



PROPOSED BUILDING

Construct a Six-Storyed Mixed-Use building, located at the corner of East Madison Street and 15th Avenue. Commercial uses at the Ground Floor, with Residential and Garage entries; Floors 2-6 will be Residential. There will be 2-1/2 levels of below grade parking.

DEVELOPMENT OBJECTIVES

Number of Residential Units	70
Number of Residential Parking Stalls	70
Commercial Square Footage	3,000 SF +/-
Live/Work Square Footage	3,000 SF +/-

LAND USE DATA

Address	1420 East Madison Street 15th Avenue
Parcel #	172800 0800 172880 0075
Zoning	NC3P-65
Urban Village	Pike/Pine Urban Center Village
FAR	Total Mixed Use: 4.75 Single Use: 4.25
Height Limit	65'-0"
Height Bonuses	SMC 23.73.014: +4'
Parking	No Minimum Requirement



PROJECT LOCATION

VICINITY MAP (NTS)

2 DEVELOPMENT OBJECTIVES

PIKE/PINE URBAN CENTER VILLAGE, Capitol Hill, Seattle

Neighborhood Context: Historically known as "auto row"- the Pike/Pine neighborhood is characterized by turn-of-the-century loft buildings blended with contemporary residential and commercial buildings. These mid-rise structures, while mixed in architectural style, have a common theme of active pedestrian streets and the design at street level provides restaurants and small buildings with high-ceilinged spaces and large amounts of transparent glazing.

The Pike/Pine neighborhood is a lively, high-density residential community active on a 24-hour basis with energized street life and public spaces. Pedestrian oriented and close to the downtown office core, connected to the north and south via streetcar and soon light rail, the neighborhood is steadily evolving as an urban residential community.

Our Development Objectives embrace both the Seattle and Pike/Pine Urban Center Village Design Guidelines:

1. Context and Site:

- Create a visible connection to the street, recognizing the importance of this corner site and responding to the nearby architectural context.

2. Public Life:

- Enhance the pedestrian activity, accessibility and uses already present. Add to the character and vitality of the neighborhood.

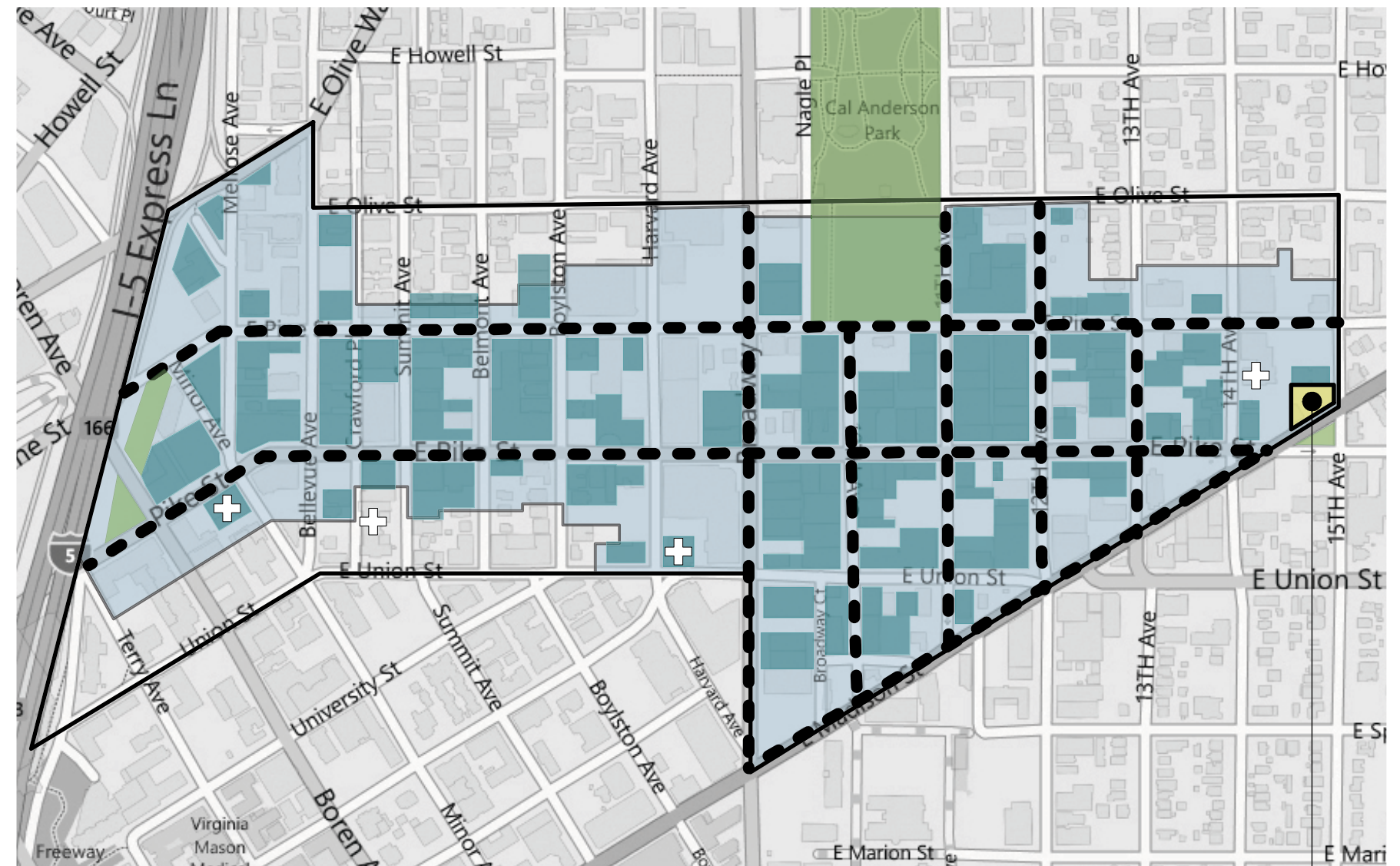
3. Design Concept:

- Respond to a challenging site with a coherent solution, considering the adjacent properties, topography, access limitations, utility restrictions and an evolving architectural context.

- Create a building that is accommodating to adjacent properties and that provides for the future evolution of the neighborhood.

- Design a building that in its form, massing and materials relates and responds to the nearby context of contemporary and traditional buildings.

- Given the complex site, the goal is to integrate the structure into the developing Pike/Pine urban streetscape, with strong massing reflecting the recent adjacent properties and developing a fine-grained architectural treatment to provide scale and visual interest.

