



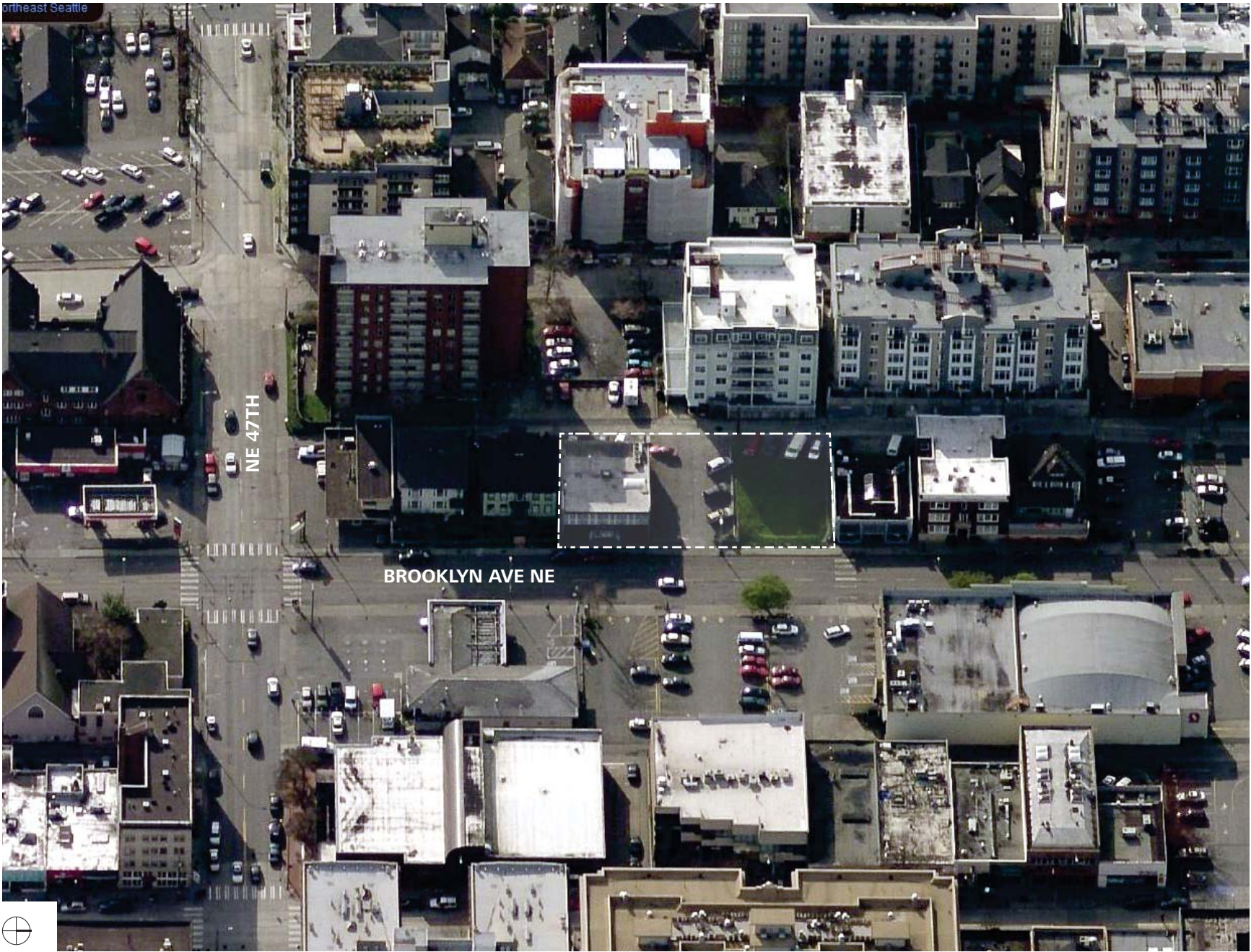
caron

07.15.2013 RECOMMENDATION MEETING PRESENTATION  
4717 BROOKLYN AVE NE / DPD PROJECT # 3014328

washington 2505 3rd avenue•suite 300C , Seattle, WA 98121 • california 1993 Santa Barbara Street, San Luis Obispo, CA 93401 • [www.caronarchitecture.com](http://www.caronarchitecture.com)

PAGE 1





The area surrounding the site is part of the University Gardens Mixed Use Core, designated per neighborhood plan approved in 2009 as the area of the University District extending generally from Brooklyn Ave. NE to 7th Ave NE and from NE 50th St. to NE 43rd St. It is primarily zoned as Neighborhood Commercial and is surrounded by Low Rise and Multifamily zones. The University Community Urban Center Neighborhood Plan (2009) has targeted this area to be developed into “a more intense pedestrian oriented, mixed-use area, with amenities, open space, and transit accessibility supporting a wide variety of compatible activities”

The proposed site is composed of three parcels; two vacant lots and one existing two-story commercial structure (Weaving Works). It is zoned NC-65 and lies within a Station Area Overlay District (SAOD), accounting for the future Northgate Link light rail expansion along Brooklyn Avenue. Light rail construction is scheduled to begin in 2012 with services starting in 2021.

The site is designated as part of the University District Northwest Urban Center Village and within a frequent transit corridor. Designated Pedestrian Area overlay exists nearby along University Way and on Brooklyn Ave south of NE 47th.

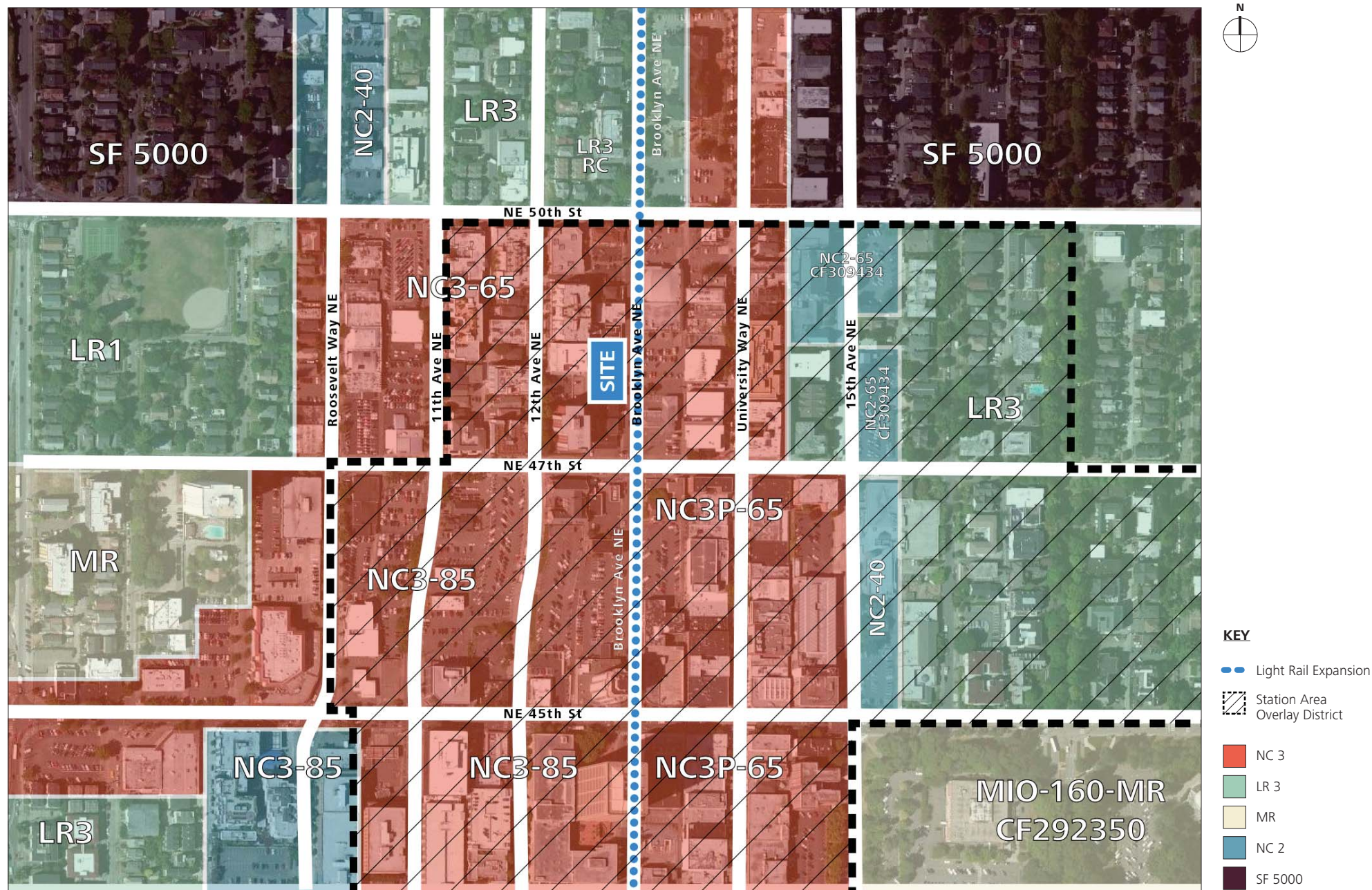
Recently built mixed-use projects are located across the alley and along 12th Avenue NE, ranging in size from six to seven stories in height. Located directly across Brooklyn Ave is a large open surface parking lot servicing Safeway to the north and the gas station to the south.

