

GUIDING PRINCIPLES

Establish a **sense of place** within the South Lake Union community

Create a street presence that **engages the pedestrian**

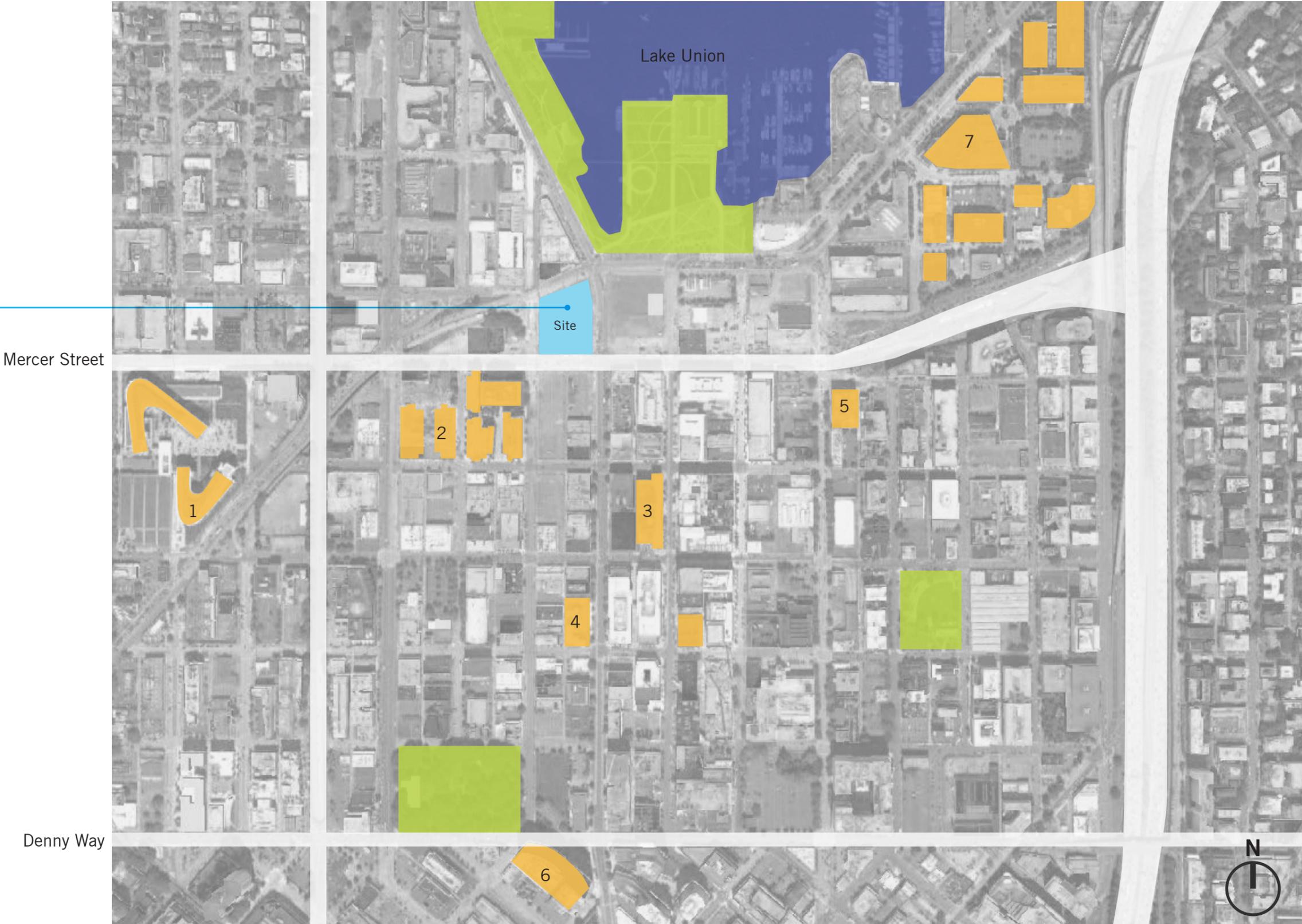
Respect the **neighborhood**

Focus on **sustainability**, with a target of LEED Gold

Thoughtfully accommodate **open space**

Respect and engage the site's existing **landmark structures**

Grow the **Health Sciences Community** in South Lake Union



The development is designed to accommodate a highly unique research program for the Allen Institute for Brain Science. It will be a valuable addition to the research community currently growing in South Lake Union. The basic program for the Allen Institute for Brain Science includes:

- 240,000 SF of research space
- Below-grade parking for 240 cars
- 8,000 SF of retail space
- The reassembled Pacific McKay and Ford McKay landmarks at the corner of Westlake and Mercer

1. Gates Foundation
2. UW Medicine
3. Institute for Systems Biology
4. Seattle Biomed
5. Novo Nordisk
6. PATH
7. Fred Hutchinson Cancer Research Center

CONCEPTS

TGlass clad laboratory volumes are unified by a single “container” expressed as a metal clad folded plane with a “virtual surface” on it’s western face (6).

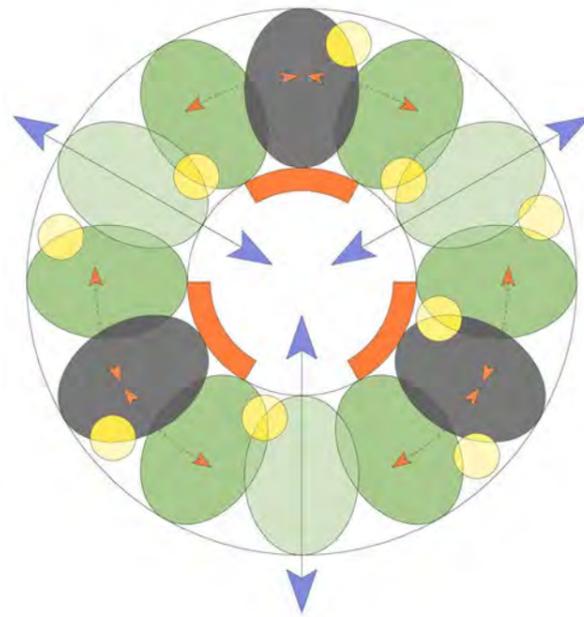
The three primary research lab ‘towers’ are expressed as distinct crystalline volumes (5).

Operable windows and shading devices provide additional texture and modulation with the introduction of perforated and woven metals onto the surface and embedded within the glazing system (4).

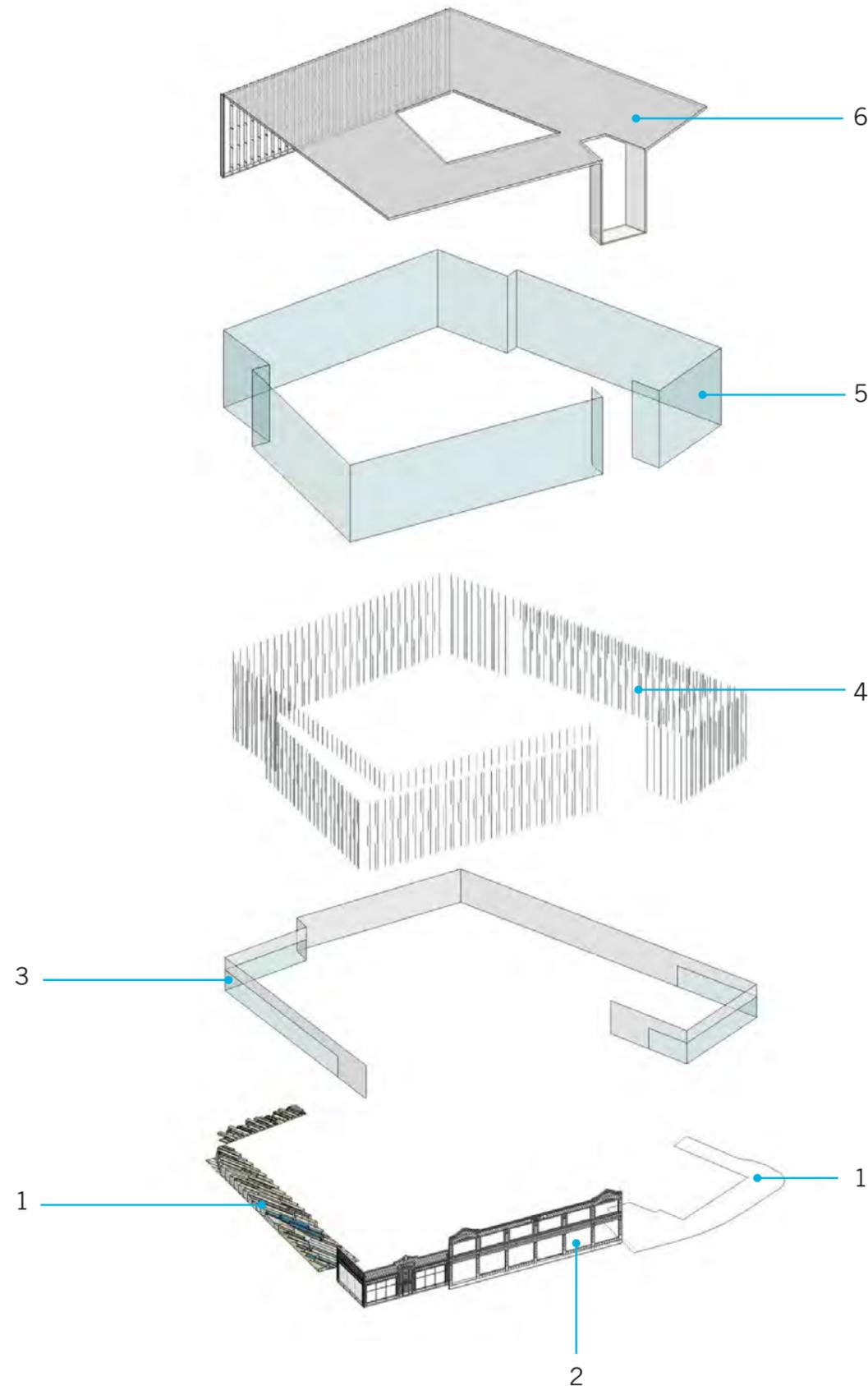
A podium clad in natural stone provides solidity and substance at the base of the building, establishing a scalar relationship to the reconstructed historic facades (3,2).

Significant public open spaces are provided including two paved plazas and a planted landscape space with water feature (1).

1. Public space
2. Historic facades
3. Podium
4. Modulated skin
5. Glass volumes
6. Container



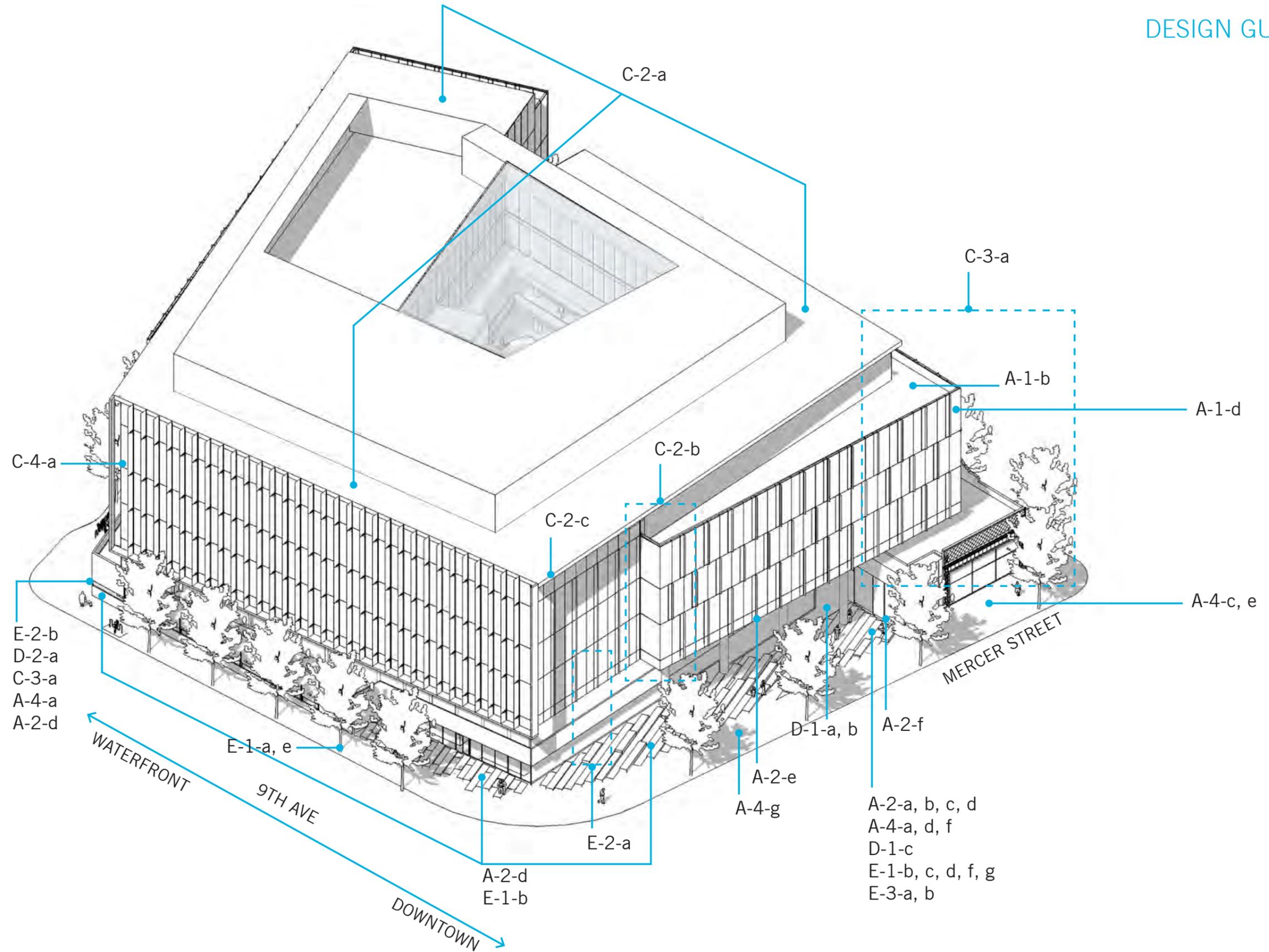
The petal diagram describes the conceptual relationship between interior program areas of the building. The connectivity between spaces, as well as their relationship to both the interior atrium and exterior views is a priority.



SITE CONTEXT



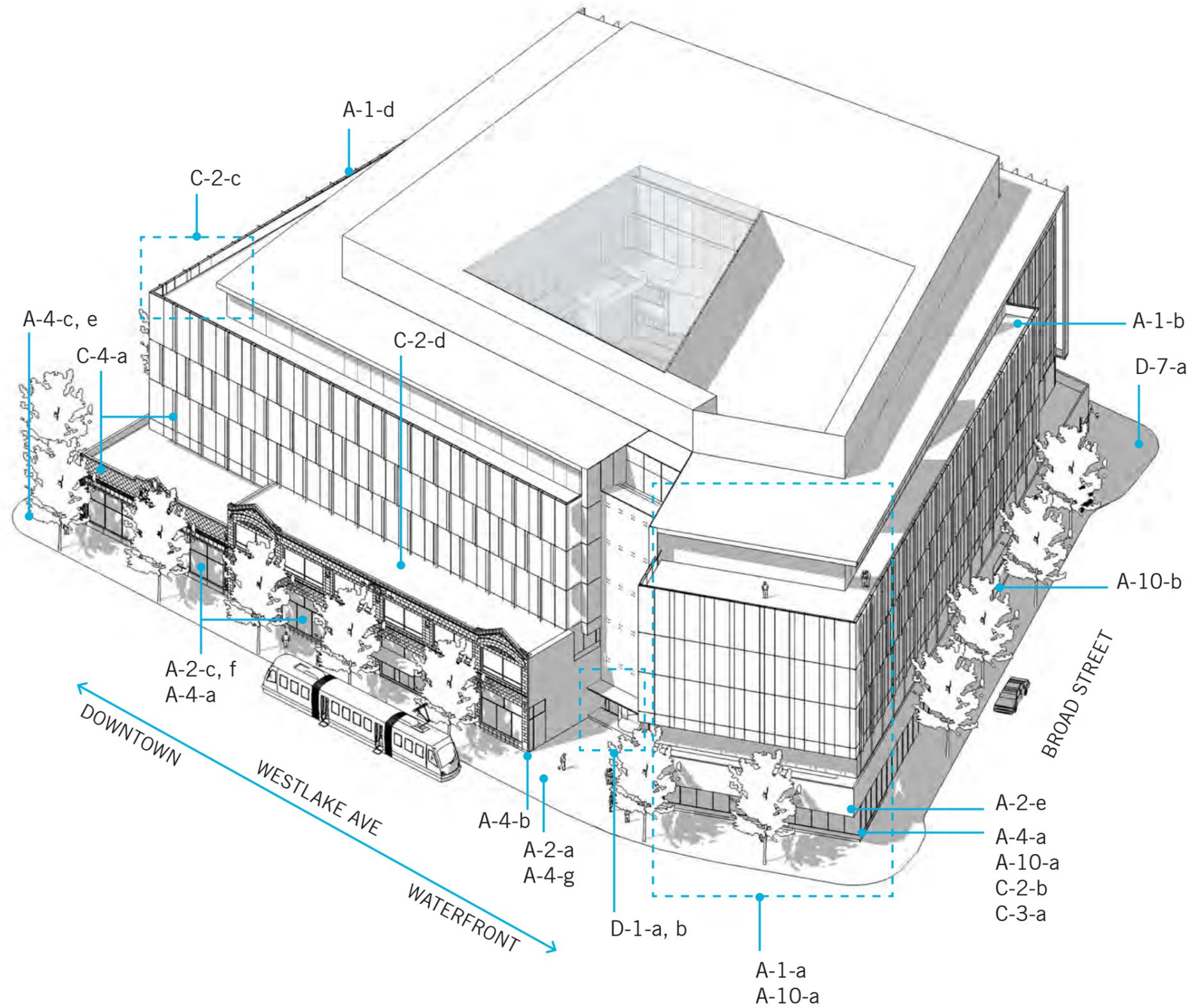
- 1. Project site
- 2. Block 44
- 3. UW Medicine
- 4. Lake Union Park

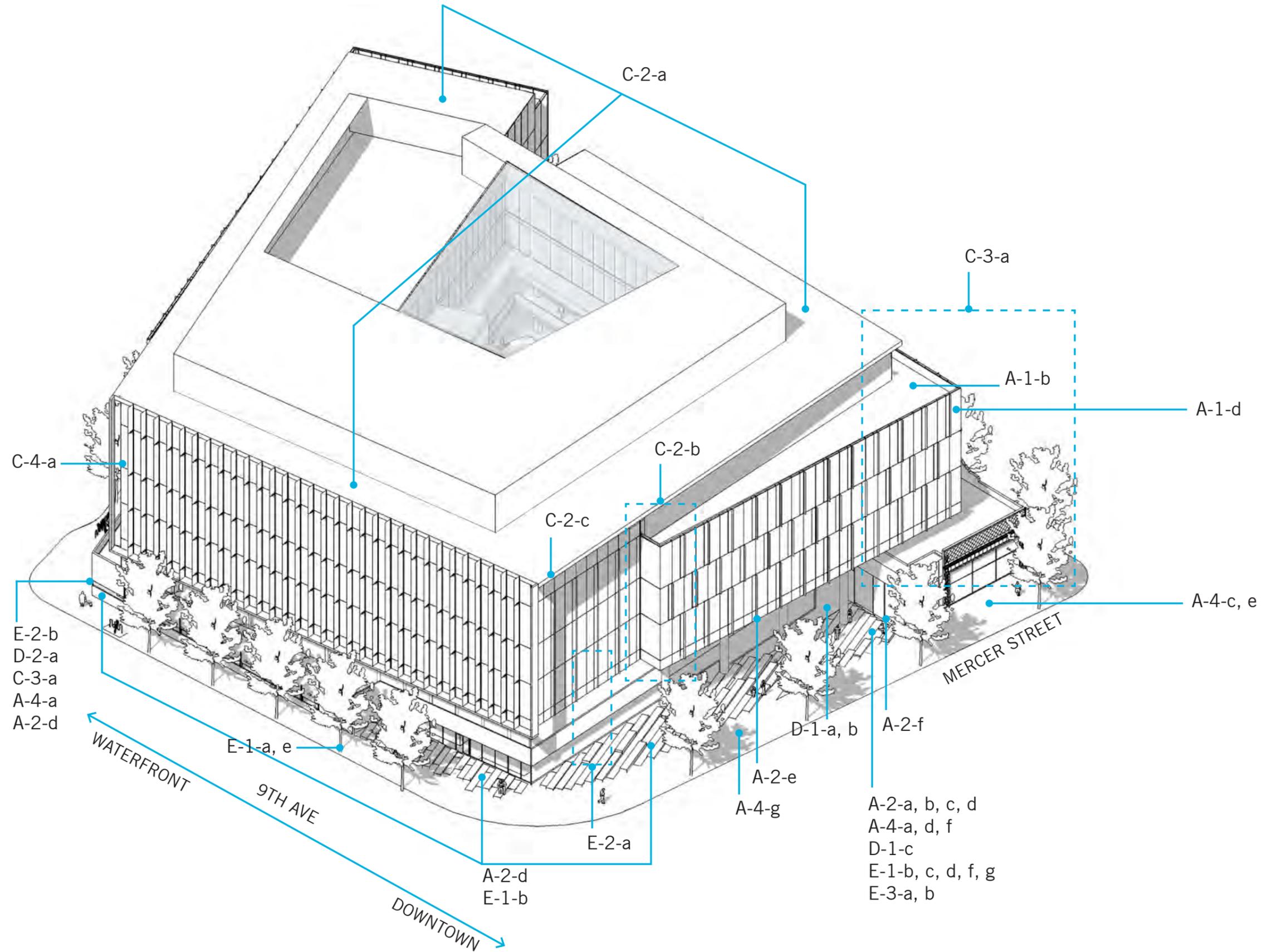


DESIGN GUIDELINES

The Design Review Board's Early Design Guidance has been carefully implemented by the design team to create a holistic and responsive building and active pedestrian environment.

The guidelines listed were identified as design priorities at the EDG meeting on December 19th, 2012 and call out key elements of the project's response.





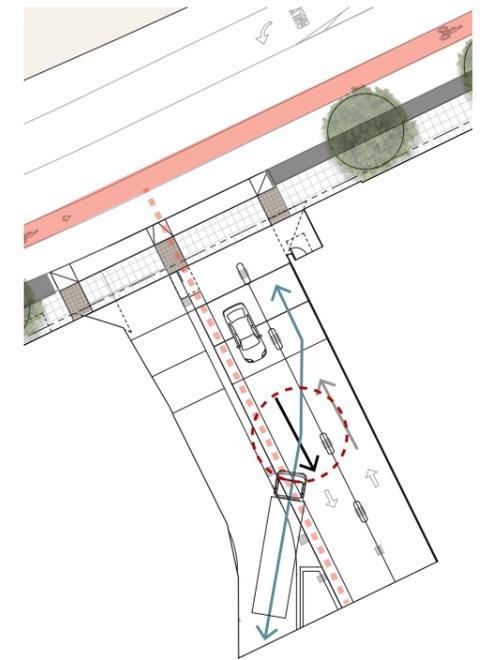
GROUND LEVEL

The ground floor creates connections to the community and integrates the historic Pacific and Ford McKay buildings. Exterior public spaces create an institutional presence and provide engagement opportunities for the community, including the auditorium and retail spaces, entry plaza, and south facing public landscape and plaza.



1. Entry plaza
2. Retail, Ford McKay
3. Retail, Pacific McKay
4. Lobby
5. Auditorium
6. Atrium
7. Electron microscopy
8. Secure research space
9. Landscape
10. Water feature
11. Public plaza
12. Media wall
13. Loading and parking ramp
14. Bike share station
15. Bike racks

SITE ACCESS



Truck/vehicle conflict with single curb cut scenario.

- Bike lanes
- - - Bike access route
- Bike rack location
- Bike share station
- - - Pedestrian routes
- ↔ Truck in/out
- ← Vehicle in
- Vehicle out
- Secured egress only
- ▲ Retail entry
- ← Institute entry
- ✱ Bike Fix It Station



Bike Share Station, Melbourne



Bike Share Station, Montreal



Bike Share Station, Washington, DC



Bike Rack



Bike Rack



Bike Fix-it Station

LEVEL 2



- 1. Laboratory
- 2. Office
- 3. Workstations
- 4. Tech work
- 5. Collaboration pod
- 6. Open atrium
- 7. Conference center
- 8. Roof of Pacific McKay
- 9. Roof of Podium

TYPICAL LEVEL (3-5)

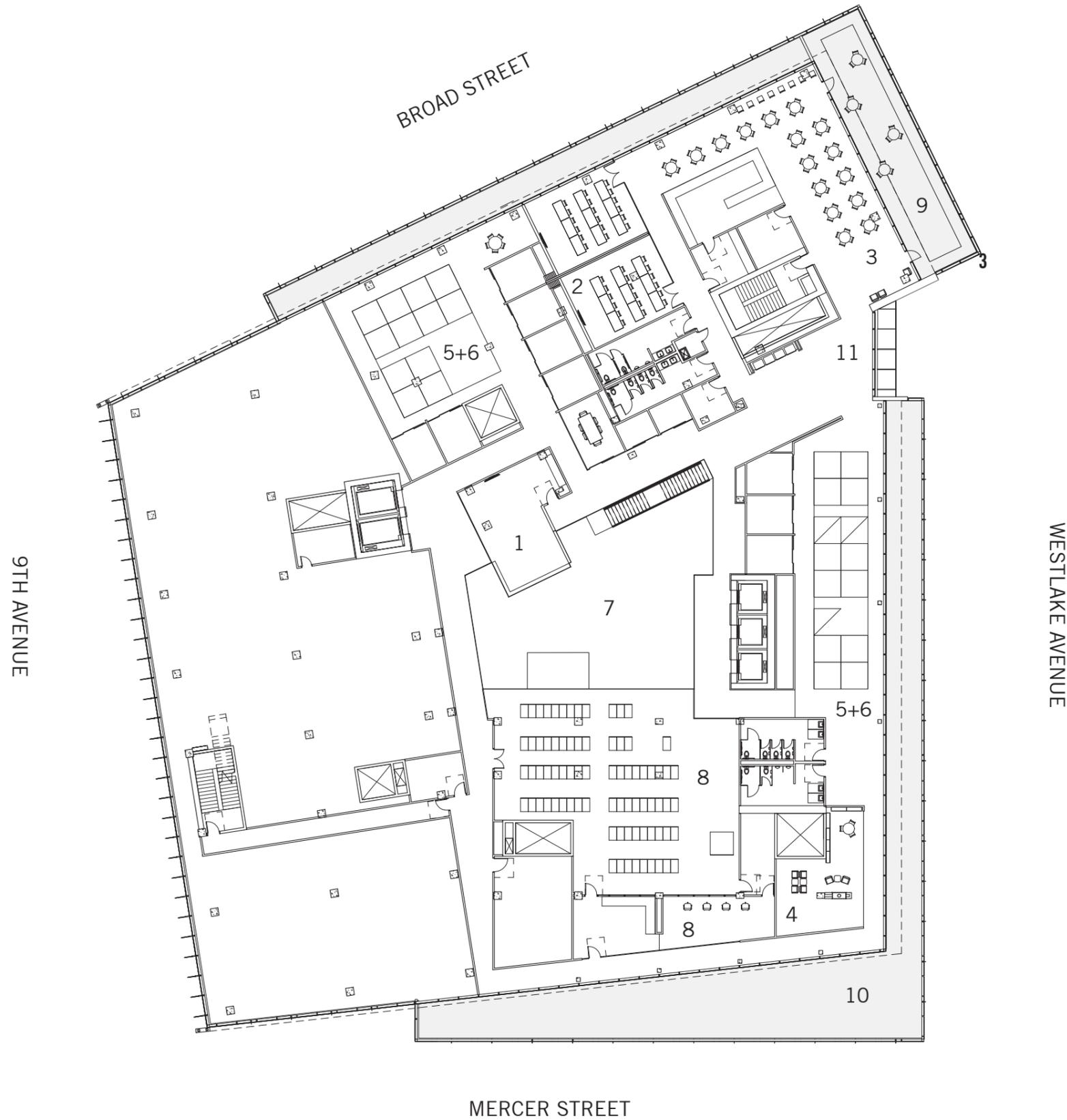
The typical levels of the Institute are designed to maximize daylight, visibility of the research activities and opportunities for informal and formal collaboration.



