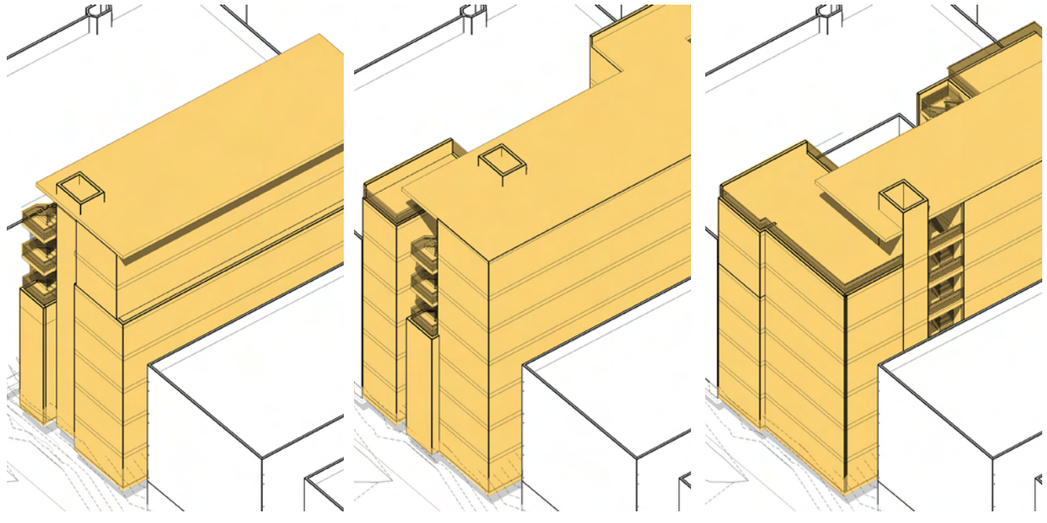
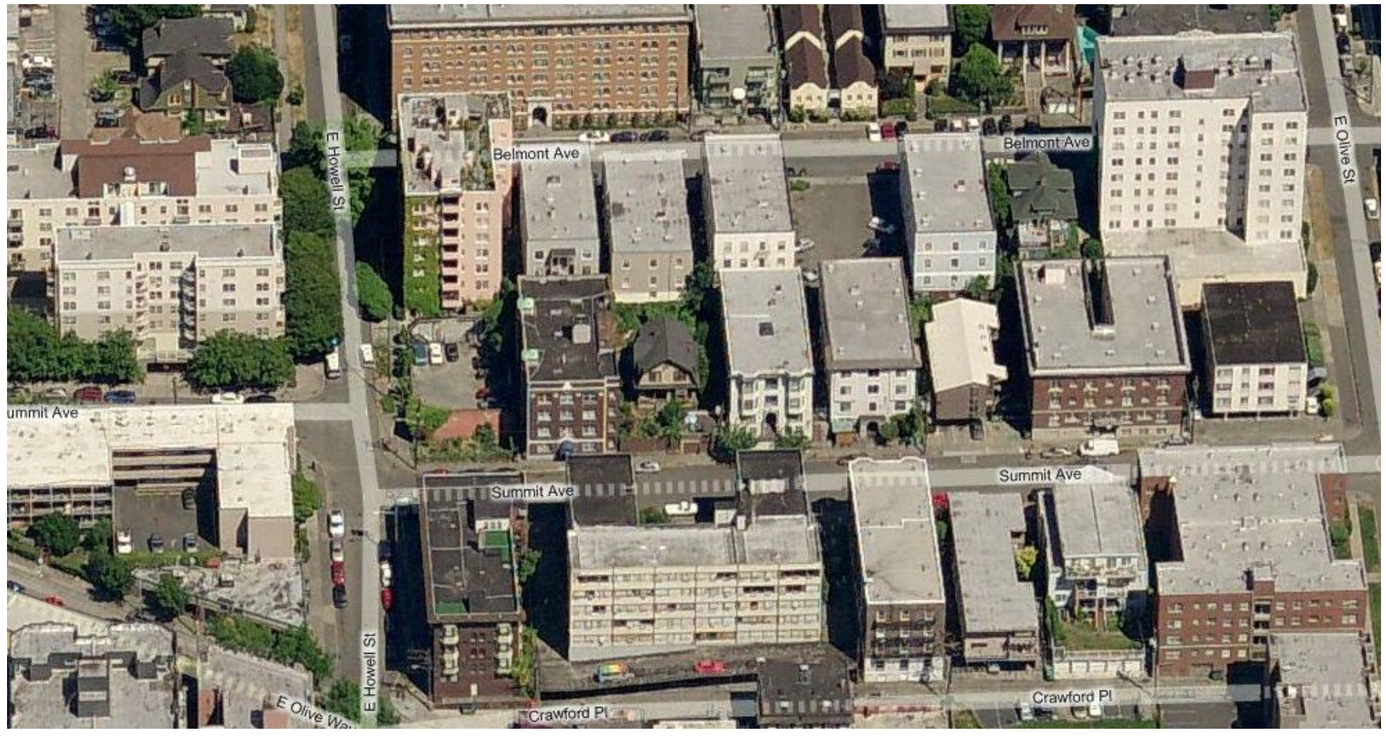
Summit Apartments

1728 Summit Avenue





PROJECT INFORMATION

Address: 1728 Summit Avenue

DPD Project #: 3013254

Owner / Developer: Triad Capital Partners
Brett Allen, Senior Vice President
brett@triadcappartners.com

Architect / Applicant: grouparchitect
2222 eastlake avenue east
Seattle, WA 98102
Phone (206) 365-1230
Fax (206) 365-1857

Contact: Brian Palidar AIA CSBA
brian@grouparch.com

DEVELOPMENT OBJECTIVES

Please describe the applicant's development objectives, indicating types of desired uses, structure height, number of residential units, amount of commercial square footage, and number of parking stalls, etc.

The proposed project is a multi-story, residential apartment building containing residential apartment units in the upper levels with bike parking at grade. The height of the building is anticipated to reach the maximum limit of 60 feet. Refuse / recycling is proposed in the south setback. No vechicular access is proposed due to the small site, lack of an alley and the proximity to tranist. To meet development objectives and to meet the need of affordable housing smaller more dense 'economy' style units are proposed. Amenitys anticipated to be provided in roof deck open space features as options allow. Strong building presence at the street is anticipated to limit access to the site and provide extra security to tentants who access units via exterior corridors.

The development objectives for this project are as follows (all values are approximate):

Number of residential units:	45+ Units
Number of parking stalls:	None
Area of residential levels:	16,000sf + F.A.R.
Area of parking level:	None

