

## 42ND AVE S.TOWNHOMES

5207 & 5213 42ND AVENUE S.



## EARLY DESIGN GUIDANCE

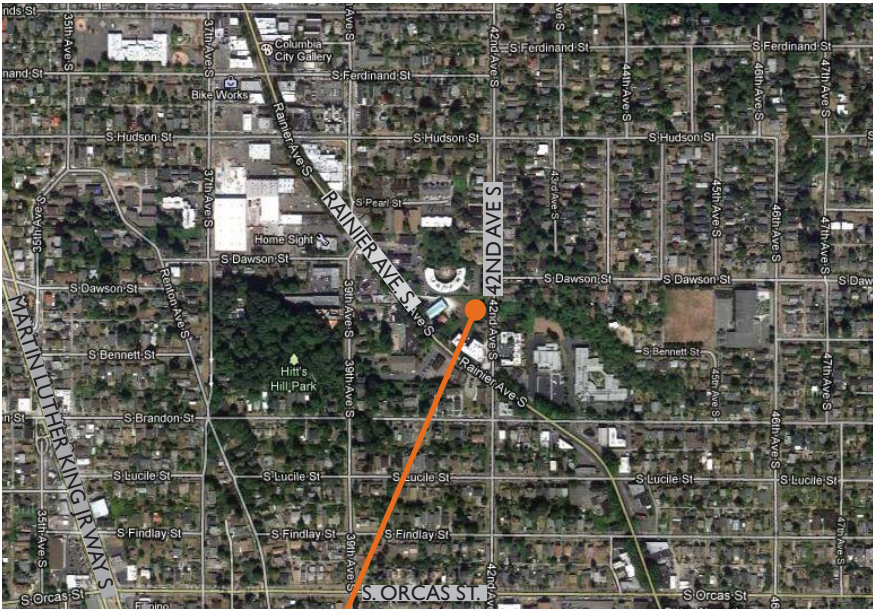
DPD #3013309

310 FIRST AVE S SUITE 45  
SEATTLE, WA 98104  
206.933.1150  
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PROJECT DESCRIPTION



ADDRESS: 5207 & 5213 42nd Avenue S  
DPD PROJECT #: 3013309  
OWNER: Quadrant Homes  
APPLICANT: Nicholson Kovalchick Architects  
CONTACT: Bill Barton

DEVELOPMENT OBJECTIVES

The property owner’s objective is to construct 16 to 17, 3-story townhouses with as many single car private parking stalls per unit as topography and design will allow. The proposed design encourages social interaction, while incorporating sustainable design strategies to preserve resources.

The vast majority of the parking will be accessed from a common drive court that is entered using the alleyway off of Rainier Ave S. Amenity area will be provided in the form of public and private yards.

Departure requests from development standards stem from viewing the site more as a low-rise residential zoning project. Side setback goals at residential zoning abutments associated with the height and depth of typical commercial buildings are met through the reduced scale of the proposed design. Parking requirements associated with the lower density provide higher safety standards to the pedestrian of 42nd Ave.

PROJECT PROGRAM

Number of Residential Units:	16-17
Number of Parking Stalls:	16-17
Area of Residential Uses:	Approximately 25,300 sf
Area of Enclosed Individual Garages:	Approximately 4,000 sf
Total Area:	Approximately 29,300 sf

EXISTING SITE

The site is composed of 2 tax parcels located mid block along 42nd Avenue S, between Rainier Ave to the southwest and 42nd Ave S to the east. The parcel is trapezoidal and contains approximately 18,508 s.f. The site slopes from the highest point on the eastern side to the lowest point on the northwest corner. The site is separated from adjacent commercial properties to the west by an alley right of way that is accessed off Rainier Ave S.

ZONING AND OVERLAY DESIGNATION

The parcel is zoned NC2-40 and is located within the Columbia City Residential Urban Village. The area immediately surrounding the project site is zoned NC2-40 with the exception of an LR2 abutment on the north side property line and SF 5000 zoning located across 42nd Ave S to the east.

Per the DPD’s GIS map, this area is a located within a Frequent Transit Corridor, the Airport Height Overlay and the Southeast Seattle Reinvestment Area.

NEIGHBORING DEVELOPMENT

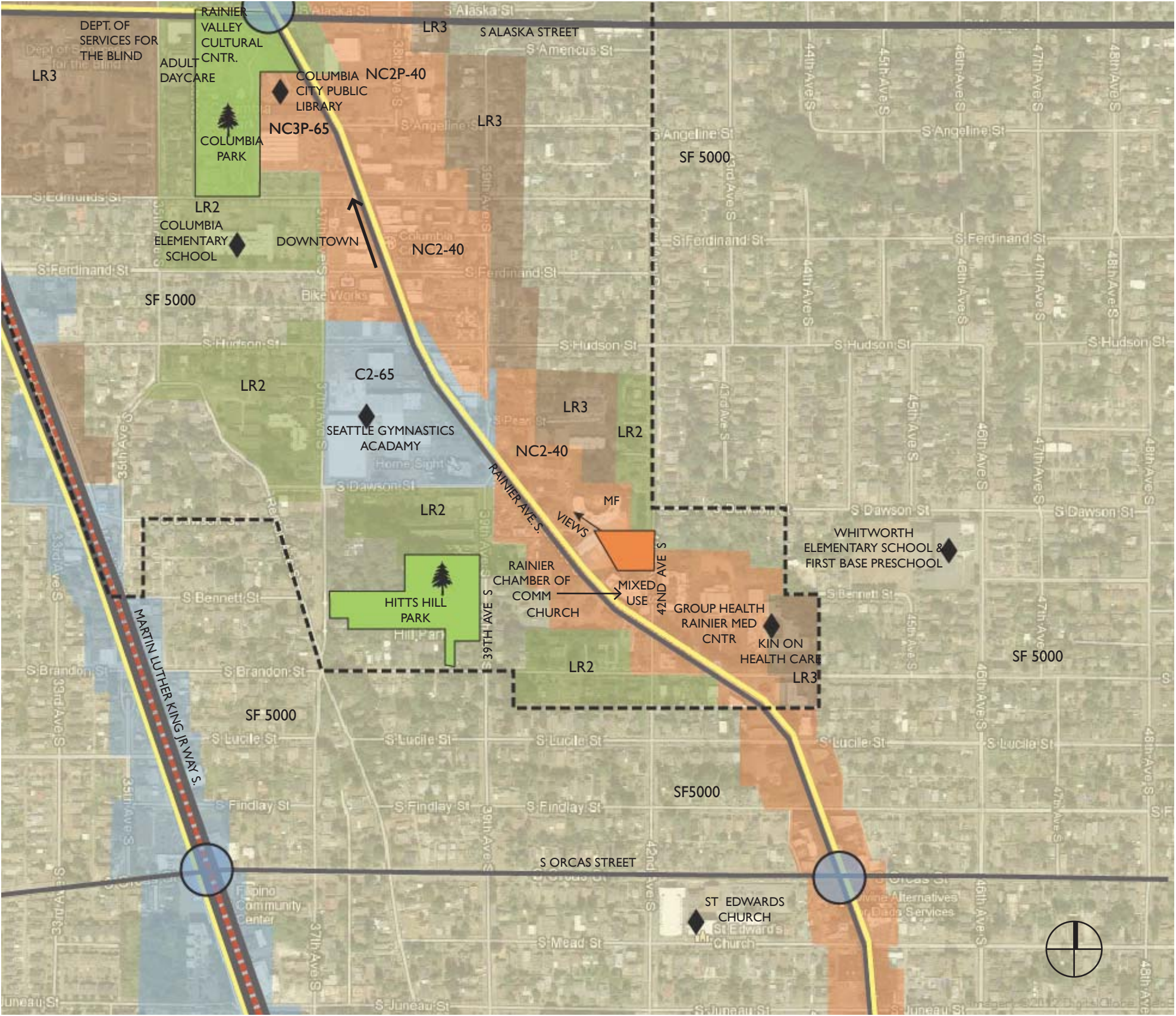
The site is located in Columbia City, which is an incredibly diverse neighborhood with a wide array of building typologies. In the immediate vicinity of the proposed project there are single-family houses, townhouses, mid-size condominiums, several churches, and a medical center. Also noteworthy is the commercial district located along Rainier Avenue S to the northwest of the project and a light rail station located to the southwest.

The site is located in an extremely walk-able neighborhood (having a 92% “Walk Score”) and would be part of an emerging transit-oriented “urban village.” The Columbia City neighborhood is known for its rapid gentrification as well as its historic district status. The area is one of the few parts of Seattle with genuine ethnic and income diversity; some claim that its zip code, 98118, is one of the most diverse in America.

The site is located amongst several residential dwelling structures, both multifamily and single family. Neighboring the site to the north are the Crescent Apartments, across the street to the east, are several single-family structures and an apartment complex. Three blocks up S Dawson St is Whitworth Elementary School, and within similar range are medical centers, parks, shopping and churches.

The project site will be accessed directly off 42nd Ave S and indirectly via an alleyway off Rainier Avenue S. Rainier Ave S and Martin Luther King Jr Way S form major north/south arterials with close proximity to the project and S Orcas St to the south with S Alaska St to the north form strong east/west arterial links to the Columbia City Residential Urban Village. Well within the frequent transit corridor, Rainier Ave hosts multiple bus stops with proximity as near as 0.2 mile from the site that support the use of the light rail transit of Martin Luther King Jr Way, and create a commuter friendly area accessing areas of Seattle and the greater Puget Sound metropolitan area.





OPPORTUNITIES & CONSTRAINTS

Connectivity

The project's proximity to transit and retail provides the opportunity to create a more pedestrian-friendly and transit-oriented design. Rainier Ave S is a primary trail into the heart of both Columbia City and Seattle with multiple access points to light rail. 42nd Ave is quieter in nature and while the site is zoned NC2-40 there is the potential to craft a project where the scope and scale follows the City's goals of reducing automobile use while respecting the residential character of the immediate surroundings.