- 1. Laboratory
- 2. Office
- 3. Workstations
- 4. Tech work
- 5. Collaboration pod
- 6. Open atrium
- 7. Conference center

LEVEL 6

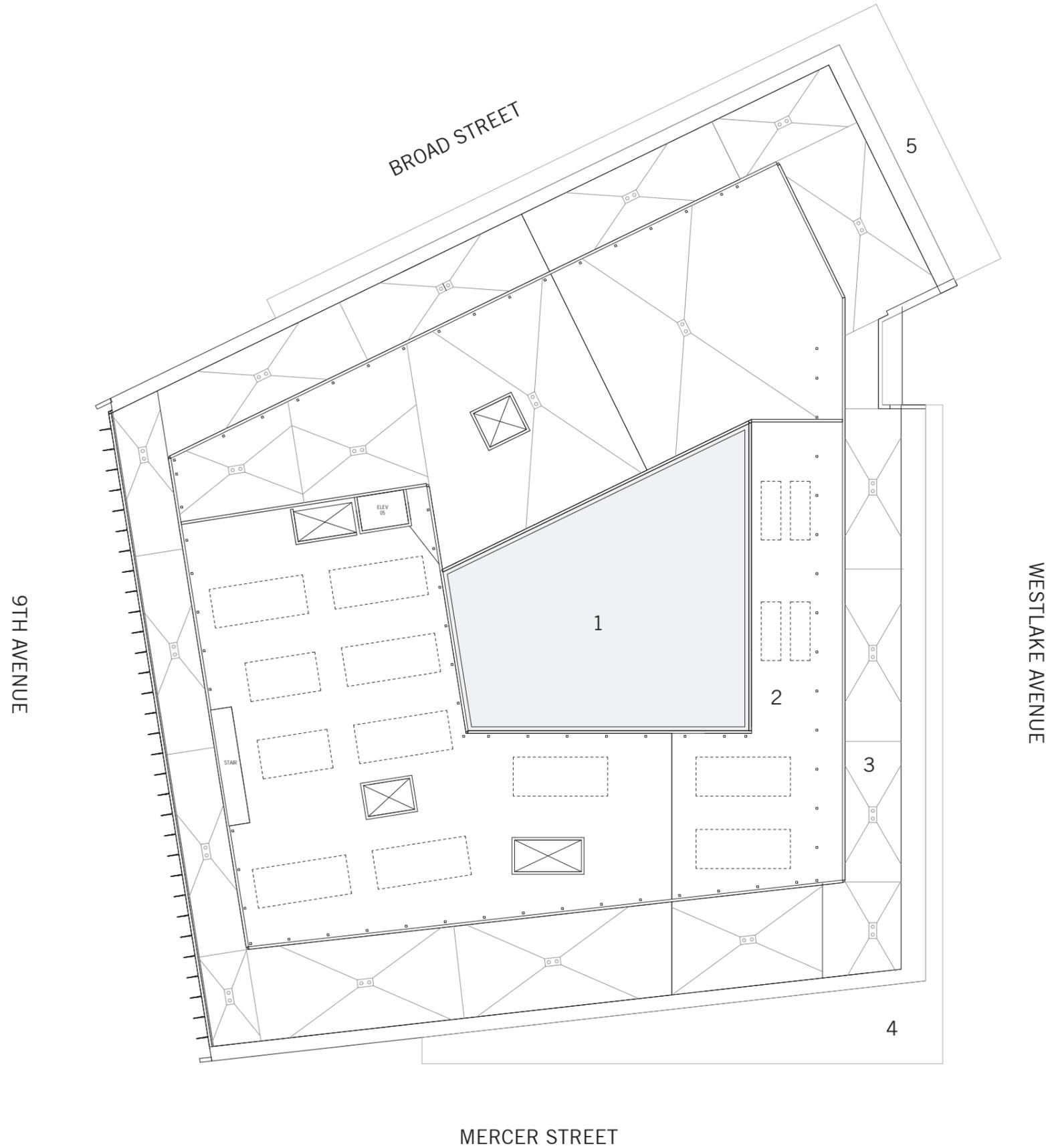
Level 6 is the gathering space for board meetings, training and education, social events, meals, access to the outdoors.



1. Board room
2. Training center
3. Café
4. Library
5. Office
6. Workstations
7. Open atrium
8. Data center
9. Roof terrace
10. Unoccupied roof
11. Skylight



UPPER ROOF LEVEL



- 1. Atrium skylight
- 2. Mechanical
- 3. Roof of Level 06
- 4. Unoccupied roof
- 5. Roof terrace below

LANDSCAPE PLAN

The landscape is designed to provide inhabitable areas of natural beauty and calm as well as an overall coherent sense of place through a unified palette of plant and hardscape materials. The compositional “grain” of the hardscape is derived from the Broad Street orientation and generally follows the topographic contour of the site which is a watershed in the direction of Lake Union. The scale and treatment of inhabited spaces take cues from the neighboring development plan while the sidewalk and street improvements provide continuity and consistency with Seattle standards for the neighborhood.



Block 44 Landscape Plan
(south of project site)



1. Permeable paving
2. Ground cover planting
3. Water feature
4. Specimen trees
5. Street trees
6. Planter strip
7. Bike share station
8. Terrace seating
9. Stone blocks
10. Public plaza
11. Entry plaza

PLANTING



Acer circinatum
Vine Maple



Carex morrowii 'Ice Dance'
Ice Dance Sedge



Carex grayii
Gray's Sedge



Mahonia aquifolium 'Nana'
Dwarf Oregon Grape



Lonicera pileata
Privet Honeysuckle



Panicum virgatum 'Shenandoah'
Switch Grass



Baccharis pilularis 'Twin Peaks'
Dwarf Coyote Brush



Calamagrostis acutiflora
Feather Reed Grass



Juncus patens 'Carmens Grey'
Grey Rush



Spirea betulifolia var. *lucida*
White Spirea



Helictotrichon sempervirens
Blue Oat Grass

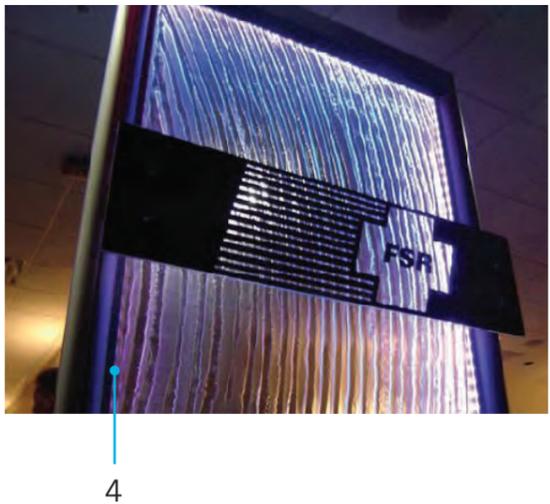
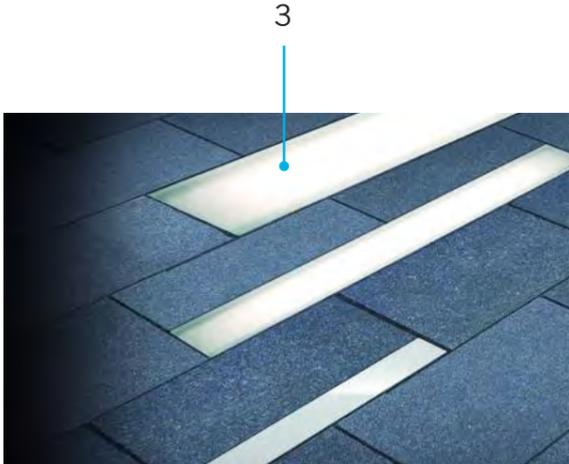
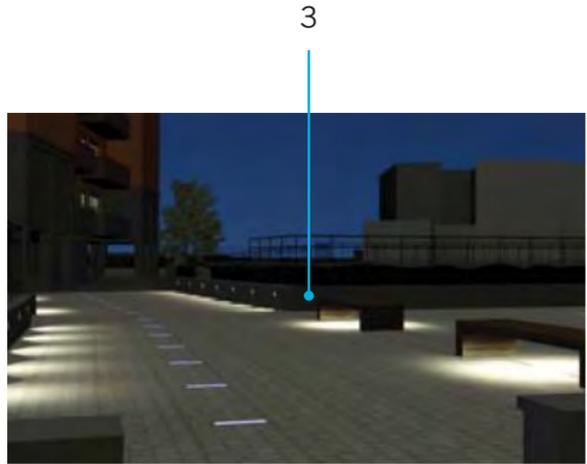
SITE LIGHTING

A variety of lighting solutions creates a well illuminated safe atmosphere throughout the site. Bollards act as small lanterns staggered along pedestrian paths for guidance, and mounted to the stone blocks in the plaza. Tenant provided lighting washes the plaza and terraced seating with soft light, and a unique fiber optic system highlights the water feature. Building entries are illuminated using soft uplighting, while egress lighting identifies building exits. Along 9th and Broad, the media wall provides a glowing light to both illuminate the path and create a strong pedestrian experience.



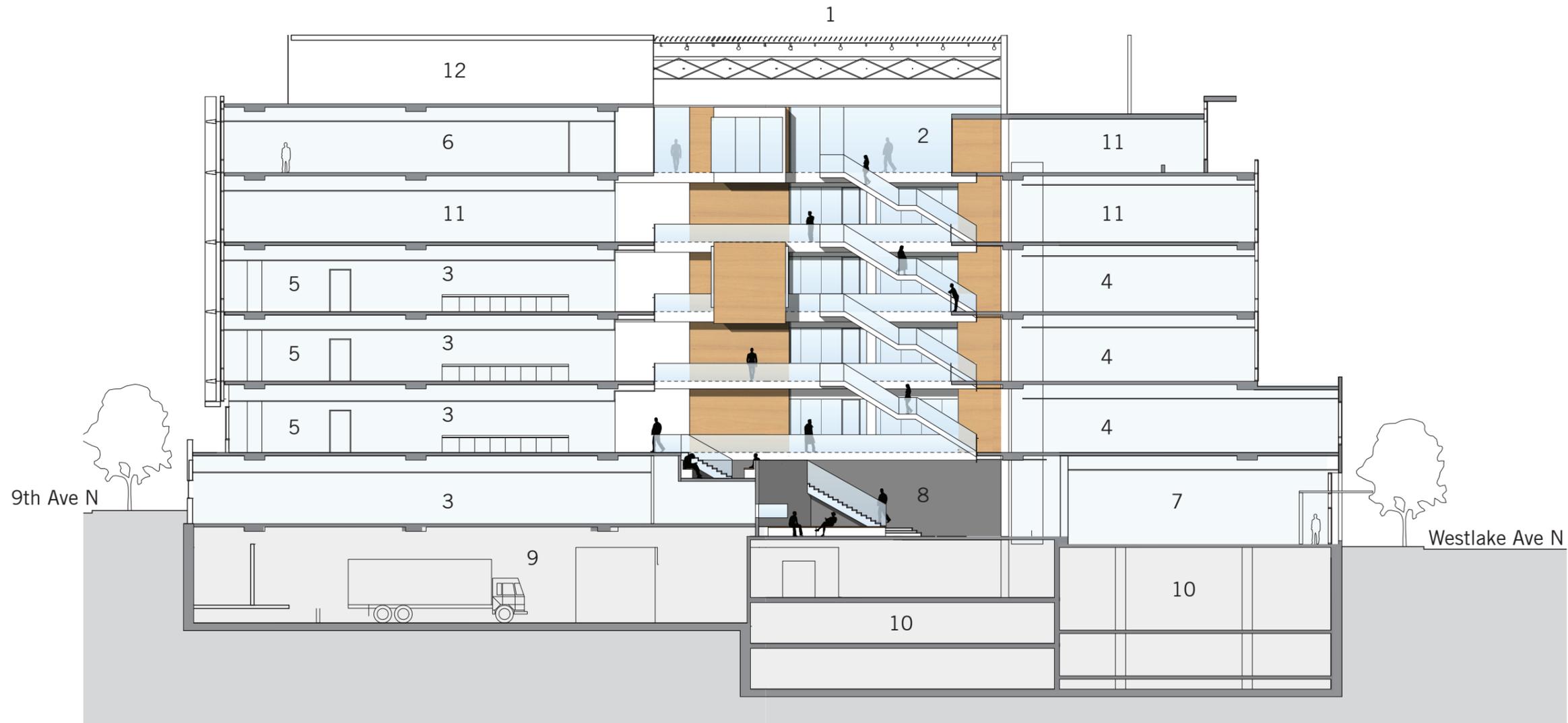
1. Bollards
2. Egress lighting
3. Linear uplighting
4. Fiber optic lighting
5. Tenant lighting

SITE LIGHTING

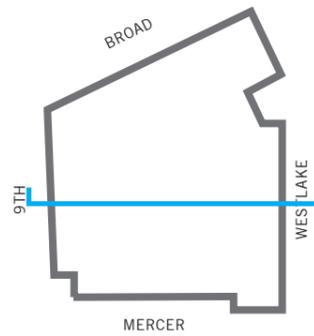


- 1. Bollards
- 2. Egress lighting
- 3. Linear uplighting
- 4. Fiber optic lighting
- 5. Tenant lighting

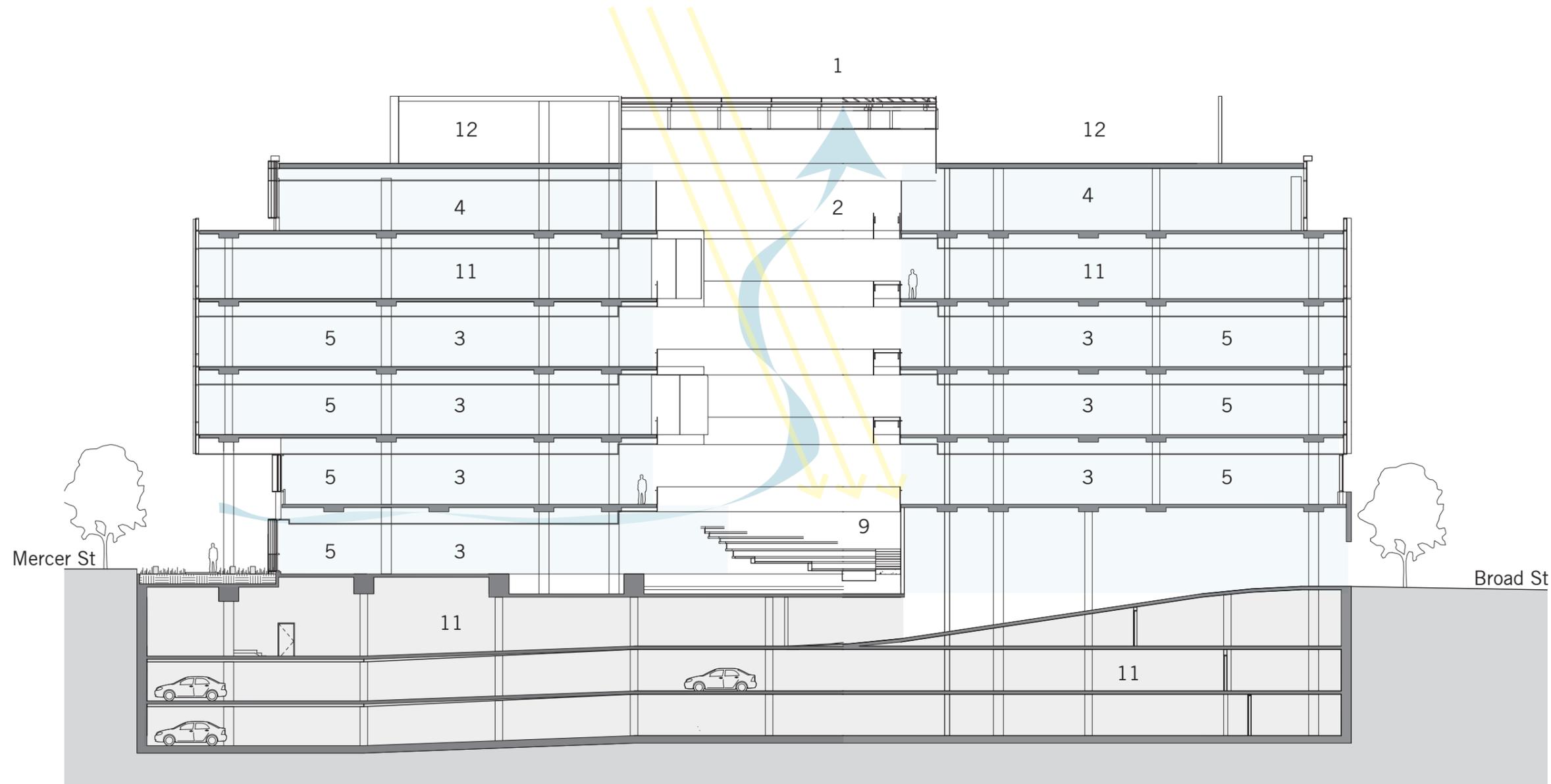
SECTION, EAST-WEST



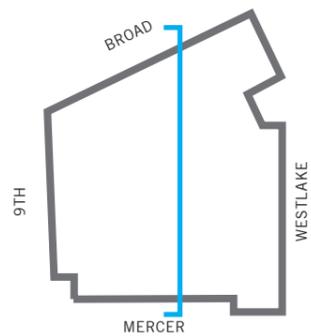
- 1. Skylight
- 2. Atrium
- 3. Laboratory
- 4. Workstations
- 5. Tech work area
- 6. Training center
- 7. Gen. Sales & Services
- 8. Terraced floor
- 9. Loading
- 10. Parking
- 11. Shell space
- 12. Mechanical



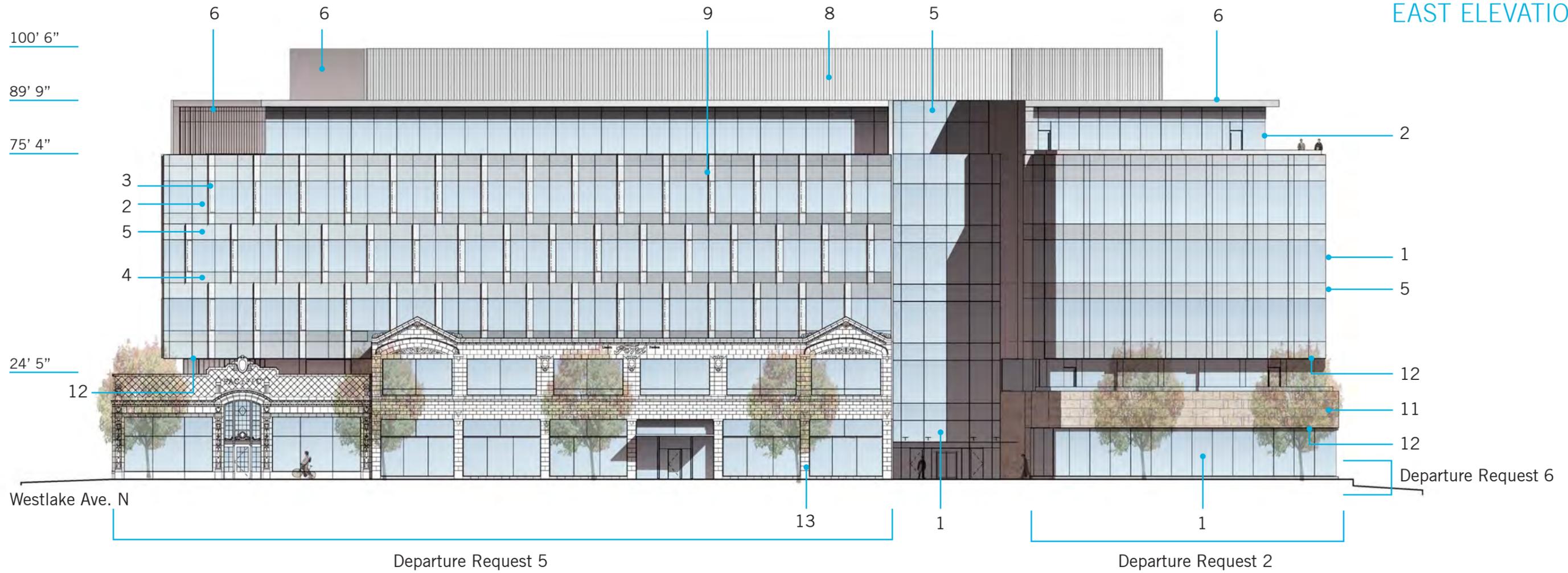
SECTION, NORTH-SOUTH



- 1. Skylight
- 2. Atrium
- 3. Laboratory
- 4. Workstations
- 5. Tech work area
- 6. Training center
- 7. Gen. Sales & Services
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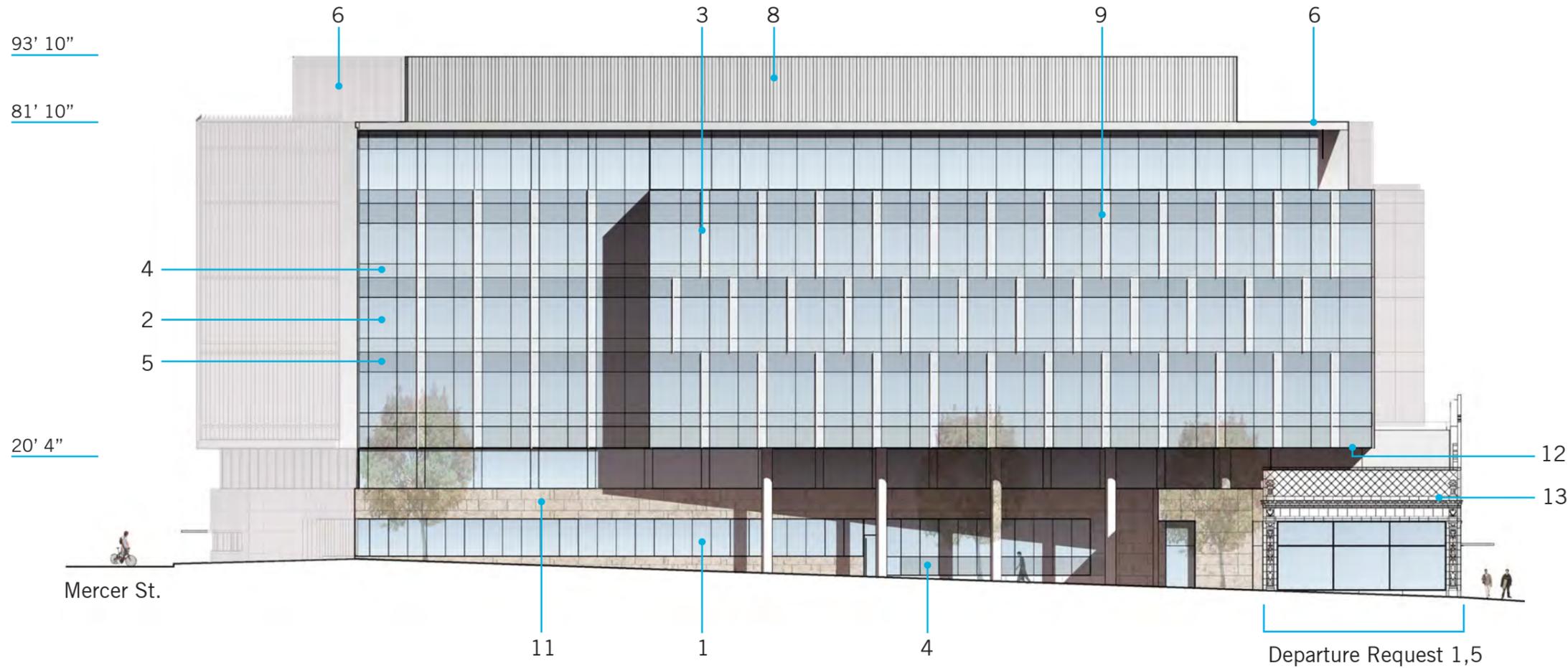
EAST ELEVATION



- 1. Vision Glazing
- 2. Vision Glazing with Line Frit
- 3. Okamesh Glazing
- 4. Shadow Box 1
- 5. Shadow Box 2
- 6. Metal Panel
- 7. Metal Screen
- 8. Perforated Metal Panel
- 9. Perforated Metal Fin
- 10. Perforated Metal Sunshade
- 11. Stone Cladding
- 12. Composite Wood Panel (at soffit)
- 13. Existing Terracotta Cladding
- 14. Media Wall

