



11.21.2013 DESIGN REVIEW RECOMMENDATION
3078 SW AVALON WAY DPD PROJECT # 3013303

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caron

PROJECT HISTORY

The Early Design Guidance meeting was held on September 13, 2012

The Master Use Permit was submitted on February 12, 2013

Soon after the September 13 EDG meeting, Caron Architecture was approached to design a second building on the three parcels to the north of this project. That project under DPD # 3014100, was slated to be designed concurrently with this proposal, 3013303. The schedule for this proposed project was slightly delayed so the two project schedules could be synchronized. Both MUP sets were submitted within weeks of each other but the buildings were placed on hold by the owners during the MUP review period due to a myriad of factors, and the application for the other project was eventually cancelled. The decision was made late summer to continue moving forward with this project only. Comments and concerns raised through the design review process for both buildings have been incorporated into this design, although only the EDG report for this project is specifically addressed in this presentation.

EXISTING SITE

The proposed project is zoned MR and abuts a SF 5000 zone across the alley to the west. A height limit of 60' is allowed in this zone.

The site has approximately a 25' slope from east to west and will have views of downtown Seattle to the north, North Delridge to the east, and Mt. Rainier to the southeast. It is currently terraced with rockery and concrete retaining walls and built out with single story structures.

The significant grade change from the alley to SW Avalon Way creates challenges for the site, as well as the adjacent properties. The drop in grade creates wonderful view opportunities and provides a diminished building height along the alley, adjacent to the SF 5000 zone. It also provides opportunities to access a multi-tiered underground parking garage from both the alley and SW Avalon Way, although no parking is required for the site. There are numerous newer multi-family buildings in the vicinity with garage access from SW Avalon Way.

PROJECT DESCRIPTION:

The proposed development will create an urban apartment building with street front residential units along SW Avalon Way and an enhanced pedestrian experience in the West Seattle Junction Hub Urban Village.

The project provides an exciting opportunity to create a high density infill project in one of the most scenic and lively neighborhoods in Seattle. Given the proximity to the Junction Hub Urban Village, high frequency transit, and breathtaking views, the site is situated perfectly to allow future residents the opportunity to enjoy all West Seattle has to offer. The proposed project will embrace and enhance the urban fabric of this burgeoning neighborhood.

Parking is not required for this site, although approximately 60 stalls will be provided in the structure. Vehicular access will be provided both from Avalon Way SW and from the alley to accommodate site grades.

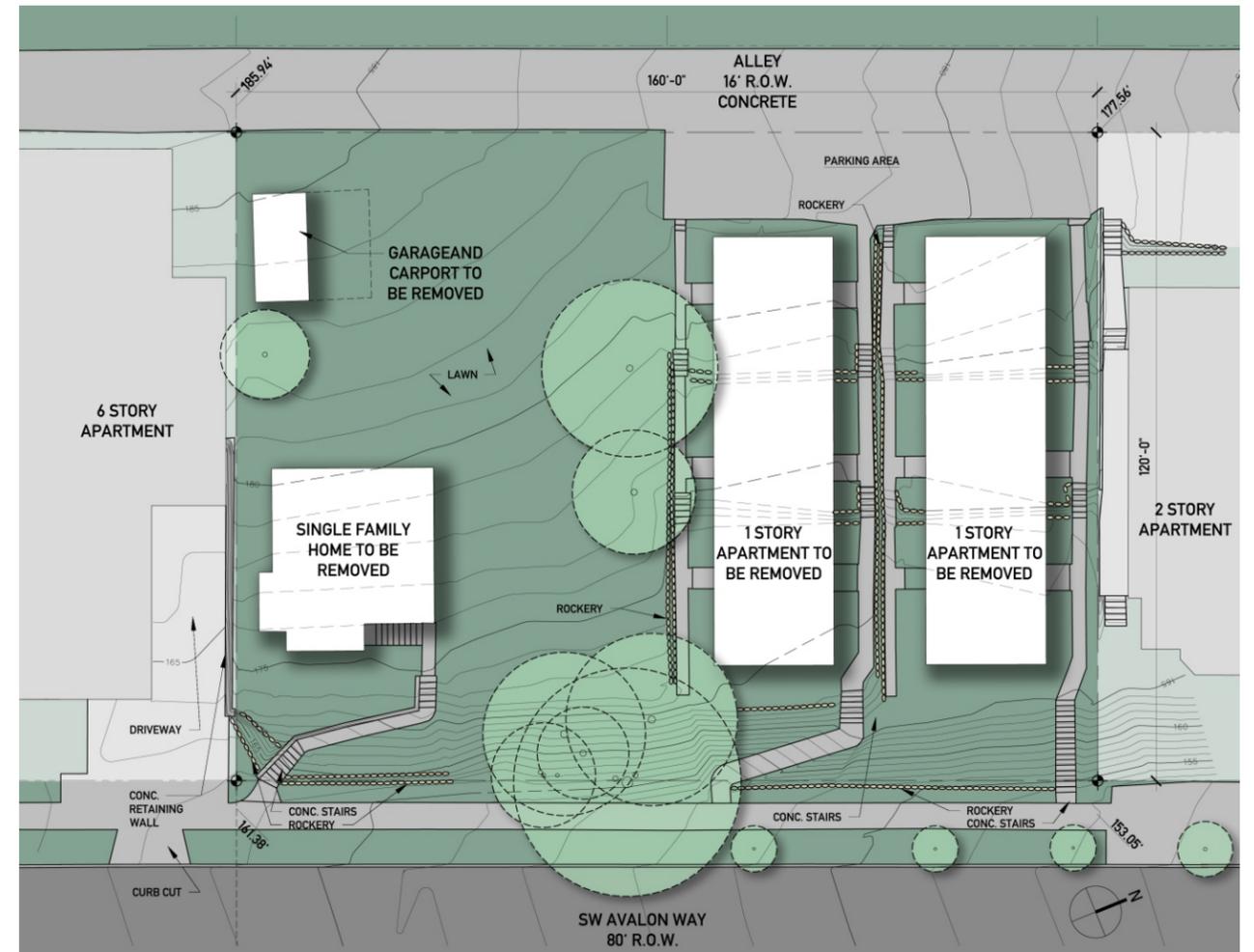
The ground level of the preferred scheme consists of a residential lobby, underground parking garage, utilities, and five residential units facing the street with direct, individual street access.

The second level consists of nine residential units, a partially buried parking garage accessed off of the alley, as well as bicycle storage and utilities.

The third level contains 17 residential units, a common elevated courtyard, refuse and service rooms.

The fourth thru sixth floors each contain 18 units per floor, and the seventh floor contains 17 units. All floors contain a mix of unit sizes and bedroom counts.

The elevated courtyard, which sits atop the second level parking level's roof, contains spaces for gathering, grilling and relaxation, as well as a grassy area to soften the space. There are also private amenity areas for the three units which have direct access to the courtyard. Parking will be located within the structure and vehicular access will be provided both from SW Avalon Way and from the alley to accommodate site grades.



EXISTING SITE PLAN

DEVELOPMENT STATISTICS SUMMARY:

- Lot Size	19,196 SF
- FAR	3.2
- Allowable FAR	61,427.20 SF
- Proposed FAR	61,368.75 SF
- Parking Stalls	60 Stalls

Floor Area Summary

	FAR SF	Units
Roof	645.36	
7th Floor	9,494.70	17
6th Floor	10,360.48	18
5th Floor	10,866.29	18
4th Floor	10,866.29	18
3rd Floor	9,848.26	17
2nd Floor	8,159.85	9
1st Floor	1,127.52	5
Total	61,368.75 sf	102 Units

DEVELOPMENT OBJECTIVES

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ZONING LEGEND

- NC3-65 Neighborhood Commercial
- LR 3 Low rise 3
- LR 1 Low Rise 1
- SF 5000 Single Family
- MR Multi-Family