Topography & Views

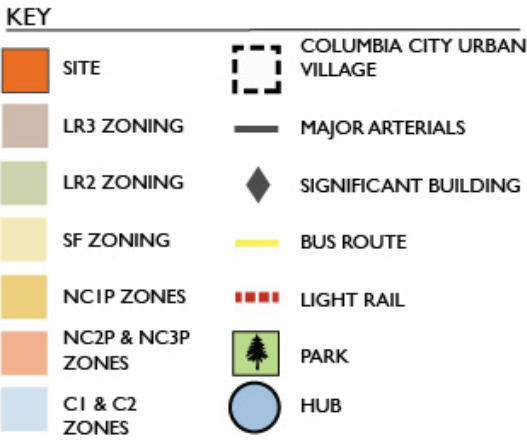
The site slopes nearly 2 1/2 stories from the front property line along 42nd Ave to the rear property line. By locating the units strategically on the site (both laterally and vertically), we propose to maximize day-lighting, views down the Rainier Valley at the Seattle Skyline, and amenity spaces. In the preferred scheme, clusters fronting 42nd allow for the best views and light, while the remaining units step in elevation down the hill and hug the property line to allow for open spaces within the site. The dynamic arrangement and design of the preferred scheme will provide a welcome alternative to the typical rowhouse scheme.

Programmatic requirements that allow for creative site layout and massing.

The Owner's desires to maximize natural light and open space, and provide private garages for individual units. This leads to design options that are not the conventional "shoulder-to-shoulder" square townhouse typology.

Site Dimensions

Both an opportunity and constraint, the relatively deep and sloping site allows for alternative layout strategies to be considered. Instead of a series of narrow, concrete drive aisles running perpendicular to the street, the site's depth allows for the creation of a central drivecourt that serves a dual purpose as motor court plus a multipurpose area for residents. A common alley access with drive court provides access to housing off of Rainier Ave and serves as interior open space for homeowners.





IMMEDIATE SITE CONTEXT



① RAINIER & 42ND AVE  
COMMERCIAL / MULTIFAMILY



② 42ND AVE S APARTMENTS



③ 42ND AVE S SINGLE FAMILY



④ 42ND AVE S SINGLE FAMILY



⑤ 42ND AVE S SINGLE FAMILY



⑥ 42ND AVE S MULTI FAMILY



⑦ 42ND AVE S CRESCENT APTMNTS



⑧ S PEARL STREET TOWNHOUSES



⑨ S PEARL STREET CONDOS



⑩ S PEARL STREET CONDOS



⑪ HITT'S HILL MULTI-FAMILY



⑫ S BRANDON STREET MULTI-FAMILY





① RAINIER VISTA MICRO COMMUNITY



④ 5251 39TH AVE S HITTs HILL MULTIFAMILY



② 3919 S ALASKA STREET SF



⑤ ANGELINE STREET ROWHOUSES



⑦ 39TH & S EDMONDS APARTMENTS



⑨ PEARL STREET CONDOS



③ EDMONDS STREET MULTIFAMILY



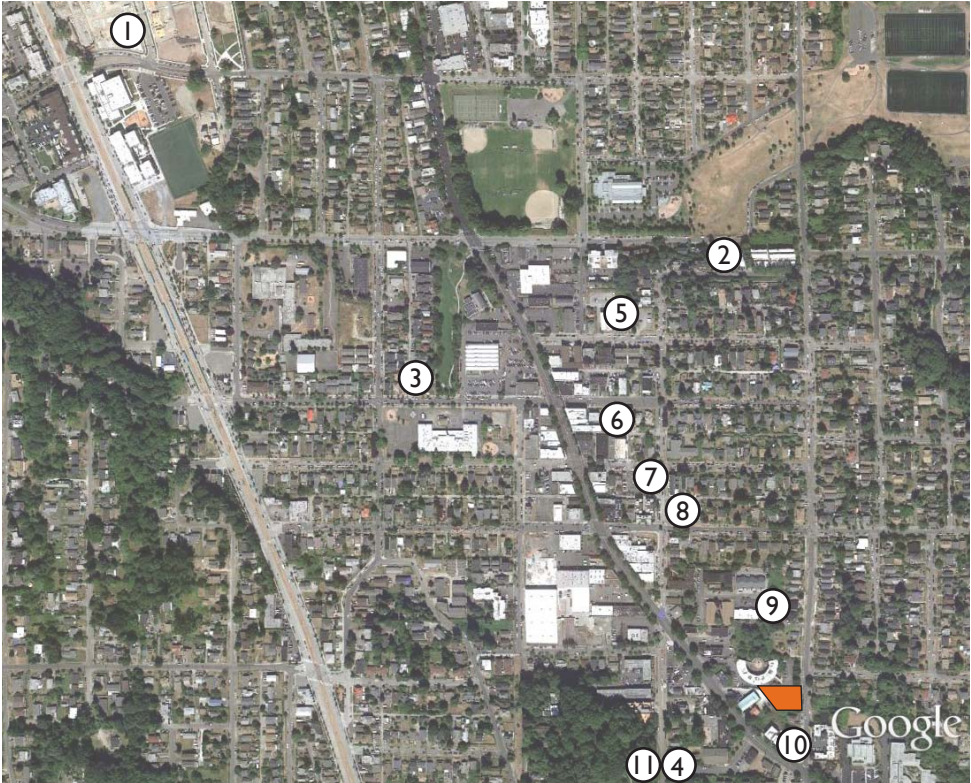
⑥ COLUMBIA CITY LIVEABOVES



⑧ 39TH & S. EDMONDS APARTMENTS



⑩ 42ND AVE S & RAINIER MIXED USE



⑪ 5251 39TH AVE S HITTs HILL MULTIFAMILY

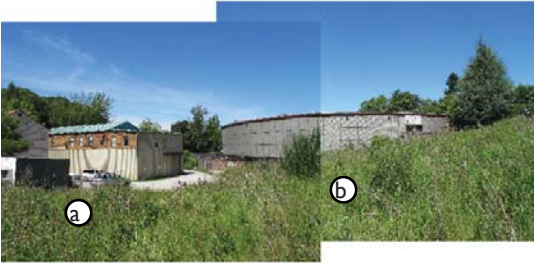
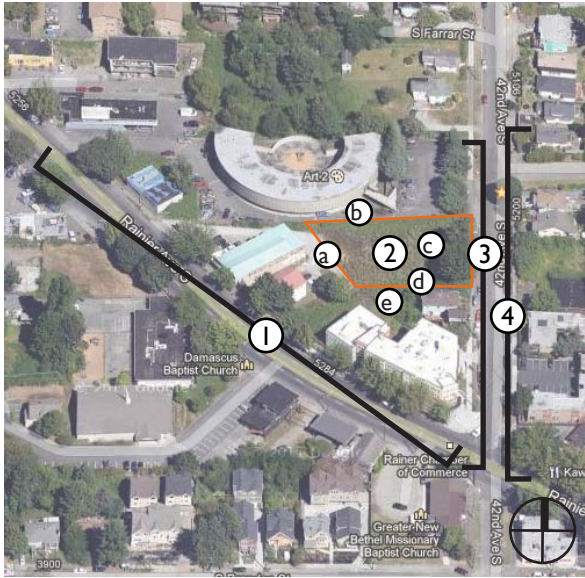