TABLE OF CONTENTS

Site Analysis	2-3
Site Context	4,
Preferred EDG Scheme	5
Development Objectives	6
Shadow Analysis	7
Site Plan/ Ground Level Plan	8
Floor Plans	9
Elevations/ Sections	10-15
Materials	16
Landscaping	17-18
Lighting Plan	19
Design Guideline Responses	20-23
Departures	24







**ADDRESSES:** 4717, 4722, 4727 Brooklyn Ave NE

**ZONING:** NC3-65

**OVERLAY DESIGNATION:** Station Area Overlay District  
Urban Village: University District Northwest Urban Center Village  
Frequent Transit Corridor

NEIGHBORHOOD AND SITE ANALYSIS

07.15.2013 RECOMMENDATION MEETING PRESENTATION  
4717 BROOKLYN AVE NE / DPD PROJECT # 3014328





## PROJECT SITE



1. BROOKLYN AVE - FACING WEST



2. BROOKLYN AVE - FACING EAST

## STREETSCAPES

07.15.2013 RECOMMENDATION MEETING PRESENTATION  
4717 BROOKLYN AVE NE / DPD PROJECT # 3014328



SE CORNER

## MASSING OPTION A (PREFERRED OPTION)

Screened facade

- PROS:
- Residential entry recessed at street level encouraging pedestrian use and movement
  - Interior courtyard creates privacy for residents
  - Full commercial base
  - Facade modulated at 100'
  - No departures required

- CONS:
- interior courtyard pushes mass to street



NE CORNER



NE AERIAL





The preferred scheme is proposing a 7 story building with commercial, residential amenity, and back-of-the-house uses found at the ground floor and 6 levels of residential units above. Units are accessed through open air balconies facing the interior residential courtyard.

The ground level of the preferred scheme consists of a residential lobby, bicycle storage, leasing office, building services and retail/commercial space. The second through fourth levels contain approximately 10 residential units, and common amenity area on second level with access to the elevated courtyard. The fifth through seventh floors each contain 9 units per floor. All floors contain a mix of unit sizes and bedroom counts.

The elevated courtyard, which sits atop the ground level commercial floor, contains hardscape and landscaped areas to soften the interior space. There are also private amenity areas provided on the roof which include open hardscaped spaces, planters, BBQ space and potential for an urban p-patch.

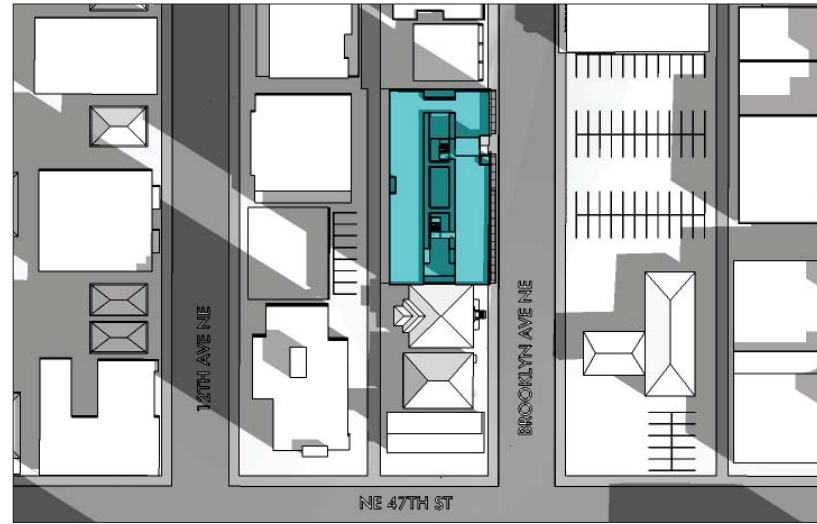
Parking is not required for this site, although 8 stalls will be provided on the ground level accessed from the alley, as well as extra space for required bicycle parking and a student shuttle service to take residents to the campus.

DEVELOPMENT STATISTICS SUMMARY

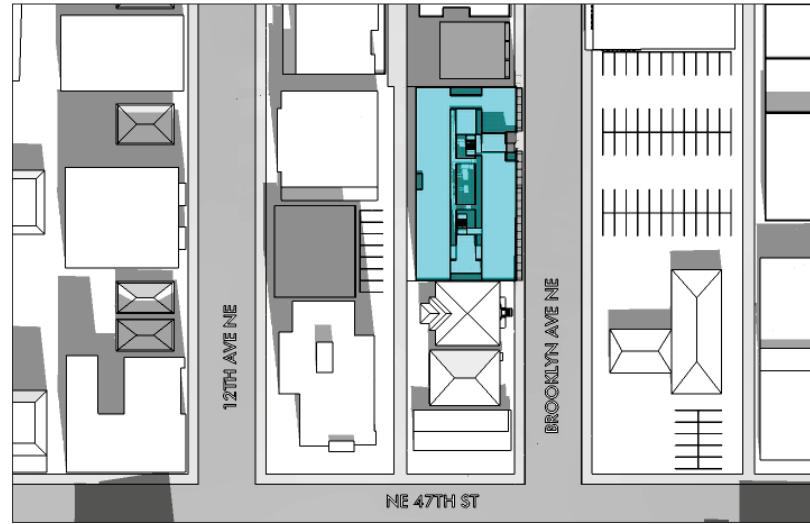
Lot Size	12,450 SF	
FAR	5.75 (Station Overlay Zone)	
Allowable SF	71,587 SF	
Proposed SF	71,241 SF	
Parking Stalls	9	
LEVEL	AREA	UNITS
Roof	487 SF	
7th Floor	9,131 SF	9
6th Floor	9,131 SF	9
5th Floor	9,131 SF	9
4th Floor	9,663 SF	10
3rd Floor	9,663 SF	10
2nd Floor	9,663 SF	9
Alley Level	6,146 SF	
Street Level	8,226 SF	
TOTAL	71,241 SF	56