ZONING MAP

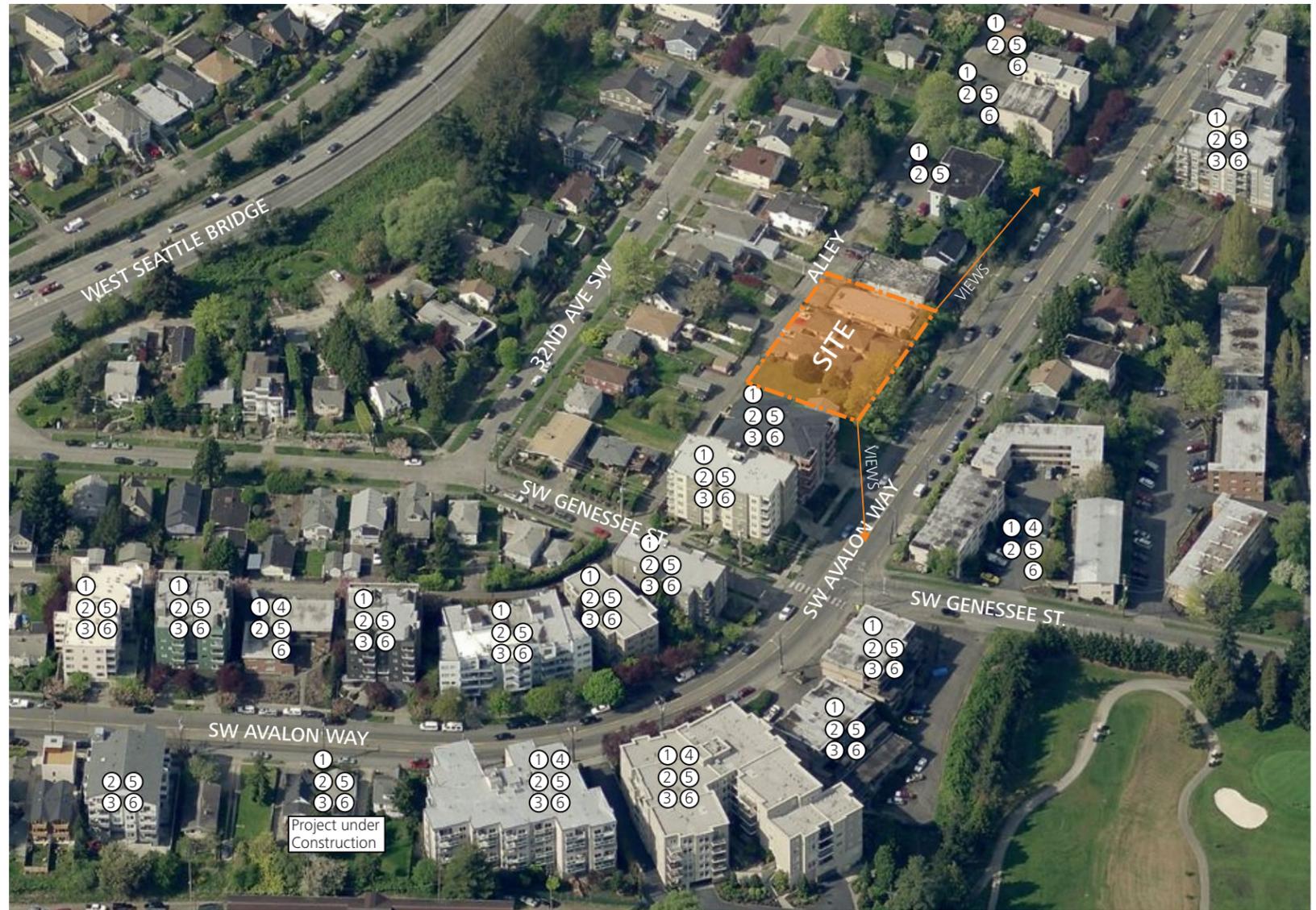
This area of West Seattle is a mixture of residential uses, with single and multi-family structures and no commercial uses along SW Avalon Way. The alley abutting the west property line is the barrier between the single-family and multi-family zones. The newer projects appear to be built up to the rear setback against the alley. The urban tree canopy in the area is limited mainly to the older vintage multi-family sites and single-family zones, although SW Avalon Way is lined with small, young street trees.

SW Avalon Way is an arterial and frequent transit corridor, as is the West Seattle Bridge and Fauntleroy Way SW to the west. Vehicular traffic is heavy on SW Avalon Way and pedestrian traffic is low to moderate. The area is relatively dense but the side streets are mostly quiet and service local traffic. The neighborhood is walkable, with the West Seattle Junction Hub Urban Village Center only a few blocks away.

The proposed project is zoned MR and abuts a SF 5000 zone across the alley to the west. A height limit of 60' is allowed in this zone. As the site is across an alley from the SF 5000 zone, Ordinance 124307 forbids the inclusion of the 15' bonus height with the addition of affordable housing, as allowed per SMC 23.45.510.

The site has a 32 foot slope from east to west and will have views of downtown Seattle to the north, North Delridge to the east, and Mt. Rainier to the southeast. It is currently terraced with rockery and concrete retaining walls and built out a single-family home and two apartment buildings.

The significant grade change from the alley to SW Avalon Way creates challenges for the site, as well as the adjacent properties. The drop in grade creates wonderful view opportunities and provides a diminished building height along the alley, adjacent to the SF 5000 zone. It also provides opportunities to access a multi-tiered underground parking garage from both the alley and SW Avalon Way, although no parking is required for the site.



SITE AERIAL

NEIGHBORHOOD DESIGN CUES ⊗

1. Flat Roof
2. Large Balconies
3. Driveway from SW Avalon Way
4. Masonry Veneer
5. Modulated Facade
6. Neutral Colors

The multi-family buildings in the vicinity are a mixture dating from the middle of the 20th century to present date, and range from two to seven stories tall. Most have similar design features, such as flat roofs with little or no overhangs, large decks, and little decorative adornment. The mid-century structures accentuate horizontal massing. Exterior materials include masonry, concrete, fiber-cement, vinyl, wood, and stucco.

Nearly every building takes garage access from SW Avalon Way even though all abut an alley. This is most likely due to the challenging grades located throughout the area.



1 - SW AVALON WAY - SOUTH OF SITE



2 - SW AVALON WAY - SOUTH OF SITE



3 - SW AVALON WAY - SOUTH OF SITE



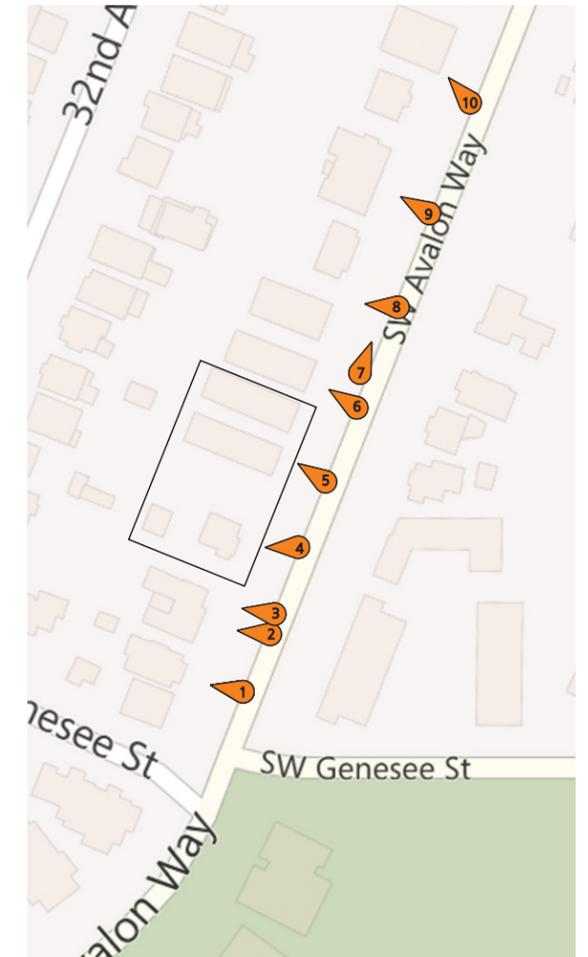
4 - SW AVALON WAY - LOOKING WEST



5 - SW AVALON WAY LOOKING WEST



6 - SW AVALON WAY NORTH OF SITE



7 - SW AVALON WAY - NORTH OF SITE



8 - SW AVALON WAY - NORTH OF SITE



9 - SW AVALON WAY - NORTH OF SITE



10 - SW AVALON WAY - NORTH OF SITE

CONTEXT PHOTOGRAPHS

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11 - SW AVALON WAY - LOOKING SOUTH



12 - SW AVALON WAY - LOOKING EAST



13 - SW AVALON WAY - LOOKING EAST



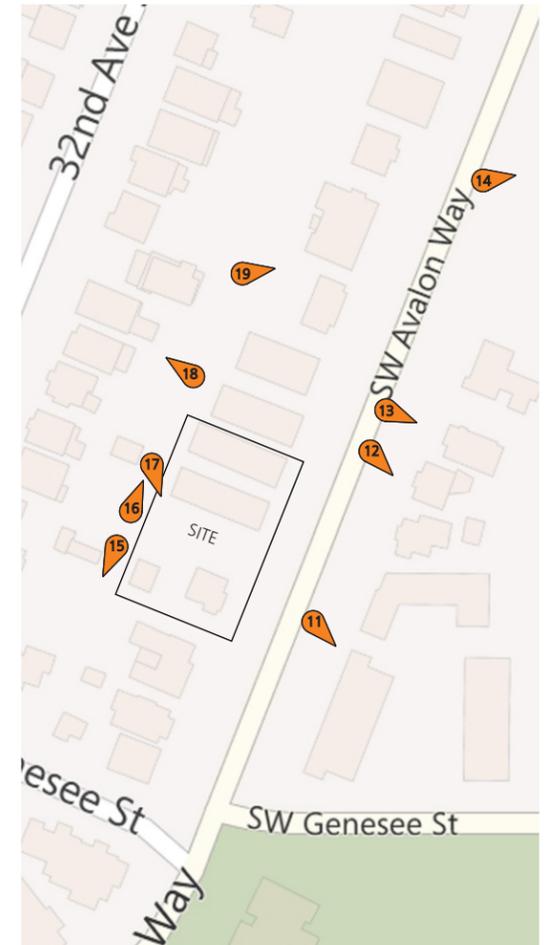
14 - SW AVALON WAY - NORTH OF SITE



15 - ALLEY - LOOKING SOUTH



16 - ALLEY - LOOKING NORTH



17 - ALLEY - LOOKING SOUTH



18 - ALLEY - LOOKING WEST



19 - ALLEY - LOOKING NORTH

CONTEXT PHOTOGRAPHS

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1 - SW AVALON WAY - LOOKING EAST



