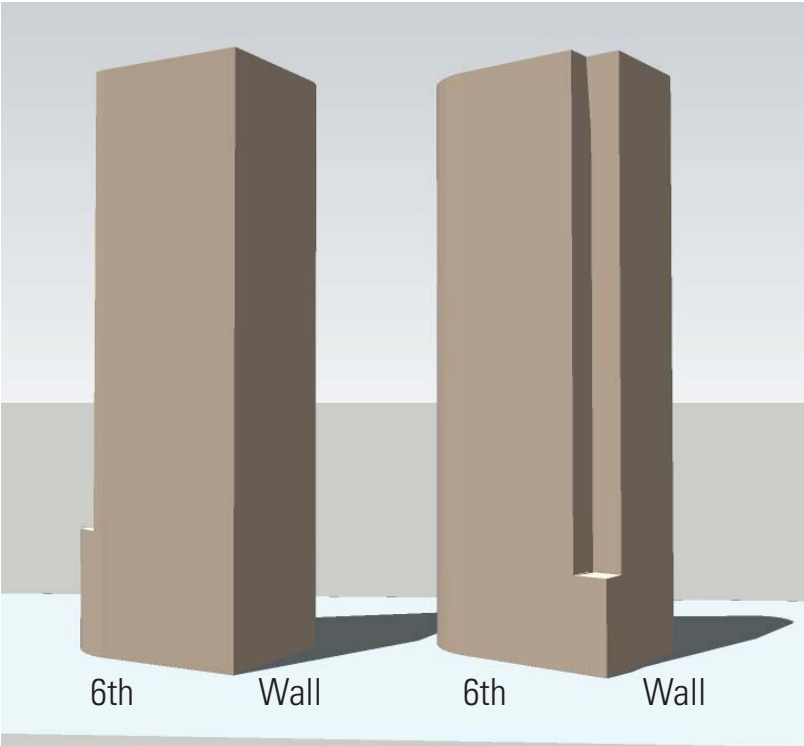


Figure 1 - Denny Way and 6th Avenue



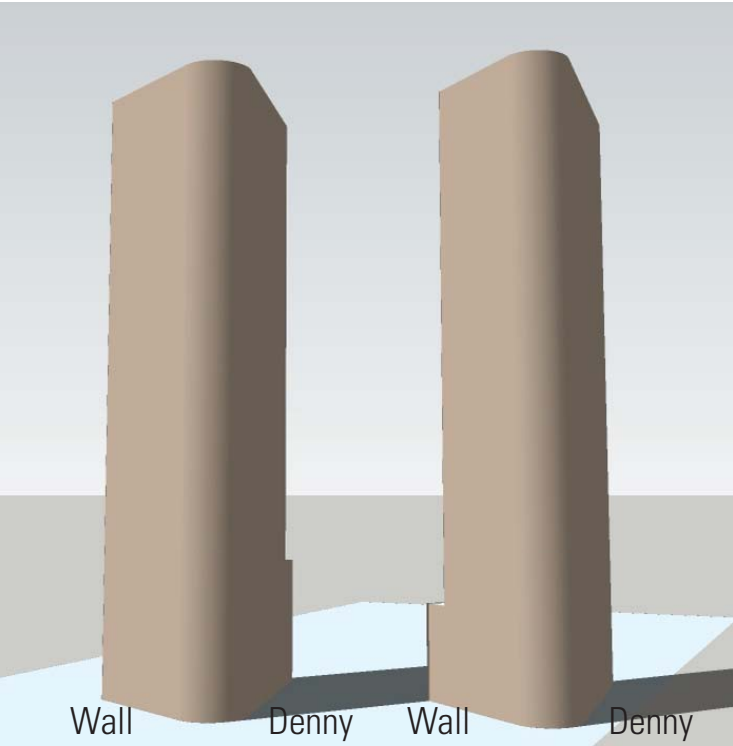
Alternative A Preferred Plan

Figure 2 - 6th Avenue and Wall Street



Alternative A Preferred Plan

Figure 3 - Wall Street and Denny Way



Alternative A Preferred Plan

CONCEPT B

This alternative is in conformance with SMC facade width requirements. The 120' facade width along 6th Ave. is achieved by setting back the tower above 85' along the length of Wall St. (See Alternative B plan). Figures 2 and 3 illustrates the building setback at the 85' elevation. The preferred plan proposes to notch the corner of 6th Ave. and Wall St. the result is a facade on 6th street that is 117' (at the property line) and less than 90' along Wall St. Figure 1 demonstrates that there is no significant visual impact when viewed towards the corner of Denny Way and 6th Ave.