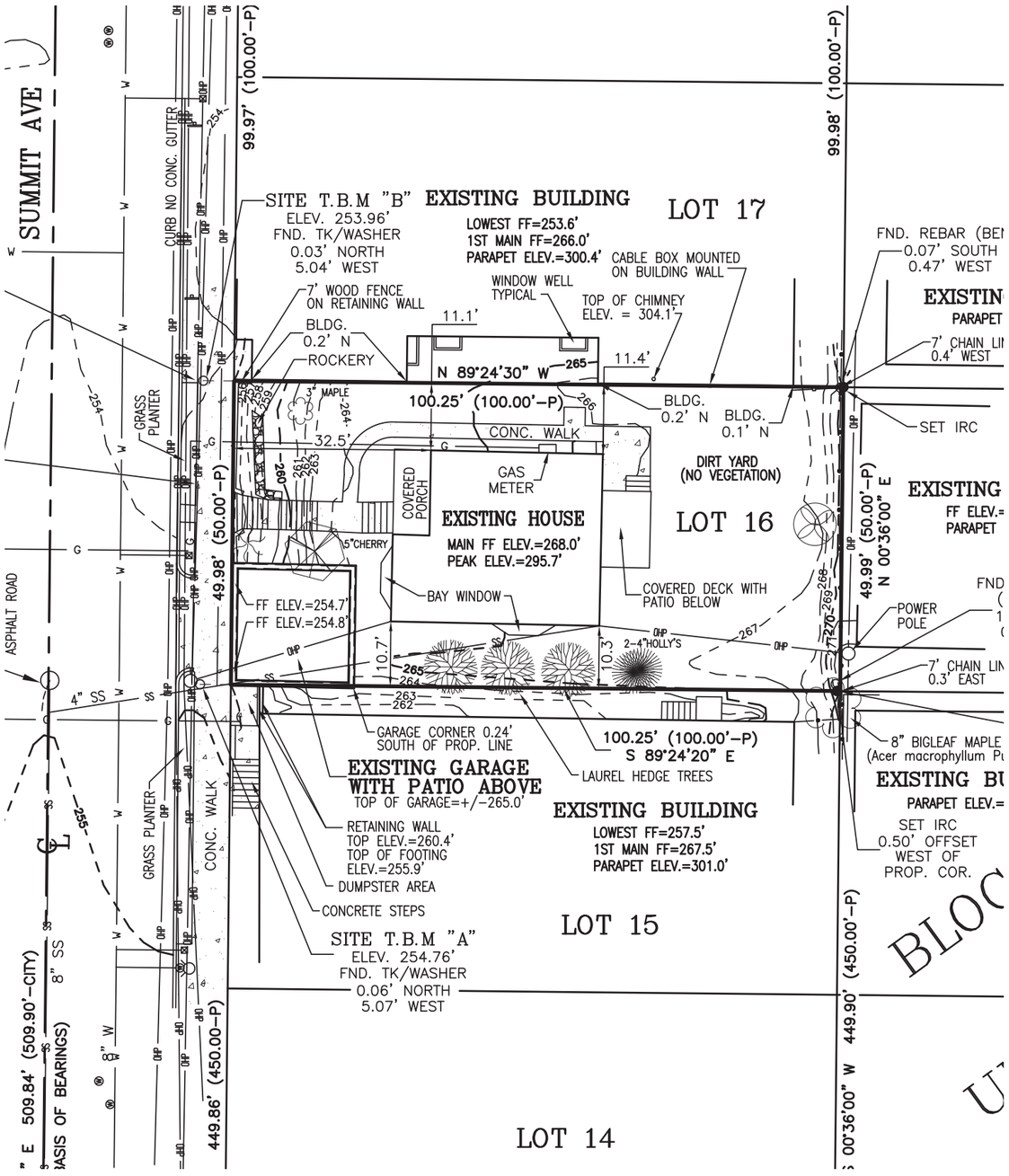
See departure study for discussion of assumed design departures.

SUSTAINABILITY OBJECTIVES

Sustainability objectives will be determined based on selected development option and need of project to extend above 60 feet height limit to attain F.A.R.

EXISTING BUILDING

Existing residential building on site has been analyzed for historical relevance in accordance with Department of Neighborhood Attachment A requirements and initial findings indicate that it is not historically significant. The complete report will be included with the MUP application as well as forwarded to Department of Neighborhoods for their review.



SITE CONTEXT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ADJACENT BUILDINGS

1. 1736 Summit Ave - Summit Vista Apartments
2. 1726 Summit Ave - Multi-Family Housing
3. 1733 Belmont Ave - Pioneer Human Services
4. 1743 Summit Ave - Parking Lot
5. 1743 Summit Ave - The Morris Apartments
6. 1723 Summit Ave - Multi-Family Housing
7. 1727 Summit Ave - Multi-Family Housing
8. 612 E Howell St - Single Family Residence
9. 506 E Howell St - Multi-Family Housing
10. 1712 Belmont Ave - Multi-Family Housing
11. 1712 Summit Ave - Angletree Apartments

STREET ELEVATIONS

STREET ELEVATION A: SUMMIT AVE LOOKING EAST

STREET ELEVATION B: SUMMIT AVE LOOKING WEST

SITE ANALYSIS

NEIGHBORHOOD CONTEXT

Please describe neighboring development and uses, including adjacent zoning, physical features, existing architectural and siting patterns, views, community landmarks, etc.

Capitol Hill Context

The site is located on the southern edge of the Capitol Hill Urban Cnter Village, and just north of the Pike/Pine Urban Center Village boundary. The site is in a predominantly multi-family residential use area along most of the adjoining strets. Located one and a half blocks to the east is E. Olive Way a primary access corridor to area via downtown. This arterial supports transit and most of the commerical uses. E. Denny Way located to the north provides similar amenities and context as E. Olive Way.

Immediate Neighborhood Context:

The predominate architecture of the neighborhood is 3 to 4 story multi-family apartments. Several taller apartments buildings dot the area, an 8 and 9 story apartment buildings are located within the same block as the subject parcel. Most of these apartment buildings have been developed over the last century leading to a wide rage of facade treatments ragining from brick, wood, and stucco.

Adjacent and Nearby Streets:

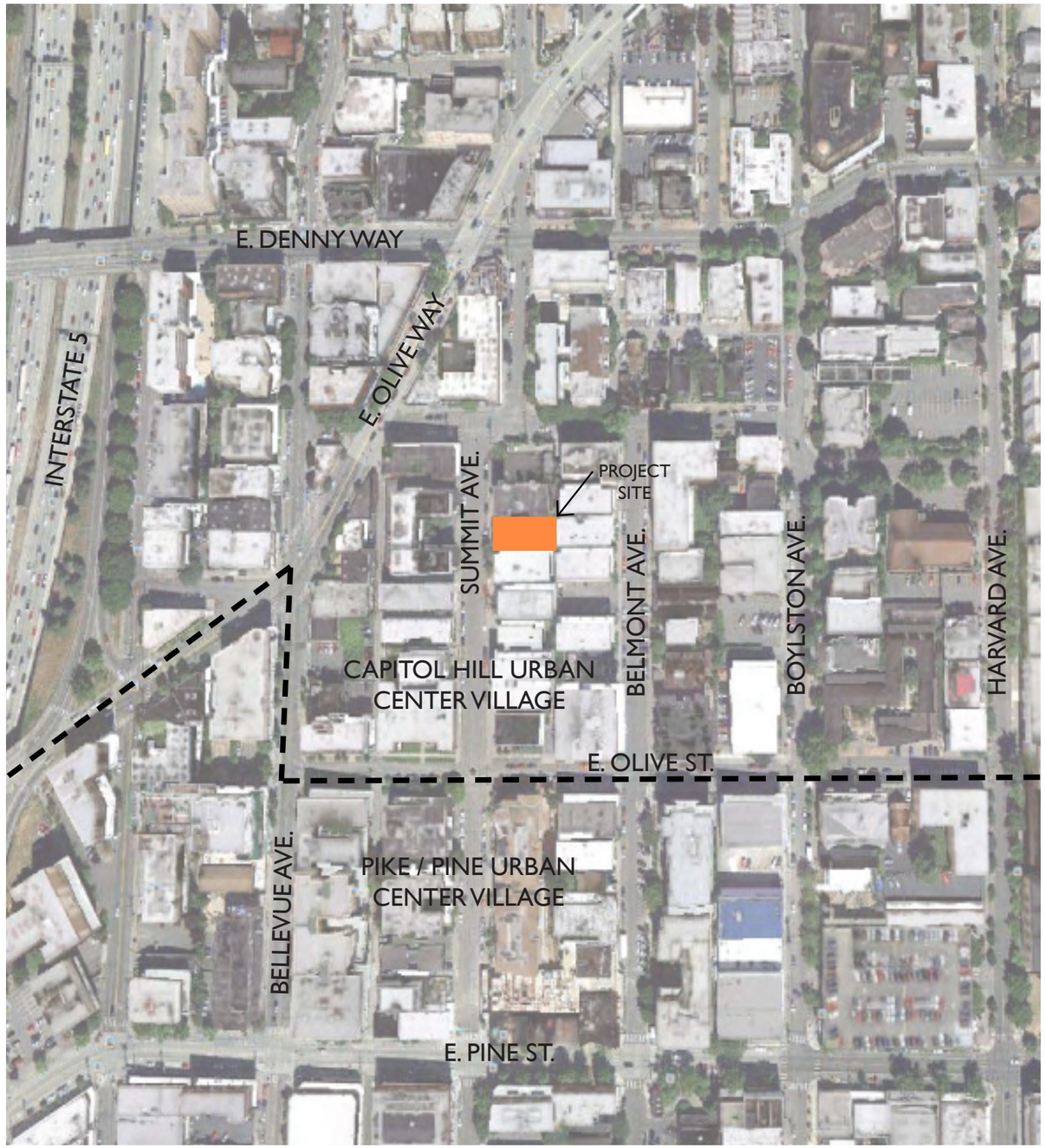
Access to the site is limited to Summit Ave. There is no alley access and the subject property is bounded on three sides by existing development. The major intersection of E. Denny Way and E. Olive Way is located approximately a block and a half to the north. None of the immediately adjacent streets have dedicated bicycle lanes and most of the pedestrian traffic is by foot.

