

**‘N’ HABIT DEXTER
1701 DEXTER AVE NORTH
SEATTLE WASHINGTON**

DRB RECOMMENDATION MEETING

DPD# 3015186, REVISIONS TO ISSUED MUP DPD# 3008741

MEETING DATE: SEPTEMBER 11, 2013

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PROJECT TEAM

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Seattle WA, 98103

ARCHITECT

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Seattle, WA 98144

LANDSCAPE ARCHITECT

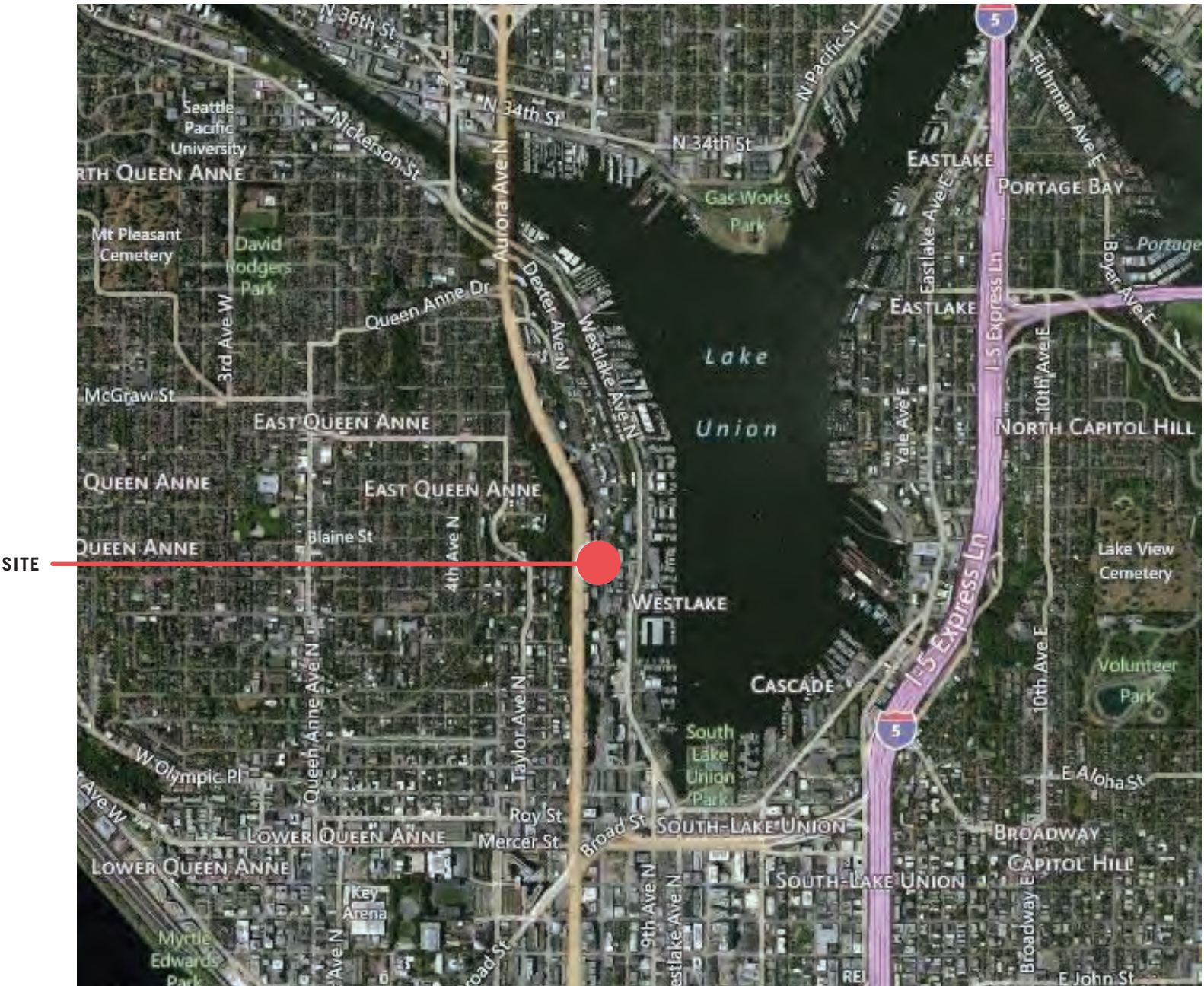
Karen Kiest I Landscape Architects
111 West John Street Suite 305
Seattle, WA 98119

CIVIL ENGINEER

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Seattle, WA 98101

SURVEYOR

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2009 Minor Ave. East
Seattle, WA 98102



Vicinity map



PROJECT HISTORY

The proposed project is a revision of MUP #3008741, which was issued on March 20, 2009.

Drawings for the previously approved MUP were prepared by Mithun/Hybrid Architects for Unico.

In 2012, the current owners, Daly Partners, acquired the project and site. The current proposal retains the major elements of the originally approved EDG. Revisions to the project include reorganized circulation, facades and unit layout and distribution. The intent behind the revisions is to improve constructability and livability aspects of the project while retaining the approved site plan and massing.

PROJECT SITE

The site address is 1701 Dexter Avenue North. The site is zoned NC3-40. Height limit for the site is 40'. 44' is permitted with 13' floor to floor at ground floor live/work.

Located on the west side of Dexter Avenue North at the intersection of Hayes Street, the site consists of 2 lots that measure approximately 150 feet north-south and 108 feet east-west. Total lot area is approximately 16,231 square feet.

DESIGN GOALS

High quality/high design value, mid-income, in-city housing.

High density, mixed use project that supports multiple modes of transportation and an active, safe ground level streetscape.

Reduced the environmental footprint of the building construction and use. Project is slated to be built as factory-built modular construction at Levels 2 - 6.

DEVELOPMENT OBJECTIVES

Terraced 59,000 SF four-story mixed-use building

Four ground floor live/work units and one small commercial space.

±62 residential units including a combination of flats and two-story apartments.

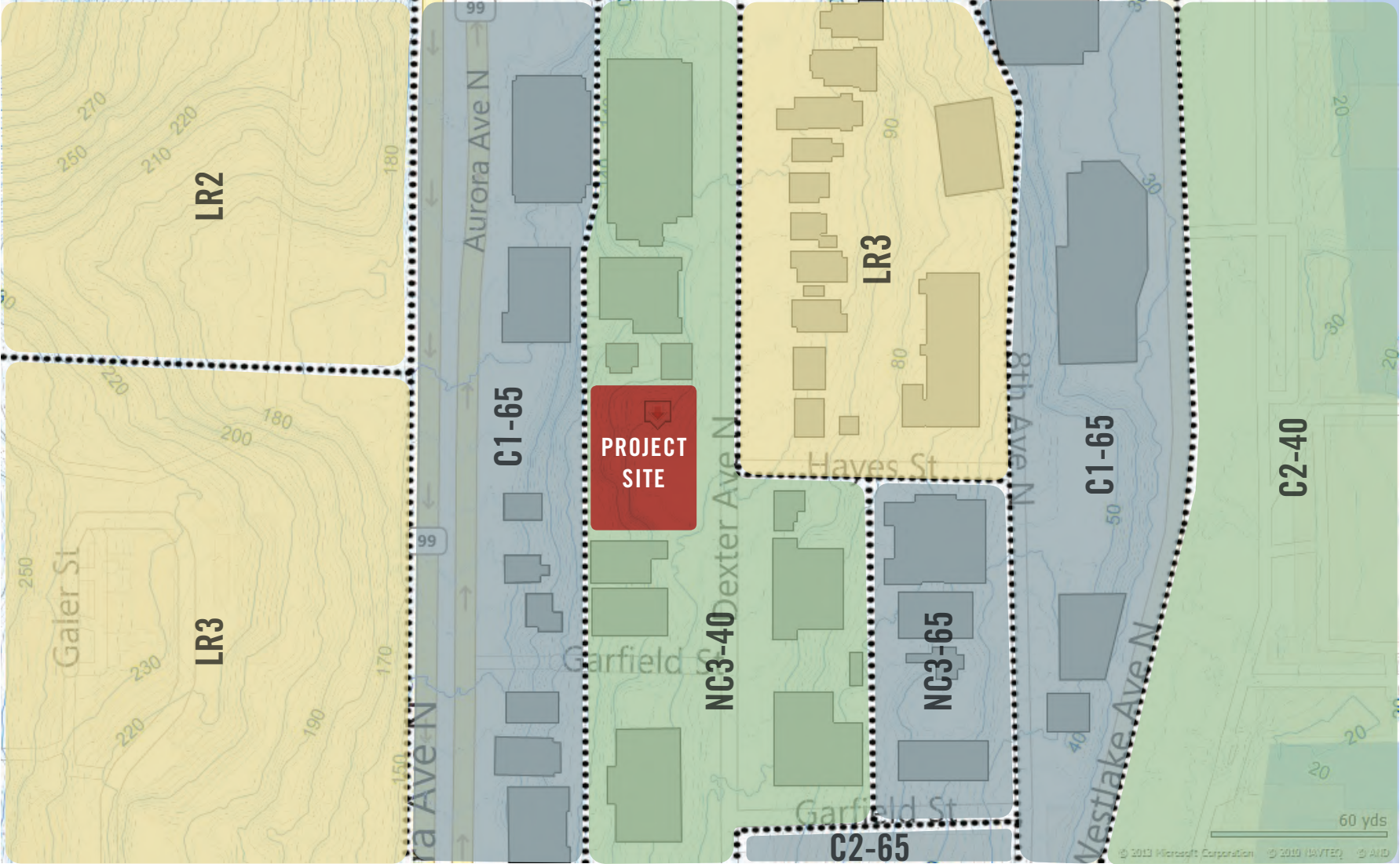
Parking for 44 vehicles accessed from Dexter Ave N.

ZONING/PROJECT VICINITY

Zoning and allowable building heights step with the topography in the area. Zoning boundaries tend to run parallel with Lake Union and 99/Aurora Ave N.

Directly west of the site is a band of C1-65 zoning that runs parallel to 99/Aurora Avenue N. Farther west, zoning shifts to LR2 and LR3 on the east slope of Queen Anne.

East of the site, the topography steps down toward Lake Union. Zoning is a mix of 65' heights to the south of Hayes and along Lake Union and lower density LR3 north of Hayes.



Zoning Map



PROJECT SITE

The project site is located on the west side of Dexter. The site is bound by Dexter Avenue North to the east and a 20-foot alley on the west. The site slopes approximately 25 feet from the alley to Dexter Avenue. West of the alley, grade continues to slope up to 99/Aurora Ave N. This hill is a vegetated buffer between the project site and 99.

Dexter Avenue North is a two-way arterial that serves as prime vehicular, pedestrian and bicycle access to the site. A bike lane on the west side of Dexter has been added since the original MUP was issued. Hayes Street, a smaller side street that bisects the site, is a shortcut to and from Lake Union and an informal spine for a cluster of residential projects on the east side of Dexter Avenue N.

The site area is mixed from the standpoint of building size and use. North of the project site are large and small-scale residential buildings including multistory apartments and single-family homes. South of the project site are large and small-scale commercial and residential buildings and surface parking lots.

The existing structure on the north half of the site is proposed to be demolished as part of the proposed project.

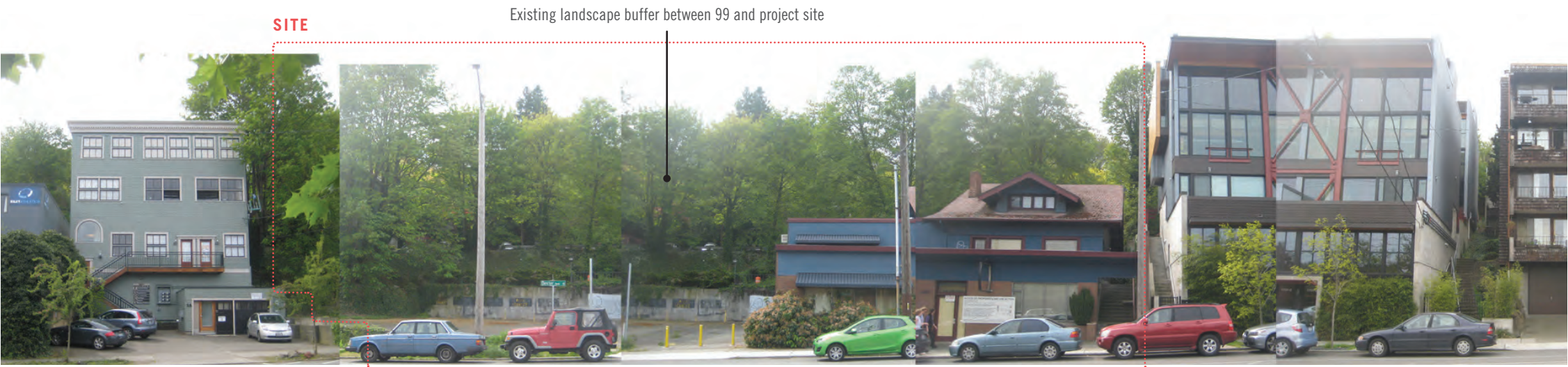


Photo of site from Dexter Avenue N



Aerial photo of site



DESIGN OPPORTUNITIES

Location on the eastern slope of Queen Anne. Site enjoys dramatic view of Lake Union and downtown.

Good solar exposure is to east.

Prevailing winds from the South. Contribute to passive ventilation of apartment units during summer months

Street frontage on Dexter Avenue N. Opportunities to create a hospitable, pedestrian-oriented streetscape that connects to the smaller scale buildings around Hayes Street.

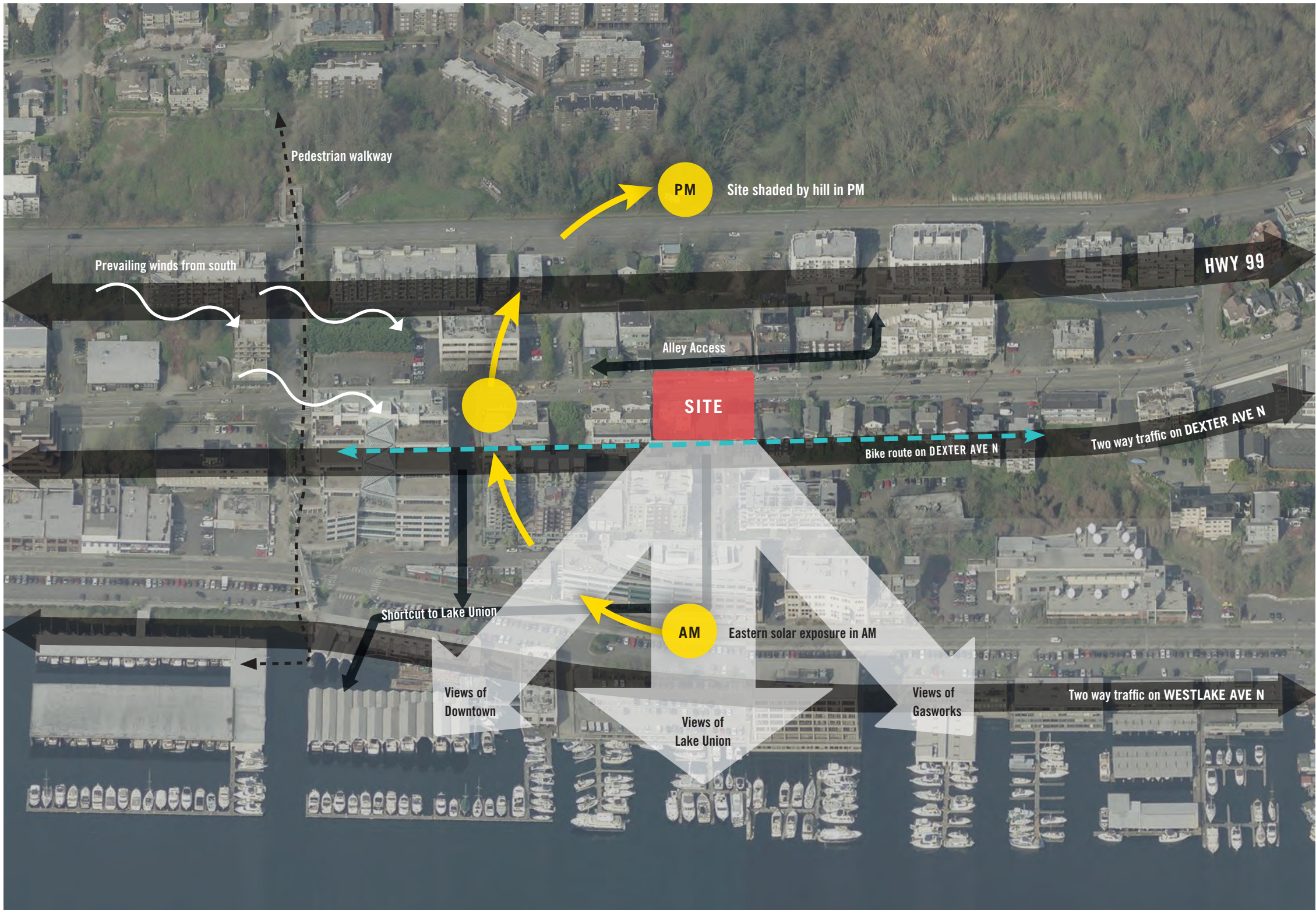
Combined pedestrian, bicycle and vehicular traffic along Dexter Ave N. Opportunities to support active streetscape.

Access to parking from Dexter Ave North is typical in the vicinity.

DESIGN CHALLENGES

Vehicular traffic along Dexter Ave N. Streetscape design should prioritize pedestrian.

Steep slope of site. Shading of the slope during the afternoon may limit daylighting potential.