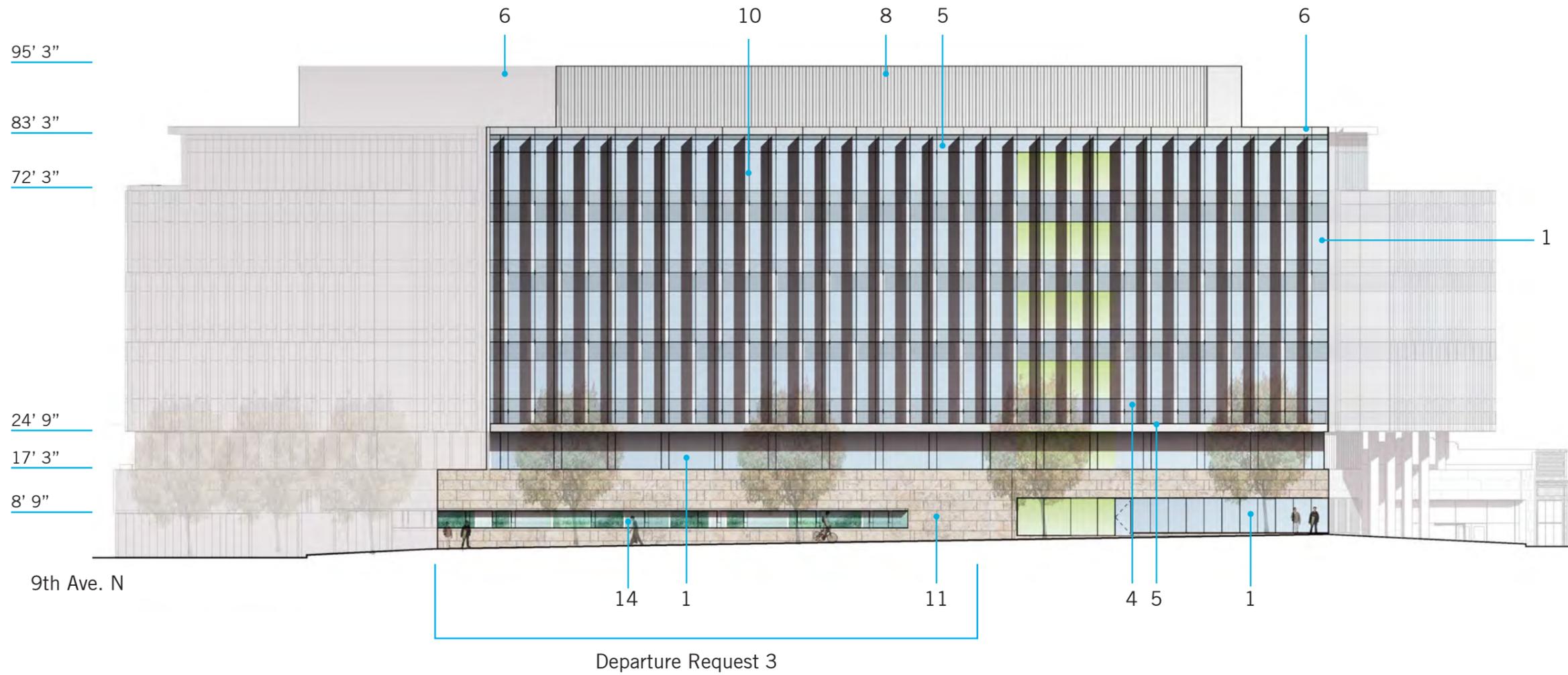


SOUTH ELEVATION



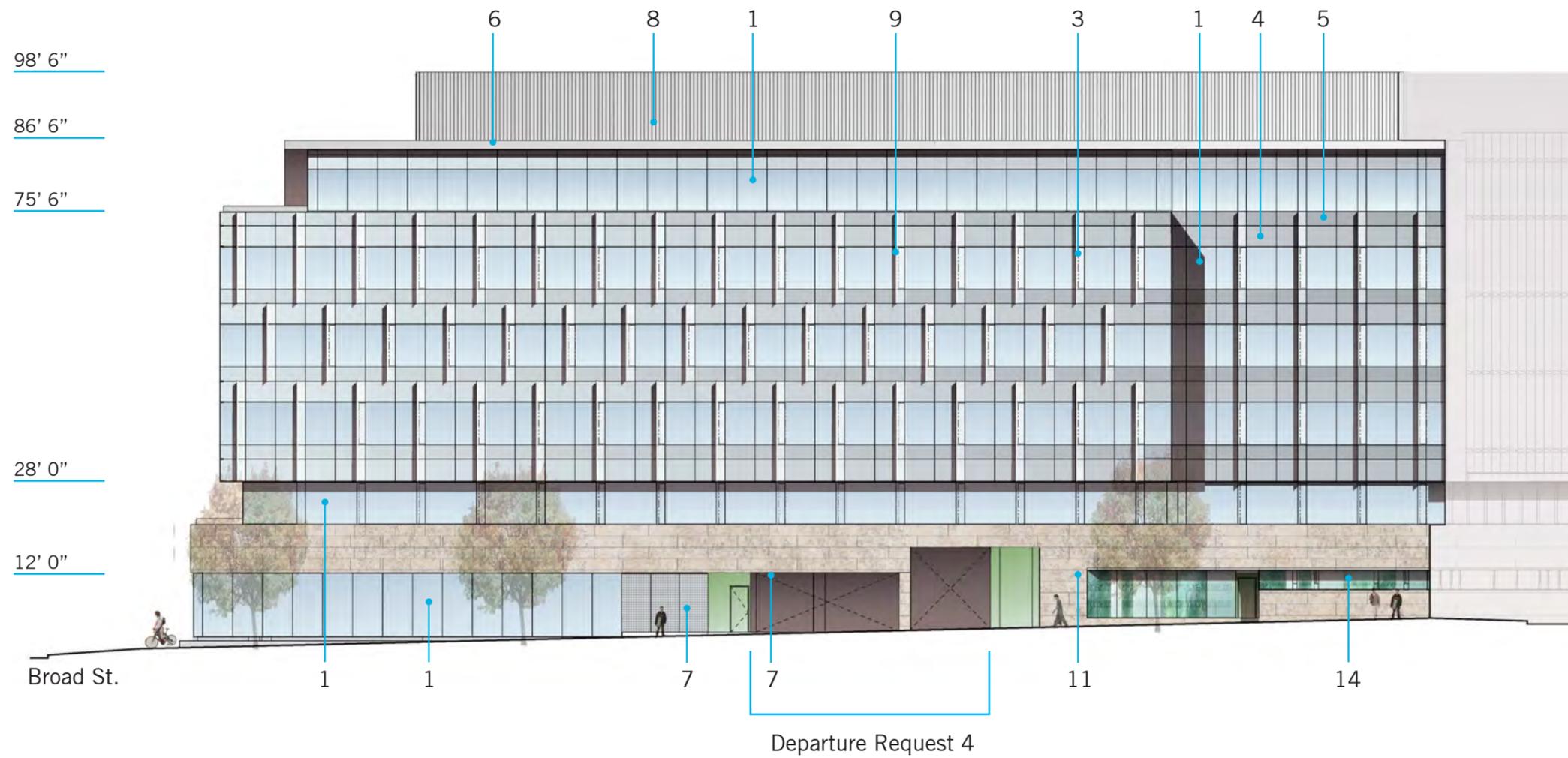
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- 14. Media Wall

WEST ELEVATION



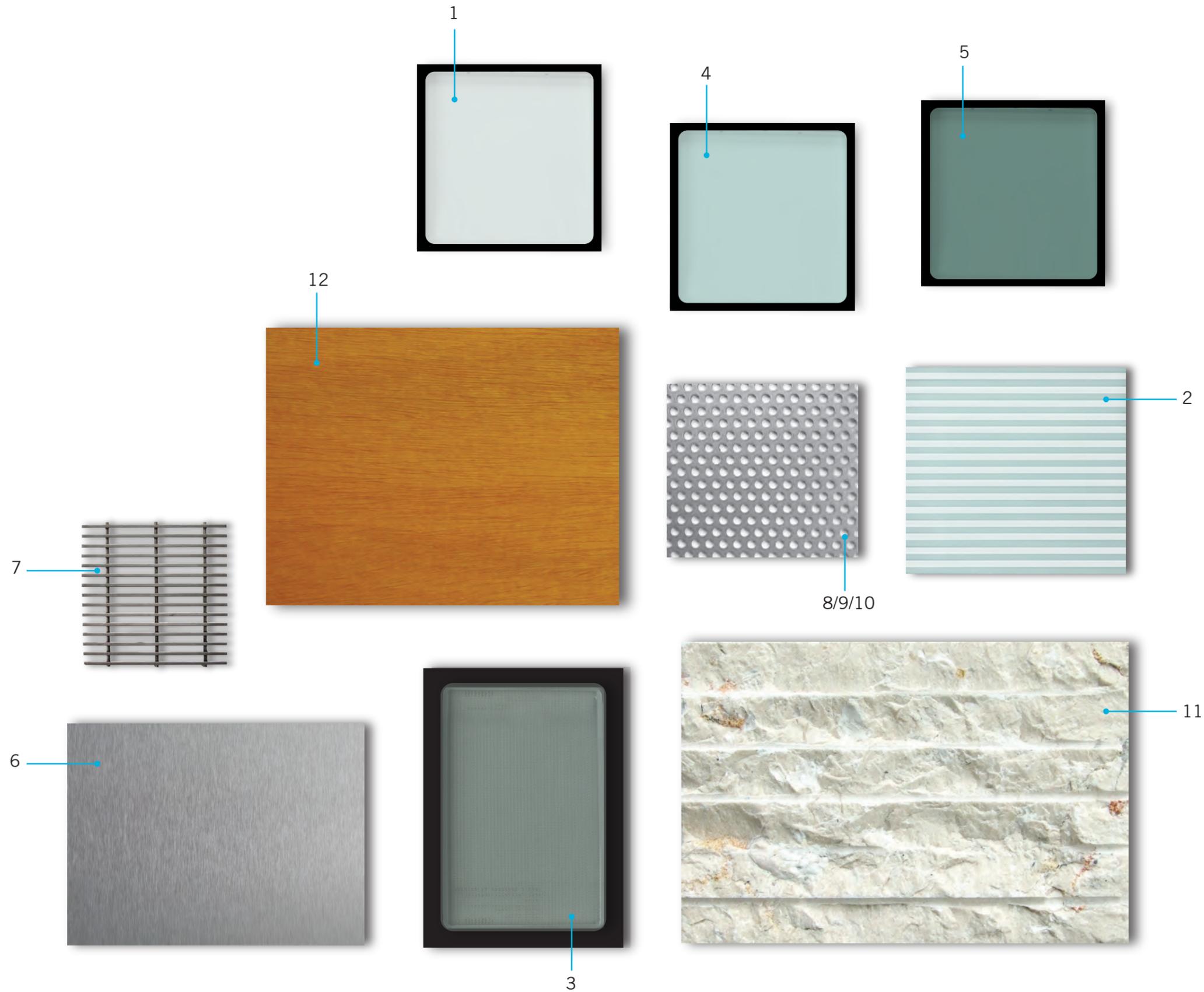
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- 9. Perforated Metal Fin
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- 11. Stone Cladding
- 12. Composite Wood Panel (soffit)
- 13. Existing Terracotta Cladding
- 14. Media Wall

NORTH ELEVATION



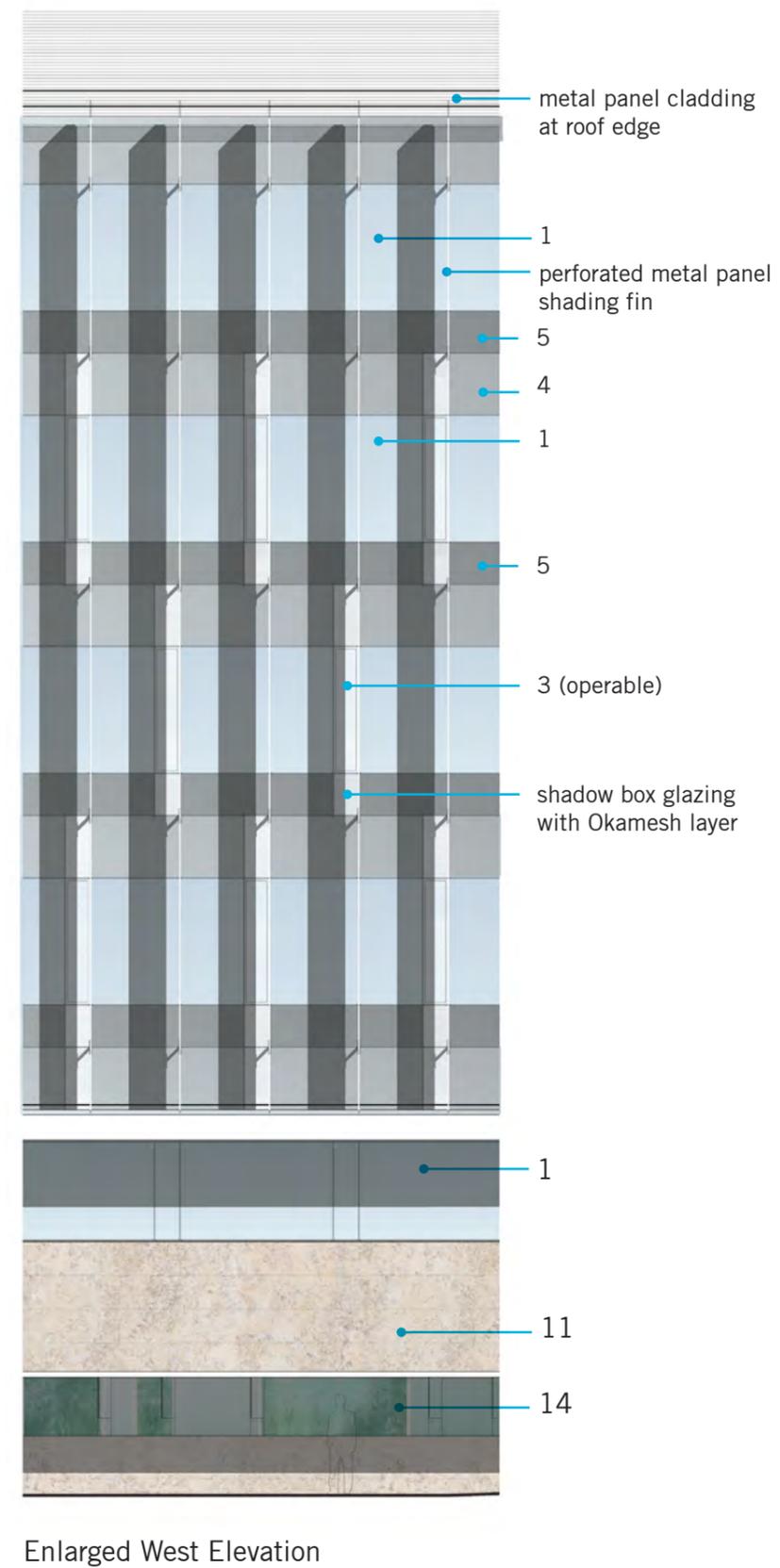
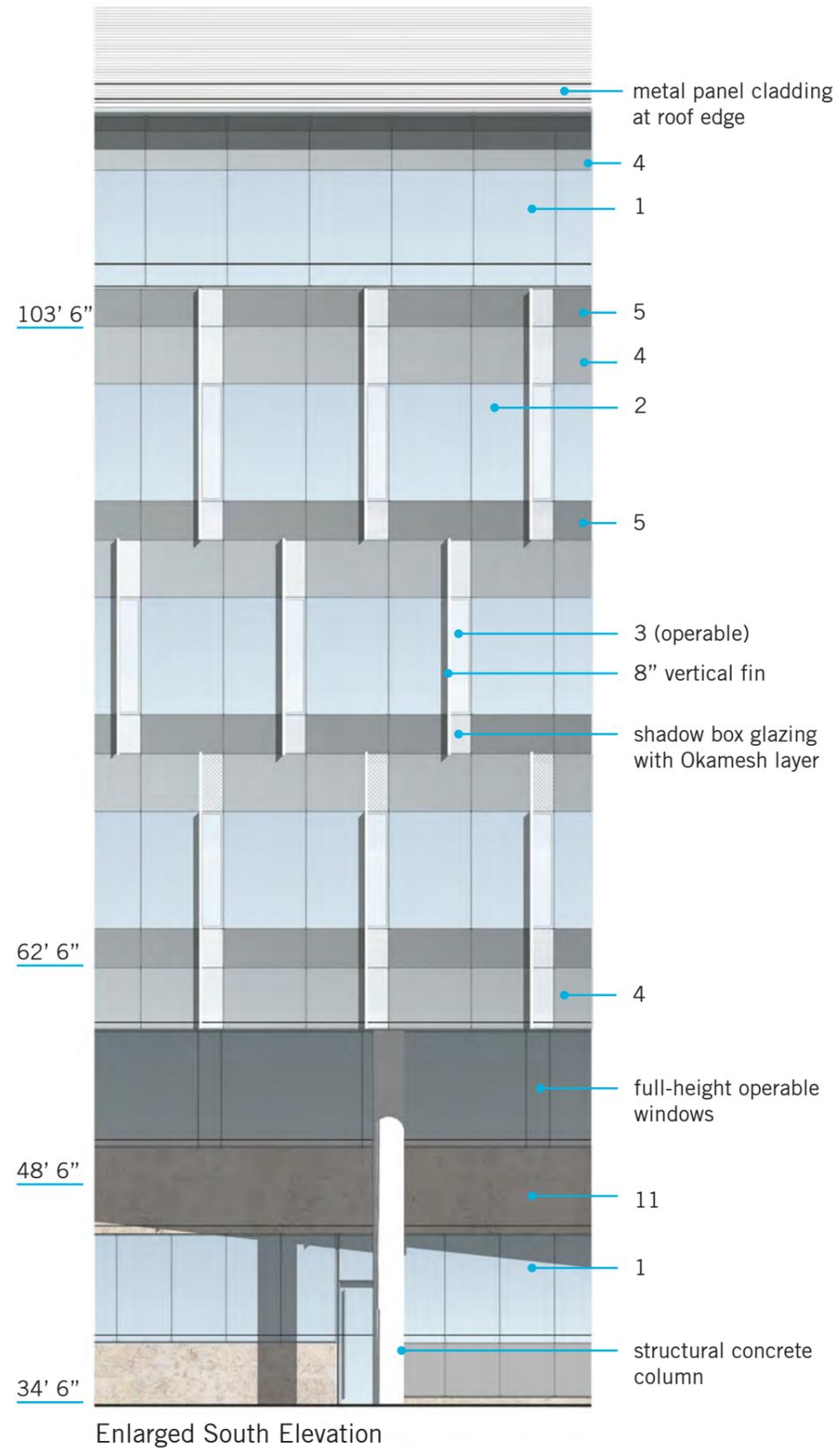
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EXTERIOR MATERIALS



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2. Vision Glazing with Line Frit
3. Okamesh Glazing
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EXTERIOR MATERIALS



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10. Perforated Metal Sunshade
11. Stone Cladding
12. Composite Wood Panel (at soffit)
13. Existing Terracotta Cladding
14. Media Wall

WESTLAKE AVE. N

Early Design Guidance:

The north edge of the Ford McKay building should thoughtfully bridge the gap between historic and new.

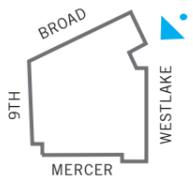
The new development should appear light and visually 'lifted' off the historic structure.

Strong horizontal banding is discouraged.

The folded 'ribbon' at either side of the entry is a strong element that could enhance vertical expression.



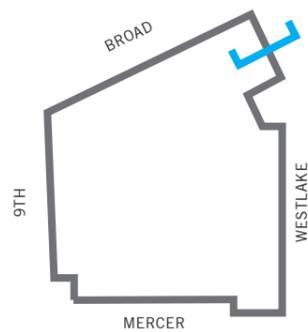
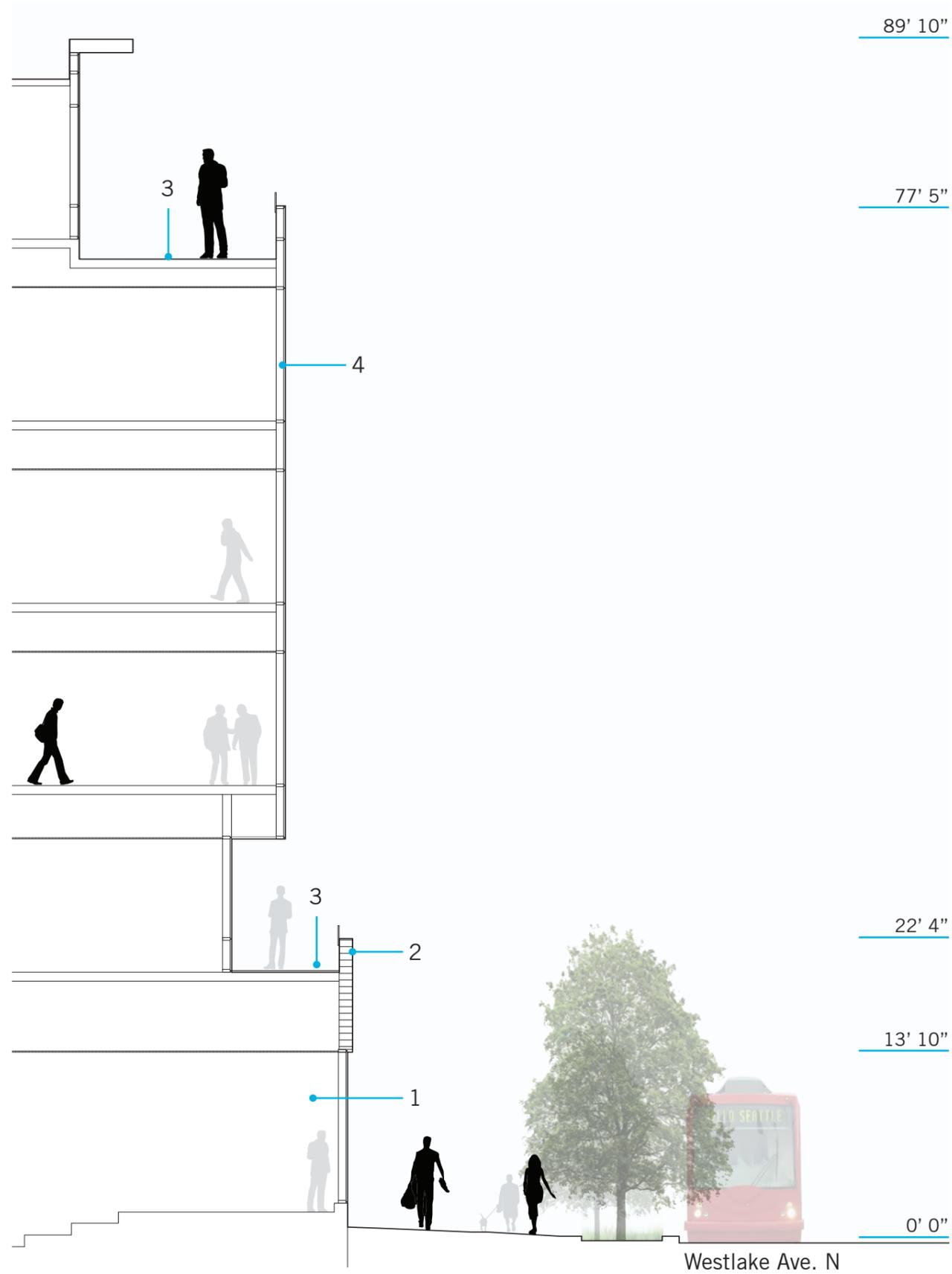
1. Entry plaza
2. Retail, Ford McKay
3. Retail, Pacific McKay
4. Lobby
5. Auditorium
6. Atrium
7. Electron microscopy
8. Loading and parking ramp
9. Bike racks
10. Bike share station







SECTION, WESTLAKE



- 1. Auditorium
- 2. Textured stone cladding
- 3. Roof terrace
- 4. Clear vision glazing

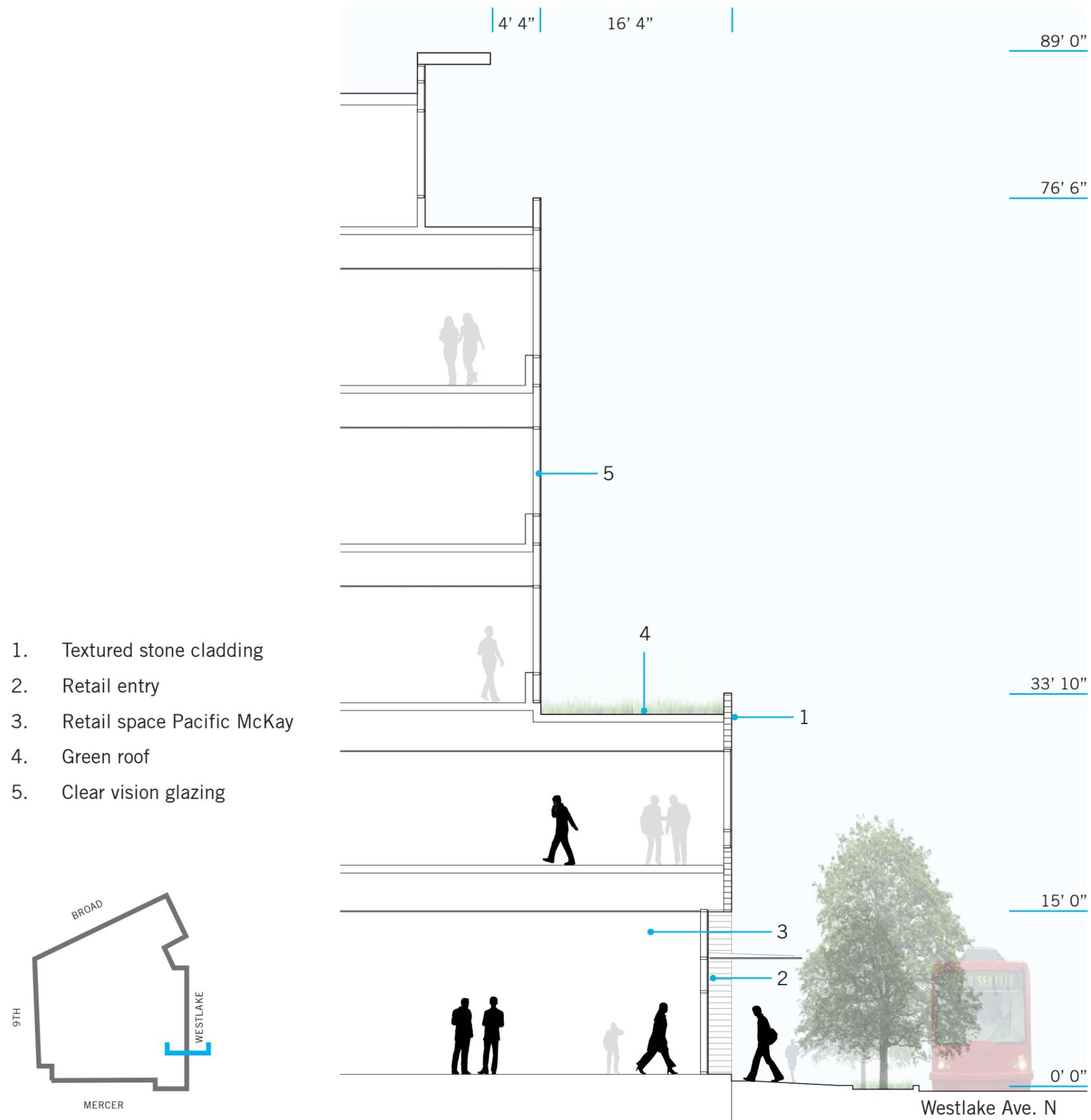
AUDITORIUM AT WESTLAKE AND BROAD



Westlake Ave. N

Broad St.

RELATION TO LANDMARKED FACADES



Pacific McKay and Ford McKay Buildings prior to deconstruction.

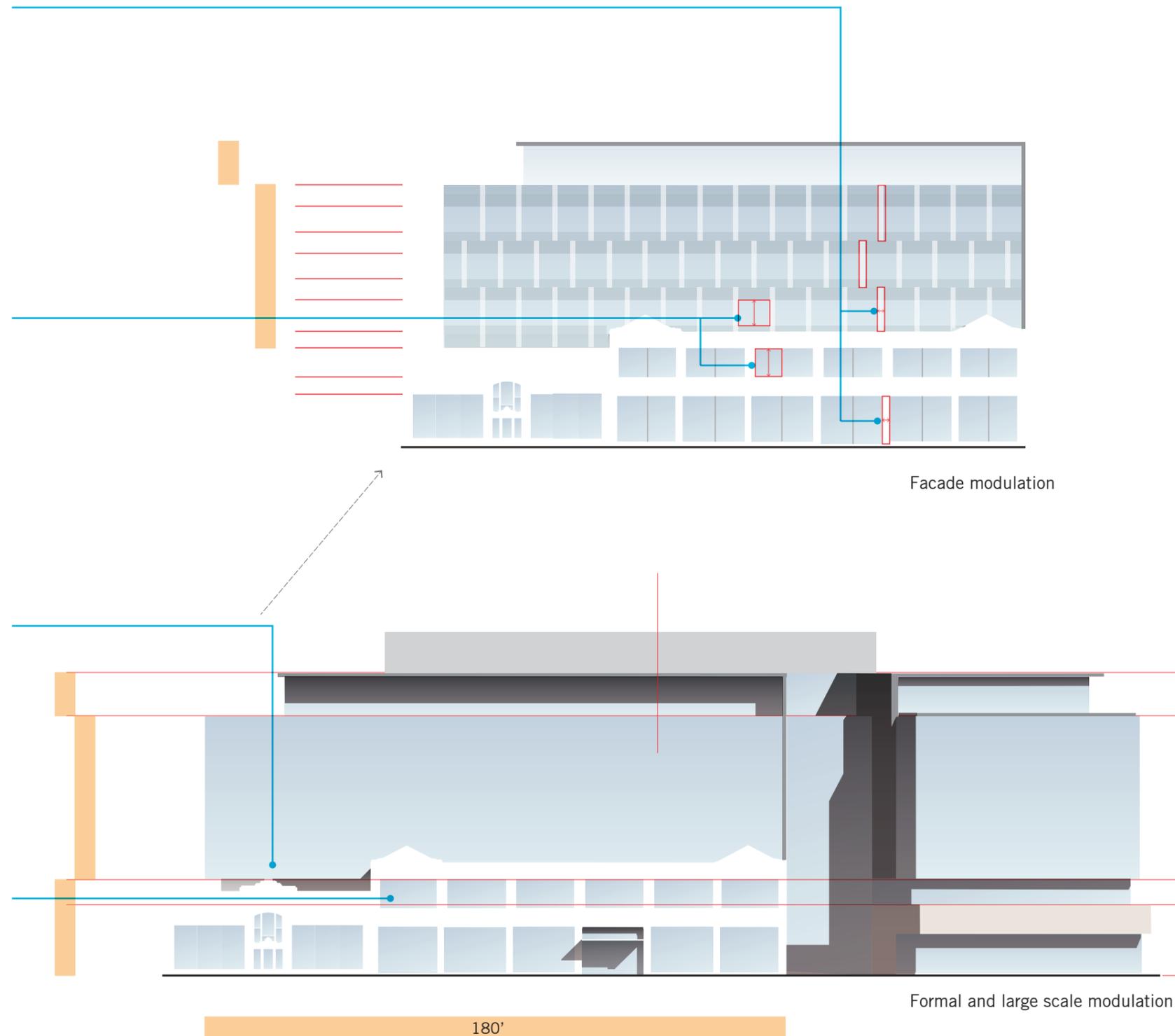
RELATION TO LANDMARKED FACADES

The vertical pilasters of the Ford McKay frame are reinterpreted as vertical bands of stainless steel mesh within the glazing system of the Institute. These bands repeat the scale found in the historic façade and follow a module that is half module of the historic façade; this allows for the Institute's request for operable windows every ten feet.