2 - SW AVALON WAY - LOOKING WEST

STREET CONTEXT

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SMC ZONING REQUIRED		PROPOSED
USE	23.45 23.41.004	The proposed use of multi-family apartments are allowed outright by the seattle land use & zoning code with updated ordinance 123495. A Master Use Permit (MUP) is required, including full design review
HEIGHT	23.45.514 23.45.514J.6	Base height limit is 60-ft. Height is measured from the average grade level per director's rule 4-2012 and smc 23.86.006.a. Elevator/stair penthouses may extend up to 16-ft. Above height limit.
FAR	23.45.510	Total permitted FAR = 3.2 x 19,196 sf. = 61,427,20 sf allowed. Proposed FAR : 61,368.75 sf
This site is within the physical area defined by new ordinance #124307, which went into effect on November 7, 2013; modifying the MR zone adjacent to single family zones to not be eligible for the extra residential area or additional height, as allowed per SMC 23.45.510.D. The design herein in complies with this new ordinance.		
SETBACK	23.45.518	Front setback: 5 ft. Minimum, 7 ft. Average Rear setback: 10 ft. Minimum Side setback: 7 ft. Minimum, 10 ft. Average
LANDSCAPE	23.45.524	Green factor score of 0.5 or greater is required for any lot with development containing more than one dwelling unit. Street trees are required on SW Avalon Way.
PARKING	23.45.020	No parking required for this site due to urban village overlay and proximity to frequent transit. Bicycle parking is required 1 space per 4 units = 25.75 spaces required
SOLID WASTE	23.45.040	575 sf. + 4 sf. Per unit over 100 units required 575 sf + 8 sf = 583 sf - 15% per 23.54.040 C = 495.55 sf required 536.98 sf trash and recycling provided.

CODE ANALYSIS

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A SITE PLANNING

A-1 Respond to Site characteristics

The refined scheme pushes the building toward SW Avalon Way to create a continuous urban edge along the street and provide a relief and transition to the SF zone across the alley.

Given the topographical nature of the site, one entry to the garage is proposed from SW Avalon Way, similar to existing precedents in newer apartments buildings on adjacent sites. A second vehicular entry is provided from the alley. This arrangement also provides decreases vehicular traffic in the alley. Services will be provided from the alley.

A-2 Streetscape compatibility

The street facing façade at the pedestrian level is modulated and adorned with finer textured materials to provide human scale details such as brick masonry, canopies, low fences, and appropriate modulation.

The main pedestrian entry and entries to the residential units are accessed directly off the street, further enlivening SW Avalon Way.

A-3 Entrances visible from street

Both the main lobby entrance (defined by a prominent 1 1/2 story tall space and significant entry canopy), as well as individual entries to the walk-up units are directly off SW Avalon Way.

A-4 Human Activity

Not applicable, zoning and street character do not support commercial use, however pedestrian entries are enlivening SW Avalon Way.

A-5 Respect for other sites

The pedestrian courtyard is located on the alley side to provide a greater setback from adjacent the SF zone. Minimal windows have been placed along the southern property line, as well as the portion of the building closest to the alley, to provide a privacy for the adjacent apartments and single family neighbors.

A-7 Residential open space

The pedestrian courtyard provides maximum sun exposure to the common outdoor amenity area. The adjacent indoor amenity space is accessible to all residents.

B HEIGHT, BULK, AND SCALE

B-1 Height, Bulk and Scale compatibility.

The majority of the mass of the building is pulled away from the SF zone toward the street. The façade is broken into base, body and top, further decreasing the perception of the height of the building. The upper most stories are further stepped back from the façade, providing additional relief.

C ARCHITECTURAL ELEMENTS AND MATERIALS

C-1 Architectural Context

We have taken cues from adjacent multifamily projects, providing corner decks and balconies, lightly painted materials on the upper levels and light colored brick along the street.

C-2 Architectural Concept

C ARCHITECTURAL ELEMENTS AND MATERIALS

C-1 Architectural Context

We have taken cues from adjacent multifamily projects, providing corner decks and balconies, lightly painted materials on the upper levels and light colored brick along the street.

C-2 Architectural Concept

The base has been differentiated to decrease the bulk and height appearance on SW Avalon Way. The body and base have been tied together with a rhythm of openings and modulation. The base is also interlocking with the body of the building at the intersection of the residential lobby. High quality materials are used both along SW Avalon Way, as well as the alley.

C-3 Human Scale

The unit entries and main lobby entry feature canopies, bringing the scale down, an exterior bench and differentiated paving adds pedestrian detail.

Walk-up units have small yards defined by low fences separating semi private spaces from the public street.

D PEDESTRIAN ENVIRONMENT

D-5 Visual Impact of Parking Structures

One entry to the parking garage is proposed from Avalon Way. The entry is minimized to de-emphasize the auto by limiting parking on this level to only 30 cars. The garage opening is recessed and kept to a minimum height to further decrease it's importance compared to the pedestrian entry.

Furthermore the vehicle entrance is similarly modulated to the street level walk-up units, following the rhythm of the base.

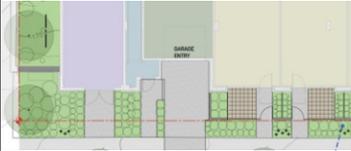
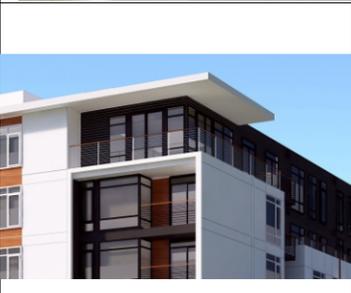
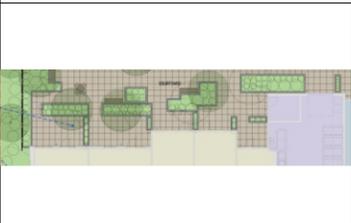
D-6 Screening of Dumpsters, Utilities and Service Areas

All utilities and trash areas are contained within structure and will be rolled to the alley only on pickup day.