The Alternative B concept results in an increase of a less modulated facade length along Wall St. The building notch in the preferred alternative provides for a strong architectural element that benefits both the 6th Ave. and Wall St. facades. The setback also disconnects the tower with the podium base this appears to be most accentuated when viewed at the Denny Wall corner.

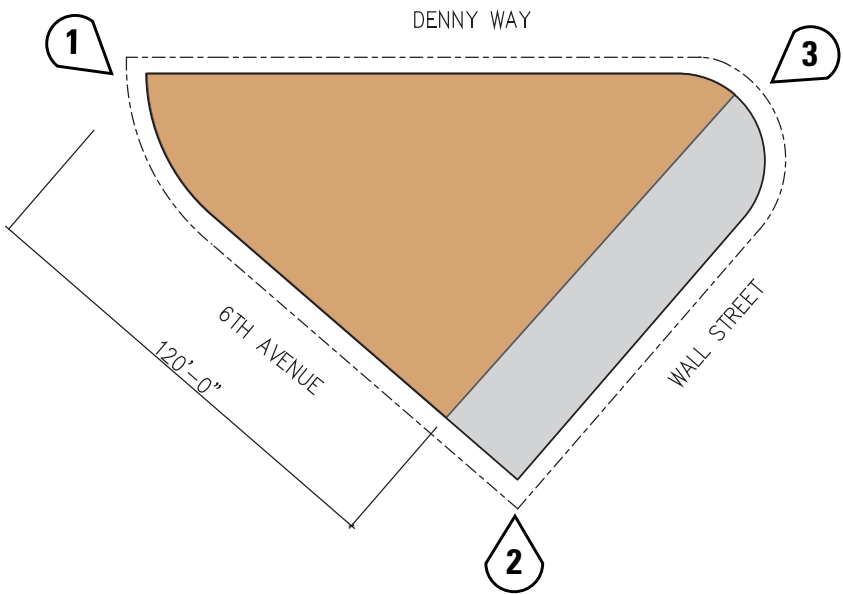


Figure 2 - 6th Avenue and Wall Street

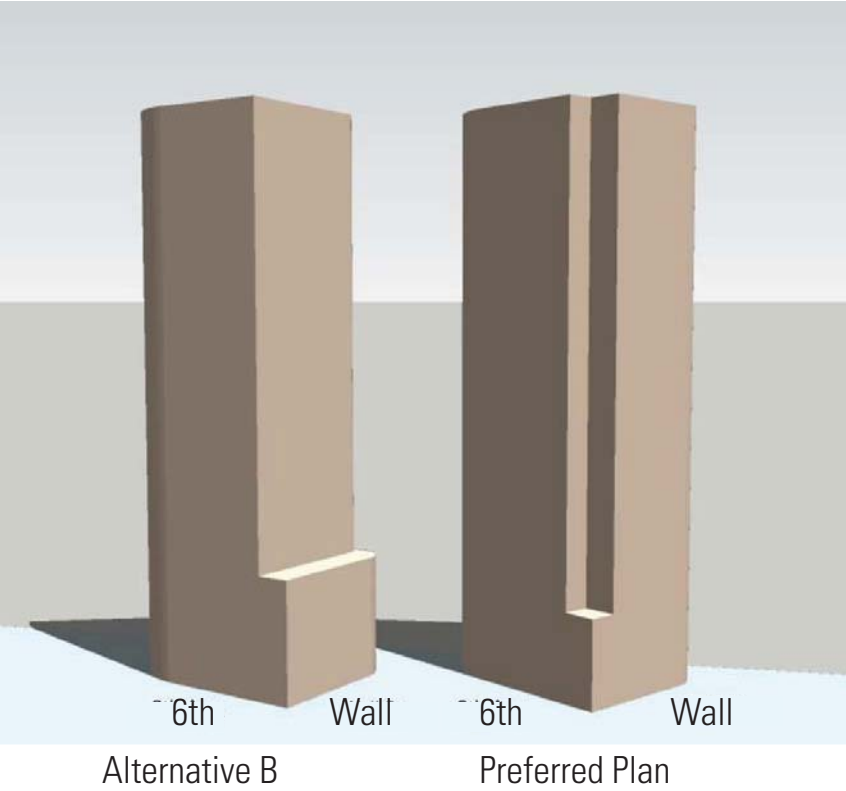


Figure 1 - Denny Way and 6th Avenue

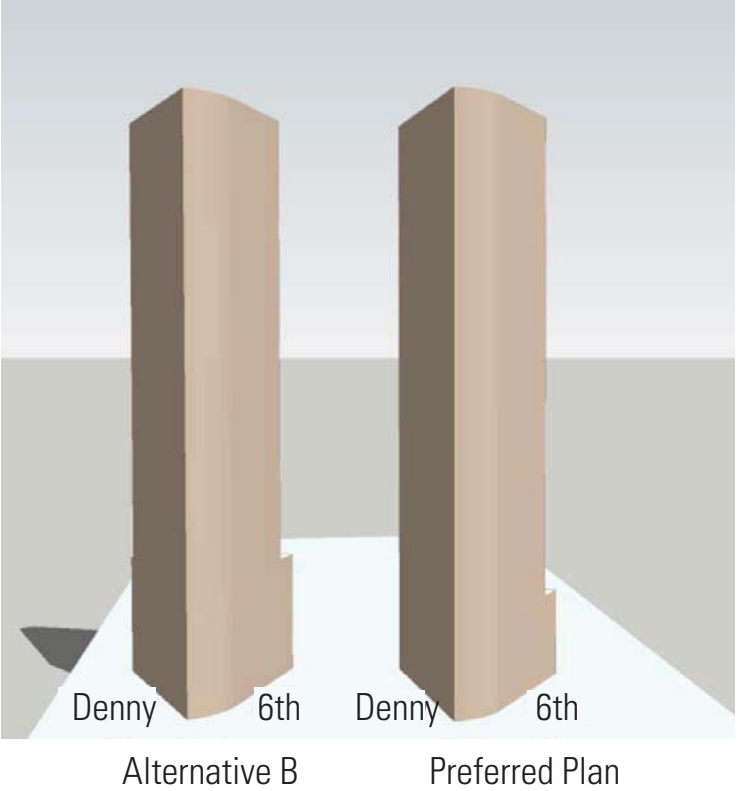
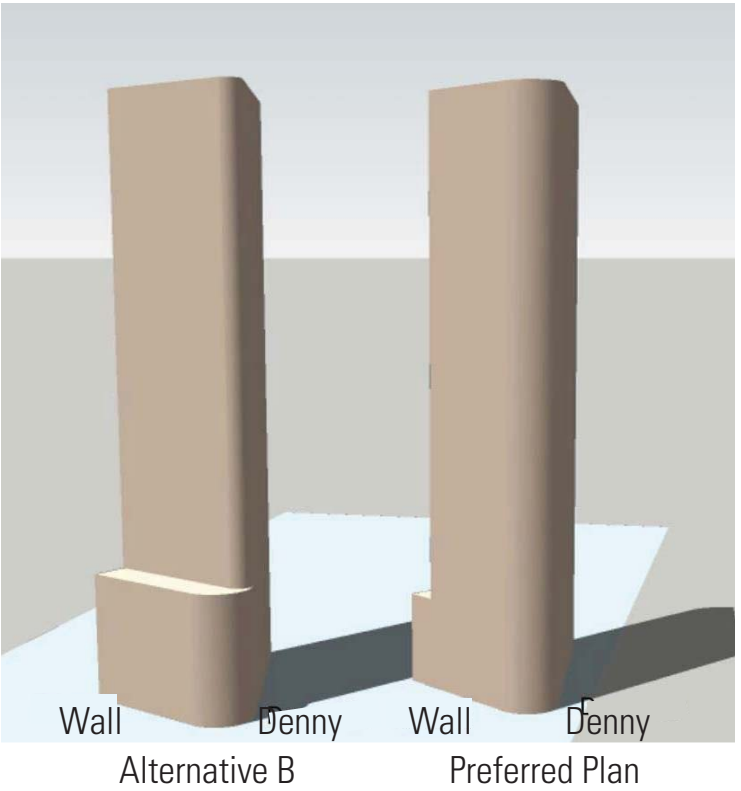