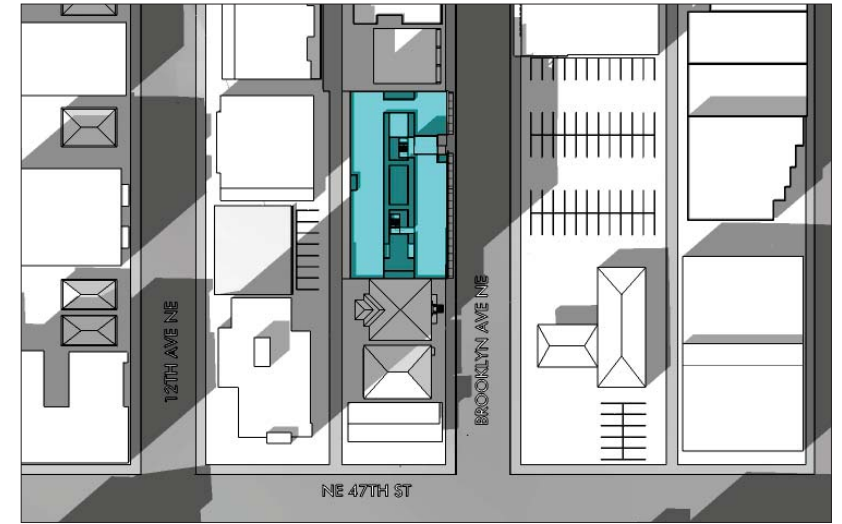




3/21 9AM



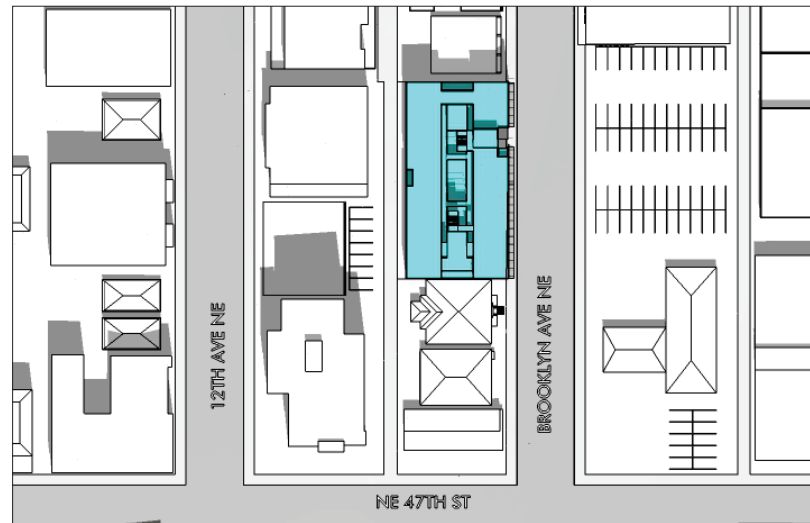
3/21 12PM



3/21 3PM



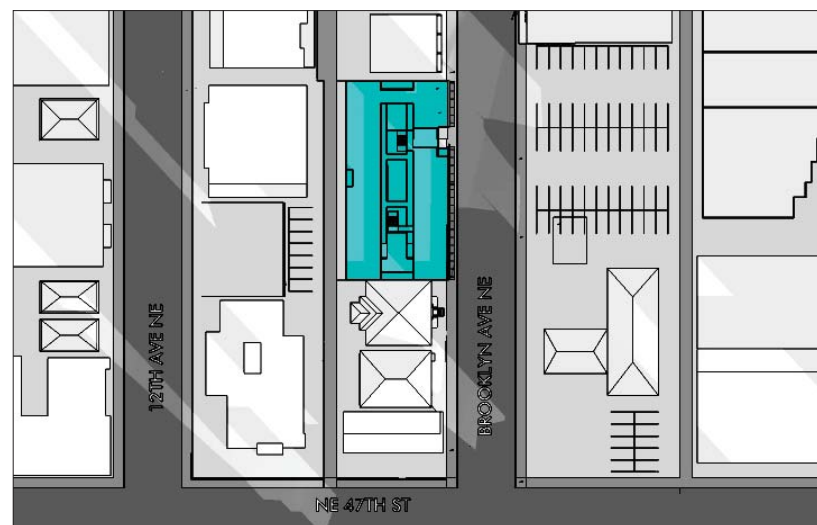
6/21 9AM



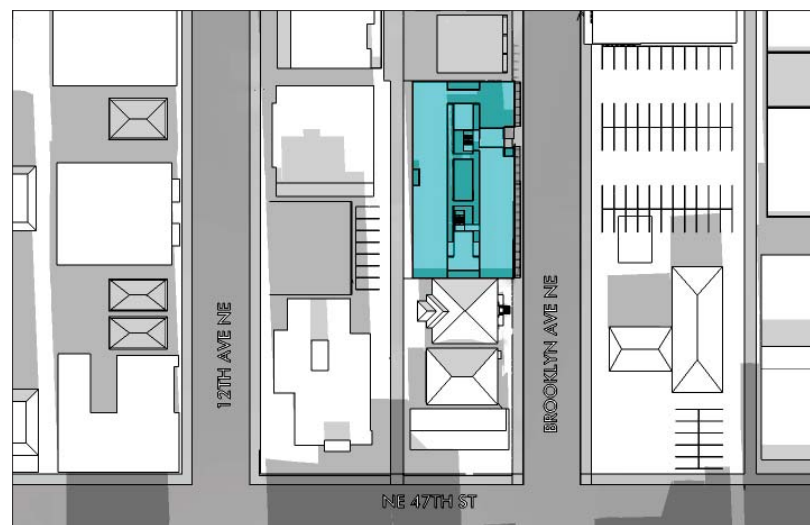
6/21 12PM



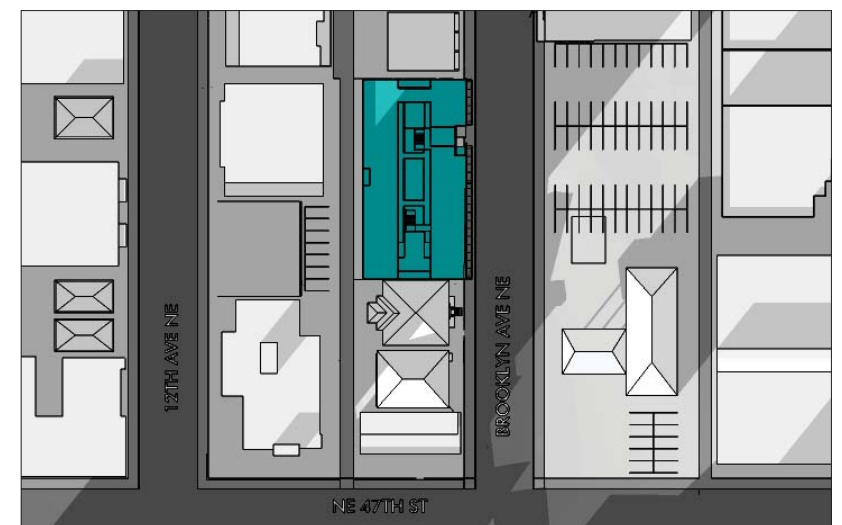
6/21 3PM



12/21 9AM



12/21 12PM



12/21 3PM

# SHADOW STUDIES

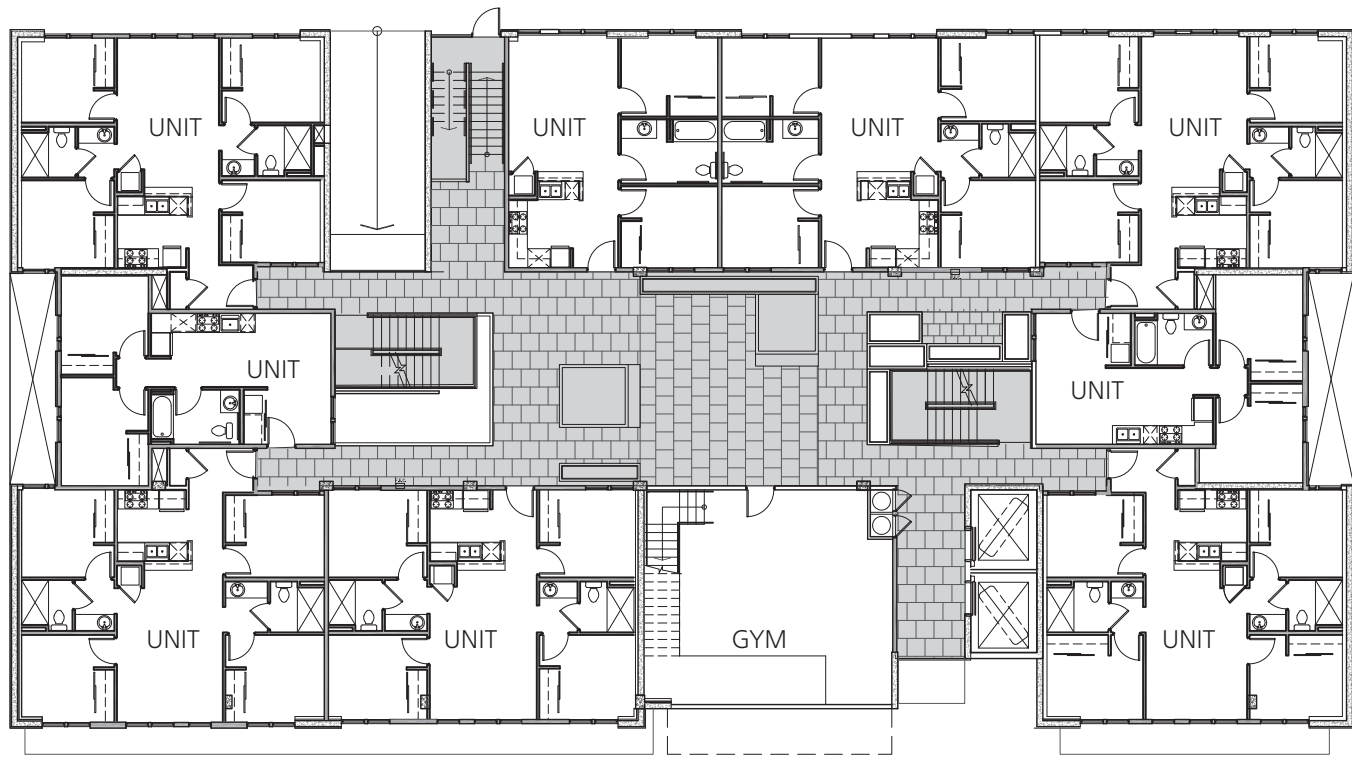
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4717 BROOKLYN AVE NE / DPD PROJECT # 3014328



