

Design Review Board

777 THOMAS STREET | SEATTLE, WASHINGTON

MACFARLANE PARTNERS / ANKROM MOISAN ARCHITECTS, INC.
07.31.2013 DPD PROJECT # 3014045



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PROJECT ADDRESS

777 Thomas Street
Seattle, WA 98109

PROJECT TEAM

OWNER/APPLICANT:

MacFarlane Partners

201 Spear Street, 14th Floor
San Francisco, CA 94105
415.356.2500
Contact: Susan Smartt
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ARCHITECT:

Ankrom Moisan Architects

117 South Main Street, Suite 400
Seattle, WA 98104
206.576.1600
Contact: Ricky Teh
RickyT@amaa.com

PROGRAM GOALS

1. Use Type III wood frame construction
2. Achieve a minimum 174 residential units
3. 0.80 parking ratio minimum

DEVELOPMENT PROFILE

1. 174 Units
2. 113,000 sf Net Rentable Residential
3. 5,070 sf Retail Space
4. 130 Parking Spaces

PROJECT GOALS

JOB / HOUSING BALANCE

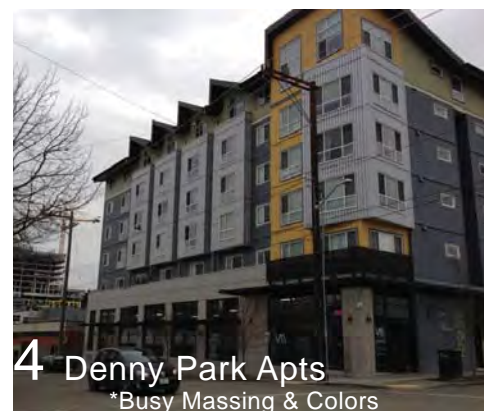
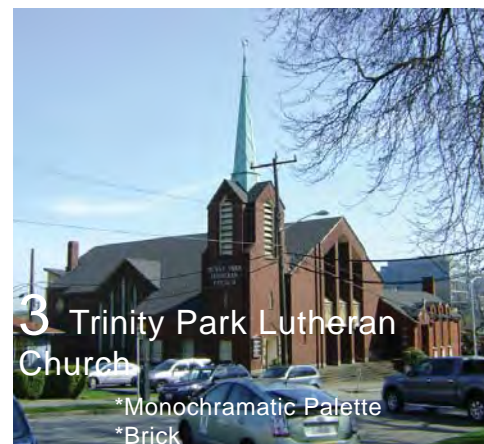
Urban infill promotes job and housing balance, promoting 24 hour environments where people work/live/play all near their home. This results in less vehicle miles traveled, more use of public transit, etc.

MAKE IT MEMORABLE

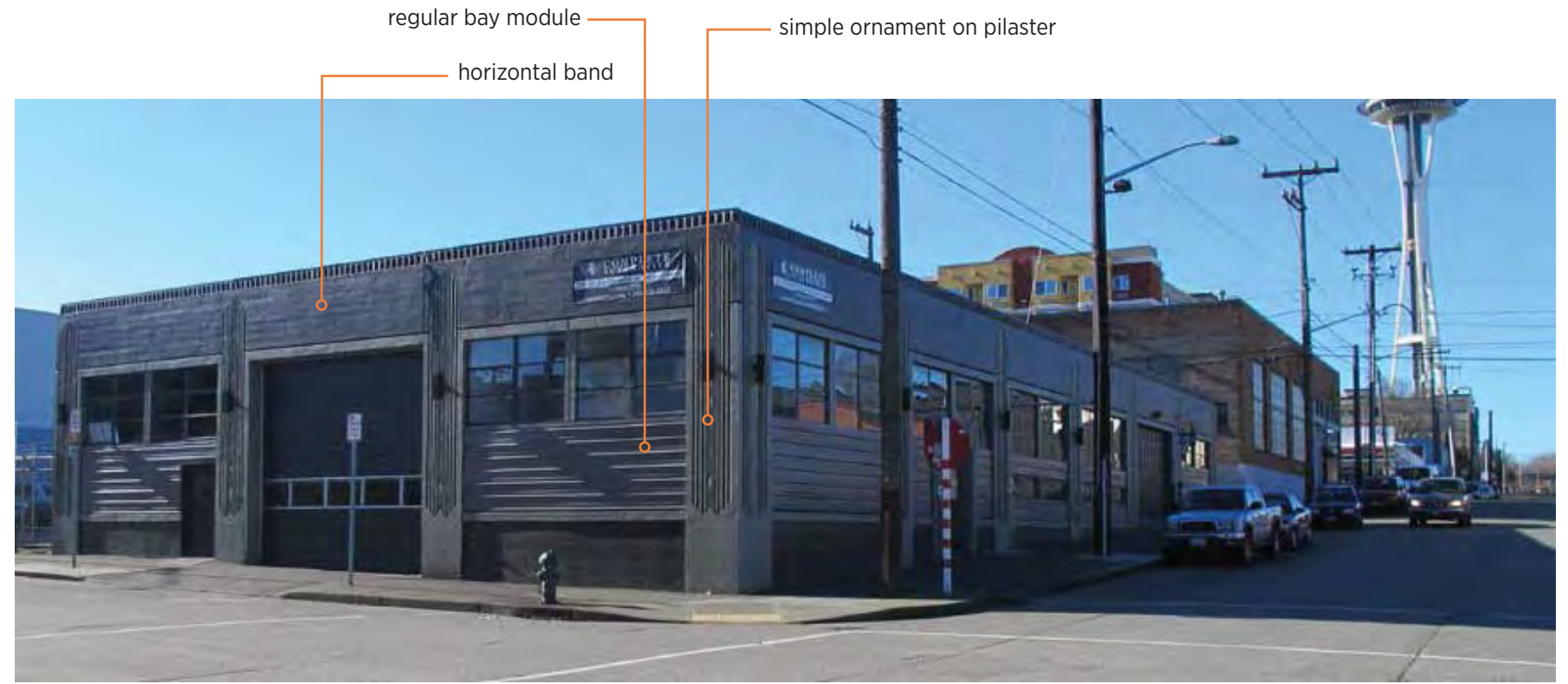
Our project thrives to be unique in a sea of non-distinct mid-rise market rate housing. Our project will tell a story unique to the neighborhood with townhomes aimed at young families, work spaces geared to small businesses and material and form used in distinctive ways.

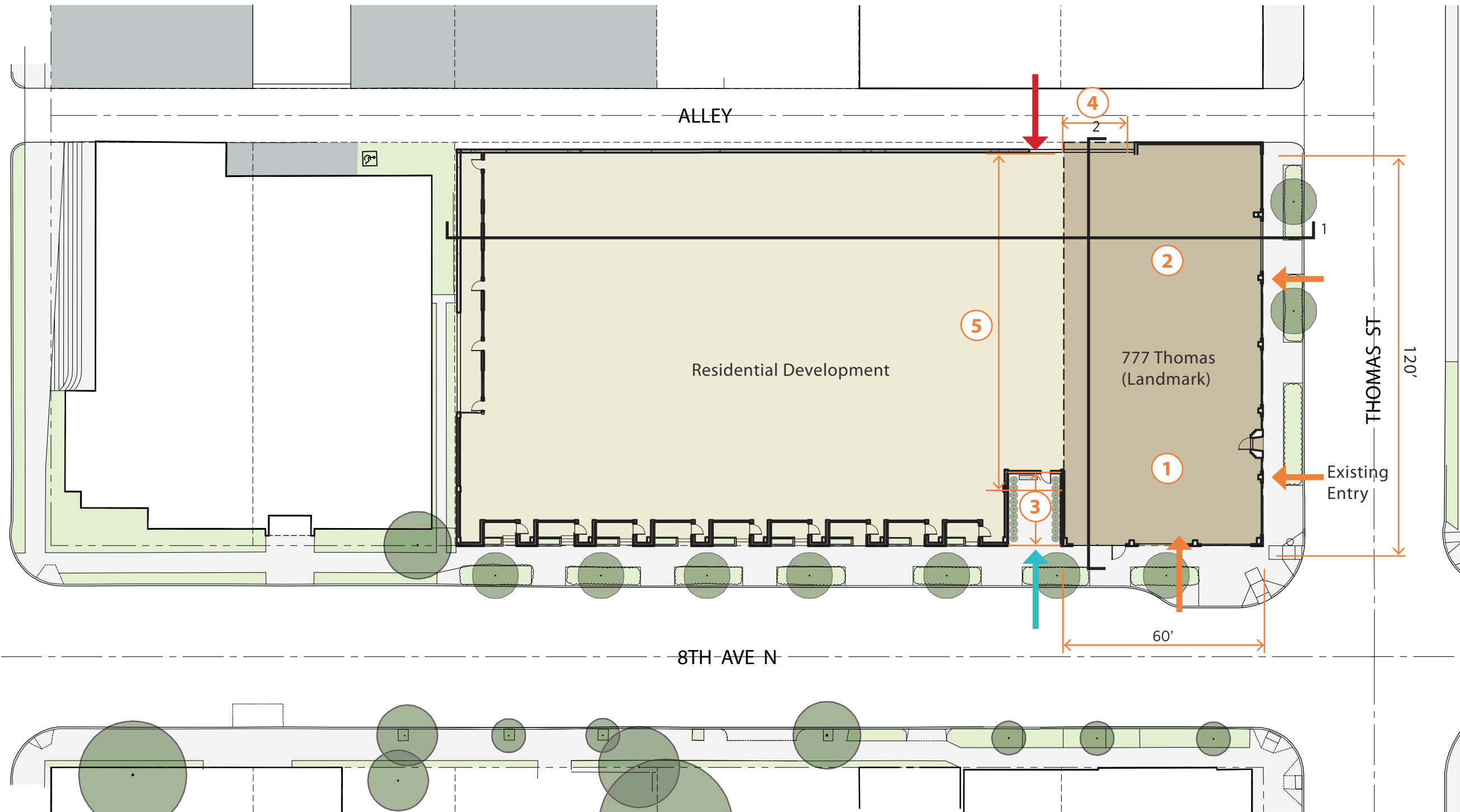


SITE ANALYSIS : context



Landmark's Features





Proposed Changes

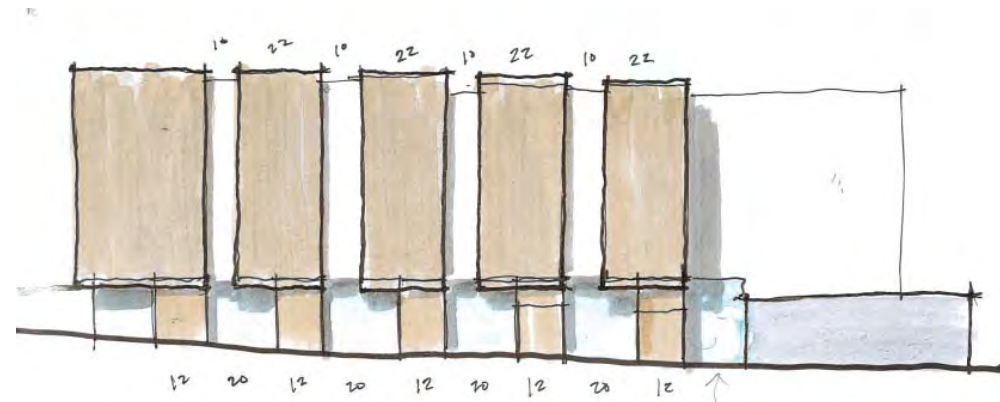
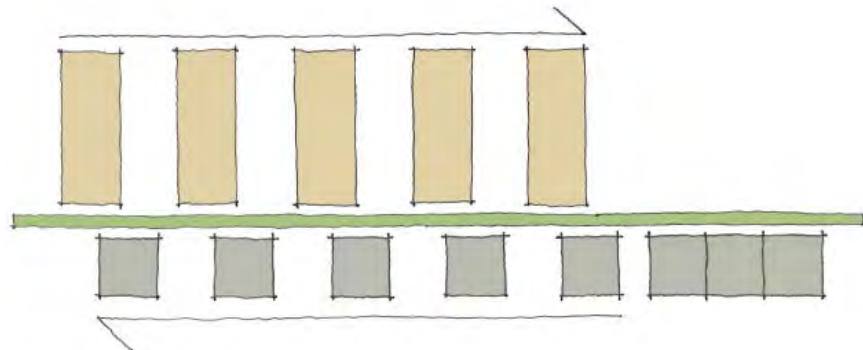
- 1** Remove existing floor slab and replace with new slab to accommodate underground parking, below landmark and adjacent new development.
- 2** Remove existing roof structure and replace with new concrete deck to accommodate new residential development and outdoor amenity space.
- 3** Remove existing wall during construction and rebuild with salvaged bricks
- 4** Remove portion of existing wall to accommodate parking entry. Site topography requires parking entry to be located here.
- 5** Remove existing wall to engage landmark with adjacent new development.

