

olive





① panoramic view along 14th Ave. E, looking east



② panoramic view along 14th Ave. E, looking west



③ panoramic view along E Olive St. looking north

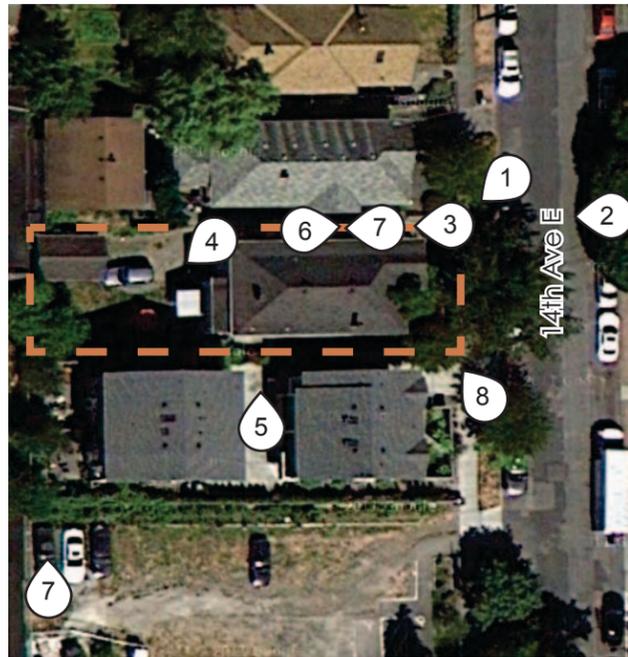


④ panoramic view along E Olive St. looking south

1. Please describe the existing site, including location, existing uses and/or structures, topographical or other physical features, etc.

Site

1623 14th Avenue, our site, is located on the west side of 14th Avenue, three properties south of E Olive Street. Its dimensions are 128 feet east-west and 40 feet north-south. The site is bounded on both sides by sites which contain multifamily uses. Two properties to the south is a large open unimproved parcel that is used for off-street parking.



Uses

The lot contains an existing structure, built in 1909 that will be deconstructed.

Topography

The site is generally flat south to north, but slopes slightly downhill to the east, creating a 12-inch grade difference between 14th Ave and the rear of the site, at its west property line.

Access

14th Avenue, between E. Pine and E. Jackson, is a collector arterial. The site is located a half block north of E Pine, offering excellent vehicular, bus, bicycle and pedestrian access. 14th is basically flat, and it links Volunteer Park to the north and Rainier Avenue S to the south. Running east-west, and a half block south of the site is E Pine St, a minor arterial, with relatively low speed traffic, and provides excellent walking, bus, vehicular and biking access to the Pike /Pine corridor, Capitol Hill neighborhood, First Hill and downtown Seattle. In the area close to our site E Pine generally slopes downhill from east to west. Street parking is available along both sides of 14th Avenue in front of the site, only along the south side of E Olive Street, and both sides of E Pine Street. One block east of the site is 15th Ave, a minor arterial, providing easy access to the 15th Ave E shopping district and Volunteer Park. E Madison Street, two blocks south of the site is a principal arterial, providing both direct access to downtown and to Lake Washington.

There are nearby bus stops at E Pine Street and 15th Ave and 13th Ave, as well as at and E Madison Street and 14th Ave. These provide access to Metro bus lines numbers 10, 11 and 12. These routes serve Capitol Hill, downtown Seattle and Madison Park/Valley.



① Looking southwest from 14th Ave. E



② Looking west from 14th Ave. E



③ Looking west from 14th Ave. E



④ Looking south west from the site



⑤ Looking north from the adjacent property



⑧ Looking northwest from 14th Ave. E



⑥ Looking east from the north edge of the site



⑦ Looking west from the walkway

Site analysis summary:

14th Avenue E

- Slopes slightly downhill to the south, gradually levelling off towards E Madison St.

E Olive Street

- Slopes downhill to the west from 18th Ave E down to Cal Anderson Park with its steepest section between 14th and 15th Ave E.

E Olive Street traffic

- Minor residential street with parking allowed on the south side only. Connects 18th Ave E to the east with Cal Anderson Park to the west.

14th Avenue E traffic

- Minor residential street from E Pine St to the north beyond site. 14th Ave E is a collector arterial between E Pine St and E Jackson St to the south. Connects to E Pine Street and E Madison Street which provides access to Capitol Hill, Downtown Seattle, Madison Valley and Madison Park.

Solar access

- The site has good solar access to the east, southeast, south, southwest and west.

Building mass

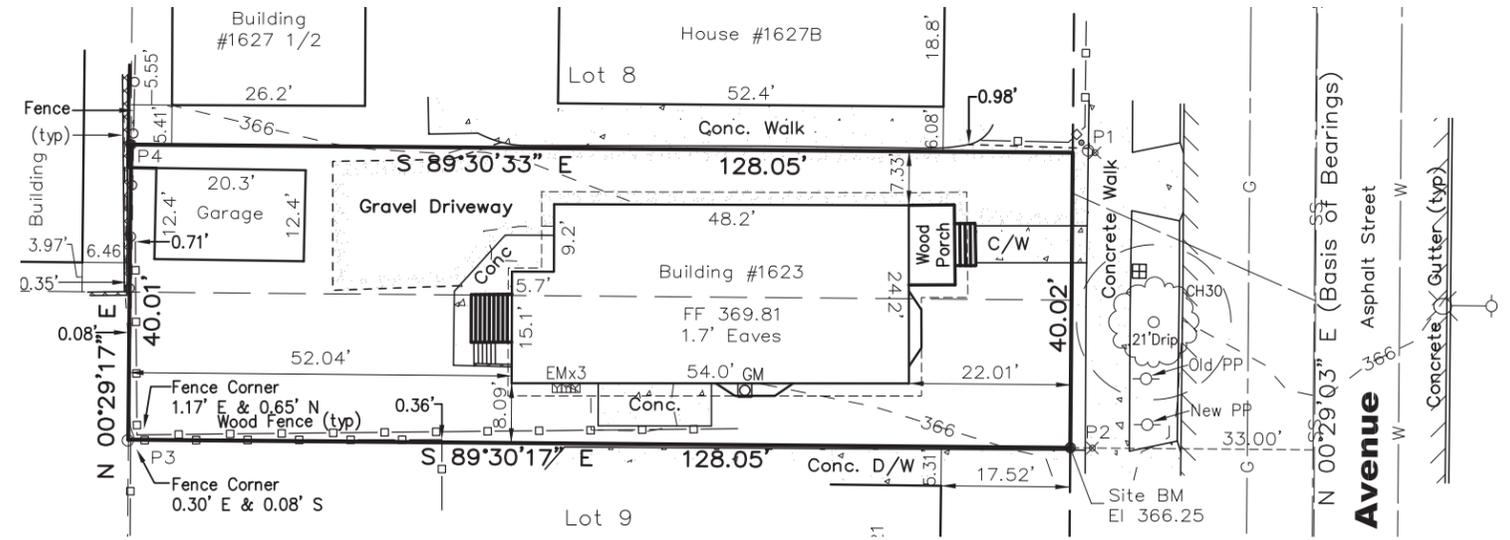
- The massing essentially runs east west, with all the homes organized to have direct southern exposure with some accessing a territorial view to the southwest. The overall approach to the massing is to provide a relatively conservative primary building mass that is interrupted by a multiple vertical contrasting elements. Modulation is created through these rhythmic elements along each façade and is used to clearly denote pedestrian access and entrances. Along the north property edge we create an active pathway that connects to a central courtyard. At the south edge a new driveway provides access to below grade parking for most of the homes. A centrally located stair provides direct access between the courtyard and parking. Homes access their roof decks from either an exterior stair or a stair penthouse, which is shaped to minimize impacts on adjacent properties.

Views

- There will territorial and possible downtown views from the units at the west edge of the property. All units will have territorial and possible downtown views from roof top decks.

Parking access

- Off-street parking will be accessed from the driveway at the southeast corner of the property. Parking will be below grade beneath the proposed building.



existing survey



2. Please indicate the site's zoning and any other overlay designations, including applicable Neighborhood Specific Guidelines.