The scale of the historic spandrel and vision zones is repeated in the Institute, providing a unifying vertical order. The Institute curtain wall provides a subtle impression of scale which is consistent with the historic façade, while also forming a sleek backdrop that allows the historic façade to be dominant in coloring and contrast.

The massing of the Institute volume is cantilevered such that at the southeast corner it literally 'floats' free of the Pacific McKay while being set back substantially and discretely engaging the Ford McKay along the Westlake façade.

The vertical scale of the second floor windows of the Ford McKay set a strong horizontal datum inscribed all around the second floor of the Institute as an inset ribbon window to allow the Institute volumes to appear to 'float' lightly above a stone podium that harmonizes in color, human scale, and material quality and to the historic facades.



The overall conceptual approach to the relationship to the landmarked buildings is consistent with the Board's guidance to maintain a 'light' and 'lifted' appearance to the Institute while taking scale and compositional clues and datums from the historic.

The ephemeral qualities of the stainless steel mesh within the glass at the Institute will provide a subtle and dynamic counterpoint to the static and opaque frame of the historic façade. The material dialogue between the buildings speaks to the evolution of exterior cladding technologies – two technological eras demonstrating different yet harmonious responses with the same purpose of maximizing light and views.

The vertical and horizontal scale and modulation of the Ford McKay are transcribed into the Institute in a manner that responds to and reinforces the scale of the historic façade while clearly differentiating new from old in accordance with Federal guidelines for additions to historic buildings.

MERCER STREET

Early Design Guidance:

The south plaza should be fully functional for building users as well as the public.

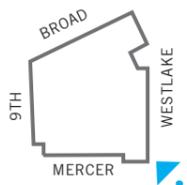
The plaza should enhance the connection between the Pacific McKay building and the 9th Ave N bike route.

The plaza should activate the street level by providing spaces that have good sun exposure as well as adequate weather protection.

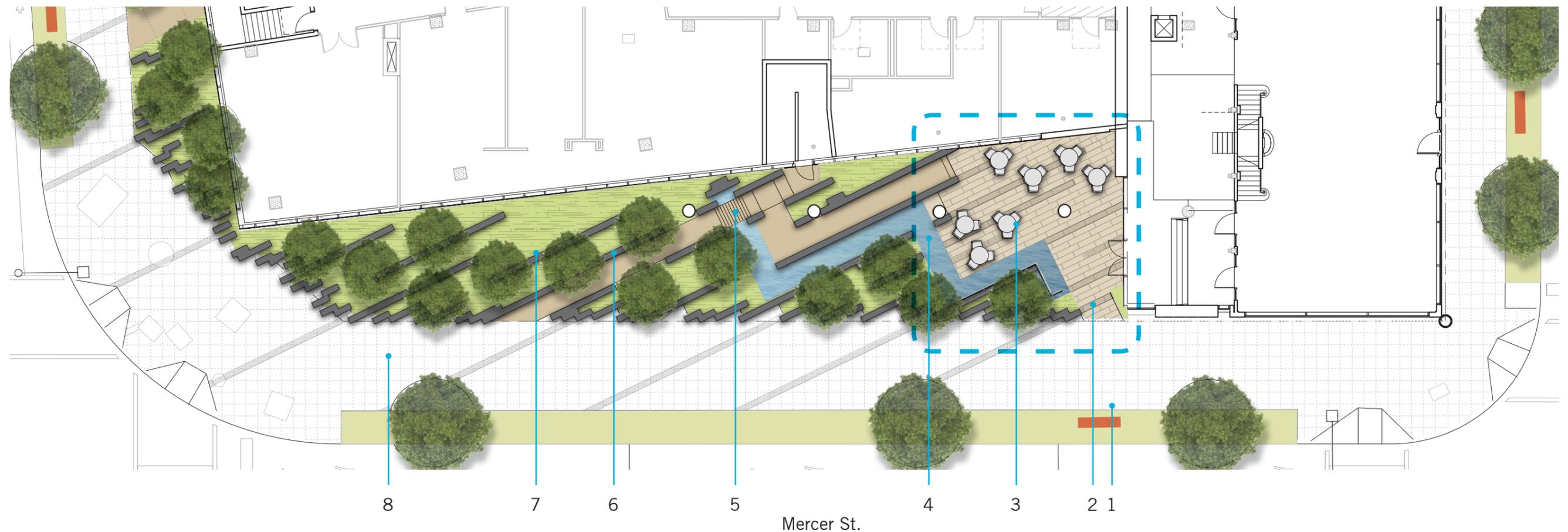
The landscape and hardscape should be consistent with the new development across the street (at Mercer).



1. Retail, Ford McKay
2. Retail, Pacific McKay
3. Atrium
4. Electron microscopy
5. Research space
6. Landscape
7. Water feature
8. Public plaza
9. Media wall
10. Bike racks

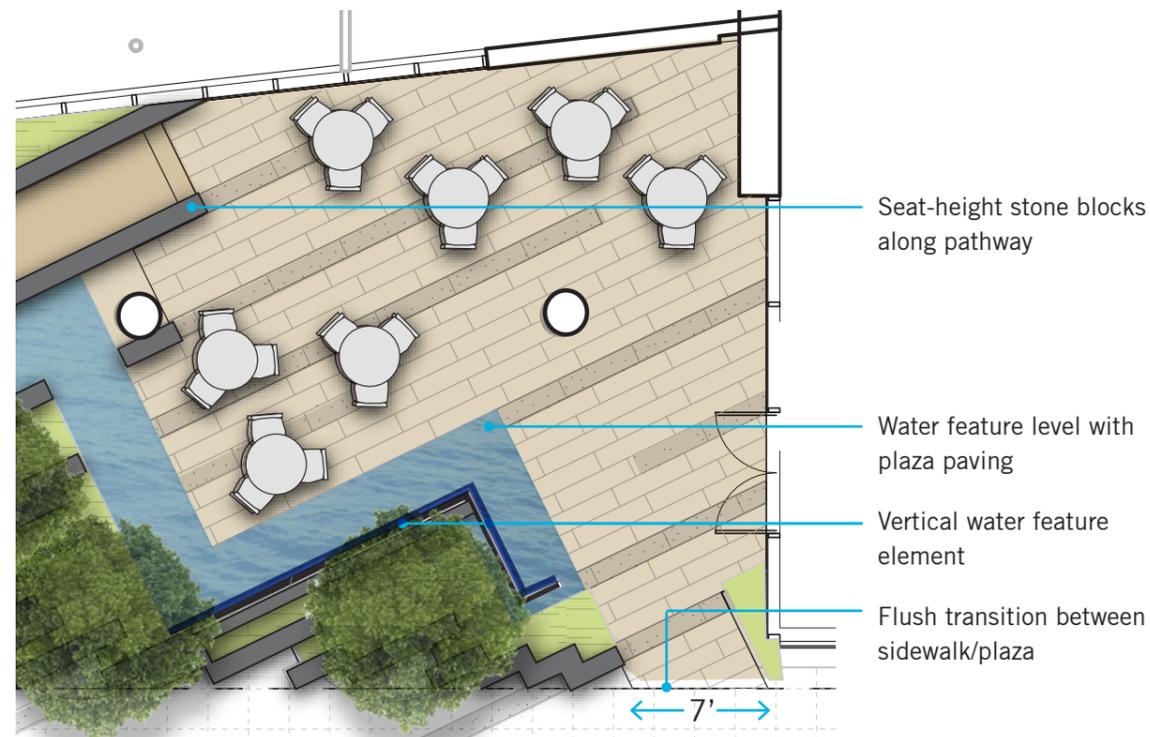


SOUTH PLAZA



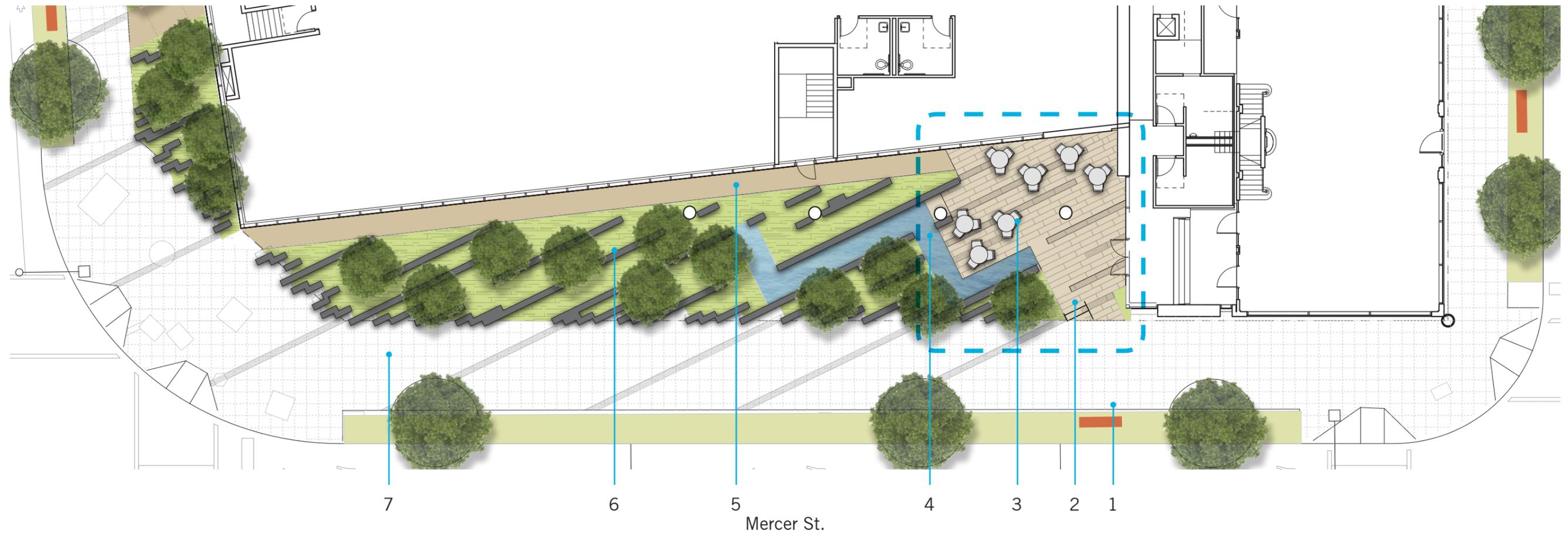
South Plaza Option 1 (preferred)

- Grain of landscape reads clearly and reinforces the design concept, which references the watershed line toward Lake Union.
- Plaza seating is supplemented with benches along the pathway, for pedestrians that would like to sit among the landscape.
- Pathway is well-integrated into the overall landscape design.
- Pedestrians have the option of accessing the plaza via the Mercer sidewalk or the quieter pathway.



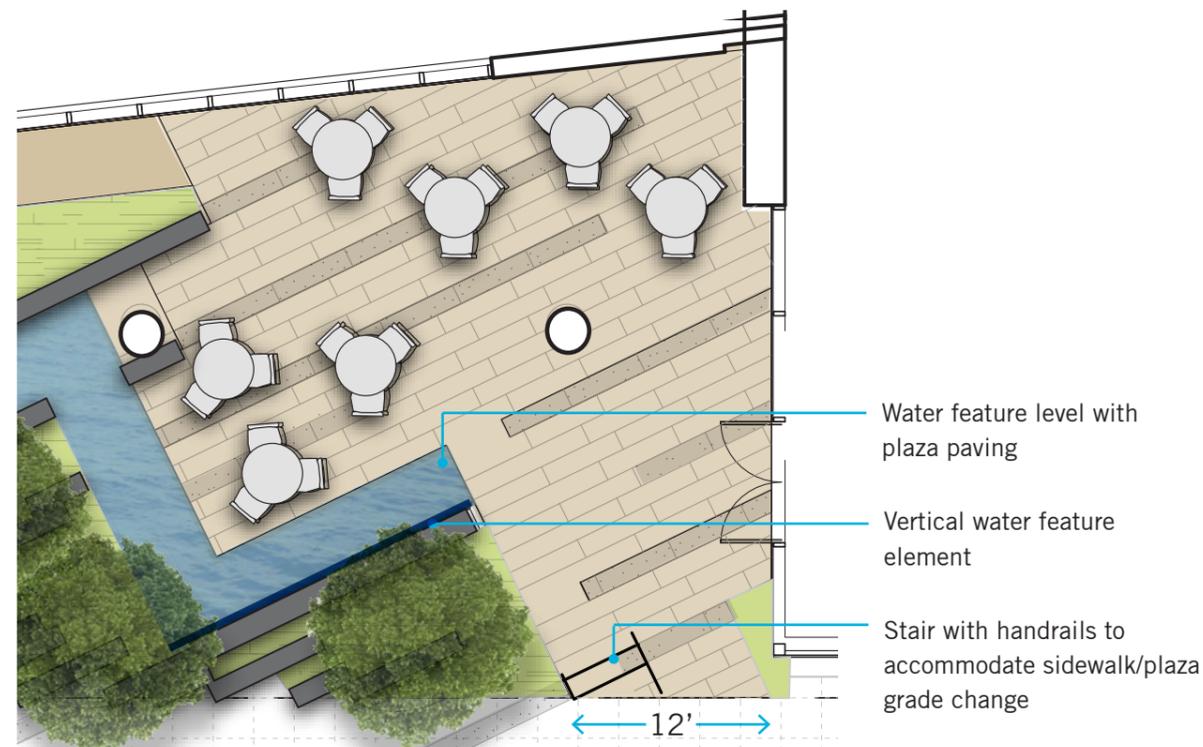
1. Bike racks
2. 7' wide plaza entry (no steps/handrails)
3. Main plaza seating
4. Water feature
5. Bridge over water feature
6. Stone benches at path
7. Stone site walls
8. 15' wide sidewalk

SOUTH PLAZA

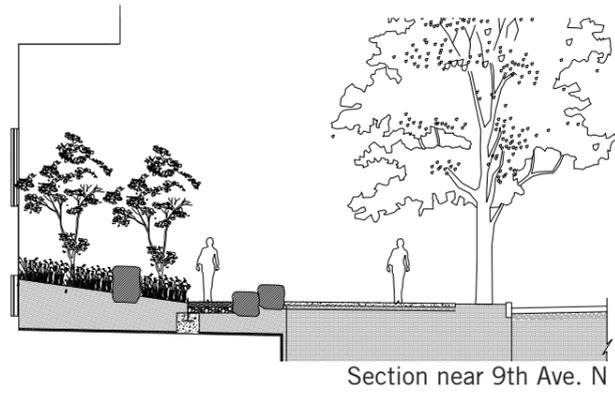


South Plaza Option 2

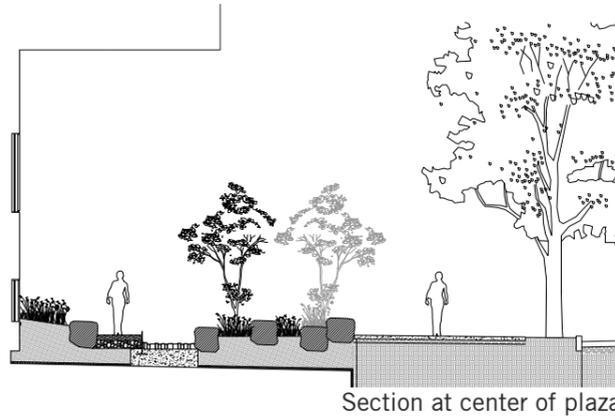
- Pathway provides a direct route from 9th Avenue N. to plaza.
- Plaza entry off of Mercer is wider, but necessitates steps and handrails at a portion of the entry to meet sidewalk grade.
- Opportunities for occupants to experience the landscape from the inside the building are limited due to “buffer” of pathway.
- Pathway does follow grain of landscape and limits opportunities to experience greenery and water up-close.



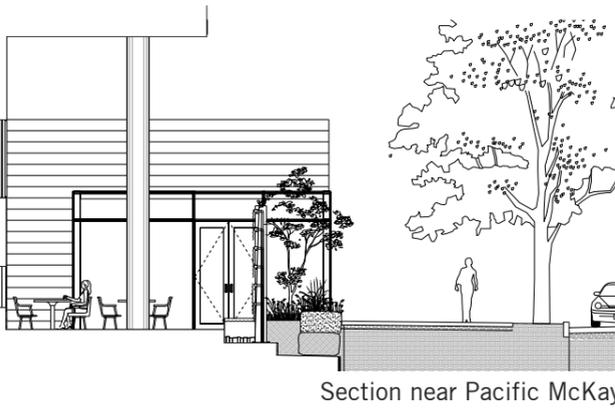
1. Bike racks
2. 12' wide plaza entry (steps/handrails at portion of path)
3. Main plaza seating
4. Water feature
5. Pathway to 9th Ave N.
6. Stone site walls
7. 15' wide sidewalk



Section near 9th Ave. N

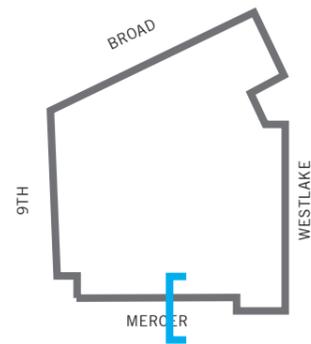


Section at center of plaza

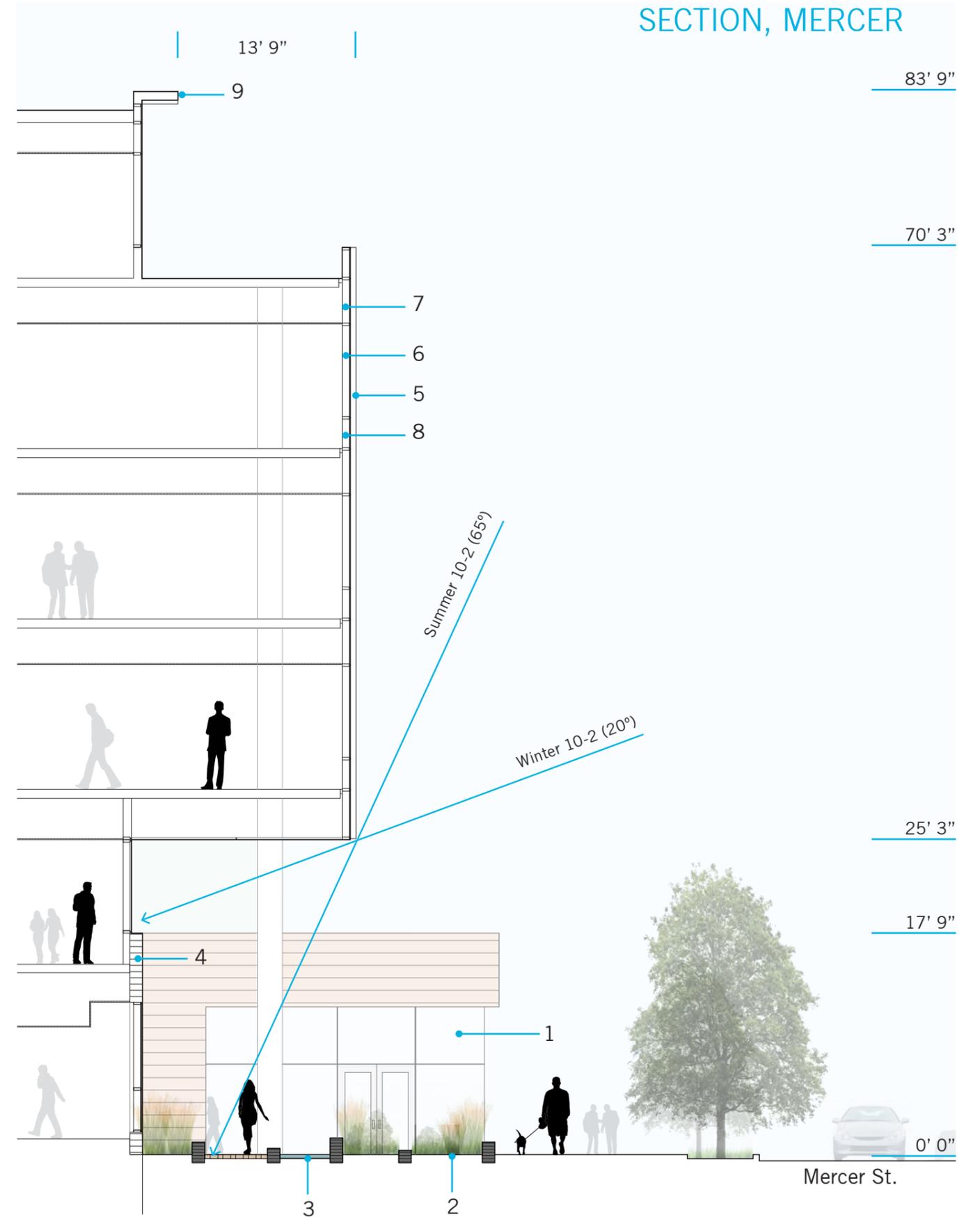


Section near Pacific McKay

1. Retail space
2. South plaza
3. Water feature
4. Textured stone cladding
5. Perforated metal fin
6. Clear vision glazing
7. Shadow box 1
8. Shadow box 2
9. Metal cladding at roof



SECTION, MERCER





9TH AVENUE N

Early Design Guidance:

Cyclist amenities should be designed to connect entries to the designated bike route on 9th.

The streetscape at 9th should be designed to slow vehicular and cyclist traffic at the street.

Make the 9th Avenue N entry functional for tenants; include cyclist amenities and seating areas near this door.

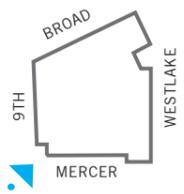
Lighting should be used to enhance building design and increase safety.

Use transparency in the facade wherever internal program allows for it.

The media wall should have a human scale.



1. Atrium
2. Electron microscopy
3. Research space
4. Landscape
5. Media wall
6. Loading and parking ramp
7. Bike racks
8. Canopy overhead





9TH AVE. N FACADE

The exterior treatment of the Institute along 9th is highly developed and provides a rich and layered variety of experiences and impressions from the scale of the pedestrian to the city.

The upper façade is captured within the folded 'container' that unifies the three 'petals' of the Institute. This gesture is at a civic scale and will be a memorable image for the place. Within this frame is a layered system with sophisticated vertical and horizontal modulation.

The outermost layer consists of perforated metal sunshades that provide glare protection and enhance the thermal performance of the curtain wall.

The 'top' zone is differentiated by clear and continuous vision glazing.

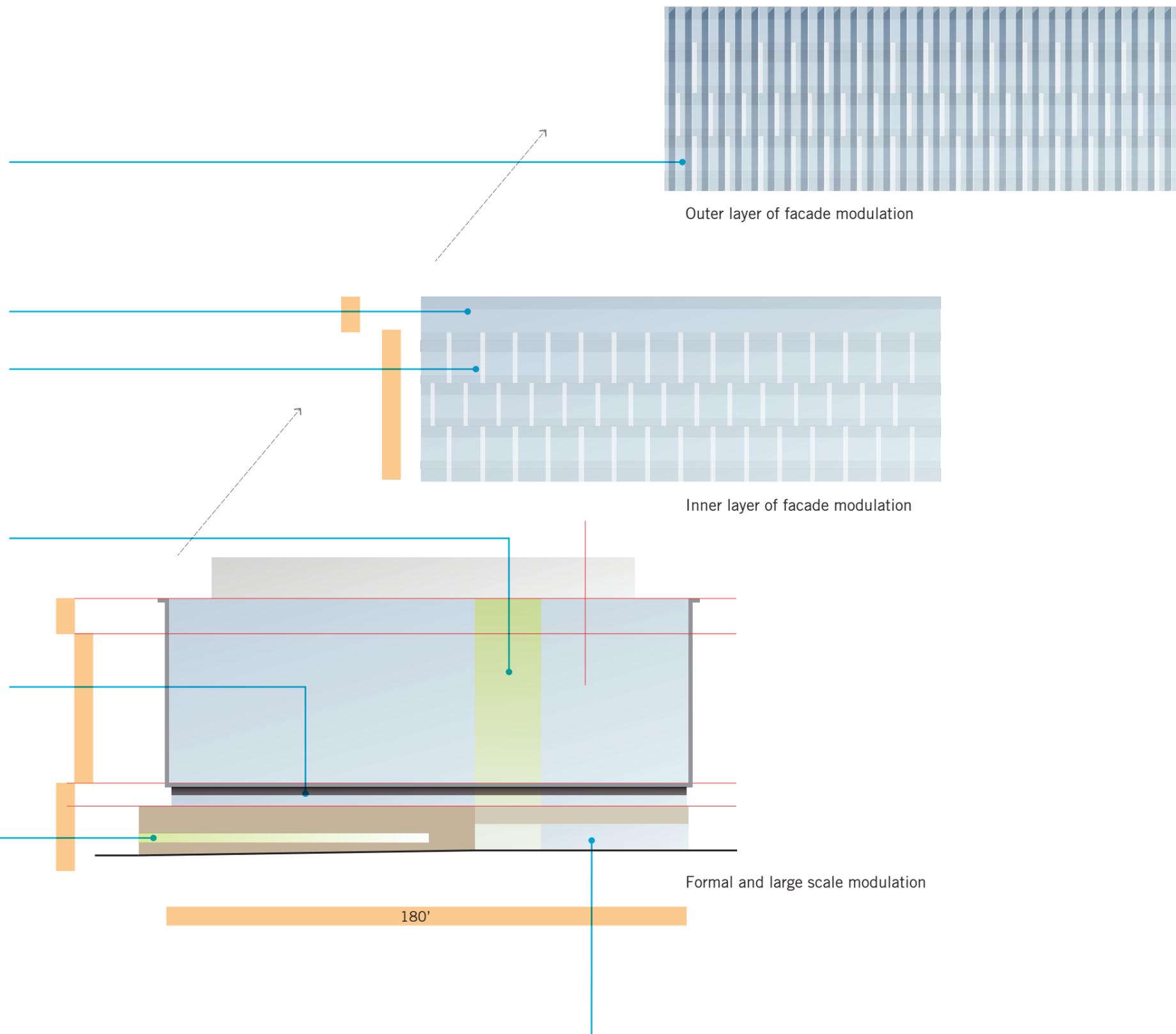
Within the glazing system stainless steel mesh vertical bands are modulated along the 10' interval, shifting by floor to create a dynamic composition within the 'middle' zone.

The internal stair tower is celebrated through with strong color that is visible from the exterior and provides asymmetry and scale shift.