Aerial photo of site with site analysis



USE ANALYSIS/VICINITY PHOTOS



View of Riley Athletics (commercial) 2



View of neighboring commercial building 1



View of The Block (mixed use) 3



View of The Kilbirnie Apartments 4



View of The Summit Apartments 5



View of Tavern with Apartments above (mixed use) across the street 20



View of commercial building across the street 19



View of Westlake Union Center 17



HUE (mixed use farther south on Dexter) 35



Union SLU (new mixed use farther south on Dexter) 36

KEY

- SINGLE FAMILY RESIDENTIAL
- MULTIFAMILY
- RETAIL/COMMERCIAL
- OFFICE
- SHOP/WAREHOUSE/LIGHT INDUSTRIAL
- MIXED USE
-
- 1

 MONOHAN BROTHERS (LIGHT INDUSTRIAL)
- 2

 RILEY ATHLETICS (COMMERCIAL)
- 3

 THE BLOCK CONDOMINIUMS (MIXED USE RESIDENTIAL)
- 4

 KILBIRNIE APARTMENTS (MULTIFAMILY)
- 5

 THE SUMMIT APARTMENTS BLDG A (MULTIFAMILY)
- 6

 OMEGA CORPORATE SECURITY (OFFICE)
- 7

 APARTMENT (MULTIFAMILY)
- 8

 THE SUMMIT APARTMENTS BLDG B (MULTIFAMILY)
- 9

 THE SUMMIT APARTMENTS BLDG C (MULTIFAMILY)
- 10

 SINGLE FAMILY RESIDENCE
- 11

 SINGLE FAMILY RESIDENCE
- 12

 SINGLE FAMILY RESIDENCE
- 13

 SINGLE FAMILY RESIDENCE
- 14

 APARTMENT (MULTIFAMILY)
- 15

 ASSOCIATION CENTER (OFFICE)
- 16

 OFFICE/RETAIL
- 17

 WEST LAKE UNION CENTER (OFFICE)
- 18

 1600 DEXTER BUILDING (OFFICE)
- 19

 UNION VIEW APARTMENTS (MIXED USE)
- 20

 TAVERN/APARTMENTS (MIXED USE)
- 21–30

 SINGLE FAMILY RESIDENCES/TOWNHOUSES
- 31

 DEXTER TERRACE APARTMENTS (MULTIFAMILY)
- 32

 CHATEAU D'MIL APARTMENTS (MULTIFAMILY)
- 33

 OFFICE/APARTMENTS (MIXED USE)
- 34

 APARTMENT (MULTIFAMILY)
- 35

 UNION SLU (MULTIFAMILY)
- 36

 HUE (MULTIFAMILY)



Alley view of The Block (mixed use)

3



Alley view of the building to the south

1



Alley view of the building to the west

10



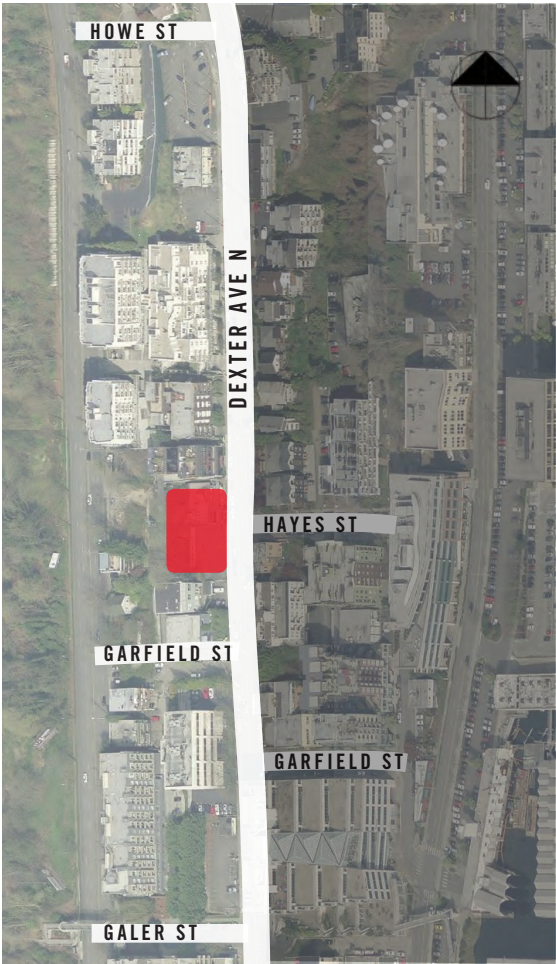
Building to the west seen from Hwy 99

10

DEXTER AVENUE N STREETSCAPE LOOKING WEST

Streetscape photos of Dexter Avenue North from Galer Street to Howe Street show the wide range of building sizes (one-story to five-story) and mix of uses (commercial, residential and surface parking lots) along Dexter Avenue North. Residential uses are more typical north of site and across Dexter on both sides of Hayes Street. Commercial uses are more common directly south of the site. Farther south on Dexter a number of large scale residential projects are also under construction.

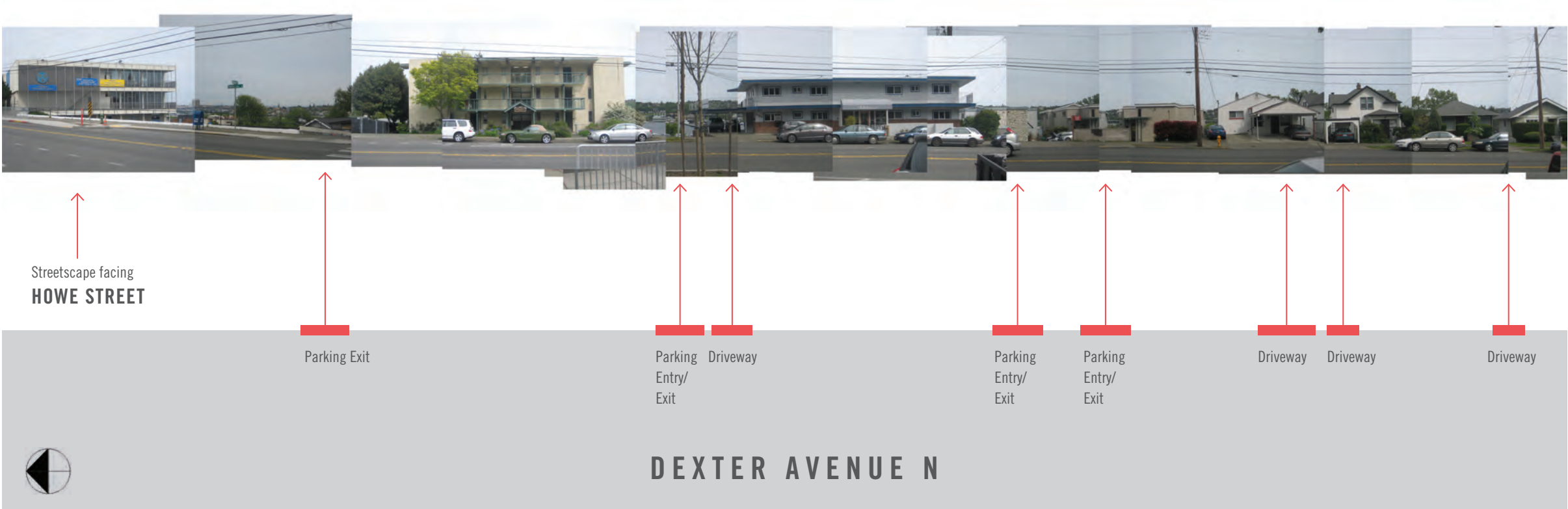
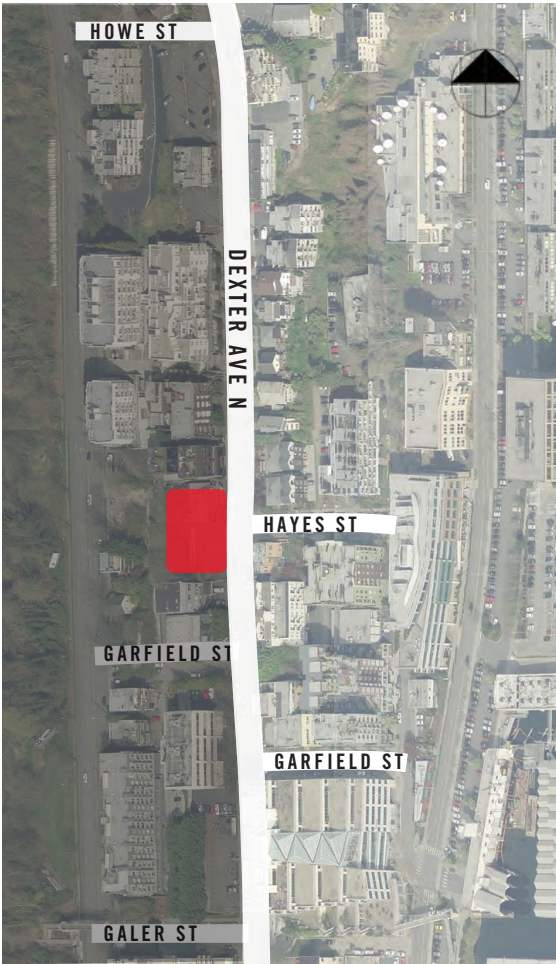
Due to steep slope that extends from Queen Anne hill down to Lake Union, parking access in the area tends to occur from the low (east) side of project sites. Most buildings on the west side of Dexter have parking access from Dexter. Smaller buildings on the east side of Dexter also typically have parking access from Dexter.



SITE



DEXTER AVENUE N STREETSCAPE LOOKING EAST





PROPOSED PROJECT

The proposed project is a revision of the approved MUP #3008741. The original MUP was issued on March 20, 2009. The Early Design Guidance meeting for the original MUP took place on April, 30 2008. The Design Review Recommendation meeting for the original MUP took place on August 6, 2008. The design team for the original MUP was Mithun/Hybrid Architects. The project owner was Unico.

In 2012, the current owners, Daly Partners, acquired the project and site. The current proposal retains the approved site plan, massing, streetscape interface and open space allocation in the original EDG. The current proposal revises the project circulation, facades and unit layout and distribution. Changes that affect the EDG approved massing include:

- Removal of parking on alley.
- Reorganization of project circulation. Vertical and horizontal circulation is now largely exterior.
- Reorganization of west (alley-facing) building massing to maximize solar access, passive ventilation, privacy and views for dwelling units.
- Modification of L2 courtyard shape and landscaping based on solar analysis.
- Relocation/reorganization of trash/recycling.

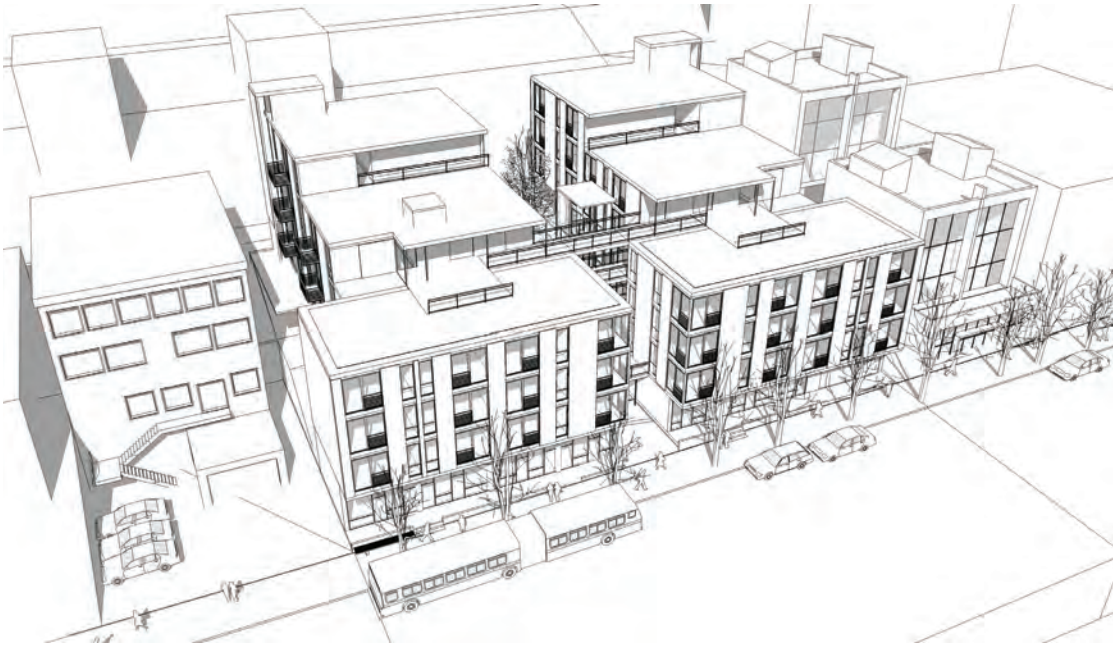
RESPONSE TO DESIGN GUIDANCE

The guidelines on the next three pages were chosen by the Board at the previous EDG AND Recommendation meetings as the highest priority for the project.

Text in the yellow bar is taken from the meeting notes for the 4/30/08 EDG meeting and the 8/6/08 Recommendation meeting.

Notes above the yellow bar (solid line) reflect aspects of the proposed design that are consistent with the previously approved MUP.

Notes below the yellow bar (dashed line) reflect additional project development in the current design.



Model view of approved EDG scheme (prepared by Mithun/Hybrid).



Model view of current project proposal

PREVIOUSLY APPROVED MUP

- Terraced building steps with topography.
- Massing has a continuous mid-block break through site aligned with Hayes Street.
- Dexter street level massing presents two separate buildings connected by circulation.
- Open air residential entry at the mid-block break between buildings leads to open air courtyard on Level 2.
- Dwelling units organized around L2 courtyard.
- West-facing common amenity roof deck at Level 5.
- Parking access from Dexter Ave N.
- Project is wood-frame factory-built modules at upper levels.

PREVIOUSLY APPROVED MUP + PROPOSED ADDITIONS

- Terraced building steps with topography.
- Massing has a continuous mid-block break through site aligned with Hayes Street.
- Dexter street level massing presents two separate buildings connected by circulation.
- Open air residential entry at the mid-block break between buildings leads to open air courtyard on Level 2.
- Dwelling units organized around L2 courtyard.
- West-facing common amenity roof deck at Level 5.
- Parking access from Dexter Ave N.
- Project is wood-frame factory-built modules at upper levels.

- +
- Building massing divided into six ground-related massing elements separated by exterior circulation (exterior stairs and egress balconies).
 - West (alley-facing) massing reconfigured. Reconfiguration allows for better solar access at L2 courtyard and west-facing units at Levels 5 and 6. Reconfiguration also allows for code compliant trash room on the alley.

CONTINUED FROM PREVIOUS MUP

Terraced building massing steps with the site topography.

Continuous mid-block break aligned with Hayes Street provides continuous opening through the site from Dexter to the alley.

The live/work units are set back from the property line and have an accessible access gallery that provides a transition between the sidewalk and live/work units.

The open air residential lobby occurs at the mid-block break aligned with Hayes.

Live/work units have 13' floor to floor height minimum.

Residential units are organized around an open air courtyard at L2.

Level 5 common roof deck provides shared access to west-facing deck with views to Lake Union and Downtown.

EDG/REC COMMENTS

A-1 RESPOND TO SITE CHARACTER 4/30/08 EDG: "The units appropriately stepped up the hillside to accommodate significant changes in site elevation." 4/30/08 EDG: "The Board acknowledged that the overall massing of the project...in the preferred option seemed right for the setting and context."	A-2 STREETScape COMPATIBILITY 4/30/08 EDG: "Human activity on the street should be promoted by the interface of sidewalk and the live/work units." 4/30/08 EDG: "...the live/work units and their access pathway with the residential entry and the public sidewalk should be finer tuned and should demonstrate a proper scale for clear interaction with the fronting sidewalk and public realm. "	A-3 ENTRANCES VISIBLE FROM STREET 4/30/08 EDG: "Providing for vehicles entering and leaving the site should not interfere or diminish in any way the desired goal of enlivening the street."	A-4 HUMAN ACTIVITY 4/30/08 EDG: "A[n] issue was the need for the live/work units to be of sufficient size properly to function as something other than mere residential units." 8/6/08 EDG: The Board agreed that the size, configuration, and placement of the live/work units as presented satisfied the concerns expressed at the EDG meeting.	A-7 RESIDENTIAL 4/30/08 EDG: "Guideline A-7 was cited to re-enforce the Board's acknowledgement that the proposed inner courtyard with lush landscaping should continue to be developed as an attractive and vital space for the residents of the project."
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ADDITIONAL PROJECT DEVELOPMENT



Interface between live/work units and sidewalk has been further developed to include an architecturally defined stoop for each live/work.

Size of mid-block break increased to 18'-4" from 15'.

Exterior vertical circulation, decorative metal entry gate and canopy, wood bench and exterior lighting create a well-marked and inviting residential entry.



Minimum live/work depth increased to 36'. Average live/work depth increased to ±45'. Increases support separation of live and work uses.



The L2 courtyard has been reconfigured to be more square for better solar orientation from the south. The courtyard has also been reconceived as a common plaza for all residents rather than a lushly planted semi-private space. The courtyard in the original MUP was an elongated rectangle oriented east-west. Courtyard shadow studies (see page 43) showed that the courtyard would likely not support the lush planting proposed in the original MUP.

L5 roof deck increased in size. Community room with kitchen added adjacent to roof deck.

Unoccupied green roof added at L5 and L6.

RESPONSE DESIGN GUIDANCE

CONTINUED FROM PREVIOUS MUP

Building massing is organized symmetrically around the mid-block break at Hayes Street and the central L2 courtyard.

The live/work units are set back from the property line and have an accessible access gallery that provides a transition between the sidewalk and live/work units.

EDG/REC COMMENTS
(continued)

B HEIGHT, BULK AND SCALE	C-2 CONCEPT AND CONSISTENCY	C-3 HUMAN SCALE	C-4 EXTERIOR FINISH MATERIALS	D-1 PED OPEN SPACES/ENTRANCES
		4/30/08 EDG: The Board noted that the project should explore opportunities to achieve a good human scale, especially as it informs the specific ways the live/work units address and provide for a transition to the sidewalk.	4/30/08 EDG: Architectural materials, scale and details should be integrated within a building whose concept is appropriate for the site and its surroundings as well as its programmatic uses. The Board was not prescriptive regarding materials, but would expect to see a choice of durable and sustainable materials...to be presented with...both proposed colors and materials at the subsequent recommendation meeting.	4/30/08 EDG: "Serious attention should be given so as not to provide too much physical or psychological separation of the live/work units from the sidewalk. Such would be detrimental to the commercial functioning of these spaces. Expression should be given to clear path-finding details and to appropriate lighting and, in particular, signage."

ADDITIONAL PROJECT DEVELOPMENT

Building massing divided into 6 separate masses connected by exterior circulation elements. Exterior circulation provides a continuous break between the buildings in north-south direction as well, which further reduces the project scale.

West (alley-facing) massing reconfigured to maximize solar access for units and courtyard and passive ventilation, privacy and views for units.

Architectural concept is cluster of ground related buildings organized around a common courtyard and connected by "light" circulation elements.

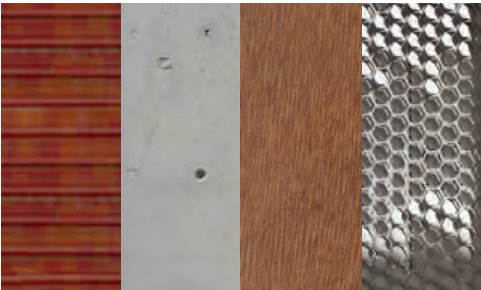
Residential units are oriented to maximize opportunities for passive ventilation, solar access, privacy and views at upper levels.