Site location:	1623 14th Avenue E 5120 square feet
Site Zoning:	Lowrise LR-3
SEPA Review: CAM 208	Required with construction of over 8 dwelling units in LR-3, Not required for this proposal; see density calculation below (23.45.008).
Permitted Uses: 23.45.004	Residential Use: Townhouse and Single Family
Height: 23.45.514	Table A. Structure Height for Lowrise Zones in Feet. Maximum height is 30 feet for Townhouse Developments in LR3 in Urban Centers, Urban Villages, and Station Area Overlay Districts. D3. In LR3 zones, for structures subject to a 30 foot height limit, the ridge of pitched roofs on principal structures may either: a. extend up to 10 feet above the height limit, if the height exception provided in 23.45.514.F is not used, and the number of full stories above grade is limited to three; or b. extend up to 5 feet above the height limit, if the height exception provided in 23.45.514.F is used. E1. Shed and butterfly roofs in LR zones. In LR zones, the high side(s) of a shed or butterfly roof may extend 3 feet above the height limits set in Table A for 23.45.514, provided that the low side(s) of the shed or butterfly roof are no higher than the height limit (see Exhibit A for 23.45.514). F. For apartments in LR2 zones, and for all residential uses in LR3 zones, the applicable height limit is increased 4 feet above the height shown on Table A for 23.45.514 for a structure that includes a story that is partially below-grade, provided that: 1. This height exception does not apply to portions of lots that are within 50 feet of a single-family zoned lot, unless the lot in the LR zone is separated from a single-family zoned lot by a street; 2. The number of stories above the partially below-grade story is limited to three stories for residential uses with a 30 foot height limit and to four stories for residential uses with a 40 foot height limit; 3. On the street-facing facade(s) of the structure, the story above the partially below-grade story is at least 18 inches above the elevation of the street, except that this requirement may be waived to accommodate units accessible to the disabled or elderly, consistent with the Seattle Residential Code, Section R322, or the Seattle Building Code, Chapter 11; and 4. The average height of the exterior facades of the portion of the story that is partially below-grade does not exceed 4 feet, measured from existing or finished grade, whichever is less
Height: Rooftop features: 23.45.514	J4. In LR zones, the following rooftop features may extend 10 feet above the height limit set in subsections 23.45.514.A and F, if the combined total coverage of all features does not exceed 15 percent of the roof area or 20 percent of the roof area if the total includes screened mechanical equipment: a. Stair penthouses, except as provided in subsection 23.45.514.J.6; b. Mechanical equipment; J6. Subject to the roof coverage limits in subsections 23.45.514.J.4 and 5, elevator penthouses may extend above the applicable height limit up to 16 feet. J7. For height exceptions for solar collectors, see Section 23.45.545
Floor Area Ratio: Table A for 23.45.010	B. Floor Area Ratios. Floor area ratio limits apply in LR zones as shown in Table A for 23.45.510. Table A. LR3, Inside an Urban Village, for Townhouse Developments 1.2 or 1.4. The higher FAR limit applies if the project meets the standards of subsection 23.45.510.C C. In LR zones, in order to qualify for the higher FAR limit shown in Table A for 23.45.510, certain standards shall be met regarding: green building performance standards; alley access and improvement standards; parking location if parking is required; access to parking if parking is provided. E. The following floor area is exempt from FAR limits: 3. The floor area contained in structures built prior to January 1, 1982 as single-family dwelling units that will remain in residential use, provided that: a. no principal structure is located between the existing single-family dwelling unit and the street lot line along at least one street frontage. If the single-family dwelling unit is moved on the lot, the floor area of the dwelling remains exempt if it continues to meet this provision; and b. the exemption is limited to the gross square footage in the single-family dwelling unit as of January 1, 1982.
Density: Table A for 23.45.512	Table A. Density Limits in Lowrise Zones: 1/1600 or No limit for Townhouses in LR3 zone. For townhouses that meet the standards of subsection 23.45.510.C, there is no density limit in LR2 and LR3 zones.

Structure Width: Table A for 23.45.527	Table A. LR3 inside Urban Villages, Urban Centers or Station Area Overlay Districts is 150 feet. B1. The maximum combined length of all portions of facades within 15 feet of a lot line that is neither a rear lot line nor a street or alley lot line shall not exceed 65 percent of the length of that lot line, except as specified in subsection 23.45.527.B.2.															
Setbacks and Separations: Table A for 23.45.518	For All LR Zones: <table border="0" style="width: 100%;"> <tr> <td>Setback</td> <td>Single-Family</td> <td>Townhouse Dev.</td> </tr> <tr> <td>Front</td> <td>7 feet avg, 5 min.</td> <td>7 feet avg, 5 min.</td> </tr> <tr> <td>Rear</td> <td>0 with Alley, 7 if no alley</td> <td>7 feet avg, 5 min.</td> </tr> <tr> <td>Side Setback for Facades 40 feet or less</td> <td>5 feet</td> <td>5 feet</td> </tr> <tr> <td>Side Setback for Facades 40 feet or greater</td> <td>5 feet min.</td> <td>7 feet avg, 5 min.</td> </tr> </table>	Setback	Single-Family	Townhouse Dev.	Front	7 feet avg, 5 min.	7 feet avg, 5 min.	Rear	0 with Alley, 7 if no alley	7 feet avg, 5 min.	Side Setback for Facades 40 feet or less	5 feet	5 feet	Side Setback for Facades 40 feet or greater	5 feet min.	7 feet avg, 5 min.
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Side Setback for Facades 40 feet or less	5 feet	5 feet														
Side Setback for Facades 40 feet or greater	5 feet min.	7 feet avg, 5 min.														
23.45.518	F1. Separations between multiple structures. In LR zones, the minimum required separation between principal structures at any two points on different interior facades is 10 feet															
Parking: 23.54.015 Required Parking 23.54.030 Parking Space Standards 23.45.536 Parking location, access and screening	Table B Residential Uses – Residential uses in commercial and multifamily zones within urban centers or within the Station Area Overlay District(1) - No minimum requirement. B1. If parking is required, it shall be located on the same lot as the use requiring the parking, except as otherwise provided in this subsection 23.45.536.B . B2. Except as otherwise provided in this subsection 23.45.536.B, surface parking may be located anywhere on a lot except: a. between a principal structure and a street lot line b. in the required front setback or side street side setback; and c. within 7 feet of any street lot line.															
Landscaping standards: 23.45.524	A1. All landscaping provided to meet requirements under this Section 23.45.524 shall meet standards promulgated by the Director to provide for the long-term health, viability, and coverage of plantings. A2a. Landscaping that achieves a Green Factor score of 0.6 or greater, determined as set forth in Section 23.86.019, is required for any lot with development containing more than one dwelling unit in Lowrise zones. Vegetated walls may not count towards more than 25 percent of a lot's Green Factor score. B1. 1. Street trees are required if any type of development is proposed, except as provided in subsection 23.45.524.B . 2 and B.3 below and Section 23.53.015. Existing street trees shall be retained unless the Director of Transportation approves their removal.															
Amenity Area: 23.45.522	A1. The required amount of amenity area for rowhouse and townhouse developments and apartments in LR zones is equal to 25 percent of the lot area. A2. A minimum of 50 percent of the required amenity area shall be provided at ground level, except that amenity area provided on the roof of a structure that meets the provisions of subsection 23.45.510.E.5 may be counted as amenity area provided at ground level. A4. For apartments, amenity area required at ground level shall be provided as common space. D1. All units shall have access to a common or private amenity area. D3. Projections into amenity areas. Structural projections that do not provide floor area, such as garden windows, may extend up to 2 feet into an amenity area if they are at least 8 feet above finished grade. D5. Common amenity area for rowhouse and townhouse developments and apartments shall meet the following conditions: a. No common amenity area shall be less than 250 square feet in area, and common amenity areas shall have a minimum horizontal dimension of 10 feet. b. Common amenity area shall be improved as follows: 1) At least 50 percent of common amenity area provided at ground level shall be landscaped with grass, ground cover, bushes and/or trees. 2) Elements that enhance the usability and livability of the space for residents, such as seating, outdoor lighting, weather protection, art, or other similar features shall be provided.															
Design standards: 23.45.529	B. Application of provisions. The provisions of this Section 23.45.529 apply to all residential uses that do not undergo any type of design review pursuant to Chapter 23.41, except single-family dwelling units.															
Standards for certain accessory uses: 23.45.545	C3. Solar collectors on roofs. Solar collectors that meet minimum written energy conservation standards administered by the Director and that are located on a roof are permitted as follows: a. In Lowrise zones up to 4 feet above the maximum height limit or 4 feet above the height of elevator penthouse(s).															



1 View from E Olive St looking West



2 Spring Street Mini Park at 15th Ave and E Spring St.



3 Commercial Building on 14th Ave and E Pine St.



Madison Market at 16th Ave E and E Pine St.



The Oddfellows Building at 10th Ave and E Pine St.



6 Cal Anderson Park

3. Please describe neighboring development and uses, including adjacent zoning, physical features, existing architectural and siting patterns, views, community landmarks, etc.

Zoning

The site, shown in orange on the adjacent page in the aerial photo and zoning map is located in the LR3 zone surrounded by NC3P-40 to the south and NC3-40 to the west. LR3 continues to the east and north with the NC zoned land including the Pike/Pine, 12th Avenue and E Madison corridors.

Uses and Physical Features

There exists a variety of three, four and six-story mixed-use buildings interspersed with two and three-story multi and single-family dwellings in the vicinity. Immediately adjacent to the south is a recently completed three-story auto-court oriented townhouse project. To the north is a single-story single-family structure and two-story structure in its backyard. This is a very dense neighborhood, located within the Pike/Pine Urban Center Village and immediately adjacent to the Capitol Hill Urban Center Village. Pike/Pine and Capitol Hill are vibrant commercial districts with a variety of uses including the cafés and restaurants, retail uses, yoga studios, the Northwest Film Forum, Elysian Brewing Company, schools, bed and breakfasts, temples and churches.

Existing Architecture

The architecture on 14th Avenue varies in scale and building type with most structures three-stories or taller. Across the 14th to the east of the site there are four-story mixed-use buildings adjacent to three-story multi-family structures. On the west side of 14th there are smaller multi-family and single-family structures. There are commercial buildings mixed with multi-family dwellings of varying ages along E Olive Street with a large surface parking lot at the corner of 14th and Olive. Along E Pine are commercial structures of varying ages with character structures and recent mixed-use development. There is no single dominant building typology or architecture, reflecting this mix of zones.

Views

There will be territorial and possible downtown views from the units at the west edge of the property. All units will have territorial and possible downtown views from roof top decks.

Community Landmarks

Cal Anderson Park, nearly 7.5 acres, recognized by Forbes.com as one of the nation's best parks, is two and half blocks to the east of the site. It includes a fountain, texture pool and reflecting pool, promenade paths, landscaping, a shelterhouse, a plaza, a children's play area, a wading pool, a lighted sports field, and a number of oversize chess boards. This open park invites walking, sitting, reading, contemplation, informal sports in the meadow, and organized sports on the athletic field.