Views and Amenities:

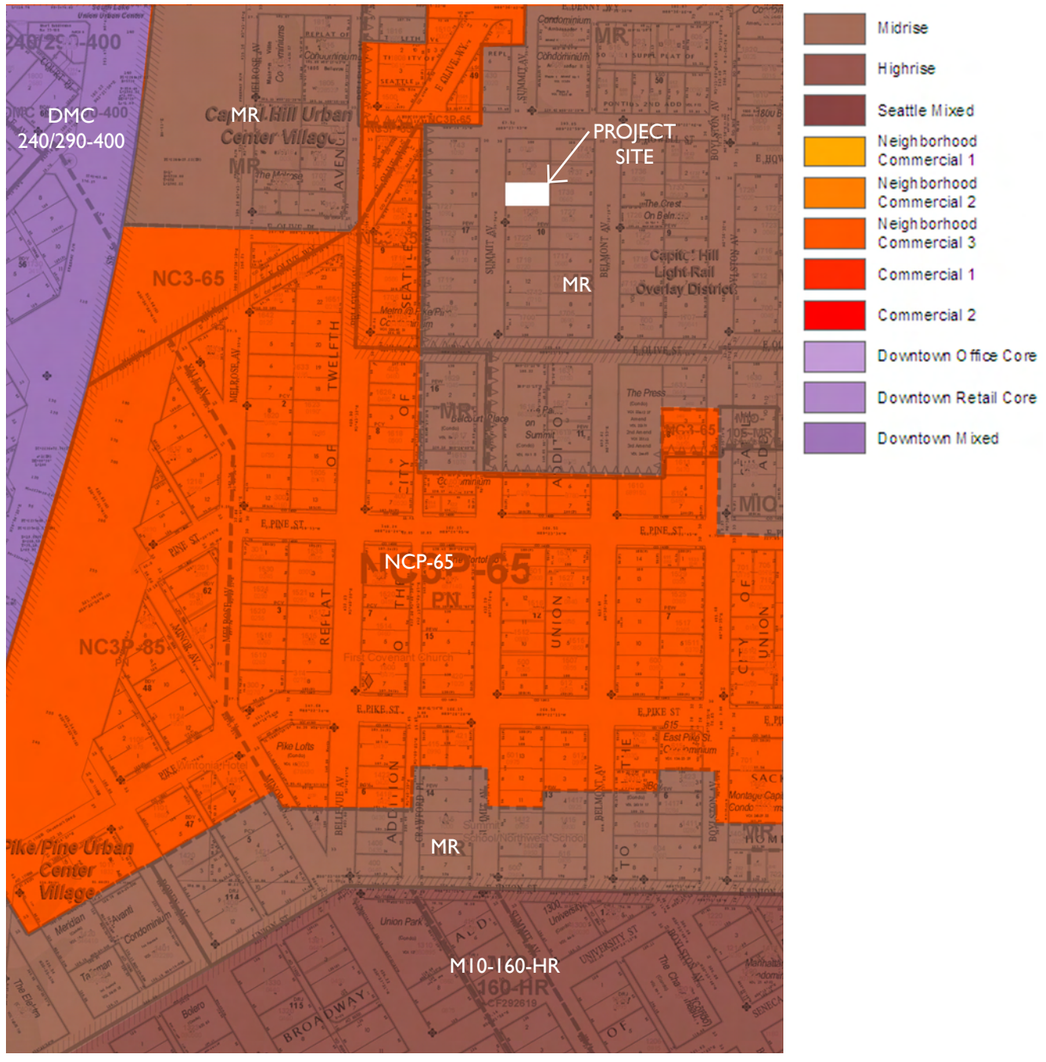
The project will benifit from views the the west including the downtown core, elliot bay and the space needle. The views will likely only be accessible from the upper 4-6 stories. The majorty of pedestrian focused amenites will revolve around the access to transportation and neighborhod eateries located within a one block walk.

Future Projects & Additions to Neighborhood Context:

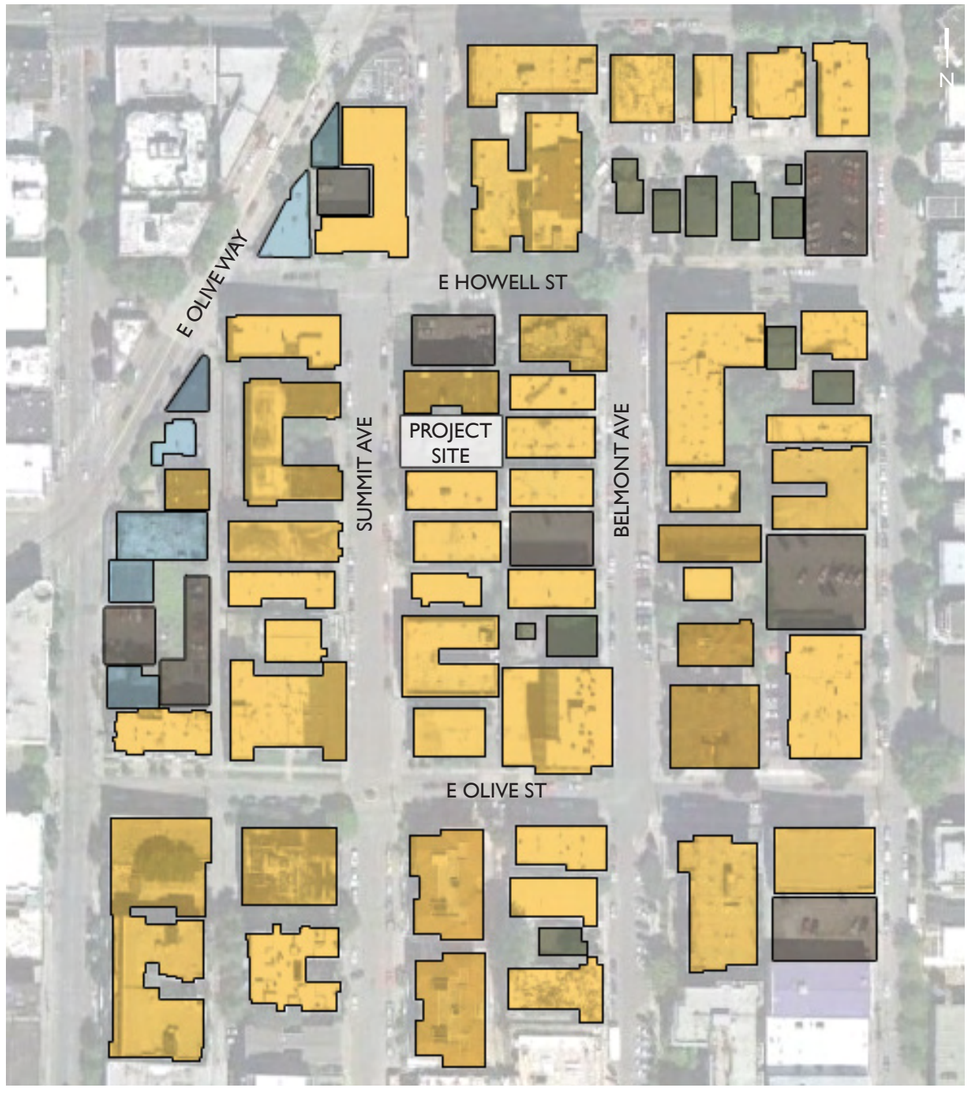
According to city records no new projects are occurring in the immediate vicinity that would have impact on the streetscape surrounding the project site. However, the majority of the surrounding multi-family apartment builings are under developed by current land use code. The density in the neighborhood will substantially increase as these parcels are developed.