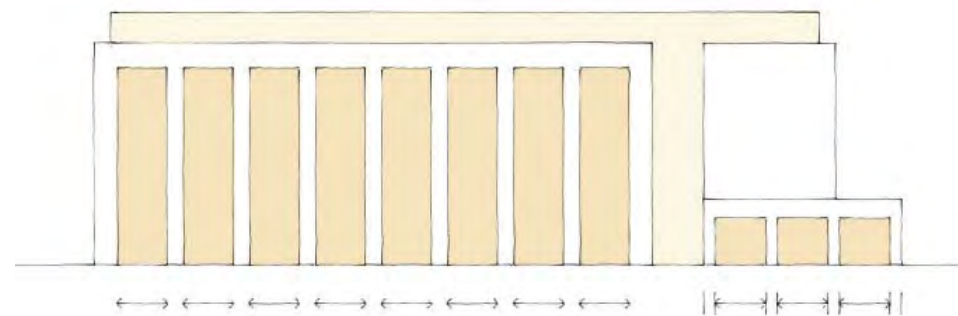
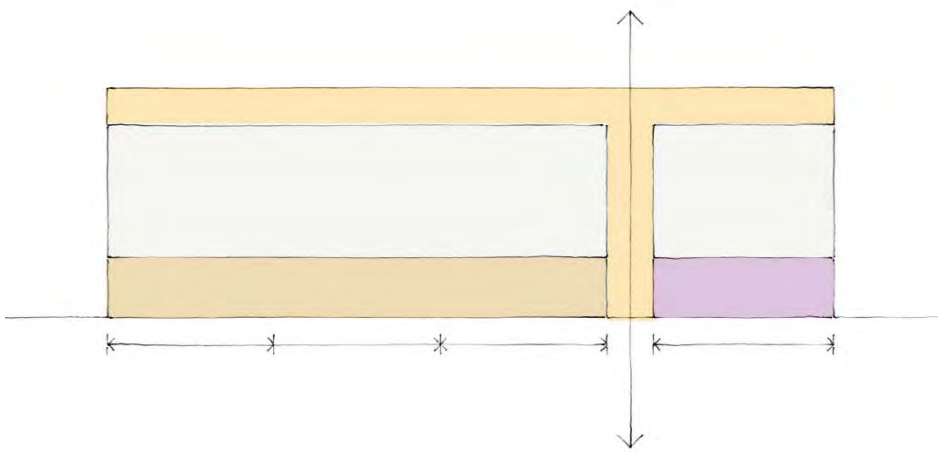
-  Residential Entry
-  Retail Entry
-  Vehicle Entry

Site Plan - Proposed Design

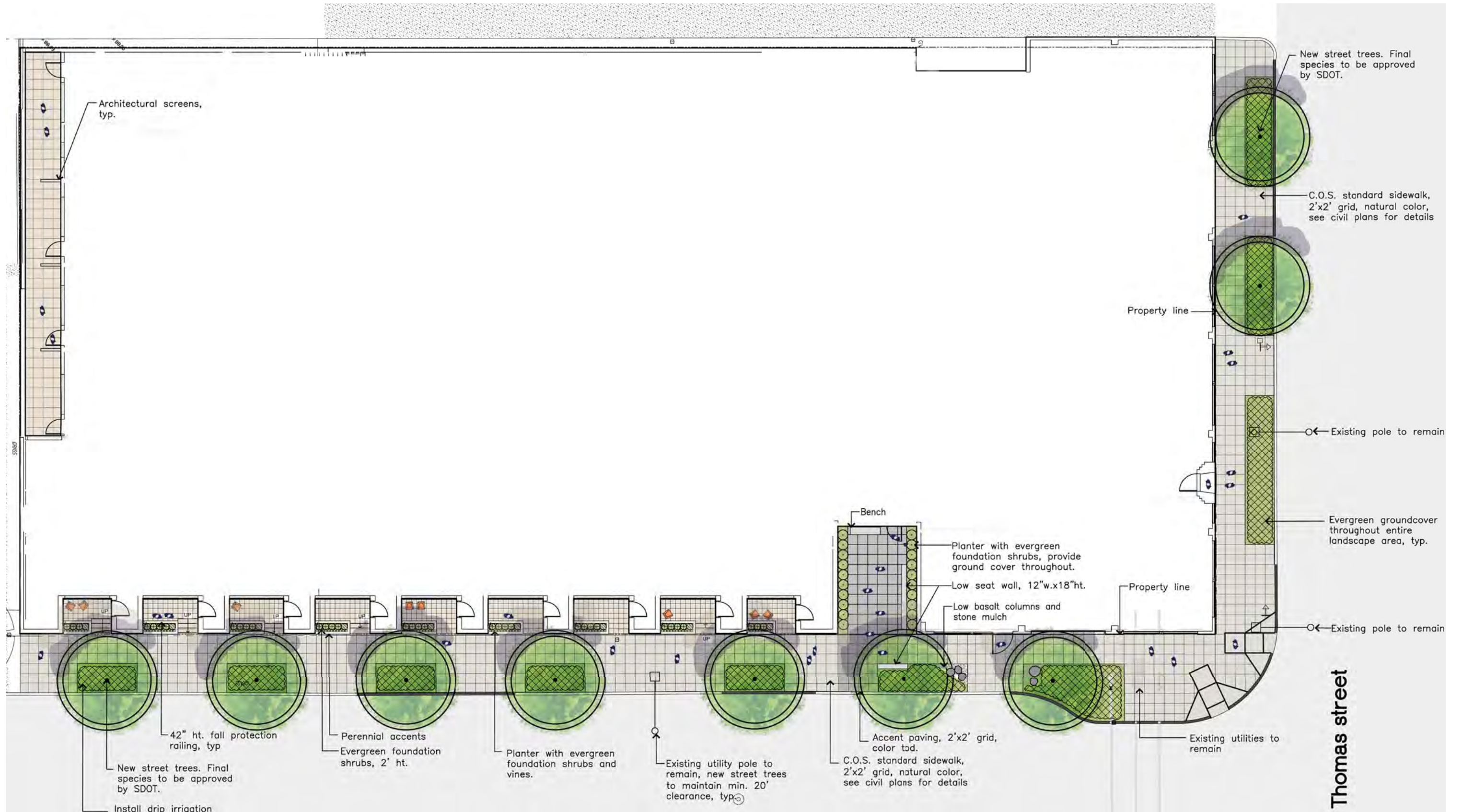
0 8' 16' 32'







Proposed Site Plan



Proposed Development: Ground Floor Plan



Proposed Development: Second Floor Plan



SCALE: 1"=20'

0 5' 10' 20'

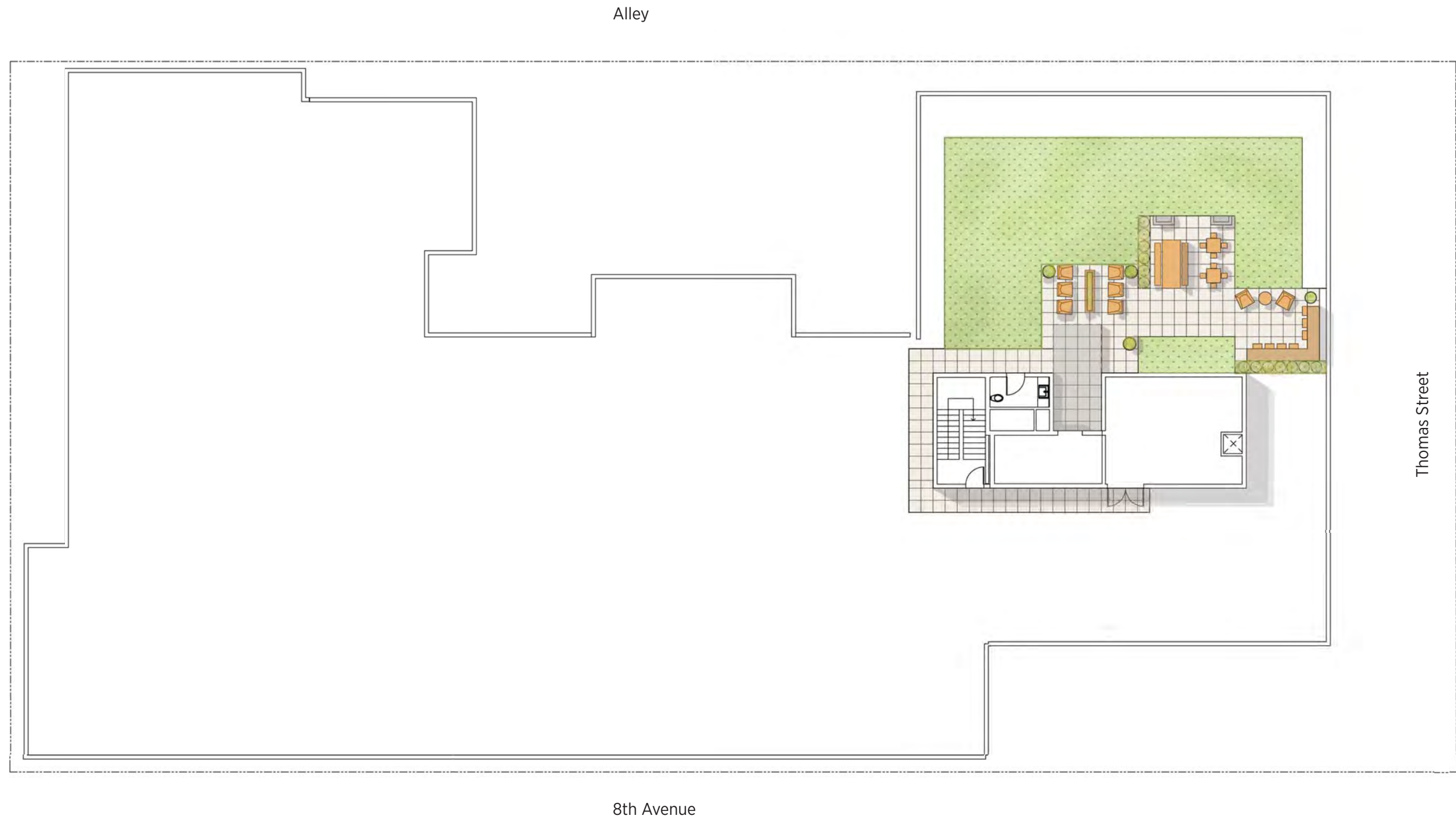




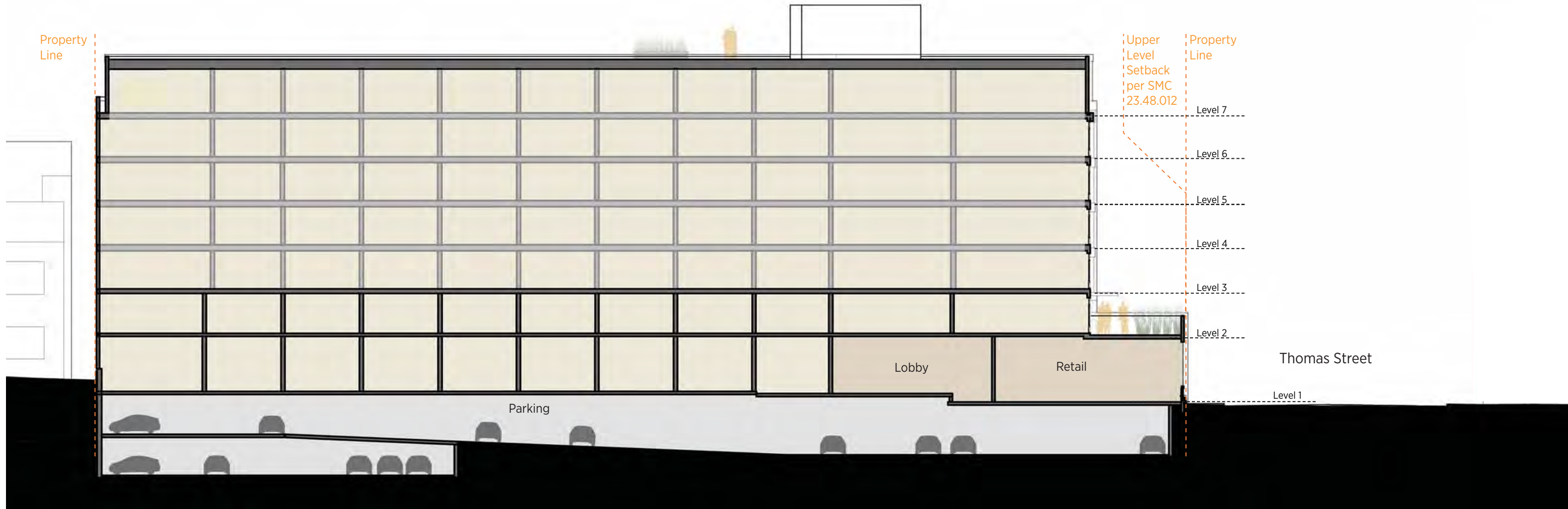
SCALE: 1"=20'

0 5' 10' 20'