See A-2, A-3 and A-4 for street level human scale elements associated with live/work.

Exterior finish materials aim for a clear material logic on each building mass and to create opportunities for lighter colored materials on the interior courtyard faces of each building.

Proposed materials are a combination of durable materials that will weather over time (corten steel, concrete frames), materials that provide warmth (wood panels), fine detail (metal screen and canopy at L1, painted exterior structural steel circulation elements) and contrast (painted concrete and fiber cement board).

Each live/work has an architecturally defined stoop, ROW landscaping, overhead weather protection and lighting. Signage will be developed within framework of live-work stoops.



CONTINUED
FROM
PREVIOUS
MUP

Trash room and mechanical equipment located off the alley.

Building massing is organized symmetrically around the mid-block break at Hayes Street and the central L2 courtyard.

EDG/REC
COMMENTS
(continued)

D-6 SCREENING OF DUMPSTERS, UTILITIES AND SERVICE AREAS

8/6/08 REC: "...the size of space allotted for waste and recyclables was minuscule...the design team should explore whether there might be some give in the size of the proposed existing stairs at the northwest and southwest corners... to eke out a bit more room for the storage of disposables."

D-12 RES. ENTRIES AND TRANSITIONS

4/30/08 EDG: "...the opening into the building aligned opposite the Hayes Street intersection was a desirable feature of the proposal as was the courtyard located at a higher level at the heart of the project."

E-2 LANDSCAPING TO ENHANCE BUILDING/SITE

4/30/08 EDG: Landscaping should be designed with the goal of realizing the prioritized guidelines, should soften the edge conditions where appropriate, and should contribute to an attractive and usable interior open space. The design should incorporate specific treatments to provide for an attractive transition between the sidewalk and the live/work units."

BUSSTOP

8/6/08 REC: Since the applicants had made much of promoting alternative modes of transportation for the residents within these units, the Board had requested at the Early Design Guidance meeting that design development of the Dexter Avenue N. façade and adjoining streetscape should look for ways to incorporate the bus stop into the architecture.

DRIVEWAY ACCESS

8/6/08 REC: "the Board was agreed that taking access for all the parking from the alley was impracticable given the steep topography between street and alley (see Guideline A-1) and given the size, scope and innovative (using modular, pre-fabricated units) nature of the project. The Board recommended that the design team explore recessing the placement of the garage door even further interiorly and away from the sidewalk edge.

ADDITIONAL
PROJECT
DEVELOPMENT

As part of the reorganization of the west (alley-facing) portion of the project, the trash room has been relocated and enlarged. The room accommodates the required number of trash, recycling and compost bins. The previously requested design departure has been eliminated as a result of these changes.

The street scape elements define each live/work unit and create a clear zone of residential entry at the mid-block break across from Hayes. The distinction between live/work and residential entries is architecturally defined and enhanced by landscaping.

See A-2/A-3/A-4 for a summary of streetscape development and D-1 for residential entry.

Landscape plans show proposed plans for the Dexter Avenue streetscape, L2 courtyard and private patios, L3 patios, L5 common roof deck, private patios and unoccupied green roof, and L6 unoccupied green roofs. Where landscaping is provided, it is integrated into the architecture, used to soften the edge of the building and to provide privacy and outdoor space for the related units.

The bus stop that was in front of the site has been removed since original MUP was approved. As a result, the current proposal does not include a bus stop on Dexter Avenue.

The driveway has been shifted 8'-3" to the south. The driveway in the original approved MUP was 3' off the north property line and the driveway in the current proposal is 11'-4" off the property line. The garage door has also been set back an additional 1' from what was shown in the original MUP. See Departure Request 1.

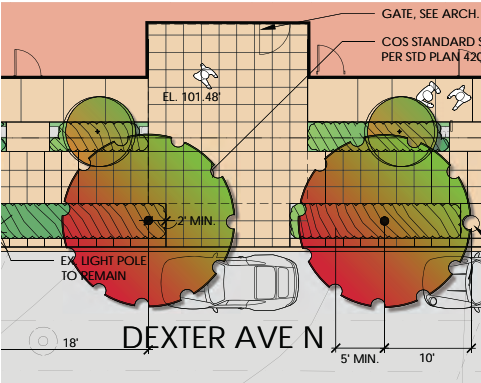
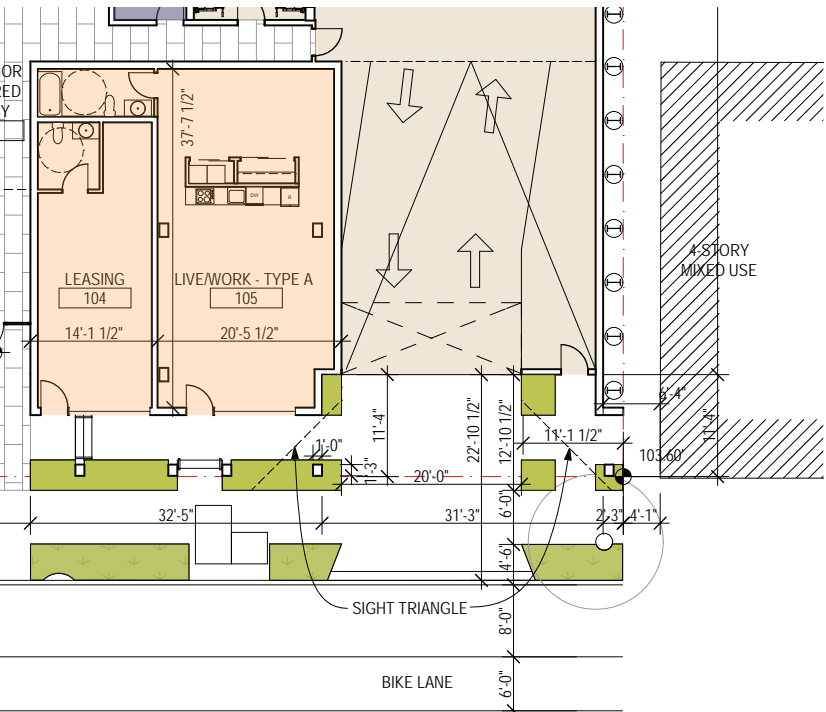


TABLE COMPARISON OF CURRENT DESIGN AND PREVIOUSLY APPROVED MUP

4/30/08 EDG MEETING		ORIGINAL MUP 3008741	MUP REVISIONS	REVISION NOTES	IMPORTANT EDG GUIDELINES/BOARD ISSUES AT RECOMMENDATION MEETING	
ALT 3	building massing steps with site topography	yes	yes	Revised MUP retains approved massing approach and site planning.	B: Overall massing of Alt 3 preferred	
	mid-block break in massing aligned with Hayes street	yes	yes			
	gallery transition between sidewalk and live-work	yes	yes			
	central courtyard	yes	yes			
	central residential entry	yes	yes			
8/6/08 DRB RECOMMENDATION MEETING		ORIGINAL MUP 3008741	MUP REVISIONS	REVISION NOTES	IMPORTANT EDG GUIDELINES/BOARD ISSUES AT RECOMMENDATION MEETING	
DESIGN OPPORTUNITIES						
	Dramatic slope/ views	yes	yes			
	Solar exposure to east - morning daylighting	yes	yes			
	Prevailing winds from south	partial	yes	Plans reconfigured to allow through-unit ventilation at majority of units.		
	Contribute to pedestrian streetscape	yes	yes			
	Ped/Bike/Vehicular traffic on Dexter	yes	yes			
	Terraced, four-story commercial/residential building	yes	yes	Building massing steps with site similar to original MUP.		
	Ground floor Livework facing Dexter Ave N	yes	yes			
DESIGN CHALLENGES						
	Vehicular traffic on Dexter	yes	yes			
	Shading of slope may limit daylight potential	yes	yes	Units reoriented for greater number of east-west facing units.		
DEXTER STREETFRONT						
	Live Work, Landscape	4	4	Revisions maintain approved MUP interface between live/work access and sidewalk. Façade design/materials revised to enhance human scale and individuality of each live-work unit. Area and depth of all live/work units increased. Minimum depth is ±36'. Average depth is ±45'.	A-2 Streetscape Compatibility A-3 Entrances Visible from Street A-4 Human Activity A-7: Residential Open Space B Height, Bulk, Scale C-3: Human Scale D-12: Residential Entries and Transitions E-2 Landscaping to Enhance Building and/or Site	Issue: Livework size/depth important, emphasize work Issue: Bus stop
ALLEY				Trash pickup remains on the alley.	D-12: Service functions on alley	
PARKING (ACCESS FROM DEXTER)		yes	yes	Garage door setback additional 1', shifted 8'-3" south (away from adjacent property)	A-7: Residential Open Space	Issue: driveway functionality, safety important
	Level 1 garage and below grade partial story	50	44	Parking stackers eliminated, below grade parking added		
	Alley	10	0	Parking spaces on alley eliminated		
RESIDENTIAL UNITS		62	62		A-1 Respond to Site Characteristics C-2 Architectural Concept and Consistency D-12: Residential Entries and Transitions	
RESIDENTIAL OPEN SPACE						
	Central courtyard @ Level 2	yes	yes	Central semiprivate courtyard reconfigured as common open air courtyard.	A-7: Residential Open Space	
	Green roof/community deck @ Level 4	yes	yes	Indoor community room added adjacent to roof deck.	D-12: Residential Entries and Transitions E-2 Landscaping to Enhance Building/Site	
DEPARTURES						
	Street access to on site parking	yes	yes			
	Fenced in enclosure for waste/recycle room	yes	no	Trash room size increased and relocated. Departure request eliminated.		
	Sight Triangle	no	yes	Departure request to allow small encroachment on sight triangle.		

ZONING/DEPARTURES

Project Site Zoning Zoning Adjacent to Project Site	1701 Dexter Ave N	NC3-40							
	North	NC3-40/mixed use							
	East	NC3-40/mixed use and LR3/residential							
	South	NC3-40/commercial/office							
	West	C1-65/mixed use							
Permitted Uses	23.47A.004	Residential Live/work	Permitted Permitted	62 Units 4 Units					
Street Level Use	23.47.005.C	No more than 20% street-level residential use @ Dexter Ave N		No Residential Provided COMPLIANT, SEE A2.01					
Street Level Development Standards	23.47A.008.A.2.b	Blank segments of street-facing façade between 2' & 8' may not exceed 20' in width		COMPLIANT, SEE A3.01					
	23.47A.008.A.2.c	Total blank façade segments may not exceed 40% of the width of the street-facing façade		21% Provided COMPLIANT, SEE A3.01					
	23.47A.008.B.2.a	60% of the street-facing façade between 2' & 8" above the sidewalk shall be transparent		66% Provided COMPLIANT, SEE A3.01					
	23.47A.008.B.3	Nonres. uses shall be 30' and min. depth of 15' from the street-level, street-facing façade.		36'-3" Min Provided COMPLIANT, SEE A2.01					
	23.47.008.B3.b	Nonresidential uses at street level shall have a floor-to-floor height of at least 13'.		13' Min Provided COMPLIANT, SEE A3.01					
Structure Height	23.47A.012	Allowable structure height = 40'							
	23.47A.012.A.1.a	44' structure height allowed if 13' or more floor-to-floor height provided @ street level nonres. uses		42'-11 3/4" Max Provided COMPLIANT, SEE A3.02 and A3.04					
	DR 4-2012	Height measurement based on "Option for calculating average grade level to measure height" per SMC 23.86.006.A.2.		SEE G0.05					
	DR12-2005	Height measurement based on interpolated Historic Grade		SEE G0.05					
FAR	23.47A.013, Table A 23.47A.013.D	FAR = 3.25 x 16,231 = 52,750 S.F. Max. Gross floor area below grade is not counted towards FAR.		2.86 FAR Provided COMPLIANT, SEE G0.03					
Setback Reqs.	23.47A.014	No setbacks required		N/A					
Landscape and Screening Standards	23.47A.016.A.2 23.47.A.016.B	With more than 4 units, landscaping must achieve a Green Factor score of .30 or greater Street trees are required.		COMPLIANT, SEE L1.05					
Amenity Area	23.47A.024.A 23.47A.024.B1 23.47A.024.B.4 23.47A.024.B.5	5% of total gross residential floor area, excluding area used for mechanical equipment All residents shall have access to at least one common or private amenity area Common amenity area shall be 250 s.f. min and no horiz. dimension shall be less than 10'. Private balconies/decks shall be 60 s.f. min and no horiz. dimension shall be less than 6'.		3671 SF Provided COMPLIANT, SEE G0.04					
Parking Location & Access	23.47A.032.A.1	Access to parking shall be from the alley if the lot abuts an alley improved to standards of Section 23.53.030.C		DEPARTURE REQUESTED SEE DEPARTURE LIST/G0.01					
Required Parking	23.54.015, Table B	Residential	1 space/dwelling unit: 62 x 1 = 62 spaces required	62					
	23.54.015, Table B	Live/work units	Units less than 1,500 s.f.: 0 spaces required per unit	0					
	23.54.020.F.2a	Transit Reduction	50% reduction permitted if project within 1,320 ft. of frequent transit service	31 44 Spaces Provided COMPLIANT, SEE A2.00/A2.01					
Parking Space Standards	23.54.030.B.1.b.		60% of parking spaces shall be striped for Medium vehicles.				63.6% Medium Stalls Provided	COMPLIANT, SEE A2.00/A2.01	
			Small	Medium	Barrier Free	Total			
			Park Level	11	18	29			
			Level 1	2	10	3			15
			Total	13	28	3			44
	Percent	29.5%	63.6%	6.8%					
	23.54.030.D.2.a.2 23.54.030.D.3 23.54.030.G.2	Nonresidential uses	Two-way traffic driveways shall be 22' min. & 25' max. width. Max. driveway slope is 15%. For two-way driveways 22' wide or more, a sight triangle on the exit side of the driveway shall be provided. Entrance and exit lanes shall be clearly identified.				No non-res parking provided 5% Slope Max Provided Provided	COMPLIANT, SEE A2.01 COMPLIANT, SEE A2.01	
Bike Parking	23.54.015, Table E	Residential Live/work	1 stall/4 dwelling units Live/work is not in Table E: no min. requirement	62 dwelling units/4= 16 Spaces Required	18 Spaces Provided	COMPLIANT, SEE A2.01			
Solid Waste	23.54.040.B	Mixed use development that contains both residential and nonresidential uses shall meet the storage space requirements shown in Table A for 23.54.040 for residential development plus 50 percent of the requirement for nonresidential development. Storage space for garbage may be shared between residential and nonresidential uses, but separate spaces for recycling shall be provided. For 9 dwelling units or more, the min. horiz. dimension of required storage space is 12'.				402 SF Provided Alley access provided SEE SPU APPROVAL LETTER & A2.03 COMPLIANT COMPLIANT, SEE A2.03			
	23.54.040.D								
	From Table 23.54.040 Table A	Required (51-100 dwelling units)	375 s.f. + 48 (4 s.f./ qty units above 50) = 423 s.f.						
		Required (0-5000 SF commercial)	82 s.f. x 50% = 41 s.f.						
	23.54.040.F	Total required storage space	464 s.f.						
		Direct access from alley or street required for containers larger than 2 cubic yards							
SENSITIVE AREAS	Potential Landslide Area	SEPA Checklist			NO CHANGE FROM ORIGINAL MUP				
		Geotechnical Report			PROVIDED WITH ORIGINAL MUP				
		Extended Survey			UPDATED SURVEY PROVIDED				



DEPARTURE REQUEST 1: GARAGE ACCESS FROM DEXTER (SMC 23.47A.032)

REQUEST: Allow parking garage entry from Dexter Avenue N instead of alley.

REASON: The site slope makes entry from the alley a true hardship. A curb cut currently exists on Dexter. Our proposal would move it from the center of the site (facing Hayes Street) to the north.

From approved MUP #3008741:From the “In responding to this issue, the [original] applicant relied on their traffic engineer’s estimation (Transpo Report, July 28, 2008) that a single-lane in-and-out driveway would pose serious safety issues and was not a viable solution...[T]he Board was agreed that taking access for all the parking from the alley was impracticable given the steep topography between street and alley (see Guideline A-1) and given the size, scope and innovative (using modular, pre-fabricated units) nature of the project.

The proposed MUP Revisions do not change the parking access or size, scope and proposed construction methods (modular pre-fabricated units) from the original MUP. The proposed revisions shift the driveway 8'-3” to the south, which helps create a safe landscape buffer for pedestrians on both sides of the driveway. The garage door has also been setback an additional 1’ from the original MUP.