7 Multi Family development at 11th Ave and E Howell St.



10 A multi family building on at 15th Ave and E Pine St.



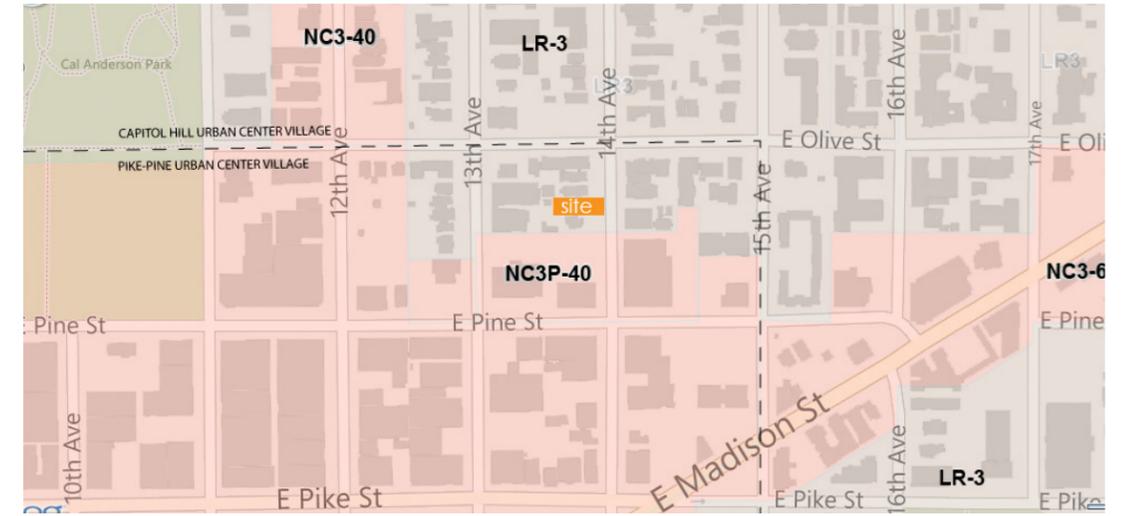
8 Single family houses just south of 14th Ave and E Howell St.



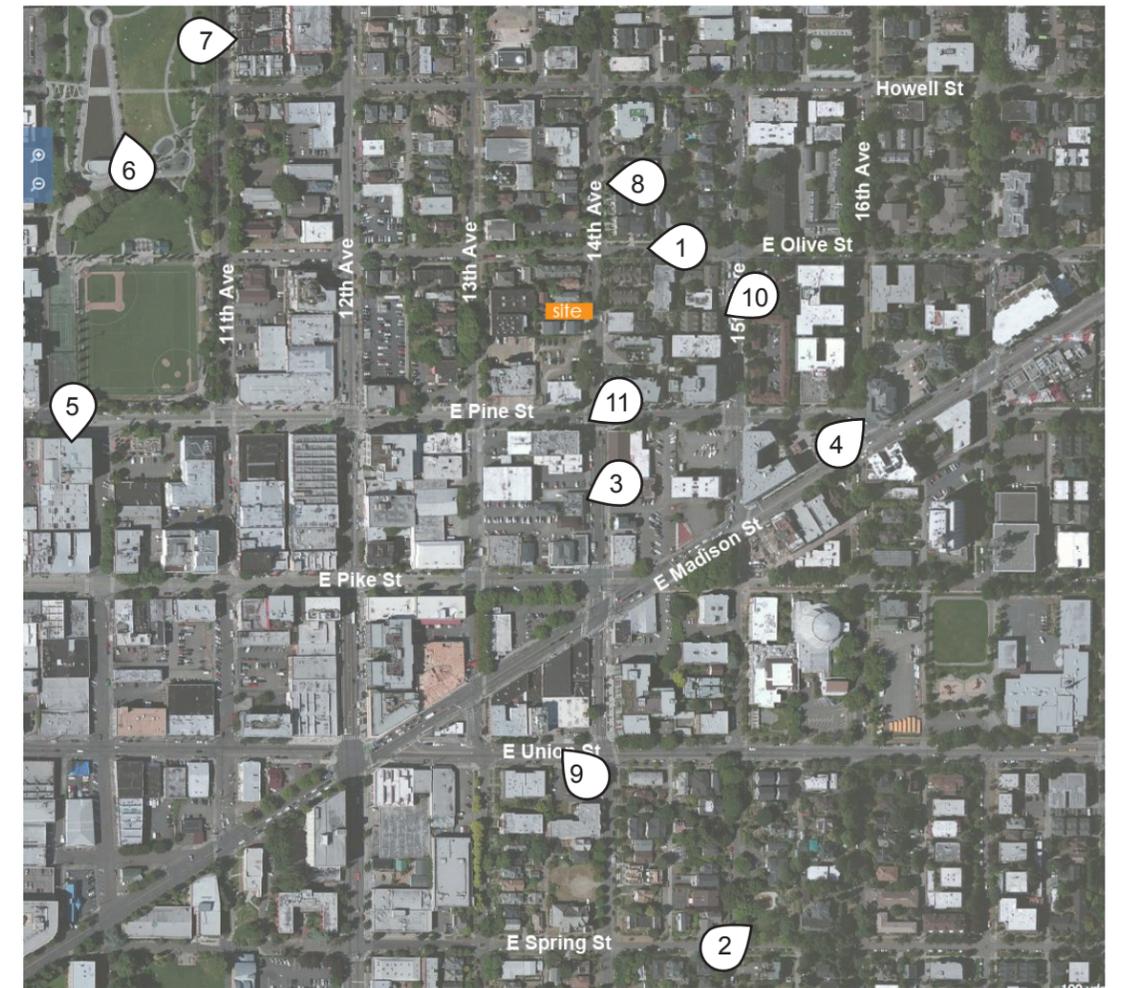
9 A mixed use building at 13th Ave and E Union St.



11 A mixed use building at 14th Ave and E Pine St.



Seattle DPD Zoning Map



4. PROPOSAL

Please describe the applicant's development objectives, indicating types of desired uses, structure height (approx), number of residential units (approx), amount of commercial square footage (approx), and number of parking stalls (approx). Please also include potential requests for departure from development standards.

Development Objectives

We strive to create a compelling community of homes; homes that acknowledge the area and the times, while injecting fresh concepts and energy into an existing neighborhood.

Type of Uses

We are proposing a group of six new attached townhomes. All homes shall have access to an outdoor, shared courtyard. The courtyard provides a communal space at the heart of the project and access via a communal stair to the shared below-grade parking structure. All homes will connect visually to a communal walkway along the north edge of the property with the street-facing unit connecting to 14th Avenue as well. The north facing façade will be the true front of the project with its second front facing 14th Ave. Private roof decks extend the interior living spaces and take advantage the south and southwest views. All but one of the townhomes will have access to daylight on three sides.

Desired Uses

Townhomes

Six townhomes at approximately 1200 square feet each located in the heart of the Pike/Pine neighborhood in Capitol Hill will provide the opportunity for residents to easily access Cal Anderson and Volunteer Parks and Pendleton Miller Playfield. Just one block away is a Route 10, 11 or 12 bus stop providing access to downtown Seattle. The new Capitol Hill Light Rail stop will be located within a 10-minute walk from the site. The proposed design will orient the townhomes primarily south providing excellent access to daylight and territorial views of the park and downtown beyond. Roof decks providing private outdoor space and views will be accessed from within the units. Thoughtful fenestration, double-height spaces, and massing choices express progressive thinking and innovation. Further environmental consciousness will be expressed through material choices, building practices, and site planning. Thoughtful architecture and green building practices will demonstrate that these homes are informed by time and place. Further development of the building's exterior through modulation of the exterior wall combined with decks and variation in materials and window treatment will express individual spaces and reduce the mass of the building.

Structure Height

While the townhomes will use the maximum allowable height, stair penthouses will be limited and oriented to minimize impacts on adjacent sites.

Access and Parking

Given the site's location within the Pike/Pine Urban Center Village and immediate proximity to the Capitol Hill Urban Center Village, no on-site parking is required. The development proposes to provide parking for most of the townhomes in a common below-grade garage structure.



5. Design Guidelines.

A brief description of how the proposal meets the intent of the applicable citywide and neighborhood design guidelines. Identify design guidelines most relevant to the proposal.

A-1. Respond to Site Characteristics:	The building's massing reflects a desire to address all four abutting sides, with specific modulation in the massing to minimize the overall volume along its four edges and in the courtyard at the project's center. The driveway access is paired with the existing driveway of a recently completed townhouse project to the south. This pairs the pedestrian access to all townhomes along the north side of the property abutting the smaller neighboring structure.
A-2. Streetscape Compatibility:	All homes will be accessed from a shared walkway that connects the street to the courtyard along the north property boundary. The street-facing townhouse will directly engage the street, with a visible recessed entrance and entry canopy. Landscaping will be provided that is appropriate in this open space between the townhouse structure and the street including new street trees where required.
A-3 Entrances Visible from the Street:	The new townhouse structure will have entrances that directly relate to the shared walkway to the north with the street-facing unit visible from both the street and the walkway. The massing strategy and entry sequence from the courtyard towards the street will announce the other homes presence, and their entrances, to the street.
A-4 Human Activity:	The new townhouse structure abutting the street will create a modern front porch that connects the home to the street and to the shared walkway. All entrances will be visible from the street and directly connected to the common walkway that abuts north property boundary. This space will be active through the comings and goings of owners and visitors as it provides the primary pedestrian access to each home. In addition, the courtyard between the two proposed structures accesses this walkway as well with a direct connection to the below-grade parking structure. Automobiles will access the site from the driveway along the south side of the property providing a direct connection to the courtyard above.
A-5 Respect for Adjacent Sites:	The topography of the site is essentially flat. The three-story townhouse structure is slightly elevated at the street to create an appropriate separation at the right-of-way. The remainder of the structure is level at grade to reduce the impact on adjacent sites. This corresponds with the location of the below grade parking structure. Given the potential zoning of a 40-foot tall four-story apartment structure, the proposed solution is a more respectful neighbor by creating a longer solution that is limited to 3 stories with a generous courtyard at its center to break up the mass.
A-6 Transition Between Residence and Street:	The proposed structure abutting the street provides a setback consistent with the applicable zoning of 5 feet, with an additional 2 feet to the sidewalk edge. The abutting townhouse project to the south, built in 2006, provides a 5-foot tall fence at the property line and a driveway that connects to the center of their parking court. In contrast we propose to locate a common walkway along the shared boundary with the property to the north that will provide a thoughtful and assuring transition from the street to the homes. This allows for a strong pedestrian connection to all homes on the site and to the common courtyard at the center of the project.
A-7 Residential Open Space:	The amenity space requirement is provided in a private front yard for the street-facing townhouse. Additionally there will be a common courtyard accessible to all homes at the center of the site. Amenity space is also provided on private roof decks for each of the new homes.
A-8 Parking and Vehicular Access:	The development provides a new driveway along its south property boundary that will access a common below-grade parking structure. This structure currently proposes 5 parking spaces for the 6 homes.