PROJECT SITE

① RAINIER AVENUE LOOKING EAST

STREETSCAPE PHOTO LOCATIONS



PROJECT SITE



② SERIES OF PHOTOS TAKEN FROM MID SITE

PROJECT SITE

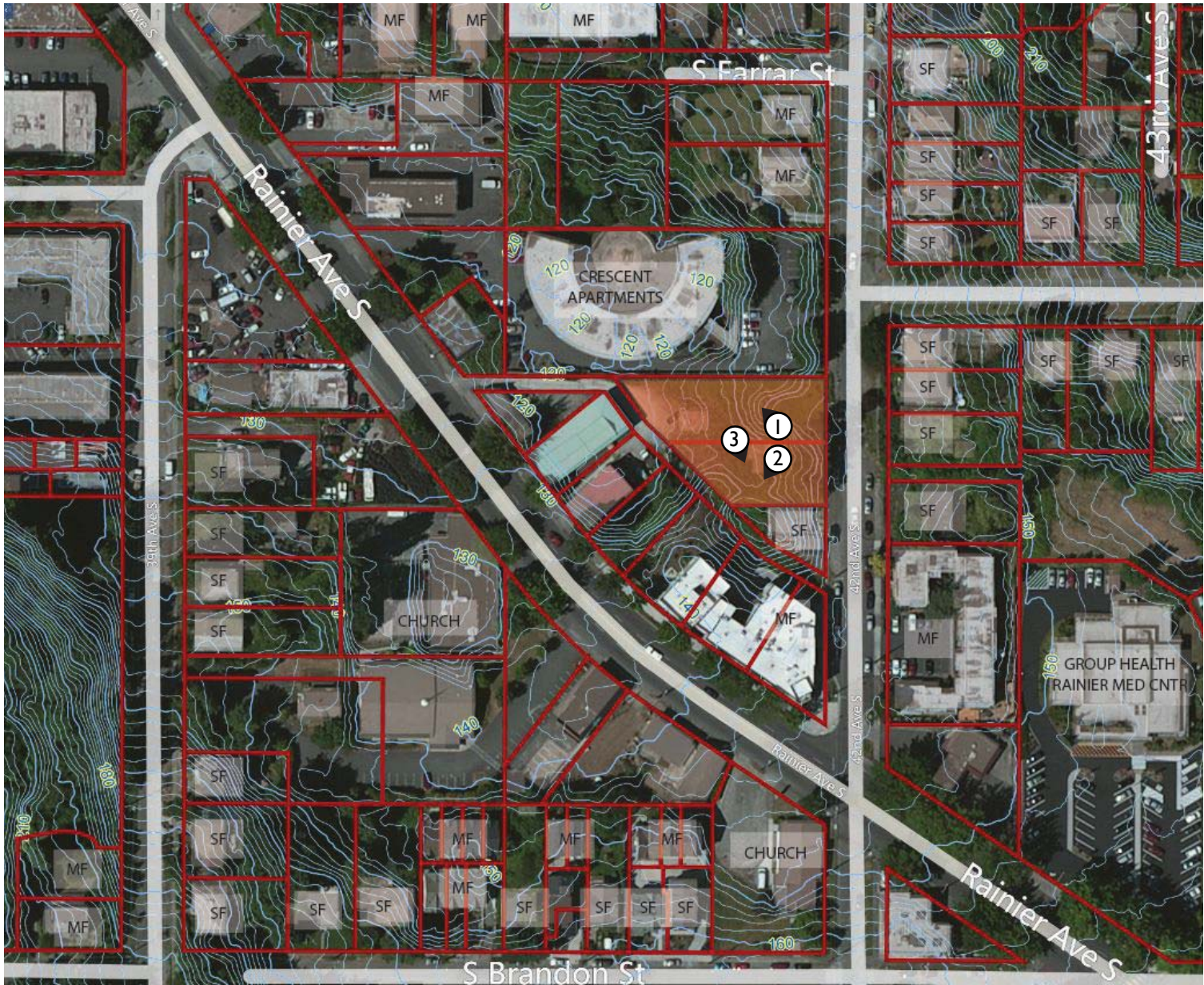


③ 42ND AVENUE S LOOKING WEST



④ 42ND AVENUE S LOOKING EAST





RELEVANT DESIGN GUIDELINE PRIORITIES

- A-1** Responding to Site Characteristics: The siting of buildings should respond to specific site conditions and opportunities.
- Buildings step up toward the east, following the topography allowing greater sun-light penetration throughout the project and views looking down the Rainier Valley toward the Seattle skyline for upper lots.
- A-2** Streetscape Compatibility: The siting of buildings should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.
- The immediate context is predominantly single family but approx. 40% of the adjacent properties are mixed-use and apartments. The proposed project reinforces the single family scale and provides a transition between low and high density developments.
- A-7** Residential Open Space: Residential projects should be sited to maximize opportunities for creating usable, attractive, well-integrated open space.
- The orientation of units in the preferred schemes around a central drive court and courtyard maximizes usable, attractive, well-integrated common open spaces throughout the project.
- A-8** Parking and Vehicle Access: Siting should minimize the impact of automobile parking and driveways on the pedestrian environment, adjacent properties and pedestrian safety.
- The preferred scheme provides the majority of the desired parking access from the alleyway minimizing the impact on 42<sup>nd</sup> Ave pedestrians. Individual garages are then accessed through a common drive court. Parking at the street-level is minimized and of similar nature to SF5000 located to the east across 42<sup>nd</sup> Ave.
- B-1** Height, Bulk, and Scale Compatibility: Projects should be compatible with the scale of development anticipated by the applicable Land Use Policies for the surrounding area. Projects on zone edges should be developed in a manner that creates a step in perceived height, bulk and scale.
- The project, especially in the preferred scheme demonstrates a step in perceived height, bulk and scale between a previous mixed use project on 42<sup>nd</sup> Ave S and Rainier Ave and the residential units across 42<sup>nd</sup> Ave S to the east.
- C-4** Exterior Finish Materials: Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.
- The building materials will be durable and attractive, enhancing the neighborhood while remaining in context with the surrounding structures.
- D-7** Personal Safety and Security: Project design should consider opportunities for enhancing personal safety and security in the environment under review.
- Orientation of units, in the preferred schemes, around central drive court provides 'eyes on the street' which promotes safety and security as well as active pedestrian-oriented open space.
- D-12** Residential Entries and Transitions: For residential projects in commercial zones, the space between the residential entry and the sidewalk should provide security and privacy for residents and a visually interesting street front for pedestrians.
- The preferred scheme provides attractive visually interesting Rowhouse complexes with unit entry doors spaced securely off the street, and pedestrian access to interior units via a beautifully lit stepped path.
- E-2** Landscaping to Enhance the Building and/or Site:
- Orientation of units, in the preferred schemes, around central drive court provides ample opportunities for lush landscaping and special features to enhance the site and promote human interaction.



① VIEW OF RAINIER VALLEY TO CITY CENTER



② VIEW OF SITE LOOKING TO THE SOUTHWEST



③ VIEW OF SITE LOOKING TO THE SOUTHEAST



ZONING ANALYSIS

PARCEL #: 1480400006, 1480400020  
ZONING: NC2-40  
OVERLAYS: FREQUENT TRANSIT, SEATTLE REINVESTMENT AREA, COLUMBIA CITY RESIDENTIAL URBAN VILLAGE  
LOT AREA: 18,263 SF



CHAPTER 23.47A COMMERCIAL

23.47A.005 PERMITTED USES

Per Table A for 23.47A.004:  
F.) Livework units and J.) Residential Uses in NC2 are permitted

23.47A.005 STREET-LEVEL USES

- C.1. Residential uses are permitted anywhere in a structure
- C.4. Residential use may occupy 100% of street level street facing façade in a structure if the structure
  - c. is not located in pedestrian designated zone or a zone w/ ht limit of 85’ or higher and
  - d. does not face a designated principal pedestrian street.

23.47A.008 STREET-LEVEL DEVELOPMENT STANDARDS

- A.2.a. and c. Blank segment of the street facing façade between 2’ and 8’ above sidewalk may not exceed 20’ in width and the total of blank façade not to exceed 40% width.
- A.3. Street level street facing facades shall be located w/i 10’ of street lot lines unless wider sidewalk, plaza, or other landscape are provided.

23.47A.012 STRUCTURE HEIGHT

Mapped height limit = 40’  
May exceed limit up to 4’ if residential use at street level is at least 4’ above sidewalk grade and the additional ht would not allow an additional story that could not otherwise be added.  
A pitched roof may extend up to 5’ above height limit where sections of the roof above the max height have a roof pitch of not less than 4:12

23.47A.013 FLOOR AREA RATIO

Per Table A for 23.47A.013: Maximum Floor Area Ratio (FAR)  
I. Total permitted for a single-purpose structure in height map category 40’ = 3.0

23.47A.014 SETBACK REQUIREMENTS

- B. Setback requirements for lots abutting or across the alley from residential zones. (Specific to the abutting zone LR2 on property’s north side lot line):
  - I. Triangular setback at NE corner of property per Exhibit A for 23.47A.014 where each leg of the triangle is 15’ long and extending along the side and front property lines and the setback is formed by the long side of the triangle.
  - 3. Side setback where structure contains residential use is 15’ for portions of structure above 13’ in height to a max of 40’
  - 5. No entrance, window, or other opening is permitted closer than 5’ to an abutting residentially zoned lot.
  - 8. No dumpsters or trash receptacles located outside of structure permitted w/i 10’ of lot line that abuts a residential zone and must be screened (23.47A.016)

23.47A.016 LANDSCAPING AND SCREENING STANDARDS

- A.2. Landscaping to achieve a Green Factor score of .30 or greater per 23.86.019
- B.1. Street trees required and existing street trees shall be retained unless the Director of Transportation approves their removal.

Table D Screening:  
c. Garbage cans to have min. 3’ high screening along areas where garbage cans are located.  
d. Garbage dumpsters to have min. 6’ high screening.

23.47A.024 AMENITY AREAS

- A. Amenity areas required in amount equal to 5% of the total gross floor area in residential use (does not include garage or mechanical areas)
- B.1. All residents shall have access to at least one common or private amenity area.
- B.3. Woonerf may provide a maximum of 50% of the amenity area

23.47A.030 REQUIRED PARKING

Per Table B for 23.54.015: Parking for Residential Uses: Section II. Residential Use Requirements with Location Criteria:  
M. Residential use in commercial and MF zones within urban villages that are not w/i urban center or the Station Area Overlay District, if the residential use is located w/i 1,320’ of a street with frequent transit service— no minimum requirement.  
Bicycle long-term parking: 1 per 4 units

23.54.030 PARKING SPACE STANDARDS

F. Curb cuts. The number of permitted curb cuts is determined by whether the parking served by the curb cut is for residential or nonresidential use, and by the zone in which the use is located. I. Residential Use; a. number of curb cuts; 1) lots not located on a principal arterial; Per Table A for 23.54.030 Where street frontage is >80’ and up to 160’ (2) curb cuts permitted.

23.47A.032 PARKING LOCATION AND ACCESS

- A.1. Access to Parking in NC zones:
  - a. Shall be from the alley if the lot abuts an alley improved to the standards of 23.53.030.c
  - d. For each permitted curb cut, street-facing facades may contain one garage door, not to exceed the maximum width allowed for curb cuts.
- B.1. Location of parking in NC zones:
  - a. Parking shall not be located between a structure and a street lot line.

DEPARTURE REQUEST SUMMARY:

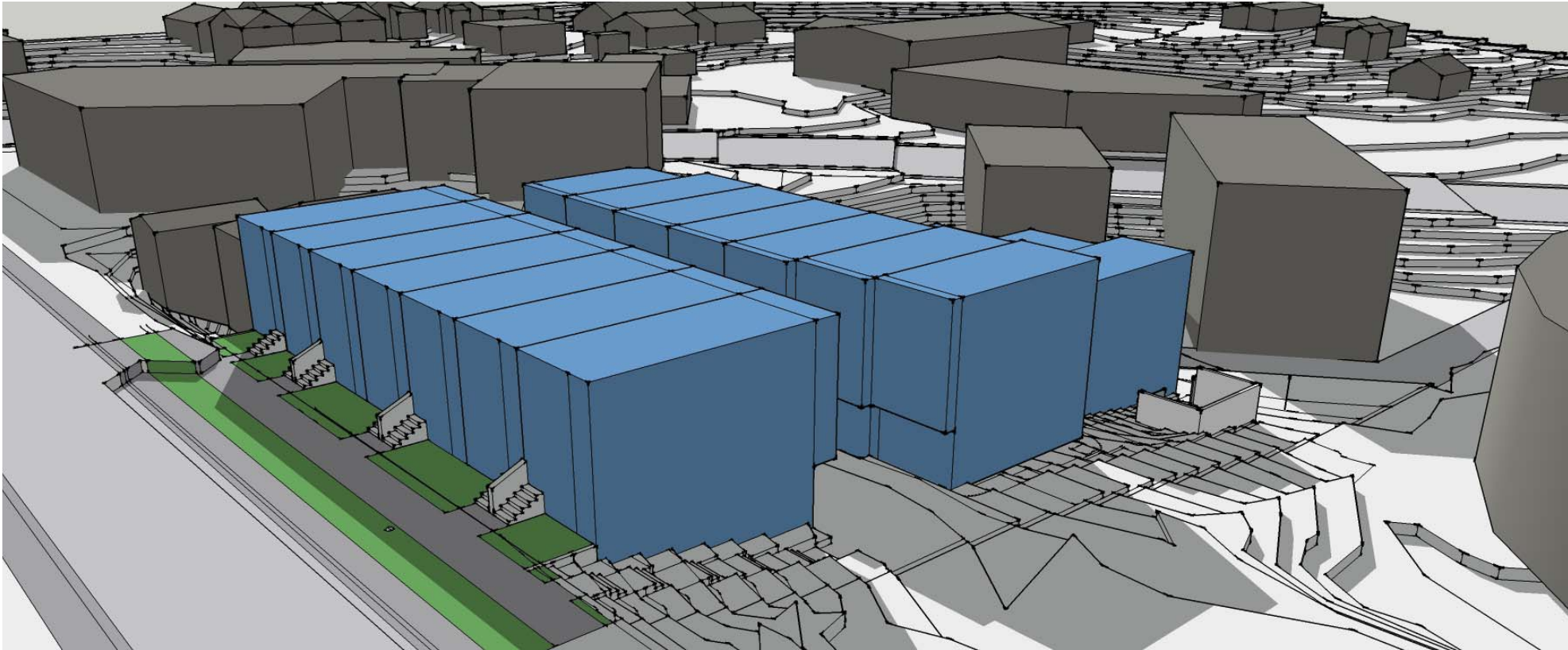
REQUESTED DEPARTURE	OPTION 1	OPTION 2	OPTION 3
#1 TRIANGULAR & SIDE-YARD SETBACKS		X	X
#2 SIDEYARD DECKS		X	X
#3 CURB CUTS & GARAGE DOORS		X	
#4 4’ ELEVATION ABOVE STREET		X	X



NC2 ZONING CODE	REQUIREMENT	PROPOSED	DEPARTURE AMOUNT	REASON FOR DEPARTURE	DESIGN REVIEW GUIDELINES
#1 SETBACKS: NC TRIANGLE & SIDE YARD (SMC 23.47A.014)	Where a lot abuts a residential zone, a triangular area setback of 15' from side and front lot line is required.  For a residential structure, a setback is required where it abuts a residential zone. 15' for portions higher than 13' and up to 40'.	OPTIONS 2 + 3: No triangular setback  5' side yard setback at grade and above.	OPTIONS 2 + 3:  0' at grade and 10' for portions above 13'	Our interpretation is that these code requirements are intended to prevent zero lot line commercially zoned structures from "crowding" adjacent, residential structures.  The adjacent, LR2 site contains a portion of a large, crecent-shaped apartment complex set approximately 60' back from the street with a parking lot inbetween. A large setback imposed on the proposed townhomes will not serve the any intent of the code requirement and detract from the goals of the Design Review Guidelines.  Furthermore, the zoning and development pattern of the surrounding area, coupled with the current use of the site make future development of detached, single family homes highly unlikely.	A-1 Site Character A-5 Respect for adjacent sites B-1 Hght, Bulk, Scale
#2 SIDEYARD DECKS (SMC 23.47A.014)	Decks that are accessory to residential uses and are no more than eighteen (18) inches above existing or finished grade, whichever is lower, are permitted within five (5) feet of a lot in a residential zone.	OPTIONS 2 + 3: No required setback		Ideally, the northern side of the site will be used for private amenity areas and this departure request is intended to help create the best possible spaces for residents. One challenge is that the north side of the site slopes significantly from east to west and is bordered by a 6' wooden fence. This makes the creation of grassy terraces prohibitive (which would be difficult to maintain, regardless) and the shade cast by the proposed units will create areas unable to support grass. Altogether, we are concerned that these conditions will create unusable 'dead zones' on the north side of the property.  We propose a solution to install decks up to the lot line, which would create usable space over the sloping grade and be much more attractive than the dirt or patchy grass that you commonly see along the north side of houses, but the slope requires the eastern edge of a deck to be more than 18" above grade in places.	A-7 Open Space
#3 PARKING LOCATION & ACCESS (SMC 23.47A.032)	Access to parking shall be from the alley if the lot abuts an improved alley.  Per Table A for 23.54.030:Where street frontage is >80' and up to 160' (2) curb cuts permitted.  For each permitted curb cut, street facing facades may contain one garage door.	OPTION 2: Parking access via both alley and 42nd Ave. (2) garage doors per curb cut	OPTION 2: (+2) curb cuts (+2) garage doors	The intent of the neighborhood commercial parking access language is to reduce external site traffic to a limited zone for pedestrian safety. Additionally the topography is such that the improvements of the alley right of way have been waved for connection to 42nd and those lots that front 42nd Ave S are much better served via direct access from the street. This will also result in less regrading.  In our option 2 scheme we have provided two, safe, highly visible access routes to four, single car garages, similar to exhibits in A-8 of the Design Review Guidelines, while maintaining a smaller setback to improve street character.  The proposed garages are for single cars and in elevations, the adjacent garages are architecturally articulated and differentiated rather than a blank, monolithic garage door the code is intended to prevent.	A-1 Site Character A-5 Respect for adjacent sites A-8 Access
#4 SETBACKS: RESIDENTIAL AT STREET LEVEL- STREET FACING (SMC 23.47A.008)	Floor of dwelling unit to be at least 4' above or below sidewalk grade or be setback 10' min from the sidewalk	OPTIONS 2 + 3: Floor elevations allowed to be less than 4' above the sidewalk without the 10' setback.	OPTIONS 2+3: 4' or less	This provision is designed to create a separation of a residence from a busy, commercial street. 42nd Ave is primarily a low-traffic residential street, so no additional separation is necessary.  Both options contain a setback from the sidewalk for privacy/security as well as first floor elevations above the sidewalk. Because of the grading challenges on the site, a high first floor level creates very high basement walls at the rear of the units, which are both cost prohibitive to build and increase "canyon-ing" in the rear access drive.  We propose to build well-designed units that follow best practices and the intent of the Design Guidelines with lower floor elevations.	A-2 Streetscape A-6 Transition to Street



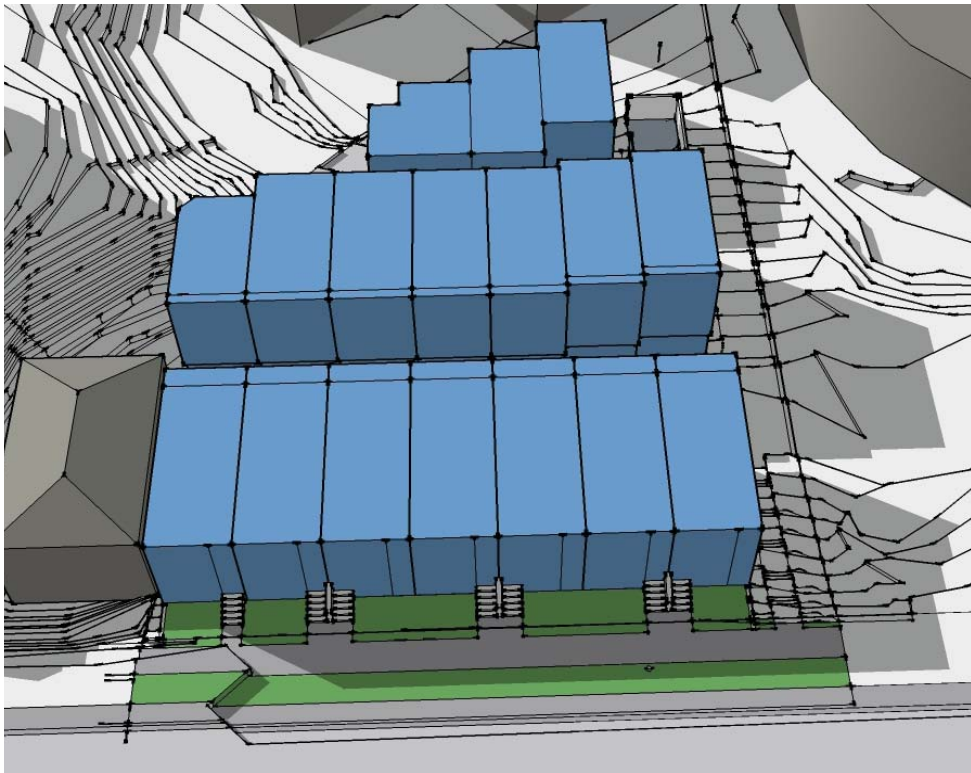
OPTION I - CODE COMPLIANT



NORTHEAST AERIAL VIEW



SOUTHEAST AERIAL VIEW



AERIAL VIEW

DISTINGUISHING FEATURES

- 16 Townhouse and “Rowhouse” Units
- 16 private parking garages all accessed from the alley