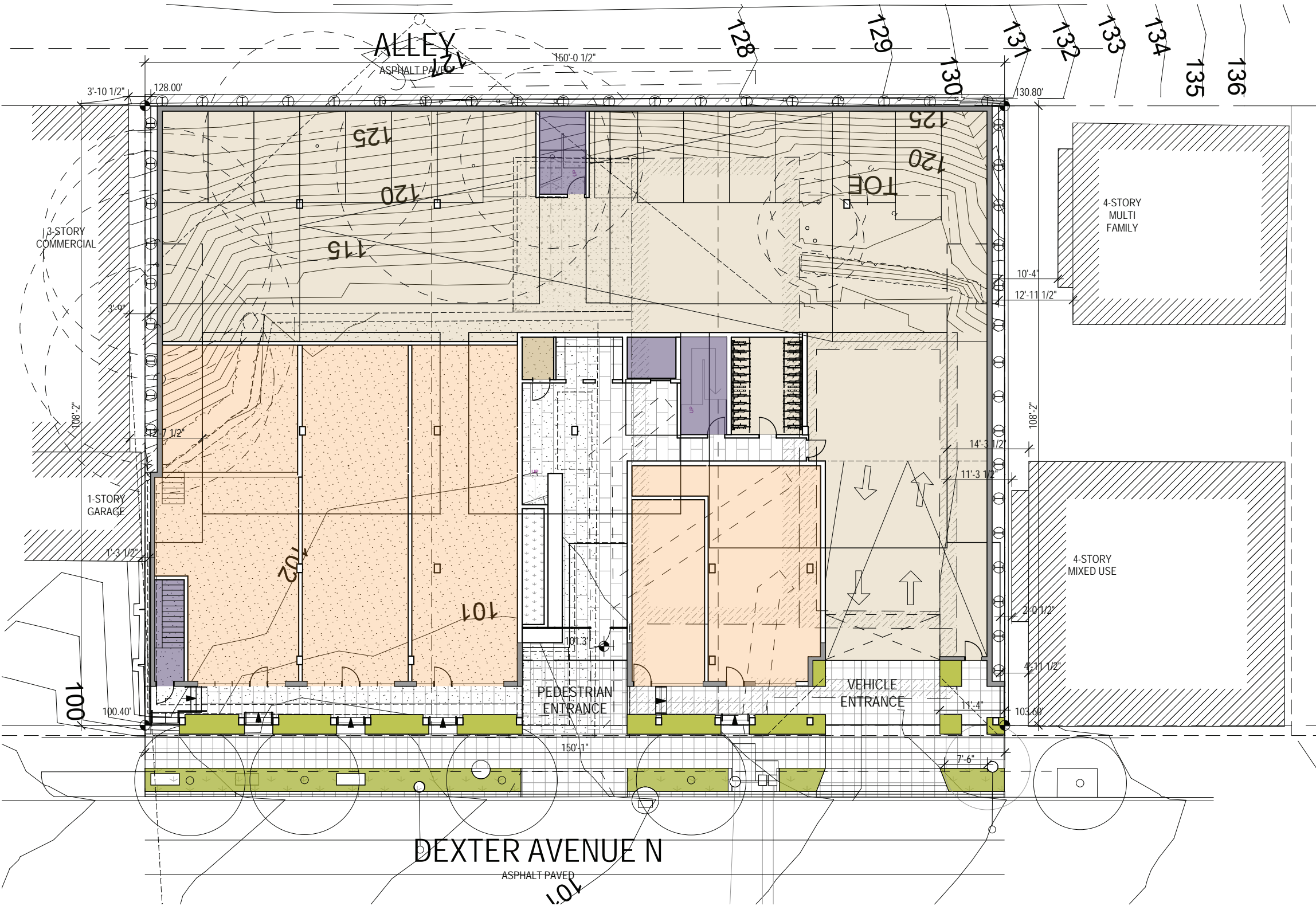
DEPARTURE REQUEST 2: SIGHT TRIANGLE (SMC 23.54.030G.2: a sight triangle on the side of the driveway used as an exit shall be provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection of the driveway with sidewalk. The entrance and exit lanes shall be clearly identified).

REQUEST: Allow concrete column to obstruct 1.25 SF of sight triangle on the right turn side of driveway.

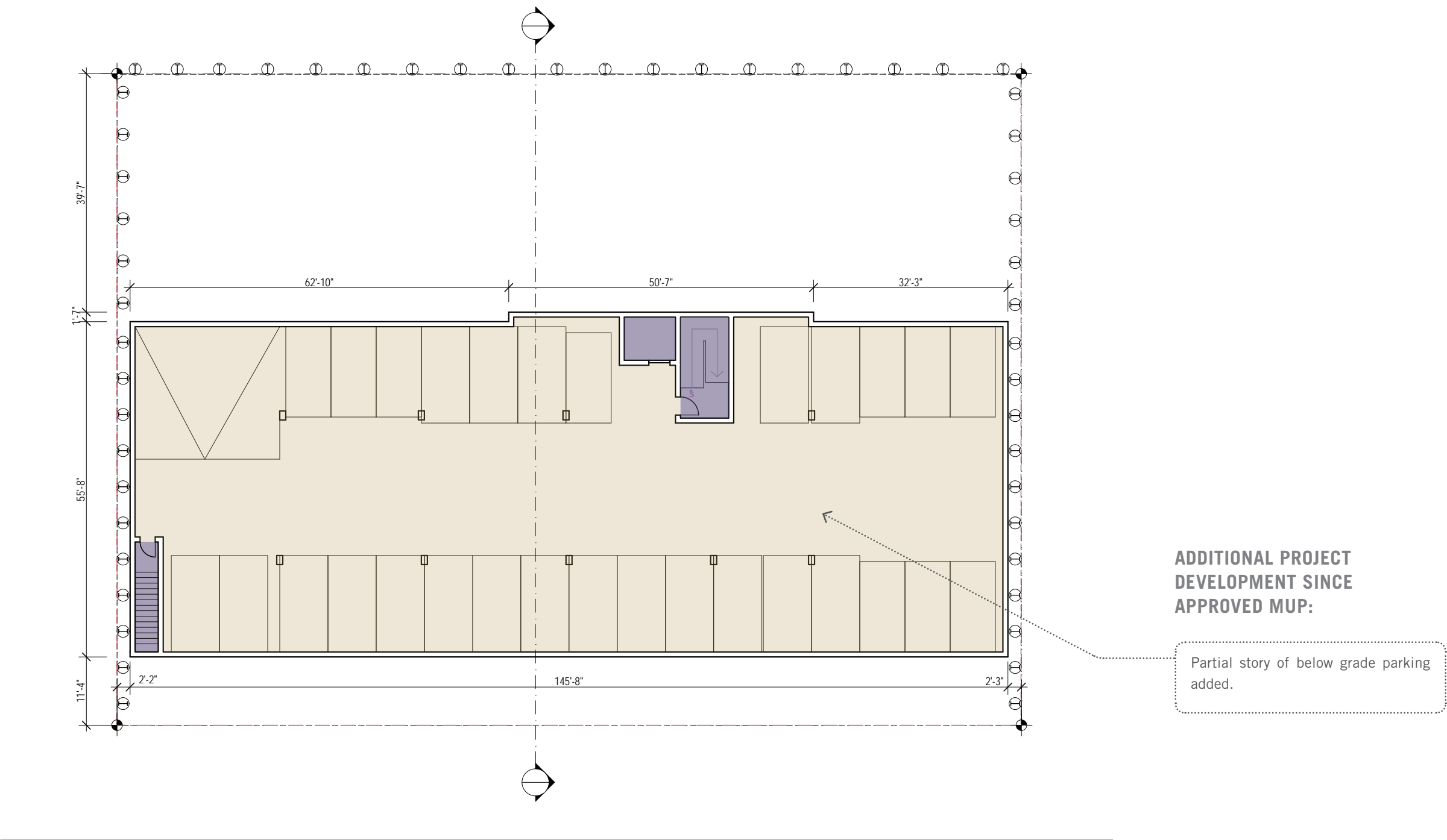
REASON: The column is part of the concrete frames that architecturally define the live/work at street level. The frames contribute to the project structure and concept and help integrate the garage entry into the overall building design. In conjunction with the shifted driveway location, and given that the amount of obstruction is minimal and there is good visual access on both sides of the column, the applicant feels the proposed design allows safety for pedestrians while also helping to create well-defined live/work units at the street and a unified structure that integrates lower and upper levels of the project.

KEY

- LIVE/WORK UNITS
- DWELLING UNITS
- PARKING
- STORAGE/MECH/UTILITY
- VERTICAL CIRCULATION
- HORIZONTAL CIRCULATION (INTERIOR)



CURRENT PROPOSAL



- KEY**
- LIVE/WORK UNITS
 - DWELLING UNITS
 - PARKING
 - STORAGE/MECH/UTILITY
 - VERTICAL CIRCULATION
 - HORIZONTAL CIRCULATION (INTERIOR)

PROJECT BY UNICO WITH
MITHUN AND HYBRID
ARCHITECTURE.

This architectural floor plan illustrates the ground floor of a building, featuring a variety of functional spaces and parking areas. The plan is oriented with a north arrow pointing towards the top-left corner.

Rooms and Spaces:

- Live/Work Areas:** Four live/work units are labeled: LIVE/WORK 101, LIVE/WORK 102, LIVE/WORK 103, and LIVE/WORK 104. These units are situated along the bottom and right sides of the plan.
- Common Areas:** A central LOBBY (268 SF) is located near the center of the plan. Adjacent to it is a SECURITY GATE and a SPRINKLER ROOM (XXX).
- Service Areas:** A designated area for BICYCLES (XXX) is located near the center-right. A 4'x6' ELEC. SERVICE VAULT BELOW is indicated near the bottom right corner.
- Parking and Storage:** The plan includes several parking spaces, including a designated space for approved car-sharing programs (PER SMC 23.54.020.J). There are also areas for preferred parking and a designated area for approved car-sharing programs.
- Structural and Mechanical:** A MECHANICAL STACK PARKER SYSTEM FOR INDEPENDENT MOVING OF AUTOMOBILES, TYP. SEE PRODUCT INFORMATION THIS SHEET is located in the upper left. A GARAGE EXHAUST ABOVE is indicated near the center.
- Other Features:** A BUS STOP BENCH is located near the bottom left. A SIGHT TRIANGLE (PER SMC 23.54.030.G2) is indicated near the bottom right corner. A SOLDIER PILE RETAINING WALL ON PROP. LINE is shown on the right side.

Dimensions and Elevation:

- The plan includes numerous dimensions for room sizes, setbacks, and overall building footprint. For example, the overall width is 150'-0" and the overall depth is 17'-0".
- Elevation markers are provided throughout the plan, such as EL. 95.25', EL. 101.50', EL. 101.48', EL. 102.75', and EL. 103.20'.

Notes and References:

- DESIGNATED PARKING SPACE FOR APPROVED CAR-SHARING PROGRAM PER SMC 23.54.020.J
- MECHANICAL STACK PARKER SYSTEM FOR INDEPENDENT MOVING OF AUTOMOBILES, TYP. SEE PRODUCT INFORMATION THIS SHEET
- GARAGE EXHAUST ABOVE
- PARKING GARAGE (XXX)
- BOLLARDS
- SOLDIER PILE RETAINING WALL ON PROP. LINE
- OVERHEAD COILING GRILLE GARAGE DOOR
- RAMP DN 10.5% W/ TRANSITIONS
- RAMP UP 1:20
- ELEV. CTRL. CLOSET MAIL EL. 101.50'
- SECURITY GATE
- SPRINKLER ROOM (XXX)
- ACCESS
- 4'x6' ELEC. SERVICE VAULT BELOW
- SIGHT TRIANGLE PER SMC 23.54.030.G2

- LIVE/WORK UNITS
- DWELLING UNITS
- PARKING
- STORAGE/MECH/UTILITY
- VERTICAL CIRCULATION
- HORIZONTAL CIRCULATION (INTERIOR)

DESIGN ELEMENTS CONTINUED FROM PREVIOUS EDG:

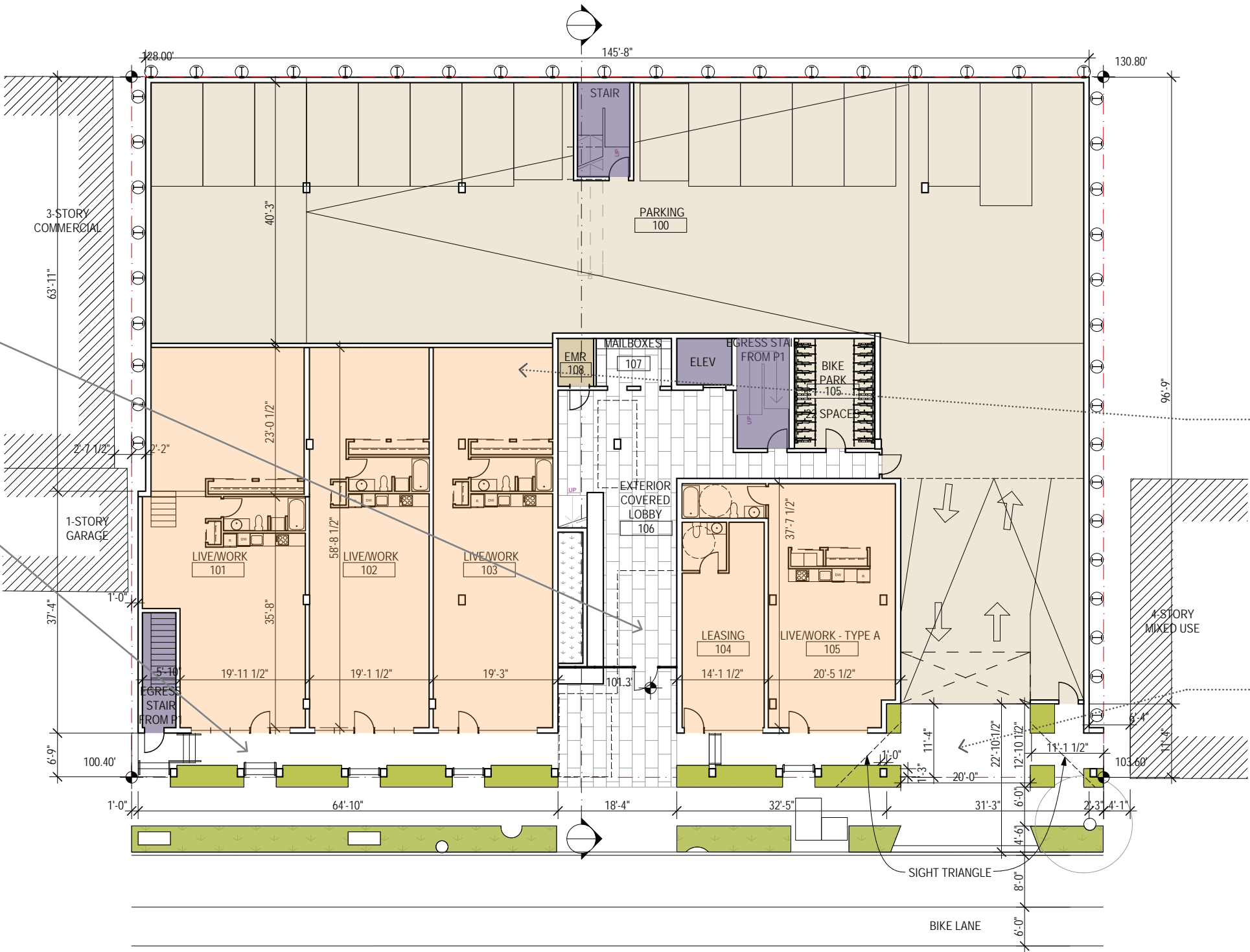
The open air residential entry occurs at the mid-block break aligned with Hayes Street. Residential entry and exterior vertical circulation lead to an open air courtyard on Level 2.

Live/work units set back 6'-9" from the property line. Access gallery provides an accessible transition between the sidewalk and live/work units.

Parking garage entry at the north side of the site.

KEY

- LIVE/WORK UNITS
- DWELLING UNITS
- PARKING
- STORAGE/MECH/UTILITY
- VERTICAL CIRCULATION
- HORIZONTAL CIRCULATION (INTERIOR)



ADDITIONAL PROJECT DEVELOPMENT SINCE APPROVED MUP:

Live/work units made deeper.

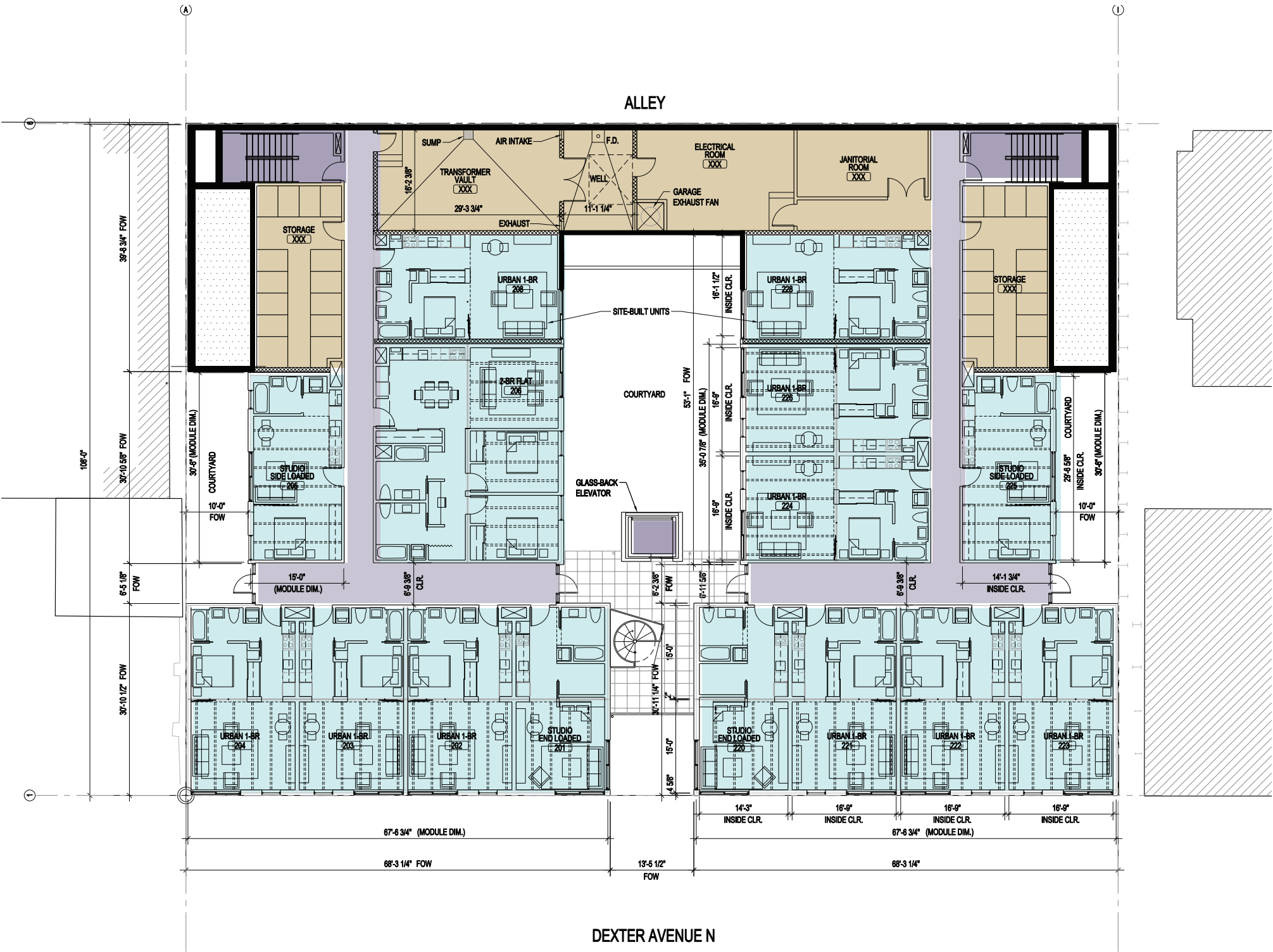
Project no longer includes bus stop. Bus stop has been relocated two blocks south of the site.

Driveway shifted 8'-3" to the south for required separation from light pole that was installed after the original MUP was approved. The driveway in the approved MUP was 3' off the north property line and the driveway in the current proposal is 11'-4" off the property line. The garage door has also been set back an additional 1'.

PREVIOUSLY APPROVED
MUP #3008741
ISSUED MARCH 20, 2009

PROJECT BY UNICO WITH
MITHUN AND HYBRID
ARCHITECTURE.