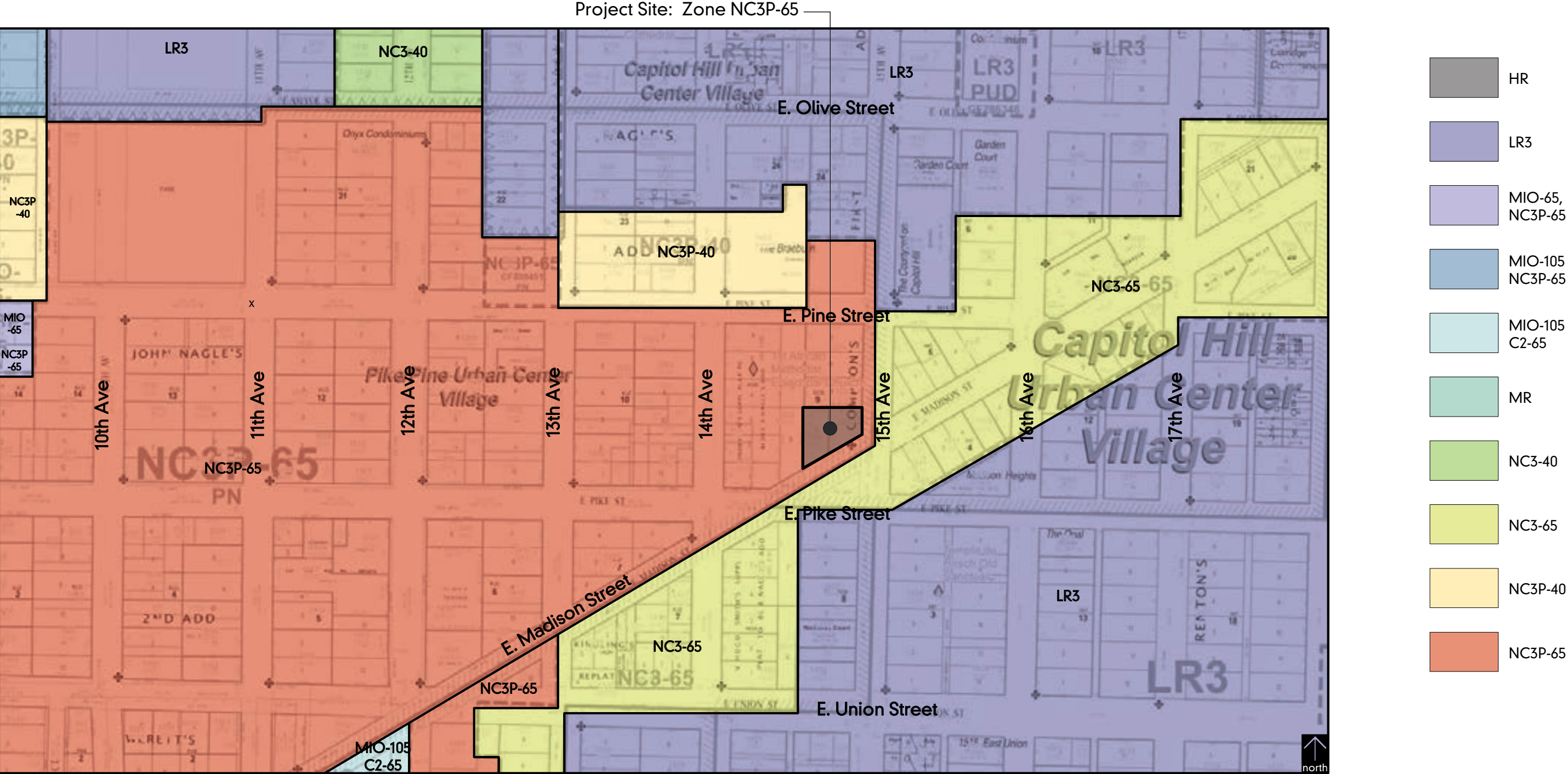


Uses at Street Level & Historical Character

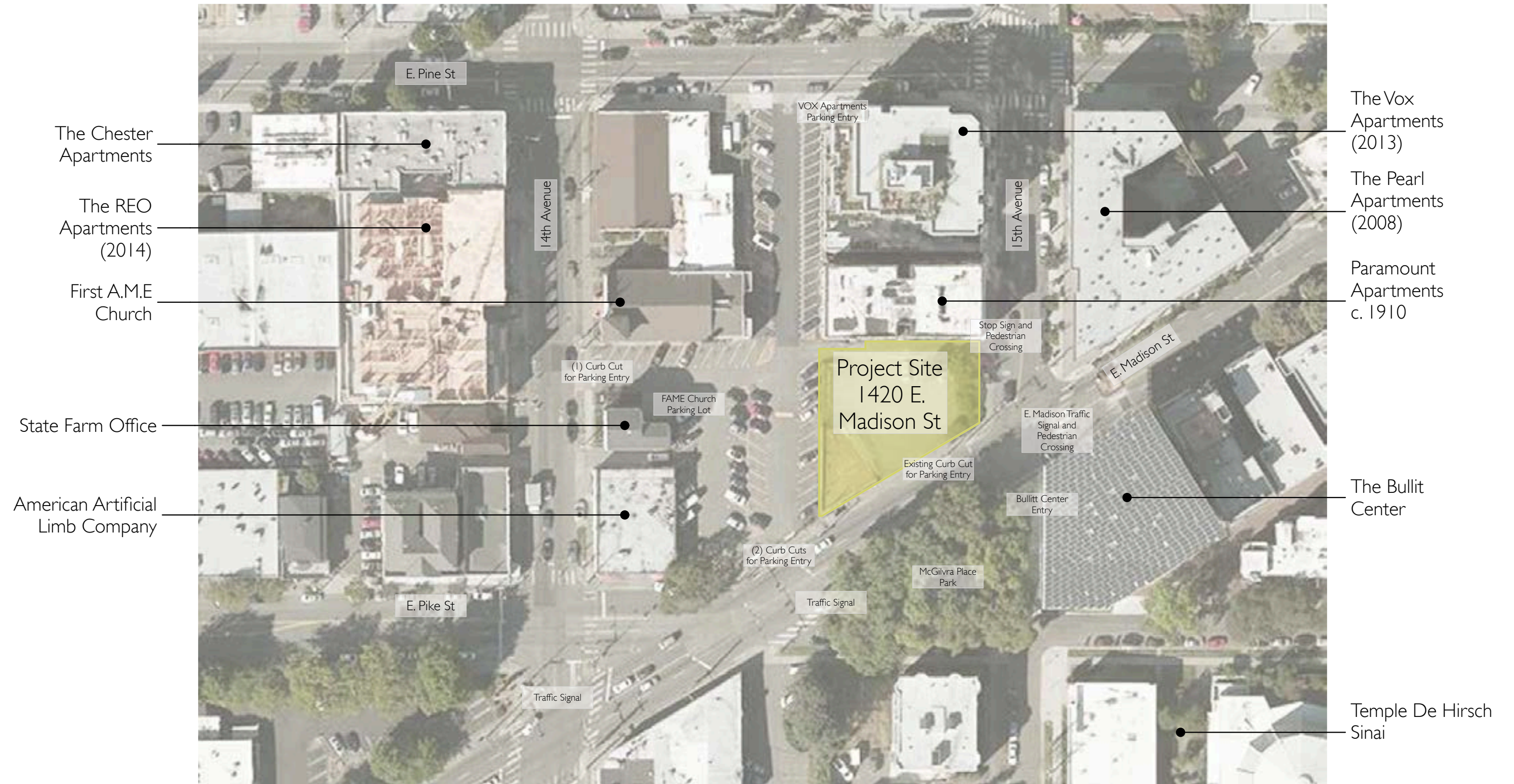
- Pike/Pine Conservation Overlay District
- 'Character Structures' 75+ Years
- City-Owned Open Space

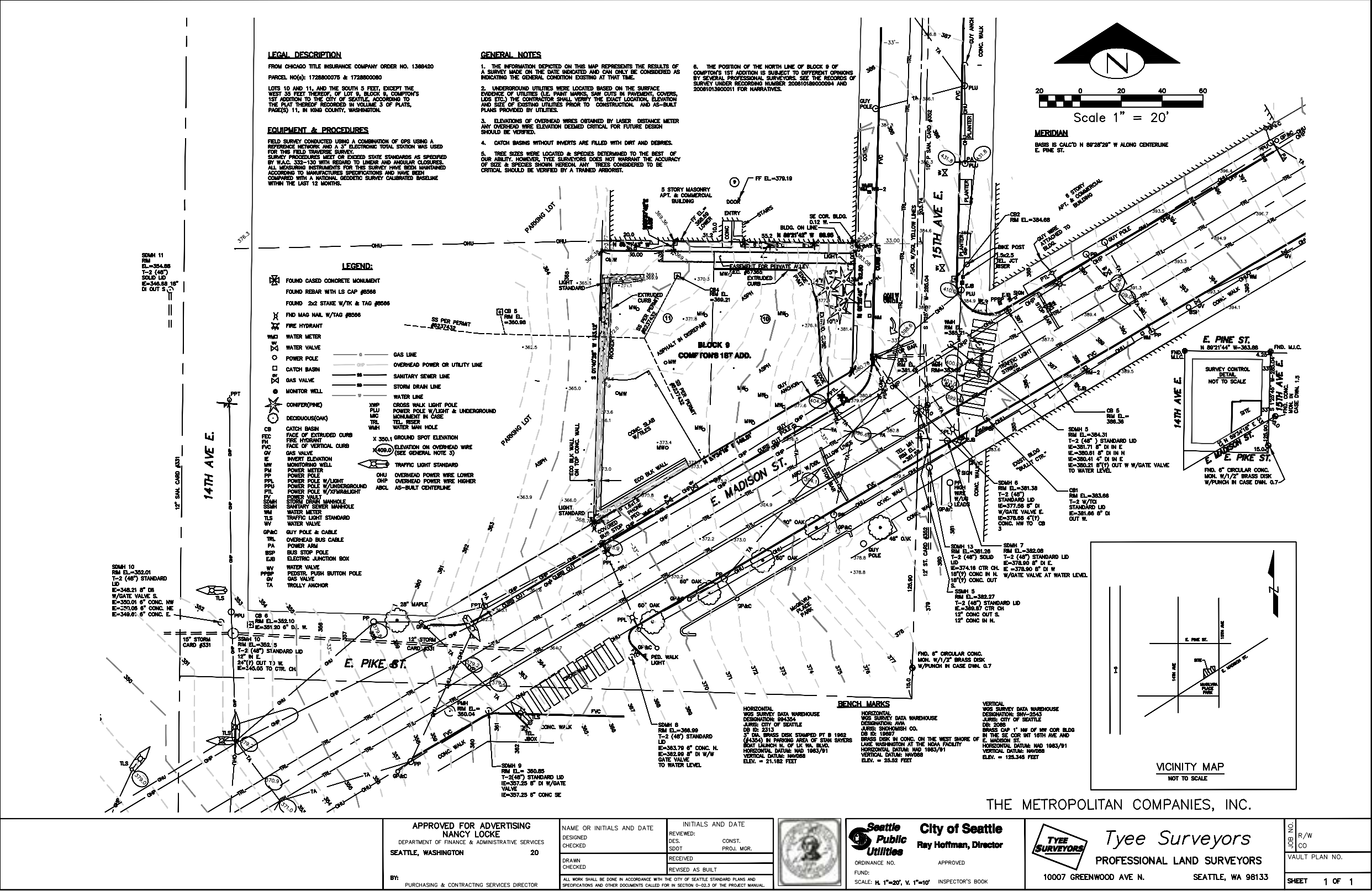
- Pike/Pine Urban Village
- Principal Pedestrian Street
- Landmark

Project Site:
1420 E. Madison St.