PROS

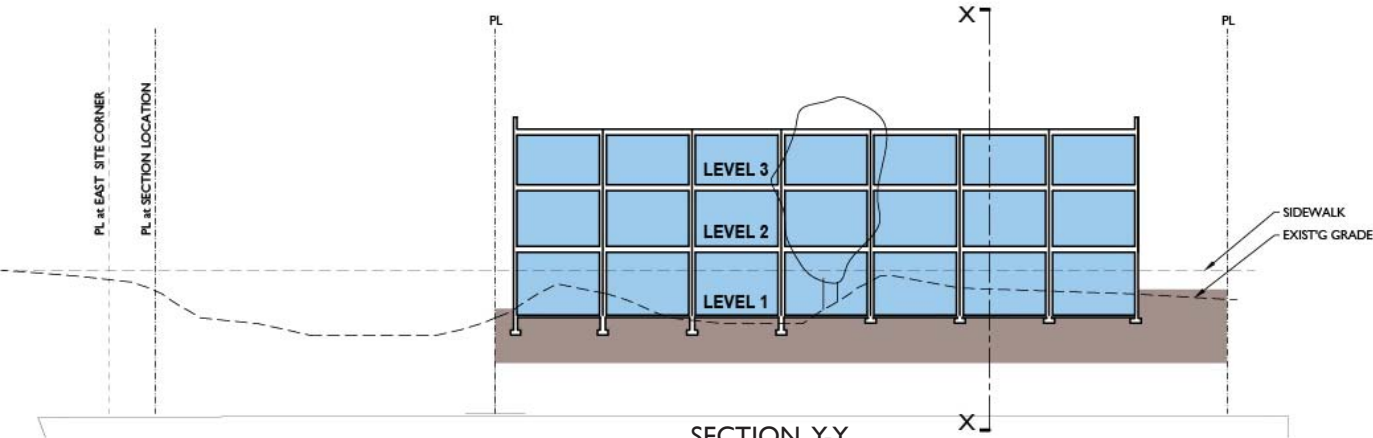
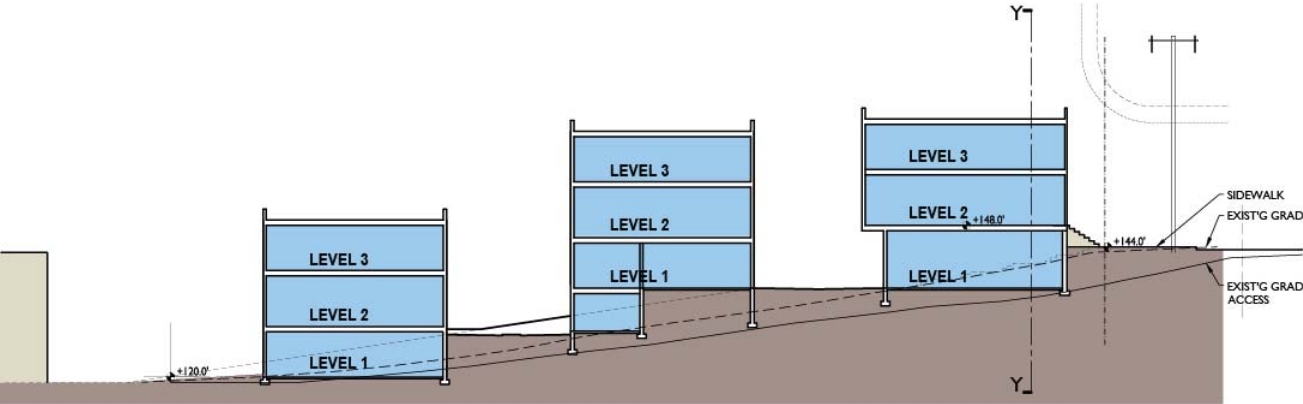
- Common area provided between the front doors of units will increase interaction.
- Maximizes units on higher portions of the site.
- Minimizes alley paving.

CONS

- Wasted space due to required setbacks from the adjacent parking lot.
- Layout creates large ‘canyon’ drive courts between units
- Requires more regrading than other schemes.