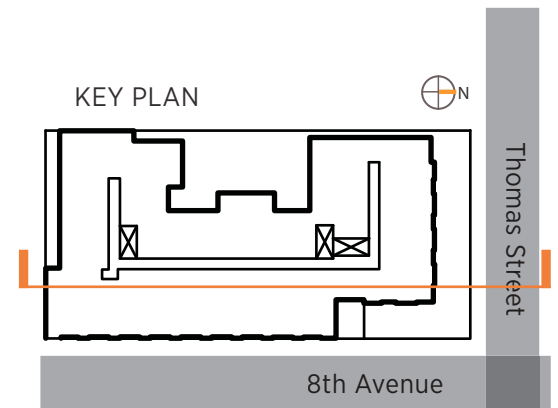




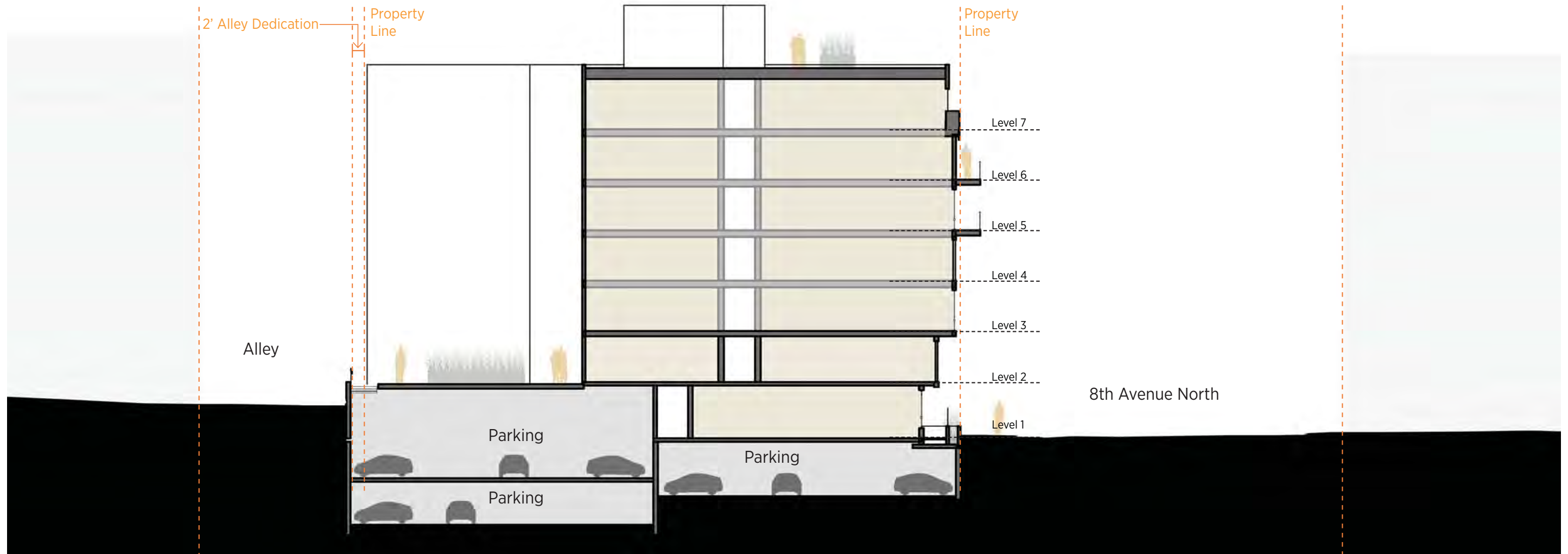
SECTIONS : A - North South



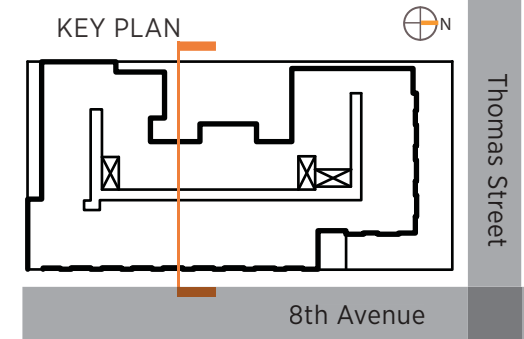
- Units
- Parking/BOH
- Retail



SECTIONS : B - East West



- Units
- Parking
- Retail





BRICK - COLE CREEK



FIBER CEMENT PANEL



FIBER CEMENT PLANK

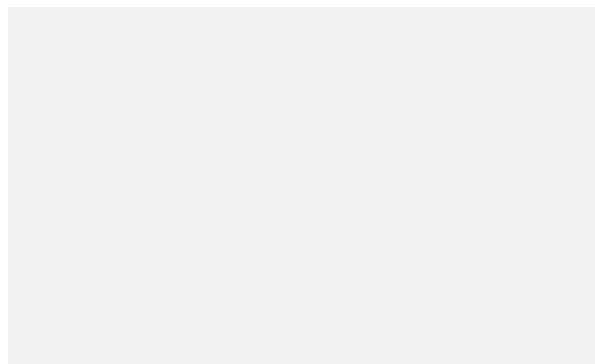


PAINTED CONCRETE



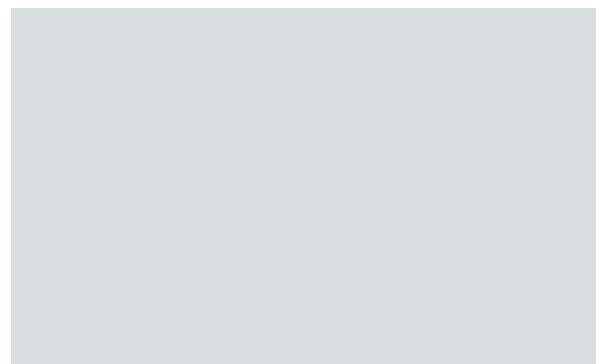
WHITE VINYL WINDOWS

MAIN BODY



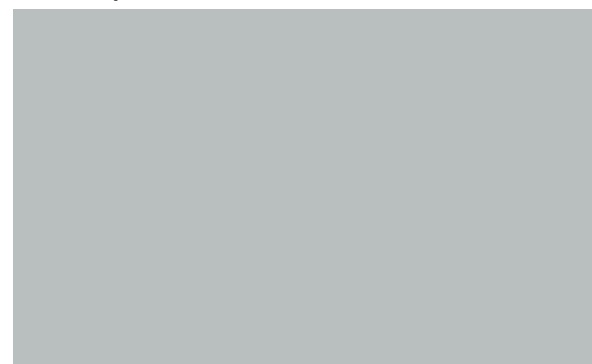
MISTY GRAY

PLANKS



BUNNY GRAY

FRAME/TRIM



THUNDERCLOUD GRAY

PENTHOUSE

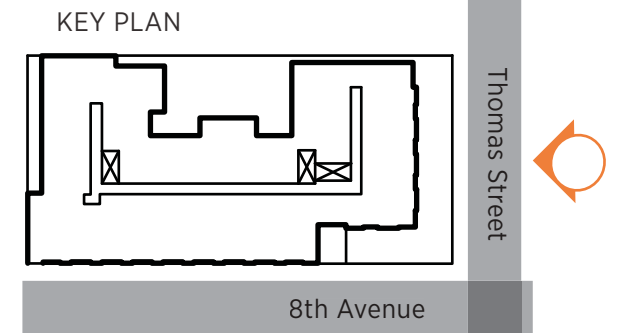


DEEP SILVER

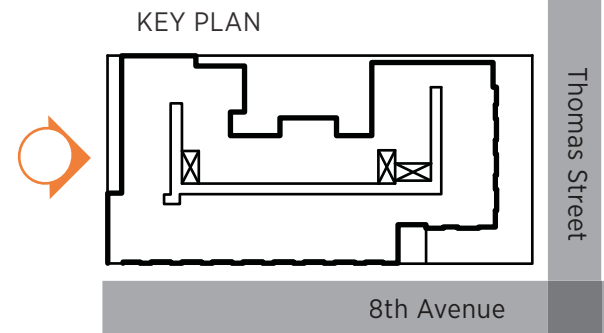
ELEVATIONS : East - 8th Avenue



ELEVATIONS : North - Thomas Street







MAJOR THEMES

- 1: 8TH AVENUE
- 2: EMPHASIZING PRIMARY RESIDENTIAL ENTRY
- 3: RESPONSE TO ALLEY
- 4: RESPONSE TO NEIGHBORING CHURCH
- 5: RESPONSE TO HISTORIC LANDMARK
- 6: LIGHTING AND SIGNAGE
- 7: DEPARTURES

Design Review Board Comments from DRB 1

GUIDELINE	DRB RECOMMENDATION	GUIDELINE	DRB RECOMMENDATION
<div style="background-color: #4CAF50; width: 15px; height: 20px; margin-bottom: 5px;"></div> A-2 STREETScape COMPATIBILITY	The Board reiterated the applicant should discuss options with SDOT to move the curb line further out to the street and provide a curb bulb.	<div style="background-color: #9C27B0; width: 15px; height: 20px; margin-bottom: 5px;"></div> D-8 TREATMENT OF ALLEYS	The Board noted that locating the garage door entry should line up with the wall of the building above.
<div style="background-color: #4CAF50; width: 15px; height: 20px; margin-bottom: 5px;"></div> A-3 ENTRANCES VISIBLE FROM THE STREET	The Board suggested that the planter walls be replaced with lower planters and taller landscaping to soften and screen the patios.	<div style="background-color: #795548; width: 15px; height: 20px; margin-bottom: 5px;"></div> D-10 COMMERCIAL LIGHTING	Operable rollup doors, lighting, and any other strategies that can help to enhance the commercial viability on Thomas St would be strongly supported by the Board.
<div style="background-color: #4CAF50; width: 15px; height: 20px; margin-bottom: 5px;"></div> A-6 TRANSITION BETWEEN RESIDENCE AND STREET	The Board recommended that the planters separating the residential entries and the sidewalk should relate to a human scale and provide a visual transition to the entries.	<div style="background-color: #FF5722; width: 15px; height: 20px; margin-bottom: 5px;"></div> D-11 RESPECT FOR ADJACENT SITES	Operable rollup doors, lighting, and any other strategies that can help to enhance the commercial viability on Thomas St would be strongly supported by the Board.
<div style="background-color: #00BCD4; width: 15px; height: 20px; margin-bottom: 5px;"></div> D-1 PEDESTRIAN OPEN SPACES AND ENTRANCES	The Board noted that the area adjacent to the south facing wall of the historic landmark should be designed to enhance the recessed entry with items such as lighting, landscaping, and benches.	<div style="background-color: #00BCD4; width: 15px; height: 20px; margin-bottom: 5px;"></div> D-12 TRANSITION BETWEEN RESIDENCE AND STREET	The Board recommended that the ROW should include items to delineate the entry location, such as benches or landscaping.
<div style="background-color: #9C27B0; width: 15px; height: 20px; margin-bottom: 5px;"></div> D-6 SCREENING OF DUMPSTERS, SERVICE AREAS	The Board expressed concern the solid waste staging area may not be sufficient. The applicant should demonstrate how the staging area will sufficiently accommodate the collection schedule and amount of solid waste on collection days.	<div style="background-color: #FFEB3B; width: 15px; height: 20px; margin-bottom: 5px;"></div> E-2 LANDSCAPING TO ENHANCE THE BUILDING	The Board would like to see details of landscaping at the base of the dark masonry piers. The landscaping should serve to soften the material and enhance the residential entries. The landscape plan on the south facade should be designed to enhance the individual patio areas, rather than linear landscaping that appears to delineate a walkway.
<div style="background-color: #9C27B0; width: 15px; height: 20px; margin-bottom: 5px;"></div> D-7 PERSONAL SAFETY AND SECURITY	The Board recommended bringing the garage door entry closer to the alley property line to assist with safety and security.		