SECOND FLOOR PLAN



KEY

- LIVE/WORK UNITS
- DWELLING UNITS
- PARKING
- STORAGE/MECH/UTILITY
- VERTICAL CIRCULATION
- HORIZONTAL CIRCULATION (INTERIOR)

DESIGN ELEMENTS CONTINUED FROM PREVIOUS EDG:

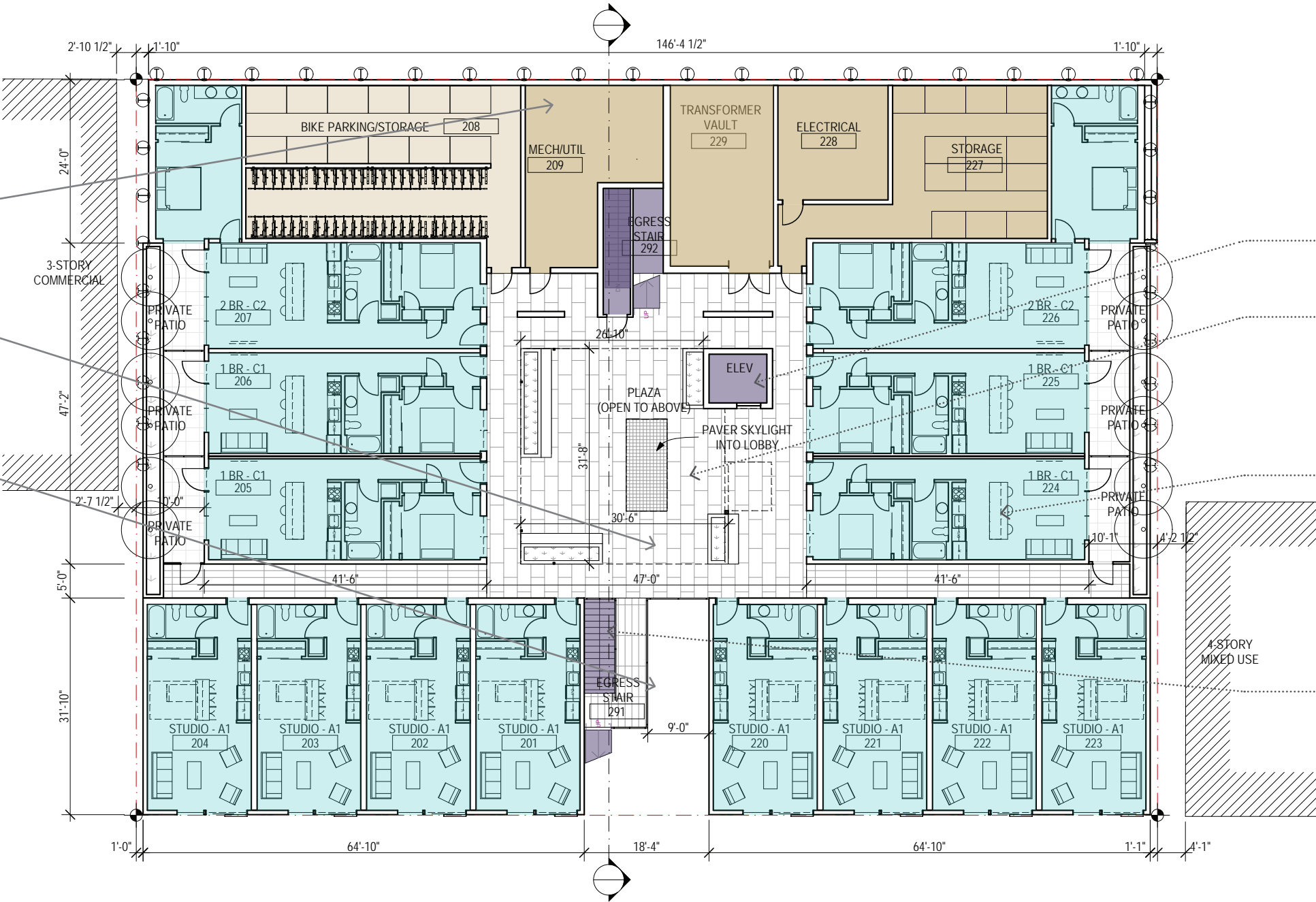
- Mechanical and utility areas located below grade on alley.
- Open air courtyard Level 2 is the heart of the project.
- Mid-block break aligned with Hayes Street.

ADDITIONAL PROJECT DEVELOPMENT SINCE APPROVED MUP:

- Elevator relocated.
- L2 courtyard reconceived as a common amenity plaza.
- Units reorganized—each set of units has single loaded access and faces L2 courtyard and one property line.
- Horizontal and vertical circulation reconfigured, made largely exterior.

KEY

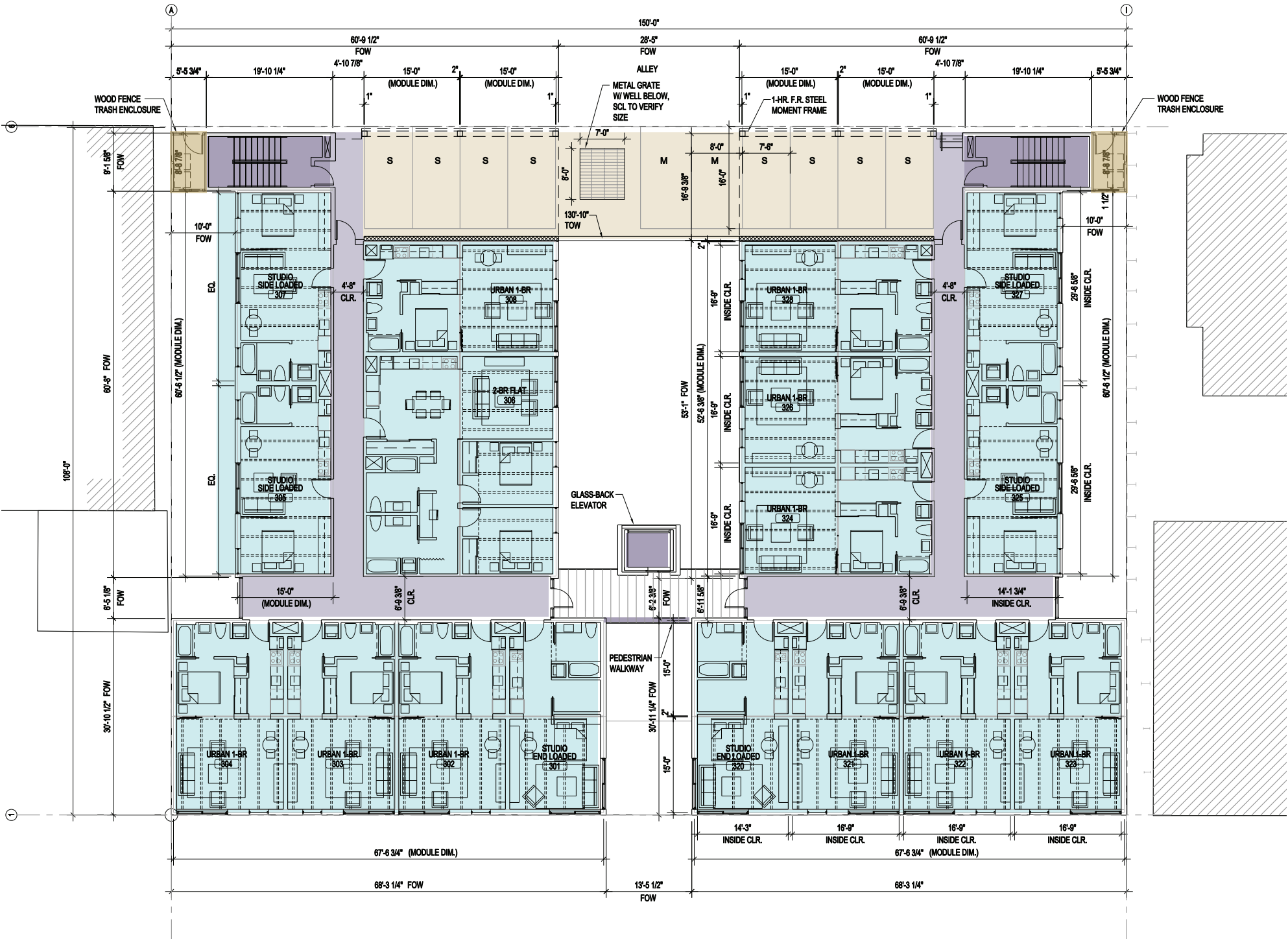
- LIVE/WORK UNITS
- DWELLING UNITS
- PARKING
- STORAGE/MECH/UTILITY
- VERTICAL CIRCULATION
- HORIZONTAL CIRCULATION (INTERIOR)



PREVIOUSLY APPROVED
MUP #3008741
ISSUED MARCH 20, 2009

PROJECT BY UNICO WITH
MITHUN AND HYBRID
ARCHITECTURE.

THIRD FLOOR PLAN



- KEY**
- LIVE/WORK UNITS
 - DWELLING UNITS
 - PARKING
 - STORAGE/MECH/UTILITY
 - VERTICAL CIRCULATION
 - HORIZONTAL CIRCULATION (INTERIOR)

DESIGN ELEMENTS CONTINUED FROM PREVIOUS EDG:

Open air courtyard is the heart of the project.

Mid-block break aligned with Hayes Street extends through all levels.

KEY

- LIVE/WORK UNITS
- DWELLING UNITS
- PARKING
- STORAGE/MECH/UTILITY
- VERTICAL CIRCULATION
- HORIZONTAL CIRCULATION (INTERIOR)



ADDITIONAL PROJECT DEVELOPMENT SINCE APPROVED MUP:

Trash room relocated, made larger.

Alley parking removed.

West portion of project reorganized to face east/Lake Union.

Elevator relocated.

Courtyard circulation structure creates "vertical courtyard" with southern solar access.

Units reorganized on all levels.

Horizontal and vertical circulation reconfigured, made largely exterior.

PREVIOUSLY APPROVED
MUP #3008741
ISSUED MARCH 20, 2009

PROJECT BY UNICO WITH
MITHUN AND HYBRID
ARCHITECTURE.

FOURTH FLOOR PLAN



- KEY**
- LIVE/WORK UNITS
 - DWELLING UNITS
 - PARKING
 - STORAGE/MECH/UTILITY
 - VERTICAL CIRCULATION
 - HORIZONTAL CIRCULATION (INTERIOR)

DESIGN ELEMENTS CONTINUED FROM PREVIOUS EDG:

Open air courtyard is the heart of the project.

Mid-block break aligned with Hayes Street extends through all levels.

KEY

- LIVE/WORK UNITS
- DWELLING UNITS
- PARKING
- STORAGE/MECH/UTILITY
- VERTICAL CIRCULATION
- HORIZONTAL CIRCULATION (INTERIOR)



ADDITIONAL PROJECT DEVELOPMENT SINCE APPROVED MUP:

West portion of project reorganized to face east/Lake Union.

Elevator relocated.

Courtyard circulation structure creates "vertical courtyard" with southern solar access.

Units reorganized on all levels.

Horizontal and vertical circulation reconfigured, made largely exterior.

PROJECT BY UNICO WITH
MITHUN AND HYBRID
ARCHITECTURE.

This architectural floor plan illustrates a building layout with two symmetrical wings. The plan includes the following details:

- Dimensions:**
 - Overall width: 108'-0"
 - Overall depth: 150'-0"
 - Wing width: 60'-9" FOW
 - Wing depth: 68'-2 1/4" FOW
 - Unit widths: 19'-10 1/4", 15'-0", 2'-0", 15'-0"
 - Unit depths: 8'-5 3/4", 8'-5 7/8", 10'-0", 1'-1/2"
 - Common area widths: 60'-6 1/2" (MODULE DIM), 68'-10 3/4" (MODULE DIM), 68'-2 1/4" FOW
 - Common area depths: 70'-8 3/4" FOW, 68'-10 3/4" (MODULE DIM), 16'-9", 16'-9", 16'-9", 31'-1" FOW
 - Staircase widths: 8'-1 1/4" FOW
- Unit Types and Layouts:**
 - Left Wing:**
 - 1-BR LOFT (507)
 - 2-BR 2-LEVEL (509)
 - 2-BR 2-LEVEL (508)
 - 3-BR FLAT (506)
 - STUDIO SIDE-LOADED (505)
 - Right Wing:**
 - 1-BR LOFT (527)
 - 2-BR 2-LEVEL (529)
 - 2-BR 2-LEVEL (528)
 - URBAN 1-BR (526)
 - URBAN 1-BR (524)
 - STUDIO SIDE-LOADED (525)
- Common Areas and Amenities:**
 - MTL AWNING ABOVE COMMUNITY ROOF DECK AMENITY, 722 SF
 - AREA OF GREEN ROOF
 - GLASS-BACK ELEVATOR
 - PEDESTRIAN WALKWAY
- Other Features:**
 - Staircases and elevators are centrally located within each wing.
 - Common areas are situated between the wings and along the outer edges.
 - The plan includes various furniture and fixture layouts for units and common spaces.

- LIVE/WORK UNITS
- DWELLING UNITS
- PARKING
- STORAGE/MECH/UTILITY
- VERTICAL CIRCULATION
- HORIZONTAL CIRCULATION (INTERIOR)

DESIGN ELEMENTS CONTINUED FROM PREVIOUS EDG:

- Open air courtyard is the heart of the project.
- Mid-block break aligned with Hayes Street extends through all levels.

ADDITIONAL PROJECT DEVELOPMENT SINCE APPROVED MUP:

- West portion of project reorganized to face east/Lake Union.
- Elevator relocated.
- Courtyard circulation structure creates "vertical courtyard" with southern solar access.
- Units reorganized to face east/Lake Union, private terraces added.
- Community room added.
- Green roof added at L5.
- Horizontal and vertical circulation reconfigured, made largely exterior.

KEY

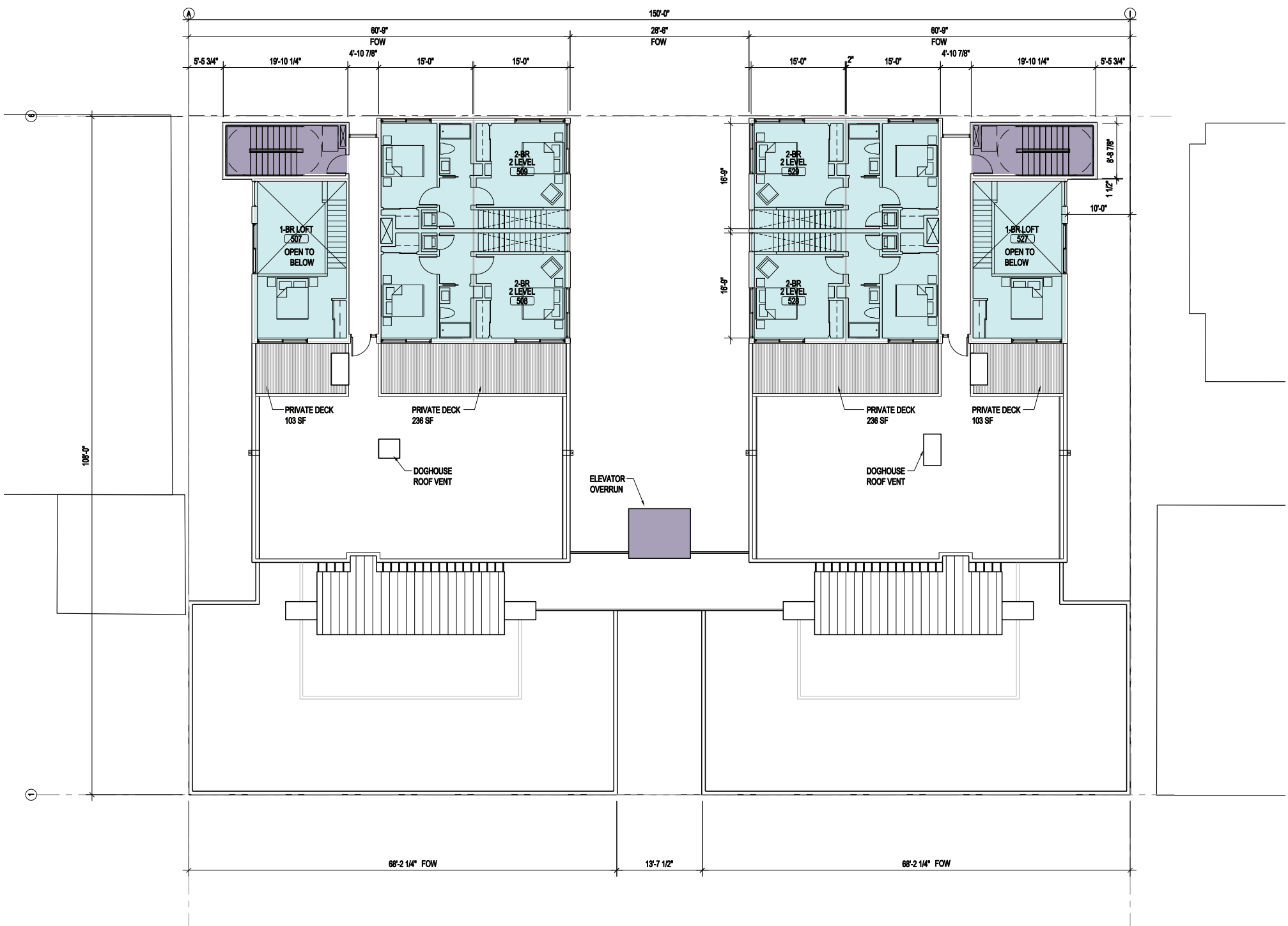
- LIVE/WORK UNITS
- DWELLING UNITS
- PARKING
- STORAGE/MECH/UTILITY
- VERTICAL CIRCULATION
- HORIZONTAL CIRCULATION (INTERIOR)



PREVIOUSLY APPROVED
MUP #3008741
ISSUED MARCH 20, 2009

PROJECT BY UNICO WITH
MITHUN AND HYBRID
ARCHITECTURE.

SIXTH FLOOR PLAN



KEY

- LIVE/WORK UNITS
- DWELLING UNITS
- PARKING
- STORAGE/MECH/UTILITY
- VERTICAL CIRCULATION
- HORIZONTAL CIRCULATION (INTERIOR)

DESIGN ELEMENTS CONTINUED FROM PREVIOUS EDG:

Mid-block break aligned with Hayes Street extends through all levels.

Open air courtyard is the heart of the project.