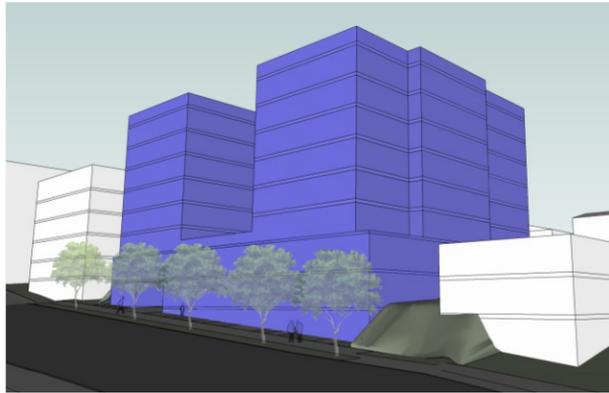
RELEVANT DESIGN GUIDELINES

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EDG Direction			Architect's Response
<p>A-2 Streetscape compatibility At the Early Design Guidance Meeting, the Board discussed that while not proposing commercial uses at street level, the appearance and quality of the 2 story loft units and the adjacent landscaping/public realm is crucial to establishing a pedestrian friendly street edge. They encouraged high quality, commercial grade materials and landscaping. The treatment along the alley should also be high quality and intentionally designed, not just a fence.</p>		<p>Elevations, Perspectives</p>	<p>Tall residential spaces fronting SW Avalon way have been retained and units have been added along Avalon Way at the second floor. High quality materials such as brick, concrete, and cedar have been included along the street frontage. The alley edge has been intentionally designed to provide a buffer between the building and the single family neighbors, while providing security and CPTED principles. Appropriate landscaping is incorporated at both edges.</p>
<p>A-5 Respect for Adjacent Sites At the Early Design Guidance Meeting, the Board encouraged the side yards to be landscaped and the proposed windows staggered to buffer the windows and privacy of adjacent neighbors (existing window overlays to be required), and the design of courtyard and roof terraces should buffer overlooks of the adjacent balconies and backyards.</p>		<p>Site Plan, Elevation</p>	<p>The sideyards have been landscaped to increase privacy between buildings. The existing building's windows were surveyed and overlaid to ensure privacy between dwelling units. The courtyard is recessed below the alley grade and significant buffers at the alley edge and roof deck shield neighboring properties from overlooking residents.</p>
<p>A-8 Parking and Vehicle Access At the Early Design Guidance Meeting, the Board discussed that both vehicular entries should consider pedestrian safety and sight lines when detail designed, yet be a minimal portal on the Avalon elevation; the Board did NOT exclude vehicle entry to only one location.</p>		<p>Site Plan, Elevations</p>	<p>Both entries have been reduced and the Avalon Way entry maintains the minimum width required by the Seattle Municipal Code. Low landscaping and fencing is used at the street entry to provide adequate sight lines. The alley access ramp has been relocated, which increases visibility of cars and people. The courtyard level has been sunken further to reduce the height of the walls abutting the alley, further increasing visibility.</p>
<p>B-1 Height, Bulk, and Scale Compatibility At the Early Design Guidance Meeting, the Board discussed this topic at length. They requested street elevations along Avalon, showing the full facades of adjacent 2 buildings to south and those to north. They considered the other massing options which placed the courtyard to the east, but referred back to option C in deference to the reduced bulk along the alley and the adjacent "less intensive zone". They agreed stepping back the upper stories on all sides is warranted, especially at the west side facing the SF zone. They supported the common roof deck at the northeast, but strongly suggested reducing the overall height of the northeast corner approximately one floor. The length of the north wall, currently generating a departure request, should be studied to trim the corners, to afford light to the northern neighbors, and/or simply shortened to reduce the bulk towards the alley.</p>		<p>Elevations, Perspectives</p>	<p>Street elevations have been provided showing the buildings to the north and south. The building has been stepped back on all sides, including the top two floors closest to the alley. The northeast corner has been stepped down one floor to decrease the perceived height from the street. The building has been sunken into the site more, removing the departure request for the rear yard setback, and consequently reducing the wall length at grade along the north property line. The departure request for relief from the facade length requirement has also been eliminated</p>
<p>C-4 Exterior Finish Materials At the Early Design Guidance Meeting, the Board agreed the proposed modulation and deep reveals of the massing are crucial to mitigating the bulk (especially along Avalon), and this should be supported by a high quality, durable and interesting cladding palette, on all sides. That palette should employ a range of colors, materials and textures more pronounced than those shown in the EDG sketches.</p>		<p>Elevations, Perspectives</p>	<p>The modulation has been modified to reduce the bulk even more. High quality materials are incorporated on all facades, as shown on the building elevations.</p>
<p>D-1 Pedestrian Open Spaces and Entrances At the Early Design Guidance Meeting, the Board requested the applicant consider a pass-through or pedestrian path in one of the side yards, and to carefully design a screen along the alley courtyard to protect adjacent backyard privacy and encourage an attractive and safe alley presence. The Board also applauded the 2 story scale and street presence of the Avalon lobby, and encouraged further scale and pedestrian interest in the street-facing loft facade designs.</p>		<p>Site plan, Elevations</p>	<p>A pass-through was considered, but given the challenges with the extreme grade change from alley to street, in addition to the relocation of the alley garage access ramp, a pass-through is not feasible. The courtyard is recessed below the alley grade and a significant landscape buffer shields neighboring properties while still not turning a blind eye to the alley.</p>
<p>D-8 Treatment of Alleys At the Early Design Guidance Meeting, the Board emphasized that adequate lighting and security along the alley is key, through good design and "eyes on the alley" strategies. They also requested a full length, detailed and dimensioned elevation of the alley elevation showing the parking wall condition and materials, including landscaping and courtyard screen design. (See also Departure #1 discussion below)</p>		<p>Site Plan, Elevations</p>	<p>Appropriate lighting has been included along the alley edge to provide security, while not blinding neighboring properties. A full length alley elevation has been provided, although the departure request has been removed.</p>
<p>E-2 Landscaping to Enhance the Building and/or Site At the Early Design Guidance Meeting, the Board encouraged maximizing the size of the roof deck, and the planted or softscape areas of the courtyard and roof deck because the site will be fully developed leaving little remaining permeable surface other than the sideyards. They also encouraged landscaping, and pedestrian features along Avalon; and trellises, play structures, seating etc in the courtyard and on the roof deck, for use and amenity to residents.</p>		<p>Site Plan, Roof Plan Courtyard Plan</p>	<p>Landscaping on the roofs and at the courtyard has been increased to mitigate hardscape and building footprint. Seating and tables have been provided in the courtyard and the roof deck will also provide amenities, such as seating and a barbeque. Low landscaping has been provided along SW Avalon Way.</p>

EDG DIRECTION AND ITEMIZED RESPONSE

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CONCEPT A

SUMMARY

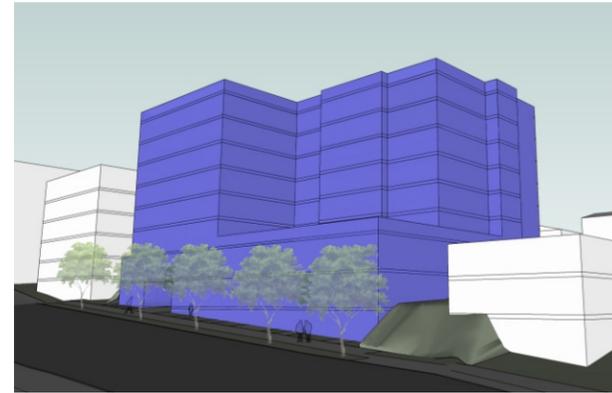
Massing Option A is an “H” shaped structure atop a two and a half story concrete podium. A diminished auto entry and prominent pedestrian entry are incorporated. This option creates a large courtyard facing SW Avalon

ADVANTAGES

- Maximizes FAR
- Breaks massing along SW Avalon Way
- Code compliant

CHALLENGES

- Less view units
- Large mass against residential zone, unsympathetic to neighbors
- Courtyard has only morning light



CONCEPT B

SUMMARY

Massing Option B is an “L” shaped structure atop a two and a half story concrete podium. A diminished auto entry and prominent pedestrian entry are incorporated. This option pushes the bulk of the massing away from SW Avalon Way toward the alley.

ADVANTAGES

- Maximizes FAR
- Less mass on Avalon Way
- Plaza has views
- More southern facing units
- Code compliant

CHALLENGES

- Less view units
- Large mass against residential zone, unsympathetic to neighbors
- Courtyard has only morning light
- Large quantity of units face adjacent apartment building to the south



CONCEPT C

as presented at the 09.12.2012 EDG meeting

SUMMARY

The preferred Option C is also an “L” shaped building, however, the bulk of the mass has been pushed toward SW Avalon Way creating a large courtyard adjacent to the alley and single family neighbors. This option diminishes the impact of the building on the single family neighbors the most of the three options. It provides a direct correlation to the adjacent project, and is most suited to the site.

ADVANTAGES

- Massing visually breaks up building scale
- Maximizes Unit Count
- Maximizes number of units with view
- Softens transition to single family zone
- Integrates with existing topography
- Sensitive to adjacent apartment building to the south
- Courtyard has almost full sun
- Maximizes FAR

CHALLENGES

- Courtyard not facing view
- More north-facing units



CONCEPT C “REFINED AND REDUCED”

SUMMARY

The “Refined and Reduced” Concept C further reduces the massing. In conformance with the new Ordinance 124307, a story has been eliminated and the FAR has been reduced to comply with the base MR zoning. The podium has been sunken into the ground further and the wing closest to the SF 5000 zone has been pulled back. The building steps along all facades at the upper levels to reduce bulk. Both the rear yard and facade length departures have been removed. The Northeast corner has been lowered one floor to diminish the perceived height from the street.

ADVANTAGES

- Massing visually breaks up building scale
- Maximizes number of units with view
- Softens transition to single family zone
- Street front is more enlivened with 2nd floor units
- Integrates with existing topography
- Street facade more pedestrian in scale
- Sensitive to adjacent apartment building to the south
- Courtyard has almost full sun
- Maximizes FAR
- Departures minimized

CHALLENGES

- Courtyard not facing view
- More north-facing units

CONCEPT SUMMARY

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NORTH ELEVATION

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WEST ELEVATION FACING ALLEY

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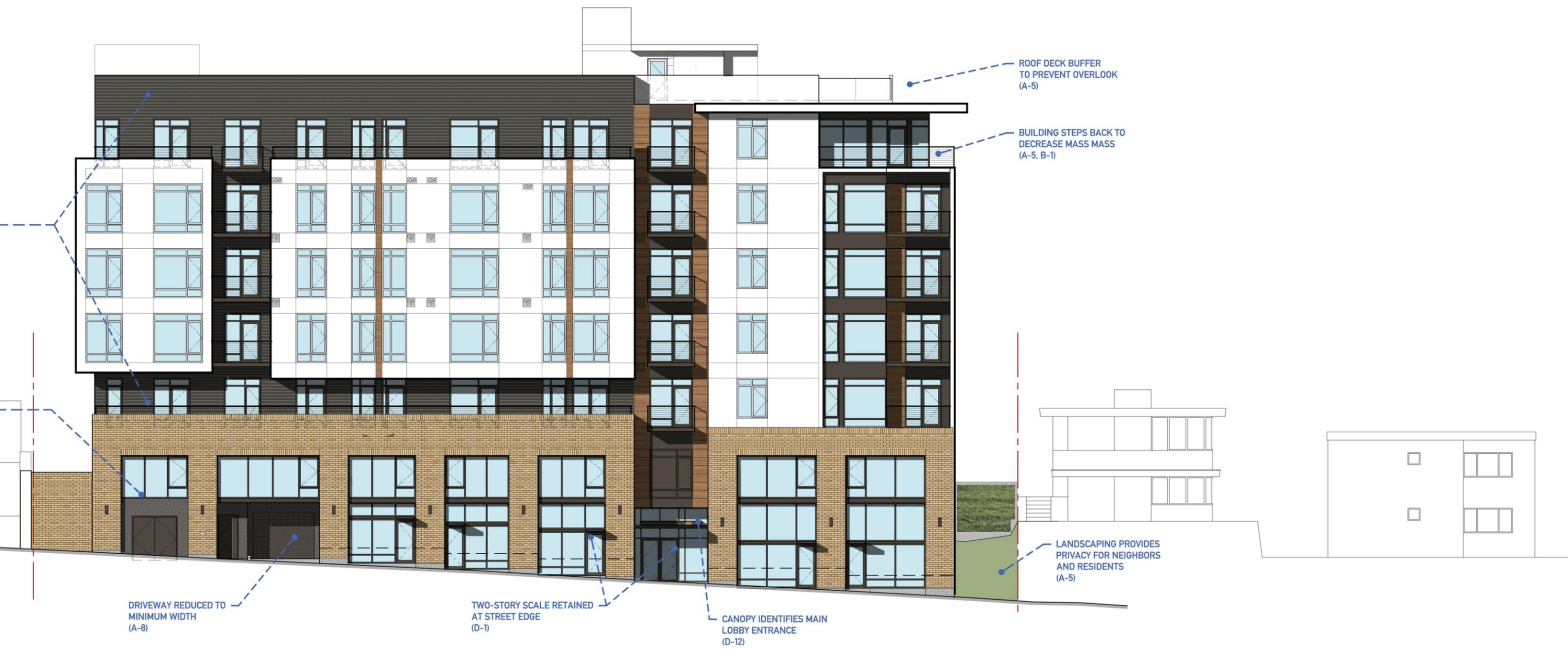
SOUTH ELEVATION