B-1. Height, Bulk and Scale Compatibility:

The proposed structure seeks to minimize the potential bulk provided on the development site while providing a dense well-designed townhouse project. Alternative 1 provides a comparison to the height, bulk and scale allowed for an apartment building in the Pike/Pine Urban Center Village. That scheme allows for 10 feet of additional height and additional Floor Area of nearly 3,100 square feet. The preferred alternative provides a similar façade area to the north and south abutting parcels but a full 10 feet shorter. All façades of the structure are modulated to create rhythm and interest to minimize bulk so that the project relates to the scale of adjacent structures. The modulation occurs at all recessed home entries that are covered with a modern front porch or projection of contrasting materials. At the courtyard the project provides a two-story bridge providing a significant reduction in bulk. The stair penthouses are limited to only two homes currently and are pulled toward the center of the structure to minimize impact on abutting sites.

C-1 Architectural Context

The proposed architectural solution provides a modern façade treatment on all sides. The structure provides a more conservative treatment similar to many of the vernacular buildings in the neighborhood contrasted with a modern element that adds scale and rhythm to the overall architecture. Durable materials, primarily painted wood and cement board panel, will be used to provide a consistent design over time. Windows will be provided in punches and larger areas of large glazing in response to interior volumes. Entries are highlighted with a natural wood canopy and wall surface for each home.

C-2 Architectural Concept and Consistency:

The design approach is informed by three primary concepts: create a vernacular style architecture that provides a base "mass", carve out a centralized open courtyard and insert an armature at the center of the courtyard that expresses itself on all façades. The armature highlights entries, adds scale and visual interest and contrasts the vernacular architecture. The project creates an environment for owners to have informal interaction starting with the strong, long east-west entry sequence, which connects the street to the central courtyard and access to the below-grade parking. The homes, each unique, are arranged along this communal walkway and courtyard in a manner that allows connection and privacy simultaneously.

C-4 Exterior Finish Materials:

We will use durable and maintainable materials at the buildings' exterior that also respect the need for sustainability. Following the Pike/Pine supplemental guidance, the siding limits the number of exterior finish materials with the majority of the siding is proposed to be painted ship-lap with areas of contrasting panels. The painted ship-lap siding will reference the industrial vernacular of the neighborhood.

C-5 Structured Parking Entrances:

In order to minimize its appearance at the street the entrance to the structured parking is below grade, 43 feet west of the front property line, connected to the street by a sloping driveway.

D-6 Screening of Dumpsters, Utilities and Service Areas:

Solid waste and recycling storage space will be provided in the below-grade parking structure for each dwelling.

D-7 Pedestrian Safety:

We intend to have all pedestrian access come from 14th Avenue E and from the garage below grade. The communal walkway at the north edge of the site will be well lit with hooded or directed lighting and have a direct view from all the homes. This walkway will be an active space providing access to the entrances of all homes. Glazing will be provided along the walkway to provide a strong visual connection from the inside of the homes. In addition the space to the south of the townhouse structure will be landscaped to provide a buffer to the abutting townhouse structure.

E-1 Landscaping to Reinforce Design Continuity with Adjacent Sites:

For this site, the landscaping approach starts at the street with the access walkway, ending at the courtyard. This shall be edged with landscaping native to the Northwest such as edibles that benefit from low light on the north side of the project. We will utilize the existing landscaping along the south property line where possible to provide continuity to adjacent sites. The courtyard shall be carefully landscaped including special pavements and planters. The existing street tree will anchor a planting strategy in the planting strip as an extension of the front yard of the street facing townhouse. In addition site furniture shall be incorporated into the design of the ground and outdoor decks above grade in the project. The landscaping, including existing and new trees, the plants in the amenity spaces, and the plantings in the courtyard shall enhance and help stitch together the project site with its surroundings.

7. Adjustments and/or Departures. A summary of potential development standard adjustments (or departures). A table comparing code requirements with the proposed design should be included.

Potential Design Departures

Structure Width and Façade Length Limits in LR zones:

The preferred alternative maintains a variety of housing options for the Capitol Hill neighborhood. Given the large number of apartment structures under construction and proposed currently, the decision to create townhomes adds to diversity in this specific area. As demonstrated in Alternative 1, this site can support a structure over 40 percent larger than what we are proposing. At four stories an apartment building would be over 10,200 square feet with a Floor Area Ratio of 2.0. At three stories, our proposal satisfies the Floor Area Ratio requirement of 1.4 for a building of approximately 7,200 square feet. Our proposed solution for this site attempts to manage height, bulk and scale by limiting height to three stories and modulating bulk with a rhythmic contrasting architecture. Refer to **Figures 3-6** on the next page. The actual facade area of the preferred alternative with departure requests is less than the code compliant facade area for the 4-story apartment structure, alternative 1.

This reduction in height results in an increase in proposed façade length beyond the allowable requirements in the code, per SMC 23.45.527.B.1. As such we are requesting a design departure for façade length on both sides of the parcel (north and south). The proposed aggregated façade length is:

South Side:

Within 5 feet of property line:
 $53'-8'' / 128' = 41.9\%$
 Within 10'-8'' of property line:
 $90'-8'' / 128' = 70.8\%$ requires departure (allowable with adjustment through SDR)

North Side:

Within 5 feet of property line:
 $96' / 128' = 75\%$ requires departure
 Within 12'-6'' of property line:
 $117' / 128' = 91\%$ requires departure

Setbacks and Separations:

In order to minimize the height and bulk of the townhouse structure, we are requesting a design departure for our north side and south side setbacks. The required setback for the townhouse structure is 5-foot minimum with 7-foot average. Our proposed north side setback maintains the 5-foot minimum requirement at grade, with a 6'-7'' average, achieved by recessed entries and the courtyard bridge. At the second and third floors, the minimum setback is 3 feet with an average of 6 feet. The projections into the setback are necessary to highlight the individual units along the walkway, add modulation, and supply visual interest along the façade. The proposed south side setback is a minimum of 3 feet with an average of 9'-10''. The projection is an excellent visual terminus above the driveway, minimizing the appearance of the vehicle entry to the below-grade parking structure. Refer to **Figures 1-2** on this page.

North Side Setback:

5-foot minimum at grade
 6'-7 1/4" average at grade (entries recessed + bridge at courtyard included) requires departure

3-foot minimum at grade
 6'-0" average at grade (entries recessed + bridge at courtyard included) **requires departure**

South Side Setback:

3-foot minimum at grade
 9'-9 3/4" average at grade (driveway setback, offset at rear + bridge at courtyard included) **requires departure**