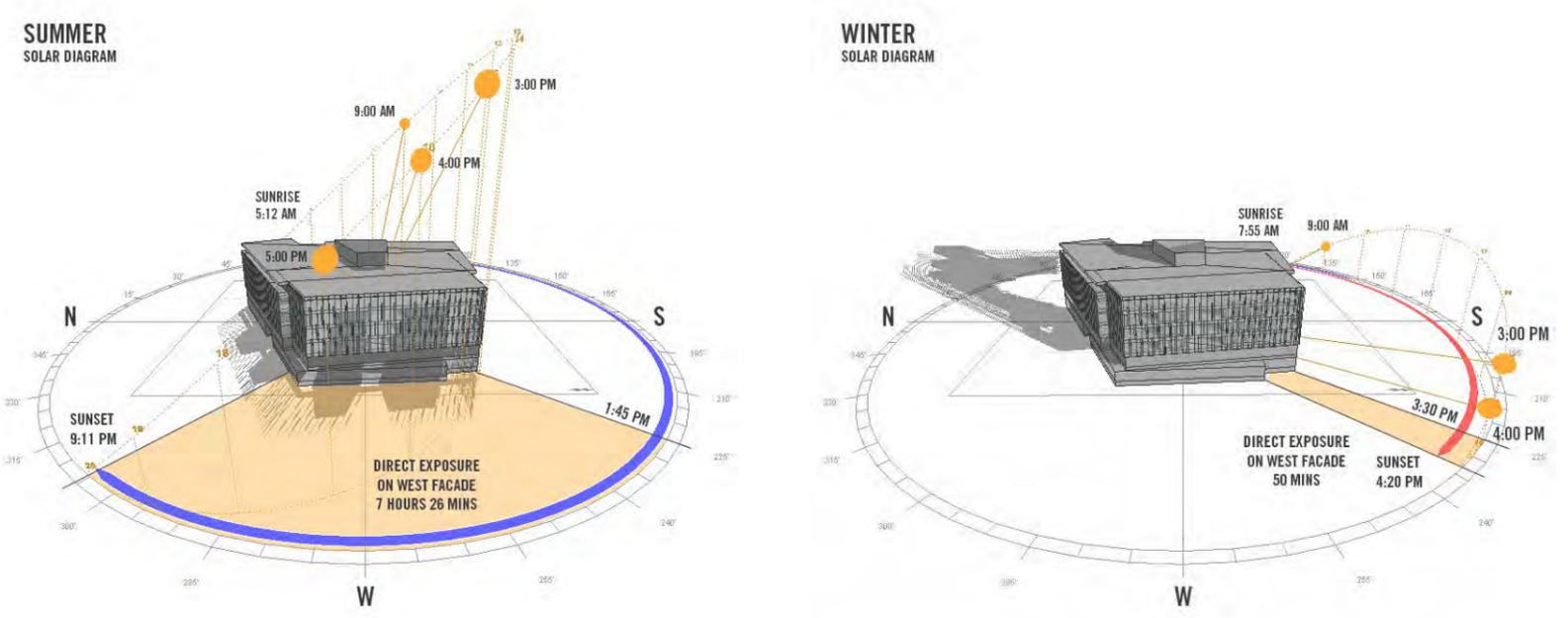
The ribbon window at the second level is setback and provides a continuous visual relief to allow the upper floors to appear lighter and provide a distinction between 'base' and middle'.

A continuous 'media wall' with integral graphics and lighting provides visual interest at the human scale.

The stone podium of the first level is substantially set back along the southwest corner to provide significant landscape and open space for pedestrians. Along this zone continuous vision glass offers visual connections inside to outside.

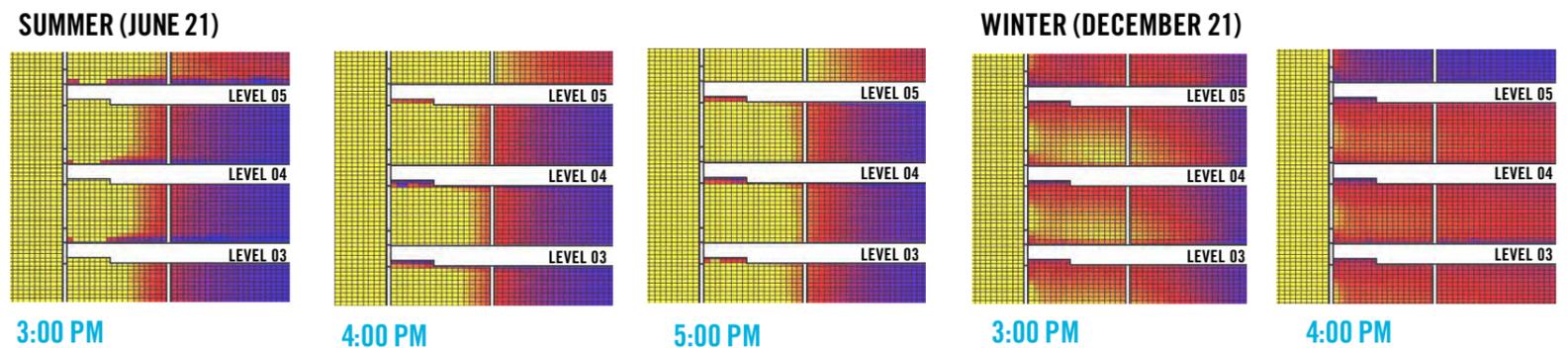
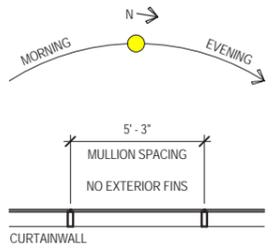


SOLAR ANALYSIS, WEST SUNSHADES

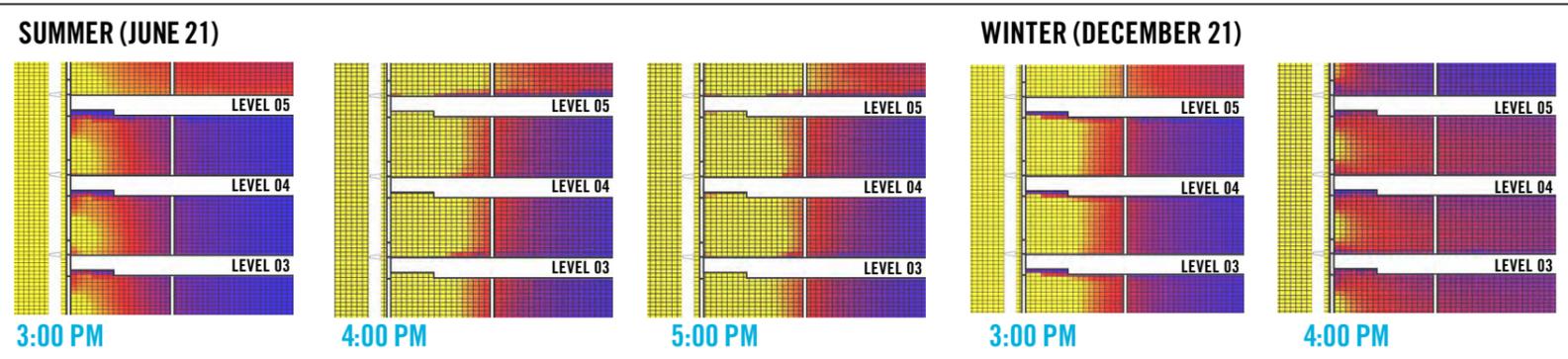
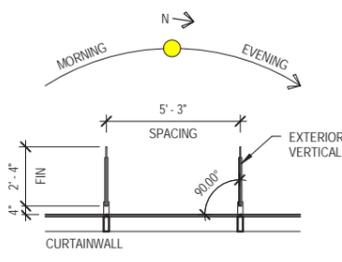


The vertical sunshades on the western facade are an integral component in the facade performance from both an energy and occupant comfort point of view. The sunshades provide a measurable reduction in heat gain and therefore cooling load and energy consumption while they also minimize glare for occupants during afternoon hours. A series of sunshade configurations were studied (except from studies shown here) to determine the optimal size, orientation and spacing to maximize the effectiveness of this device.

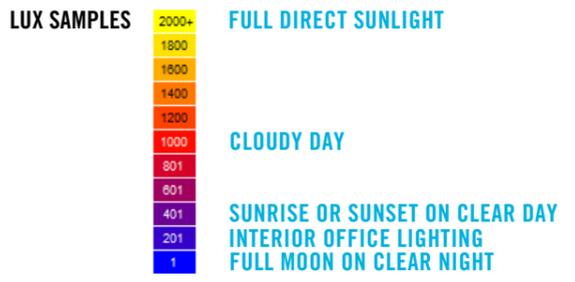
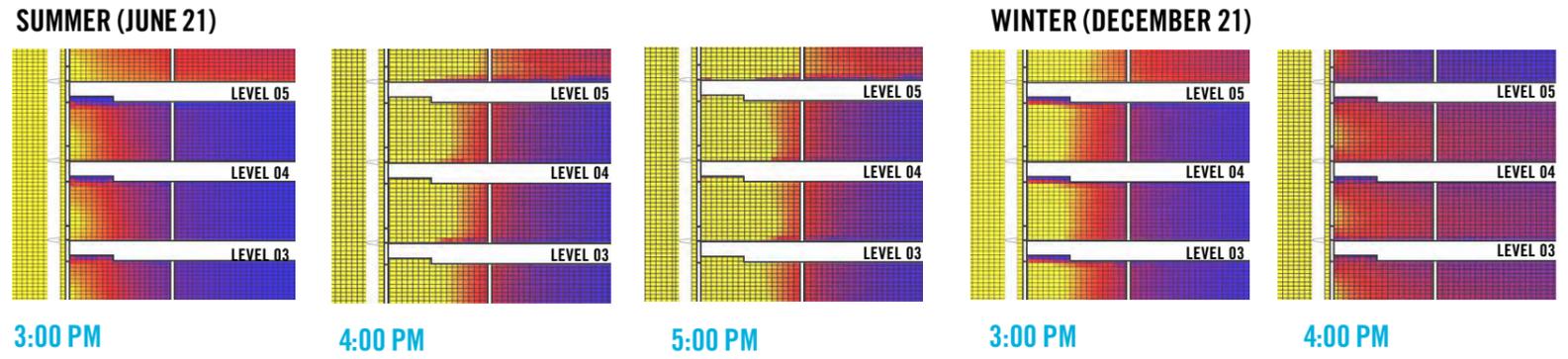
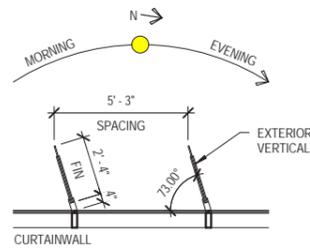
NO VERTICAL FINIS



90-DEGREE VERTICAL FINIS WITH 50% PERF



73-DEGREE VERTICAL FINIS WITH 50% PERF



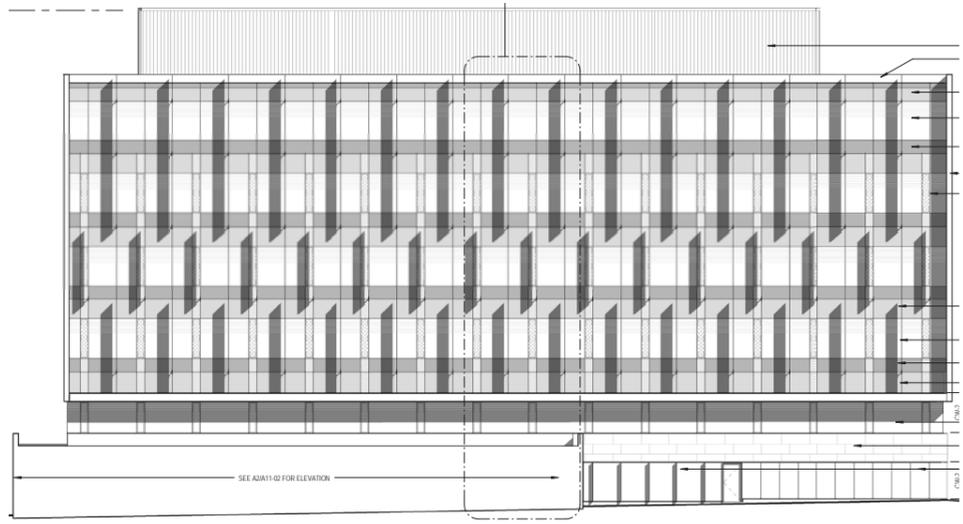
MODULATION OPTIONS, WEST SUNSHADES

Sunshade depth reduced at stair core.



Does not meet energy and occupant comfort performance requirements.

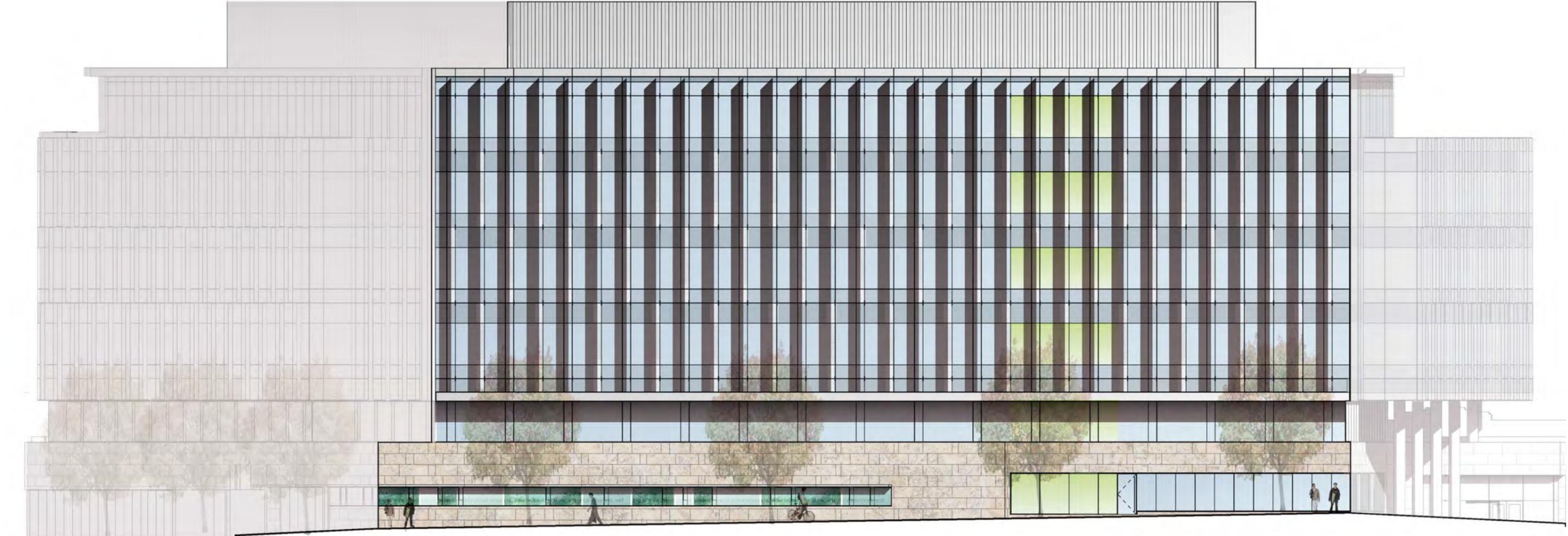
Sunshade spacing increased, location shifted



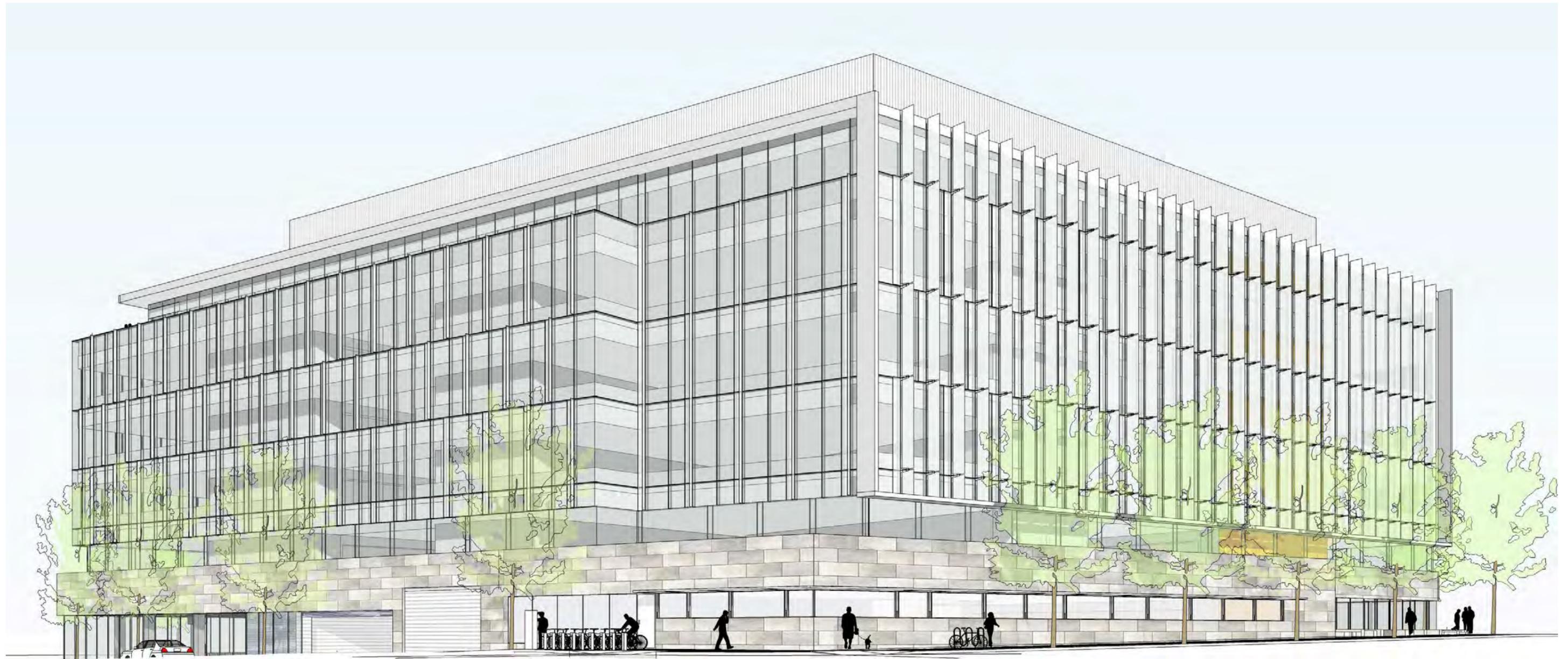
Does not meet energy and occupant comfort performance requirements.

A series of sunshade options was studied in order to investigate the possibility for additional modulation in the facade while maintaining the desired technical performance characteristics. Alternatives in which the fins were spaced too far apart, or were removed, or were not deep enough were discarded because they do not meet the performance requirements of the building enclosure. Three viable alternatives were identified that meet both the technical requirements for performance and the aesthetic goal of modulation, These three alternatives are represented on the following pages.

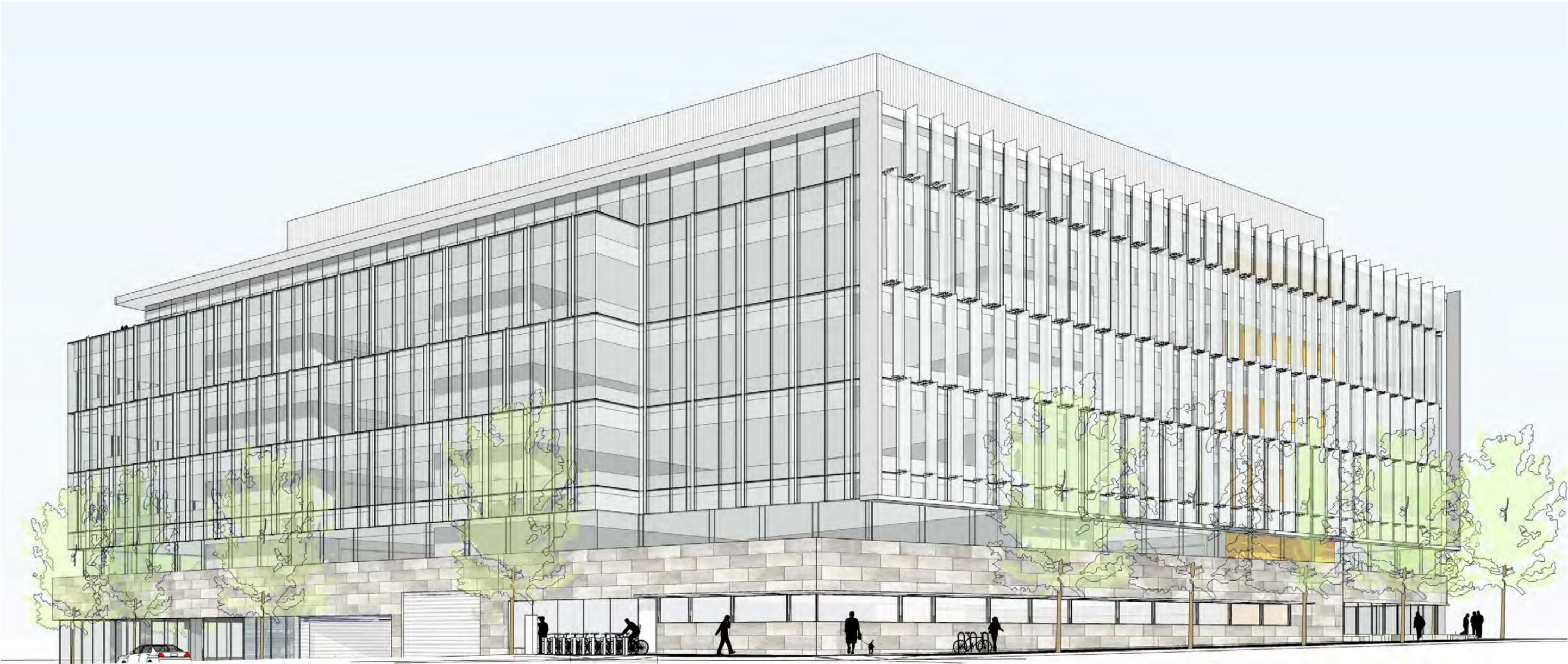
WEST SUNSHADES, OPTION 1
(PREFERRED)

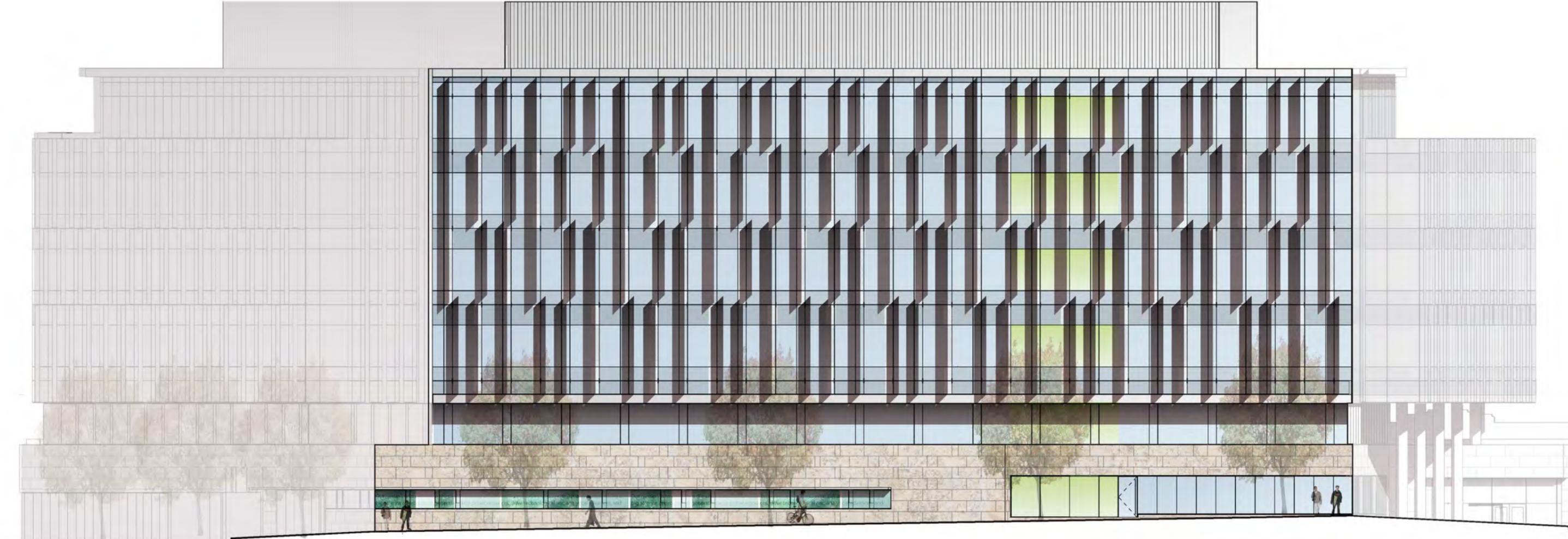


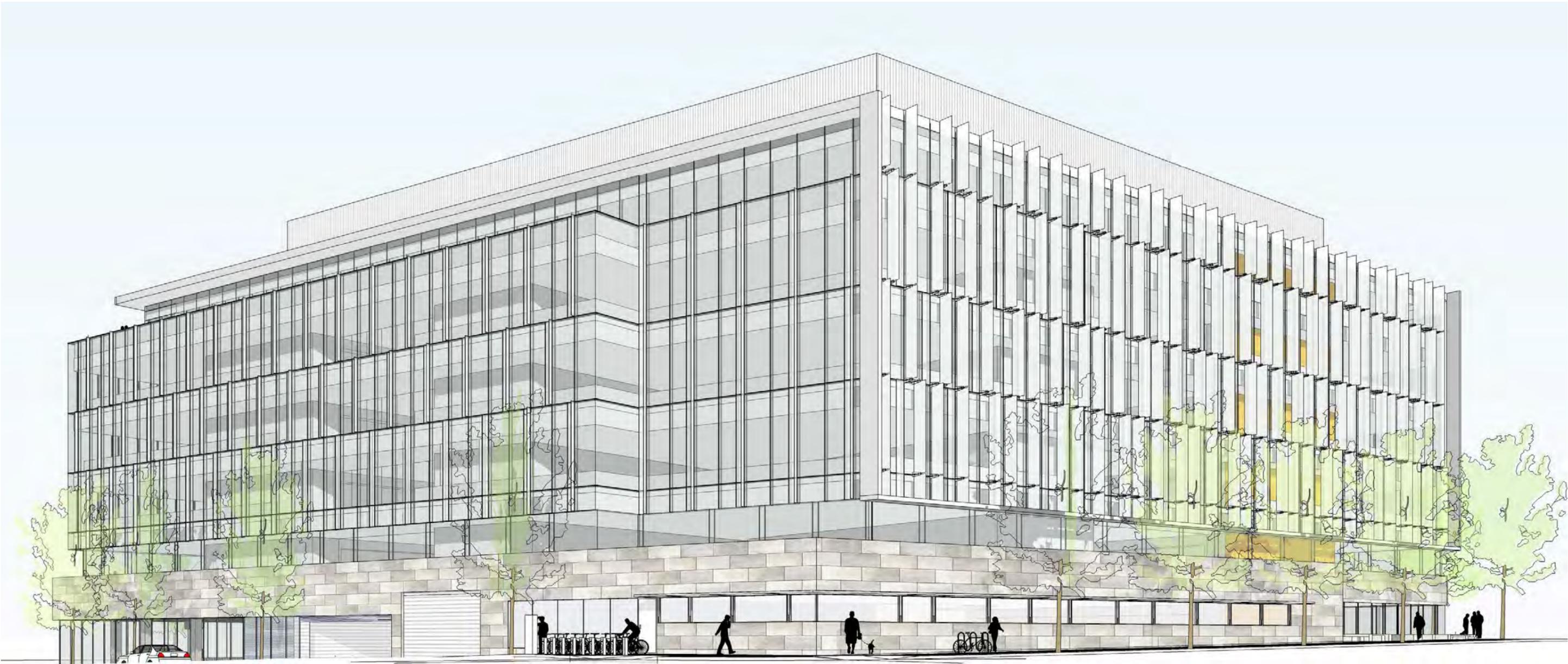
WEST SUNSHADES, OPTION 1
(PREFERRED)