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EAST ELEVATION

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ROOF DECK BUFFER
TO PREVENT OVERLOOK
(A-5)

BUILDING STEPS BACK TO
DECREASE MASS MASS
(A-5, B-1)

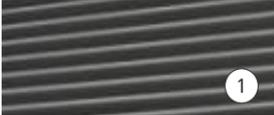
LANDSCAPING PROVIDES
PRIVACY FOR NEIGHBORS
AND RESIDENTS
(A-5)

DRIVEWAY REDUCED TO
MINIMUM WIDTH
(A-8)

TWO-STORY SCALE RETAINED
AT STREET EDGE
(D-1)

CANOPY IDENTIFIES MAIN
LOBBY ENTRANCE
(D-12)



-  1 AEP SPAN NU-WAVE CORRUGATED METAL SIDING : CHARCOAL COLOR
-  2 FIBER-CEMENT PANEL : WHITE COLOR
-  3 VINYL WINDOW: WHITE COLOR
-  4 ABET LAMANITI WOOD PANEL : 754 MEG-Sei
-  5 METAL PANEL : GLOSS WHITE COLOR
-  6 MUTUAL MATERIALS BRICK : MOJAVE BLEND
-  7 STOREFRONT DOOR/WINDOW : CHARCOAL COLOR
-  8 VINYL WINDOW : CHARCOAL COLOR
-  9 CONCRETE : NATURAL COLOR
-  10 COLORED CONCRETE : BLACK COLOR
-  11 LOUVER/FLASHING : BLACK COLOR
-  12 METAL BALCONY : BLACK COLOR
-  13 2X2 PAVORS : TAN COLOR
-  14 HORIZONTAL CEDAR FENCE: NATURALL COLOR

EXTERIOR MATERIALS

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LOOKING NORTH ON SW AVALON WAY

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VIEW FROM SW AVALON WAY

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ALLEY LOOKING SOUTH

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WEST BUILDING FACADE

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ALLEY LOOKING NORTH

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LOOKING SOUTH ON SW AVALON WAY

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LOOKING SOUTH ON SW AVALON WAY

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LOOKING NORTH ON SW AVALON WAY

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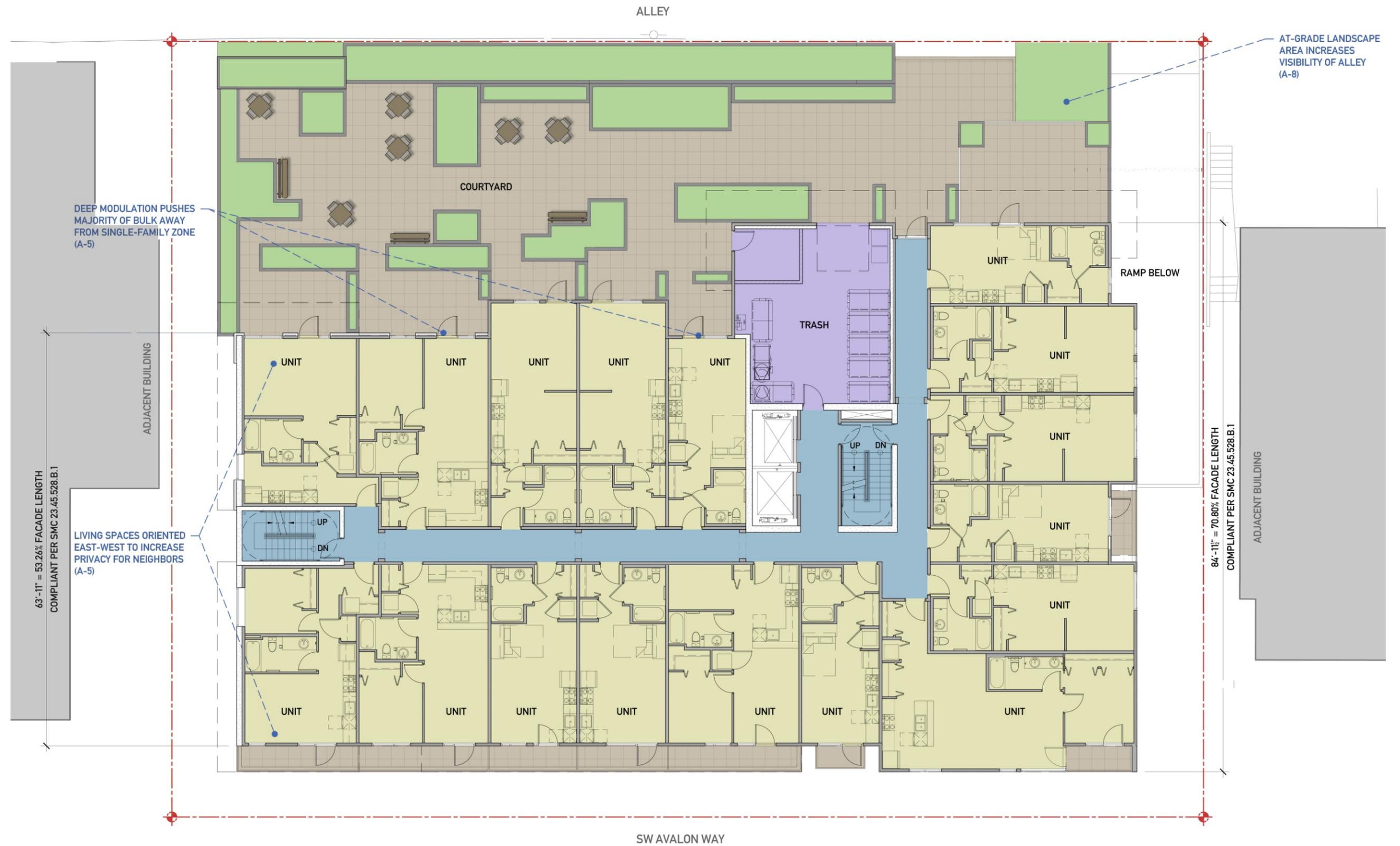
LEVEL 1 FLOOR PLAN - SW AVALON WAY

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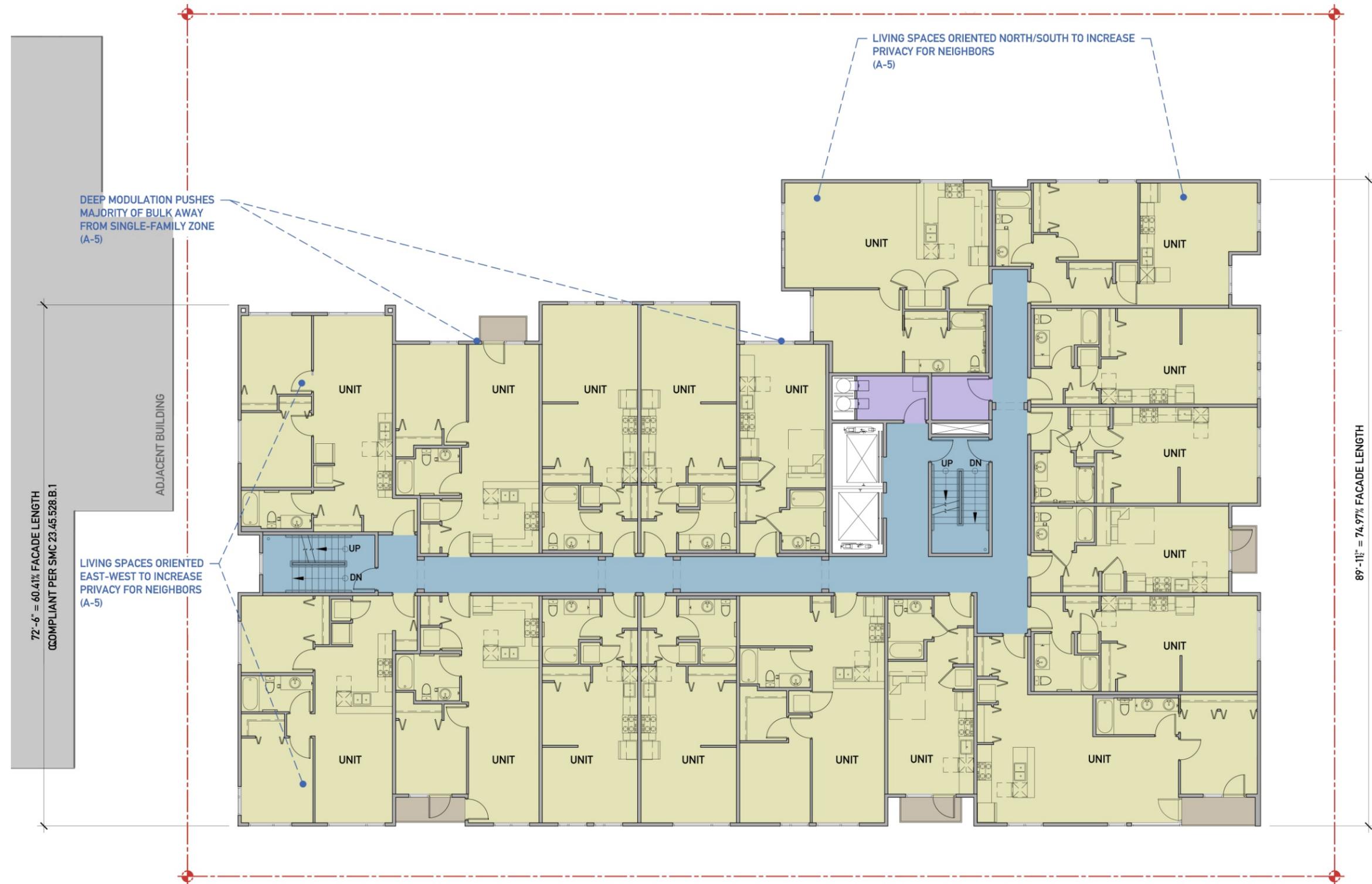
LEVEL 2 FLOOR PLAN