ADDITIONAL PROJECT DEVELOPMENT SINCE APPROVED MUP:

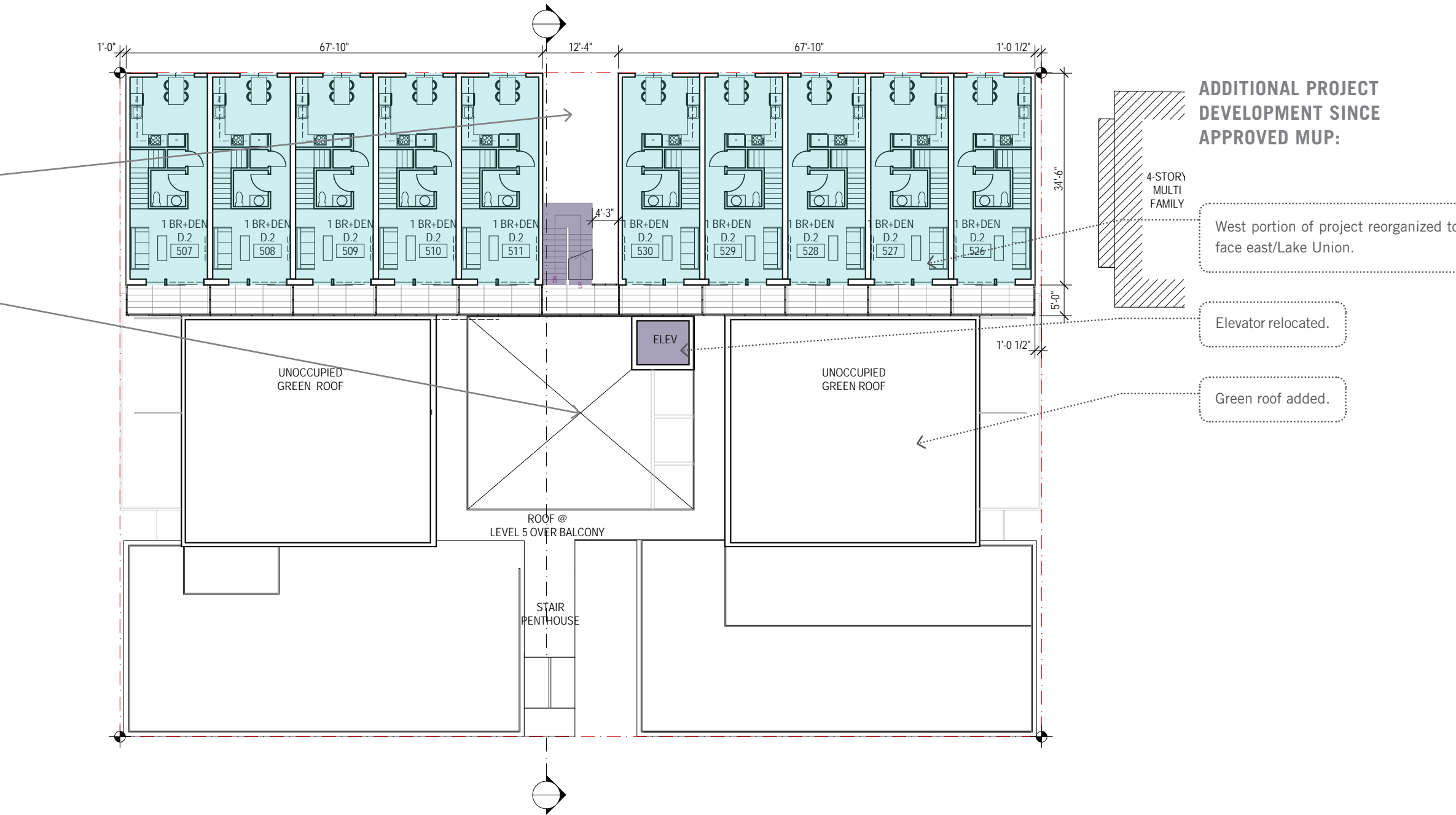
West portion of project reorganized to face east/Lake Union.

Elevator relocated.

Green roof added.

KEY

- LIVE/WORK UNITS
- DWELLING UNITS
- PARKING
- STORAGE/MECH/UTILITY
- VERTICAL CIRCULATION
- HORIZONTAL CIRCULATION (INTERIOR)



PREVIOUSLY APPROVED
MUP #3008741
ISSUED MARCH 20, 2009

EAST ELEVATION

PROJECT BY UNICO WITH
MITHUN AND HYBRID
ARCHITECTURE.



DESIGN ELEMENTS FROM PREVIOUS EDG:

Terraced building massing steps with the site topography.

The open air residential entry occurs at the mid-block break aligned with Hayes Street. Residential entry and exterior vertical circulation lead to an open air courtyard on Level 2.

Live/work units set back 6'-9" from the property line. Access gallery provides an accessible transition between the sidewalk and live/work units.

Parking garage entry at the north side of the site.



See Page 37 for Material Key

ADDITIONAL PROJECT DEVELOPMENT SINCE APPROVED MUP:

Interface between live/work units and sidewalk further developed to include architecturally and landscape defined stoop for each live/work.

Size of mid-block break increased. Entry developed to create well-marked residential entry, includes exterior vertical circulation, decorative metal entry gate and canopy, wood bench and exterior lighting.

Driveway shifted 8'-3" to the south for required separation from light pole that was installed after the original MUP was approved.

DESIGN ELEMENTS FROM
PREVIOUS EDG:

Continuous mid-block break aligned with Hayes Street provides continuous opening through the site from Dexter to the alley.

ADDITIONAL PROJECT
DEVELOPMENT SINCE
APPROVED MUP:

West portion of project reorganized to face east/Lake Union. Alley parking replaced with dwelling units.

Landscape to soften edge of building

Trash room relocated, made larger

Horizontal and vertical circulation reconfigured, made largely exterior.

Alley parking removed.



DESIGN ELEMENTS FROM
PREVIOUS EDG:

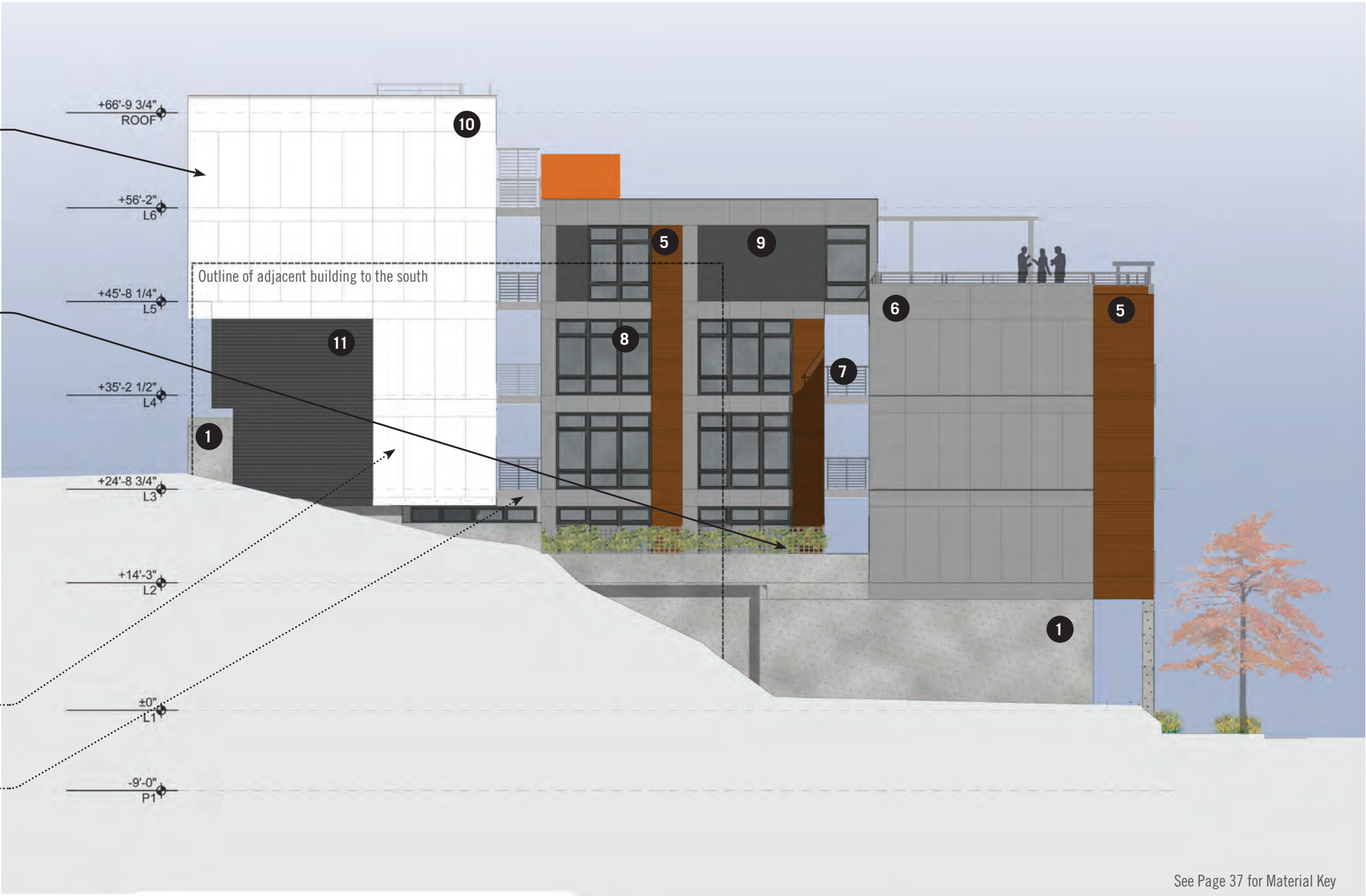
Terraced building massing steps with the site topography.

Landscape to soften edge of building.

ADDITIONAL PROJECT
DEVELOPMENT SINCE
APPROVED MUP:

West portion of project reorganized to face east/Lake Union.

Building massing divided into 6 separate masses connected by exterior circulation elements. Breaks between the buildings provide continuous views through the site in the north-south direction.





DESIGN ELEMENTS FROM PREVIOUS EDG:

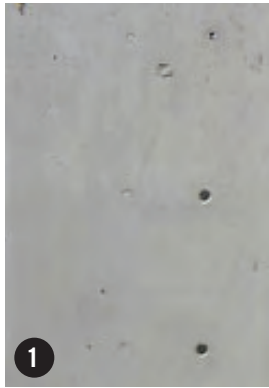
Terraced building massing steps with the site topography.

West portion of project reorganized to face east/Lake Union.

ADDITIONAL PROJECT DEVELOPMENT SINCE APPROVED MUP:

Landscape to soften edge of building.

Building massing divided into 6 separate masses connected by exterior circulation elements. Breaks between the buildings provide continuous views through the site in the north-south direction.