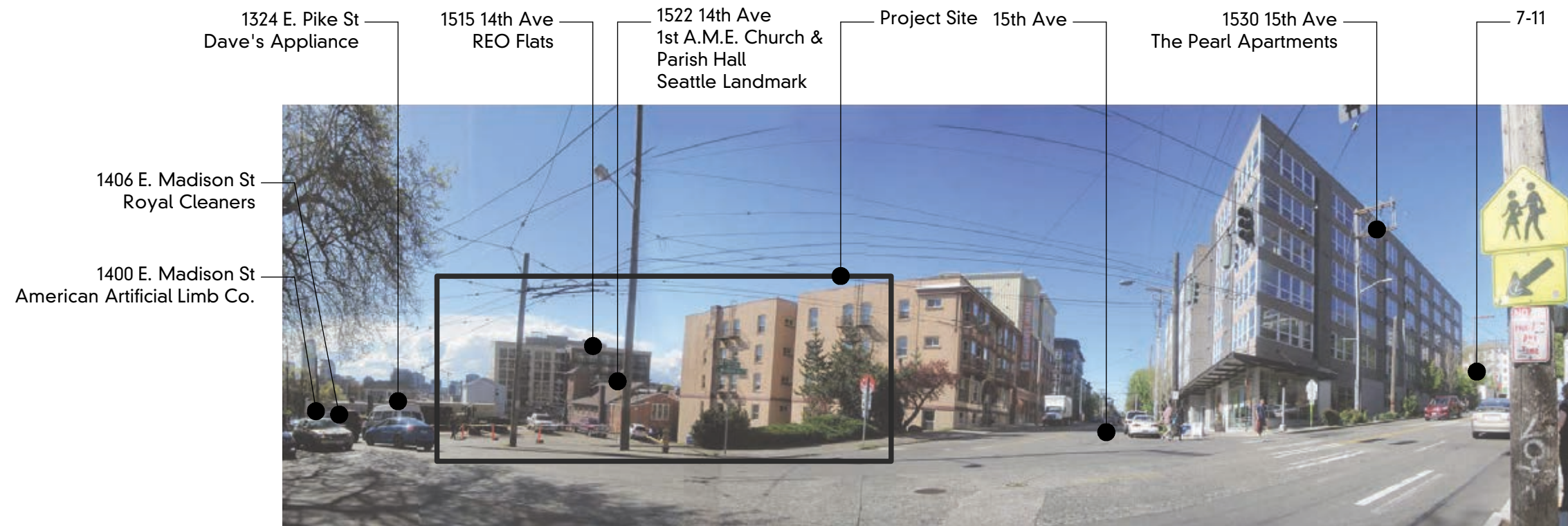


4 VICINITY MAP

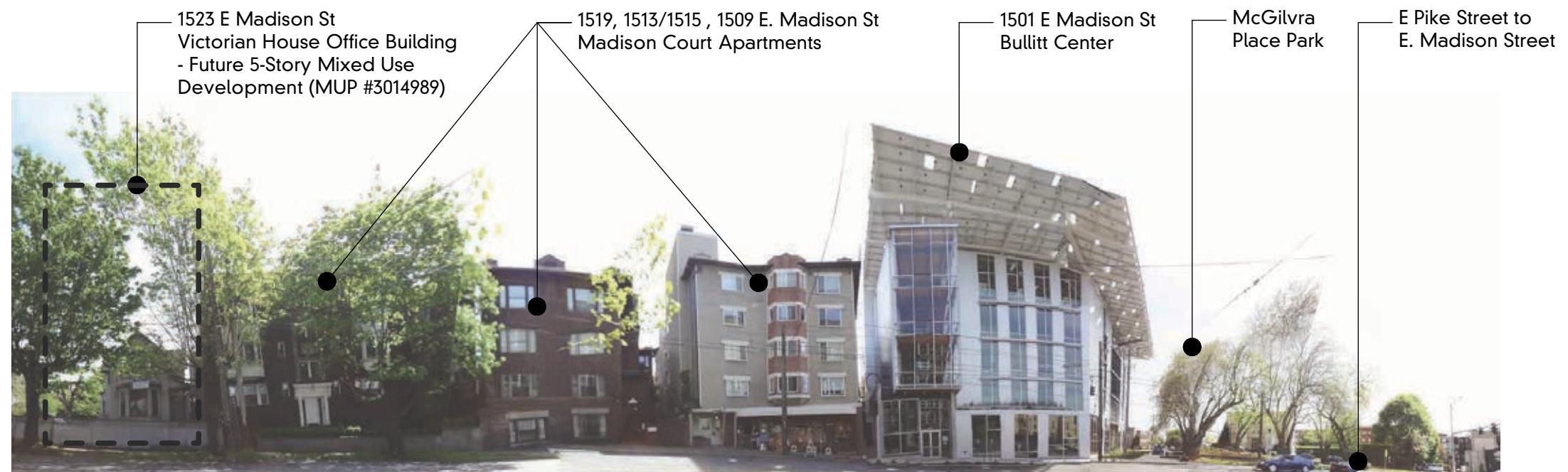




6 SITE STREETSAPES



E. Madison St - North Side of Street



E. Madison St - East Side of Street

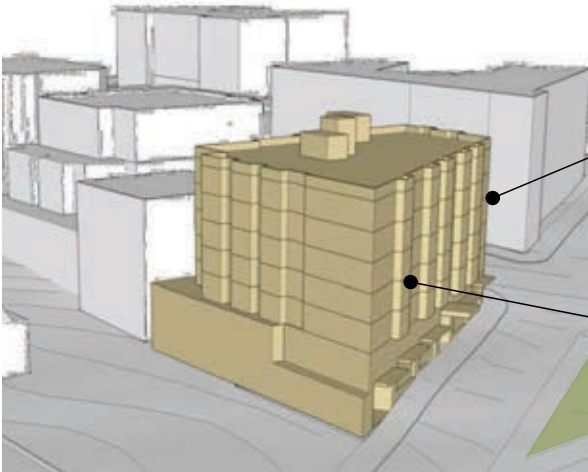


15th Avenue- West Side of Street

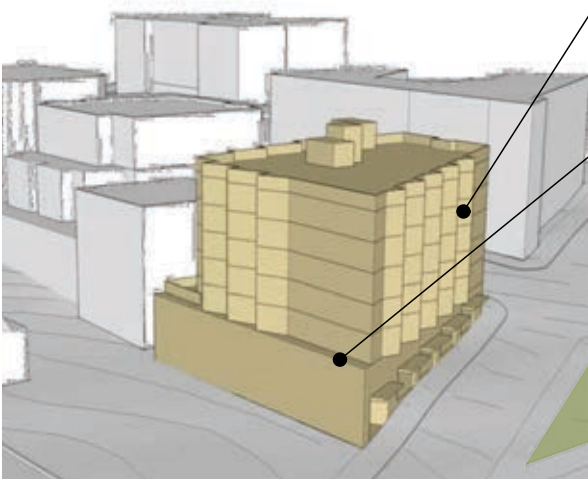


15th Avenue - East Side of Street

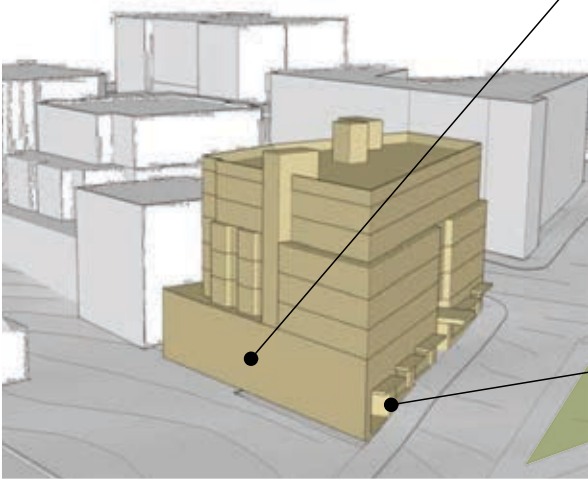
EDG OPTIONS



OPTION 1 (PREFERRED)



OPTION 2



OPTION 3

DESIGN RESPONSES

Comment: Strong architectural response at 15th Ave. corner

Response: Massing strengthened on East facade by removal of bays; Cafe terrace provided at commercial entrance in required City Light setback

Comment: "Busy bay windows" in Option 1 and 2 lack context

Response: Fewer and simpler bay windows, bays eliminated from East facade (See Sheet 13 regarding context)

Comment: Massing to respond to nearby context and site

Response: Increased setbacks on north and west respond to adjacent properties in lieu of upper level setbacks (See Sheet 13 regarding context)

Comment: Large area of blank facade on west wall

Response: Substantial redesign to increase glazing and accommodate future adjacent development:

- Increased setbacks allow substantial glazing
- Live/Work units and terrace replace 2nd floor parking
- Landscape screening at reduced blank wall

Comment: Quality materials should relate to context

Response: Materials selected to be in harmony with adjacent contemporary structures

Comment: E. Madison is the appropriate location for residential and garage entrances

Response: All primary entrances on E. Madison

- Garage entrance at SW corner
- Residential entrance mid-block



PROPOSED DESIGN

I0 EDG COMMENTS & RESPONSES

COMMENTS

I: Massing & Response to Context

I.a: Provide a design with strongly expressed forms that responds to nearby context.
The board noted that Options 1 & 2 incorporate “busy bay windows” that lack a relationship to recent nearby development.
(CS2-III, CS3-A, CS3-I, CS3-IV)

I.b: Board observed aspects of Option 3 that responded well to the context and site: upper floor setback, potential for strong corner response, change in planes (future development)
(CS2-C, CS2-I, CS2-II, CS2-III, CS3-I, DC2-B, DC2-D, DC2-I)

I.b.I: Strongly express angled corner condition
(CS2-II, CS3-I, CS3-IV)

I.c: Balance setbacks for glazing and blank walls and consider visibility of west façade.
(DC2-B-2, DC2-C, DC2-I, DC4-A)

I.d: Clearly demonstrate how massing and modulation relate to nearby context.
(CS2-II, CS2-III, CS3-A, CS3-I, CS3-IV, DC2-I)

I.e: Northwest corner setback appreciated
(CS2-B, CS2-D, DC2-A)

RESPONSES

I.a: The proposed design treats the building as a unified architectural expression, reflecting the vertically-accented design approach of the Bullitt Center and the Pearl Apartments.
The design has eliminated the bay windows from the east facade and reduced the number of bays on the south facade and articulated them into the design. *Please see additional discussion of bay window and context on sheet 11.*

I.b: The proposed design is based on Option 1- and responds to the desired aspects of Option 3:
Upper floor setback- The proposed design does not include upper level setbacks. Please note that both new and traditional nearby structures do not incorporate upper level setbacks. *Please see additional discussion of upper level setbacks and context on sheet 13.*

Strong corner response- Undergrounding the power lines requires a setback at the SE corner. The design responds by creating an open terrace and a strong vertical façade treatment.

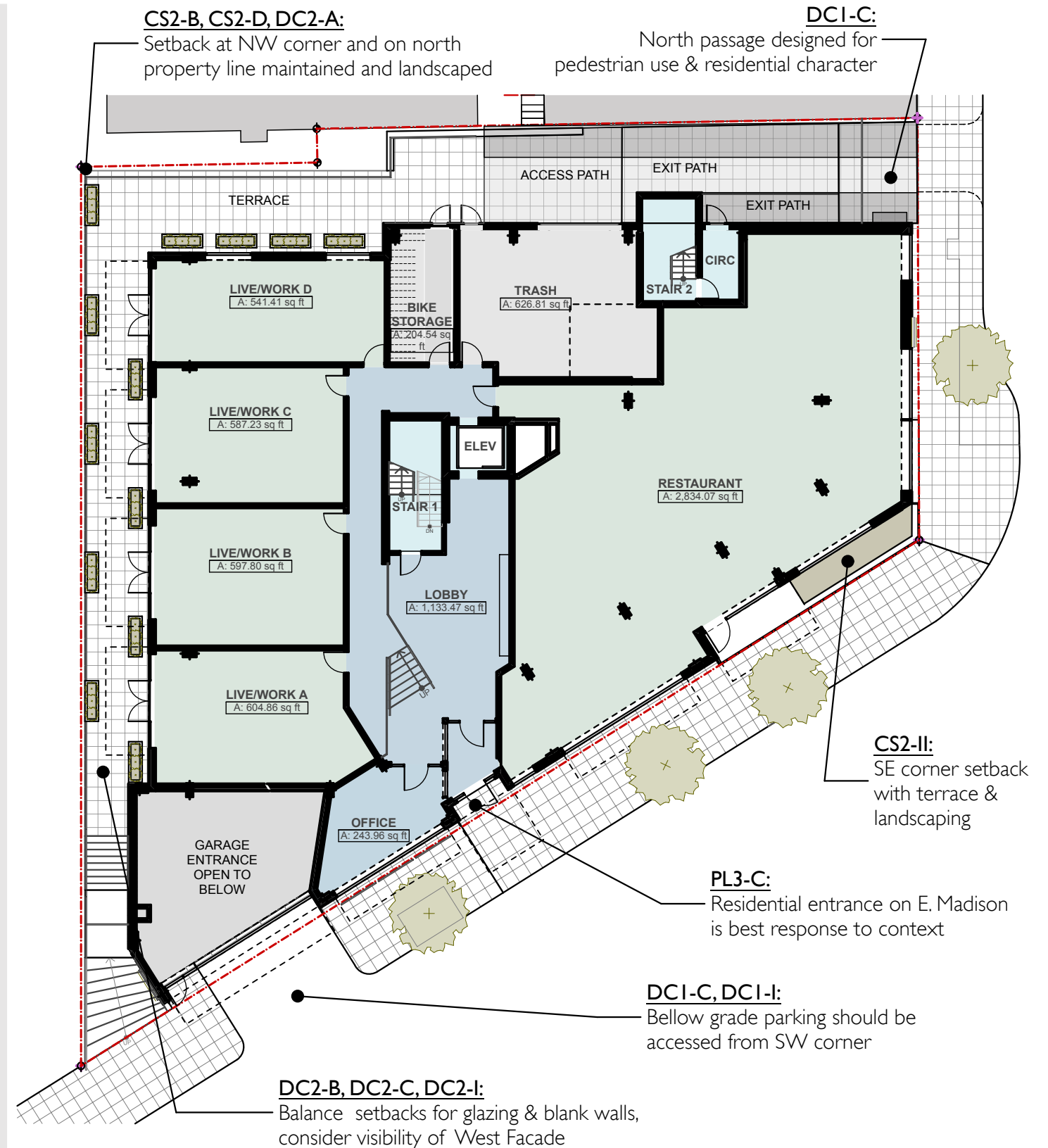
Change in planes- transition to future development- The proposed design sets the west façade back from the property line to provide a separation as well as light and air between the structure and future adjacent development.

I.b.I: The required City Light power pole location affects the massing of the 15th Avenue corner; with a setback on E. Madison. In response, we have created a strong, regular façade, referencing traditional Pike/Pine commercial buildings with large, loft-like fenestration.

I.c: The proposed design sets back the west façade to provide for glazing on both the proposed design and future adjacent development.

I.d: Graphics have been incorporated in the DRB packet demonstrating the proposed design is an appropriate response to site and context.

I.e: The setback at the north property line has been maintained and cooperation with the Paramount Apartments needs continues.



COMMENTS

2. Design Concept and Materials

2.a: Treatment of visible west façade is important to overall design.
(DC2-C, DC2-D, DC2-B-2, DC4-A)

2.b: Design concept and materials set example for future development.
(CS3-A, DC2-B)

2.c: Chose materials to relate to nearby context.
(DC4-A, DC4-I)

2.d: Supportive of strongly expressed forms either contemporary or traditional.
(DC2-A, DC2-B, DC2-C, DC2-D)

RESPONSES

2.a: The west façade is likely to be visible for a number of years to come. The façade has been given special attention with setbacks provided to allow the maximum glazing and residential character. The addition of the live/work units off the terrace provides additional activity and visual interest to that facade. Landscaping has also been provided.

2.b: The design concept, treating the structure as a unified massing, is consistent with the nearby recent development that helps maintain a consistent pattern of architectural form in this section of the E. Madison corridor.

2.c: The proposed structure's street facades are clad with a contemporary system of customized metal panels and vertical fins that relate to the recent nearby metal-clad structures including:
The Bullitt Foundation
The Pearl
Trace North

2.d: The proposed design treats the structure as a coherent contemporary form, with residential scale provided by modulation in the form of crisp bays and vertical fin elements.