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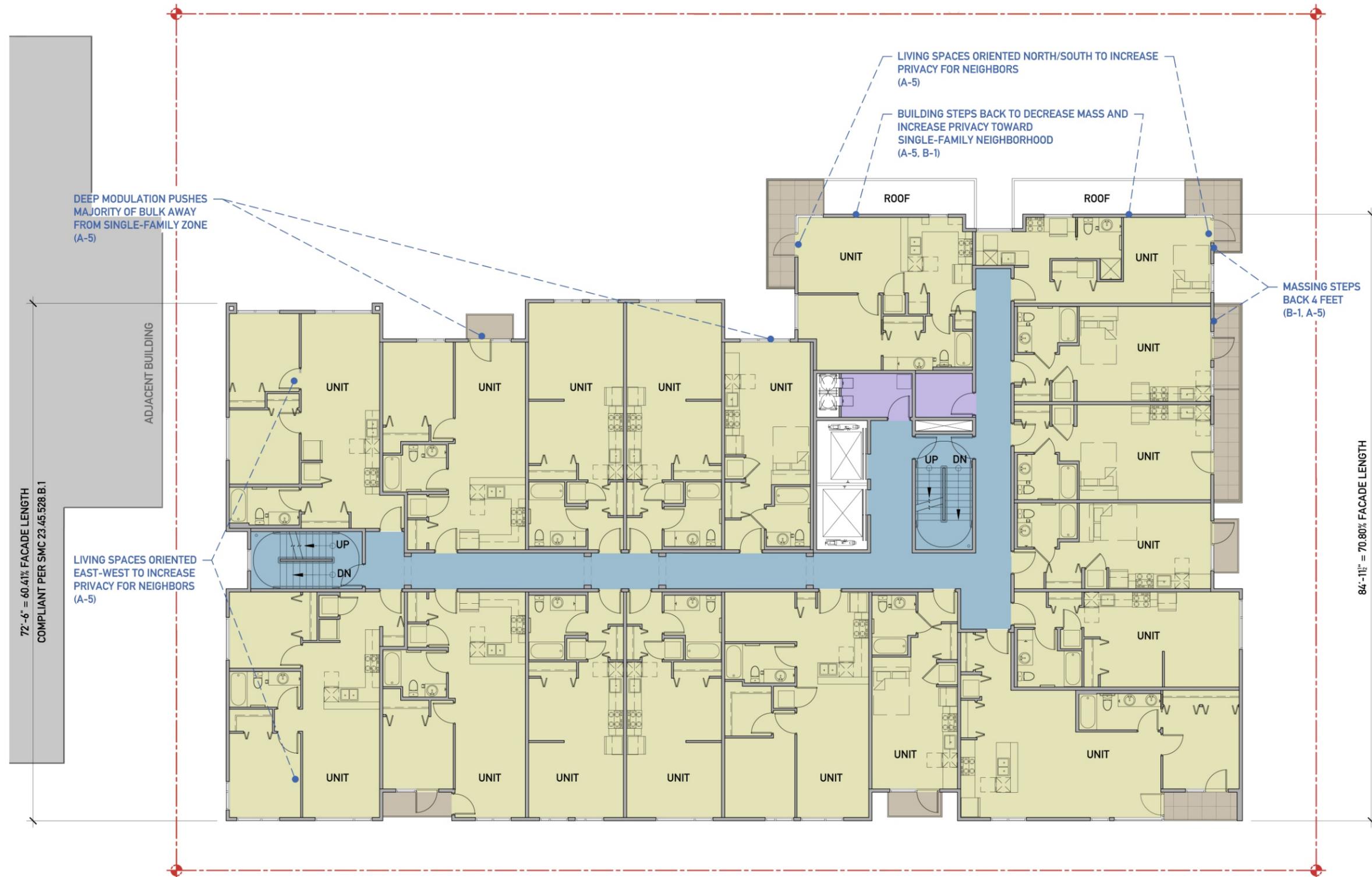
LEVEL 3 FLOOR PLAN - COURTYARD LEVEL

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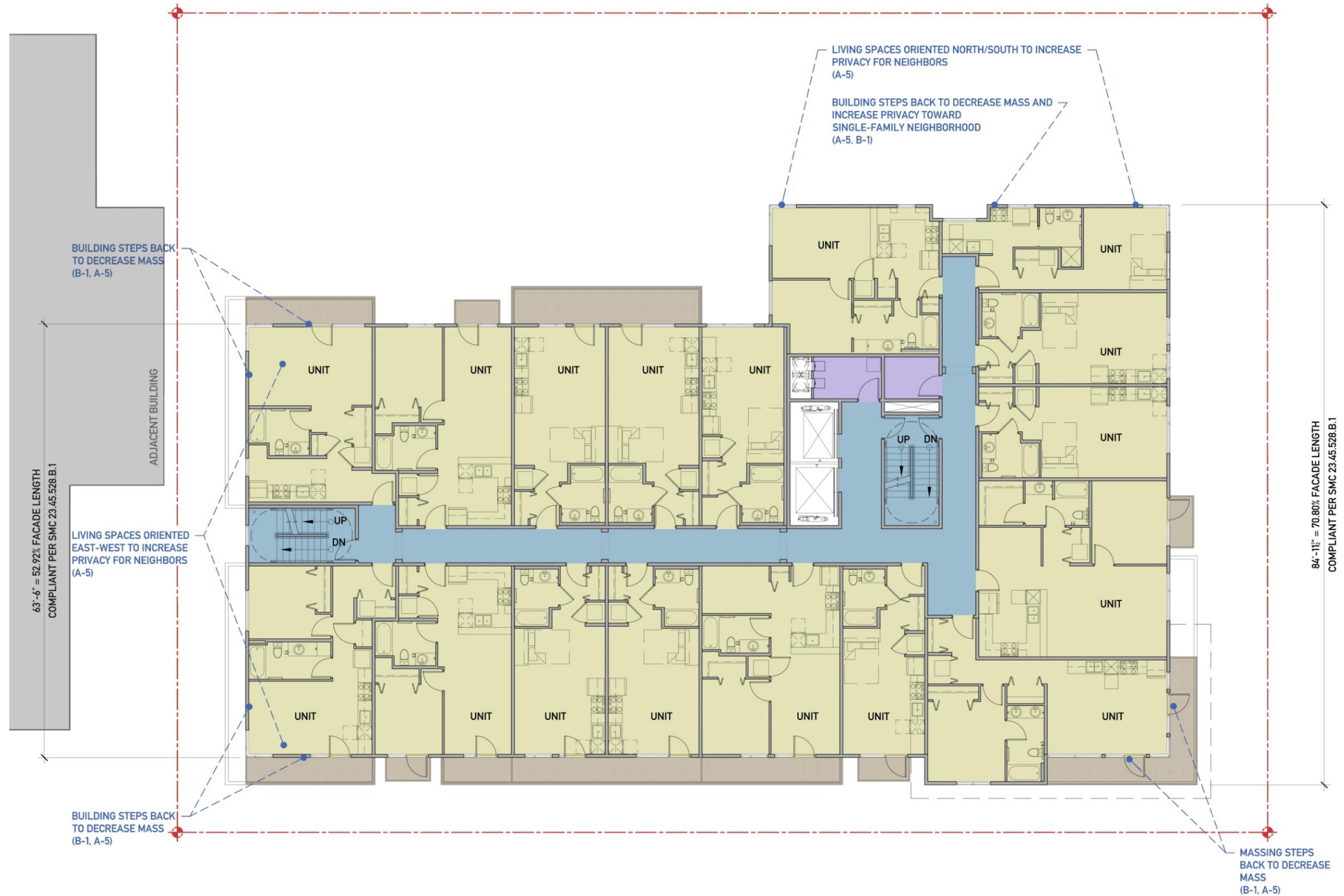
LEVELS 4 AND 5 FLOOR PLAN