Relevant Design Guidelines

- A-2 Streetscape Compatibility

- A-3 Entrances Visible from the Street

- A-6 Transition Between Residence and Street

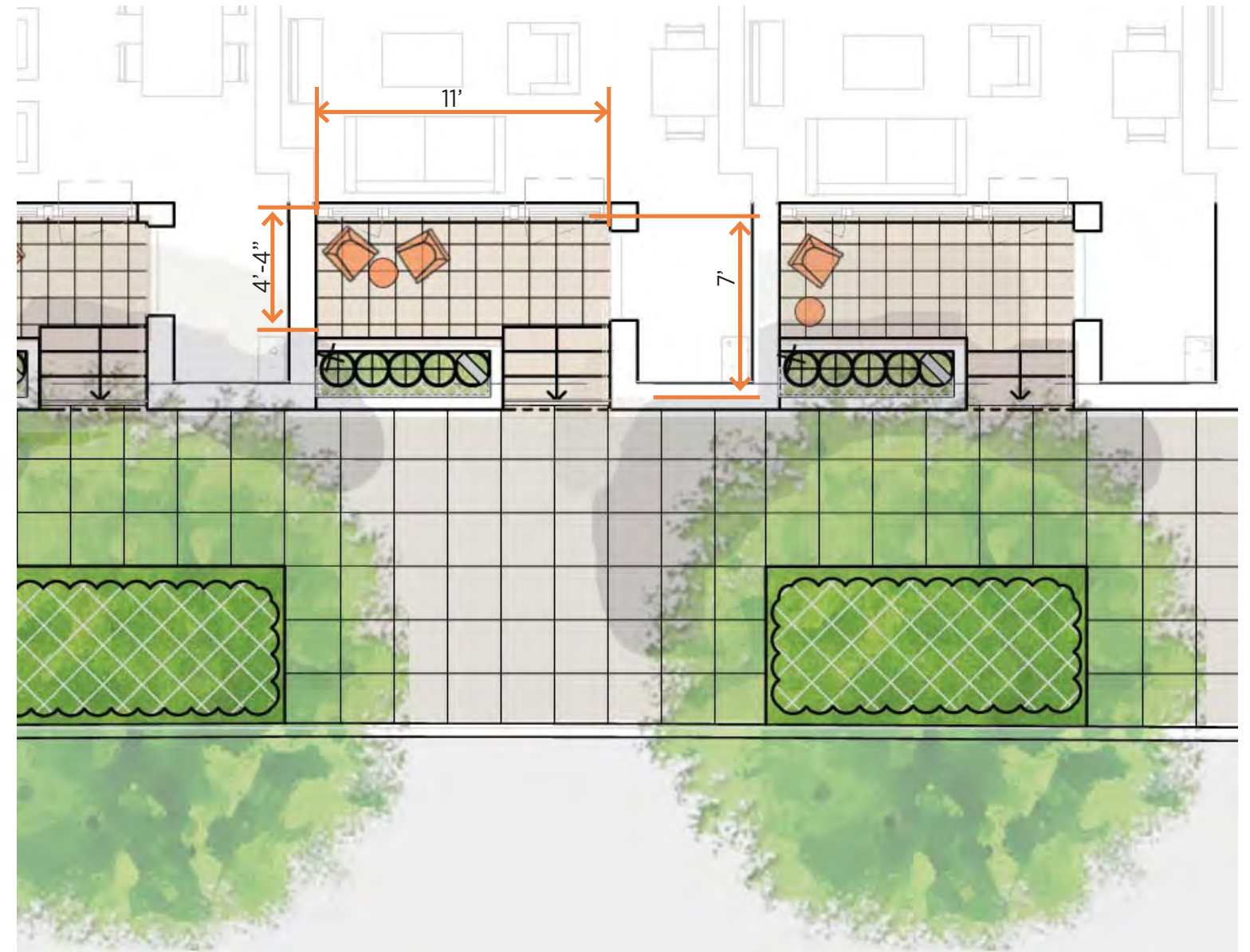
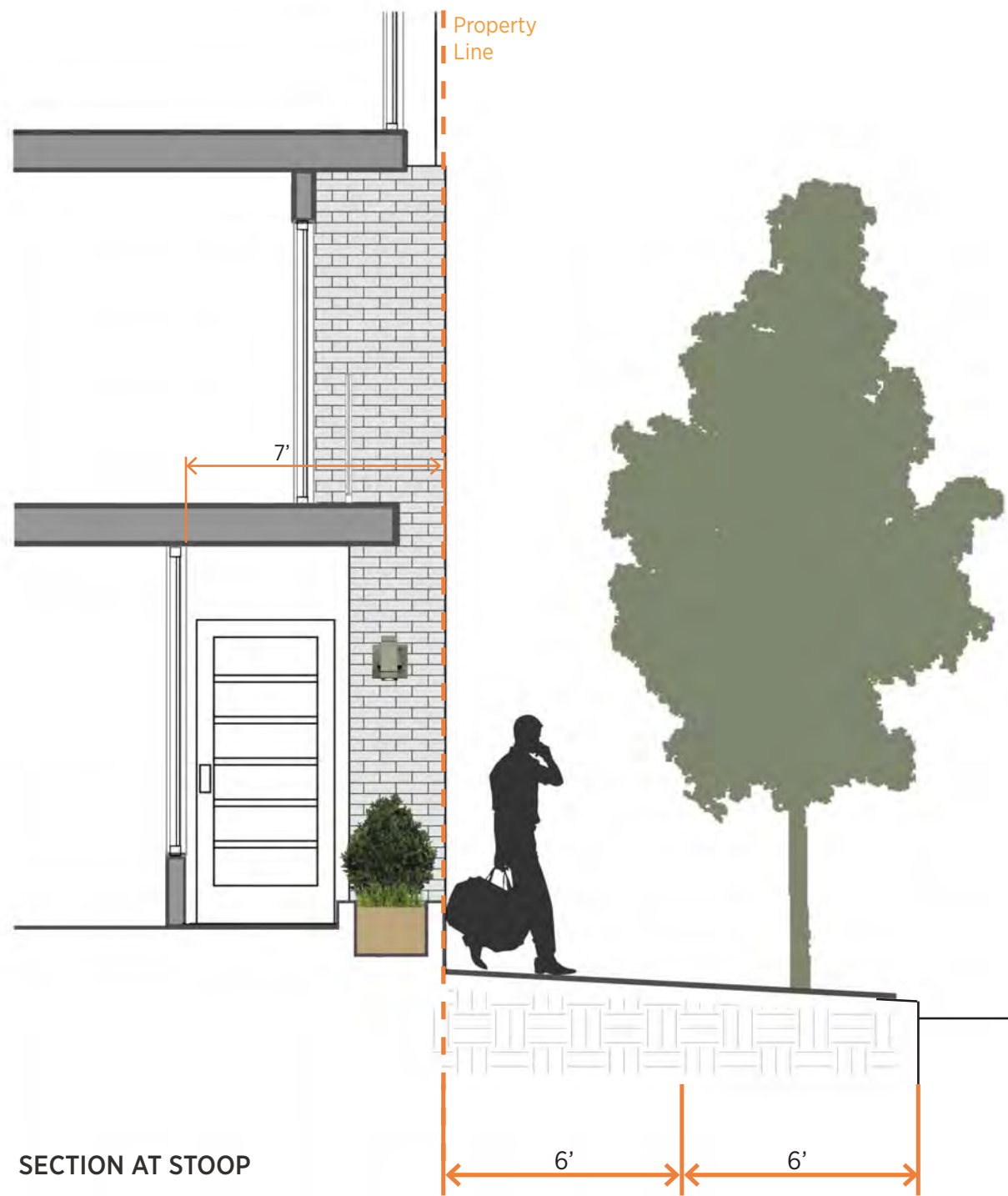
Design Review Board Recommendations

“... the planters separating the residential entries and the sidewalk should relate to a human scale and provide a visual transition to the entries.”

The Board reiterated the applicant should discuss options with SDOT to move the curb line further out to the street and provide a curb bulb.

The Board suggested that the planter walls be replaced with lower planters and taller landscaping to soften and screen the patios.

“The Board appreciated the orientation of the street level entries and felt that the design of the front patios would serve to enhance the entries and usability of the patios.”



B PERENNIALS



HYDRANGEA PANICULATA 'JANE' / LITTLE LIME HYDRANGEA



ASARUM CAUDATUM / WILD GINGER



HEUCHERA V. 'CARAMEL' / CARAMEL CORAL BELLS



HOSTA X. 'BIG DADDY' / BIG DADDY HOSTA

C GROUND COVER



FRAGARIA CHILOENSIS / BARREN STRAWBERRY (**GROUND COVER)



Relevant Design Guidelines

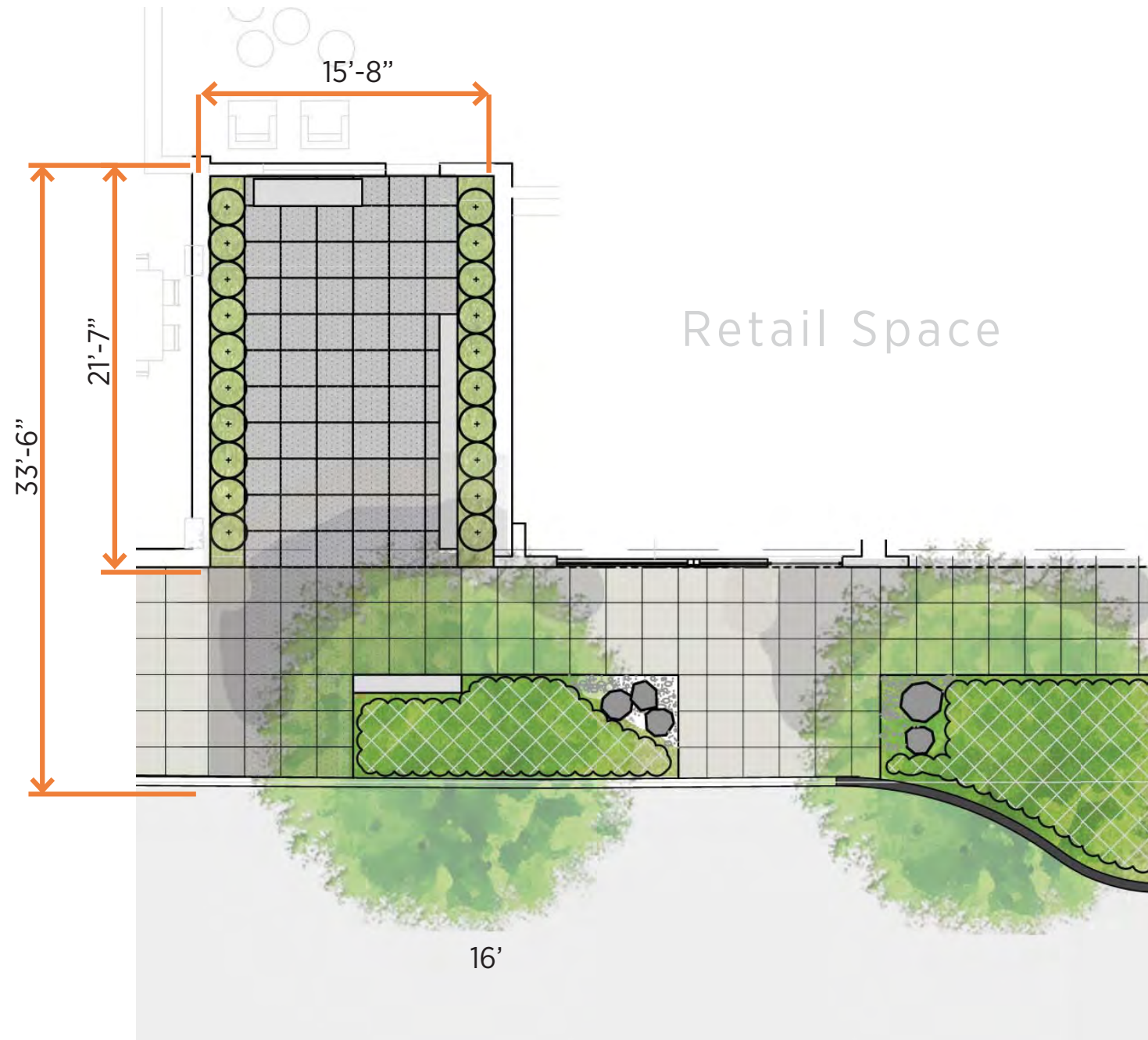
D-1 Pedestrian Open Spaces and Entrances

D-12 Residential Entries and Transitions

Design Review Board Recommendations

The Board noted that the area adjacent to the south facing wall of the historic landmark should be designed to enhance the recessed entry with items such as lighting, landscaping, and benches.

The Board recommended that the ROW should include items to delineate the entry location, such as benches or landscaping



ENLARGED ENTRY LANDSCAPE PLAN



1. LANDSCAPE FORMS PARALLEL BENCH



2. LOW ACCENT WALLS.



3. PET REST AREA IN PLANTERS.



4. PLANTINGS AT FOUNDATION AND CANOPY.





Relevant Design Guidelines

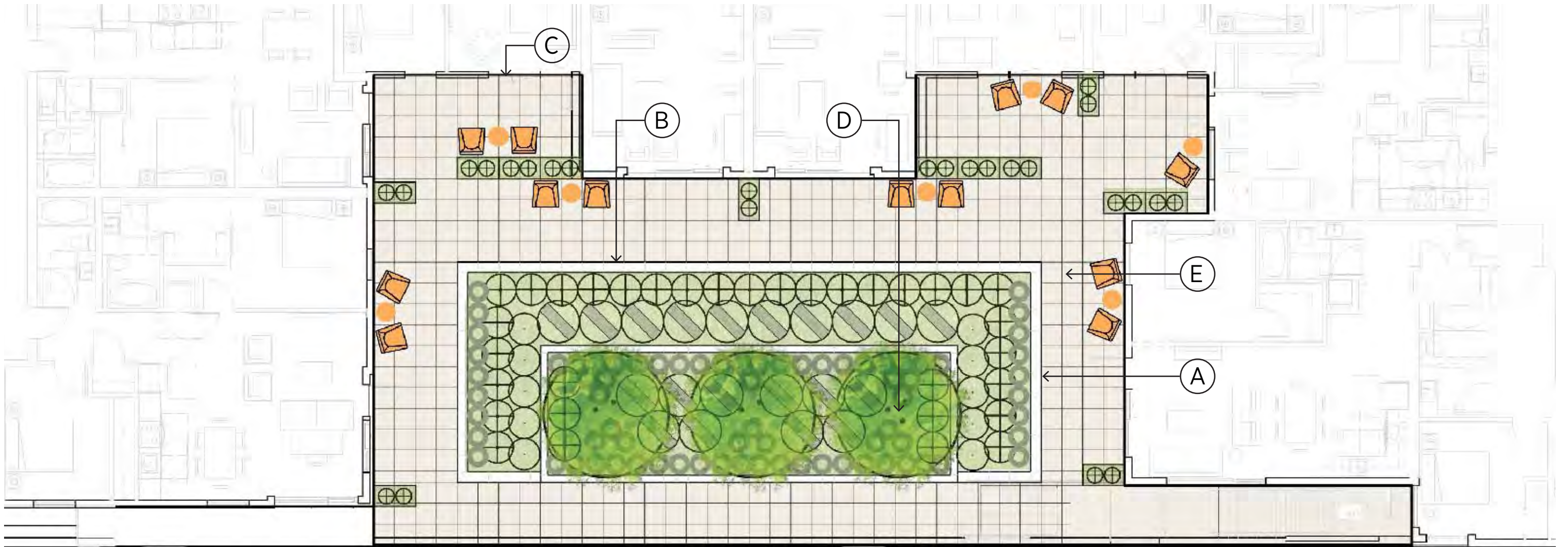
- D-6 Screening of Dumpsters, Utilities, and Service Areas
- D-7 Personal Safety and Security
- D-8 Treatment of Alleys

Design Review Board Recommendations

“The Board was concerned with the safety issues presented by the recessed garage entry.”