AERIAL PHOTOGRAPH & OVERLAY AREAS

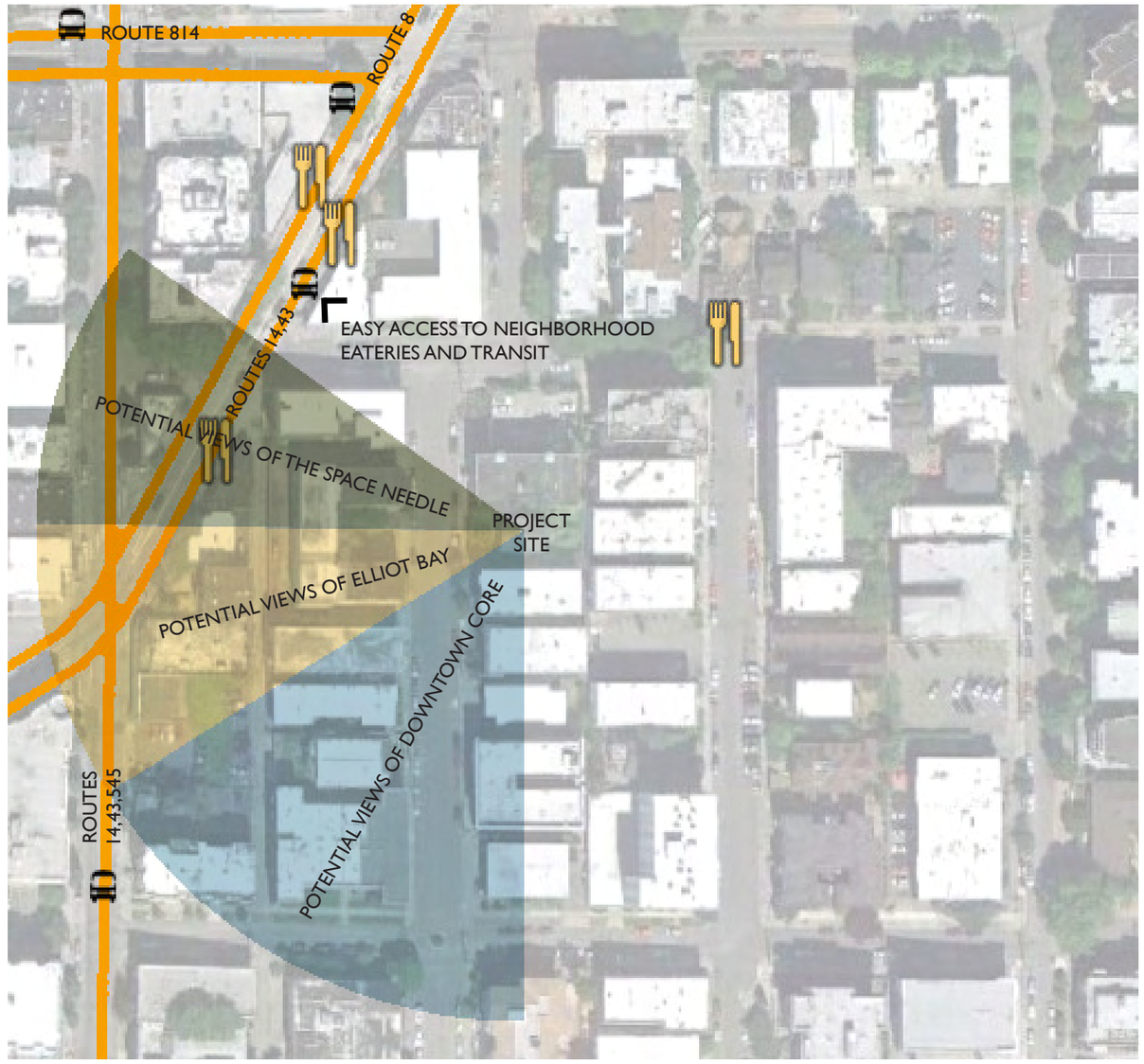


ZONING MAP

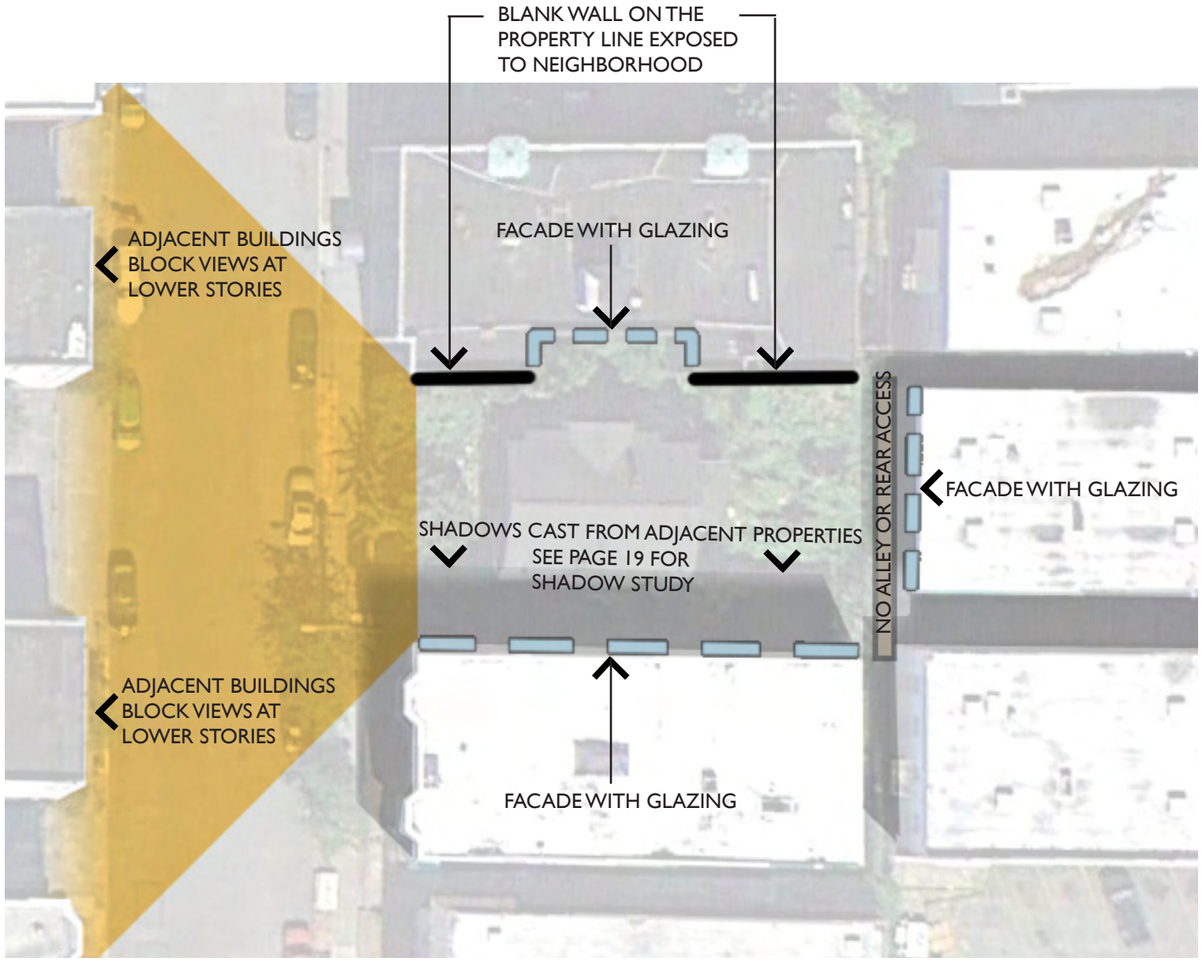


ADJACENT USE DISTRIBUTION

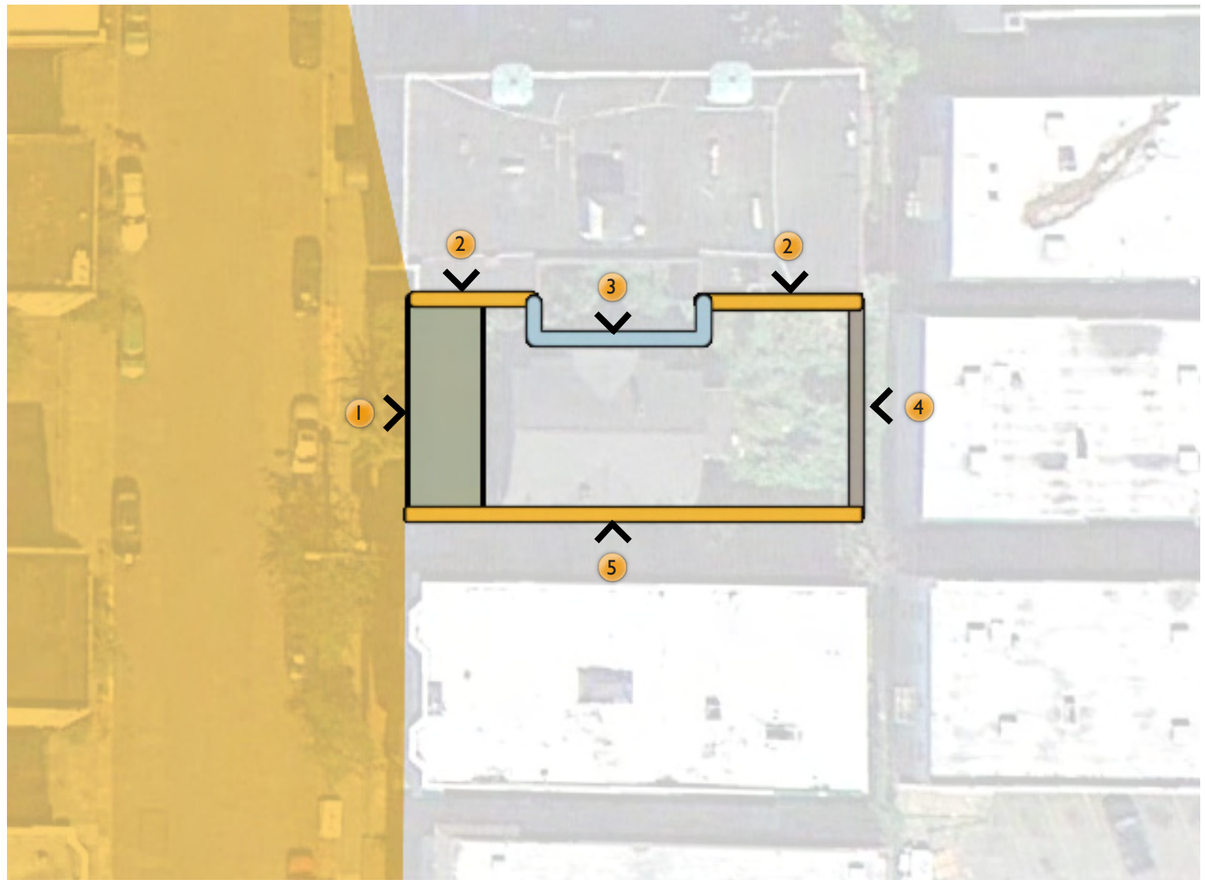
SITE CONSTRAINTS - OVERVIEW



SITE OPPORTUNITIES



SITE CONSTRAINTS - LOCALIZED PROJECT AREAS



DESIGN RESPONSES AND STRATEGIES

1. LOCATE AND ELEVATE AMENITY SPACES TO AVOID VIEW BLOCKING TO WEST
2. BLANK WALLS CAN BE MITIGATED BY PLACING PROJECT MASSING AGAINST THEM
3. COURTYARD OPPORTUNITY DEVELOPED TO RESPECT GLAZING FOR BOTH BUILDINGS
4. LACK OF ALLEY AND EXISTING LACK OF BUILDING SETBACKS AT ADJACENT STRUCTURE SUPPORTS URBAN RESPONSES. SMALL SETBACKS LIMITS GROUND LEVEL OPEN SPACE
5. MAXIMIZE SOUTHERN SETBACK AWAY FROM SOUTHERN SHADOWS AND PROTECT ADJACENT GLAZING

ADJACENT FACADES / VIEW ANALYSIS

A) SOUTH BUILDING FACADE

PROJ

B) NORTH BUILDING FACADE

F) VIEW ANALYSIS - SOUTH FACADE @ PROPERTY LINE

E) VIEW ANALYSIS - SOUTH PROPERTY LINE

D) VIEW ANALYSIS - NORTH PROPERTY LINE

C) NORTH BUILDING FACADE - INSET AT PROPERTY LINE

ZONING ANALYSIS (SEATTLE LAND USE CODE, current edition)

SEATTLE MIXED ZONING (23.45)