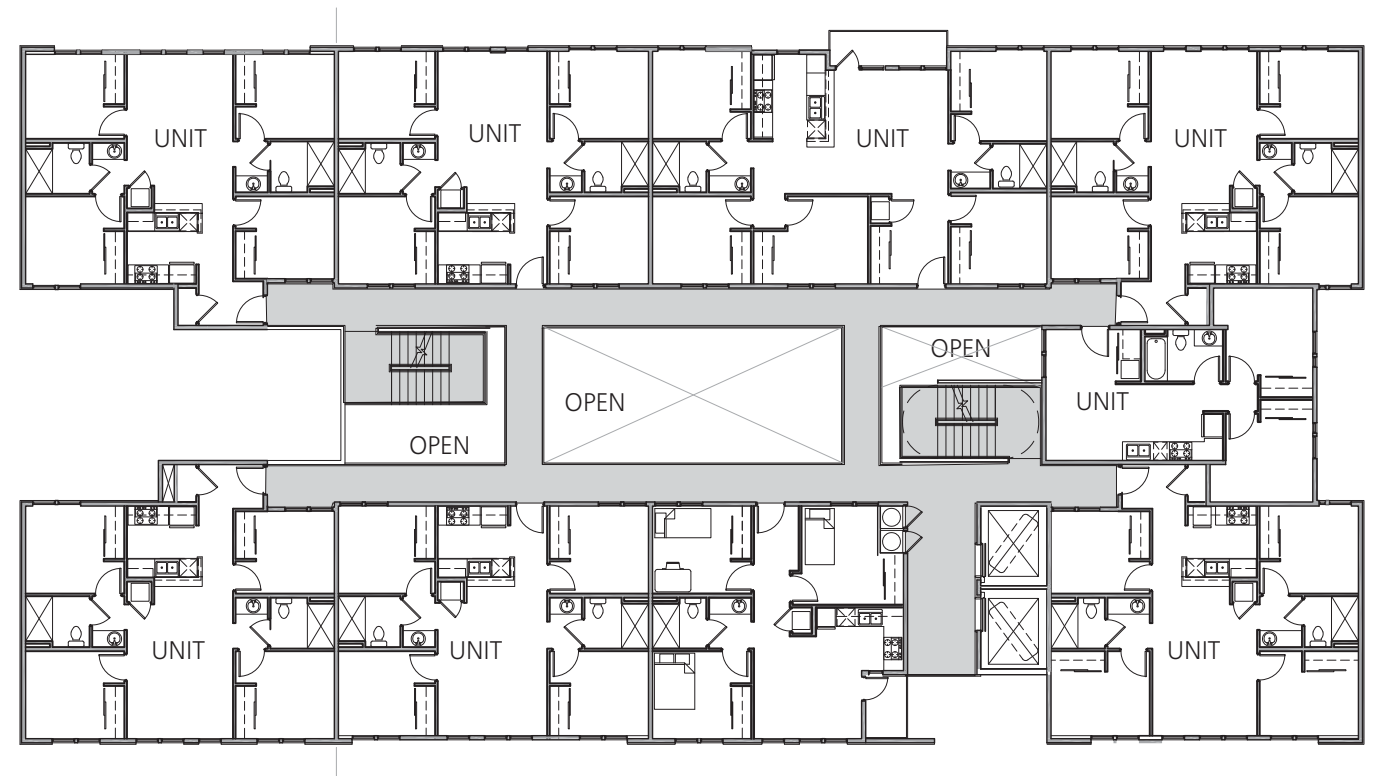
SITE/GROUND PLAN

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4717 BROOKLYN AVE NE / DPD PROJECT # 3014328

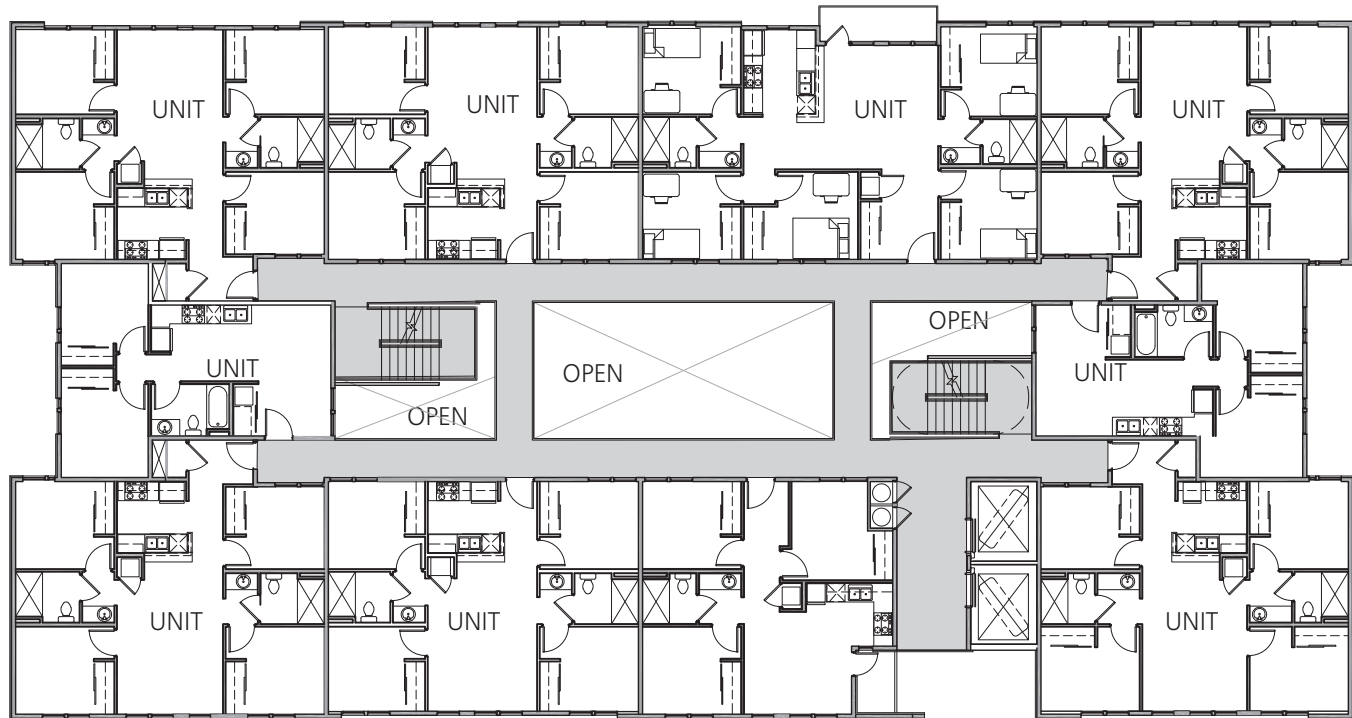




FLOOR PLAN 2



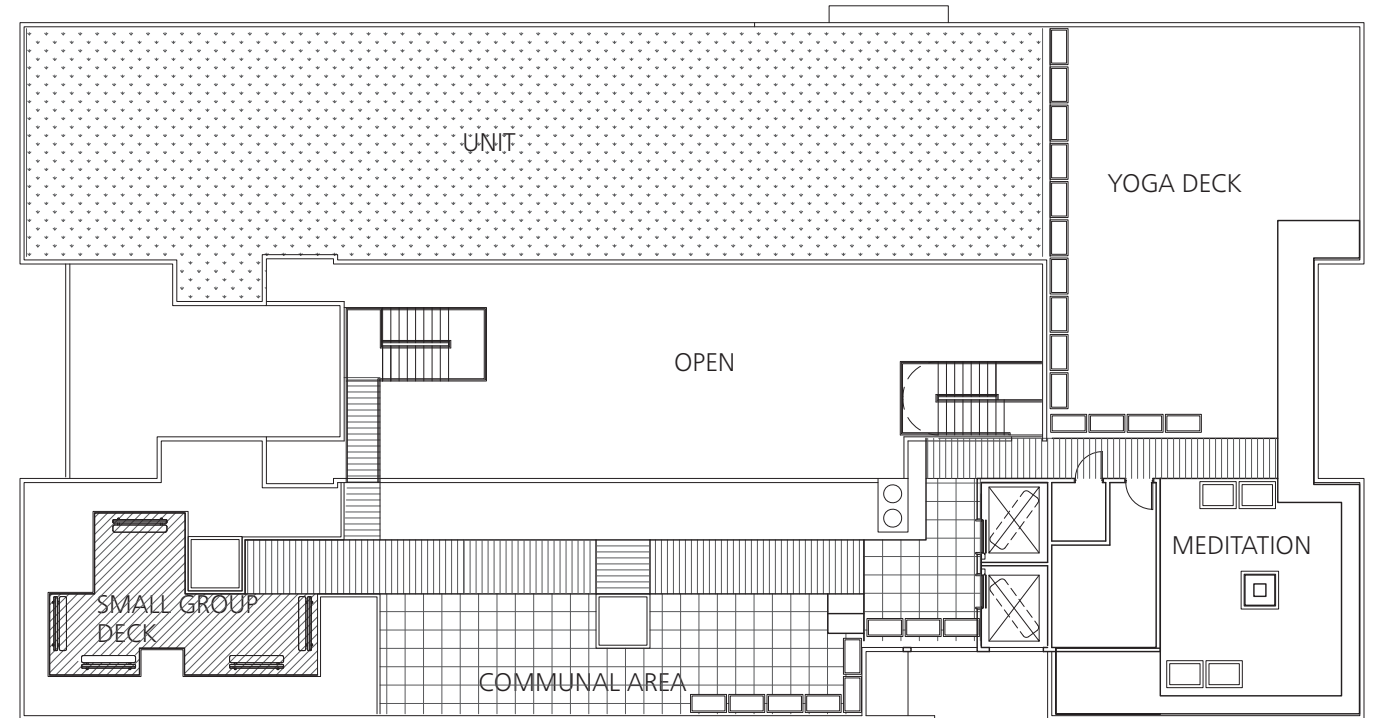
FLOOR PLANS 5-7



FLOOR PLANS 3-4

## FLOOR PLANS

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4717 BROOKLYN AVE NE / DPD PROJECT # 3014328



ROOF PLAN



MATERIALS SCHEDULE	
1.	METAL SIDING COLOR 1
2.	LAMINATED PANEL COLOR 1
3.	LAMINATED PANEL COLOR 2
4.	PREFINISHED METAL
5.	PERFORATED METAL PANEL
6.	FIBERGLASS WINDOW OR DOOR ASSEMBLY
7.	ALUMINUM STOREFRONT WINDOW
8.	STONE VENEER
9.	CONCRETE
10.	PAINTED STEEL AND GLASS CANOPY
11.	GARAGE DOOR
12.	PERFORATED PANEL AND UNIT EXHAUST, TYP.