1
ARCH CONCRETE



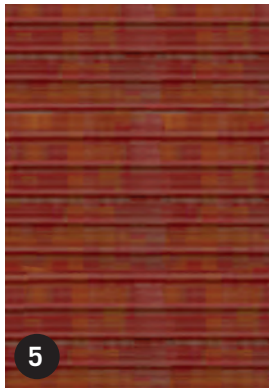
2
WOOD PANEL



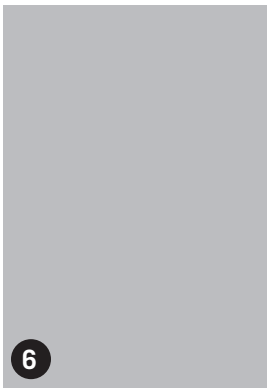
3
CLEAR
ANODIZED
ALUMINUM
STOREFRONT



4
PERFORATED DECORATIVE
ENTRANCE SCREEN



5
CORTEN STEEL



6
FIBER CEMENT SIDING



7
GALVANIZED METAL MESH RAIL



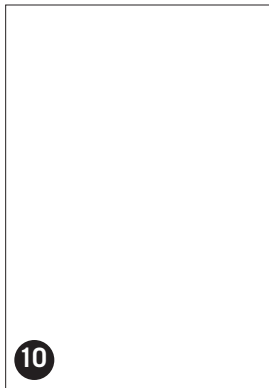
STEEL STAIR STRUCTURE



8
CHARCOAL
GRAY VINYL OR
FIBERGLASS
WINDOWS



9
FIBER CEMENT PANEL SIDING



10
FIBER CEMENT PANEL SIDING



11
FIBER CEMENT LAP SIDING



12
FIBER CEMENT ACCENT COLOR

3D VIEWS



3D VIEW LOOKING SOUTHWEST ALONG DEXTER



3D MASSING STUDY OF ALLEY LOOKING NORTH

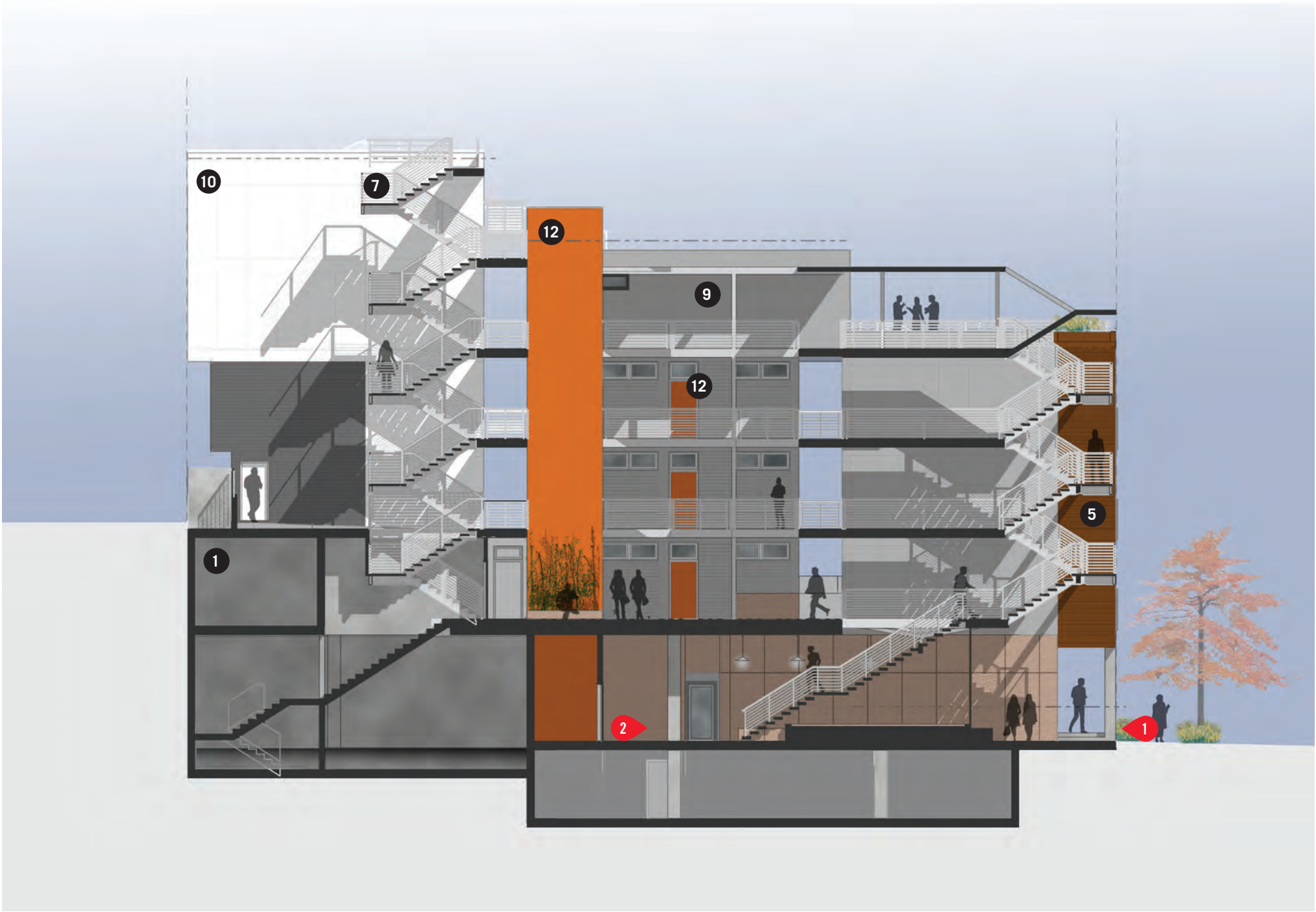


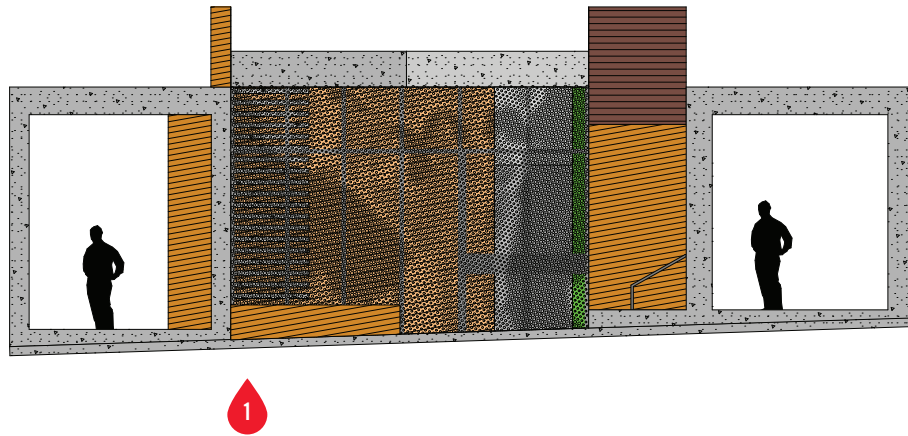
3D MASSING STUDY OF ALLEY LOOKING SOUTH



3D VIEW LOOKING NORTHEAST ALONG DEXTER

EAST-WEST SECTION THROUGH RESIDENTIAL ENTRY AT MID-BLOCK BREAK ALIGNED WITH HAYES STREET

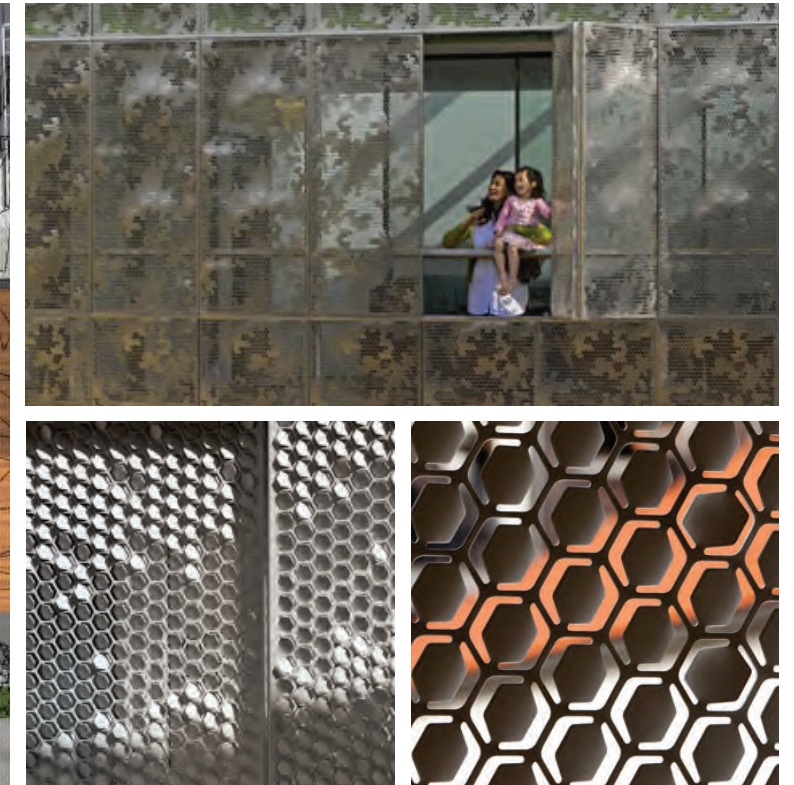




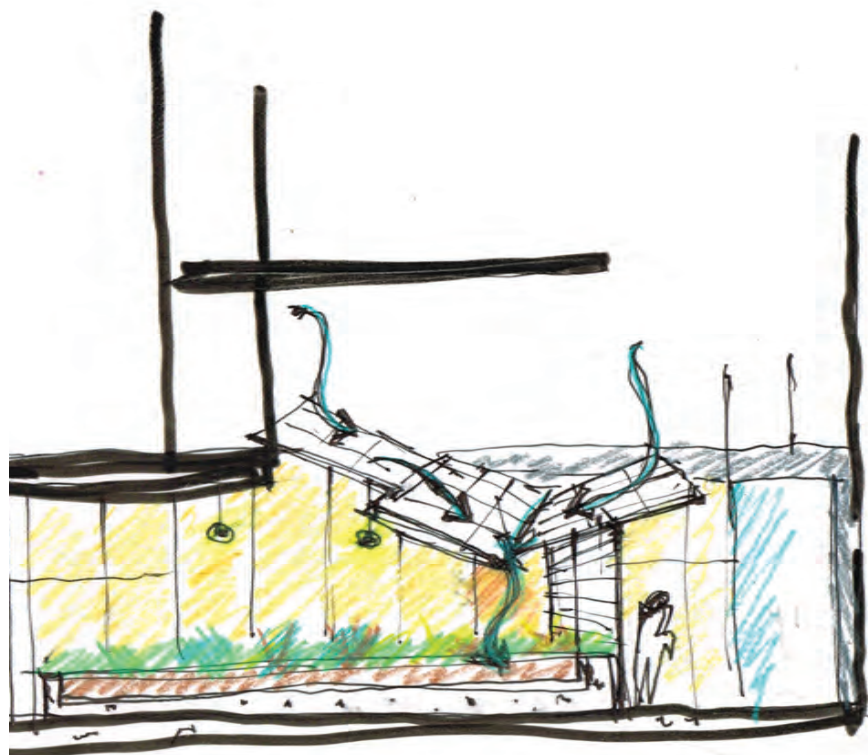
SKETCH STUDY FOR PERFORATED SCREEN AT RESIDENTIAL ENTRY



3D SKETCH OF OPEN AIR RESIDENTIAL ENTRY AT MID-BLOCK BREAK ALIGNED WITH HAYES STREET



IDEA IMAGES FOR PERFORATED DECORATIVE ENTRANCE SCREEN



SKETCH STUDY FOR STEEL AND GLASS CANOPY AT RESIDENTIAL ENTRY



3D SKETCH OF OPEN AIR RESIDENTIAL ENTRY SHOWING CANTILEVERED CONCRETE STAIR AT L1



IDEA IMAGES FOR RESIDENTIAL ENTRY: CANTILEVERED CONCRETE STAIR, LINEAR WOOD BENCH AT ENTRY AND SS CABLE RAIL

LIGHTING PLAN

The lighting design will illuminate areas in and around the building, creating safe, inviting spaces. Fixtures will be selected based on their appropriateness for each space and their energy efficiency.



1 Downlight sconce



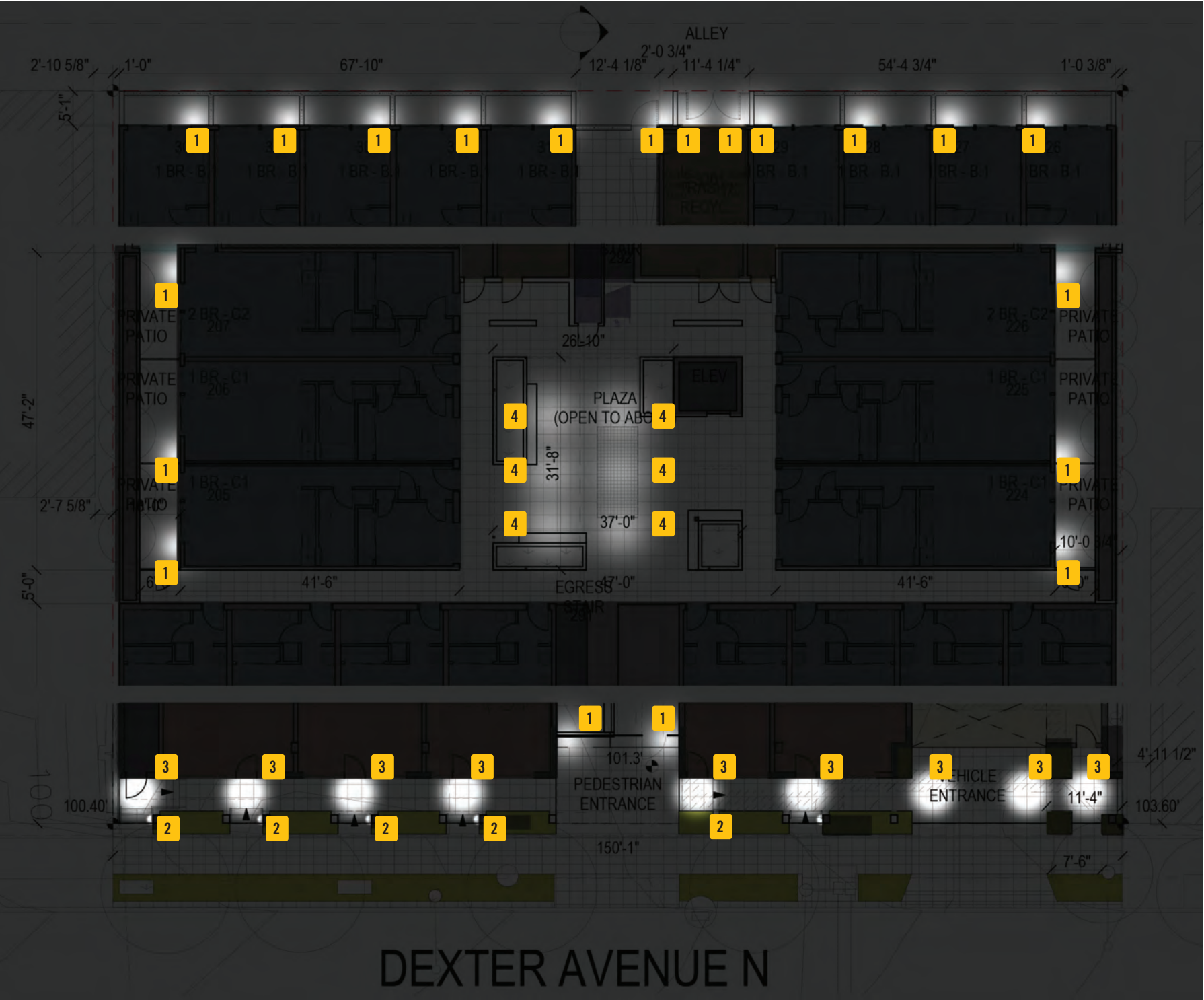
2 Step lighting



3 Recessed can lighting



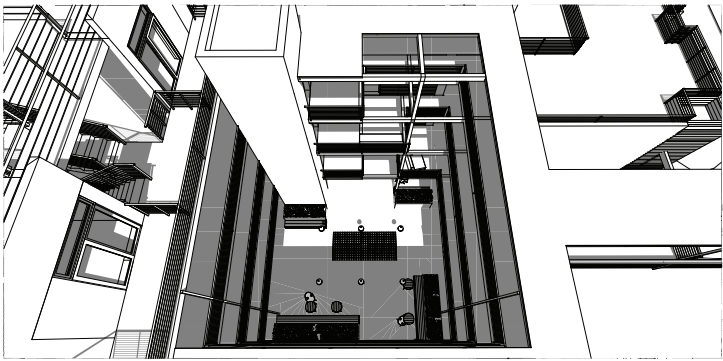
4 Hanging courtyard pendants



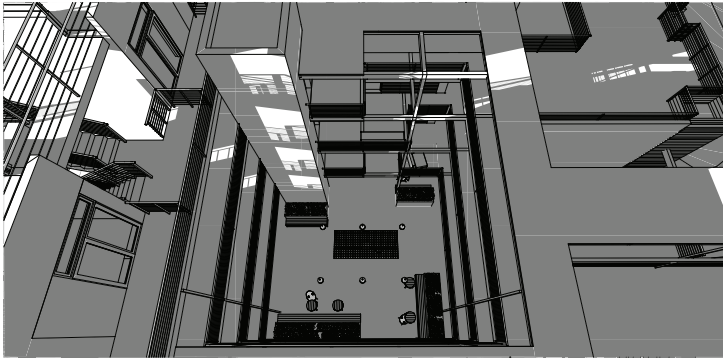
SUMMER



Summer solstice 8am

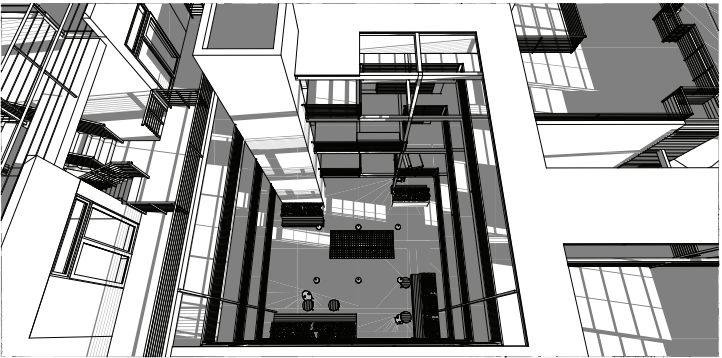


Summer solstice noon

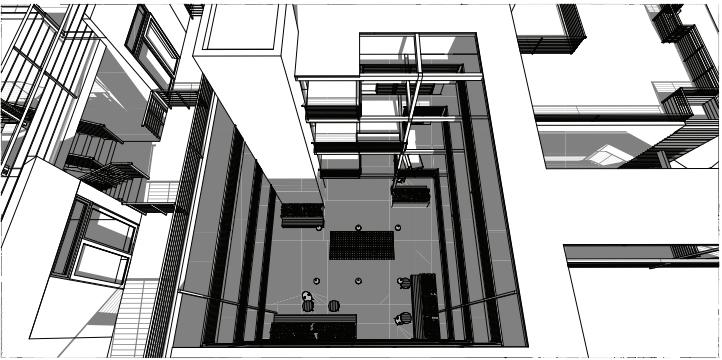


Summer solstice 6pm

SPRING/FALL



Spring/fall 8am

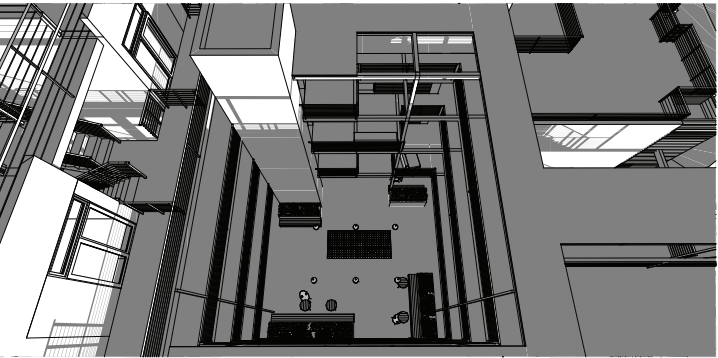


Spring/fall noon

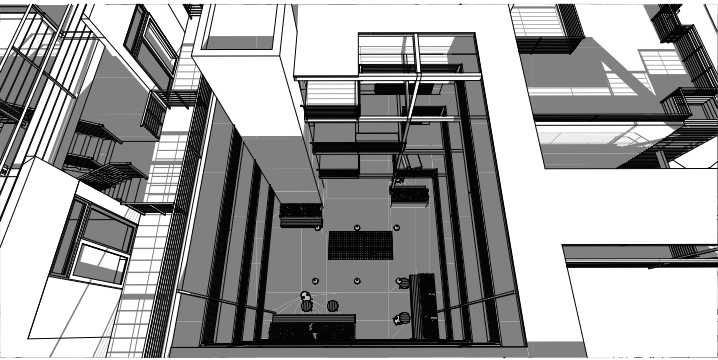


Spring/fall 6pm

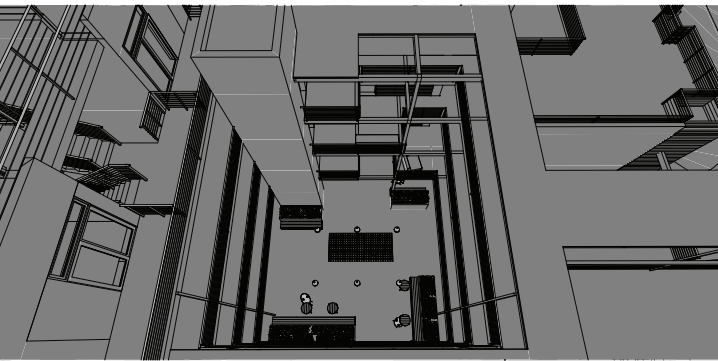
WINTER



Winter solstice 8am



Winter solstice noon

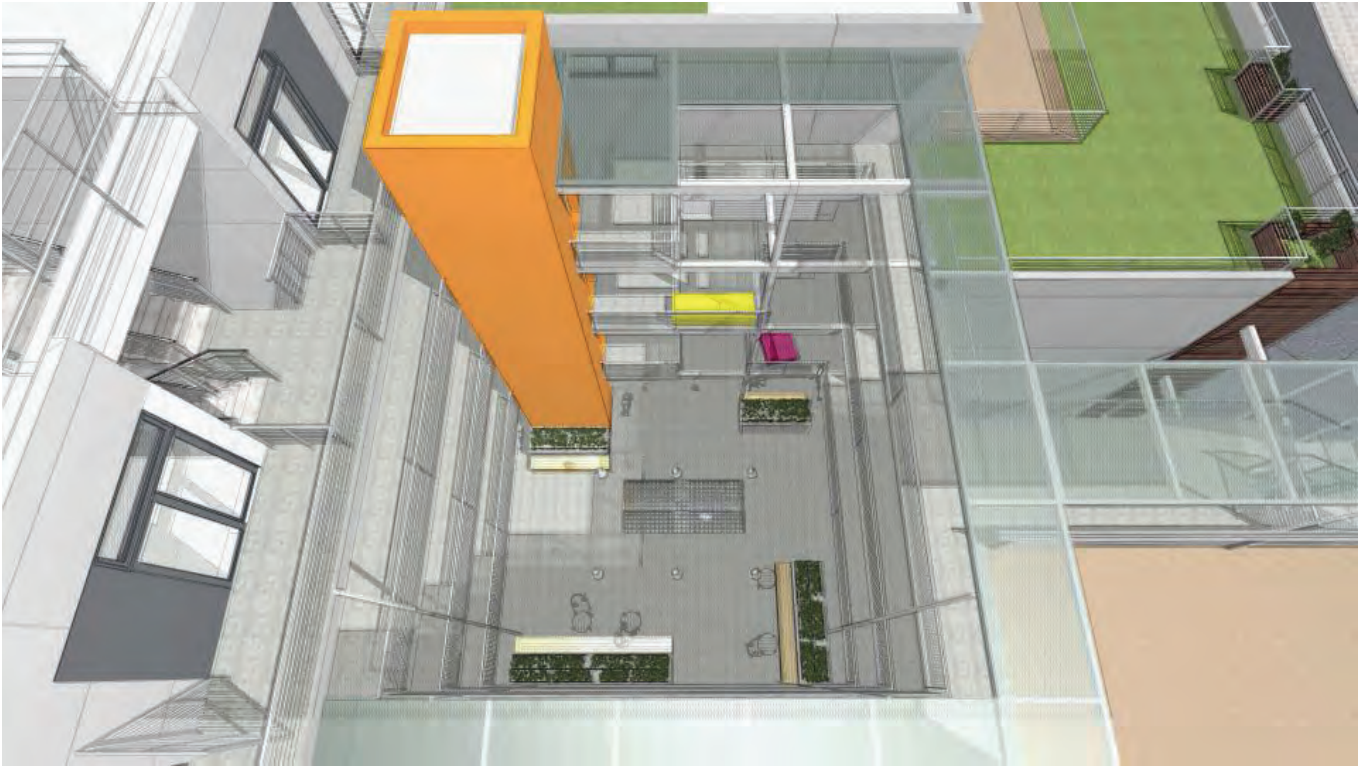
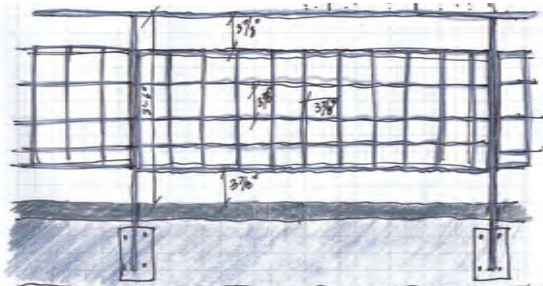
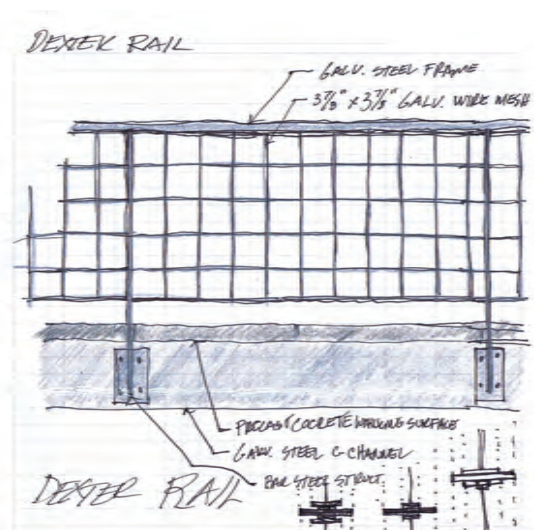
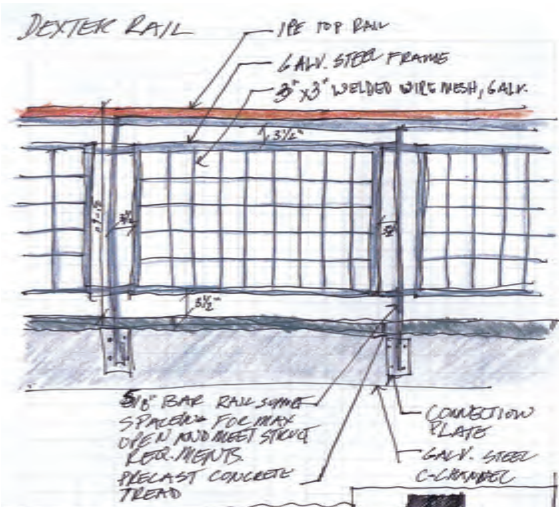


Winter solstice 6pm

L2 COURTYARD STUDIES



PRECEDENT IMAGES AND SKETCH STUDIES FOR PAINTED STEEL EXTERIOR CIRCULATION ELEMENTS INCLUDING STAIRS, EGRESS BALCONIES AND CANOPIES.