CS2-II, CS2-III, CS3-A, CS3-IV, DC2-I:

Massing and modulation designed to relate to nearby contemporary structures and reflect some traditional elements with bays and balconies

CS2-III:

Respect rhythm established by traditional structures

CS2-II, CS2-Map 1:

SE corner responds to change in street grid- strong vertical elements



PL3-C:

Residential entrance on E. Madison- best response to context

CS2-II:

SE corner setback with terrace and landscaping, reflecting McGilvra Place Park across street

DCI.C:

North passage designed for pedestrian use and residential character

12 EDG COMMENTS & RESPONSES

COMMENTS

3. Vehicular and Residential Entries

3.a: Location of residential entrance on Madison is best response to context.
(PL4-C)

3.b: Below grade parking should be accessed from SW corner.
(DCI-C, DCI-I)

3.c: Vehicular and pedestrian access on north should be paved to relate to adjacent building and pedestrian environment.
(DCI-C, DCI-I)

3.d: Garage doors should be designed for human scale, visual interest and match design concept.
(DCI-C, DCI-I)

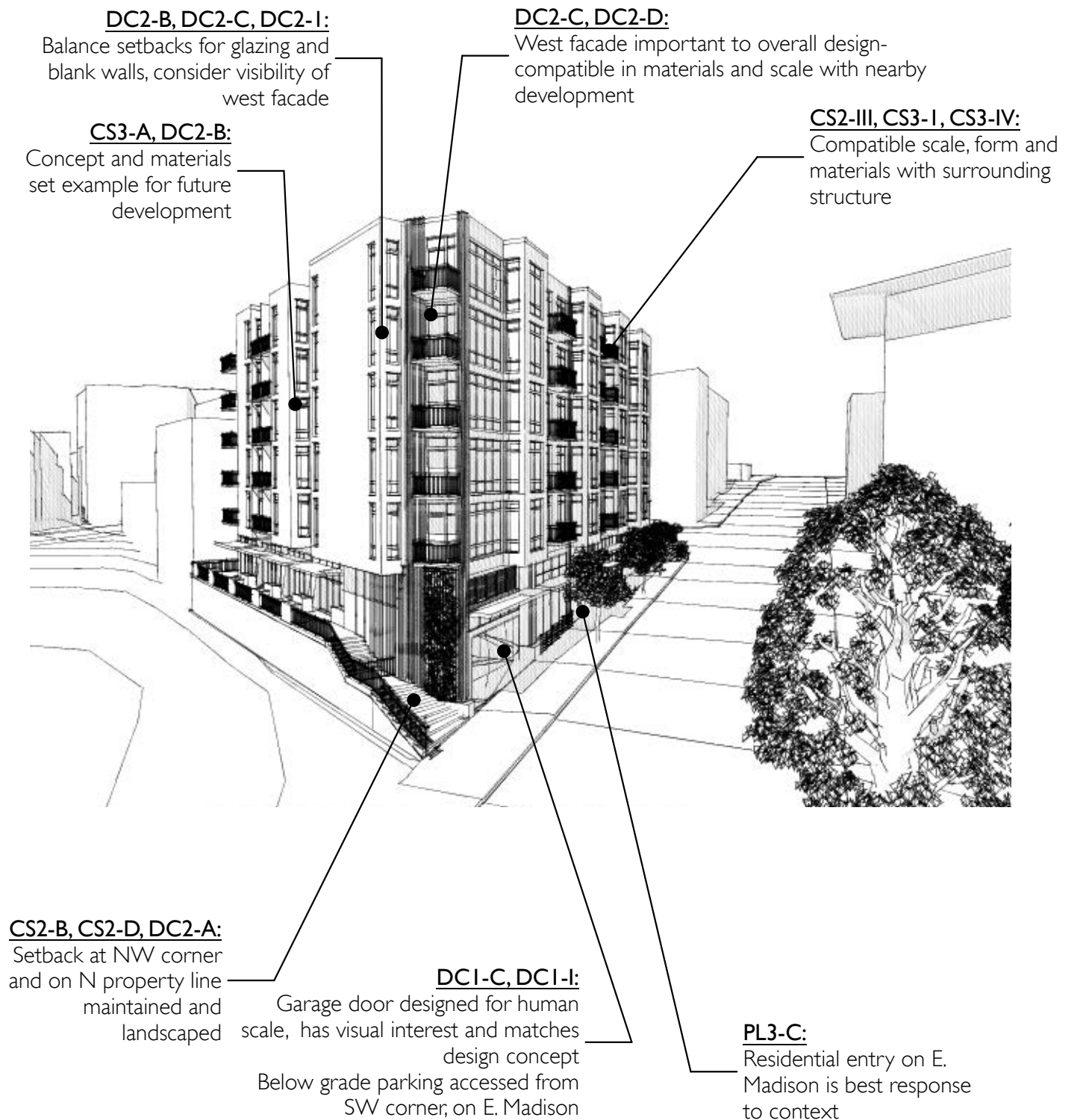
RESPONSES

3.a: The residential entrance is provided at the mid-point of the south façade on E. Madison, across from McGilvra Place Park.

3.b: The vehicular garage entrance is located at the SW corner of the site. This entrance is controlled by an open grill and visual alerts for pedestrians. An adjacent glazed door provides emergency egress. The 15th Avenue garage entrance serving 7 parking stalls has been eliminated from the design.

3.c: The area between the proposed structure and the Paramount Apartments on the north no longer provides vehicular access, and has been redesigned as a pedestrian environment, also providing emergency egress and shared trash handling for both buildings.

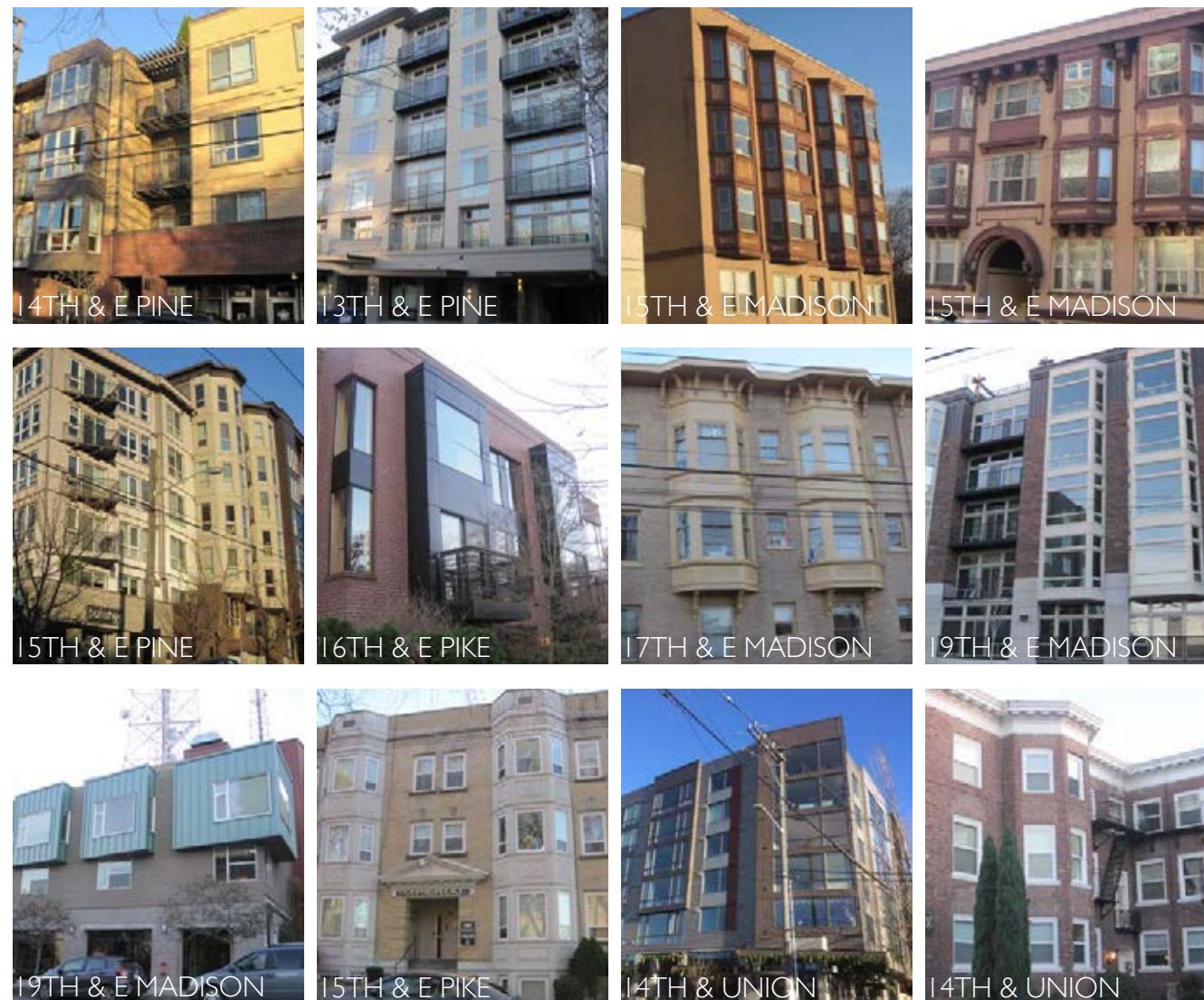
3.d: The garage door is sized to be the minimum allowed and designed as a visually transparent grille integrated into a façade matching the adjacent storefronts in detail.



Nearby Context: Bay Windows

The Design Review Board's guidance that EDG Options 1 and 2 incorporated "busy bay windows" led us to eliminate the bays from the east façade and reduce the number of bays on the south façade from five to four. We also looked carefully at the local context and found that rhythmic bay windows are a common feature in the nearby neighborhood.

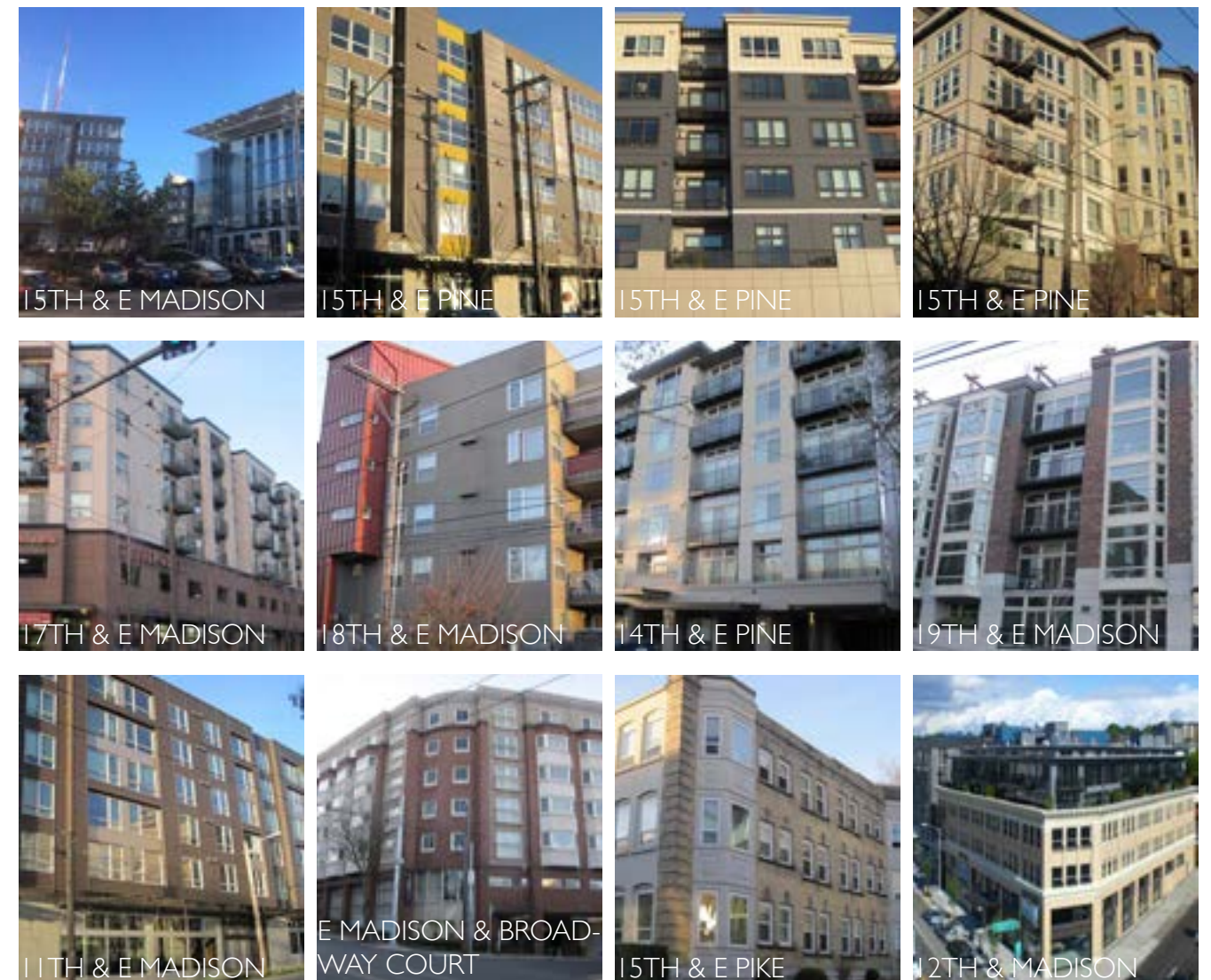
Our survey of the 9-block immediate neighborhood and in the adjacent 10 blocks along E. Madison Street centered on the site found many contemporary and most traditional structures provide regular bay windows of angled or squared design, 12 examples are shown here:



Nearby Context: Upper Level Setbacks

The Design Review Board's guidance to consider alternative massing consistent with the local context "such as upper-level setbacks" led us to analyze more closely the neighboring structures. Our survey of the 9-block immediate neighborhood and in the adjacent 10 blocks along E. Madison Street centered on the site found only one contemporary and no traditional structure with upper-level setbacks. The lone exception is Trace Lofts, a unique condition.

With this context in mind, the proposed design does not provide upper-level setbacks. However, in order to accommodate both existing and future adjacent developments, we have allocated the floor area that might have been applied to upper setbacks to increased side-yard setbacks on the west and north property lines. 12 examples are shown here:





16 PARKING PLANS

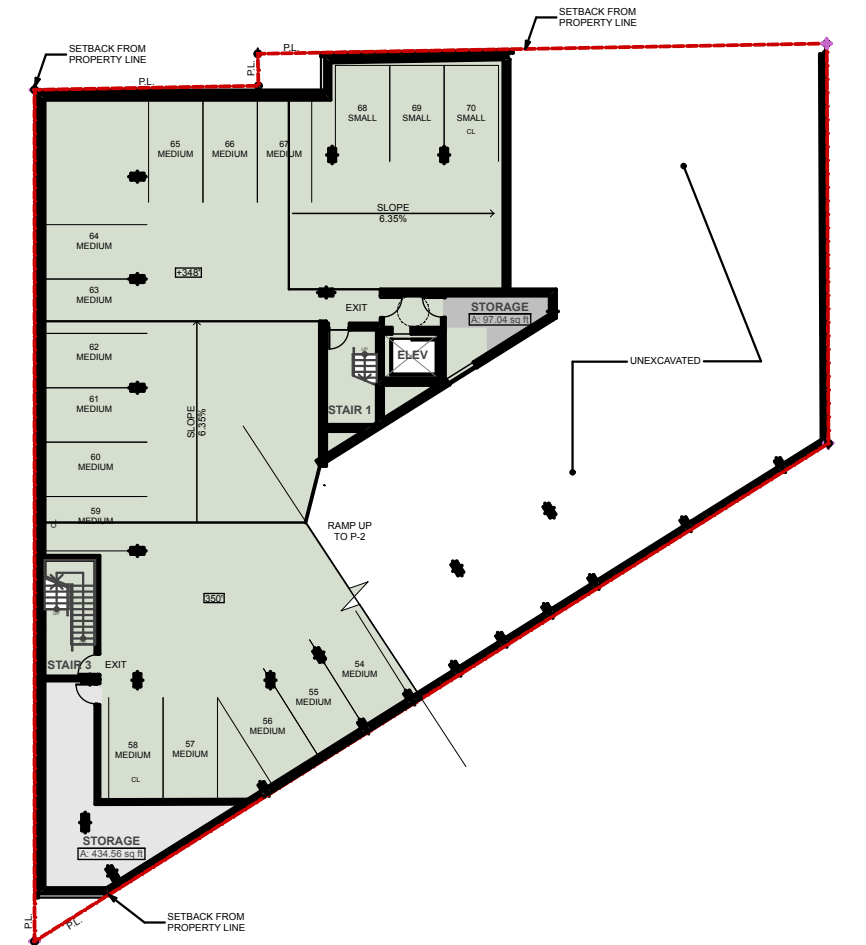


Parking 1

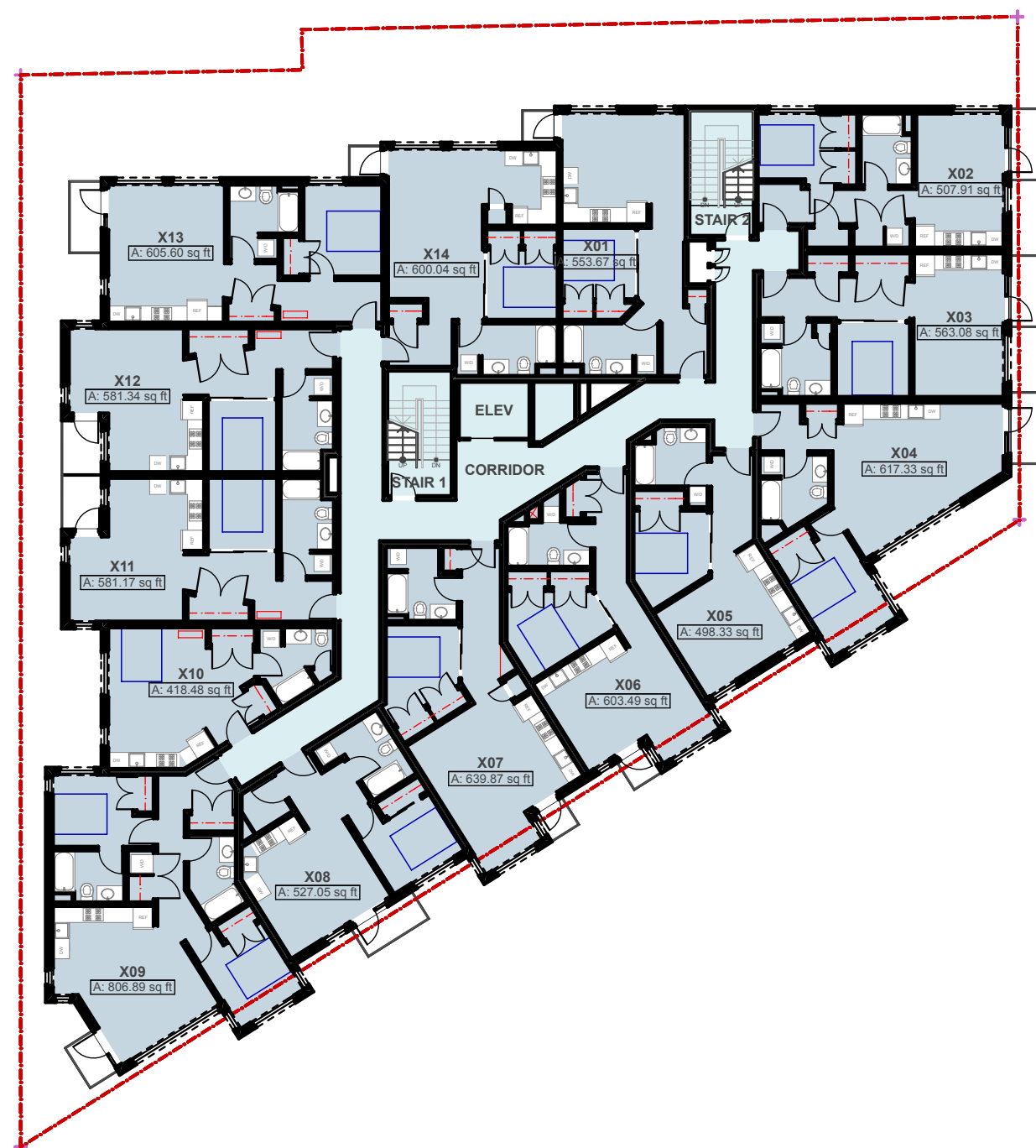
SCALE: 1" = 30' Parking 2



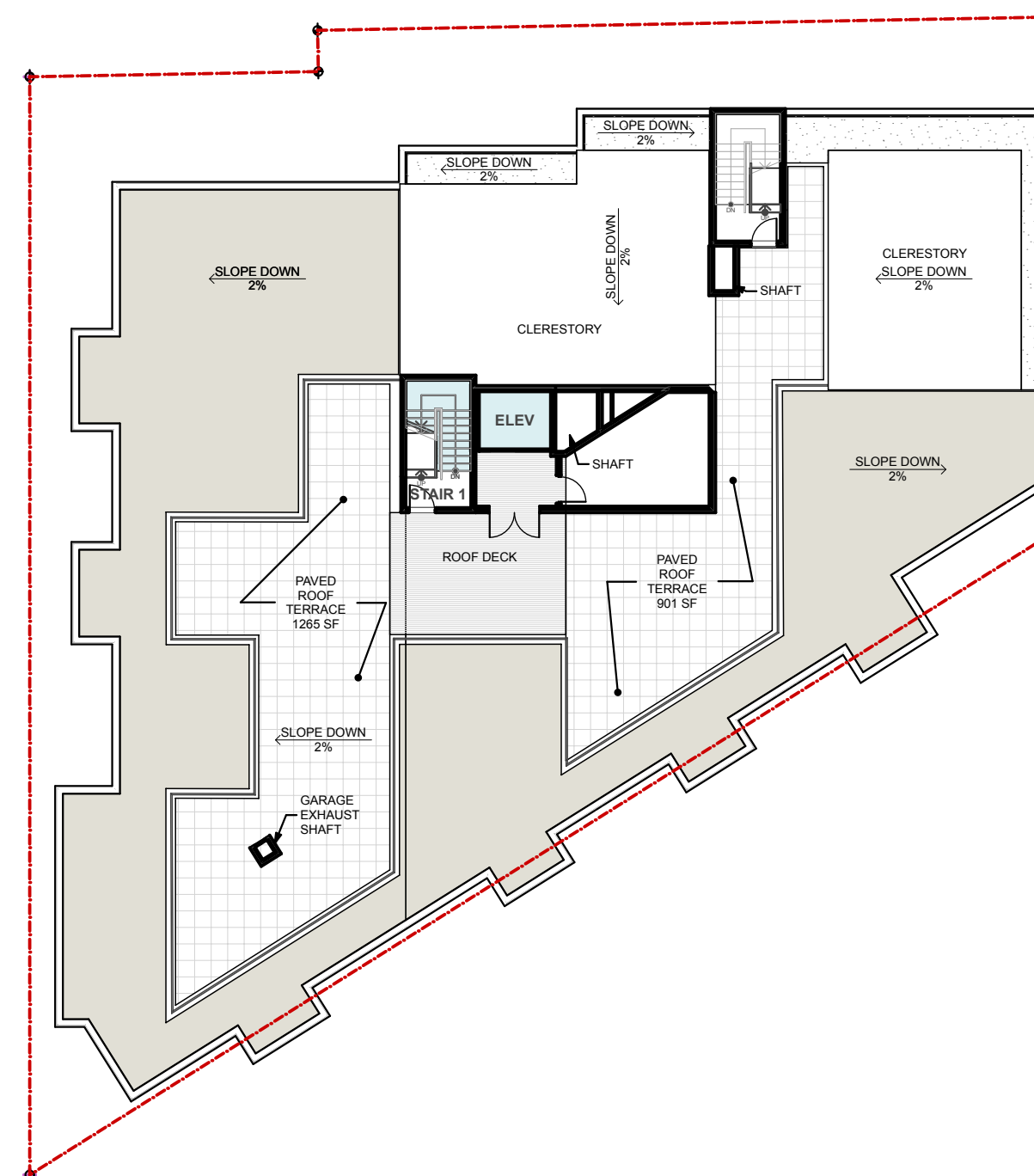
SCALE: 1" = 30' Parking 3



SCALE: 1" = 30'