BROAD STREET

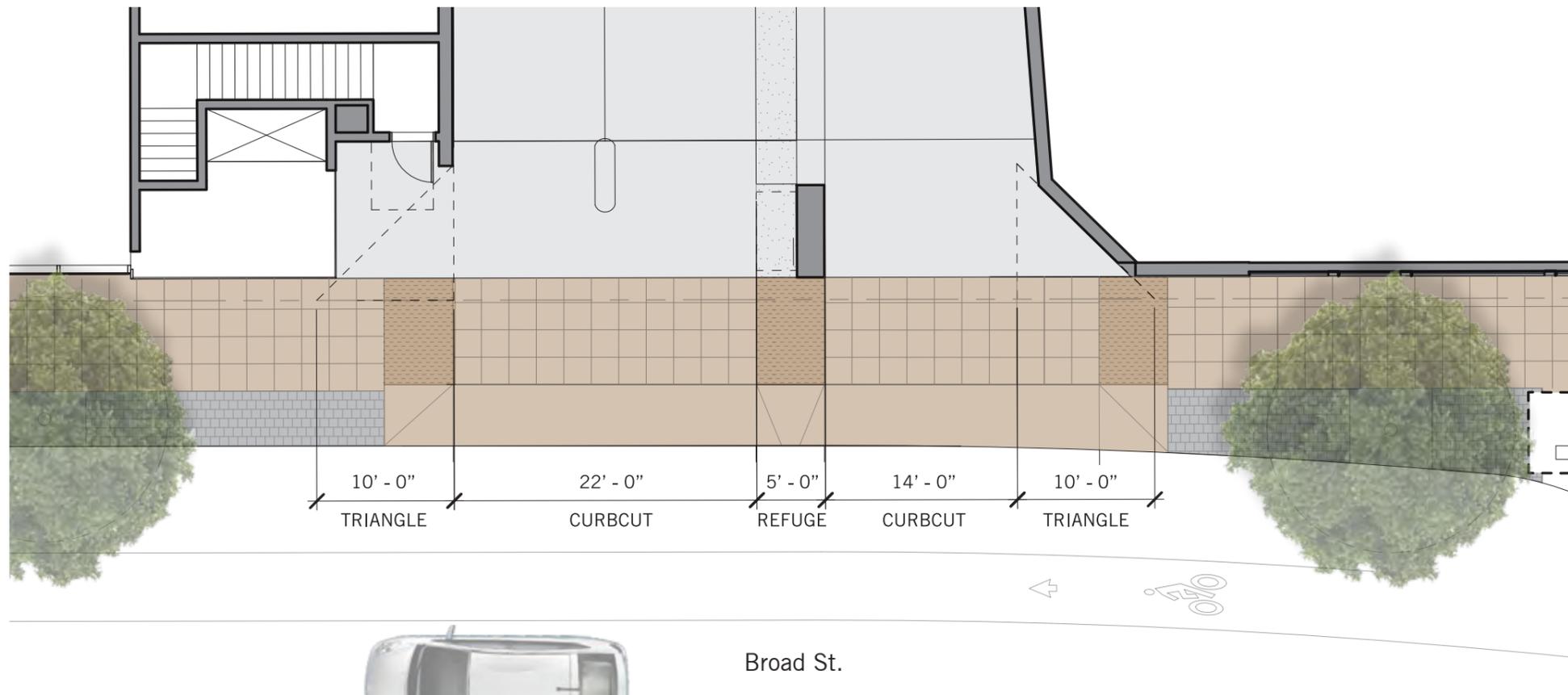
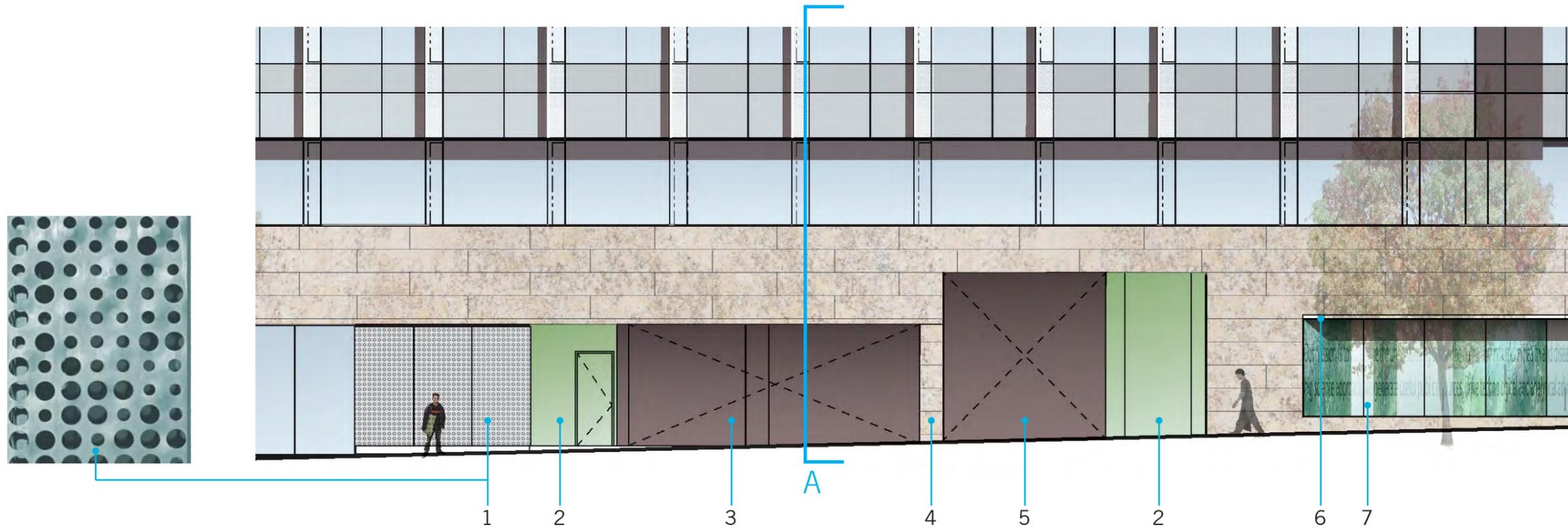
Early Design Guidance:

The garage entry should be carefully designed to enhance safety for pedestrians and cyclists.



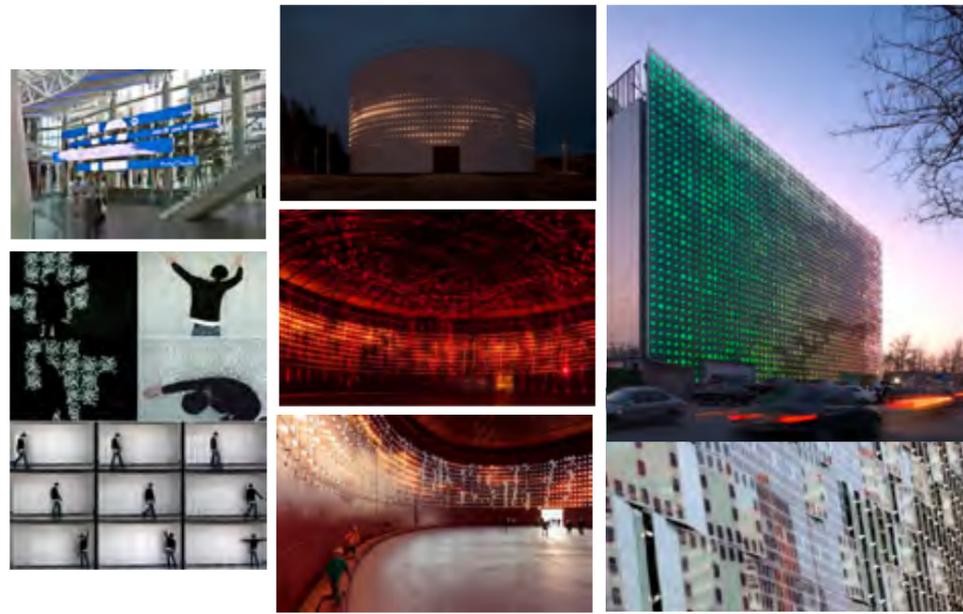
1. Entry plaza
2. Retail, Ford McKay
3. Lobby
4. Auditorium
5. Atrium
6. Research space
7. Media wall
8. Loading and parking ramp
9. Bike share station
10. Bike racks
11. Canopy overhead

GARAGE ENTRY

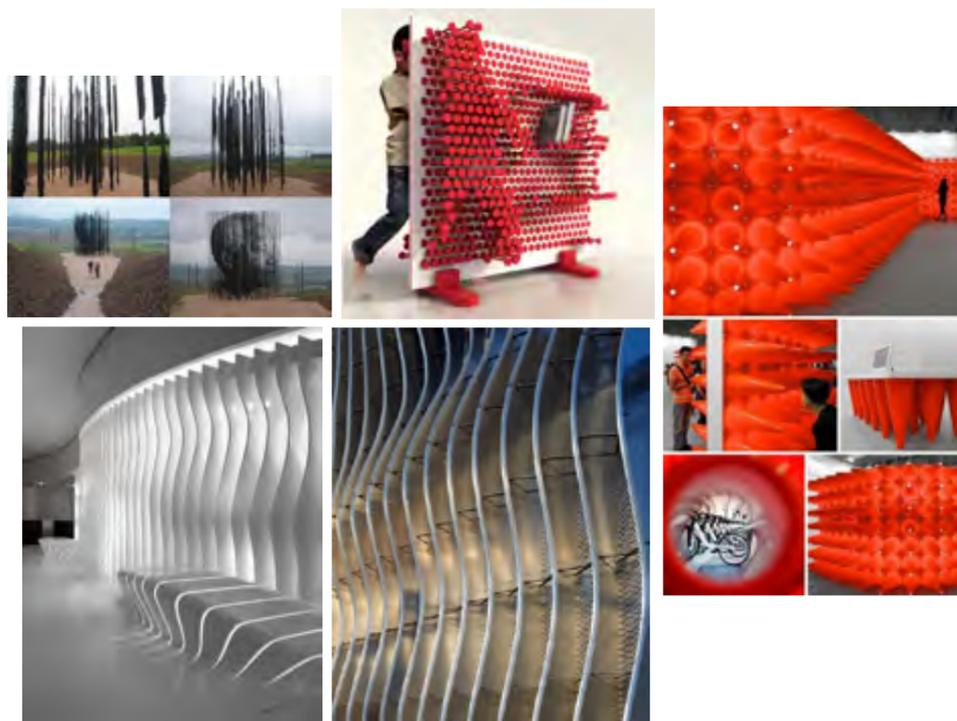


1. Perforated metal panel
2. Color at garage walls
3. Vehicle entry
4. Pedestrian refuge zone
5. Truck entry
6. Glass canopy
7. Media wall

TECHNOLOGY



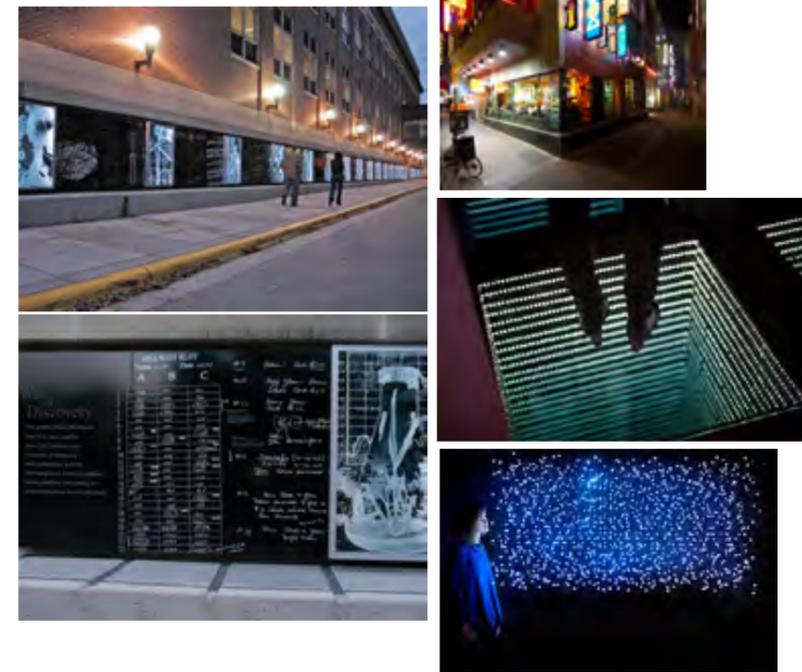
INTERACTIVE



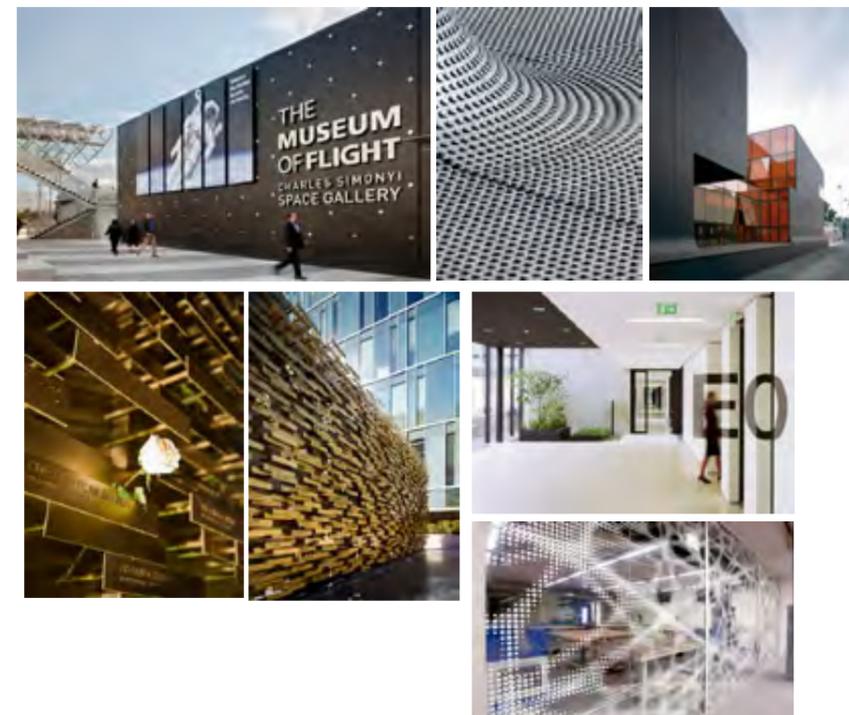
ARCHITECTURE

MEDIA WALL

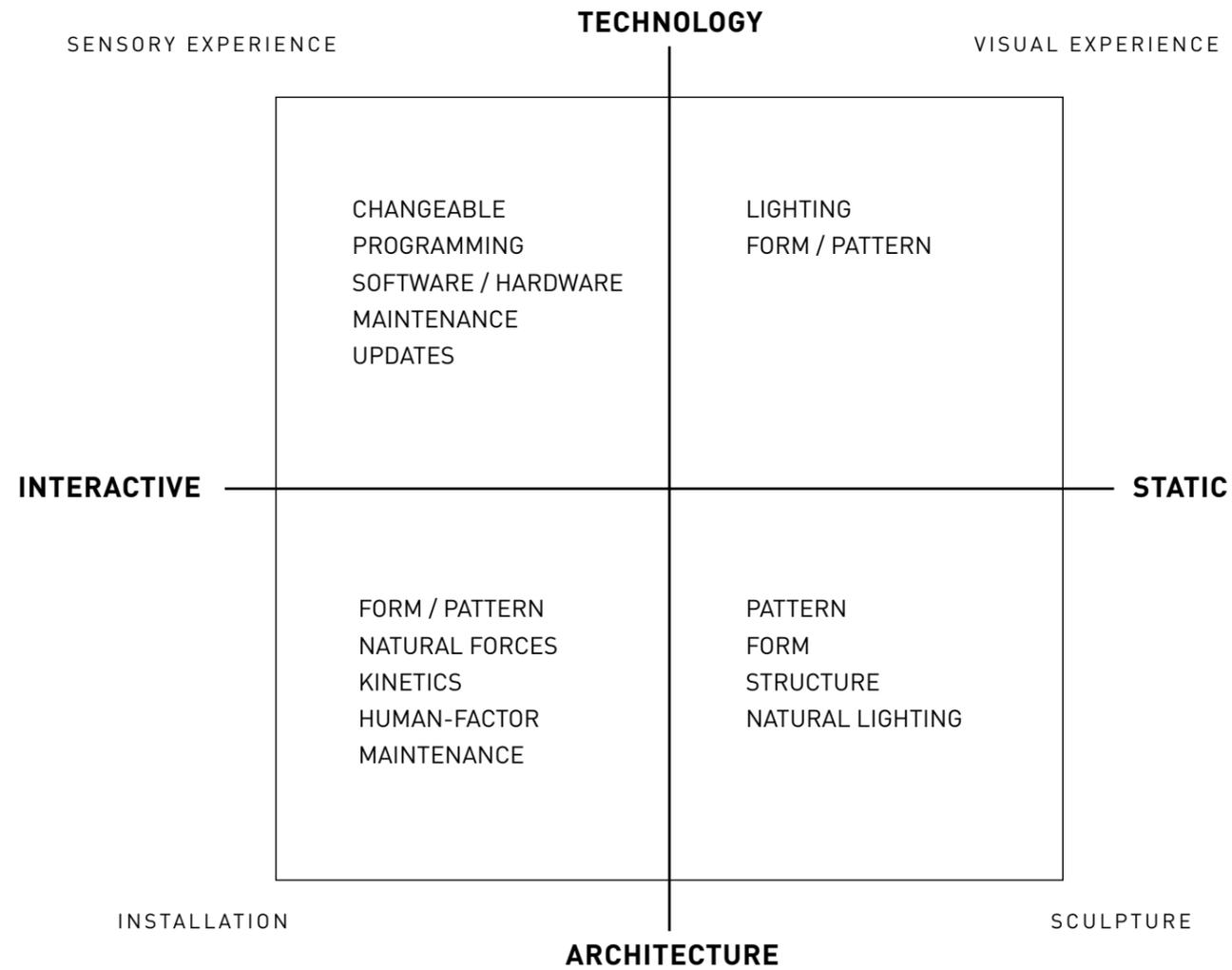
An environmental graphics consultant has been engaged to assist with design of the media wall at 9th Ave. N and Broad St. Concepts explore texture, lighting, and changeable media to provide an engaging experience for the pedestrian.



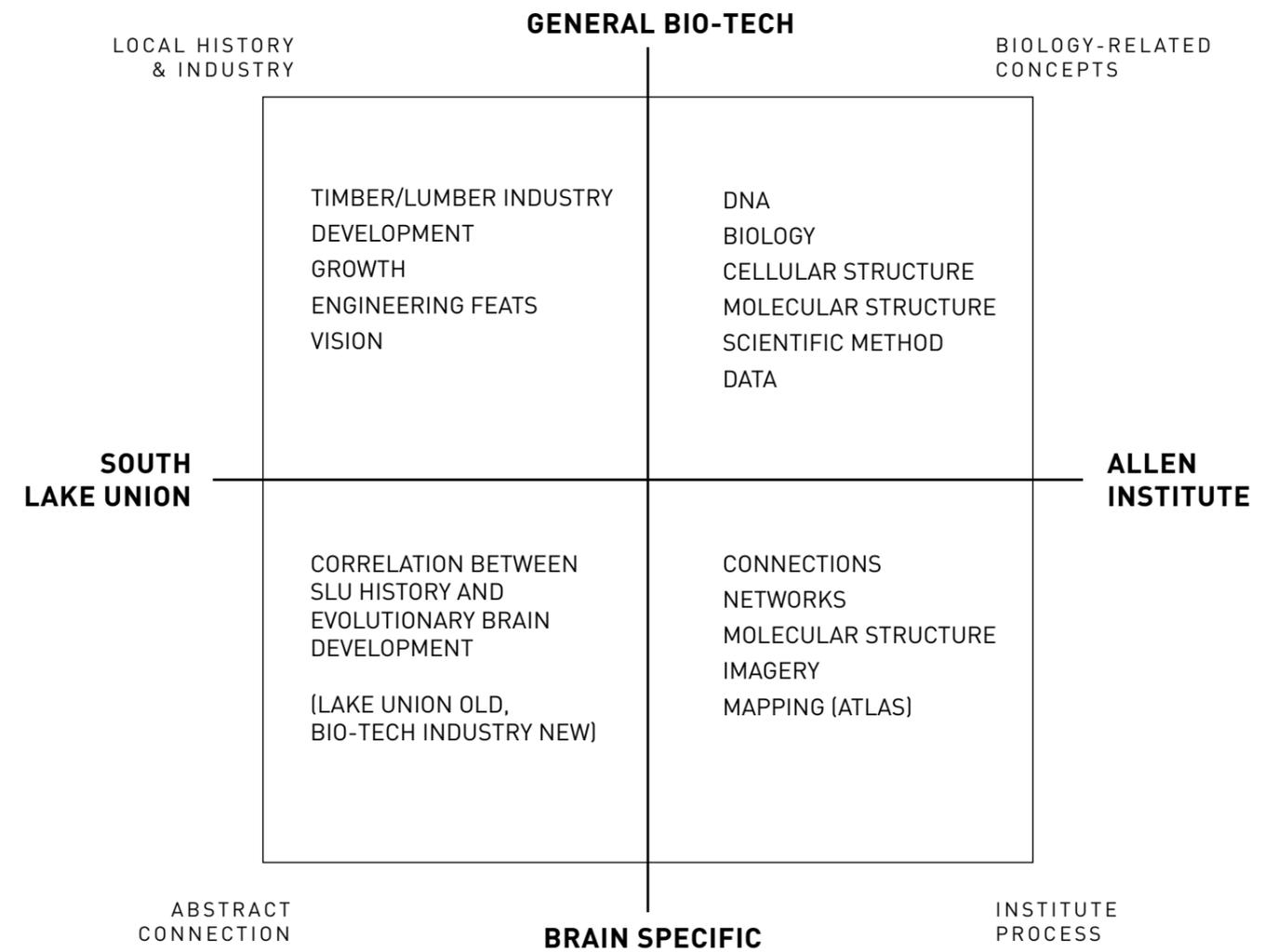
STATIC

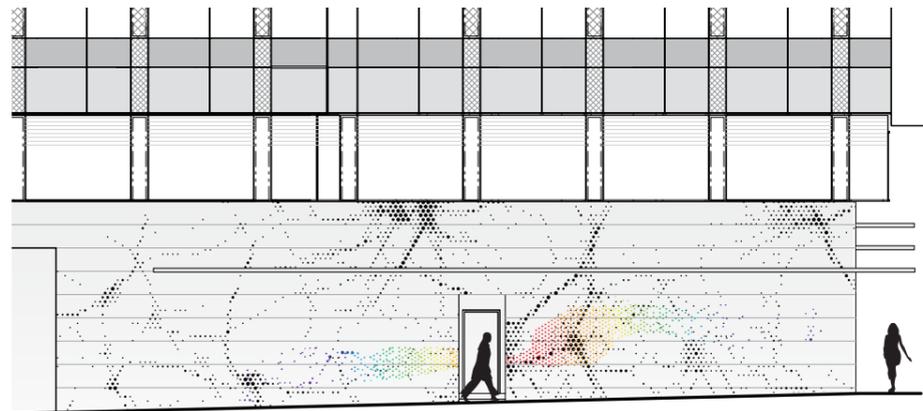


1. WHAT IS THE METHOD / FORM?

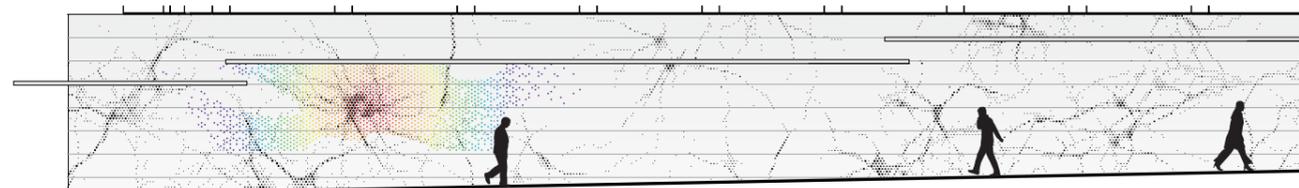


2. WHAT IS THE CONTENT/INTENT?

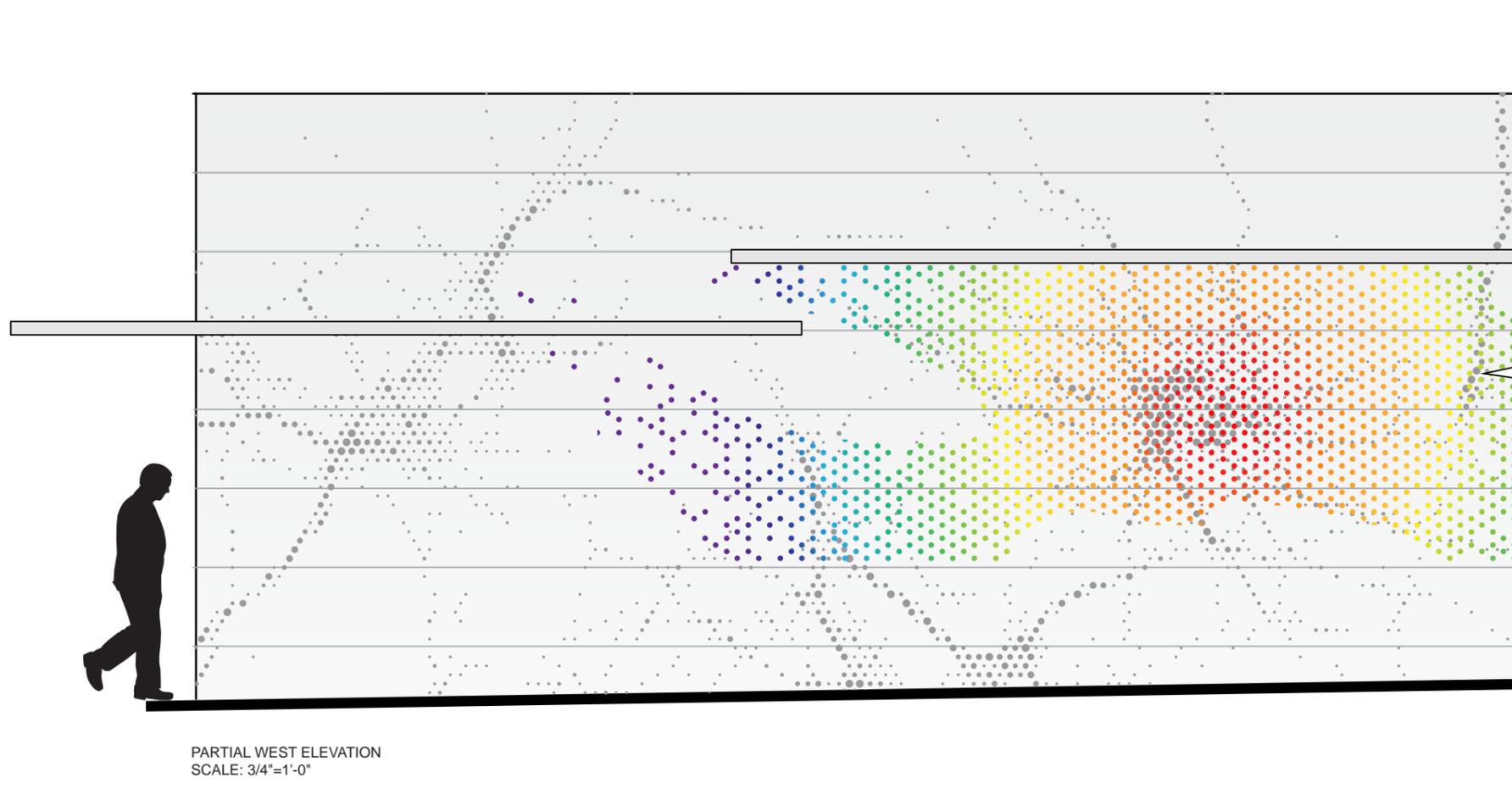




NORTH ELEVATION
SCALE: 3/16"=1'-0"



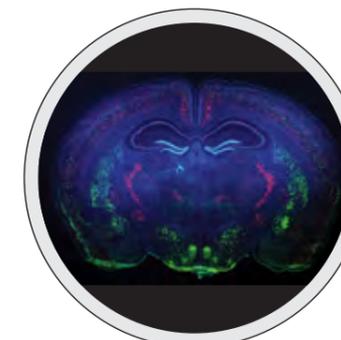
PARTIAL WEST ELEVATION
SCALE: 3/16"=1'-0"



PARTIAL WEST ELEVATION
SCALE: 3/4"=1'-0"