“The applicant should demonstrate how the staging area will sufficiently accommodate the collection schedule and amount of solid waste on collection days.”

The Board noted that locating the garage door entry should line up with the wall of the building above.



A PISA UNIT BLOCK WALLS



B LARGE MULTI-LEVEL PLANTING AREA



C TOURNESOL SITeworks PLANTERS FOR PRIVACY



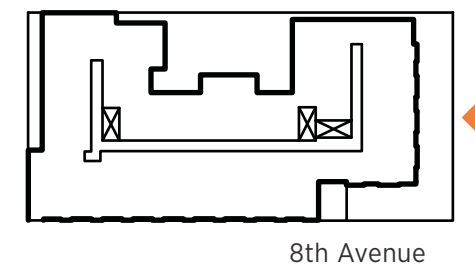
D ACCENT TREES, (STEWARTIA SP.)



E ARCHITECTURAL SLAB PAVING 24"x24" (TYP.)



KEY PLAN

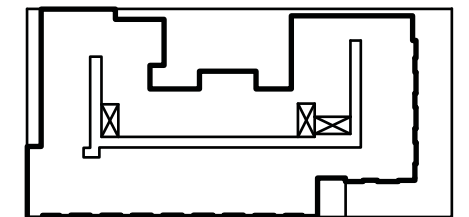


Thomas Street

8th Avenue

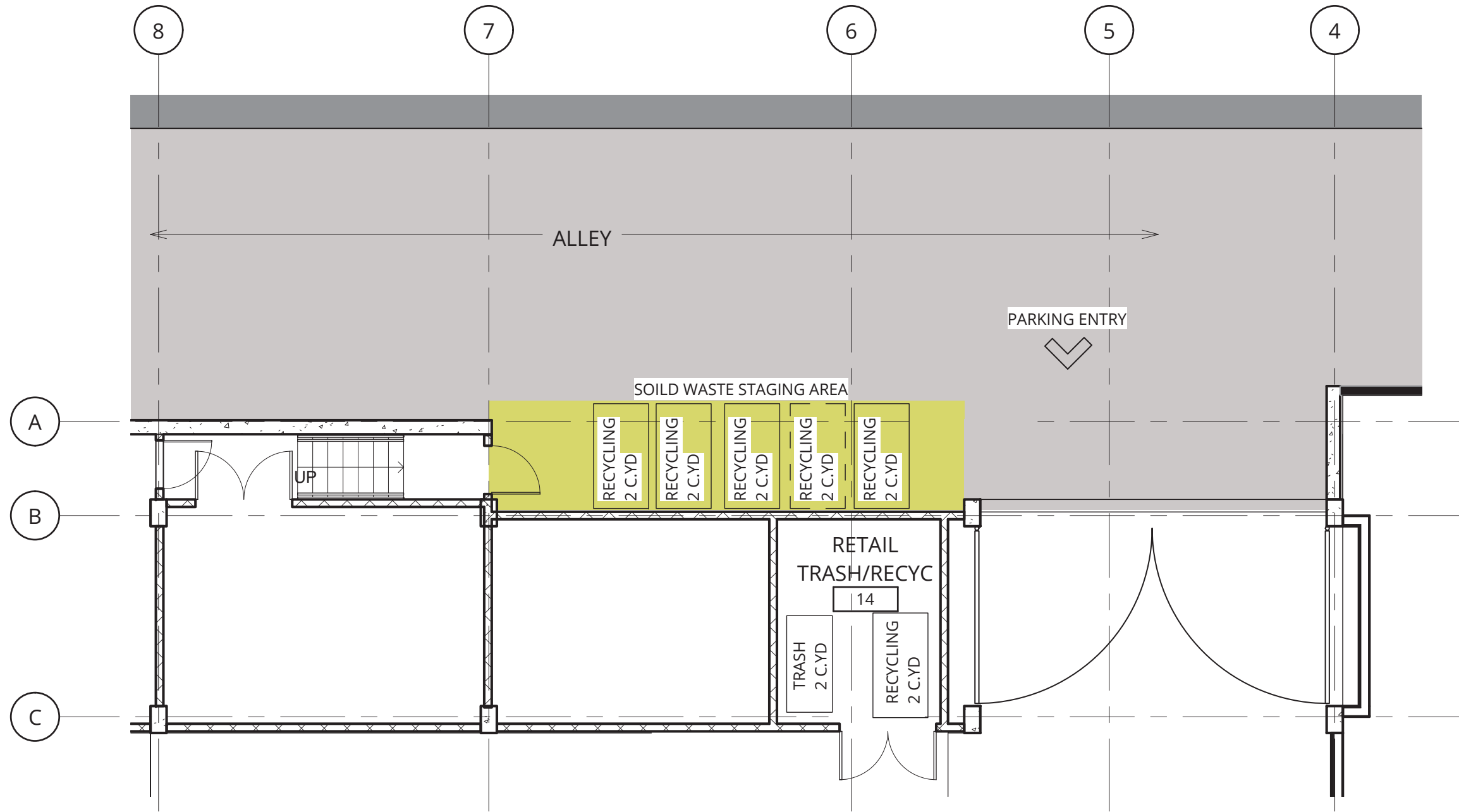


KEY PLAN



8th Avenue

Thomas Street



TRASH / RECYCLE STAGING AREA

THE TRASH AND RECYCLING STRATEGY INCLUDES A LANDING ADJACENT TO THE ALLEY TO STAGE DUMPSTERS ON COLLECTION DAYS AND ALLOW CLEAR TRAVEL PATHS THROUGH THE ALLEY.

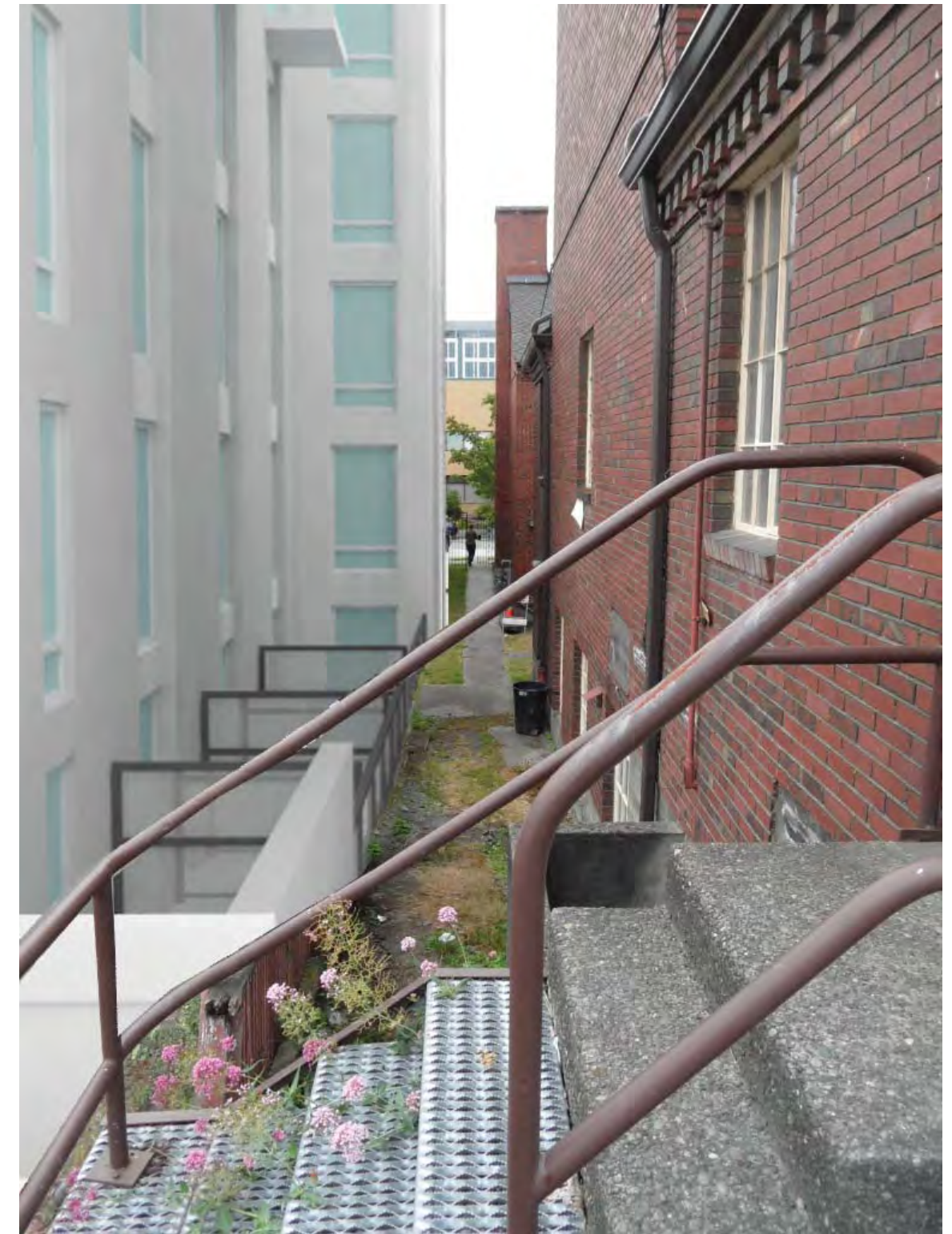
Relevant Design Guidelines

E-2 Landscaping to Enhance the Building
and / or Site

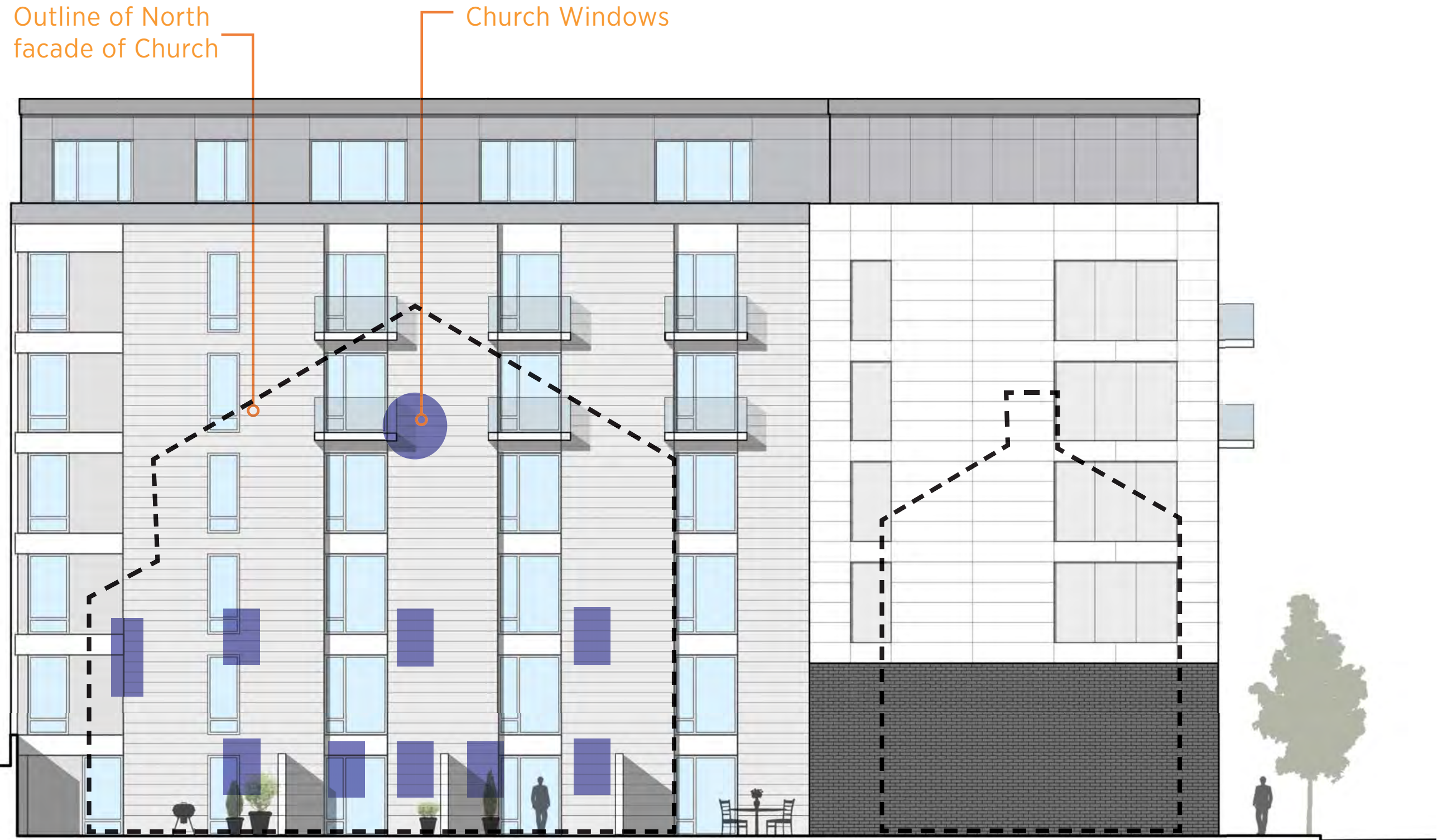
Design Review Board Recommendations

“The landscape plan should be designed to enhance the individual patio areas, rather than linear landscaping that appears to delineate a walkway.”