SCALE: 1" = 20'



SCALE: 1" = 20'

18 ELEVATIONS



South Elevation- East Madison Street




East Elevation- 15th Avenue



North Elevation



West Elevation

	Code Requirement	Departure Requests	Explanation for Departure	Design Guidelines Citations
	1 Façade Transparency Section SMC 23.73.014 A 3: Minimum of 60% street-level facades between 2' and 12' required to be transparent.	The proposed design provides 46% transparency of the street-level façade between 2' and 12'.	The proposed project has made it a priority to provide at-grade entrances along E. Madison St. The E. Madison St. frontage slopes approximately 14' up from the SW up to the NE corner along 15th Ave. This significant grade change makes it infeasible to meet the transparency requirements along 15th Ave due to the 2nd Floor structure falling within the 12' height zone. A necessary shear wall has been mitigated with a green screen trellis.	CS2-B 2: Connection to the street: Providing level, accessible entrances along E. Madison is a design priority to provide the best connection to the street. PL2 A 1: Access for all: Provide access for people of all abilities: The project is designed so that the street level spaces meet E. Madison providing universal access, and creating limitations on façade transparency on the 15th Ave. frontage
	2 Sight Triangles Section SMC 23.47.030 G 1 requires 10' sight triangles at each side of two-way driveways less than 22' wide.	In lieu of sight triangles, the proposed design includes visual signals to alert pedestrians of approaching vehicles. The DRB supported this departure at the EDG meeting #3.	The Pike/Pine Neighborhood Plan and the Design Review Guidelines both strongly prefer minimizing the width and height of garage openings. The proposed substitution of visual signals for sight triangles has been used successfully on many projects as a viable approach to minimizing garage opening size and improving the pedestrian streetscape.	DCI B1b & c: Minimize width of driveways and employ multi-sensory areas of potential pedestrian/vehicular conflict



Code Requirement	Departure Requests	Explanation for Departure	Design Guidelines Citations
3 Street Facade at SW Corner Section SMC 23.47.008.A3: Street-level, street-facing facades shall be located within 10' of street lot line unless approved landscaping or open spaces are provided.	In lieu of the street-level façade within 10' of the lot line at the SW corner, the proposed design provides a stairway to the live/work terrace and landscaping.	At the SW corner, the design provides a open, landscaped stairway, leading to a landscaped terrace and the live/work apartments. These stairs provide a visual and physical connection to the live/work units, completing a connection to the north walkway adjacent to the Paramount Apartments. The setback anticipates future development to the west.	CS2-B 2: Identify opportunities to make a strong connection to the street: CS3-A 4: Evolving Neighborhoods: Provide context for future adjacent development. PLI-B 3: Pedestrian amenities: Provide features that enhance the character and amenity of the pedestrian environment.
4 Garage Access on E. Madison Per SMC 23.47.032.C: E. Madison St. has been determined to be the front line, and per SMC 23.47A.032.A1.c that access is permitted across 15th Avenue because it is a side lot line.	The proposed design provides 46% transparency of the street-level façade between 2' and 12'.	Access from E. Madison St. results in a project compatible with existing development and minimizes potential impacts by placing all parking below grade. The only viable vehicular entrance to the site was at the SW corner, necessitated by the site's 14'+ slope, making access infeasible from the NW corner (Per EDG). Traffic analysis indicates minimal traffic impact for a driveway at the Madison location. Trash/recycling pick up will be located on 15th Ave. The proposed access minimizes impacts on the Paramount Apartments, allowing the adjacent walkway/utility access to be designed in a pedestrian-friendly manner. Proposed access eliminates 7 above grade parking stalls, reducing the western blank wall from 24' to 12' high. <i>Please see supplemental statement on pages 38 and 39</i>	CS2-D.2 Existing Site Features: Use changes in topography...to help make a successful fit with adjacent properties. CS2-D.5. Respect adjacent properties to minimize disrupting the privacy of residents in adjacent buildings. PLI-B Pedestrian Connections are provided by a paved and landscaped pedestrian walkway on the W and N sides. PL3-B Security & Privacy is enhanced for the south facing units of the Paramount Apartment. DCI-B-I: Parking Access and Location promotes pedestrian safety and minimizes conflicts between vehicles and non-motorists, by eliminating steep driveway on north. DCI-III: Pike/Pine Supplemental Guidance: Minimize visual Impact of Parking Structures.