required departures

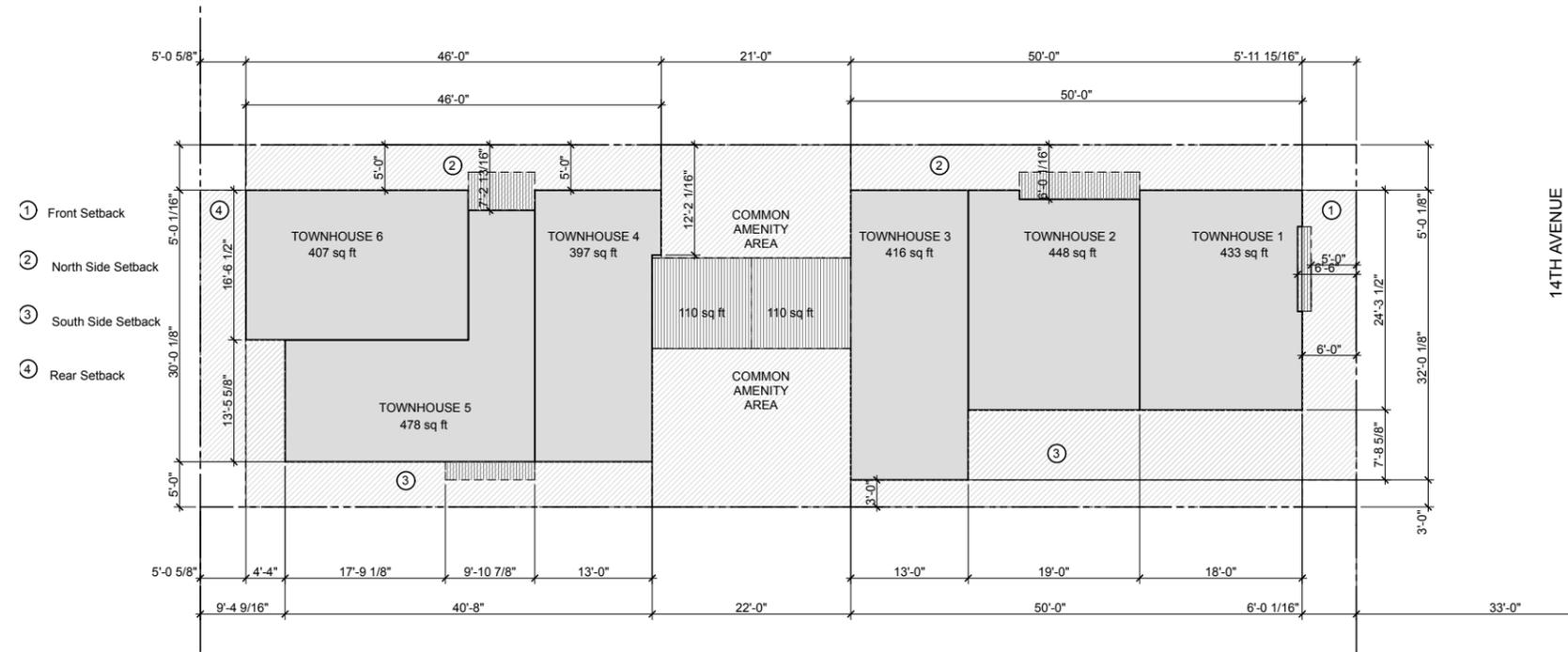


Figure 1: First Floor Plan

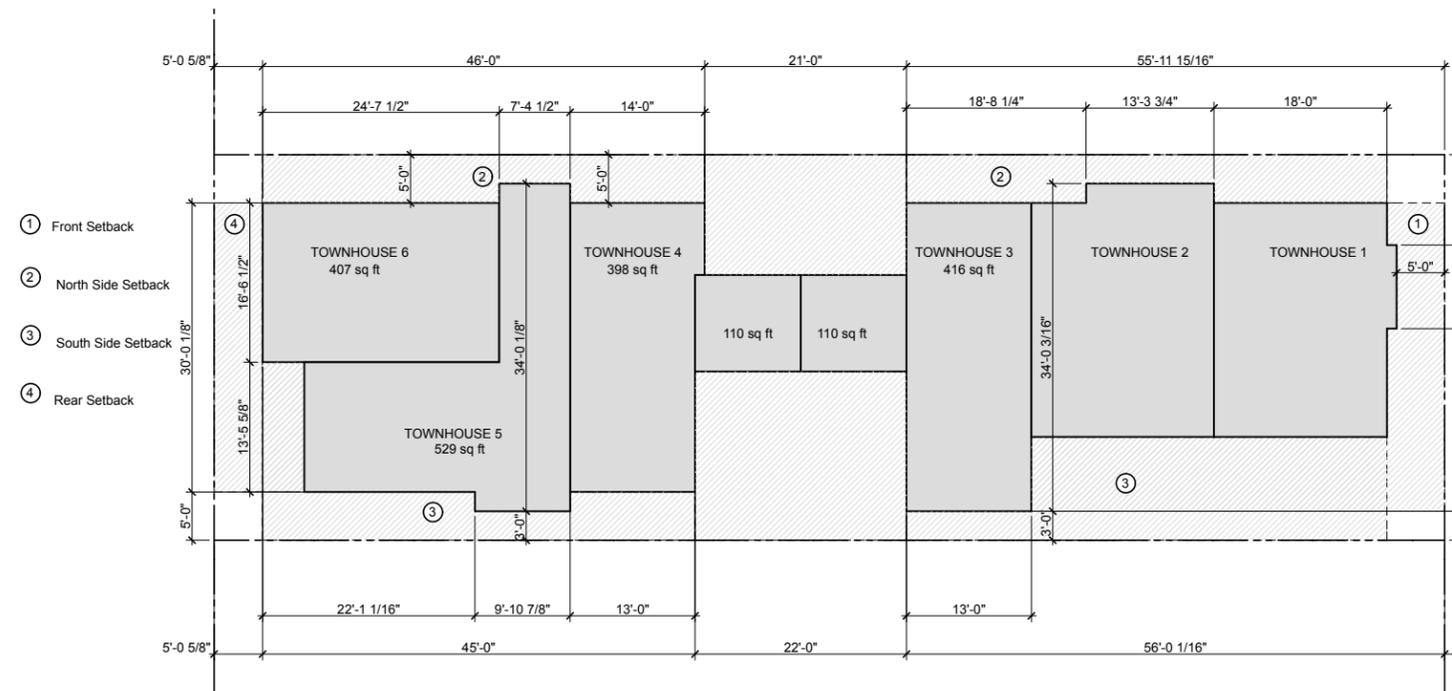


Figure 2: Upper Floor Plan

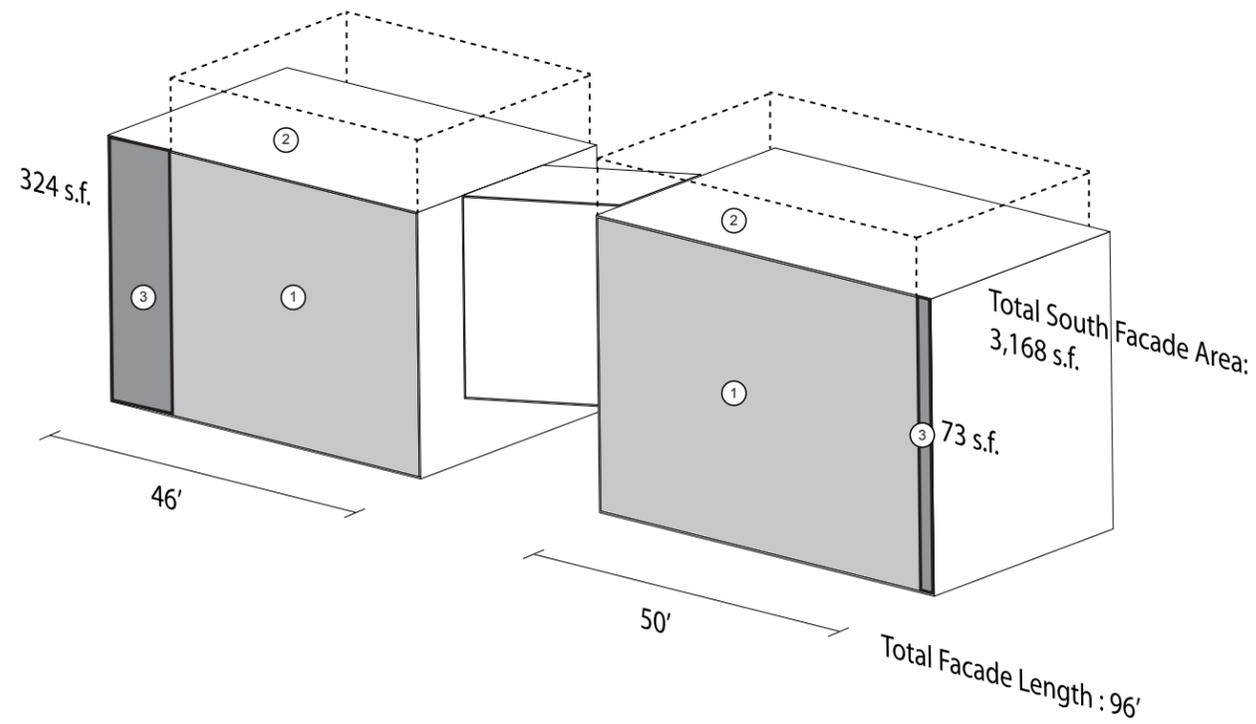


Figure 3: South Facade Area Diagram

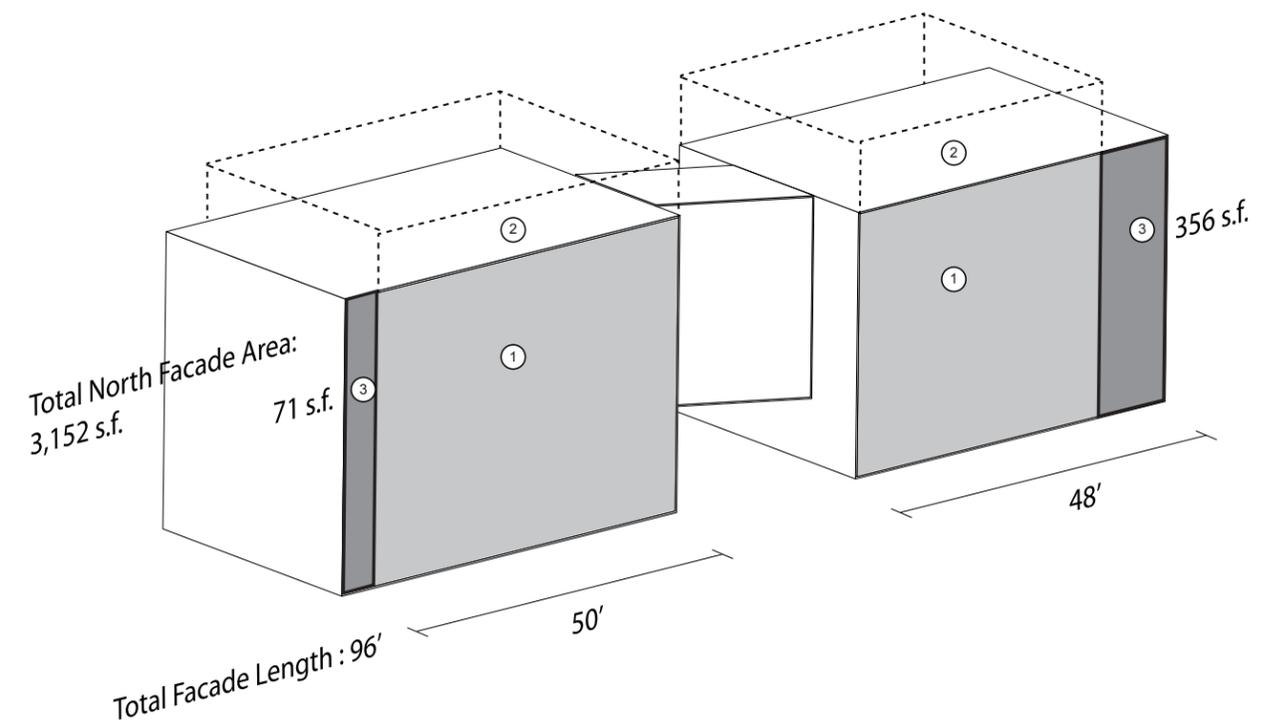


Figure 4: North Facade Area Diagram

- ① 3-STORY FACADE AREA BASED ON CODE COMPLIANT FACADE LENGTH
- ② 4-STORY FACADE AREA (ALLOWABLE IN URBAN CENTER) BASED ON CODE COMPLIANT FACADE LENGTH
- ③ PROPOSED 3-STORY FACADE AREA REQUIRES DEPARTURE REQUEST FOR FACADE LENGTH. ACTUAL AREA IS LESS THAN CODE COMPLIANT 4-STORY FACADE