SCULPTURAL STONE / CONCRETE PATTERNING

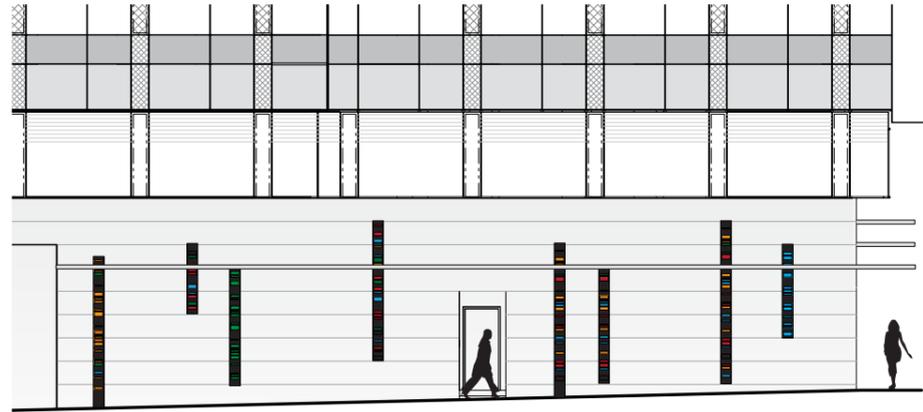


DIMENSIONAL ACRYLIC TUBES
WITH IMAGERY WHEN LIT

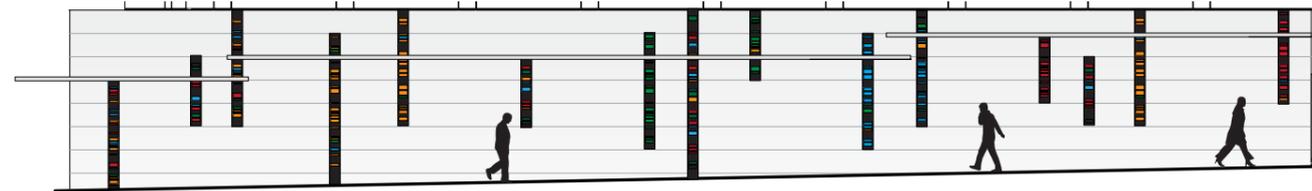
Concept 1: Pattern/Light/Texture



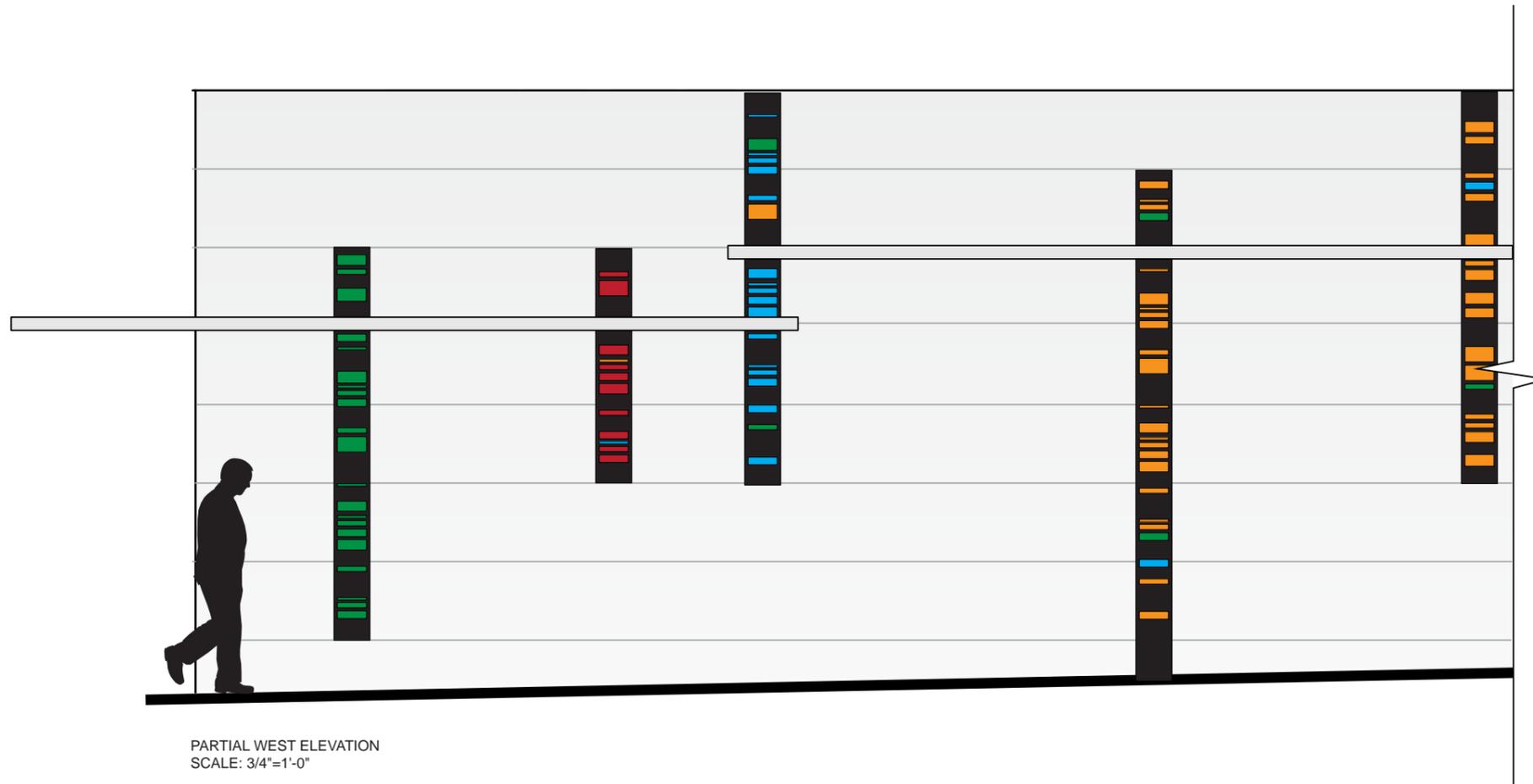
MEDIA WALL



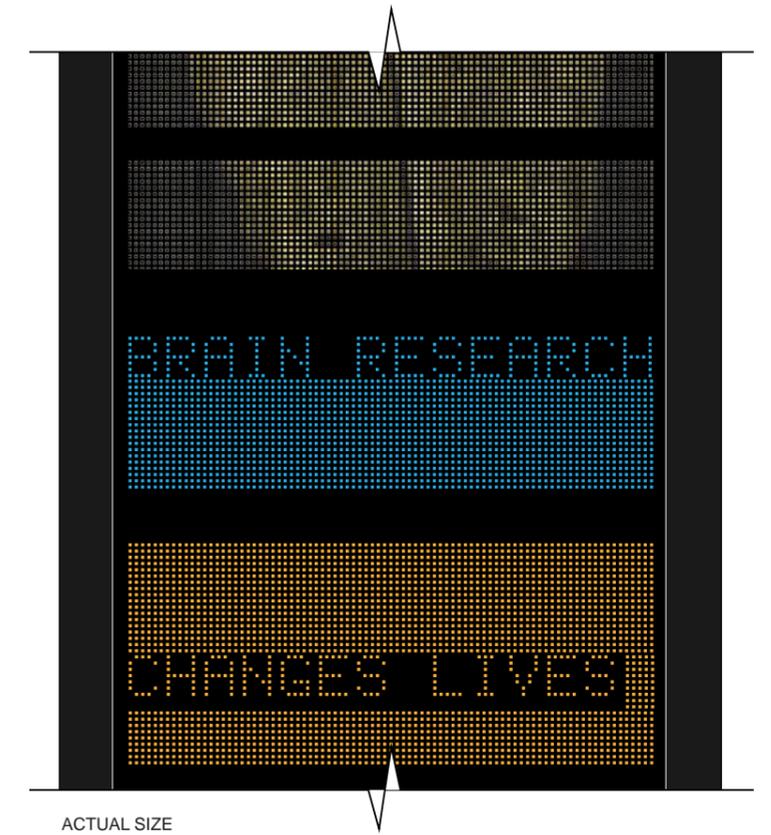
NORTH ELEVATION
SCALE: 3/16"=1'-0"



PARTIAL WEST ELEVATION
SCALE: 3/16"=1'-0"



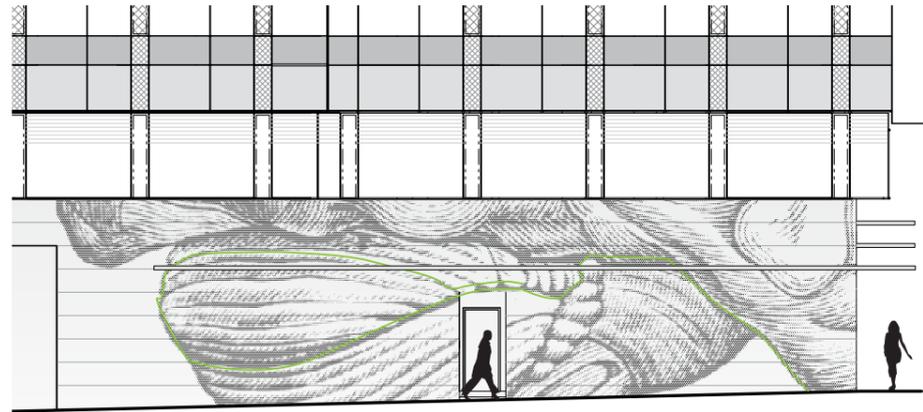
PARTIAL WEST ELEVATION
SCALE: 3/4"=1'-0"



ACTUAL SIZE



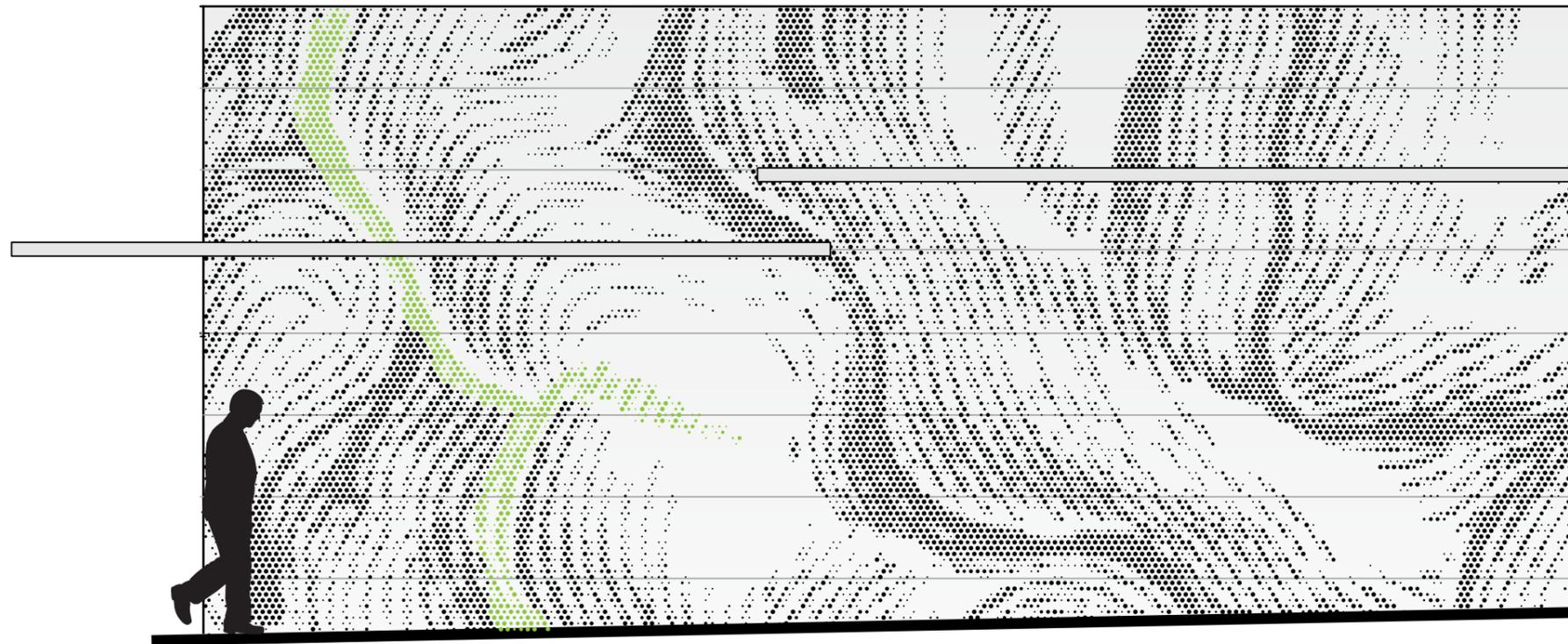
Concept 2: Light/Media/Information



NORTH ELEVATION
SCALE: 3/16"=1'-0"

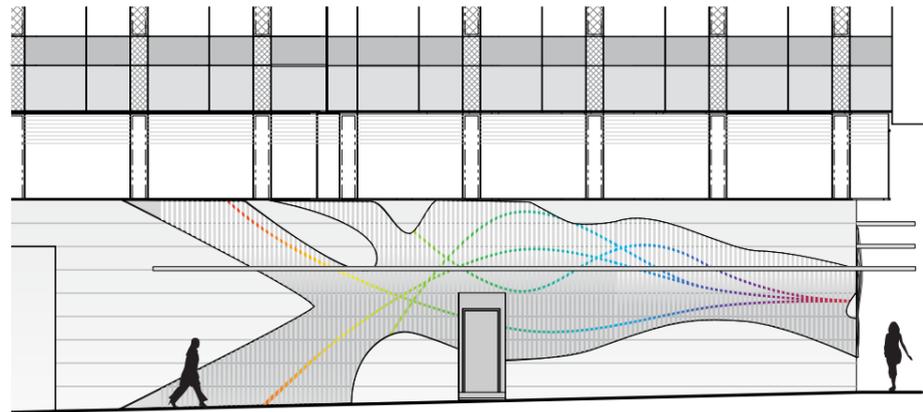


PARTIAL WEST ELEVATION
SCALE: 3/16"=1'-0"

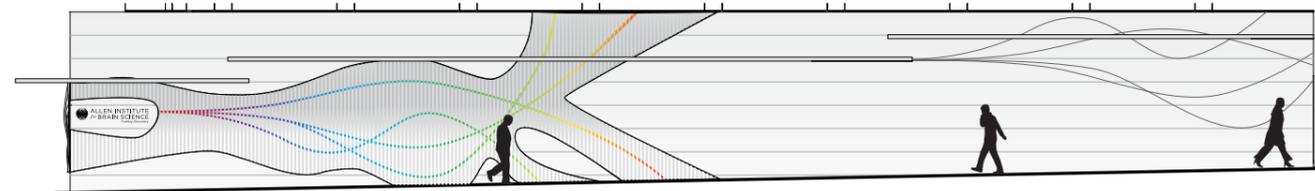


PARTIAL WEST ELEVATION
SCALE: 3/16"=1'-0"

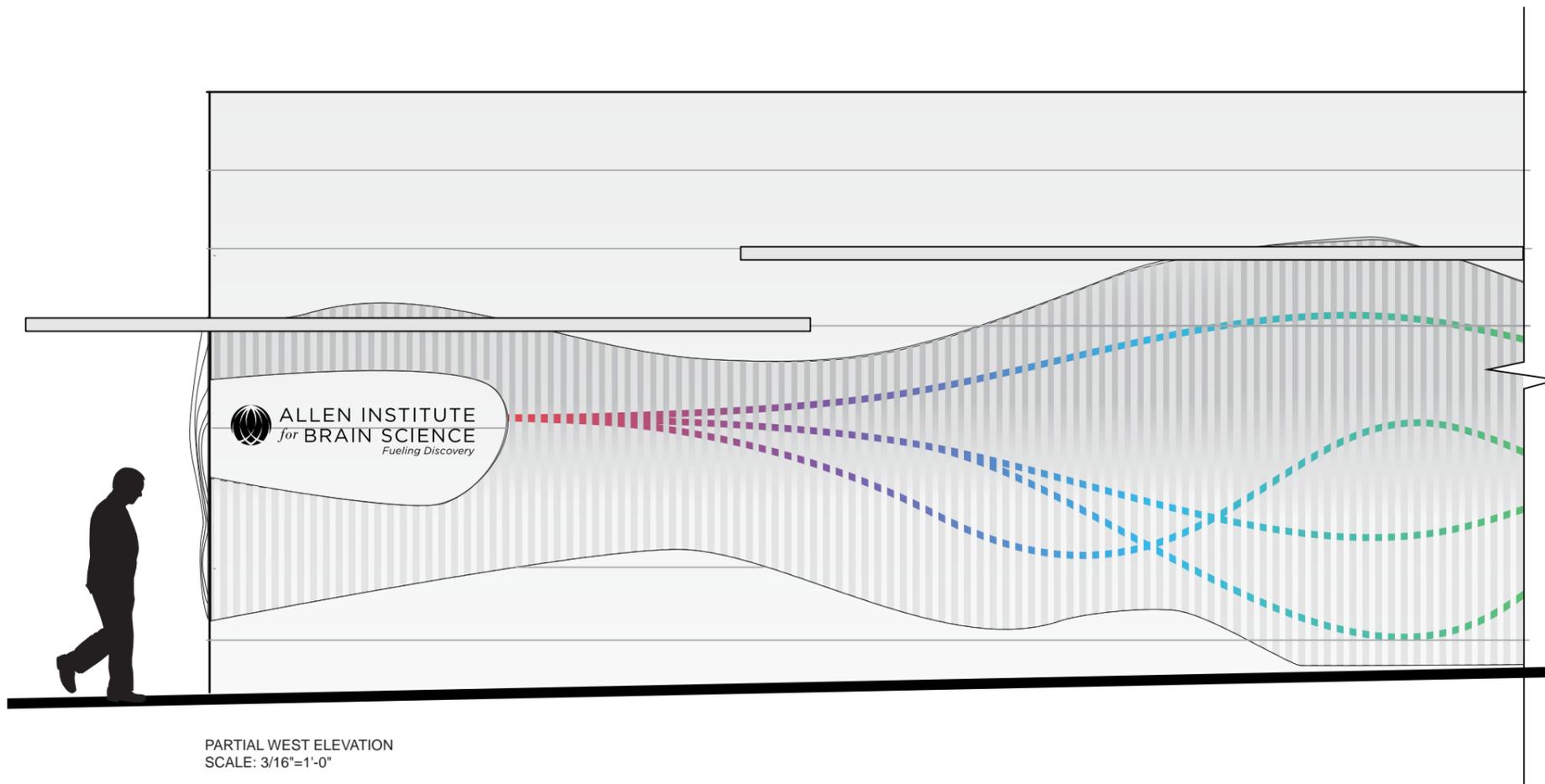




NORTH ELEVATION
SCALE: 3/16"=1'-0"



PARTIAL WEST ELEVATION
SCALE: 3/16"=1'-0"



PARTIAL WEST ELEVATION
SCALE: 3/16"=1'-0"

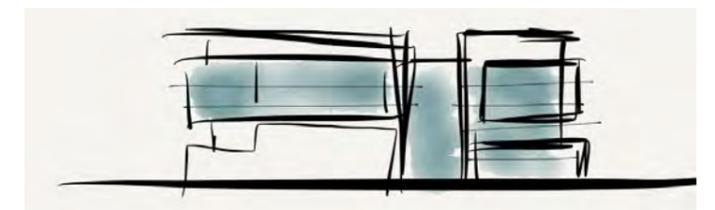


DEVELOPMENT STANDARD DEPARTURE REQUESTS, PROJECT 3014287

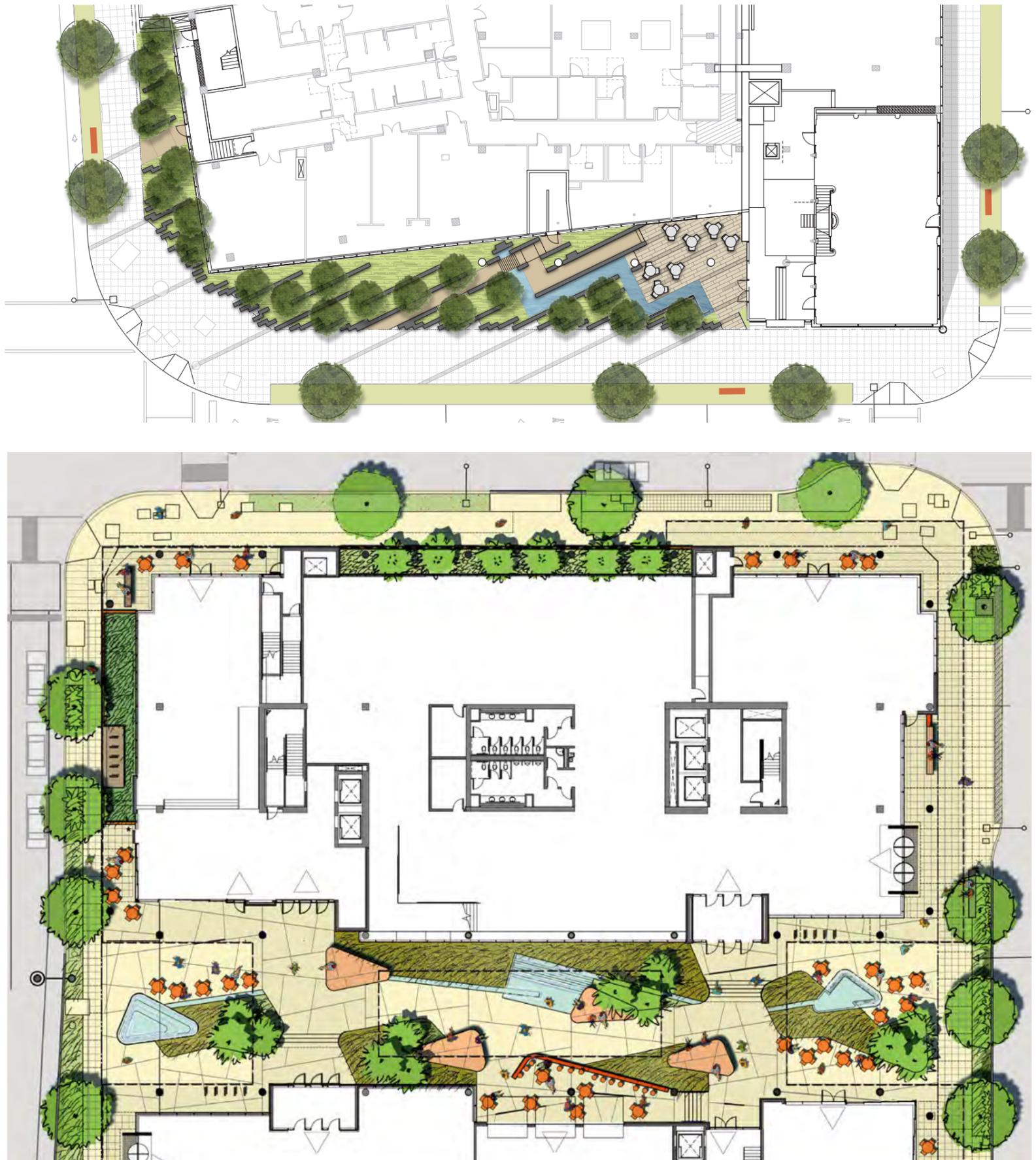
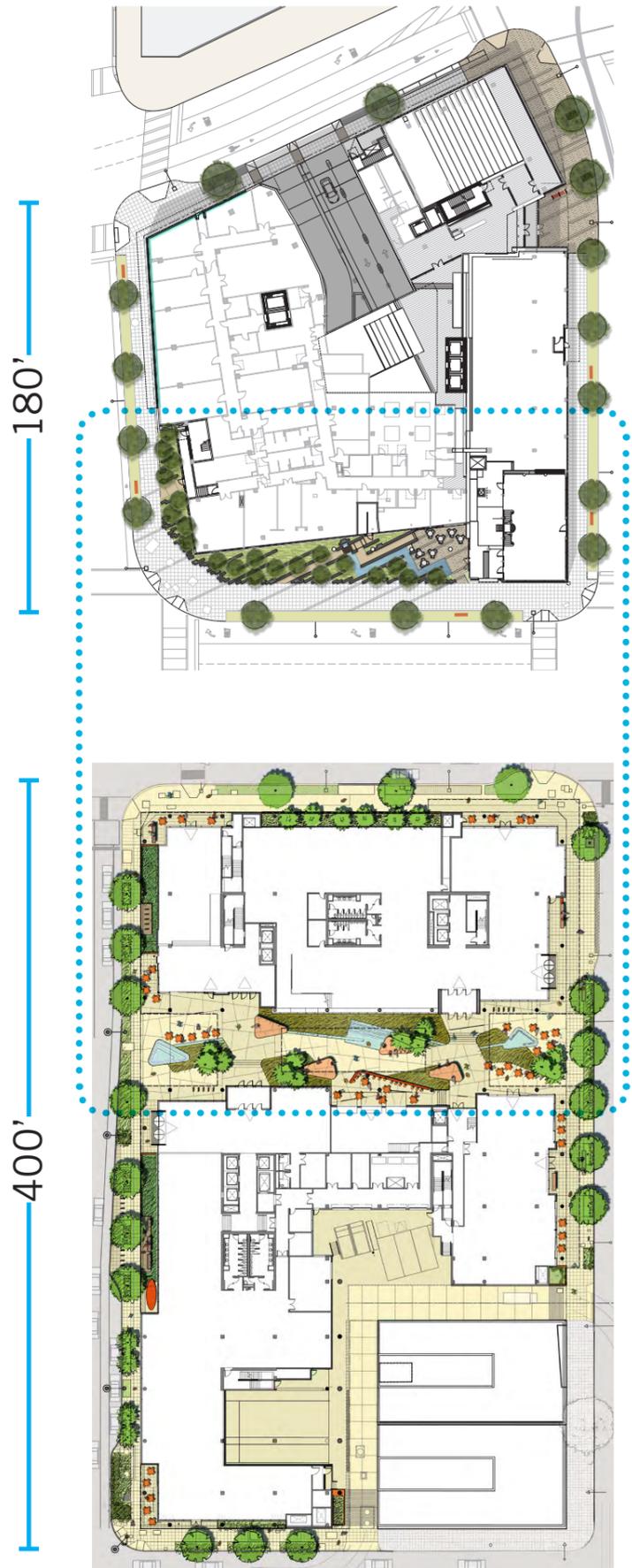
	SMC DEVELOPMENT STANDARDS	DEPARTURE REQUEST	HOW THE CODE DEPARTURE BETTER MEETS DESIGN GUIDELINES	APPLICABLE DESIGN GUIDELINES
1	<p>SMC 23.48.014D Street Level Setbacks</p> <p>Structures may be set back up to twelve (12) feet from the property line...the setback area shall be landscaped according to the provisions of Section 23.48.024 2.</p> <p>Additional setbacks shall be permitted for up to thirty (30) percent of the length of the set-back street wall, provided that the additional setback is located a distance of twenty (20) feet or greater from any street corner.</p>	<p>Request for departure from street level setback requirements</p> <p>Request to setback more than 12 feet from the property line at more than 30% of the length of Mercer Street and less than 20 feet from the street corner</p>	<p>The unique site conditions related to the reconstructed historic Pacific McKay Building provide an opportunity for a meaningful public open space and landscape buffer at the southern frontage of the site along Mercer Street. This public and accessible landscape space extends from the southwest corner and gracefully grows in dimension to create a terrace that connects to the retail use in the McKay Building. The space features a meandering path from which to experience the natural environment including plantings, a water feature, and stone seat walls.</p>	<p>A-1 Responding to Site Characteristics A-2 Streetscape Compatibility A-4 Human Activity D-1 Pedestrian Open Spaces and Entrances E-1 Reinforce Existing Landscape Character of Neighborhood E-2 Landscaping to Enhance the Building and/or Site E-3 Landscape Design to Address Special Site Conditions</p>
2	<p>SMC 23.48.019B Street Level Uses</p> <p>A minimum of seventy-five (75) percent of each street frontage at street level where street level uses are required must be occupied by uses listed in subsection A. The remaining twenty-five (25) percent of the street frontage at street level may contain other permitted uses and/or pedestrian or vehicular entrances. The frontage of any exterior outdoor common recreation area required for residential uses, shall not be counted in street frontage</p> <p>A. The following uses qualify as required street level uses: 1. General sales and service uses; 2. Eating and drinking establishments; 3. Entertainment uses; 4. Public libraries; and 5. Public parks</p>	<p>Request for departure from definition of street level uses in subsection A</p> <p>Request to take advantage of an opportunity to provide visible public education and outreach by locating the Institute auditorium at a prominent public corner and providing substantial vision glazing into the space as well as public access to special programs and events.</p>	<p>The mission and programs of the Institute provide an opportunity to locate a significant accessible street level use at a visible location that relates well to other cultural activities including Lake Union Park. The auditorium is visible to the public through extensive glazing and will provide programs that are of interest to a broad demographic, similar to other cultural programmatic resources.</p>	<p>A-4 Human Activity A-2 Streetscape compatibility D-1 Pedestrian Open Spaces and Entrances</p>
3	<p>SMC 23.48.018.B.3.a Street Level Transparency at Broad and 9th - blank facades</p> <p>Blank facades shall be limited to segments thirty (30) feet wide, except for garage doors which may be wider than thirty (30) feet. Blank facade width may be increased to sixty (60) feet if the Director determines that the facade is enhanced by architectural detailing, artwork, landscaping, or other similar features that have visual interest.</p>	<p>Request for a departure from maximum length of blank facades at street level.</p> <p>Request to consolidate the programmatically required blank facades in a manner that provides an opportunity to install a public art amenity that will engage pedestrians and provide visual interest at a human scale.</p>	<p>Note that the requirements for total width and percentage of transparency are met - the departure is for the contiguous length of allowable blank wall only. The Institute has certain programs that require light controlled and secure spaces to be located in a single location on the first floor due to below grade site conditions and functional requirements. These programs generate the need for a blank wall at street level.</p> <p>The consolidation of the blank walls into a single zone provides an opportunity to offer pedestrians a visually engaging 'media wall' that integrates artist designed graphic information with architectural details and integral lighting.</p> <p>A bike share station is proposed at the corner of Broad and Westlake - highly visible and near the bicycle travel route.</p>	<p>A-4 Human Activity C-2 Architectural Concept and Consistency C-3 Human Scale C-4 Exterior Finish Materials D-2 Blank Walls</p>