EAST (BROOKLYN AVENUE) ELEVATION

07.15.2013      RECOMMENDATION MEETING PRESENTATION  
4717 BROOKLYN AVE NE / DPD PROJECT # 3014328





NORTH ELEVATION

07.15.2013      RECOMMENDATION MEETING PRESENTATION  
 4717 BROOKLYN AVE NE / DPD PROJECT # 3014328





MATERIALS SCHEDULE <span>(X)</span>	
1.	METAL SIDING COLOR 1
2.	LAMINATED PANEL COLOR 1
3.	LAMINATED PANEL COLOR 2
4.	PERFINISHED METAL
5.	PERFORATED METAL PANEL
6.	FIBERGLASS WINDOW OR DOOR ASSEMBLY
7.	ALUMINUM STOREFRONT WINDOW
8.	STONE VENEER
9.	CONCRETE
10.	PAINTED STEEL AND GLASS CANOPY
11.	GARAGE DOOR
12.	PERFORATED PANEL AND UNIT EXHAUST, TYP.



WEST (ALLEY) ELEVATION

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SOUTH ELEVATION

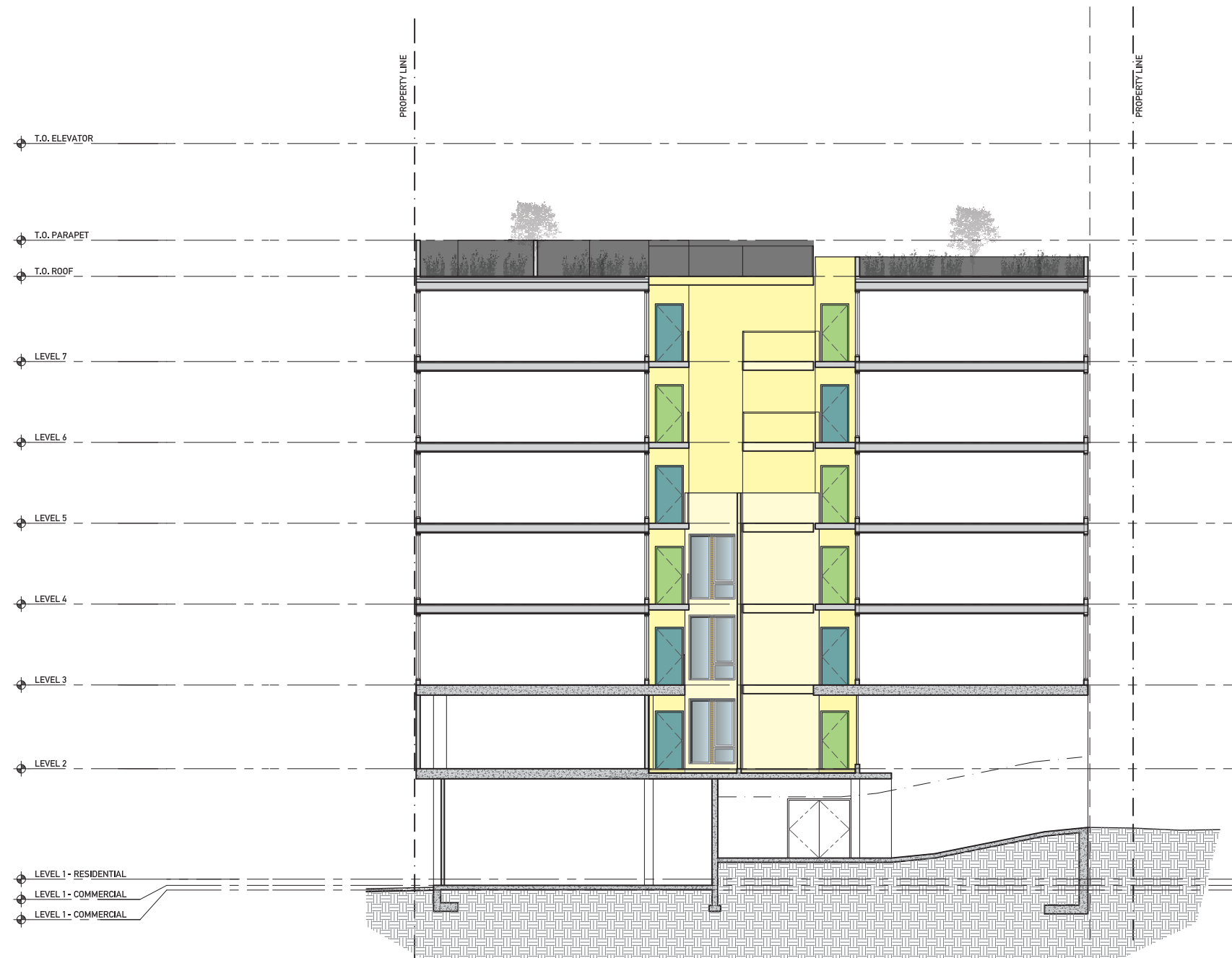
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4717 BROOKLYN AVE NE / DPD PROJECT # 3014328





## SECTIONS

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4717 BROOKLYN AVE NE / DPD PROJECT # 3014328

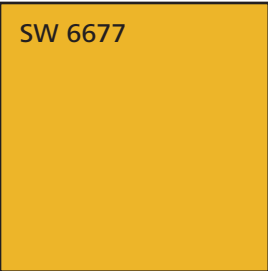


## SECTIONS

07.15.2013 RECOMMENDATION MEETING PRESENTATION  
4717 BROOKLYN AVE NE / DPD PROJECT # 3014328



MATERIAL PALLETTE



Kynar painted  
perforated metal  
screen



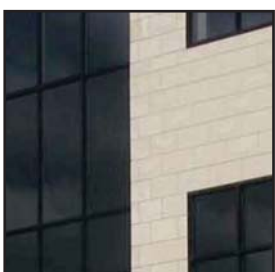
Standing seam  
metal panel



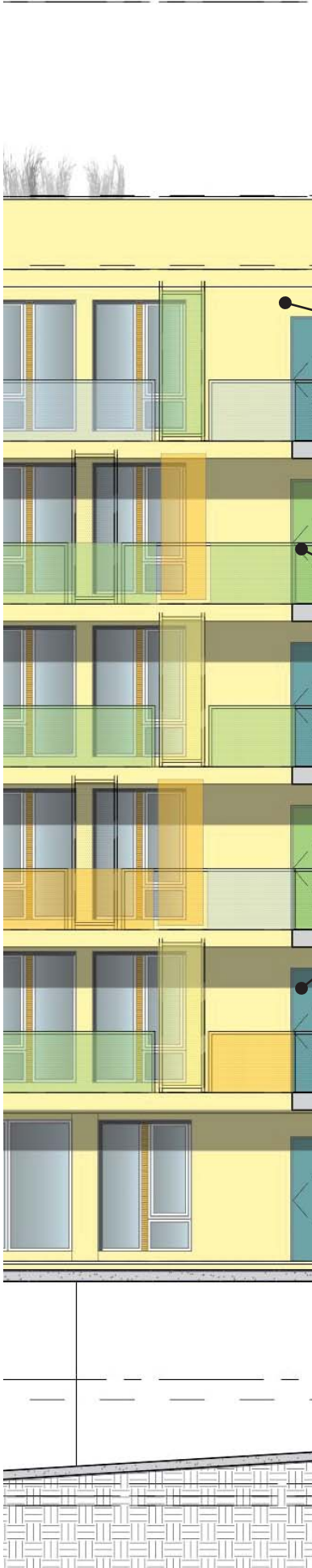
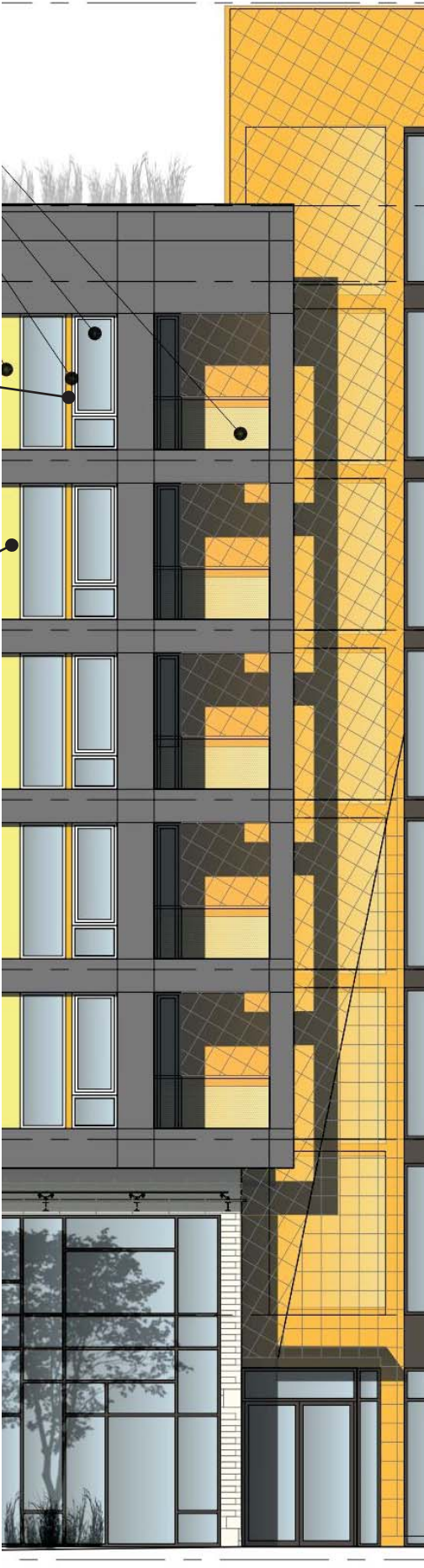
Translucent glass  
or acrylic and  
aluminum