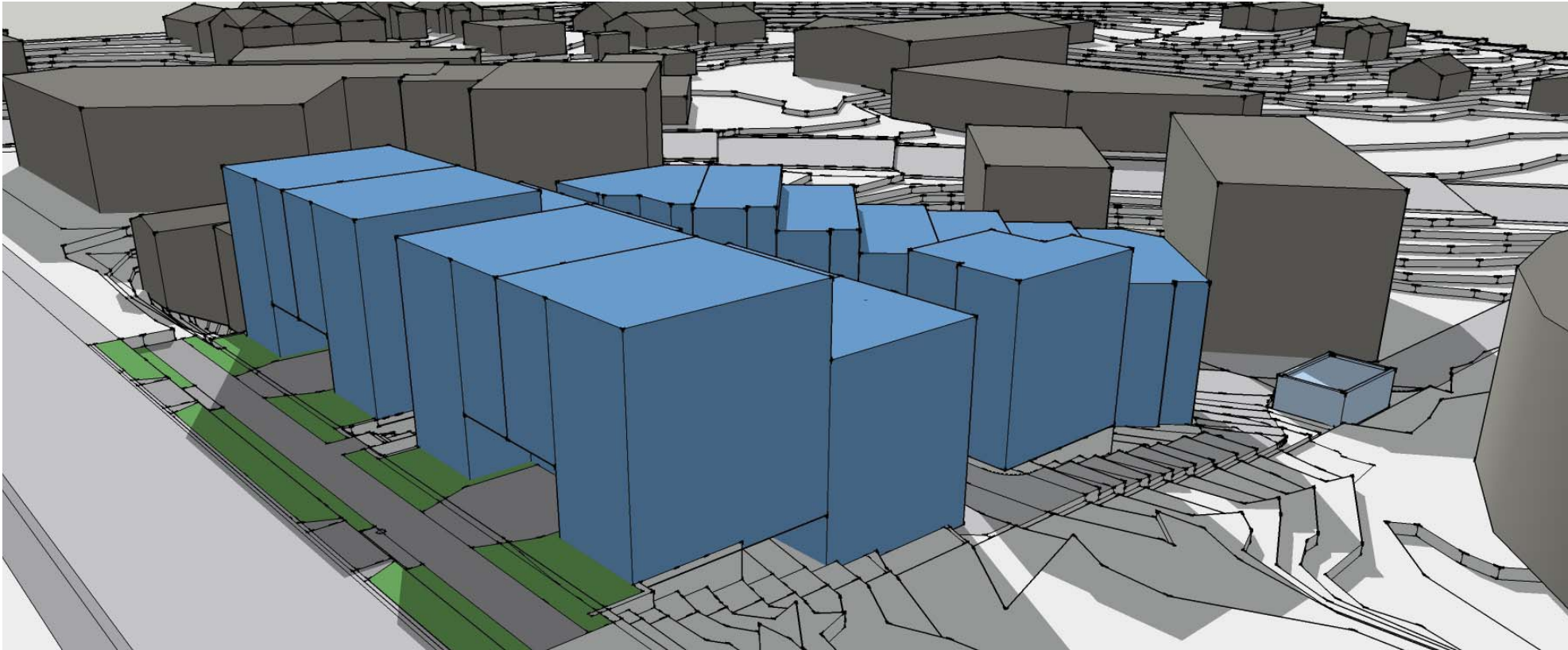


42ND AVE S - 16 UNIT TOWNHOUSE SCHEME  
SITE PLAN NTS

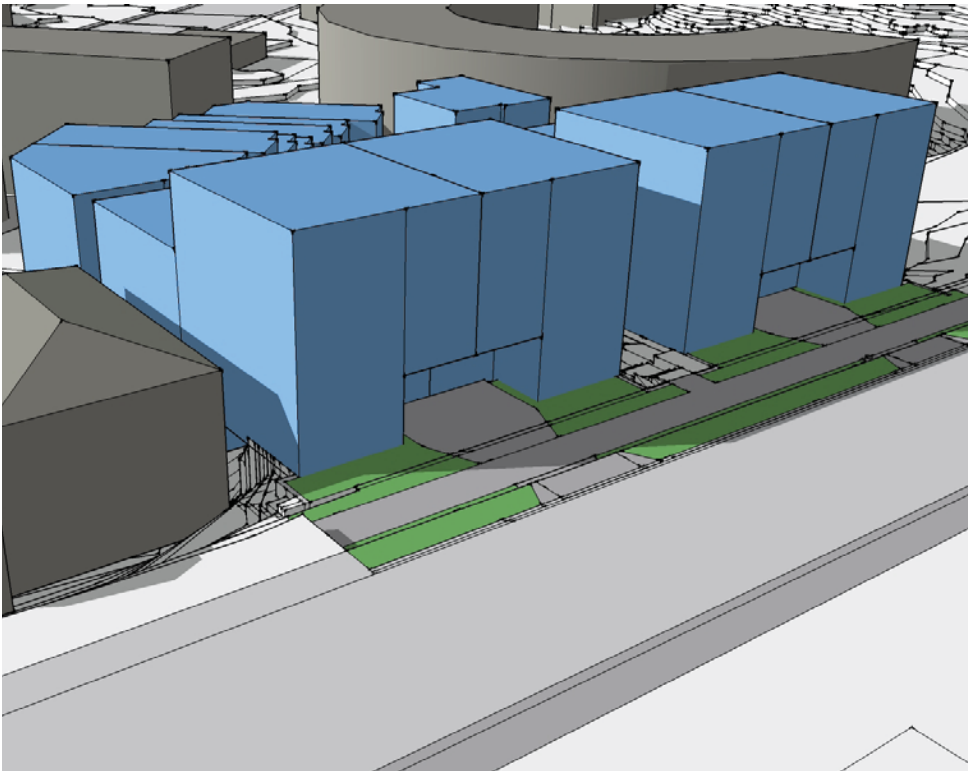




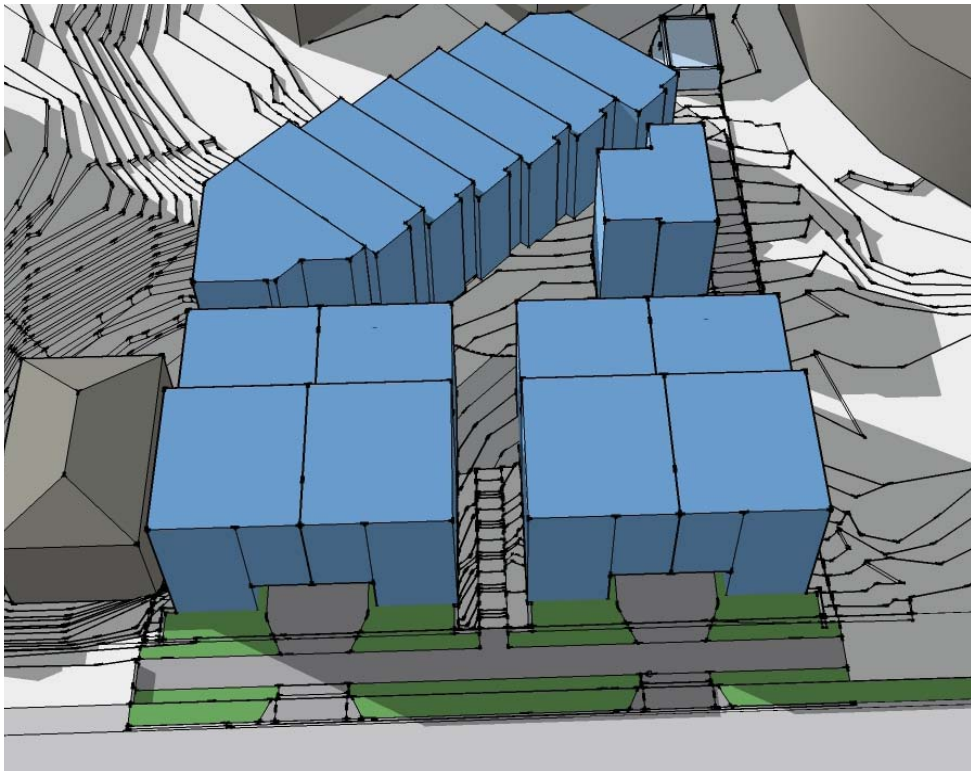
OPTION 2 - FOURPLEXES



NORTHEAST AERIAL VIEW



SOUTHEAST AERIAL VIEW



AERIAL VIEW

DISTINGUISHING FEATURES

- 16 Residential Units in a mixture of Townhomes, Single-Family and “Rowhouses.”
- 16 Private parking garages
- 2 Fourplex units occupy the east side of the site with 4 private garages accessed from the street. The remaining vehicular access comes from the alley at the bottom of the hill.
- “Rowhouse” units are stacked along the alley to minimize on-site paving and allow for more amenity space.

PROS

- Street facing fourplex units have a more traditional residential character and reflect the existing single family character of the street.
- Grading presents interesting opportunities to allow views, light and air to three or more sides of the units.

CONS

- The difficult site requires a number of awkwardly shaped units.
- This scheme requires an easing of setback requirements to work.
- A smaller amount of units can be located on the highest part of the site.
- Unit configuration requires expensive grading activity.

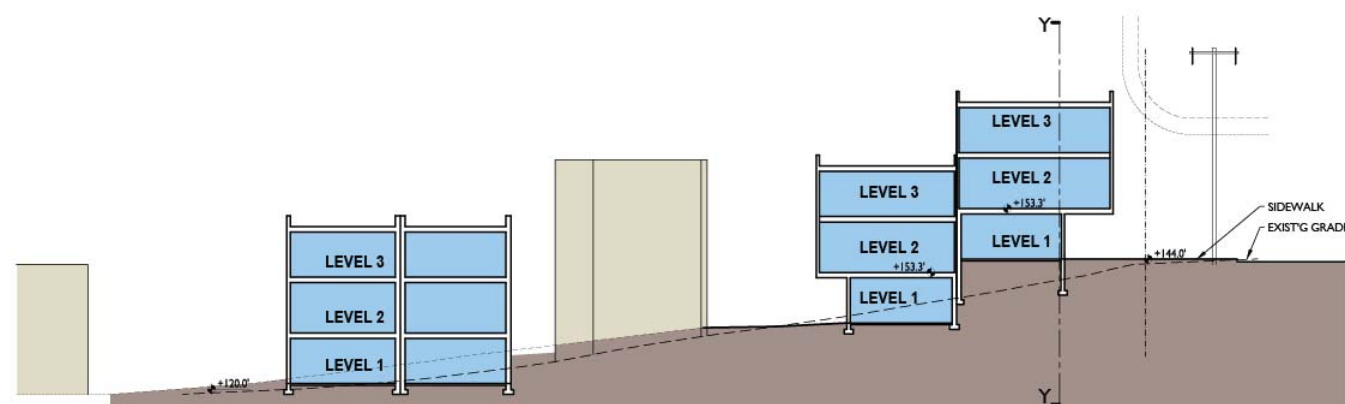
REQUESTED DEPARTURES

- Triangular setback easing
- Sideyard setback easing
- Sideyard deck easing
- Street-fronting floor elevation easing.
- Curb Cuts
- Garage doors per Curb Cut

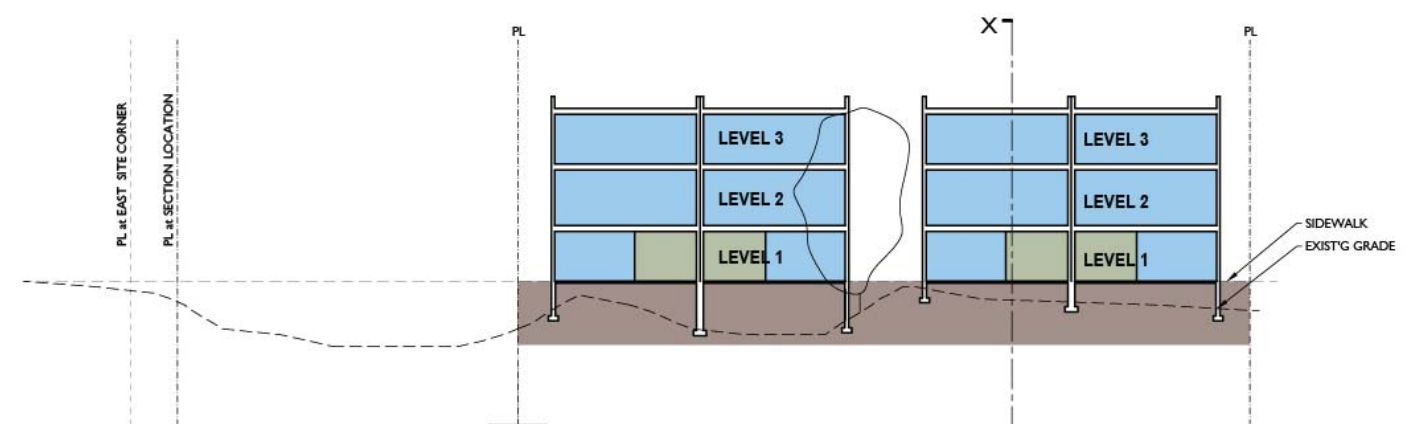
SIGNIFICANT SEATTLE CITY DESIGN GUIDELINES

- A-4: Transition between residence and street. Front stoops and setbacks for garages provide for more privacy and larger amenity areas between the unit and the street.
- A-7: Residential Open Space. The siting of the units allows some common amenity space in the center of the site.





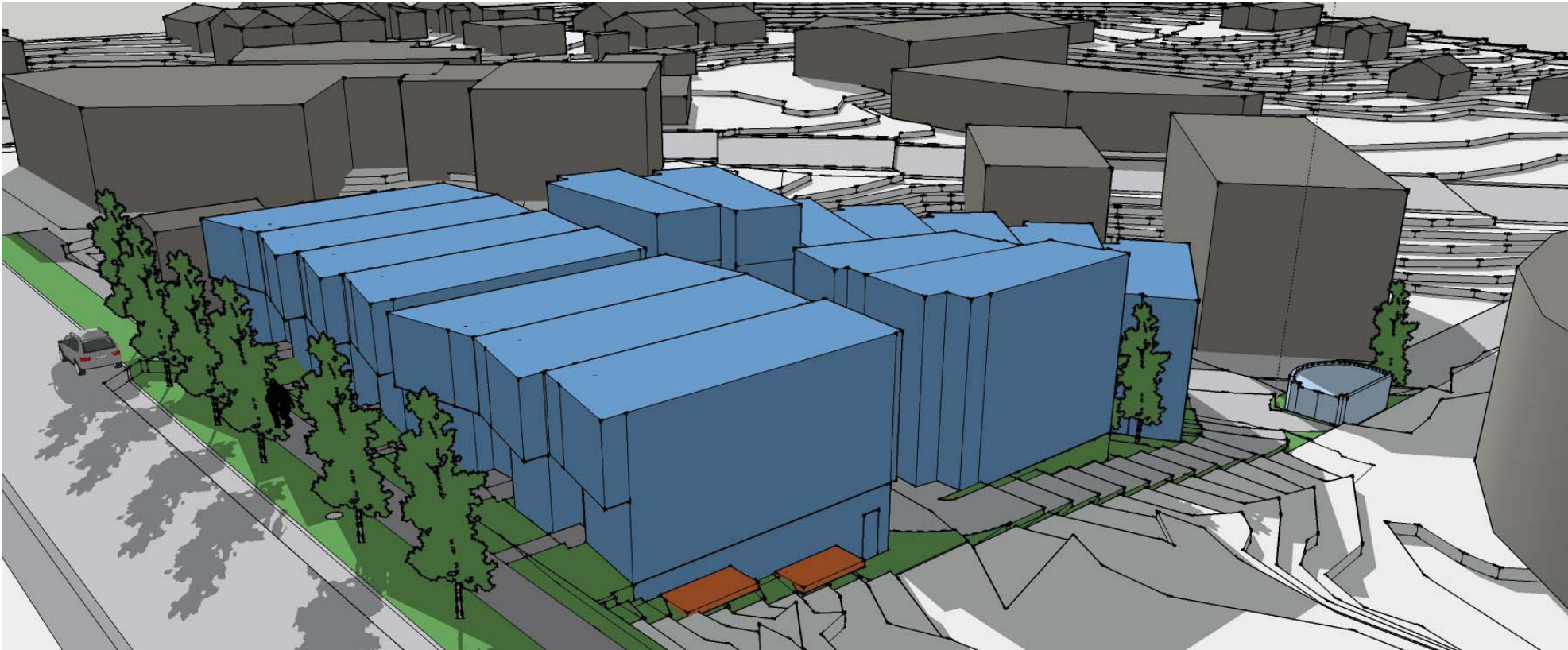
SECTION X-X  
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SECTION Y-Y  
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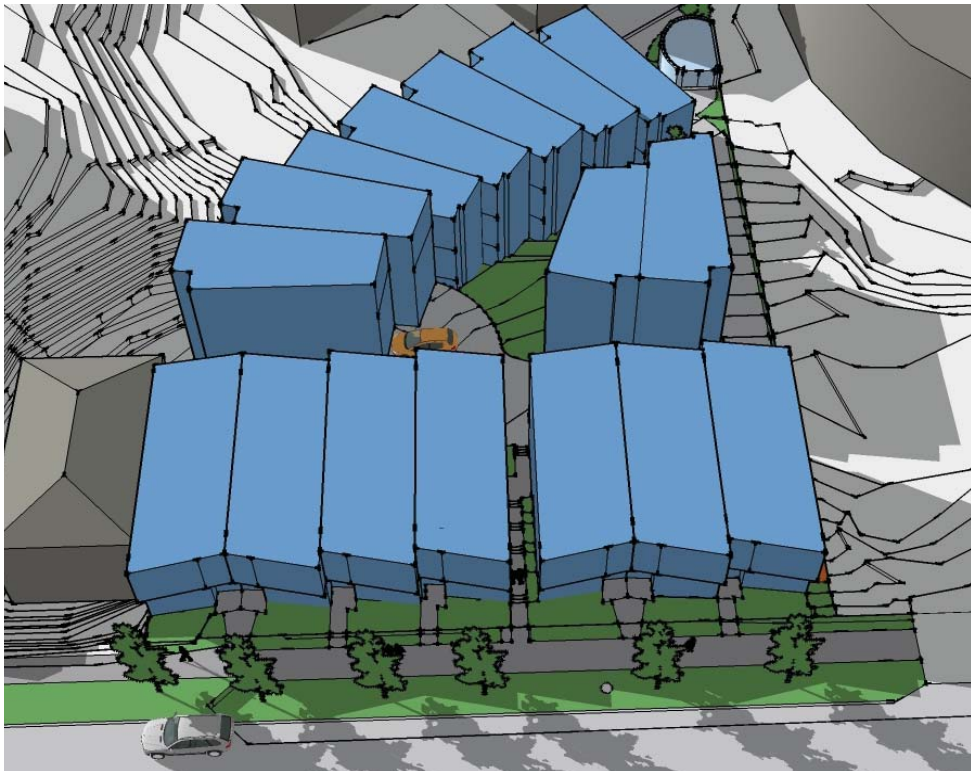
OPTION 3 - PREFERRED SCHEME