24 STREET LEVEL RENDERING AT MADISON









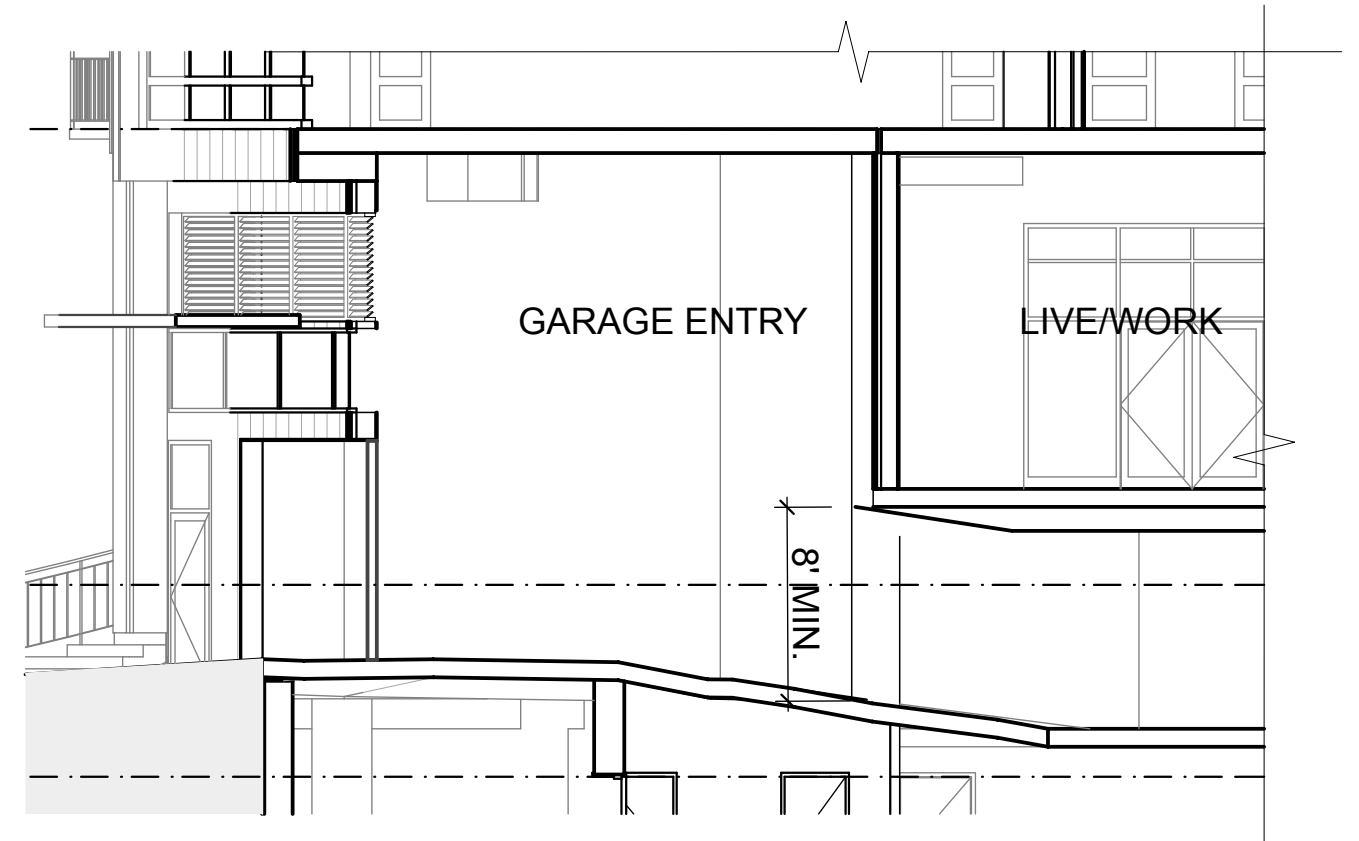


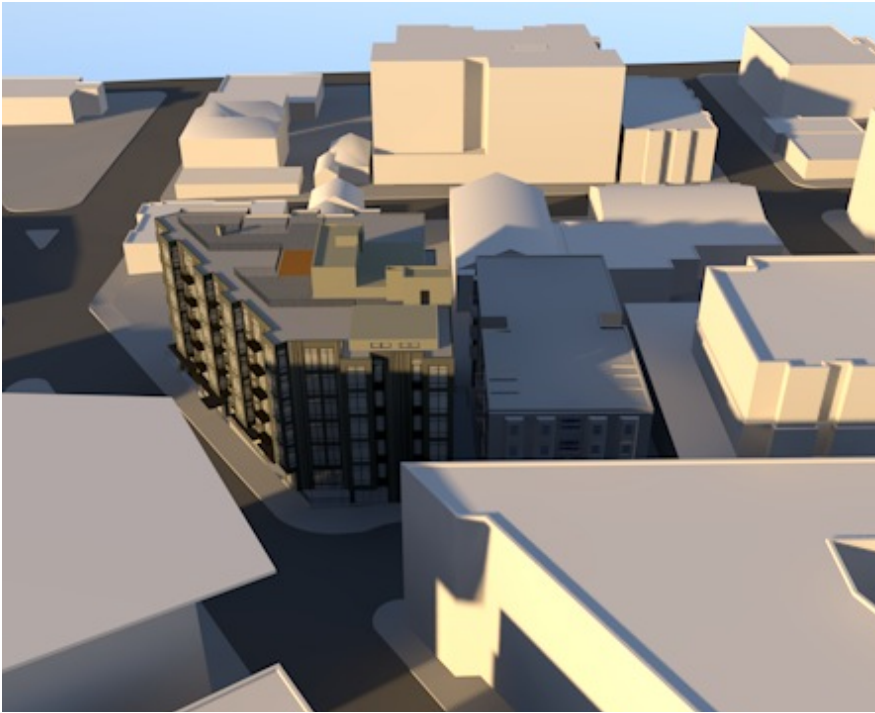


30 GARAGE ENTRY



RIGHT TURN IN, RIGHT
TURN OUT ONLY





January 20th at 10:00 AM



June 20th at 11:30 AM



March 20th at 4:00 PM



June 20th at 5:30 PM

32 MATERIALS



AEP Span Cool Dark Bronze



Dark Bronze Vinyl Windows



Dark Bronze Balconies



Hardie to match Champagne color



AEP Span Vintage



Cast-In-Place Concrete Planters



Metal Siding Mockup



AEP Span Metallic Champagne



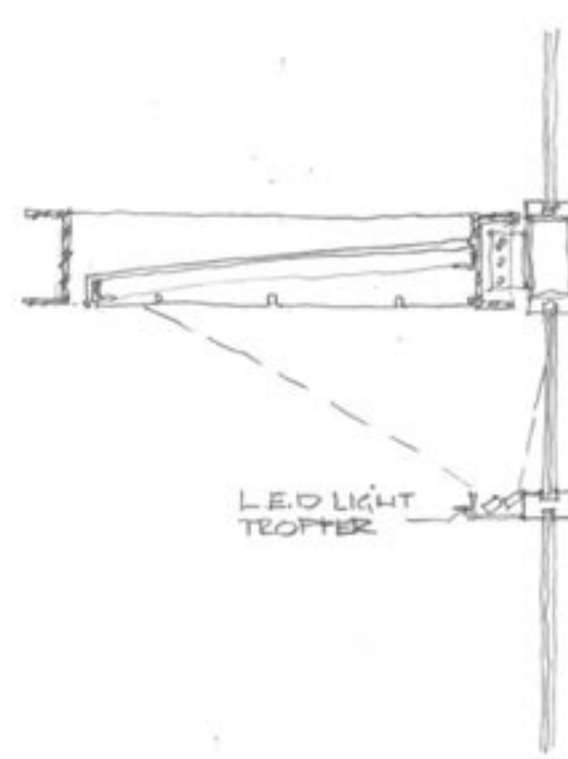
CANOPY LIGHTING



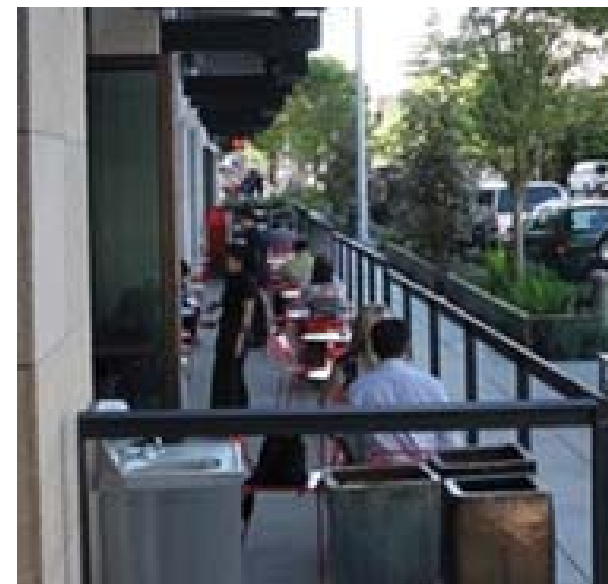
CANOPY LIGHTING



CANOPY LIGHTING



CANOPY LIGHTING



STREET RAILING



CANOPY SIGNAGE



HANGING LANDSCAPE



WALL SIGNS



WALL SIGNS



WALL SIGNS



1420 EAST MADISON • Karen Kiest | Landscape Architects



Streetside Dining Tree Buffer



Hanging Landscape Planters

STREETSCAPE



1420 EAST MADISON • Karen Kiest | Landscape Architects



fire pit and view



BBQ etc.



trees in planters



bands of green roof

ROOF

trees



'Bowhall' Maple
Acer Rubrum 'Bowhall'



Regal Prince Oak
Quercus Robur x *Q. Bicolor* 'Long'



Roof Planters: Persian Ironwood
Parrotia persica

shrubs, groundcovers & vines



'Winter Gem' Japanese Boxwood
Buxus 'Winter Gem'



'Kelsey' Red Twig Dogwood
Cornus stolonifera 'Kelsey'



Japanese Forest Grass
Hakonechloa macra



'Amber' Flower Carpet Rose
Rosa Flower Carpet 'Amber'



Boxleaf Honeysuckle
Lonicera pileata



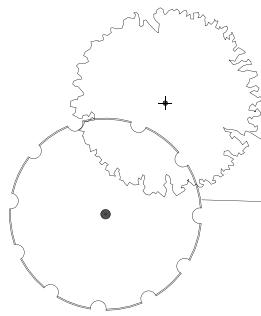
'Gulf Stream' Heavenly Bamboo
Nandina domestica 'Gulf Stream'



Vine - Evergreen Clematis
Clematis armandii



Vine - Carolina Jessamine
Gelsemium sempervirens



* DROUGHT TOLERANT PLANTS PER GREEN SEATTLE GREEN FACTOR PLANT LIST

** DROUGHT TOLERANT PLANT REFERENCES:
- SUNSET WESTERN GARDEN BOOK
- GREAT PLANT PICKS - ELISABETH C. MILLER BOTANICAL GARDEN

PLANT LIST

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION	SPACING
STREET TREES - APPROVED BY SDOT FORESTER BILL AMES 11.20.2014					
+	ACER RUBRUM 'BOWHALL'	'BOWHALL' MAPLE	2-1/2" CAL.	B&B	PER PLAN
•	QUERCUS ROBUR X Q. BICOLOR 'LONG'	'REGAL PRINCE' OAK	2-1/2" CAL.	B&B	PER PLAN
SHRUBS & GROUNDCOVERS - RIGHT-OF-WAY					
	BUXUS MICROPHYLLA JAPONICA 'WINTER GEM'*	'WINTER GEM' JAPANESE BOXWOOD	1 GAL.	CONT.	24" O.C.
	CORNUS STOLONIFERA 'KELSEY'*	'KELSEY' RED TWIG DOGWOOD	1 GAL.	CONT.	24" O.C.
	HAKONECHLOA MACRA	JAPANESE FOREST GRASS	5 GAL.	CONT.	24" O.C.
	ROSA 'AMBER' FLOWER CARPET	'AMBER' FLOWER CARPET ROSE	1 GAL.	CONT.	24" O.C.
	SARCOCOCCA RUSCIFOLIA	SWEET BOX	1 GAL.	CONT.	24" O.C.
	EPIMEDIUM ALPINUM*	EPIMEDIUM	1 GAL.	CONT.	24" O.C.
	ASTILBE X ARENDsii 'PEACH BLOSSOM'	'PEACH BLOSSOM' ASTILBE	1 GAL.	CONT.	24" O.C.
	POLYSTICHUM MUNITUM**	SWORD FERN	1 GAL.	CONT.	24" O.C.
SHRUBS & GROUNDCOVERS - ONSITE					
⊖	CAREX TESTACEA	ORANGE SEDGE	2 GAL.	CONT.	24" O.C.
⊕	LONICERA PILEATA	BOXLEAF HONEYSUCKLE	5 GAL.	CONT.	24" O.C.
⊗	NANDINA DOMESTICA 'GULF STREAM'*	'GULF STREAM' HEAVENLY BAMBOO	1 GAL.	CONT.	24" O.C.
⊞	50% PACHYSANDRA TERMINALIS* 25% DRYOPTERIS ERYTHROSORA 15% BLECHNUM SPICANT 10% HELLEBORUS X HYBRIDUS 'SUNSHINE'*	50% JAPANESE SPURGE 25% AUTUMN FERN 15% DEER FERN 10% 'SUNSHINE' HELLEBORE	1 GAL. 1 GAL. 1 GAL. 1 GAL.	CONT. CONT. CONT. CONT.	24" O.C. 24" O.C. 24" O.C. 24 O.C.
VINES					
*	CLEMATIS ARMANDII **	EVERGREEN CLEMATIS	1 GAL.	CONT.	PER PLAN
⌘	GELSEMIUM SEMPERVIRENS **	CAROLINA JESSAMINE	1 GAL.	CONT.	PER PLAN
GREENROOF PLANTING					
⊞	SEDUM TILE PREVEGETATED MATS*, 5.25" SOIL DEPTH				
COLOR MAX. AVAILABLE FROM ETERA, CONTACT DAVID GILMORE 360.661.2767					

Johnson Architecture & Planning LLC , founded in 1987, is dedicated to collaborating with our clients to achieve extraordinary projects in the urban environment. Our firm provides architectural and planning services, including site and project feasibility analysis, project development services, architecture, urban planning, space planning and construction management.

Our core philosophy of architectural practice is to find the most imaginative, appropriate and cost-effective solutions that meet the highest standards of design and amenity. Throughout our practice, we have worked on and enjoyed the challenge of highly complex and programmatically difficult projects. Above all, we approach each project as an opportunity to provide a legacy of good buildings in our community and to create a noteworthy design.



Press II



Oddfellows Renovations



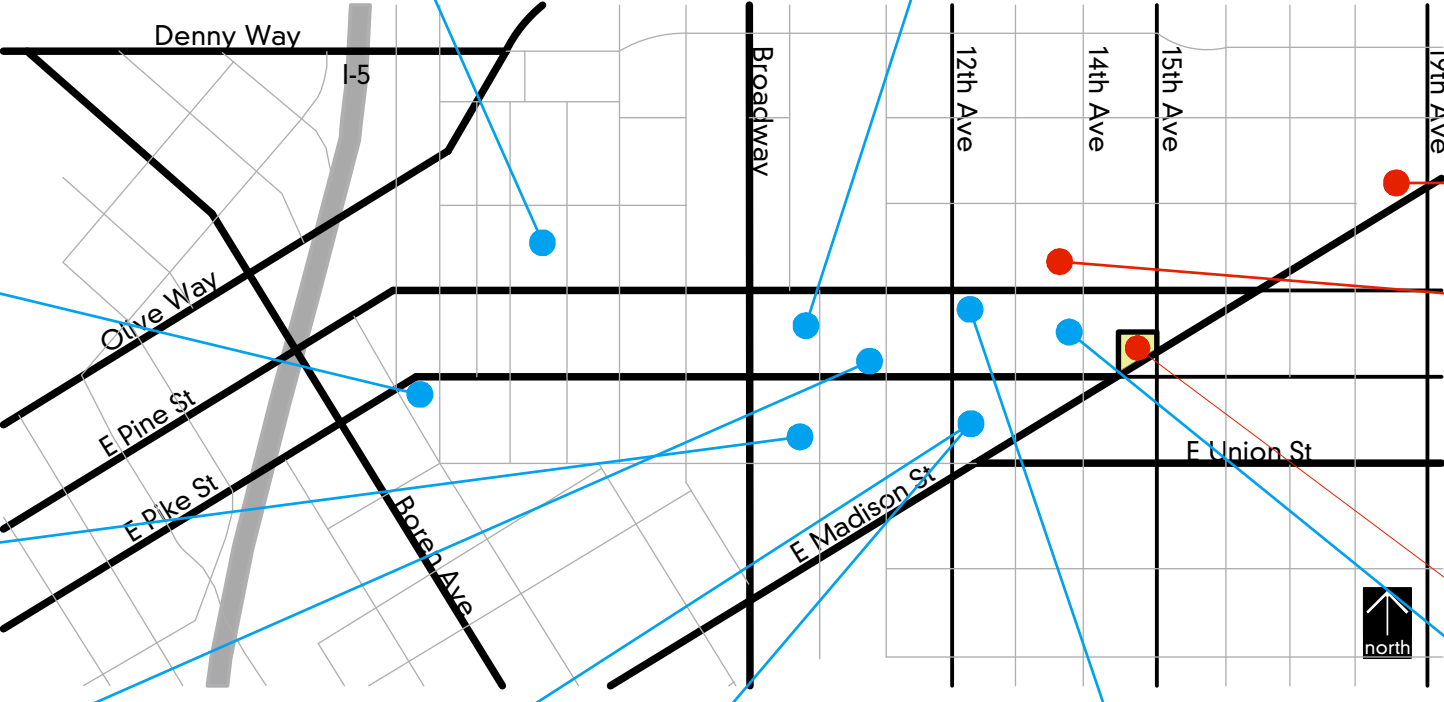
19th Avenue Lofts



Pike Lofts



10th & Union- Under Construction



Metropolitan Companies, Inc. Projects



Lawrence Lofts



Collins on Pine

Project Site: 1420 E. Madison St.