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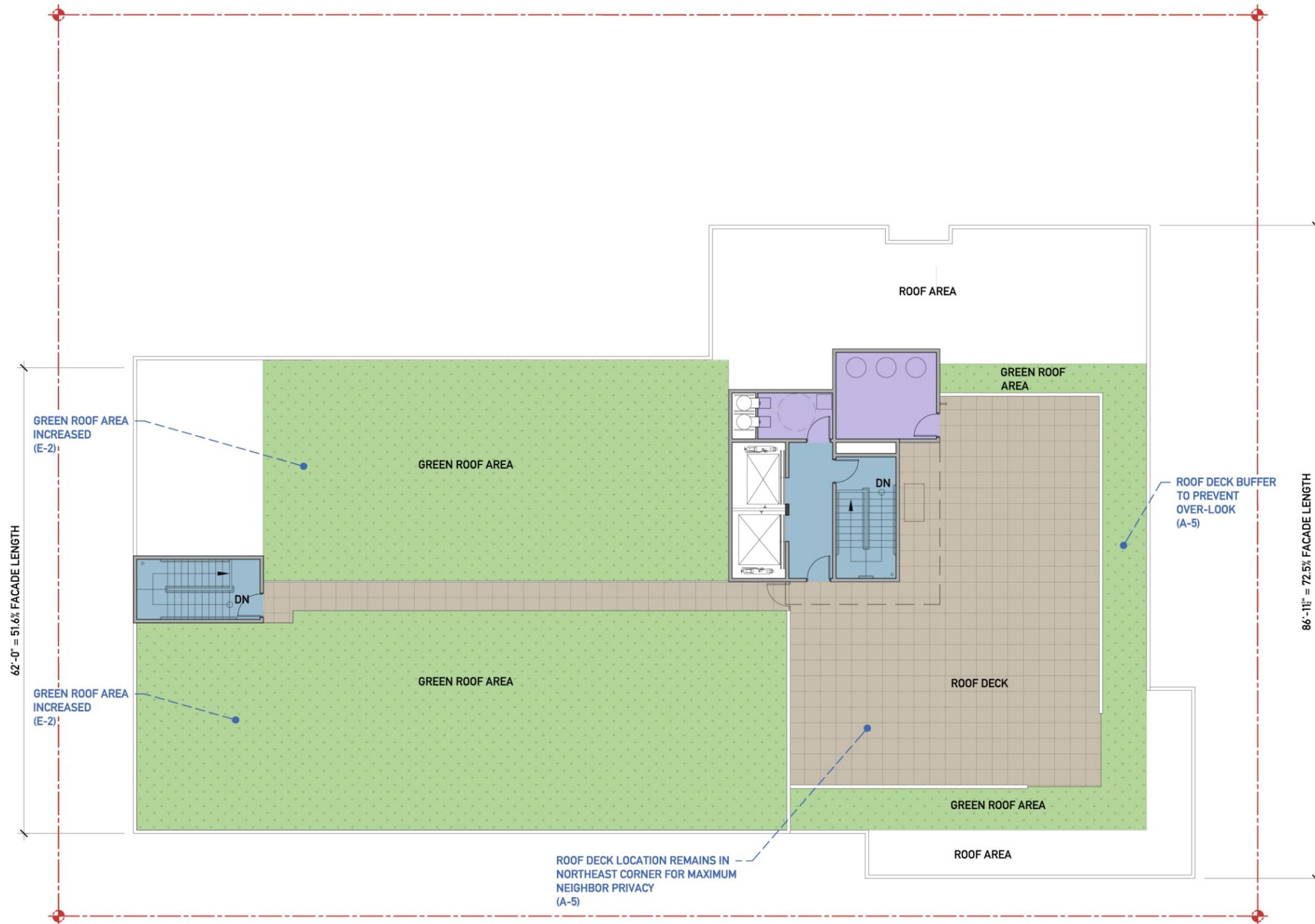
LEVEL 6 FLOOR PLAN

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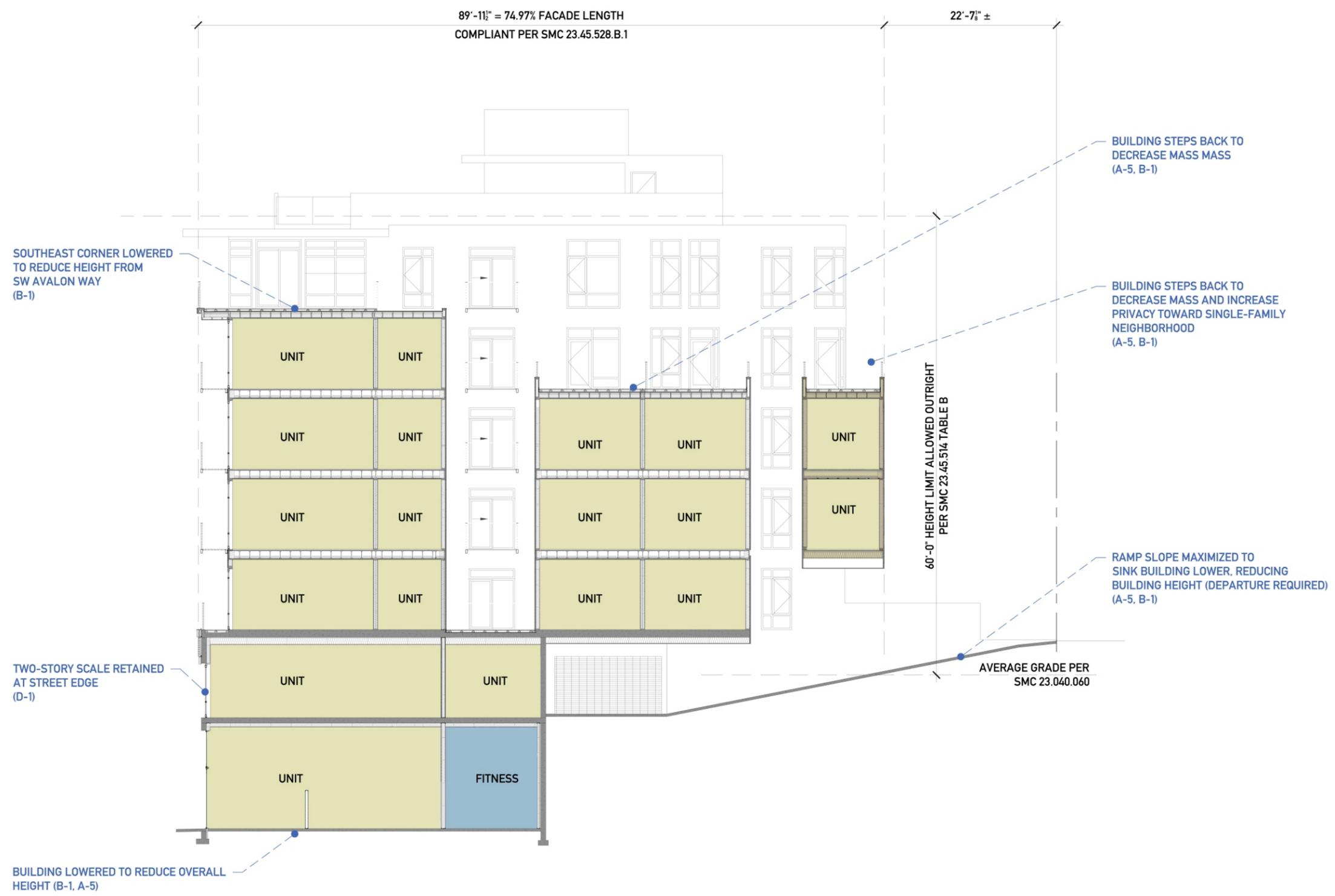
LEVEL 7 FLOOR PLAN

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ROOF PLAN

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BUILDING SECTION

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CODE REFERENCE / REQUIREMENT

DEPARTURE REQUEST:

RATIONAL FOR DEPARTURE:

SMC 23.45.536

Allow a second access from SW Avalon Way

C. Access to parking.

1. Alley access required. Except as otherwise expressly required or permitted in subsections C or D of this Section 23.45.536, access to parking shall be from the alley if the lot abuts an alley and one of the conditions in this subsection 23.45.536.C.1 is met.

- The alley is improved to the standards of subsection 23.53.030.C;
- The development gains additional FAR pursuant to Section 23.45.510.C; or
- The Director determines that alley access is feasible and desirable to mitigate parking access impacts, improve public safety, and/or maintain on-street parking capacity.

4. On steeply sloping lots, the Director may permit the use of both an alley and a street for access, provided that the following conditions are met:

- access from the street is to common parking garage in or under the structure, that is underground or extends no more than 4 feet above grade.
- the siting of development results in an increased Green Factor score, larger ground-level amenity areas, and/or reduced surface parking area than if alley access alone is used.
- In LR zones, if the project uses both the alley and street for access to parking other than required barrier-free parking spaces, the project does not qualify the higher FAR limit in Section 23.45.510.B.