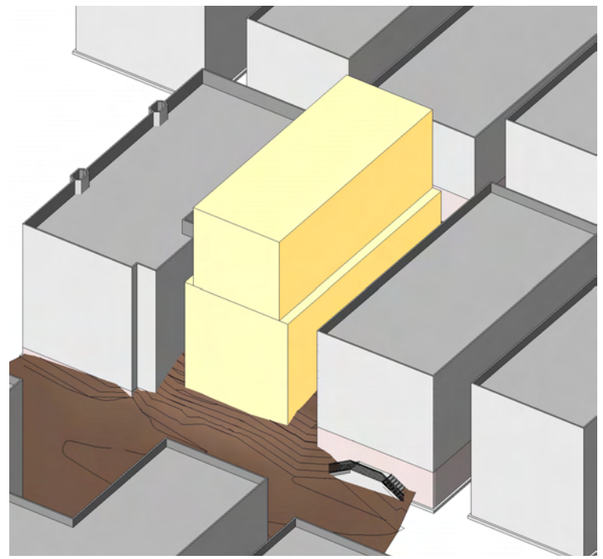
23.45.510.D	FAR limited to 3.2 for MR Structures
23.45.510.E4c	Portions of a story that extend no more than 4 feet above existing or finished grade are exempt from FAR limits
23.45.514B	60' height limit
23.45.514G1	The base height limit is increased by 5 feet if the number of stories in the structure that are more than 4 feet above existing or finished grade, whichever is lower, does not exceed six, and the FAR exemption provided in Section 23.45.510.E.4 is used
23.45.518	Setbacks: **(POSSIBLE DEPARTURE)** Front & Side setback from street lot lines: 7 foot average setback; 5 foot minimum setback Rear: 15 feet from a rear lot line that does not abut an alley Side setback from interior lot line: 7 foot average setback; 5 foot minimum setback below 42 feet in height 10 foot average setback; 5 foot minimum setback above 42 feet in height
23.45.522C	The required amenity area is equal to 5 percent of the total gross floor area
23.45.522D1	All units shall have access to a common or private amenity area
23.45.522D2b	No more than 50% of the amenity area shall be enclosed and shall be provided as common amenity area
23.45.524A2b	Landscaping shall meet a Green Factor score of 0.5 or greater.
23.45.529.B	Project is subject to Design Review and not subject to the provisions of the Design Standards

PARKING (23.54)

23.54.015	No required parking for Residential uses in multifamily zones within urban centers (Table B/L)
Chart E (D)	Bicycle parking required: 1 stall / 4 units for Residential uses

GENERAL ZONING / BUILDING INFORMATION

Parcel Number:	8804900735
Zoning:	MR
Lot Size:	5,000 s.f.
Overlay:	Capital Hill (Urban Center Village)
Mapped ECA:	None
Streets:	Summit Ave & E. Howell St
Codes:	Seattle Land Use Code (current edition), 2009 Seattle Building Code (SBC)

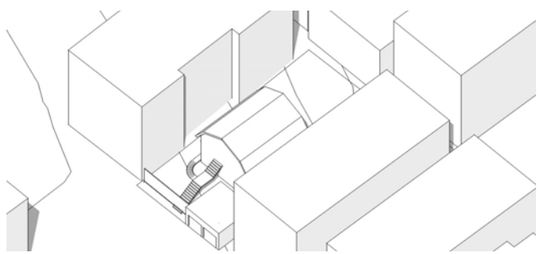


ZONING ENVELOPE

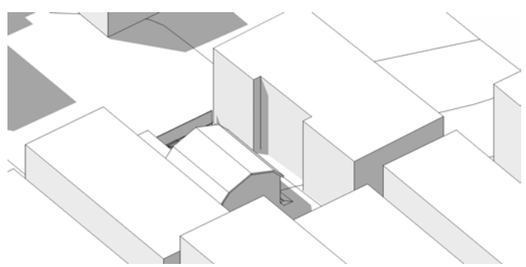
DEPARTURE REQUESTS ANTICIPATED

LAND USE CODE	CODE ITEM	DEPARTURE DESCRIPTION	DEPARTURE OPTIONS	DESIGN RATIONALE
SMC 23.45.518	Side setbacks	7' Average setback required with a min. of 5' from +0' to +42' above grade.	B,C	Existing adjacent building maintains a zero lot line blank facades in two areas. In effort to mitigate these facades the design proposes a similar condition while maintaing an open courtyard at the recessed area.
SMC 23.45.518	Side setbacks	10' Average setback required with a min. of 5' from +42' above grade to height limit	B,C	Similar height, bulk, and scale (i.e. full height walls with no modulation) are present in most nearby buildings including south building facing project site. Design proposal is consistent with architectural context.
SMC 23.45.518	Front setbacks	7' Average setback required with a minimum of 5'	B,C	The streetscape along Summit maintains a strong building presence near the sidewalk. The proposed development seeks to mimic this design rational
SMC 23.45.518	Rear Setbacks	Minimum 15' rear setback	B,C	Design proposes alignment of East facade with existing building to north. All adjacent buildings are minimal in all side / rear setbacks due to lack of an existing alley. Design proposal is consistent with architectural context.

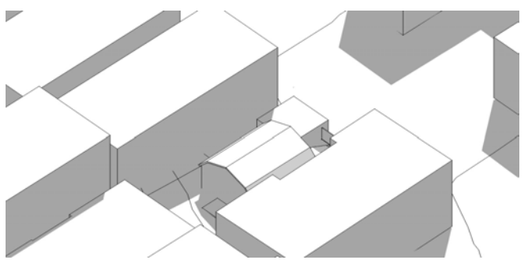
EXISTING SITE CONDITIONS



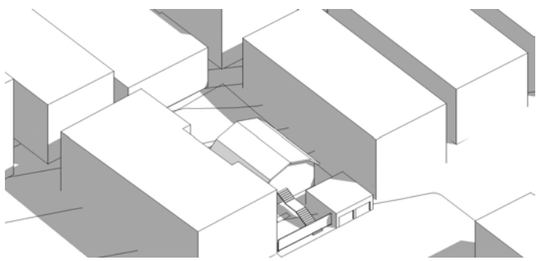
NORTHEAST CORNER



NORTHWEST CORNER



SOUTHWEST CORNER



SOUTHEAST CORNER

SUMMARY OF DESIGN REVIEW GUIDELINES

SITE PLANNING

A-1 *Responding to Site Characteristics*
The Siting of buildings should respond to specific site conditions and opportunities such as non-rectangular lots, location on prominent intersections, unusual topography, significant vegetation and views or other natural features. Solar orientation is also important consideration for this project.

A-2 *Streetscape Compatibility*
The siting of buildings should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way. Pedestrian friendly streetscapes are an important consideration for this project.

A-3 *Entrances Visible from the Street*
Entries should be clearly identifiable and visible from the street.

A-4 *Human Activity*
New development should be sited and designed to encourage human activity on the street. Graceful transition from street is an important consideration.

A-5 *Respect for Adjacent Sites*
Buildings should respect adjacent properties by being located on their sites to minimize disruption of the privacy and outdoor activities of residents in adjacent buildings.

A-6 *Transition Between Residence & Street*
For residential projects, the space between the building and the sidewalk should provide security and privacy for residents and encourage social interaction among residents and neighbors.

A-7 *Residential Open Space*
Residential projects should be sited to maximize opportunities for creating usable, attractive, well-integrated open space.

A-8 *Parking & Vehicle Access*
Siting should minimize the impact of automobile parking and driveways on the pedestrian environment, adjacent properties and pedestrian safety.

A-9 *Location of Parking on Commercial Street Fronts*
Parking on a commercial street front should be minimized and where possible should be located behind a building.

A-10 *Corner Lots*
Buildings on corner lots should be oriented to the corner and public store fronts. Parking and automobile access should be located away from corners.

HEIGHT, BULK & SCALE

B-1 *Height, Bulk & Scale Compatibility*
Projects should be compatible with the scale of development anticipated by the applicable Land Use Policies for the surrounding area and should be sited and designed to provide a sensitive transition to nearby, less intensive zones. Projects on zone edges should be developed in a manner that creates a step in perceived height, bulk and scale between anticipated development potential of the adjacent zones.

ARCHITECTURAL ELEMENTS & MATERIALS

C-1 *Architectural Context*
New buildings proposed for existing neighborhoods with a well-defined and desirable character should be compatible with or complement the architectural character and siting pattern of neighboring buildings.