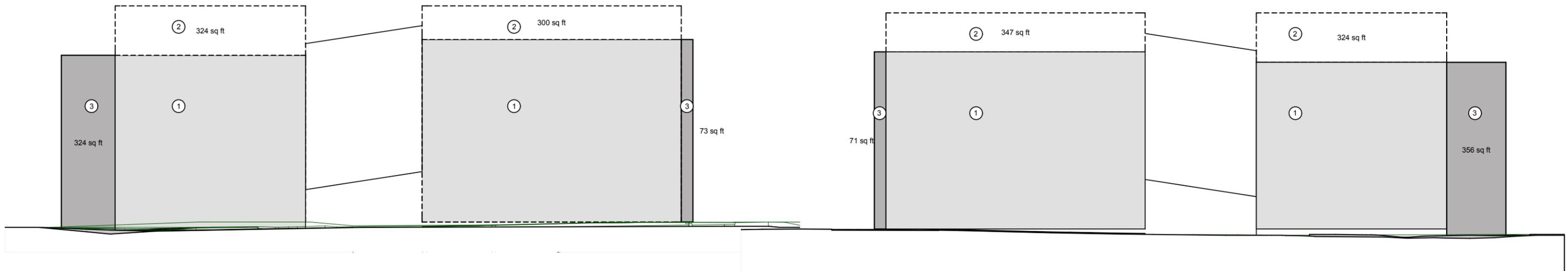


Figure 5: South Elevation View compares proposed facade area with code compliant facade area.

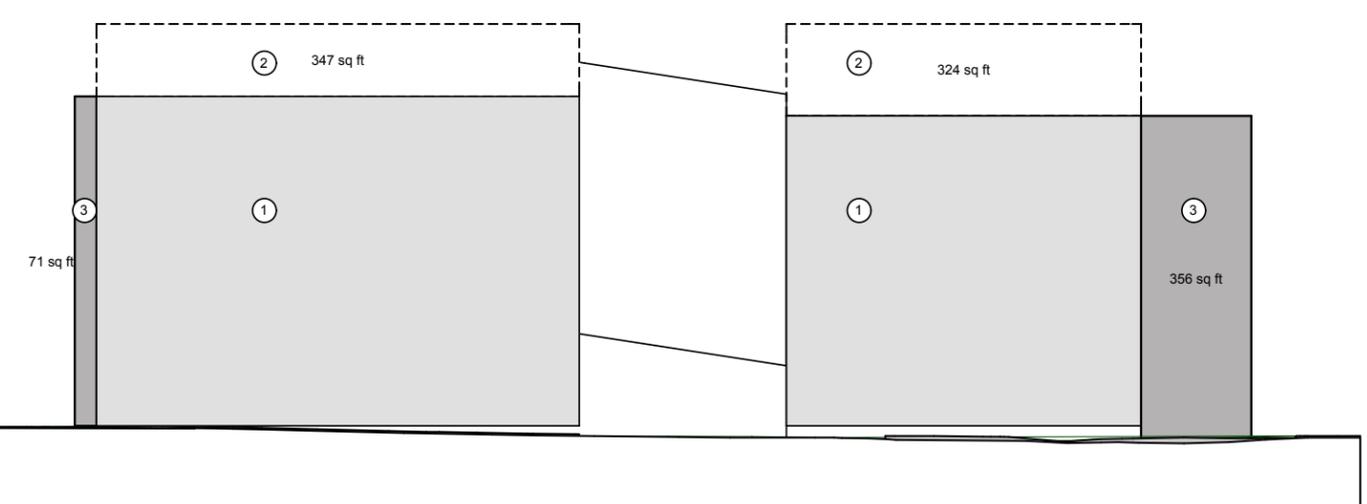
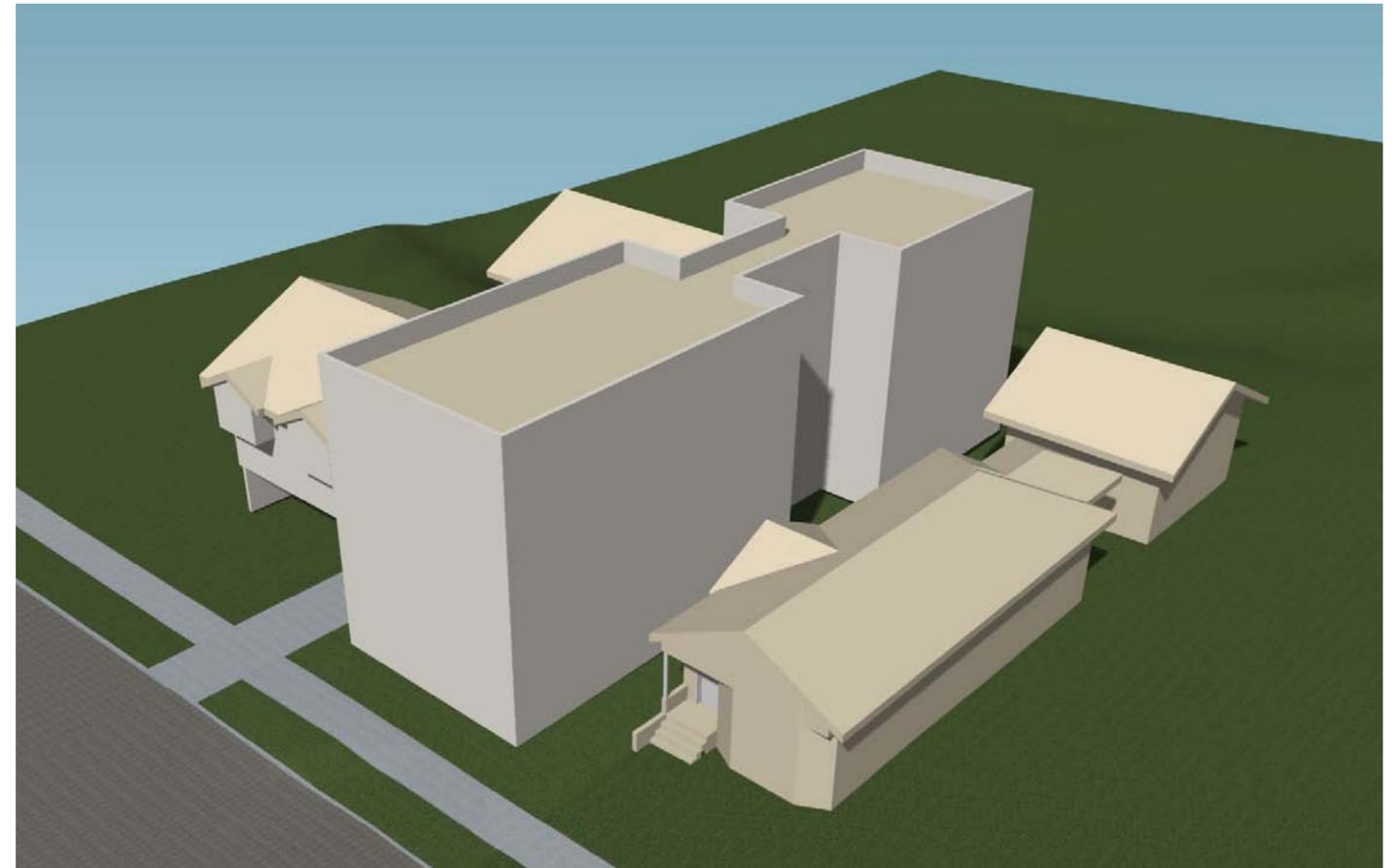
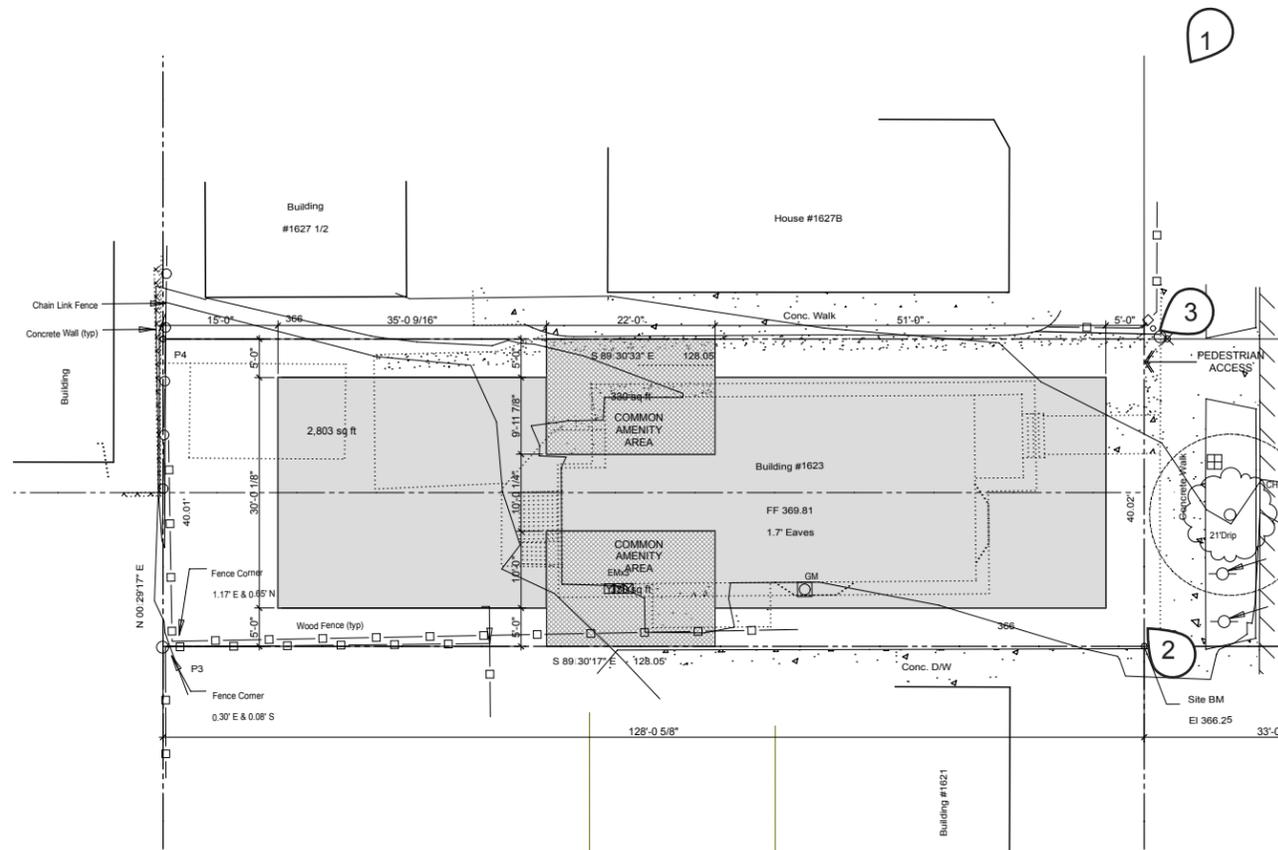