“... the design of individual terraces, setback, and reflective surfaces is a positive response to the EDG...”





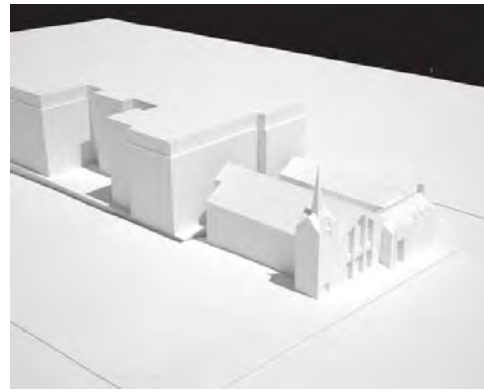


DRB Theme 4 : Response to Neighboring Church

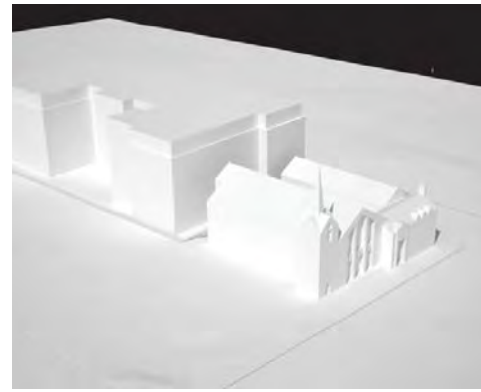
JUNE 21



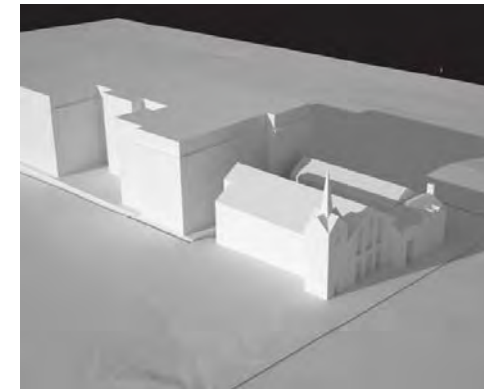
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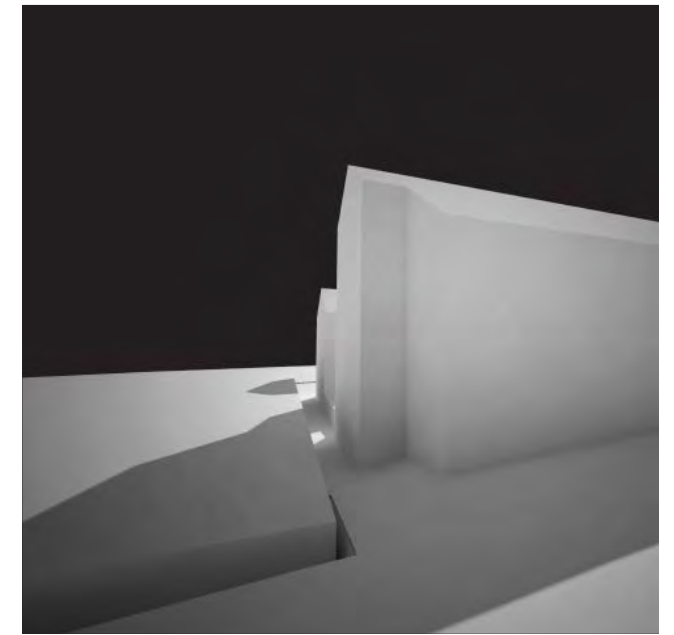
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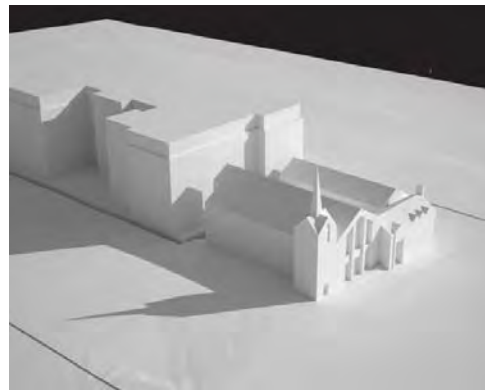
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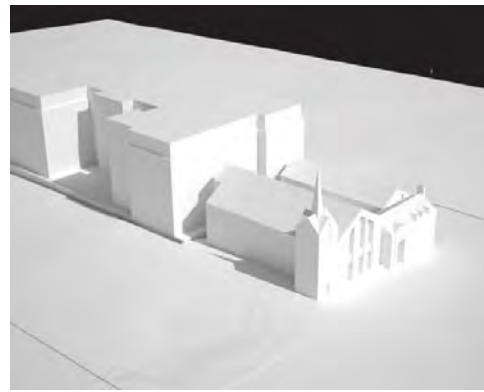
18:00



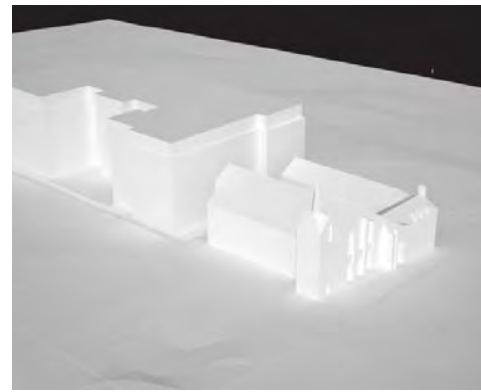
MAR 21



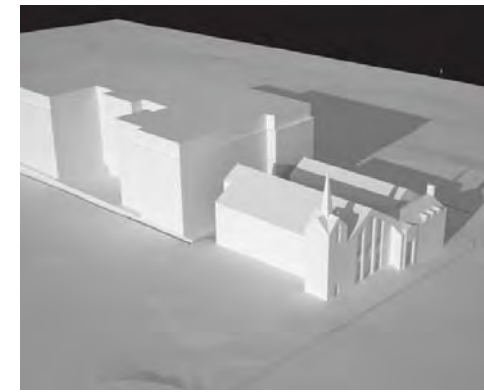
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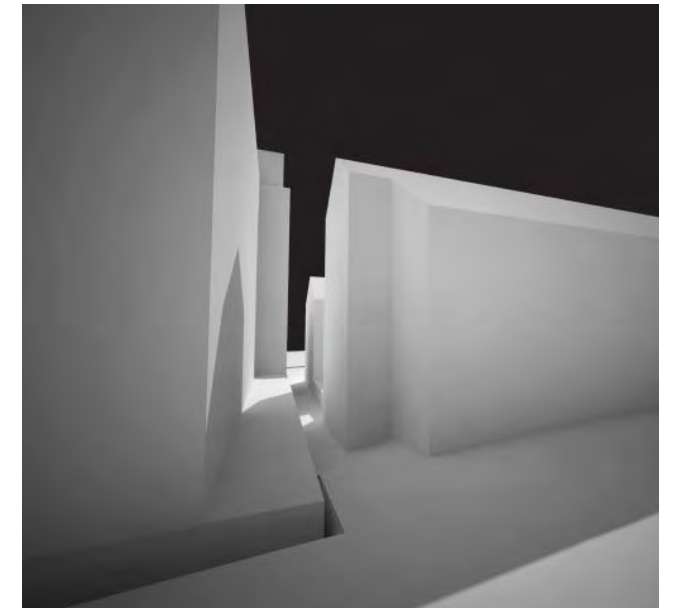
12:00



15:00



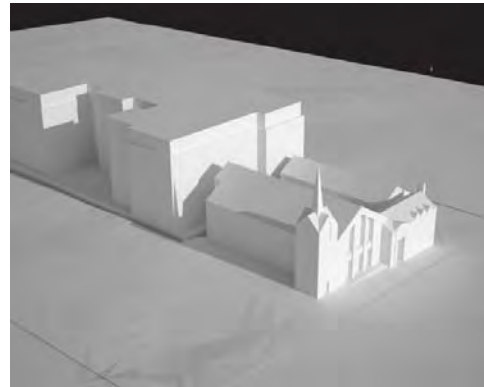
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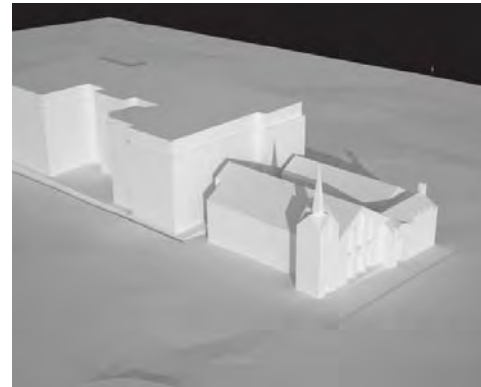
DEC 21



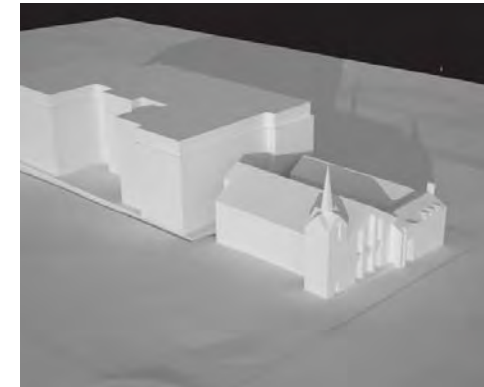
9:00



12:00



15:00



18:00

Relevant Design Guidelines

D-10 Commercial Lighting

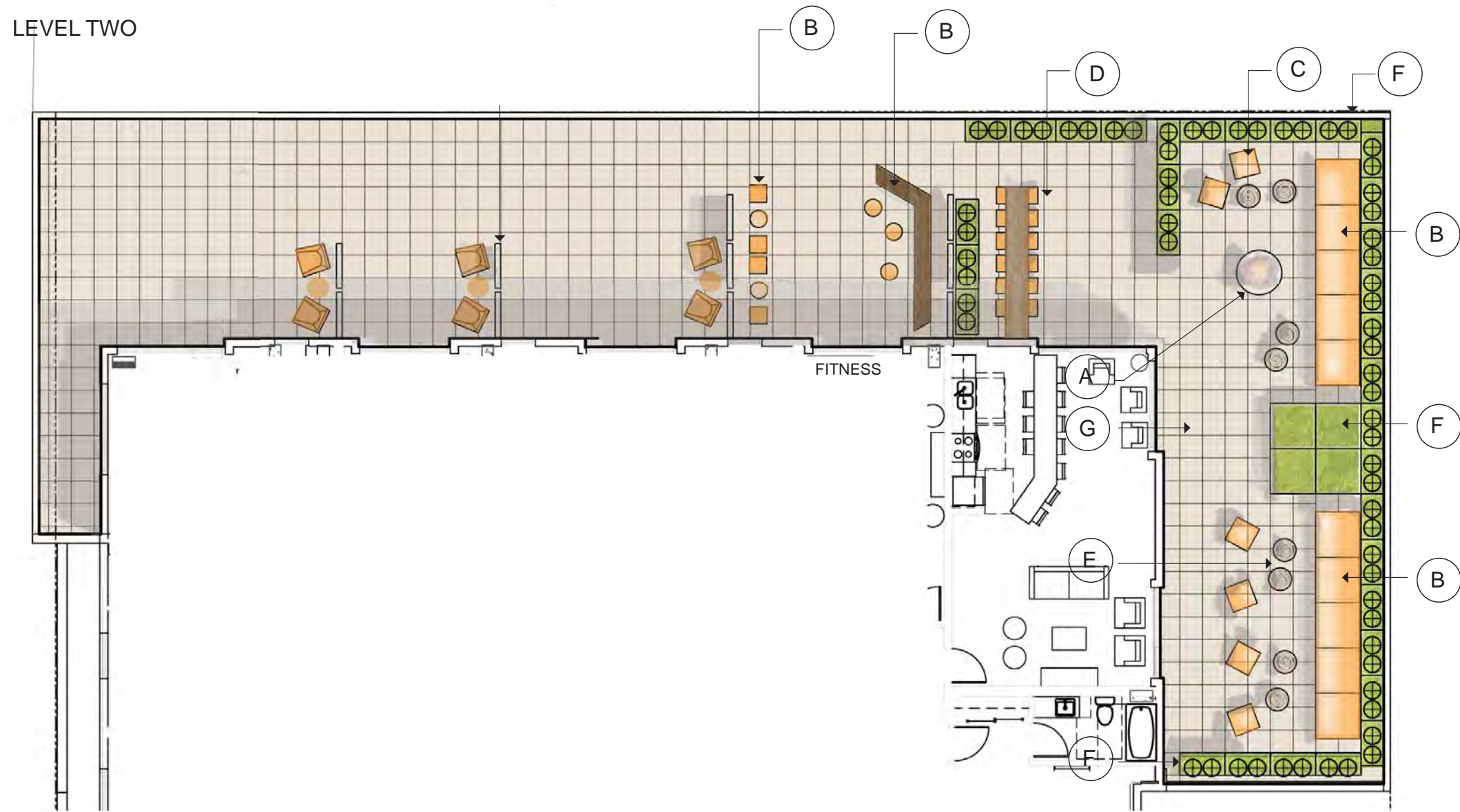
D-11 Commercial Transparency

Design Review Board Recommendations

“Uplighting and/or downlighting of the historic structure would be helpful, but the Board noted this would be in the purview of either SDOT or the Landmarks Preservation Board...”