NORTHEAST AERIAL VIEW



SOUTHEAST AERIAL VIEW



AERIAL VIEW

DISTINGUISHING FEATURES

- 16 Residential Units
- 16 private parking garages all accessed from the alley
- Large woonerf area in the middle of the site. Vehicular access is from the alley. Radial arrangement of rear units.

PROS

- Common area that's visible from most units and easily accessible by all.
- Opportunities for private amenity areas as well.
- Linear street frontage and rear, radial layout takes the most advantage of the natural terrain providing for better views, efficient use of space and more common areas.
- Unique floor plan and siting contribute to an interesting and high-quality development that will be a valued part of the neighborhood.

CONS

- Requires some setback departures to work.

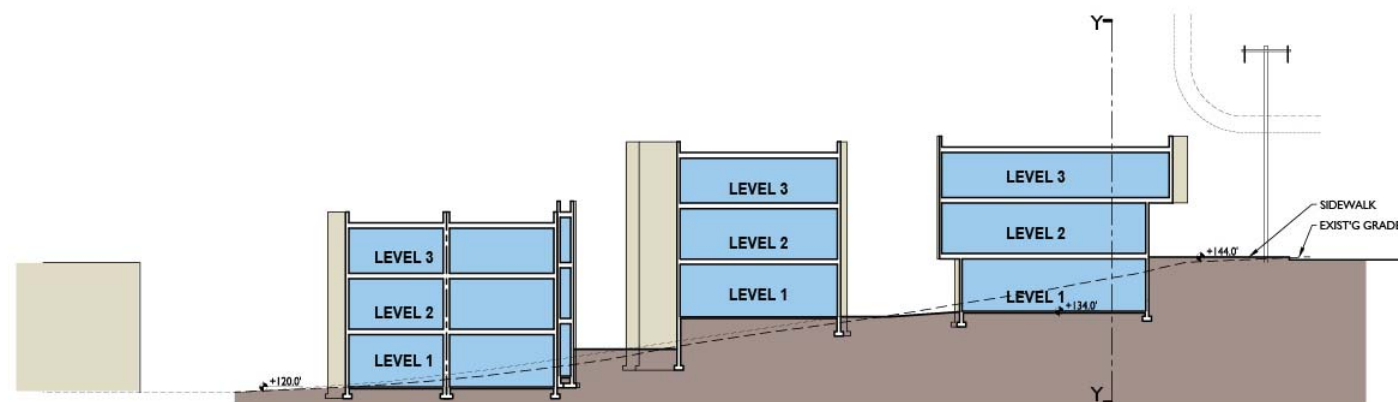
REQUESTED DEPARTURES

- Triangular setback easing.
- Sideyard setback easing.
- Sideyard deck easing.
- Street-fronting floor elevation easing.

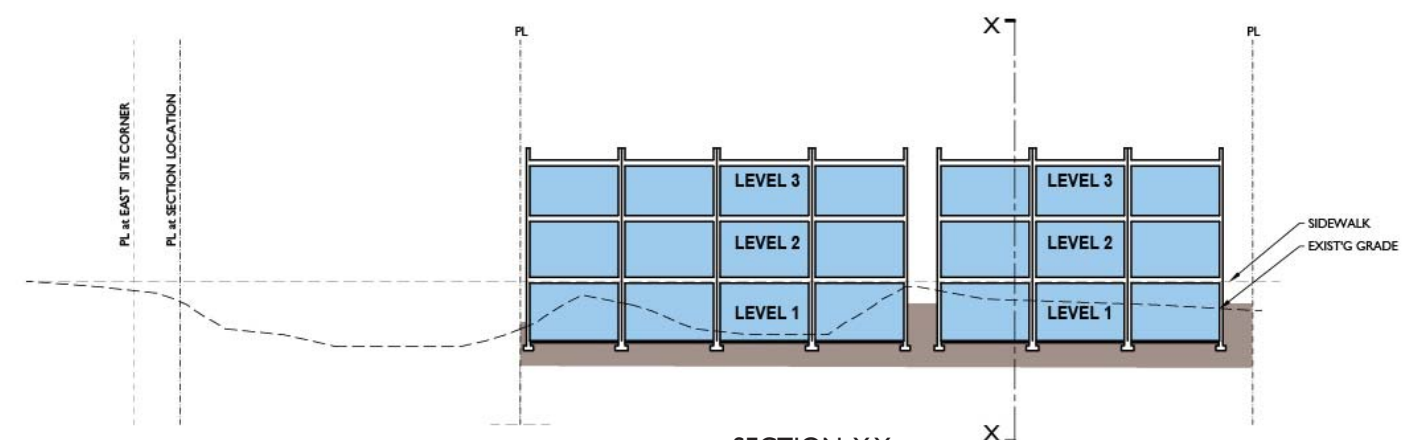
SIGNIFICANT DESIGN GUIDELINES

- A-1: Responding to Site Characteristics. Smaller buildings are able to better reflect the natural topography.
- A-4: Transition between residence and street. Rowhouse style units with front stoops provide for pedestrian security and social interaction among residents and guests.
- A-6: Transition between residence and street. Entry stoops provide a sense of entry to the street side units. Pedestrian access between buildings creates a sense of entry and allows more air and light.
- A-7: Residential Open Space. Orientation of the units around large central drive courts / woonerf create opportunities for spacious, usable, attractive, well-integrated open spaces around individual units and the perimeter.
- D-7: Personal Safety and Security. A common drive court / woonerf providing for heightened visibility as residents come and go.





SECTION X-X  
NTS



SECTION Y-Y  
NTS



OPTION 3 (PREFERRED SCHEME) LANDSCAPE



SITE PLAN - PREFERRED PLAN OPTION 3



PROPOSED FLORA







QUADRANT PROJECTS



ISSAQUAH



SINGLE FAMILY COTTAGE UNIT



ISSAQUAH



SINGLE FAMILY HOUSING UNITS





HARBOR WORK / LIVE



CREEKSIDE



ADMIRAL LOFT HOMES



SALVEO - LEED H PLATINUM



FOURTH & ROY



WALLINGFORD GREEN

42ND AVE S TOWNHOMES - DPD #3013309

EARLY DESIGN GUIDANCE



EXCEPTIONAL TREE

TREE PROTECTION REPORT SUMMARY

Scott Selby, ISA Board Certified Master Arborist

“Tree #486 meets the criteria for an exceptional tree.” However, “...a heavy infestation of ivy covers approximately 80% of the height of the tree; while the tree has been side trimmed for utility line clearance, it does not appear to have been topped....6-8” diameter deadwood and hangers throughout crown.”

“If desired, tree #486 could be retained if the ivy is removed. If the resulting tree is safe and attractive, it might provide value to the property. There is a risk that money and time invested could be wasted if it turns out that the tree is neither viable nor attractive.”

SUMMARY OF SITE WALK WITH BILL AMES, SDOT CITY ARBORIST

July 17th, 2012

“There is no quantitative argument towards the aesthetics of a tree that makes it valuable and desirable, even if it meets the requirements of an exceptional tree. There is a risk that money and time invested could be wasted if it turns out that the tree is neither viable nor attractive.”

“A tree’s canopy is part of the exceptional criteria. Given the compromised canopy, the true value is questionable.”

SEATTLE CITY LIGHT URBAN TREE REPLACEMENT PROGRAM

“City Light considers a tree to be in the wrong place when its maximum mature size will pose a recurring threat to power delivery and public safety.”

“These days when presented with the choice, City Light prefers to remove and replace a tree that’s ‘wrong’ with one that is right and sustainable for its location.”

ZONING CODE SUMMARY

SMC 25..11.080

“The Director may permit an exceptional tree to be removed only if the applicant demonstrates that protecting the tree by avoiding development in the tree protection area could not be achieved through the development standard adjustments permitted in Section 23.41.018 or the departures permitted in Section 23.41.012, a reduction in the parking requirements of Section 23.54.015, and/or a reduction in the standards of Section 23.54.030.”