Figure 6: North Elevation View compares proposed facade area with code compliant facade area.



① aerial view from northwest.

Alternative 1:

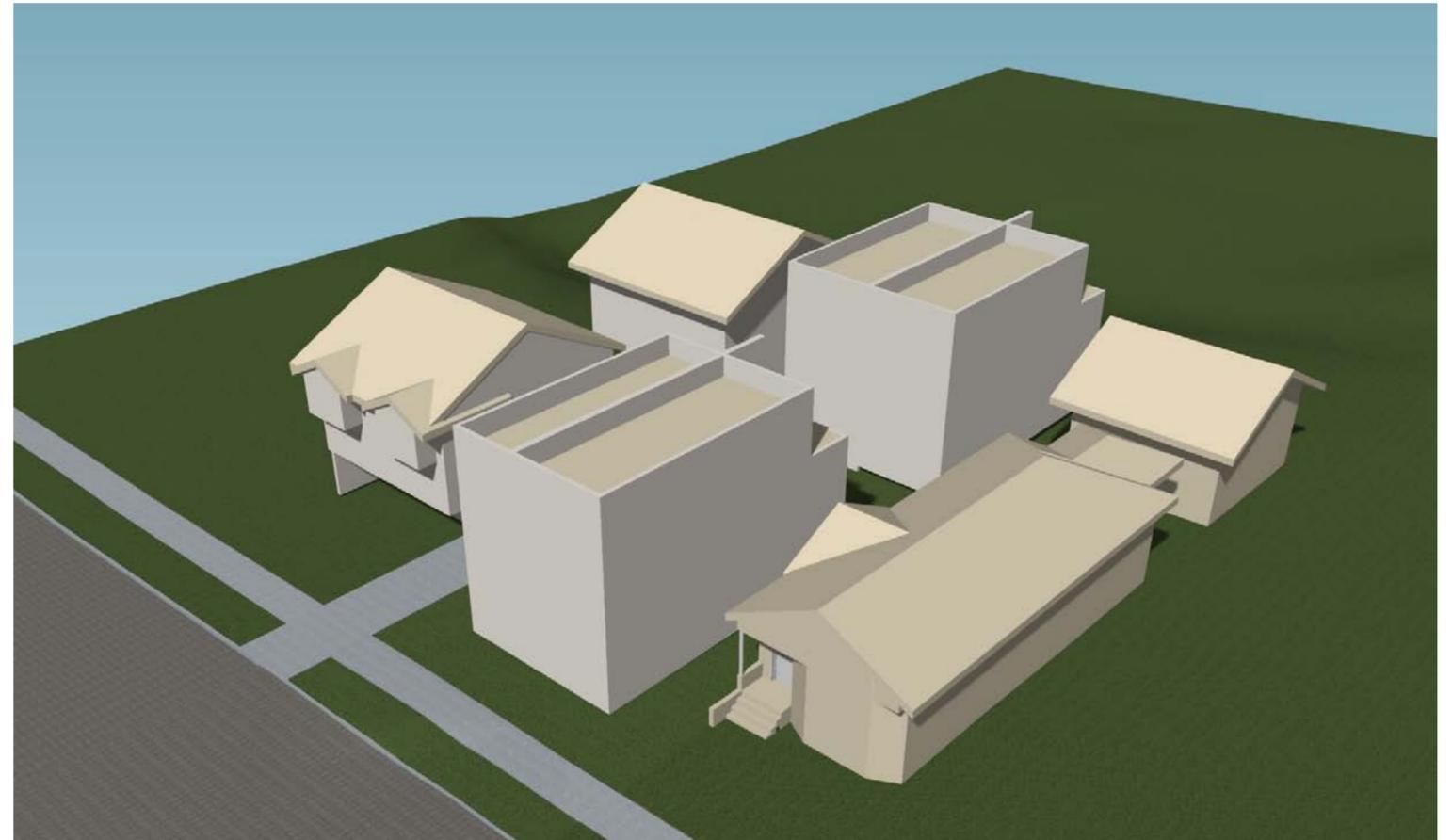
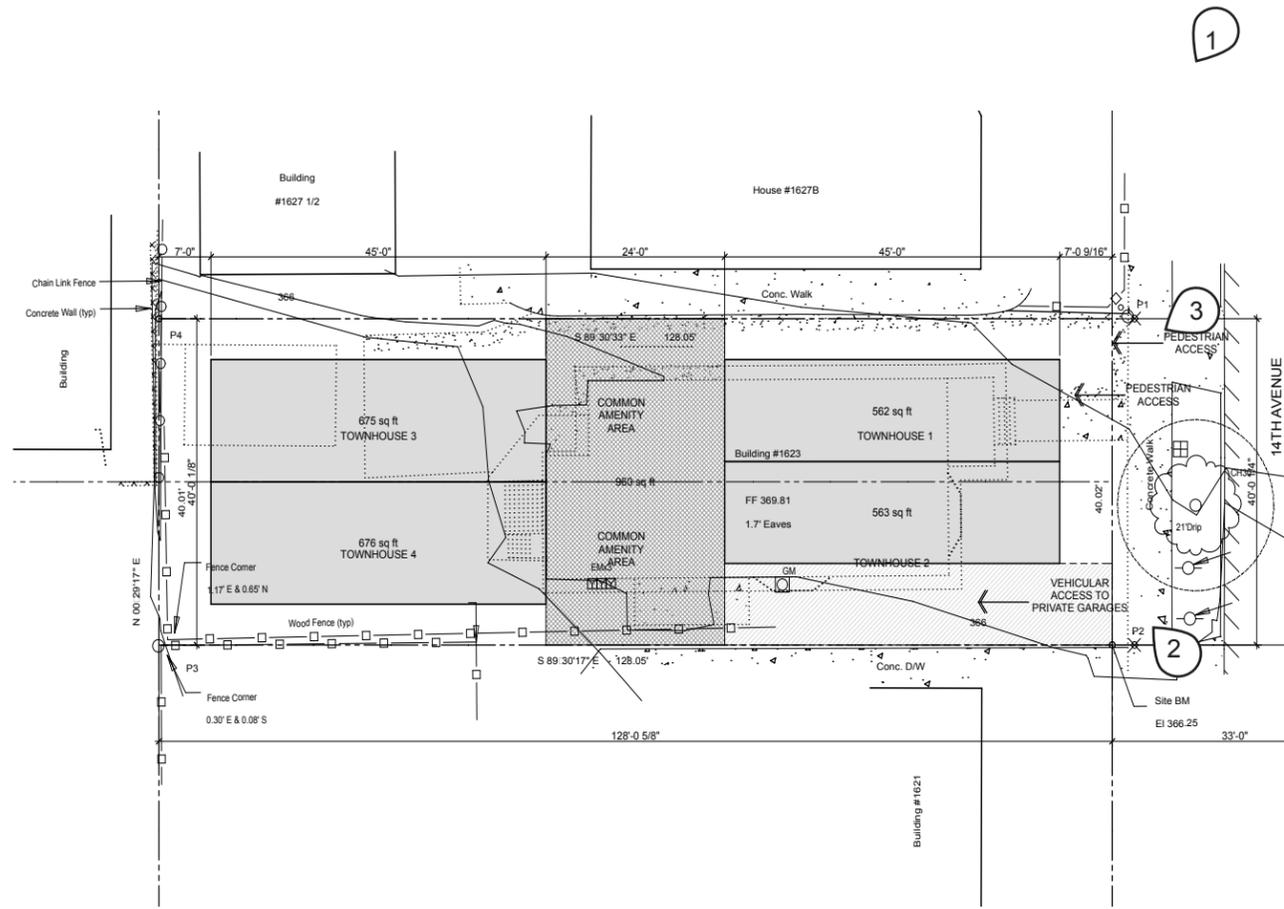
Alternative 1 proposes to construct a four-story apartment development that maximizes allowable FAR for apartment structures while providing no parking. The scheme maximizes allowable facade length and structure height for apartments at 40 feet within the Pike Pine Urban Center Village. The 40-foot height provides the maximum allowable bulk for this zone, contrasting with the immediately adjacent context. By aggregating all the units together, this scheme presents a large wall to the neighboring parcels to the north and south. The existing duplex structure is demolished to make room for this structure. Amenity space is provided at the center of the project in the form of two communal spaces at grade as shown on the site plan and on the roof deck. The scheme requires no departures.