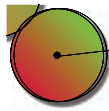
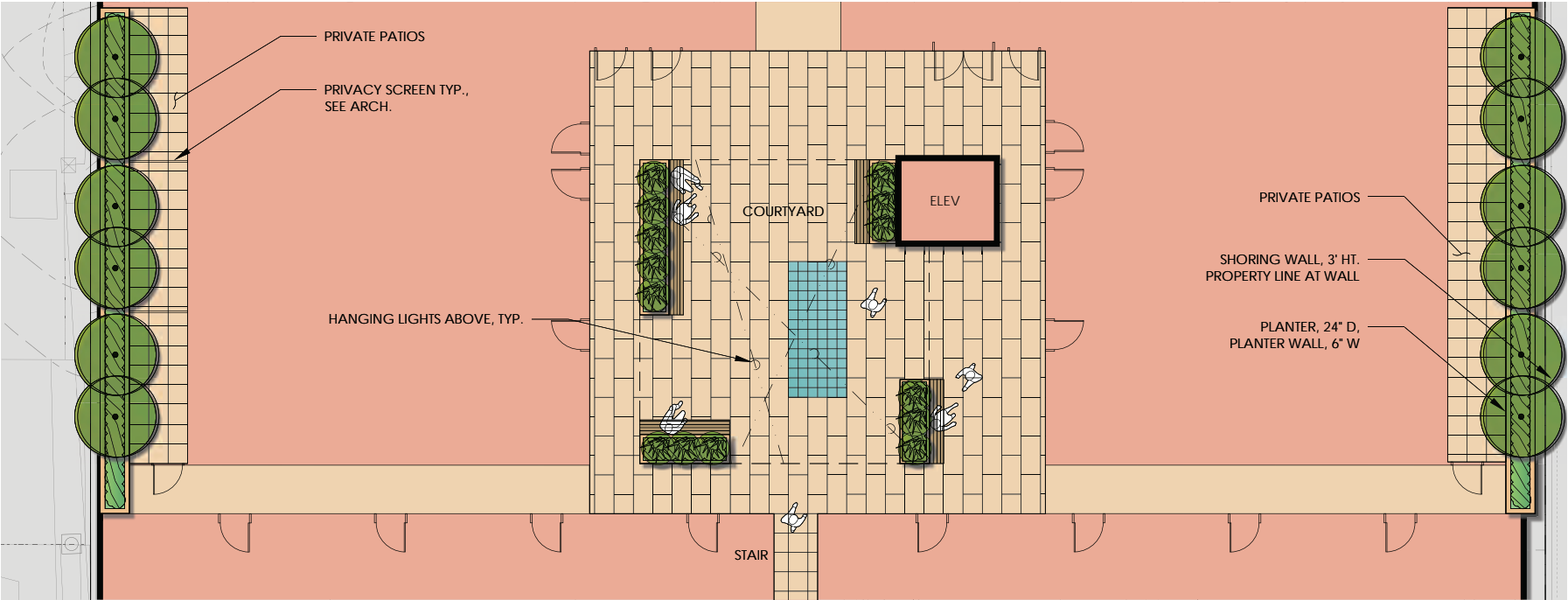


3D SKETCH OF COURTYARD FROM ABOVE



3D SKETCH OF COURTYARD

LANDSCAPE PLANS



ACER PALMATUM (GREEN) VAR. 2

JAPANESE MAPLE (GREEN) VAR. 2

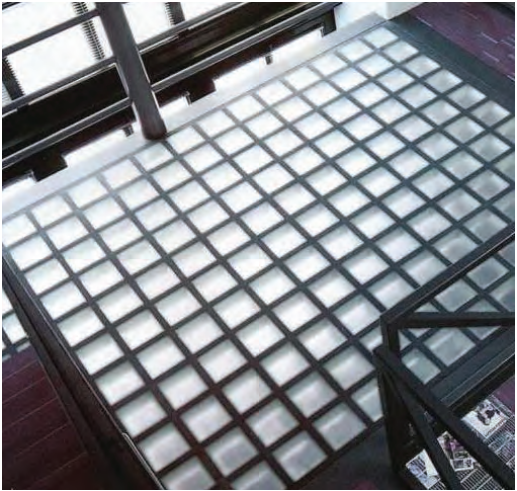
8'-10' HT. B&B PER PLAN



(2) PHYLLOSTACHYS AUREA
(6) OPHIOPOGON P. 'NIGRESCENS'

GOLDEN BAMBOO
BLACK MONDO GRASS

5 GAL. CONT. 36" O.C.
1 GAL. CONT. 12" O.C.



Lights Below



Lights Overhead



Staggered Pavers



Japanese Maple
Acer palmatum



Bamboo
Phyllostachys sp.



Planters as Furniture, Wood and Plants



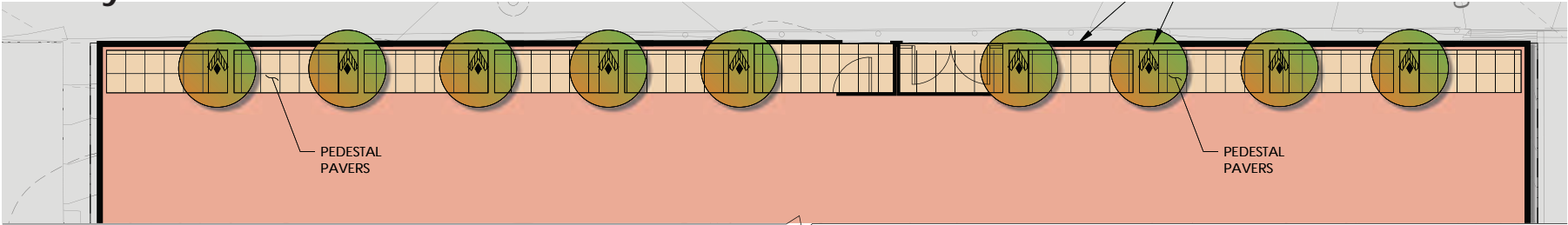
Green Ahead



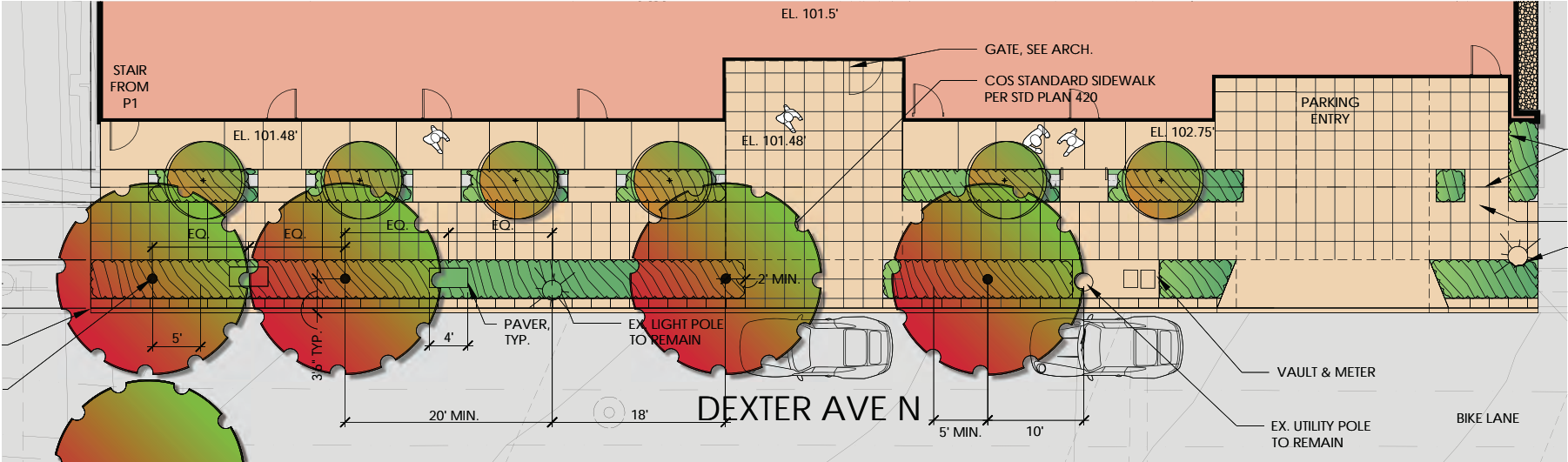
Above and Below

LANDSCAPE PLANS

alley



street



PLANT LIST

SYMBOL	BOTANICAL NAME	COMMON NAME
STREET TREE (SPECIES PREVIOUSLY APPROVED PER SDOT ARBORIST 11.25.2008)		
	NYSSA SYLVATICA	BLACK TUPELO
TREES		
	STEWARTIA MONDELPHA	TALL STEWARTIA
	ACER CIRCINATUM	VINE MAPLE



SHRUBS & GROUNDCOVERS	
	JAPANESE BOXWOOD
	PRIVET HONEYSUCKLE
	MOON BAY NANDINA
	'CAVATINE' COMPACT PIERIS
	DAVID'S VIBURNUM
	EPIMEDIUM
	OPHIPOGON PLANISCAPUS 'NIGRESCENS' BLACK MONDO GRASS



landscape layering

street tree



Black Tupelo
Nyssa sylvatica

live-work trees



Stewartia
Stewartia monodelpha

alley side tree

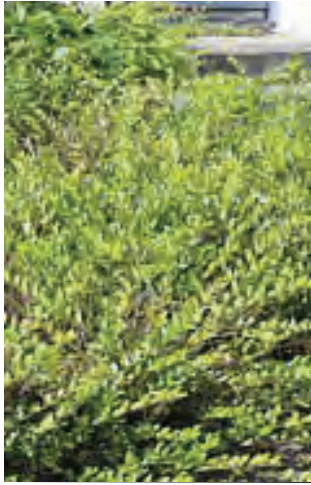


Vine Maple
Acer circinatum

streetside shrub mix



Japanese Boxwood
Buxus microphylla



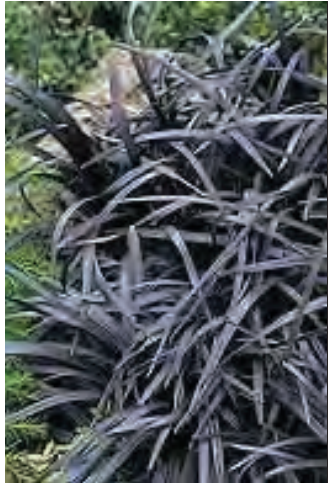
Privet Honeysuckle
Lonicera pileata



Nandina
Nandina

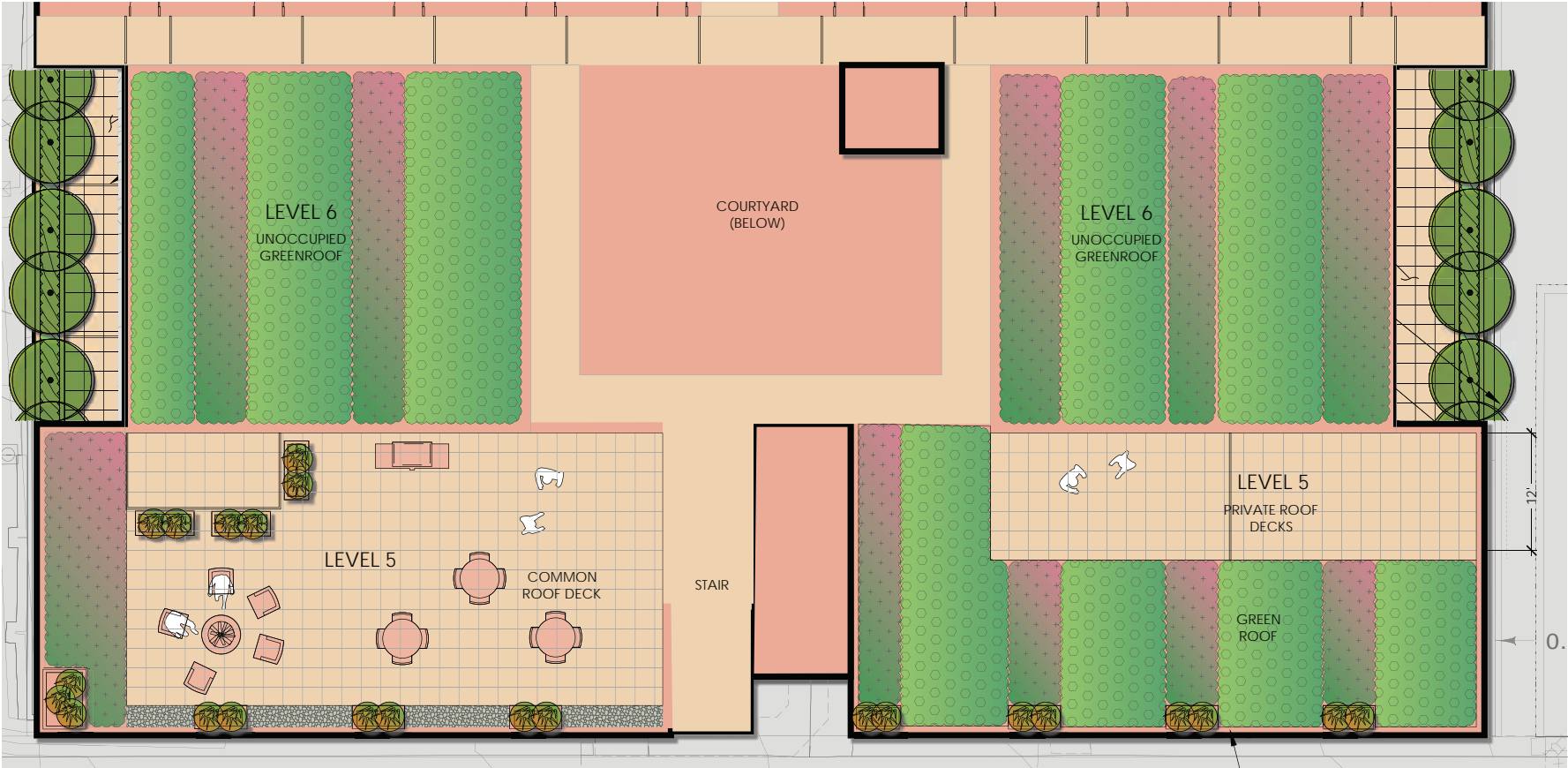


Cavatine Pieris
Pieris 'Cavatine'



Black Mondo Grass
Ophio.'nigrescens'

LANDSCAPE PLANS



GREEN ROOF PLANTING					
	SEDUM TILE PRE-VEGETATED MATS, COLOR MAX. BY ETERA				
	ALLIUM SCHOENOPRASUM	CHIVES	4" POT	CONT.	
	ECHINACEA PURPUREA	PURPLE CONE FLOWER	4" POT	CONT.	
	SEDUM 'AUTUMN JOY'	AUTUMN JOY SEDUM	4" POT	CONT.	
RECTANGULAR PLANTER					
	(2) PHYLLOSTACHYS AUREA	GOLDEN BAMBOO	5 GAL.	CONT.	36" O.C.
	(6) OPHIOPOGON P. 'NIGRESCENS'	BLACK MONDO GRASS	1 GAL.	CONT.	12" O.C.



food, fire and fun



bands of green



Sedum Mat



Chives
Allium sp.



Coneflower
Echinacea sp.



Sedum 'Autumn Joy'



Bamboo



sun spots

MODULAR CONSTRUCTION

Levels 2 through 4 will be wood-framed factory built modular construction. Wood-frame factory-built modules are completed to high standards of quality within the factory, then shipped and assembled rapidly on site. The result of this process is:

- Reduced total construction time- up to 40% shorter time overall.
- Reduced neighborhood disruption- setting modules on site takes several weeks to erect.
- High quality construction in the enclosed environment of a factory.
- Reduced construction waste is a result of building in a factory. Front-end computer calculations factor in lengths of components.

Daly Partners with Bushnaq Studio as architect is currently developing ‘N’ Habit Belltown, a 51-unit modular project currently under construction on 3rd Avenue in Belltown. 1701 Dexter is slated to use the same design and construction team.



‘N’ HABIT BELLTOWN EXTERIOR 3D RENDERING



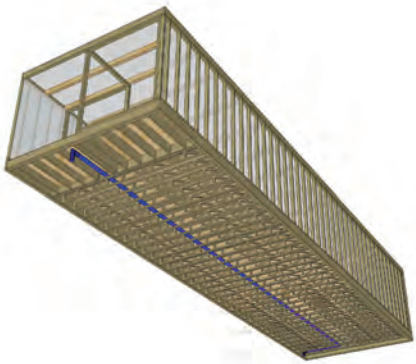
‘N’ HABIT BELLTOWN INTERIOR 3D RENDERING



‘N’ HABIT BELLTOWN MODULES IN PREPARATION FOR TRANSPORT



‘N’ HABIT BELLTOWN ELEVATION



MODULAR STUDIES



‘N’ HABIT BELLTOWN MODULES UNDER CONSTRUCTION