Monique Lofts



12th & Madison: Trace North



12th & Madison: Trace Lofts



Packard Building



REO Flats - Under Construction

38 GARAGE ACCESS DEPARTURE REQUEST SUPPLEMENTAL STATEMENT I

Requested Design Departures

I. Garage Entrance on E. Madison Street.

Pursuant to SMC 23.47.032.C the Director has determined that East Madison Street is the front lot line, and pursuant to SMC 23.47A.032.A.1.c the Director has determined that access is required from 15th Avenue because it is the side lot line.

We request a design departure from this requirement because access to parking from E. Madison Street will result in a development that better meets the intent of the design guidelines discussed below.

The proposed access off of E. Madison St. at the southwest corner of the site (where the existing 36-foot curb cut would be replaced with a 20-foot curb cut further to the west), will be at the lowest portion of the site, which will allow vehicles to get below grade as quickly as possible. This will minimize the height of the blank wall next to the parking garage, which faces the First A.M.E. church property to the west. The requested departure will allow the blank western wall to be limited to 12 feet high, allowing a landscaped terrace above the wall from which vegetation will descend to mitigate the appearance of the blank wall. The building will be a better neighbor to the church and more pleasing in appearance to passersby on E. Madison Street. The E. Madison Street access also will allow four west-facing live/work units at the landscaped terrace level, further enhancing the appearance of the west façade.

In contrast, requiring access from 15th Avenue will necessitate access from the highest portion of the site, where the grade is effectively a full story higher (14') than at the southwest corner. This will result in a 24-foot high blank wall facing the First A.M.E. church because the code limits the maximum slope of the driveway within the garage, and two stories will be required for the driveway to spiral down to parking instead of one story. A driveway off of 15th Avenue would still be steep enough (at code maximum 15% grade) to create sightline and therefore safety issues for vehicles exiting the garage onto 15th Avenue. The 24-foot high blank wall facing the A.M.E. church would leave no room for a terrace or for vegetation to mitigate the appearance of the blank wall.

At the third EDG meeting the Board agreed that “the below grade parking should be accessed from E. Madison St near the southwest corner, as proposed.” (See page 11, section 3 of the Third Early Design Guidance). The Board also stated that the easement between the new structure and the Paramount Apartments should be paved and landscaped in a pedestrian-friendly manner, and the requested departure will allow this to happen.

Specifically, the proposed access on E. Madison Street will result in a development that better meets the intent of the following city-wide and Pike/Pine design guidelines:

SEATTLE DESIGN GUIDELINES:

CS2-D HEIGHT, BULK, AND SCALE

2. **Existing Site Features:** Use changes in topography, site shape, and vegetation or structures to help make a successful fit with adjacent properties; for example siting the greatest mass of the building on the lower part of the site or using an existing stand of trees to buffer building height from a smaller neighboring building.

* * *

5. **Respect for Adjacent Sites:** Respect adjacent properties with design and site planning to minimize disrupting the privacy and outdoor activities of residents in adjacent buildings.

The E. Madison Street driveway access better meets the intent of CS2-D-2 by taking advantage of the change in topography of the site and its unusual shape to minimize the height of the blank wall adjoining the parking garage that faces the property to the west and is visible to passersby on E. Madison Street. Access from E. Madison Street also allows for four additional residential apartments facing west, enhancing the residential character of the building as viewed from the west. It also will allow a better fit with the Paramount Apartments to the north by minimizing vehicle use of the easement by eliminating a 7 car parking garage and limiting use to utility service only, and by allowing it to be paved and landscaped in a pedestrian-friendly manner.

The E. Madison Street driveway access better meets the intent of CS2-D-5 by allowing a building design that better respects both the church property to the west and the apartments to the north, and by minimizing disruptions to the privacy of the residents in the Paramount Apartments from traffic off of 15th Avenue.

By comparison, parking access from 15th Avenue results in a 24' high exposed parking structure on the west façade and a steep driveway directly adjacent to the Paramount Apartments.

PLI-B WALKWAYS AND CONNECTIONS.

1. **Pedestrian Infrastructure:** Connect on-site pedestrian walkways with existing public and private pedestrian infrastructure, thereby supporting pedestrian connections within and outside the project.

The E. Madison Street driveway access better meets the intent of PLI-B-1 by providing for a paved and landscaped pedestrian walkway between the proposed structure and the Paramount Apartments.

By comparison, driveway access from 15th Avenue would eliminate this shared walkway and replace it with a vehicular driveway on the north and a blank wall facing west.

PL3-B RESIDENTIAL EDGES

- I. **Security & Privacy:** Provide security and privacy for residential buildings through the use of a buffer or semi-private space between the development and the street or neighboring buildings. Consider design approaches such as elevating the main floor, providing a setback from the sidewalk, and/or landscaping to indicate the transition from one type of space to another

The E. Madison Street driveway access better meets the intent of PL3-B-I by offering significantly greater security and privacy for the south facing units of the Paramount Apartments, particularly the ground floor and basement units.

By comparison, driveway access from 15th Avenue would create an open, very steep driveway directly adjacent to their bedroom and living room windows.

DCI-B VEHICULAR ACCESS AND CIRCULATION

- I. **Access Location and Design:** Choose locations for vehicular access, service uses, and delivery areas that minimize conflict between vehicles and non-motorists wherever possible. Emphasize use of the sidewalk for pedestrians, and create safe and attractive conditions for pedestrians, bicyclists, and drivers by:
 - a. using existing alleys for access or, where alley access is not feasible, choosing a location for street access that is the least visually dominant and/or which offers opportunity for shared driveway use;
 - b. where driveways and curb cuts are unavoidable, minimize the number and width as much as possible; and/or
 - c. employing a multi-sensory approach to areas of potential vehicle-pedestrian conflict such as garage exits/entrances. Design features may include contrasting or textured pavement, warning lights and sounds, and similar safety devices.

The E. Madison Street driveway access better meets the intent of DCI-B-I because the proposed driveway access on E. Madison is located between two traffic signals and is downstream from the relocated transit stop. This location provides for low speed vehicular ingress and egress onto a low volume pedestrian street. (ref. Gibson traffic study). There is no alley or opportunity for shared use, and the proposed driveway will be consistent with subsections (b) and (c) by minimizing the width and employing a multi-sensory approach to pedestrian safety.

By comparison, access off of 15th Avenue will increase the potential for unsafe conflicts between vehicles and non-motorists by creating an exit where the driver's ability to see will be limited by the steepness of the grade.

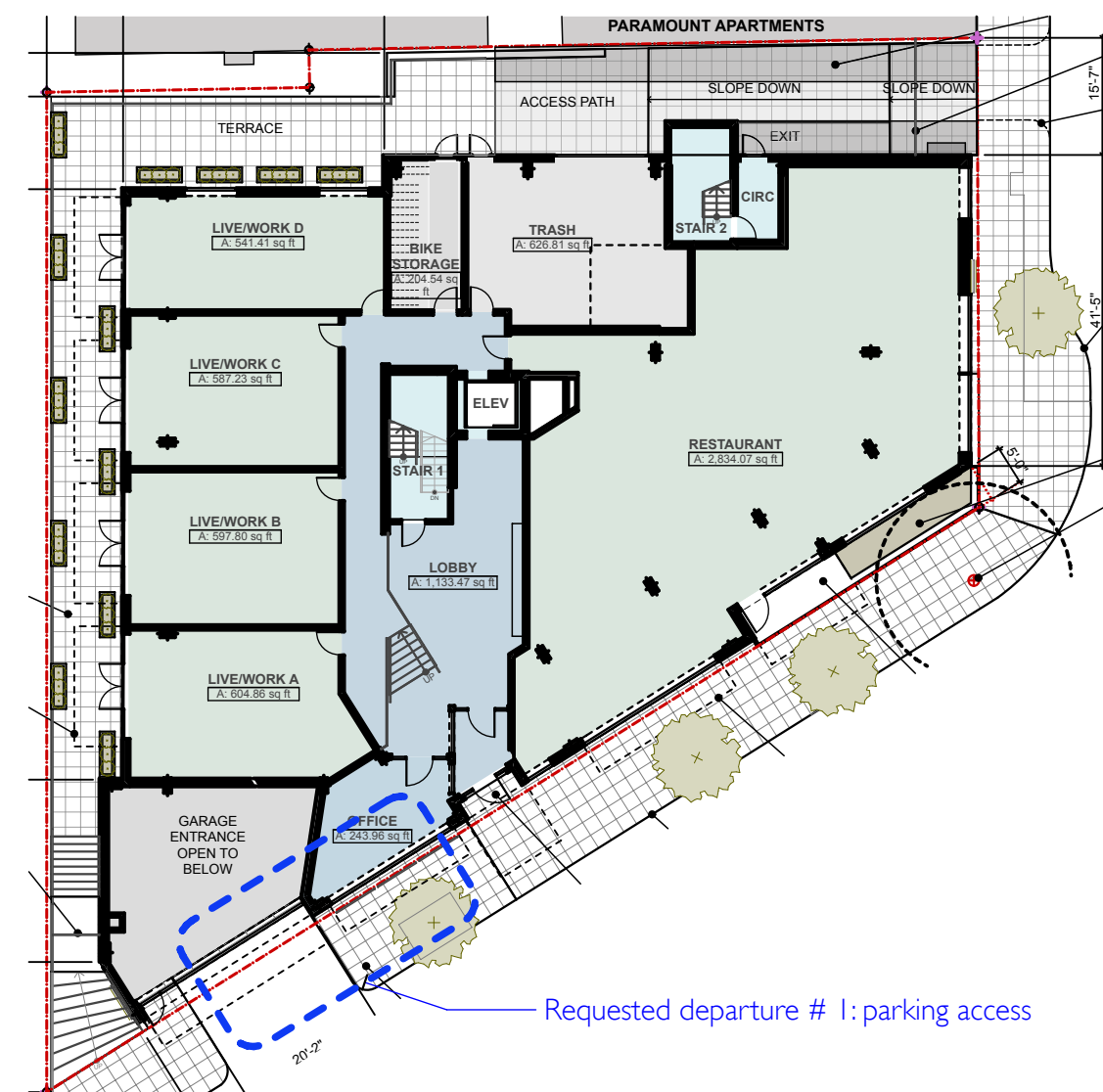
PIKE/PINE NEIGHBORHOOD DESIGN GUIDELINES:

DCI-III Visual Impact of Parking Structures

- i. Incorporate vertical landscaping (trellises) or artwork as screens where feasible
- ii. Parking structures should provide commercial or other pedestrian-oriented uses at street level.

The E. Madison Street driveway access better meets the intent of DCI-III-i and ii by minimizing exposed blank walls on the west and north facades. The western façade of the proposed design has one level of above grade parking with a landscaped terrace and four live/work apartments above, and the landscaped terrace will provide an opportunity for vertical landscaping.

By comparison, parking access from 15th Avenue results in a 24' high exposed parking structure on the west façade and eliminates the west facing 2nd Floor apartments.



40 SEATTLE CITY LIGHT REQUIRED SETBACK DIAGRAM