“... the design of individual terraces, setback, and reflective surfaces is a positive response to the EDG...”
Operable rollup doors, lighting, and any other strategies that can help to enhance the commercial viability on Thomas St would be strongly supported by the Board.

LEVEL TWO



H ARCHITECTURAL SCREENS



G ARCHITECTURAL SLAB PAVING



F TOURNESOL SITEWORKS PLANTERS



A OPTIONAL FIRE PIT



B CUSTOM BUILT IN BENCH / LOUNGE SEATING



C LOLL: LOUNGE / CHAIR



D LARGE DINING TABLE, BAR HT.



E KNOLL: MAYA LIN STONES



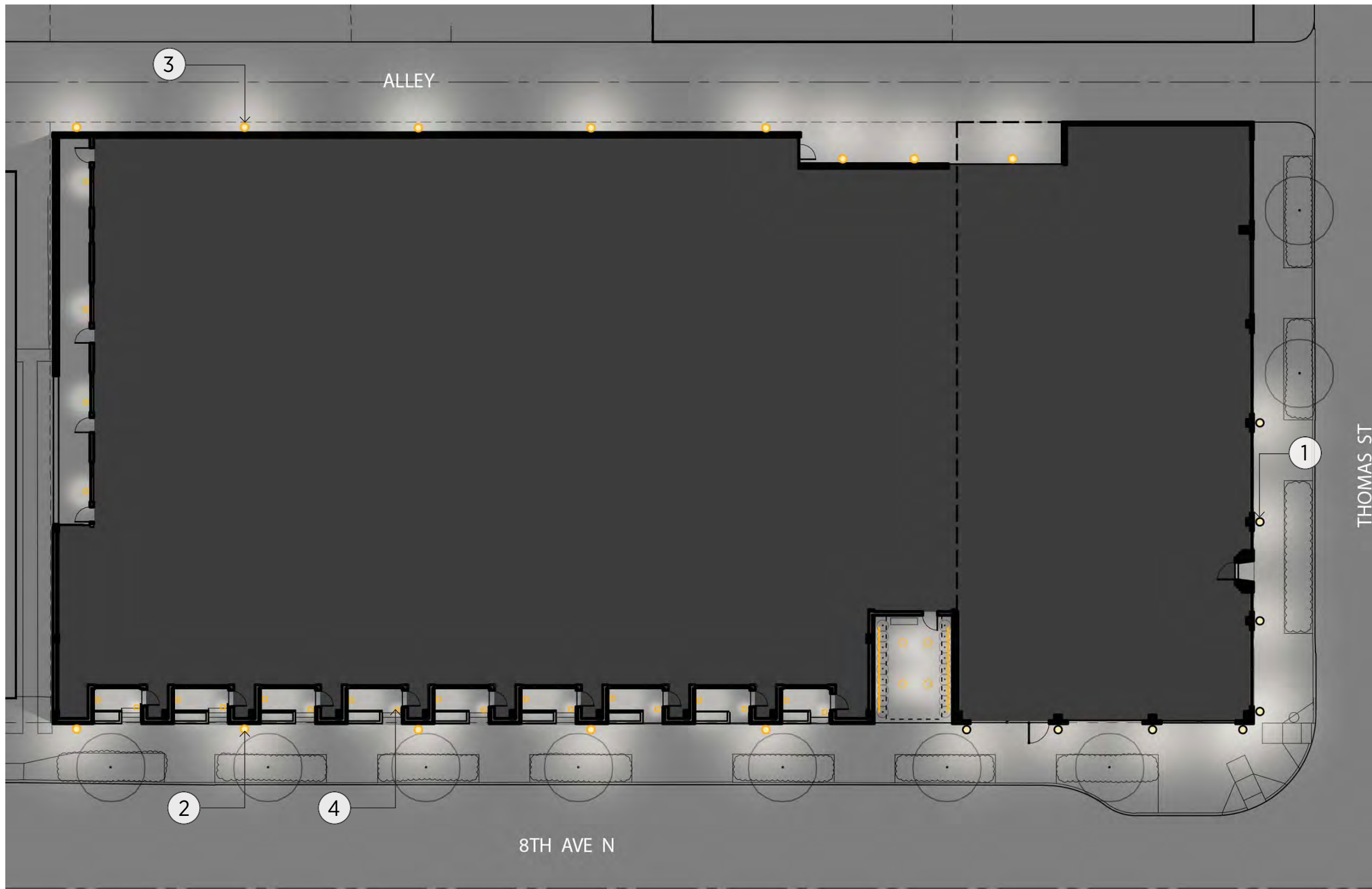


1
EXISTING LIGHTING

AMENITY SPACE ABOVE LANDMARK STRUCTURE AT CORNER OF 8TH AVE AND THOMAS ST

Relevant Design Guidelines

D-10 Commercial Lighting



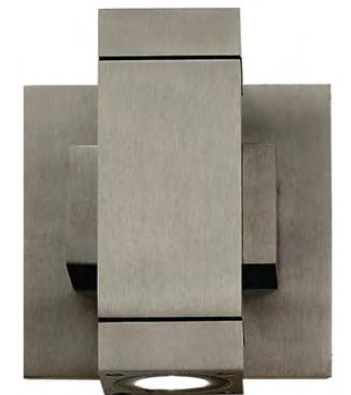
1
EXISTING LIGHTING



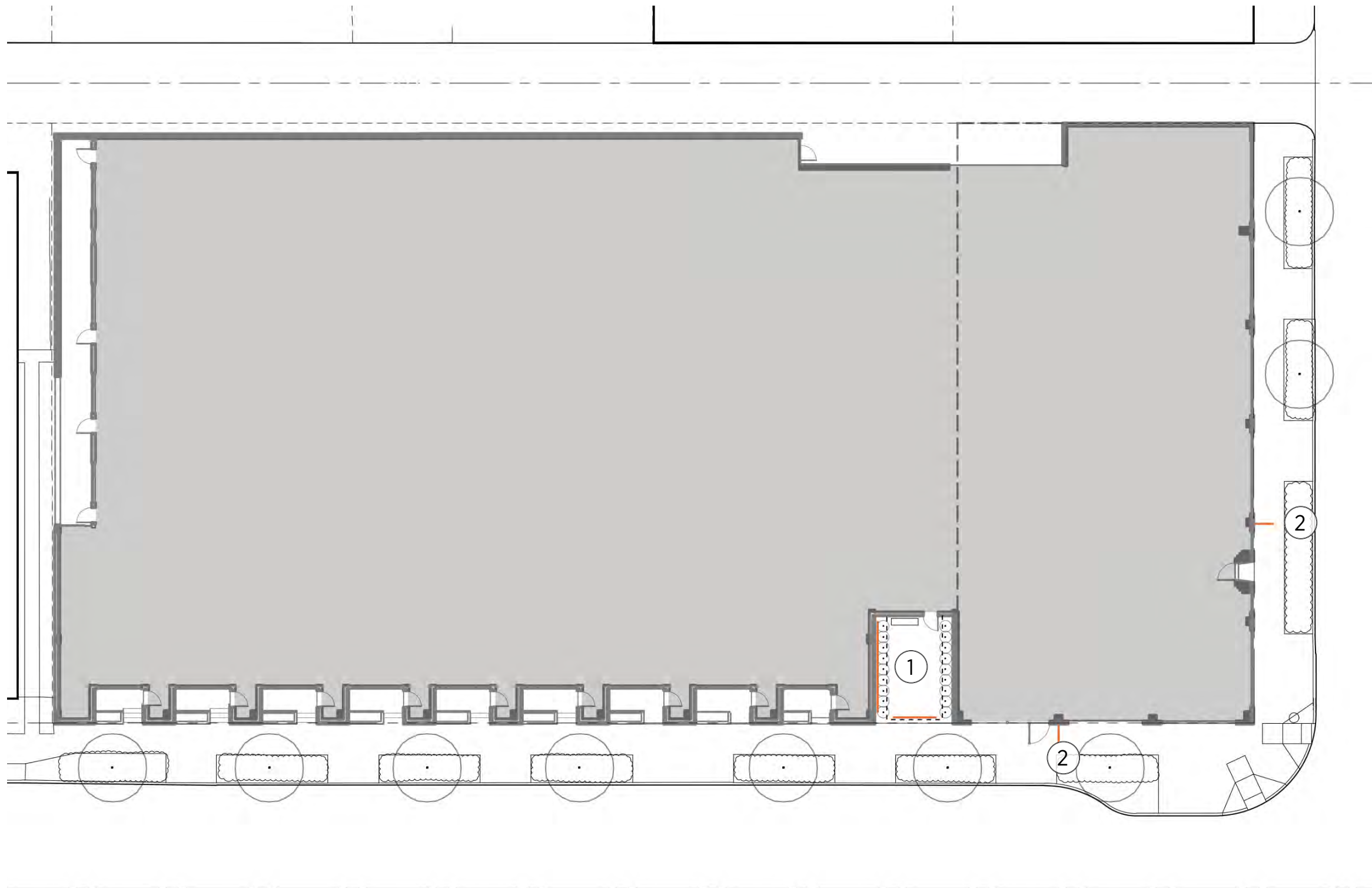
2
HINKLEY LIGHTING
- PIER FIXTURE



3
HINKLEY LIGHTING -
STOOP FIXTURE



4
EDGE LIGHTING
- UNIT DOOR FIXTURE



1) ENTRY SIGNAGE CONCEPT



2) RETAIL SIGNAGE CONCEPT

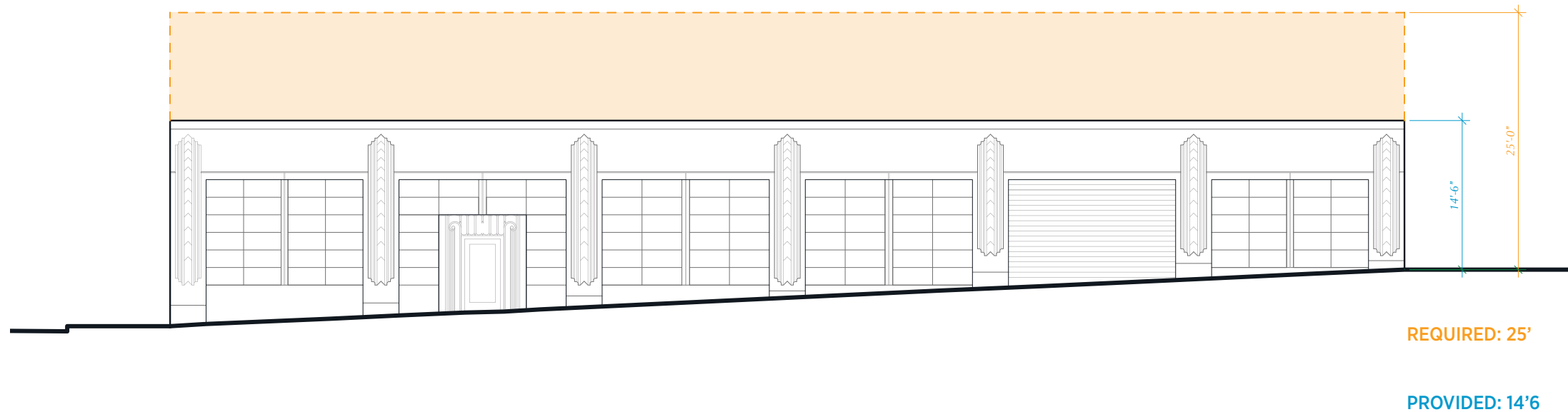


PERSPECTIVE LOOKING SOUTHWEST FROM 8TH AVE AND THOMAS ST

DEPARTURE MATRIX

	DEVELOPMENT STANDARD	REQUIREMENT	REQUEST
1	23.48.014B2	“ON CLASS 2 PEDESTRIAN STREETS, AS SHOWN ON MAP B, ALL FACADES SHALL HAVE A MINIMUM HEIGHT OF TWENTY-FIVE (25) FEET”	THE HISTORIC LANDMARKED STRUCTURE FACING THOMAS PROVIDES A MINIMUM FAÇADE HEIGHT OF FOURTEEN FEET SIX INCHES (14'-6). WE PROPOSE TO LEAVE THE HISTORIC STRUCTURE UNALTERED.
2	23.48.018A1A	“ON CLASS 1 AND 2 PEDESTRIAN STREETS, SHOWN ON MAP B, LOCATED AT THE END OF THIS CHAPTER: A MINIMUM OF SIXTY (60) PERCENT OF THE WIDTH OF THE STREET LEVEL FACADE MUST BE TRANSPARENT”	THE DEVELOPMENT FRONTS A CLASS 2 PEDESTRIAN STREET ALONG THOMAS ST. WHICH REQUIRES A MINIMUM OF 60% OF THE WIDTH OF THE STREET LEVEL FACADE TO BE TRANSPARENT. THE HISTORIC LANDMARKED STRUCTURE FACING THOMAS IS ONLY 56% TRANSPARENT. WE PROPOSE TO LEAVE THE HISTORIC UNALTERED.
3	23.48.018B2A	“BLANK FACADES SHALL BE LIMITED TO SEGMENTS FIFTEEN (15) FEET WIDE, EXCEPT FOR GARAGE DOORS WHICH MAY BE WIDER THAN FIFTEEN (15) FEET. BLANK FACADE WIDTH MAY BE INCREASED TO THIRTY (30) FEET IF THE DIRECTOR DETERMINES THAT THE FACADE IS ENHANCED BY ARCHITECTURAL DETAILING, ARTWORK, LANDSCAPING, OR OTHER SIMILAR FEATURES THAT HAVE VISUAL INTEREST”	THE DEVELOPMENT PROPERTY FRONTS A CLASS 2 PEDESTRIAN STREET ALONG THOMAS ST. CLASS 2 PEDESTRIAN STREETS REQUIRE BLANK FACADES TO BE LIMITED TO SEGMENTS NO LONGER THAN 15 FEET WIDE. THE HISTORIC LANDMARKED STRUCTURE INCLUDES A NON-TRANSPARENT SEGMENT TWENTY TWO FEET AND 22'3" WIDE FACING THOMAS AND A 22'2"NON-TRANSPARENT SEGMENT FACING 8TH AVENUE. WE PROPOSE TO LEAVE THE HISTORIC UNALTERED.

DEPARTURE 1: MINIMUM FACADE HEIGHT



CODE

23.48.014B2: MINIMUM FACADE HEIGHTS SHALL NOT APPLY WHEN ALL PORTIONS OF THE STRUCTURE ARE LOWER THAN THE ELEVATION OF THE REQUIRED MINIMUM FACADE HEIGHT LISTED BELOW.

1. ON CLASS 1 PEDESTRIAN STREETS, AS SHOWN ON MAP B, PEDESTRIAN STREET CLASSIFICATIONS, LOCATED AT THE END OF THIS CHAPTER, ALL FACADES SHALL HAVE A MINIMUM HEIGHT OF FORTY-FIVE (45) FEET.