PROPOSAL

Although the tree meets the technical criteria for an exceptional tree based on diameter, there is additional criteria based on health and canopy, both of which are in question after years of neglect and infestation. Per the arborist’s report, likelihood of restoration is very risky. Furthermore, its proximity to power lines demand that the canopy be permanently cut back into a very attractive “L” shape.

PROPOSAL (CONTINUED)

It is also important to note that the area around the tree has a significant down-slope from the street toward the west, and will not be desirable space for residents to utilize if the tree is retained.

Finally, the development standard adjustments permitted under the code to compensate for the presence of an exceptional tree by avoiding development do not adequately make up for the tree’s presence. Most notably, increases in height do not benefit a townhome development; since no parking is required, such departures make no impact; and given that this is an NC2 zone with a high F.A.R., there is no gain from density increases.

We propose replacing this tree with a row of trees along 42nd which will improve the street character, be more appealing to residents, conform to SDOT’s *Approved Street Tree List* and allow the site to be developed to its full potential.



PHOTO 3-- CLOSE UP OF IVY ON TREE #486

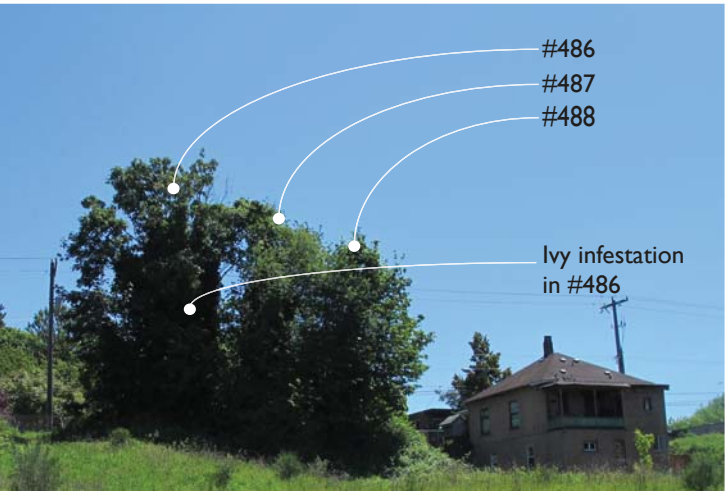
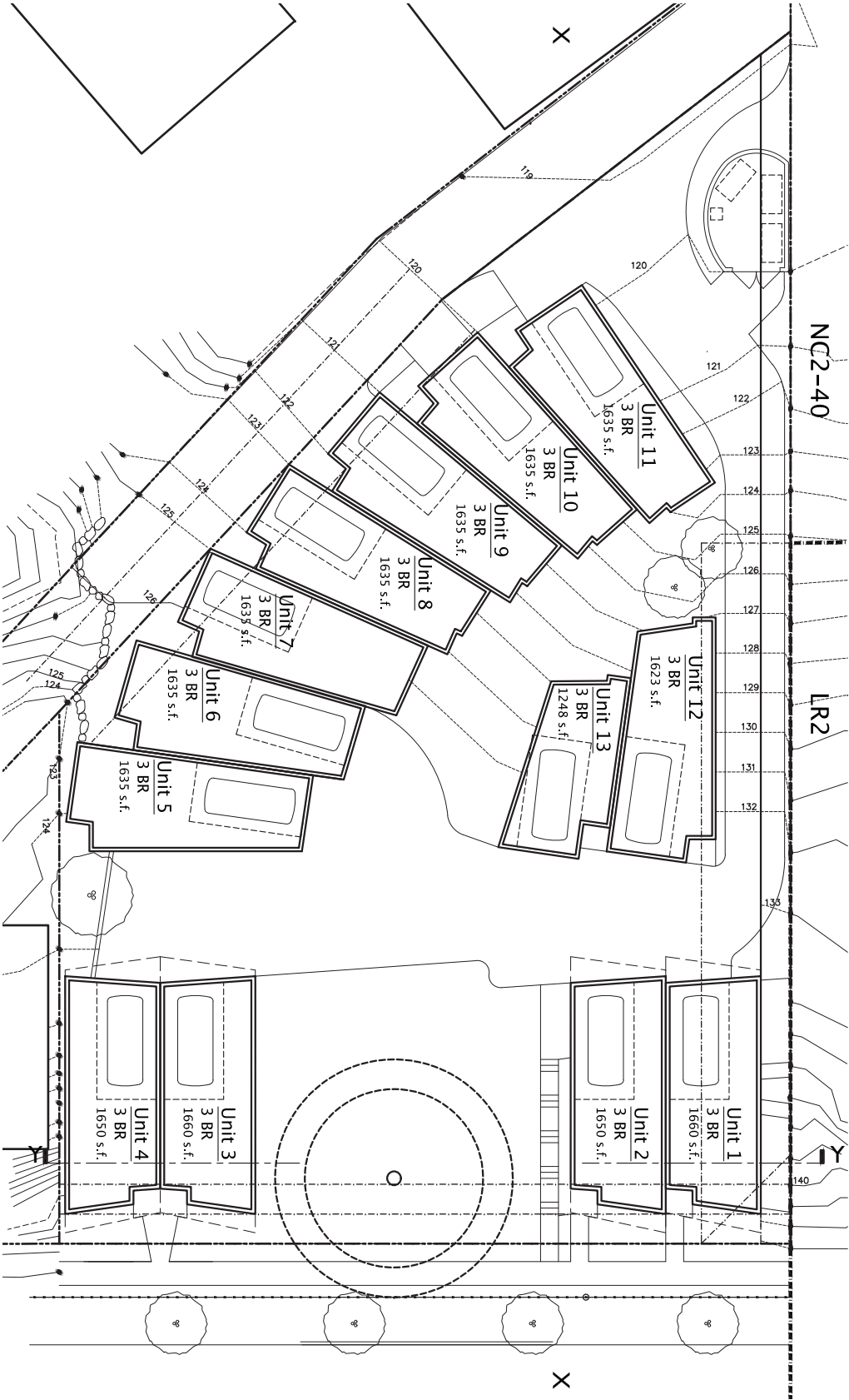
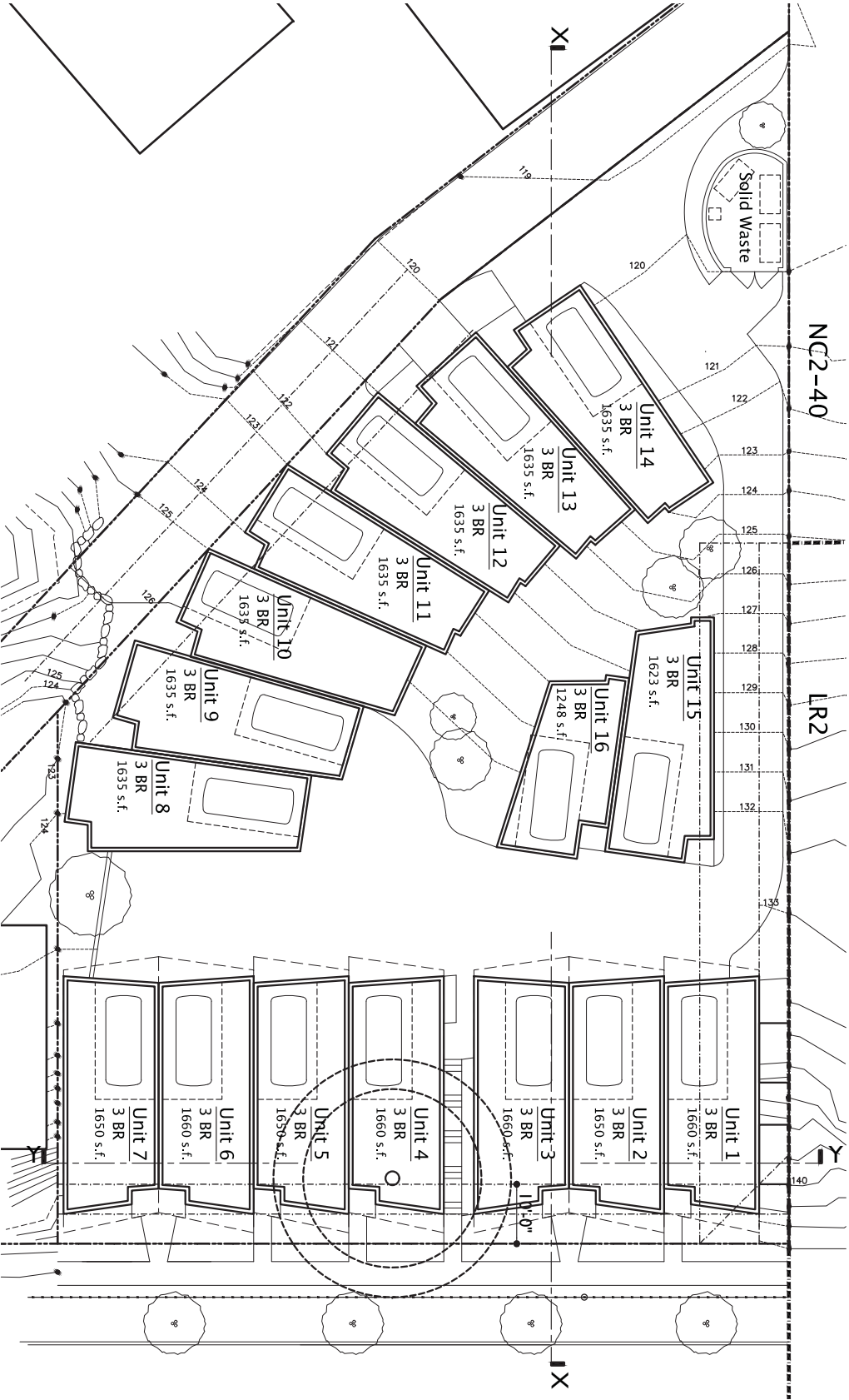


PHOTO 1-- SITE AND TREES: VIEW LOOKING EAST FROM LOWER LOT







PREFERRED SCHEME 3 - SITE PLAN WITH TREE LOCATION

PREFERRED SCHEME 3 - IMPACT OF TREE