② view from 14th Ave E to the northwest.



③ view from 14th Ave E to the southeast.



① aerial view from the northwest.

Alternative 2

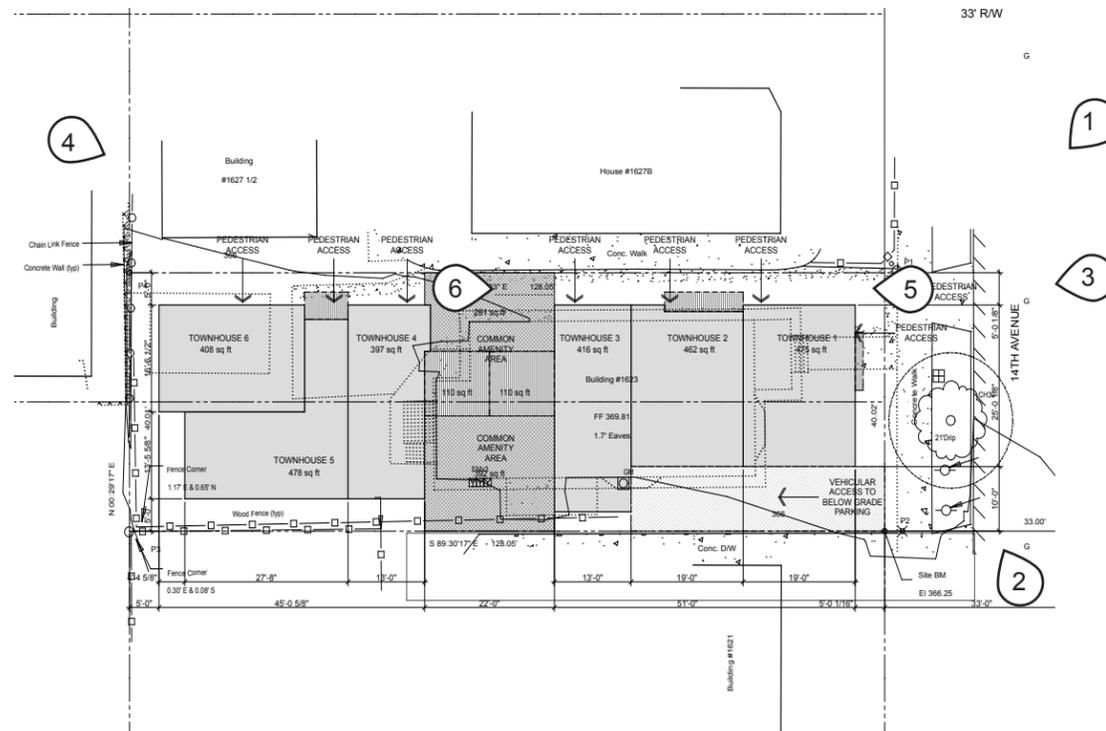
Alternative 2 proposes to construct a three-story townhouse development that maximizes allowable FAR for townhouses while providing private garage parking. This scheme provides roof decks with parapets to the maximum allowable height in the zone for townhouses. Amenity space is provided in the form of small private yards and privately accessed roof decks for each unit. The only communal space is the automobile access, proposed from the southeast corner of the site, abutting the adjacent driveway. This scheme requires a departure for façade length of 71 percent along the north and south sides.



② view from 14th Ave E to the northwest.



③ view from 14th Ave E to the southeast.



1 aerial view from the northeast

Preferred Alternative

The preferred alternative seeks to create a compelling community of homes that acknowledge the area and the times, while injecting fresh concepts and energy into an existing neighborhood. The scheme proposes a three-story townhouse development that provides below-grade parking with access from 14th Ave E. Six houses at approximately 1200 square feet are accessed from a shared walkway at the northern edge of the site. A communal space is provided at the center of the scheme with access to parking to below. The eastern most unit orients to the street while each of the interior units take advantage of their particular situation to take advantage of southern light and potential territorial views to the southwest. Amenity space is provided in a communal centrally located courtyard and privately on each home's roof deck. Departures are requested for facade length, front setback, north side setback and south side setback.



2 view from 14th Ave E. looking northwest



3 view from 14th Ave E. looking southwest



4 view from northwest looking towards 14th Ave E.



⑤ view towards communal entry walkway from 14th Ave E.



⑥ view of the courtyard and communal walkway looking towards 14th Ave E.

preferred alternative - 1903 4-square + 2012 triplex

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Alternative 1

Brief Description

This design includes an apartment structure that fills the allowable zoning envelope, taking advantage of the higher FAR allowable in LR3 in the Pike Pine Urban Center Village. In addition, no parking is provided due to its location in an Urban Village. There is no abutting alley required for improvement.

Advantages

Apartment uses in this zone take advantage of the highest allowable FAR. There is a large communal roof deck on the structure taking advantage of the views to the east and south.

Issues

This scheme provides the largest building solution allowable through FAR. Although there is open space for the tenants at the ground level, the bulk of the open space is at the roof of the structure where it is unlikely to be used. This scheme presents a very large wall to the neighboring parcels to the north and south and sharply contrasts in scale with the surrounding structures.



Alternative 2

Brief Description

This design includes townhouse structures that fill the allowable zoning envelope, taking advantage of the higher FAR allowable in LR3 in the Pike Pine Urban Center Village. Parking is provided in the form of private garages for each unit.

Advantages

Townhouse uses in this zone provide a smaller structure than the apartment proposed in Alternative 1. Each home has a private garage, yard and roof deck.

Issues

This scheme provides an automobile oriented solution with little difference from the too common 4 pack style of townhome. Much of the ground level is taken up by automobile, as opposed to pedestrian, oriented space.



preferred alternative

Brief Description

The Preferred Alternative explores the potential of the new Multifamily code. While maintaining sensitivity to the scale of the existing fabric, the scheme allows for increasing density in a growing neighborhood in the Pike Pine Urban Center Village. While the northern edge will be the true facade of the building, all facades are treated with materials and lighting that are sensitive to the pedestrian realm. Along each facade, two story expressions will bring the structure into a scale that appropriately bridges the immediate context between the dense commercial core to the south and the existing fabric.

Advantages

This scheme provides a townhouse structure that utilizes thoughtful material contrast and modulation that scales appropriately to its immediate surroundings. While each home has access to a private roof deck, communal space at the ground level in the shared walkway and courtyard provide opportunities for interaction and safe passage. The automobile is minimized in a fully below-grade structure with a connection to the courtyard.

Issues

In order to provide the two-story articulations from the building, we are requesting design departures for our front, north and south side setbacks and facade length. These departures add modulation to the structure in order to minimize the height and bulk of the proposed solution.

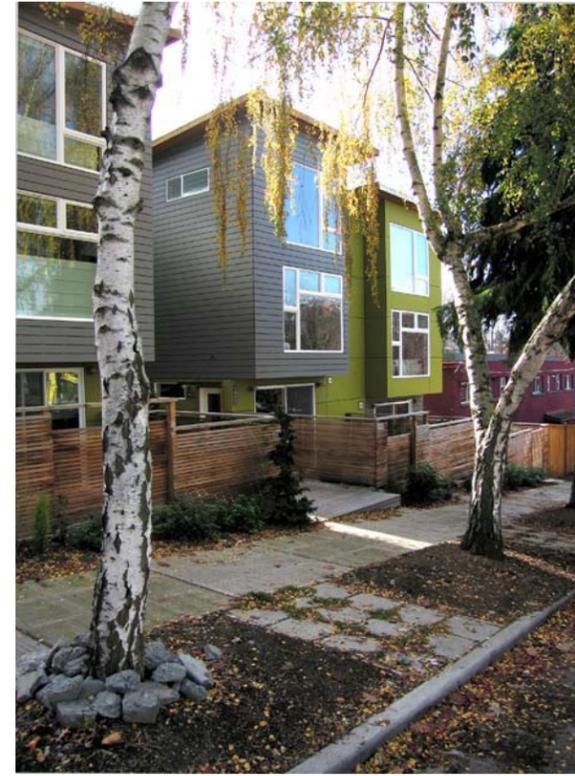




① 208 18th Ave. E. exterior view from street



② 1504 19th Avenue Duplex behind SF House



③ 1411 E. Fir St. exterior view from street



④ 1911 E. Pine St. courtyard view from a deck



⑤ 1911 E Pine St. view at interior of canyon



⑥ 1818 E Yesler Way. view of a woonerf



⑦ 1411 E. Fir St. interior boardwalk view



⑧ 1911 E. Pine St. view from street

completed work examples