This departure would allow a second access from SW Avalon Way in addition to access from the alley. SMC 23.45.536 C.4 states; *On steeply sloping lots, the director may permit the use of both an alley and a street for access, provided that the following conditions are met: a. Access from the street is to a common parking garage in or under the structure, that is underground or extends no more than 4 feet above grade. B. The siting of development results in increased green factor score, larger ground floor amenity areas, and/or reduced surface parking area than if alley access alone is used.*

The garage level accessed from SW Avalon Way meets the provisions of SMC 23.45.536 C.4 as it is a common parking garage under the structure and the level which would be accessed by the second access from SW Avalon Way extends no more than four feet above grade. The departure also increases the amenity area of the project and reduces surface parking, as the proposed pedestrian courtyard would become surface parking in order to maintain the same parking count. Although no parking is required for this site, the proposed project provides 61 under-building parking spaces hidden from view. Eliminating surface parking and replacing it with pedestrian amenity area benefits the project's residents as well as the neighbors to the south and west who abut and look out at the courtyard the departure allowance also increases safety in the alley by providing more "eyes on the street" than a parking area would.

SMC 23.54.030

Allow 20% driveway slope

D. Driveway. Driveway requirements for residential and nonresidential uses are described below. When a driveway is used for both residential and nonresidential parking, it shall meet the standards for nonresidential uses described in subsection 23.54.030. D.2.

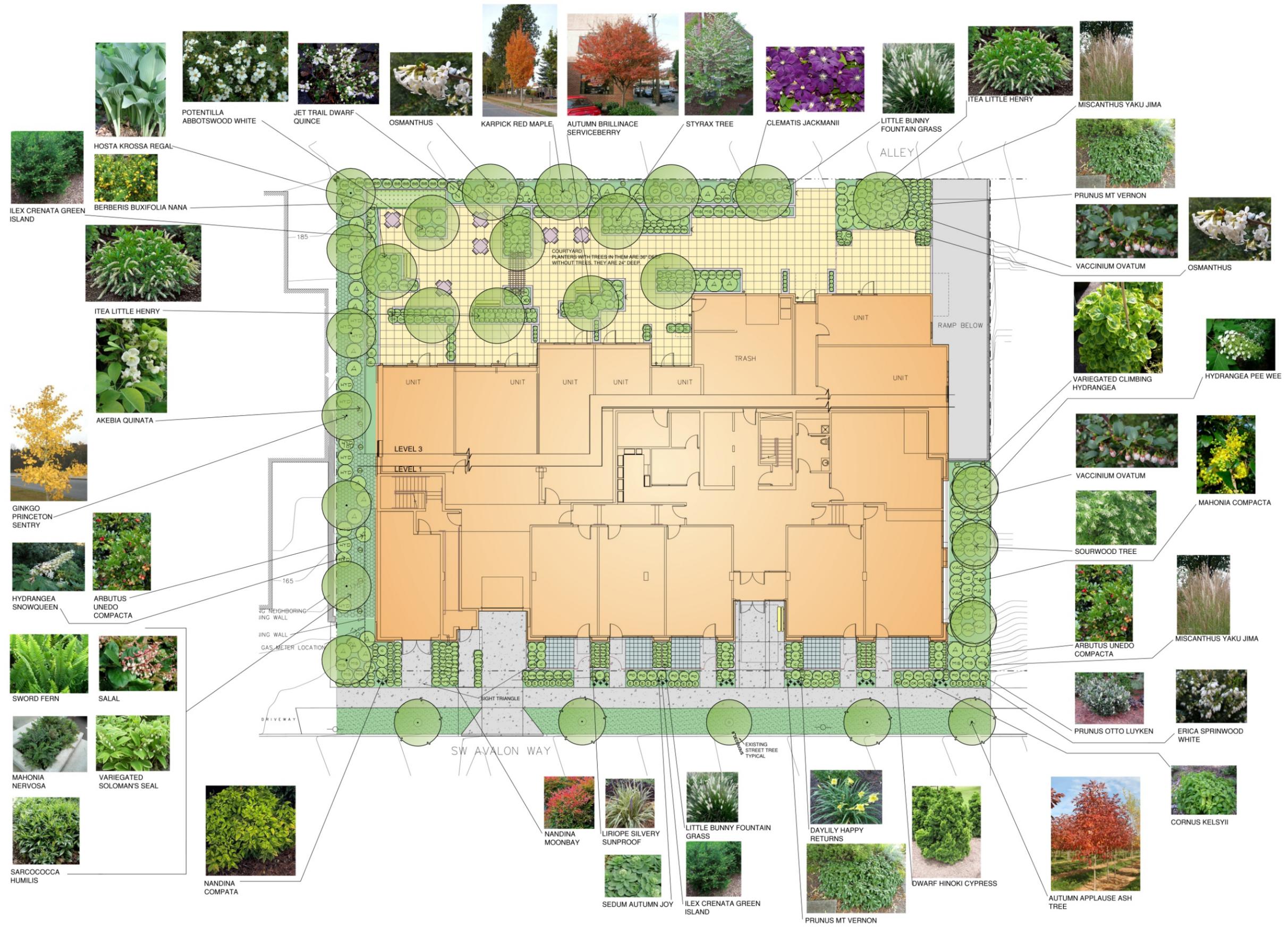
3. Driveway slope for all uses. No portion of a driveway, whether located on a lot or on a right-of-way, shall exceed a slope of 15 percent, except as provided in this subsection 23.54.030.D.3.

The steeper ramp slope allows the building to be sunken further into the ground and reduce the overall height of the buildings as seen from the single-family zone across the alley to the west.

DEPARTURE REQUEST

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caron



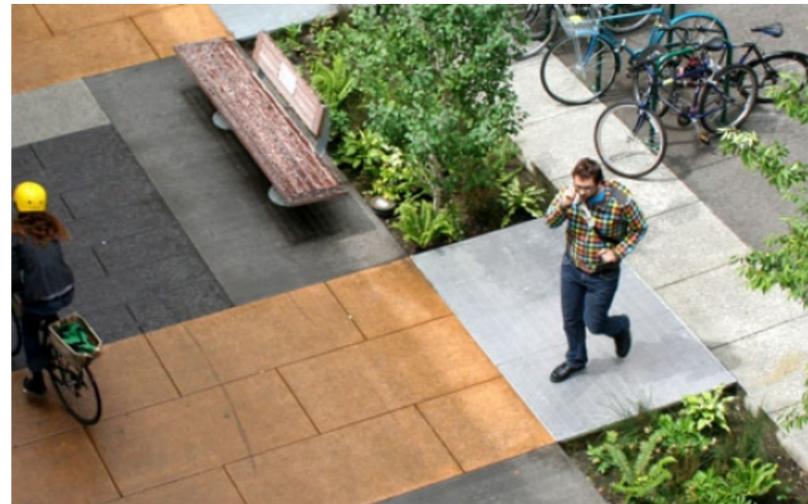
LANDSCAPE PLAN

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STREET LEVEL

The walks leading to the pedestrian entries will be treated with a texture or color to differentiate them from the public sidewalk. Landscaping will help to soften the transition to the building from the public way.

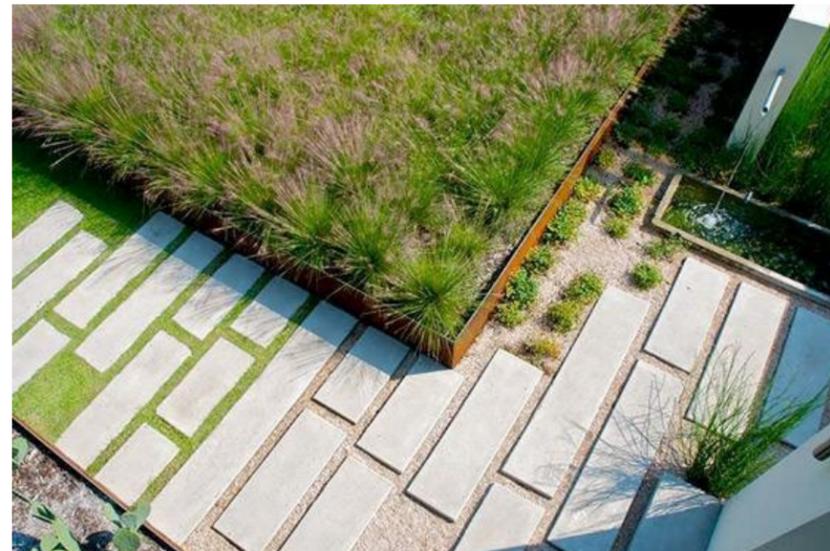


Response to Architectural Guidelines

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Private and Common Courtyard

Private landscaping areas will be provided at for the courtyard level units. Plantings and hardscape will be used to enhance private decks. A lawn area will further soften the hardscape along the alley and provide a place for relaxation. In addition to creating room for patios and landscaping, setting the building back from the property line ensures that the residents have ample daylight.



Response to Architectural Guidelines

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① SLV Lighting Rox Pro G8.5



② ERCO Lighting Midipoll



③ SLV Lighting Downunder LED



④ 4" Recessed LED Can Light



EXTERIOR LIGHTING

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VOLTA



BREMERTON WATERFRONT CONDOMINIUMS



23RD AND MADISON



306 QUEEN ANNE AVE N (Currently under construction)



LEGACY AT PRATT PARK



MURIEL'S LANDING

ARCHITECT'S RELATED PROJECTS

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THANK YOU FOR YOUR CONSIDERATION

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