Figure 3 - Wall Street and Denny Way

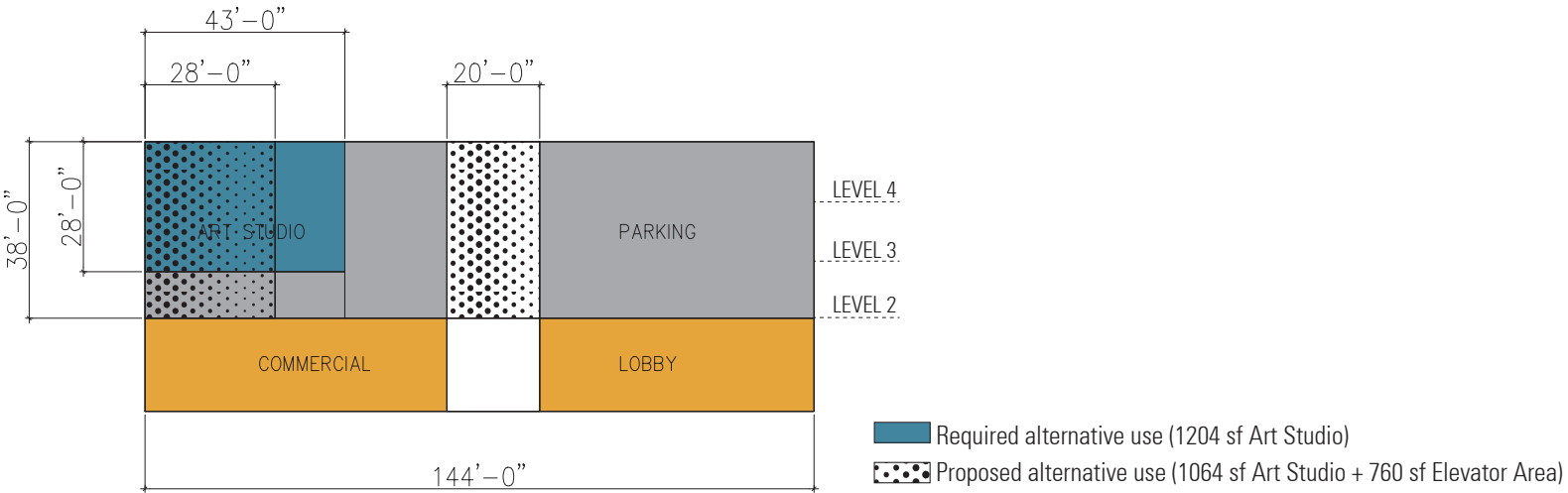


DEPARTURE 2: SEPARATION OF USES



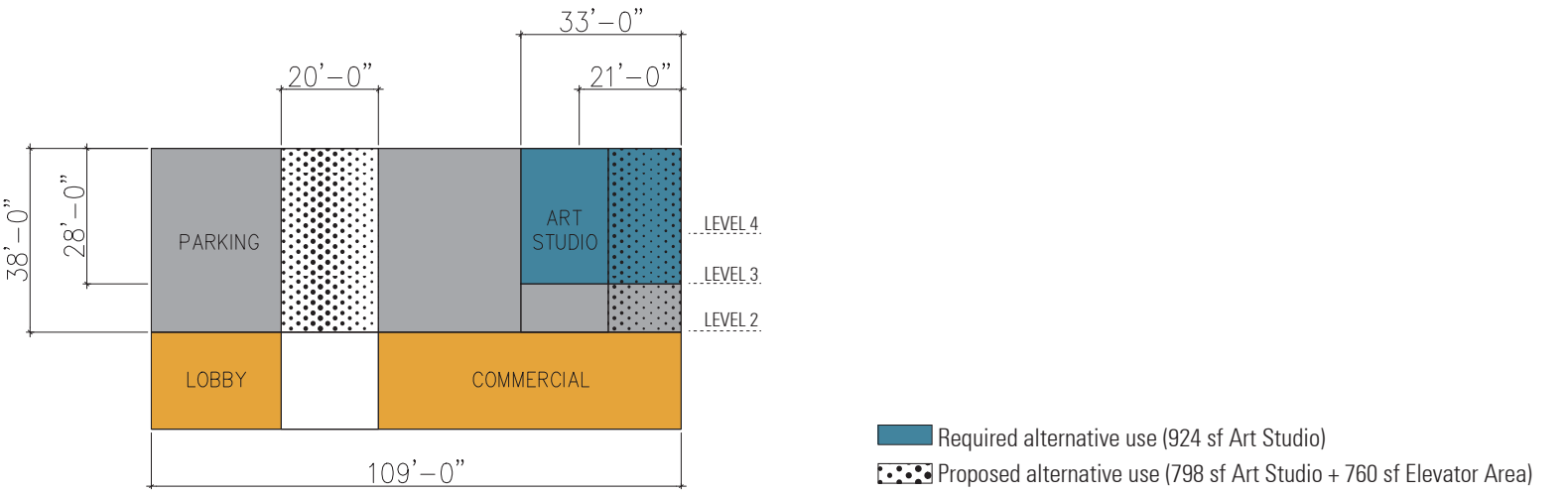
Development Standard	Requirement	Proposed	Rationale
23.49.01.2	(2) Parking above the third story of a structure shall be separated from the street by another use for a minimum of thirty (30) percent of each street frontage of the structure. For structures on lots located at street intersections, the separation by another use shall be provided at the corner portion(s) of the structure.	A departure is requested to reduce the alternative use minimum of 30% street frontage above level 3 along 6th Ave to 20% frontage use separation over levels 2 through 5. Along Wall Street the frontage use separation is 20% over levels 2 through 5. The facade area on 6th Avenue would be approx. 11% less than required and approximately 15% less than required on Wall Street. If the car elevator is allowed as an alternative use then the required lengths and areas would exceed minimum requirements.	The above grade automated parking system provides stacking for five levels (38' floor to floor) at close to the same height as a conventional parking structure with four levels (37' floor to floor). There will not be any visible vehicular movement or light glare from automobiles. There also will not be a need for exterior natural ventilation for car exhaust since cars will be transported by a mechanical system. We also propose to expose the vertical car elevator to public view from the street. The car elevator will be illuminated with low level LED lighting. Patterned clear and colored glass panels will provide an animated glass art mural, which will represent a distinctive visual break within the facade.