DEVELOPMENT STANDARD DEPARTURE REQUESTS, PROJECT 3014287

	SMC DEVELOPMENT STANDARDS	DEPARTURE REQUEST	HOW THE CODE DEPARTURE BETTER MEETS DESIGN GUIDELINES	APPLICABLE DESIGN GUIDELINES
4	<p>SMC 23.48.034.C.3 & SMC 23.54.030.F.2.b.2 Parking and Loading access limited to one two way curb cut and 22' minimum curb cut width</p> <p>If the lot does not abut an improved alley, parking and loading access may be permitted from the street. Such access shall be limited to one two-way curb cut. In the event the site is too small to permit one two-way curb cut, two one way curb cuts shall be permitted.</p> <p>One curb cut greater than 10 feet but in no case greater than 20 feet in width may be substituted for each two curb cuts permitted by subsection 23.54.030.F.1.a;</p>	<p>Request for a departure from quantity and minimum size of curb cuts</p> <p>Request to provide a separate curb cut for service vehicle access, and to size the service curb cut to a smaller than minimum size. The design and configuration of the vehicle related access to the development has been carefully coordinated with SDOT and the recommendations of the civil engineer.</p>	<p>Note that the shape of the site, topography, and the lack of an alley limit options for servicing and vehicle and bicycle ingress and egress. Site and program requirements require separate vehicle and service entries for safety and operational security of the tenant, and for safe vehicle maneuvering within the structure.</p> <p>Separating the service and vehicle drives provides the best overall safety for pedestrians, cyclists, motorists, and service vehicles. Care has been taken to minimize the width of curb cuts, provide pedestrian refuge, provide maximum landscaping, use signage, color, and visual cues to ensure safety and a pleasant pedestrian experience.</p>	<p>A-8 Parking and Vehicle Access C-5 Structured Parking Entrances D-7 Personal Safety and Security</p>
5	<p>SMC 23.48.014.B.1 & SMC 23.48.014.B.2 Minimum Façade Height at Westlake</p> <p>On Class 1 Pedestrian Streets all facades shall have a minimum height of forty-five (45) feet. 2. On Class 2 Pedestrian Streets, as shown on Map B, all facades shall have a minimum height of twenty-five (25) feet.</p>	<p>Request for a departure from minimum façade heights of 45 feet on Westlake and 25 Feet on Mercer</p> <p>Note that this request relates only to the facades of the historic buildings.</p>	<p>The landmarked Pacific McKay and Ford McKay are a valuable asset to the community and the site and are required to be reconstructed at their original height, which does not meet the current zoning requirement.</p>	<p>C-1 Architectural Context</p>
6	<p>SMC 23.48.014.C Façade frontage at Westlake</p> <p>All facades on Class 1 Pedestrian Streets shall be built to the street property line along a minimum of seventy (70) percent of the facade length.</p>	<p>Request for a departure from building to the property line where the property line curves at the northeast corner.</p>	<p>The Institute architectural design is an articulated assemblage of three volumes that reflect the 'petal concept' of the research organization within. The northeast corner of the building provides a strong visual termination and expression of this design logic and by being set perpendicular to Broad Street effectively signifies a strong rotation of the city street and sidewalk grid system and creates additional public open space with overhead weather protection at the auditorium.</p>	<p>A-1 Responding to Site Characteristics A-2 Streetscape Compatibility A-4 Human Activity A-10 Corner Lots C-2 Architectural Concept and Consistency D-1 Pedestrian Open Spaces and Entrances</p>



ADDENDUM

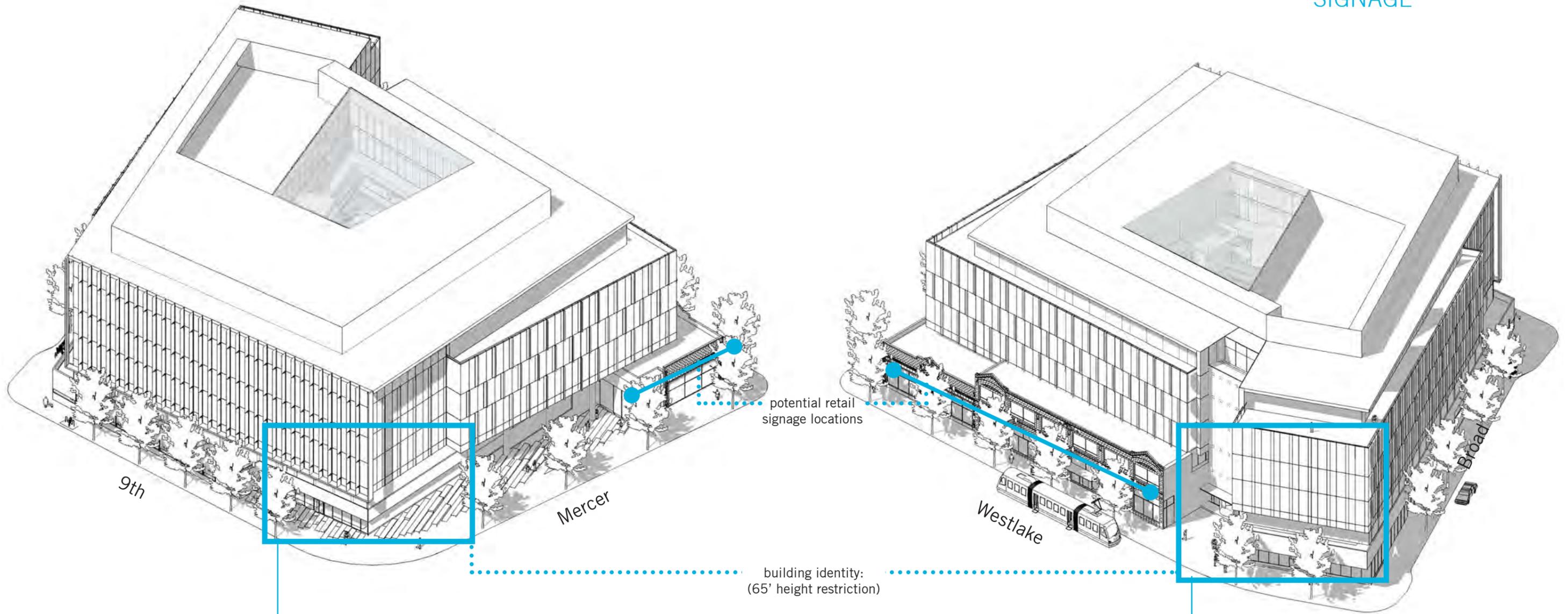


STREETSCAPE

Streetscape design follows the standards set in place by SDOT; this allows for consistency with adjacent blocks in the following areas:

- Sidewalk paving pattern
- Tree/plant species
- Planting bed locations/dimensions

SIGNAGE

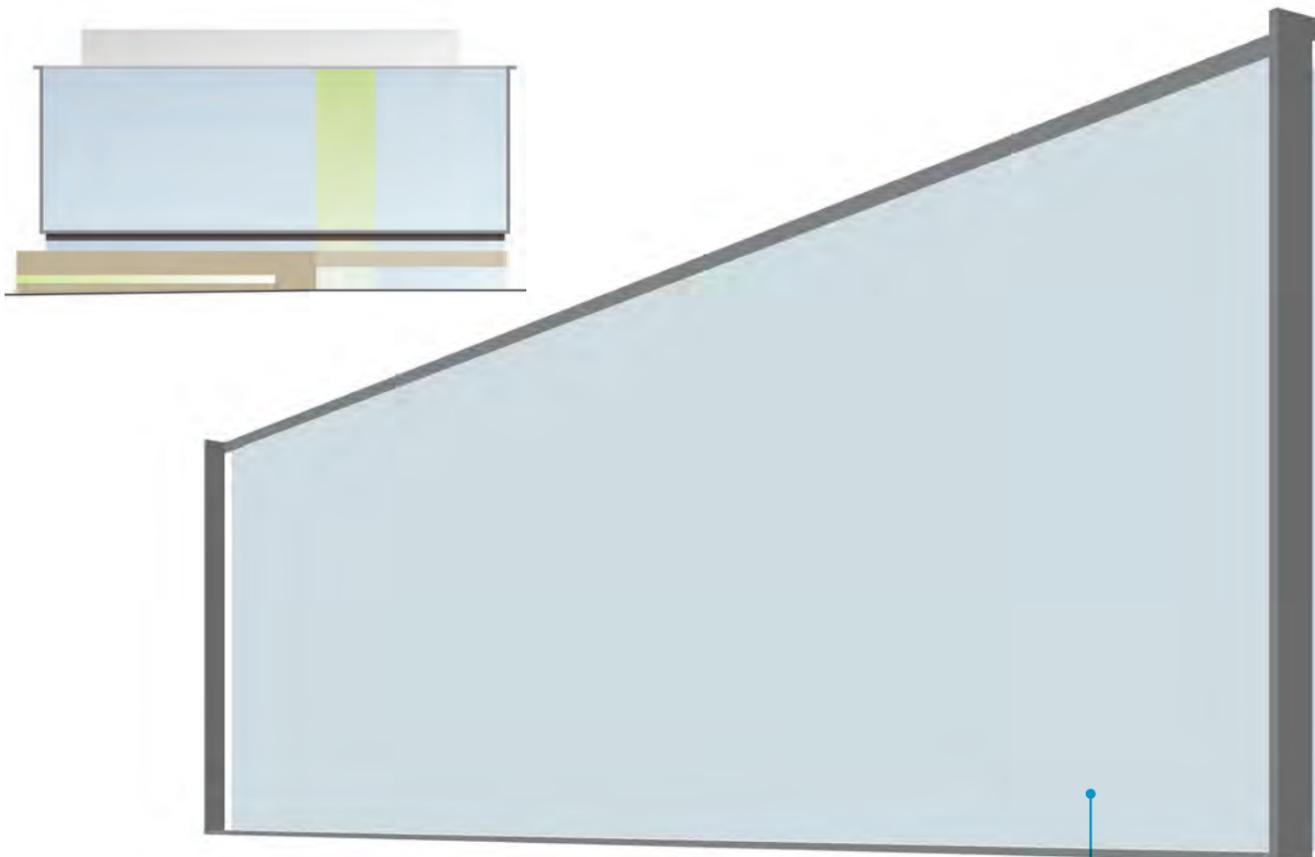


Opportunity to maximize visibility on Mercer
Southwest bird's-eye view

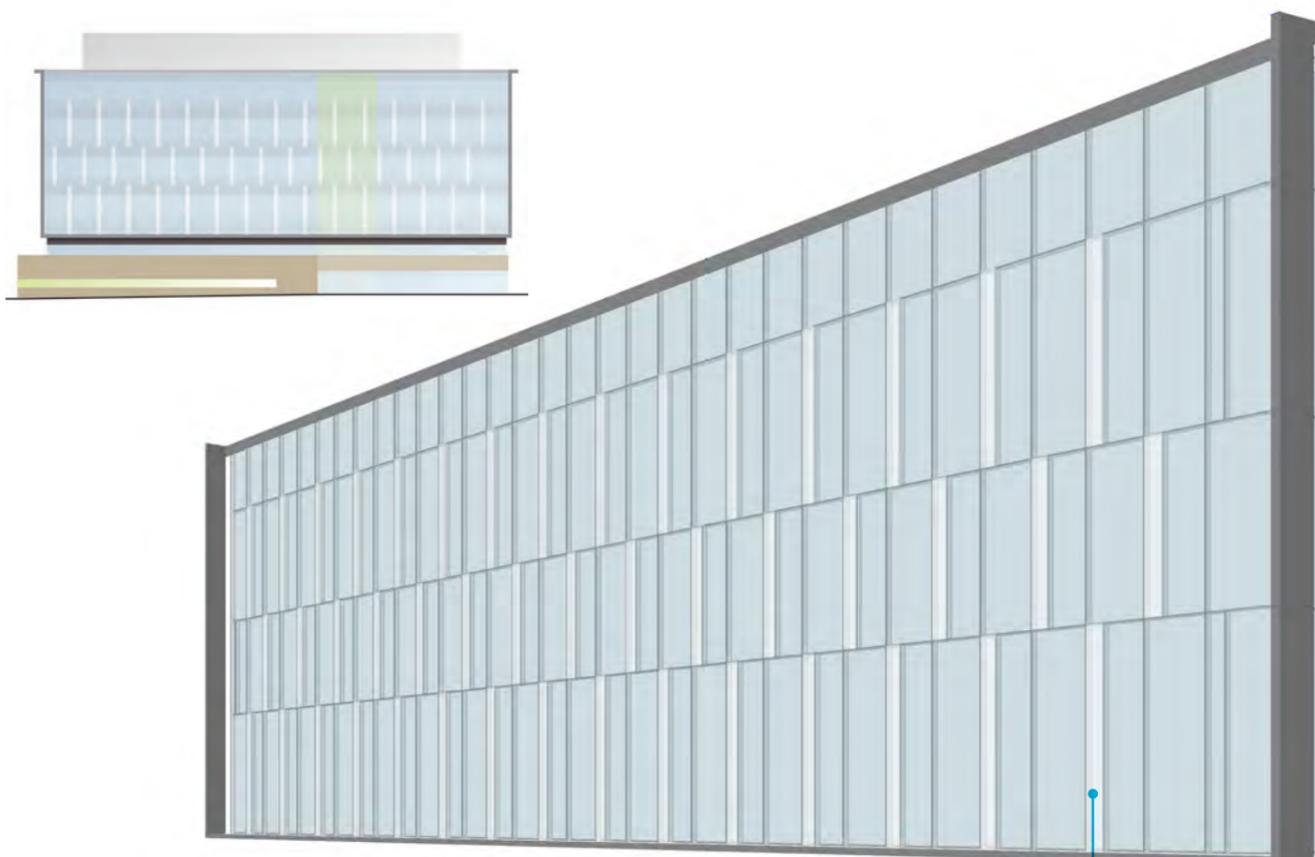
Opportunity to maximize visibility on Westlake
Northeast bird's-eye view



WEST FACADE

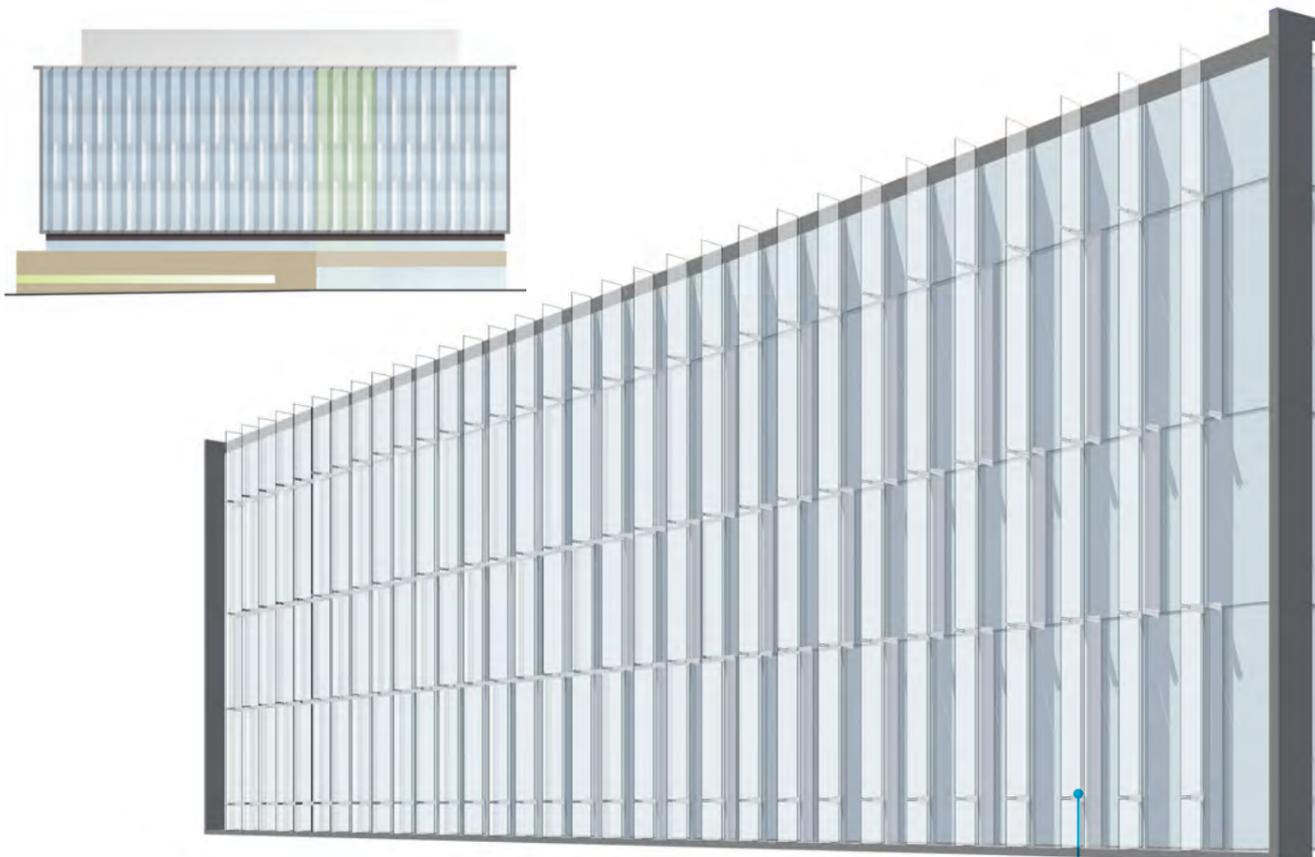


inner-most layer is an expression of transparency and reflectivity within a “container” that frames the architectural concept, allowing for maximum daylight and views

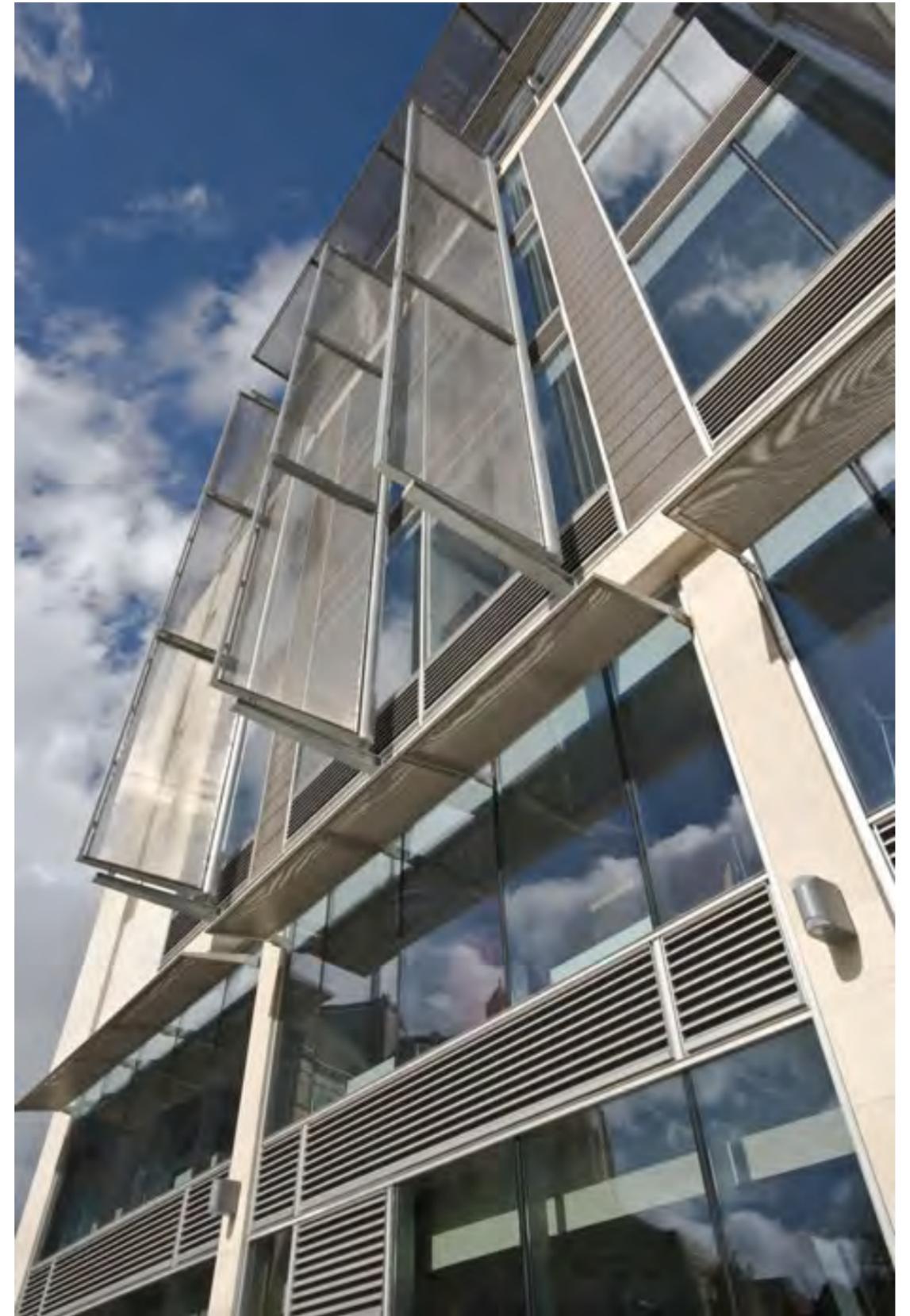


operable windows, set a staggered pattern and inset with fine metal mesh, add complexity and visual interest to the facade while increasing occupant comfort

WEST FACADE



perforated metal fins add a further layer of modulation to the facade and provide necessary solar shading at vision glazing, while animating the facade through the play of dynamic shadows





DEPARTURES



Departure 4a: Parking/Loading Access (23.48.034.C.3)

- driveways limited to one two-way curbcut to access parking and loading
- proposal is for one two-way curbcut for vehicles and one reversible curbcut for trucks

Departure 4b: Curbcut Width (23.54.030.F)

- curbcuts to have a minimum width of 22' and maximum width of 25'
- proposal is for truck curbcut to have a width of 20' (narrower than minimum)

Departure 6: Facade Frontage at Westlake (23.48.014.C)

- min. frontage at Westlake to be built at property line: 70%
- proposed frontage at Westlake to be built at property line: 62%

Departure 2: Street-Level Uses (23.48.019.B)

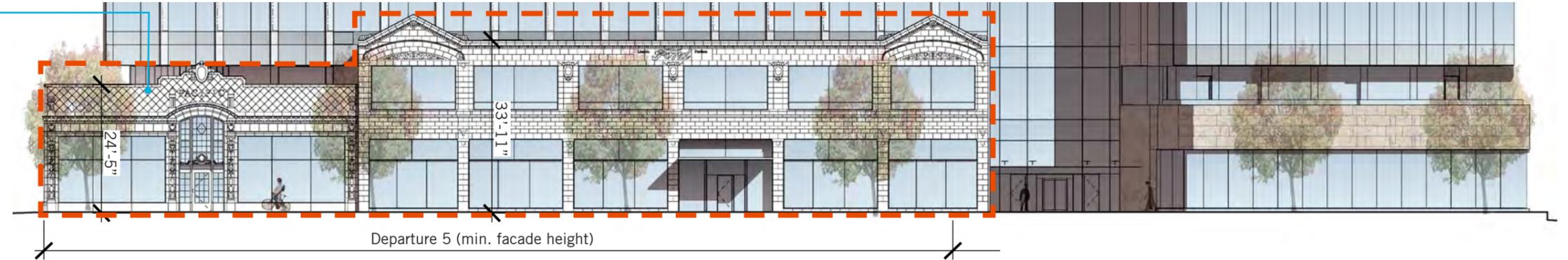
- min. frontage at Westlake to be occupied by street-level uses: 75%
- proposed frontage at Westlake to be occupied by street-level uses: 62%

Departure 1: Street-Level Setbacks (23.48.014.D)

- max. setback from property line: 12'
- max. amount of facade that is set back more than 12': 30%
- proposed amount of facade that is set back more than 12': 69%

Departure 5: Minimum Facade Height
(23.28.014.B.1)

- min. facade ht. at Westlake: 45'
- proposed (required by Landmarks) facade height at McKay: 24'-5" / 33'-11"



Westlake Avenue N Elevation

Departure 5: Minimum Facade Height
(23.28.014.B.2)

- min. facade ht. at Mercer: 25'
- proposed (required by Landmarks) facade height at McKay: 23'-8"



Mercer Street Elevation

Departure 3: Street-Level Transparency
(23.48.018.B)

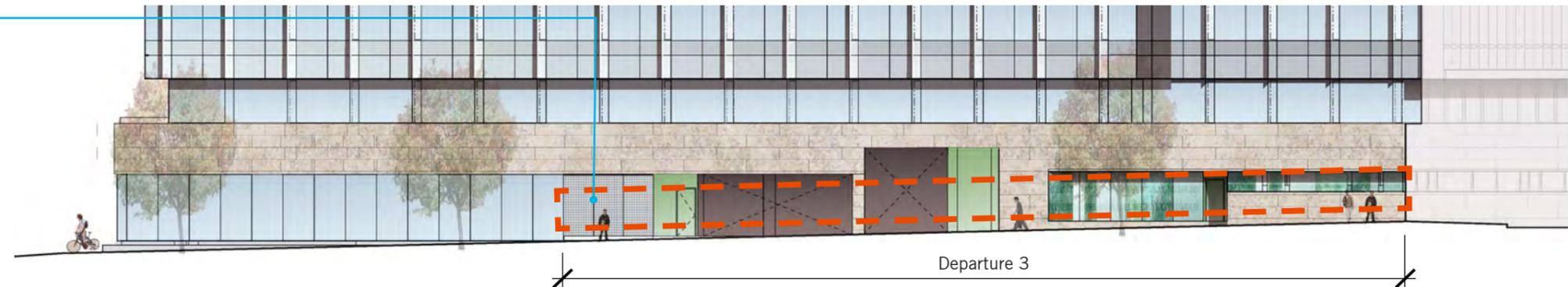
- max. length of any blank segment: 30'
- proposed length of blank segment: 102'-5"
- *note that requirements for overall transparency are met



9th Avenue N Elevation

Departure 3: Street-Level Transparency
(23.48.018.B)

- max. length of any blank segment: 30'
- proposed length of blank segment: 141'-5"
- *note that requirements for overall transparency are met



Broad Street Elevation

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