C-2 *Architectural Concept and Consistency*
Building design elements, details and massing should create a well proportioned and unified building form and exhibit an overall architectural concept. Buildings should exhibit form and features identifying the functions within the building. In general, the roof line or top of the structure should be clearly distinguished from its facade walls.

C-3 *Human Scale*
The design of new buildings should incorporate architectural features, elements, and details to achieve a good human scale.

C-4 *Exterior Finish Materials*
Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have a texture, pattern, or lend themselves to a high quality of detailing are encouraged.

C-5 *Structured Parking Entrances*
The presence and appearance of garage entrances should be minimized so that they do not dominate the street frontage of a building.

PEDESTRIAN ENVIRONMENT

D-1 *Pedestrian Open Spaces and Entrances*
Convenient and attractive access to the building's entry should be provided to ensure comfort and security, paths and entry areas should be sufficiently lighted and entry areas should be protected from the weather. Opportunities for creating lively, pedestrian-oriented open space should be considered.

D-2 *Blank Walls*
Buildings should avoid large blank walls facing the street, especially near sidewalks. Where blank walls are unavoidable they should receive design treatment to increase pedestrian comfort and interest.

D-3 *Retaining Walls*
Retaining walls near a public sidewalk that extend higher than eye level should be avoided where possible. Where high retaining walls are unavoidable, they should be designed to reduce their impact on pedestrian comfort and to increase the visual interest along the streetscape.

D-4 *Design of Parking Lots Near Sidewalks*
Parking lots near sidewalks should provide adequate security and lighting, avoid encroachment of vehicles onto the sidewalk, and minimize the visual clutter of parking signs and equipment.

D-5 *Visual Impacts of Parking Structures*
The visibility of all at-grade parking structures or accessory parking garages should be minimized. The parking portion of a structure should be architecturally compatible with the rest of the structure and streetscape. Open parking spaces and carports should be screened from the street and adjacent properties.

D-6 *Screening of Dumpsters, Utilities and Service Areas*
Building sites should locate service elements like trash dumpsters, loading docks and mechanical equipment away from the street front where possible. When it is not possible to locate these elements away from the street front,
they should be screened from view using high quality and compatible materials and should not be located in the pedestrian right-of-way.

D-7 *Personal Safety and Security*
Project design should consider opportunities for enhancing personal safety and security in the environment under review.

D-8 *Treatment of Alleys*
The design of alley entrances should enhance the pedestrian street front.

D-9 *Commercial Signage*
Signs should add interest to the street front environment and should be appropriate for the scale and character desired in the area.

D-10 *Commercial Lighting*
Appropriate levels of lighting should be provided in order to promote visual interest and a sense of security for people in commercial districts during evening hours. Lighting may be provided by incorporation into the building
façade, the underside of overhead weather protection, on and around street furniture, in merchandising display windows, in landscaped areas, and/or on signage.

D-11 *Commercial Transparency*
Commercial storefronts should be transparent, allowing for a direct visual connection between pedestrians on the sidewalk and the activities occurring on the interior of a building. Blank walls should be avoided.

D-12 *Residential Entries and Transitions*
For residential projects in commercial zones, the space between the residential entry and the sidewalk should provide security and privacy for residents and a visually interesting street front for pedestrians. Residential
buildings should enhance the character of the streetscape with small gardens, stoops and other elements that work to create a transition between the public sidewalk and private entry.

LANDSCAPING

E-1 *Reinforce Existing Landscape Character of Neighborhood*
Where possible, and where there is not another overriding concern, landscaping should reinforce the character of neighboring properties and abutting streetscape.

E-2 *Landscaping to Enhance the Building and/or Site*
Landscaping, including living plant material, special pavements, trellises, screen walls, planters, site furniture and similar features should be appropriately incorporated into the design to enhance the project.

E-3 *Landscape Design to Address Special Site Conditions*
The landscape design should take advantage of special on-site conditions such as high-bank front yards, steep slopes, view corridors, or existing significant trees and off-site conditions such as greenbelts, ravines, natural
areas, and boulevards.

DESIGN CUES FROM SURROUNDING CONTEXT

- Varied architectural styles: Adjacent buildings range in style from turn-of-the-century brick apartments, Queen Anne single family residences, builder townhome development, to 1980's and 1990's multi-family development. A common thread in the apartment development is a majority rectilinear box design with various materiality. Massings are oriented to the street with street facing entries. Horizontal datums through window paterns, trim, and flat roof lines are the predominate ordering patern of the neighborhood.

- Neighboring multi-family building scale: The project site is bordered on three sides by 3 to 4 story multifamily buildings. Projects to the north and south are both large nearly full lot developments. Similar size developements exist to the east, but are accentuated the significat slope from the west to east.

- Zero Lot lines: Many of the adjacent lots have zero lot line conditions, or at the least greatly reduced setbacks from current standards. The lot to the north faces to blank wall facades toward the subject property; both blank walls are currently exposed to the public. To the south, many unit windows face the subject parcel. This is a typical condition in the neighborhood. No alley exists at the site which highlights further the urban density of the neighborhood.

- Change in grade along street-scape: The project street scape is virutally flat running north to south. However, from the east to west the site slopes approximate 20' in total. Adjacent developements have cut buildings into this slope with recessed units on the lower level.

- Eclectic use of materials: Material use seems to vary with the era of development. Turn of the century apartment structures are predominately brick with wood windows, parapets and detailing. The 1980's/90's development is predominately stucco with wood detailing. The limited amout of single family residences tend to be wood siding with varying degrees of detailing. Quality of materials varies, with many older structures needing maintenance, especially of wood components. To add to the present eclecticism, the project proposes to incorporate materials to highlight form and roof lines that are in common use in today's market.

RESPONSES TO ANALYSIS

Capitol Hill Context

Respect massing of adjacent sites	B-1 *Height, Bulk & Scale Compatibility*
Mixed use of exterior materials	C-4 *Exterior Finish Materials*

Immediate Neighborhood Context:

Blank wall facades	A-1 Responding to site characteristics
Strong building presence at the street	A-4 *Human Activity* / A-2 *Streetscape Compatibility*
Direct street entry	A-3 Entrances Visible from the Street
Reduced facade setbacks @ adj. bldg. windows	A-10 *Respect for Adjacent Sites*
Clear building massing	C-2 *Architectural Concept & Consistency*
Detailing to reinforce pedestrian street-level interaction	C-3 *Human Scale*

Adjacent and Nearby Streets:

Emphasize pedestrian entrance in facade	D-1 *Pedestrian Open Spaces and Entrances* / A-3 *Entrances Visible From Street*
Utilities located within building envelope	D-6 *Screening of Dumpsters, Utilities and Service Areas*
Residential entry placed in visible location	D-1 *Pedestrian Open Spaces and Entrances* / D-7 *Personal Safety and Security*

Views and Amenities:

Street-level pedestrian entrance	A-7 *Residential Open Space*
Inner courtyard (some options); Roof Deck (some options)	A-7 *Residential Open Space*
Planters, landscaping and common courtyard	E-2 *Landscaping to Enhance the Building and Site*
Roof deck on small lot	E-3 Landscaping to Address Special Site Conditions

Landmarks:
N/A

DESIGN GUIDELINES OF HIGHEST PRIORITY

A-2	*Streetscape Compatibility*	**C-2**	*Architectural Concept and Consistency*
A-5	*Respect for Adjacent Sites*	**D-2**	*Blank Walls*
B-1	*Height, Bulk & Scale Compatibility*	**D-7**	*Personal Safety and Security*

ZERO LOT LINES

EGRESS BALCONY

STRONG BUILDING PRESENCE @ STREET

DIRECT STREET FACING ENTRY

OPTION SUMMARY

OPTION A - "CODE COMPLIANT"