Charcoal  
storefront system



Thinstone tile  
(limestone or  
light color)



Painted  
fibercement  
panels



SW 6912

Painted steel  
staircases



SW 6703

Translucent glass  
or acrylic privacy  
panels



SW 6493

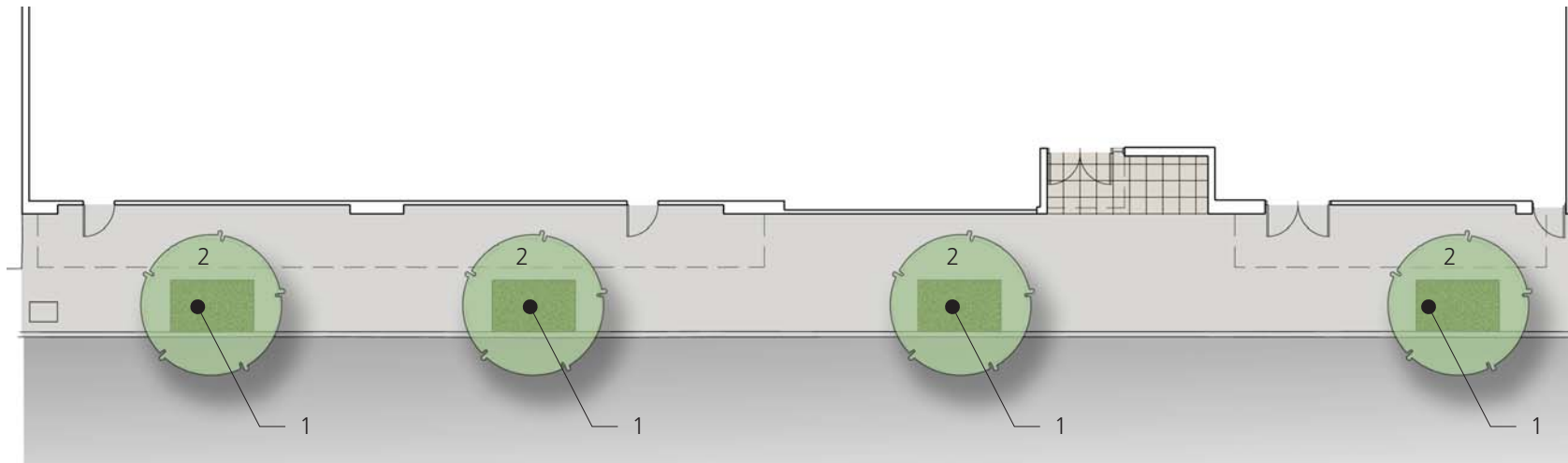
Acrylic railing  
panels



Cone tied  
concrete







1. CORAL BEAUTY GROUNDCOVER

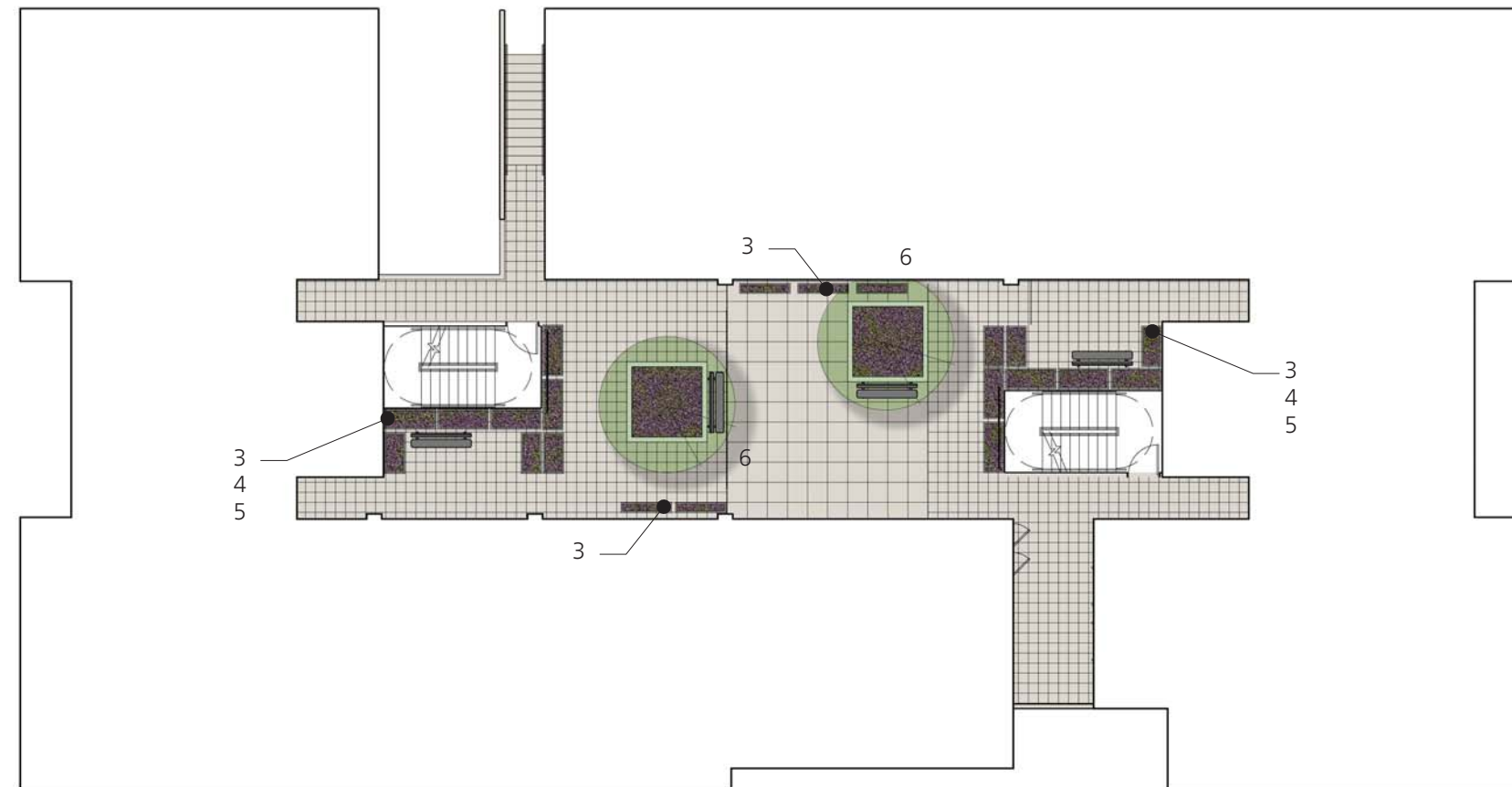


1. CORAL BEAUTY GROUNDCOVER



2. GREEN COLUMN MAPLE

## GROUND FLOOR STREETScape



7. JAPANESE HOLLY



6. AMERICAN HORNBEAM



5. AUGUST MOON



4. DWARF SWEETSPIRE



3. AZTEC GRASS

## SECOND FLOOR COURTYARD

LANDSCAPE PLAN - STREET AND SECOND FLOOR COURTYARD

07.15.2013 RECOMMENDATION MEETING PRESENTATION  
4717 BROOKLYN AVE NE / DPD PROJECT # 3014328



LANDSCAPE FEATURES

1. CEMENT PAVER

2. WOOD COMPOSITE DECKING

3. ROSE BUSH

4. FOUNTAIN GRASS

5. DWARF BOX-LEAFED BARBERRY
6. FLAME MAPLE TREE

7. AZTEC GRASS

8. BLUE SWITCH GRASS

9. GOLDEN BAMBOO

10. GREEN ROOF SEDUM TRAY SYSTEM



10



6



9



8



7



5



4



3



2



1

ROOF DECK

LANDSCAPE PLAN - ROOF DECK

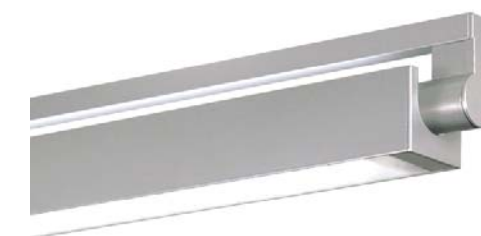
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4717 BROOKLYN AVE NE / DPD PROJECT # 3014328