2. ON CLASS 2 PEDESTRIAN STREETS, AS SHOWN ON MAP B, ALL FACADES SHALL HAVE A MINIMUM HEIGHT OF TWENTY-FIVE (25) FEET.
3. ON ALL OTHER STREETS, ALL FACADES SHALL HAVE A MINIMUM HEIGHT OF FIFTEEN (15) FEET.

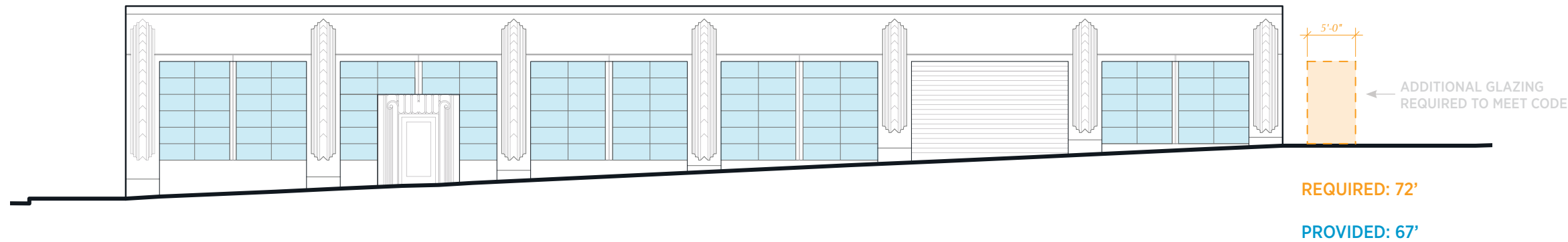
REQUEST

THE HISTORIC LANDMARKED STRUCTURE FACING THOMAS PROVIDES A MINIMUM FAÇADE HEIGHT OF FOURTEEN FEET SIX INCHES (14'-6). THE DESIGN TEAM PROPOSES TO LEAVE THE HISTORIC STRUCTURE FACADE UNALTERED.

WHY THIS DEPARTURE MEETS THE INTENT OF DESIGN GUIDELINES

ALLOWING THE LANDMARK'S FAÇADE TO REMAIN UNALTERED HELPS PRESERVE THE NEIGHBORHOOD'S HISTORIC FABRIC.

DEPARTURE 2: FACADE TRANSPARENCY REQUIREMENTS



CODE

23.48.018A1A: TRANSPARENCY REQUIREMENTS APPLY TO ALL STREET LEVEL FACADES, EXCEPT THAT TRANSPARENCY REQUIREMENTS DO NOT APPLY TO PORTIONS OF STRUCTURES IN RESIDENTIAL USE.

1. TRANSPARENCY SHALL BE REQUIRED AS FOLLOWS:

A. CLASS 1 AND 2 PEDESTRIAN STREETS, SHOWN ON MAP B, LOCATED AT THE END OF THIS CHAPTER: A MINIMUM OF SIXTY (60) PERCENT OF THE WIDTH OF THE STREET LEVEL FACADE MUST BE TRANSPARENT.

B. ALL OTHER STREETS: A MINIMUM OF THIRTY (30) PERCENT OF THE WIDTH OF THE STREET-LEVEL FACADE MUST BE TRANSPARENT.

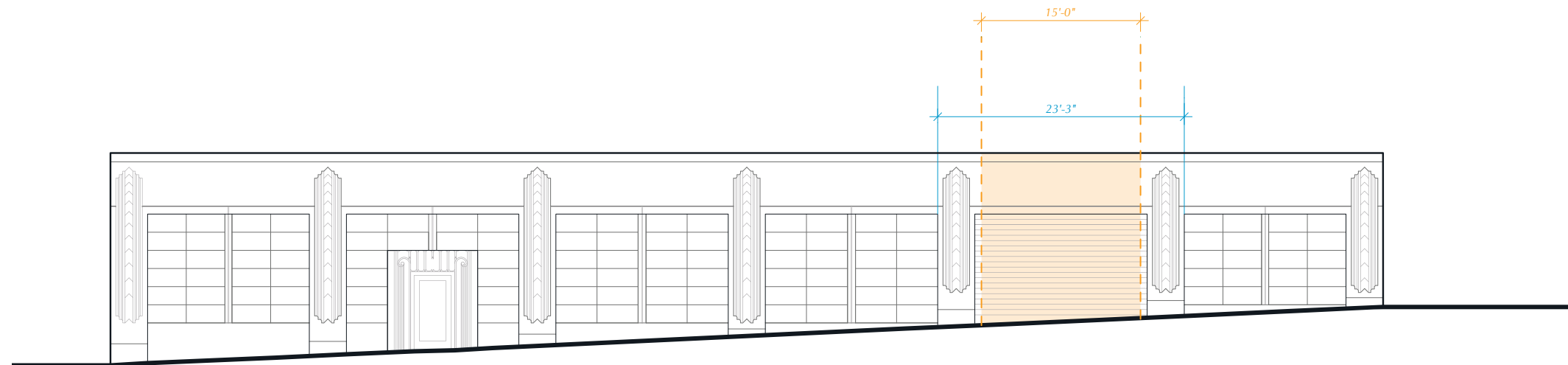
REQUEST

THE DEVELOPMENT FRONTS A CLASS 2 PEDESTRIAN STREET ALONG THOMAS ST. WHICH REQUIRES A MINIMUM OF 60% OF THE WIDTH OF THE STREET LEVEL FACADE TO BE TRANSPARENT. THE HISTORIC LANDMARKED STRUCTURE FACING THOMAS IS ONLY 56% TRANSPARENT. WE PROPOSE TO LEAVE THE HISTORIC FACADE UNALTERED.

WHY THIS DEPARTURE MEETS THE INTENT OF DESIGN GUIDELINES

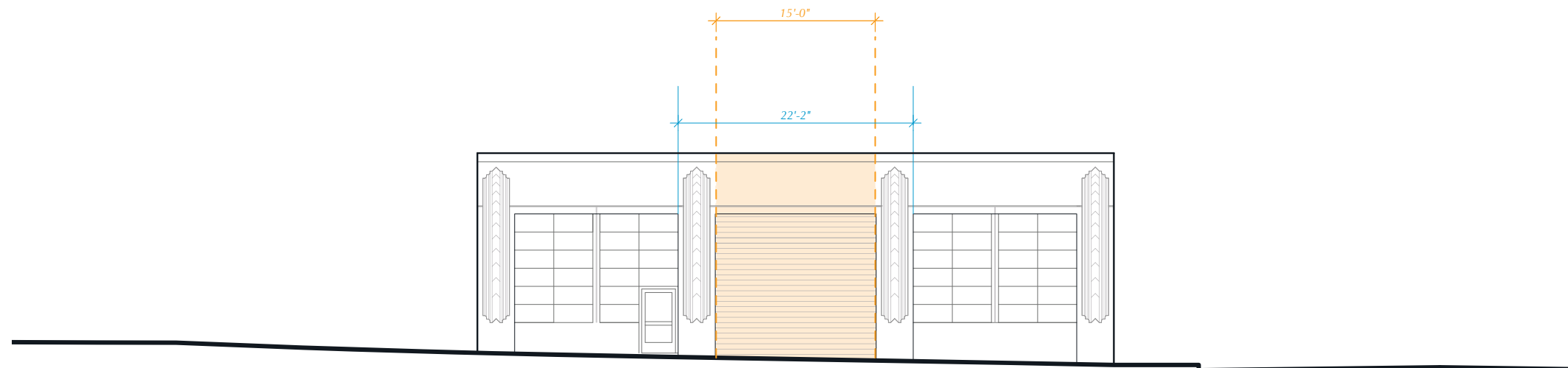
ALLOWING THE LANDMARK'S FAÇADE TO REMAIN UNALTERED HELPS PRESERVE THE NEIGHBORHOOD'S HISTORIC FABRIC.

DEPARTURE 3: BLANK FACADE LIMITS



REQUIRED: 15'

PROVIDED: 23'3"



REQUIRED: 15'

PROVIDED: 22'2"

CODE

23.48.018B2A:

1. ANY PORTION OF THE FACADE WHICH IS NOT TRANSPARENT SHALL BE CONSIDERED TO BE A BLANK FACADE.

2. BLANK FACADE LIMITS FOR CLASS 1 AND 2 PEDESTRIAN STREETS.

A. BLANK FACADES SHALL BE LIMITED TO SEGMENTS FIFTEEN (15) FEET WIDE, EXCEPT FOR GARAGE DOORS WHICH MAY BE WIDER THAN FIFTEEN (15) FEET. BLANK FACADE WIDTH MAY BE INCREASED TO THIRTY (30) FEET IF THE DIRECTOR DETERMINES THAT THE FACADE IS ENHANCED BY ARCHITECTURAL DETAILING, ARTWORK, LANDSCAPING, OR OTHER SIMILAR FEATURES THAT HAVE VISUAL INTEREST. THE WIDTH OF GARAGE DOORS SHALL BE LIMITED TO THE WIDTH OF THE DRIVEWAY PLUS FIVE (5) FEET.

B. ANY BLANK SEGMENTS OF THE FACADE SHALL BE SEPARATED BY TRANSPARENT AREAS AT LEAST TWO (2) FEET WIDE.

REQUEST

THE DEVELOPMENT PROPERTY FRONTS A CLASS 2 PEDESTRIAN STREET ALONG THOMAS ST. CLASS 2 PEDESTRIAN STREETS REQUIRE BLANK FACADES TO BE LIMITED TO SEGMENTS NO LONGER THAN 15 FEET WIDE. THE HISTORIC LANDMARKED STRUCTURE INCLUDES A NON-TRANSPARENT SEGMENT TWENTY TWO FEET TWO INCHES WIDE FACING THOMAS AND A TWENTY TWO FOOT TWO NON-TRANSPARENT SEGMENT FACING 8TH AVENUE. WE PROPOSE TO LEAVE THE HISTORIC FACADE UNALTERED.

WHY THIS DEPARTURE MEETS THE INTENT OF DESIGN GUIDELINES

ALLOWING THE LANDMARK'S FAÇADE TO REMAIN UNALTERED HELPS PRESERVE THE NEIGHBORHOOD'S HISTORIC FABRIC.