Proposed Departure for 6th Ave.



Development Standard	Required	Proposed w/o Elevator	Proposed w/ Elevator
	6th Ave.		
Facade Height	28' (Levels 3,4,5)	38' (Levels 2,3,4,5,6)	38' (Levels 3,4,5,6)
Facade Length	144'	144'	144'
Length of Alternative Use	30% alternative use required = 43.2'	28' (19%)	48' (33%)
Area of Alternative Use Above 2nd Floor	Area of alternative use above 3rd floor = 43.2' x 28' = 1,210 sf	28' x 38' = 1,064 sf	28' x 48' = 1,344 sf

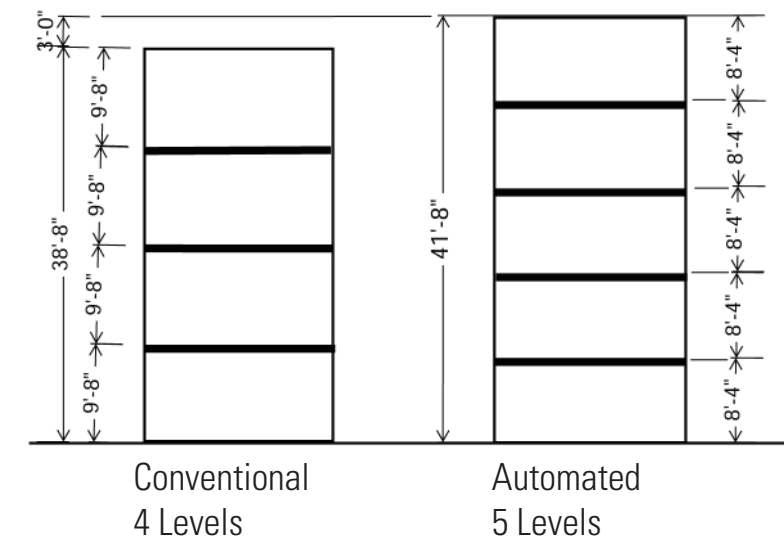
Proposed Departure for Wall Street



Development Standard	Required	Proposed w/o Elevator	Proposed w/ Elevator
	Wall St.		
Facade Height	28' (levels 3,4,5)	38' (levels 3,4,5,6)	38' (levels 3,4,5,6)
Facade Length	109'	109'	109'
Length of Alternative Use	30% alternative use required = 33'	21' (19%)	41' (38%)
Area of Alternative Use Above 2nd Floor	Area of alternative use above 3rd floor = 33' x 28' = 924 sf.	21' x 38' = 798 sf	21' x 48' = 1,968 sf

DEAPRTURE 3: PARKING

Comparative Parking Sections



Development Standard	Requirement	Proposed	Rationale
23.49.019.2.A.1	One (1) story of parking shall be permitted above the first story of a structure for each story of parking provided below grade that is of at least equivalent capacity, up to a maximum of four (4) stories of parking above the first story.	A departure is requested to allow 5 levels of automated parking above the first floor.	WASHDOT has established an area around the tunnel in which no construction is allowed (see adjacent diagram). By special dispensation, WASHDOT has agreed to allow the tiebacks for our below grade garage to encroach a certain distance into the no-construction area. This allows us 6 parking levels only below grade. The 5 levels of above grade parking are contained within a height (41'-8"), which is only slightly greater than the height of a 4 level conventional parking structure (38'-8").

Parking level behind wall panel

Three levels of parking behind translucent wall panels

Parking level behind wall panel



SHORING + TIE BACK STUDY

TOPOGRAPHIC SURVEY AND SHORING EXCAVATION

Diagram of Tunnel Proximity