①



②



③



## SITE LIGHTING PLAN

07.15.2013 RECOMMENDATION MEETING PRESENTATION  
4717 BROOKLYN AVE NE / DPD PROJECT # 3014328





VIEW LOOKING NORTH ALONG BROOKLYN



STREET VIEW ALONG BROOKLYN AVENUE

DESIGN GUIDELINES: A

07.15.2013 RECOMMENDATION MEETING PRESENTATION  
4717 BROOKLYN AVE NE / DPD PROJECT # 3014328

## A-2 Streetscape Compatibility

Acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.

The project will not create a large shadow pattern over public right of way. Due to its orientation, the narrow width of the site and need to provide a 5' alley dedication, the project is proposing to step back 2' only at the street level to provide for a wider sidewalk allowing more space for pedestrians and landscaping. Also, at the south end of the project, the units have been eliminated from the upper three floors to allow for further light penetration into the open air courtyard at the interior of the building.

## A-3 Entrances Visible From Street

Entries should be clearly identifiable and visible from the street

*EDG Board Comment: ...setting back the circulation seam of the elevators and their vestibules aligned with the inset street-level residential pedestrian entrance was a strong architectural move to be kept and refined.*

All commercial spaces will be directly accessed from Brooklyn Avenue, with overhead protection, individual signage, and new street trees provided. The residential lobby will be distinguished with a different canopy profile and is further set back from the street to align with the elevator tower element, providing an asymmetrical seam in the fabric of the facade. The proposed tower material is to be painted perforated metal to allow for air and light penetration.

## A-4 Human Activity

New development should be sited and designed to encourage human activity on the street.

*EDG Board Comment: Design development and refinement should clearly convey a sense of heightened and pleasant pedestrian experience.*

The existing sidewalk on Brooklyn avenue is approximately 10' wide and the project is proposing to pull the ground level floor back 2' to allow for a 12' wide sidewalk. The project will also provide translucent overhead protection, to allow for further light penetration at the commercial spaces, and new street trees with 8' long planting strips allowing for further vegetation to be installed.



RESIDENTIAL ENTRY

**caron**





ALLEY PERSPECTIVE LOOKING SOUTH

#### A-5 Respect for Adjacent Sites

Buildings should respect adjacent properties by being located on their sites to minimize disruption of the privacy and outdoor activities of residents in adjacent buildings.

*EDG Board Comment: While it is acknowledged that infill development cannot always accede to the desires of adjacent property owners, good design will seek ways to minimize abrasiveness and obtrusiveness.*

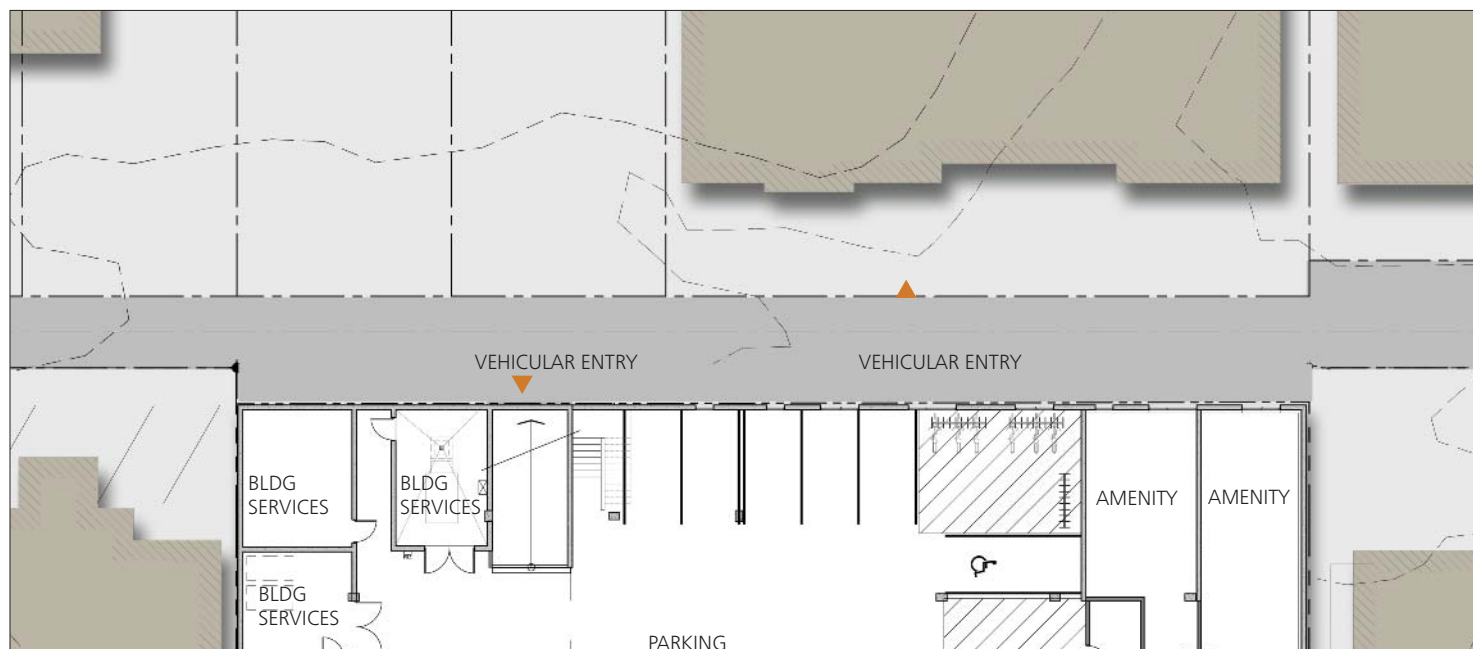
The project provides an elevated interior courtyard and roof deck for the residents use, rather than at the street, to help reduce any noise or light pollution. At the north and south property lines, the building steps in at the center third to allow for light infiltration and all tall elements, i.e. the elevator and stair towers, have been kept inset to the property lines so as to not further impact the neighboring buildings. All utilities and parking will be screened within the structure and the neighboring projects across the also have building services oriented toward the alley. There are no adjacent residential uses.

#### A-8 Parking and Vehicle Access

Siting should minimize the impact of automobile parking and driveways on the pedestrian environment, adjacent properties and pedestrian safety.

*EDG Board Comment: The applicant should study how the access from the alley actually works for the intended structure and in concert with adjacent properties and strive to avoid conflicts wherever possible.*

The project is providing the city required 5' alley dedication so as to assist in the widen of the existing alley as well as taken care to located the new garage ramp south of the adjacent garage entrance as illustrated in the diagram located in the lower left corner. Both the dedication and ramp location coordination have been done in the hopes of preventing any potential alley congestion in the future.



ALLEY PLAN



VIEW OF RESIDENTIAL GARAGE/COURTYARD ENTRY

DESIGN GUIDELINES: A

07.15.2013 RECOMMENDATION MEETING PRESENTATION  
4717 BROOKLYN AVE NE / DPD PROJECT # 3014328





## C-2 Architectural Concept and Consistency

Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept.

*EDG Board Comment: The board selected this guideline to be of special urgency in attempting to integrate the attractive energy of the facade in Option B with the organizational diagram of Option A, all while preserving the strength of the asymmetrical entry and circulation seam within what would be a 'busier' facade.*

In response to the board's recommendations for the Brooklyn street façade; we have explored Option B, however upon further review, the need for substantial design departures from structural overhangs and SDOT lease agreements made that option non feasible. Similarly we have reassesses the facade of the building, given its context as mid-block project and decided that a more 'calm' facade would serve well for future street edge and as catalyst for adjoining properties development. However, we translated some of the playfulness of option B into a set of color variations and modulation of the facade. The building exterior will maintain the asymmetrical entry 'seam', while taking the playfulness and energy of option B and applied the organization of option A to create an elegant box that thru a 'peeling' of the faced reveals its multicolored interior quality and provides the opportunity for the interior courtyard to have lighter colors. The solid ground floor commercial space acts as a solid base for the units above and gives emphasis to the residential entry 'seam'

## C-3 Human Scale

The design of new buildings should incorporate architectural features, elements and details to achieve a good human scale.

*EDG Board Comment: The board was especially interested in seeing details of the interactions with the retail space entry from the pedestrian, eye level perspective along Brooklyn Avenue NE.*

To achieve the human scale factor, the building off sets 2' to provide a wider corridor, creating a better pedestrian experience. The façade will provide glazed canopies throughout the length of the retail storefronts, their transparency allows light to reach the pedestrian on warm sunny days and provide shelter during rain months. Varying insets and canopy heights interact and add interest to the pedestrian experience.

## C-4 Exterior Finish Materials

Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, patterns or lend themselves to a high quality of detailing are encouraged.

*EDG Board Comment: Architectural materials, scale and details should be integrated within a building whose concept is appropriate for the site and its surroundings as well as its programmatic uses. The Board was not prescriptive regarding materials, but would expect to see a choice of durable and sustainable materials and to be presented with samples of proposed colors and materials at the subsequent recommendation meeting.*

At street level, a unique pattern in masonry will be utilized, which is a durable material used on University of Washington campus and will help the orient the pedestrian and emphasize the connection to the surrounding University District community. Also provided is linear modulation by breaking up the storefront façade with columns long the east corridor. The entry 'seam' is to be constructed of perforated metal panels and a glass elevator, which allows airflow and light into and out of the interior courtyard space and adds contrast and visual interest at the buildings core. Low maintenance materials such as metal panels on less then 25% of the street elevation and masonry have bee chosen to provide durable and cost effective materials. Additionally, fiber cement panels and prefinished metal is used to add color and interest to the upper levels.



DESIGN GUIDELINES: C

07.15.2013 RECOMMENDATION MEETING PRESENTATION  
4717 BROOKLYN AVE NE / DPD PROJECT # 3014328



PEDESTRIAN ORIENTED STREET FRONTAGE

#### D-1 Pedestrian Open Spaces and Entrances

Convenient and attractive access to the buildings entry should be provided. To ensure comfort and security, paths and entry areas should be sufficiently lighted and entry areas should be protected from the weather. Opportunities for creating lively, pedestrian-oriented open space should be considered.

The ground level of the project is offset 2' from the property line to allow a much more ambient walking experience while anticipating the occupied retail spaces at street level. Covering the sidewalk with a glazed canopy allows light into those spaces located at the ground floor and provides a well lit, secure path leading to the seam entry area and will allow for visual surveillance for personal safety. Landscape of the corridor enhanced with large tree planting strips will add to the positive approach to the entrance.

#### D-7 Personal Safety and Security

Project design should consider opportunities for enhancing personal safety and security in the environment under review.

The beauty of having this street front aligned with storefront is that it can provide well lit areas all year round for all pedestrians who walk this corridor. As for those whom reside, a lighted and stepped residential entry allows for safe access to the building with open air corridors above allowing for the surveillance of the entries and lends to a heightened personal safety without having to access on the main corridor. See lighting plan provided.

#### D-10 Commercial Lighting

Appropriate levels of lighting should be provided in order to promote visual interest and a sense of security for people in commercial districts during evening hours.

The projects glazed canopies allows for adequate lighting for the sidewalk and commercial spaces, and in the evening, lights will be mounted to the canopy structure so as to provide down-lighting without of the sidewalk but not cause light pollution for the units above. At the residential entry, shielded wall wash lighting will be used up the length of the shaft to emphasize the entry element as well as down-lights at the two-story residential gym. Lighting will also be provided at the residential garage ramp and second floor courtyard accessed from the alley. See lighting plan provided.

#### D-11 Commercial Transparency

Commercial storefronts should be transparent, allowing for a direct visual connection between pedestrians on the sidewalk and the activities occurring on the interior of a building. Blank walls should be avoided.

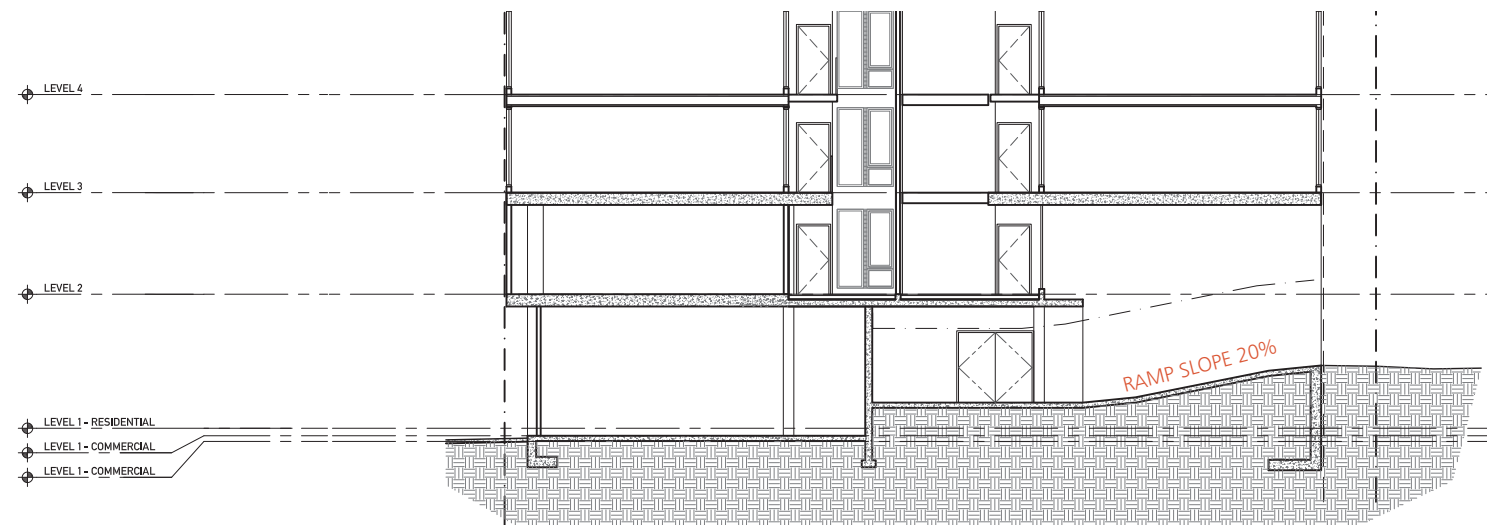
The project provides a ground level that is primarily composed of glazed storefront for an enhanced interaction between the pedestrian and commercial spaces. Masonry columns have been used to help break up and add identity to each ground level space, and also provide a solid base for the six residential floors above.

*EDG Board Comment for Section D: The above four guidelines dealing with the Pedestrian Environment were cited by the Board as being of highest priority for the success of the project under review. The Board was particularly interested in seeing clearly how one would enter the building, on foot or in a vehicle from the alley, and as a pedestrian from the front sidewalk. The Board would like to see a complete outdoor lighting plan, one that illuminated the various entrances and provided security without causing glare for neighbors. The Board would also like to see graphics that conveyed a sense of the experience of the building and its*

DESIGN GUIDELINES: D

07.15.2013 RECOMMENDATION MEETING PRESENTATION  
4717 BROOKLYN AVE NE / DPD PROJECT # 3014328





SECTION AT GARAGE ENTRY



STREET LEVEL USES PROPOSED

## DESIGN GUIDELINES: E & REQUESTED DEPARTURES

07.15.2013 RECOMMENDATION MEETING PRESENTATION  
4717 BROOKLYN AVE NE / DPD PROJECT # 3014328

## E-2 Landscaping to Enhance the Building and/or Site

Landscaping, including living plant material, special pavements, trellises, screen walls, planters, site furniture, and similar features should be appropriately incorporated into the design to enhance the project.

*EDG Board Comment: The Board felt that enhancing the street front along Brooklyn was particularly important. A landscape plan should also convey details of the rooftop amenity space as well as shown how the buildings north and south edges addressed the abutting neighbors of this in-fill building.*

The street trees and limited ground level landscaping at the curb on Brooklyn Avenue will be installed to enhance and soften the transition from the building to the public right of way. Limited landscaping islands versus continuous planting strip enhances commercial use and access to curbside parallel parking. Outdoor amenity spaces will be provided at both the second and roof levels. Both spaces will have planting areas, hardscape and seating for the residents use. Also, the building has been stepped in at the center of each north and south property line elevation in order to allow light infiltration.

## Departure #1: Driveway slope for all uses. (23.54.030.D.3)

No portion of the driveway shall exceed a slope of 15 percent, except as provided in subsection 23.54.030.D.3.

**Departure Requested:** Allow a 20% maximum driveway slope with 10% crest and sag.

The steeper ramp allows for the ground floor commercial space to be a minimum of 30' deep for a more functional use of space.

## Departure #2: Street-level Uses. (SMC 23.47A.005)

Residential uses may occupy no more than 20% of the street-level street-facing facade.

**Departure Requested:** Allow for 70.5% of the Brooklyn Avenue NE facade to be utilized for commercial use. It is proposed that a two-story workout residential amenity space be provided that would utilize another 14% of the street-facing facade. This would make approx. 84.5% of the street facing facade function as active space and meets the intent of the requirement.