39 Units / 300 SF / unit residential unit average
0 Vehicle parking stalls provided / 10 Bicycle parking stalls provided

DISTINGUISHING FEATURES
Meets all zoning code requirements

PROS
- Code compliant scheme with no departures

CONS
- All units face the southern building facade
- Blank walls of the northern building facade are exposed to the public and neighboring buildings
- Does not maintain the strong building presence to the street like adjacent developments
- View potential limited to one unit on the west end
- No roof top amenities provided in order to achieve development objectives

OPTION B

42 Units / 373 SF / unit residential unit average
0 Vehicle parking stalls provided / 10 Bicycle parking stalls provided

DISTINGUISHING FEATURES
Strong circulation element with modulated building massing

PROS
- Blank walls of the north building are masked
- Building mass somewhat reduced at the street
- Separation between the north building windows and circulation is incressed

CONS
- Roof top amenities are limited due to development objectives
- Does not maintain the strong building presence to the street like adjacent developments

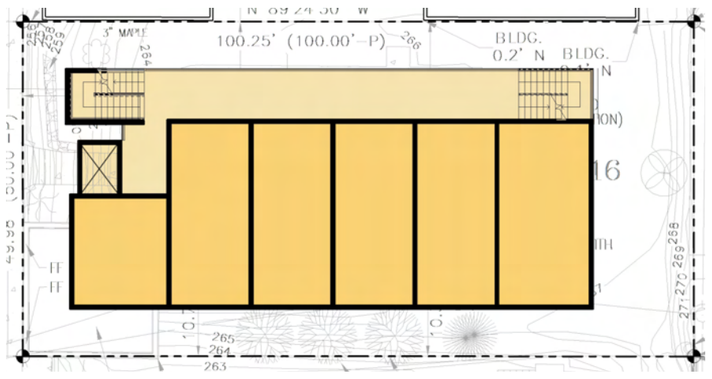
OPTION C - "PREFERRED"

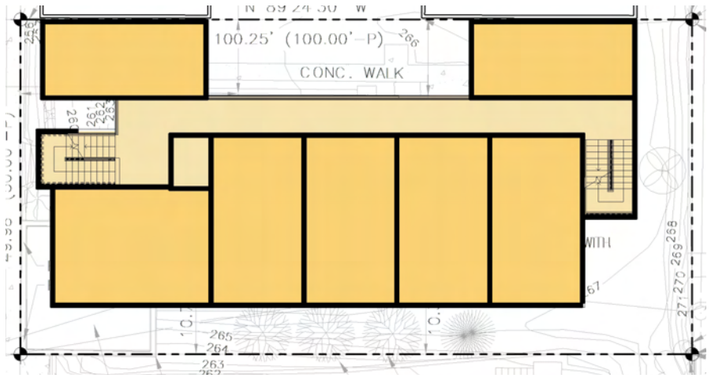
46 Units / 285 SF / unit residential unit average
0 Vehicle parking stalls provided / 12 bicycle parking stalls provided

DISTINGUISHING FEATURES
Large roof top deck amenity with reduced street facing building scale

PROS
- Blank walls of the north building are masked
- Maintains strong building presence to the street like adjacent developments
- Roof deck amenity at view side full width
- Separation between the north building windows and circulation is increased
- More units face the view side

CONS
- Proximity to eastern lot reduced
- Appears bulky but fits context
- Reduced potential to activate the ground level





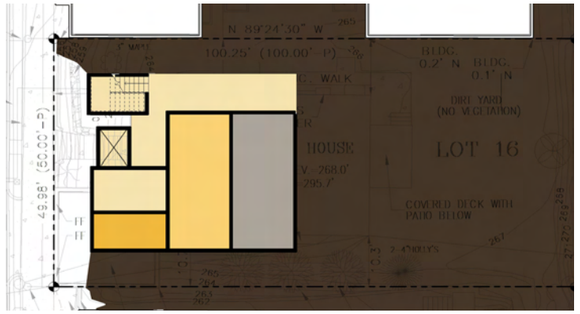
OPTION A - "CODE COMPLIANT"

39 Units
300 SF / unit residential unit average
0 Vehicle parking stalls provided
10 Bicycle parking stalls provided

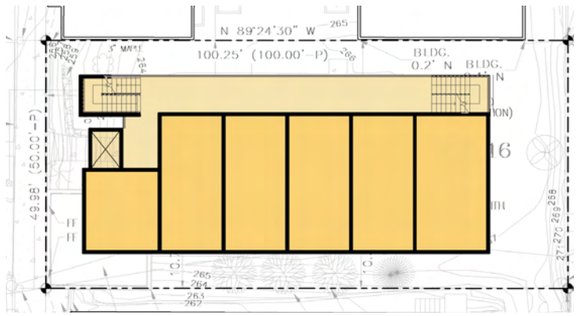
DISTINGUISHING FEATURES
Meets all required setbacks of the zoning code

PROS
- Code compliant scheme with no departures

CONS
- All units face the southern building facade
- Blank walls of the northern building facade are exposed to the public and neighboring buildings
- Does not maintain the strong building presence to the street like adjacent developments
- View potential limited to one unit on the west end
- No roof top amenities provided in order to achieve development objectives



GROUND FLOOR



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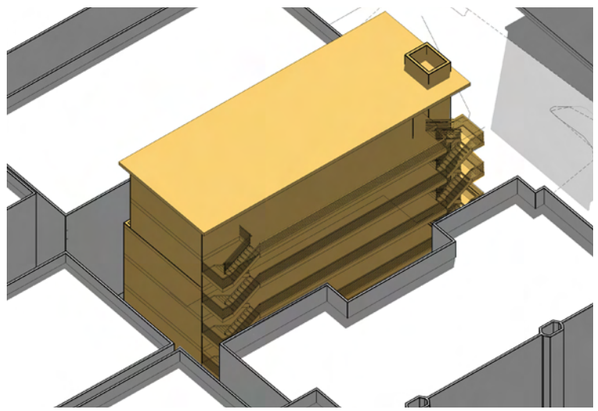
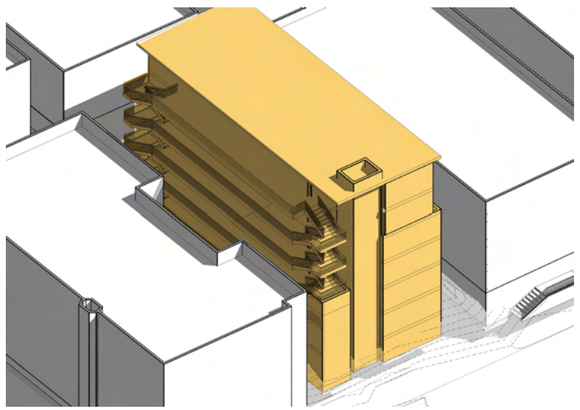


NORTHEAST CORNER



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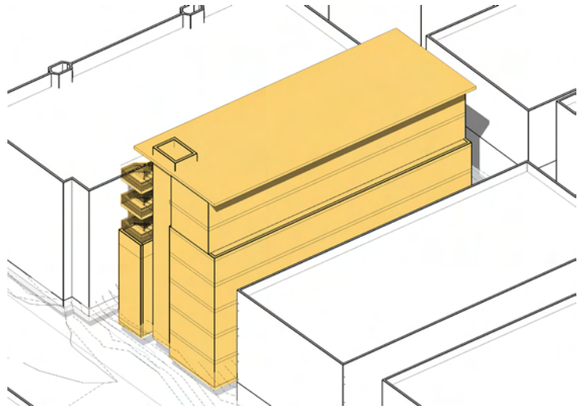

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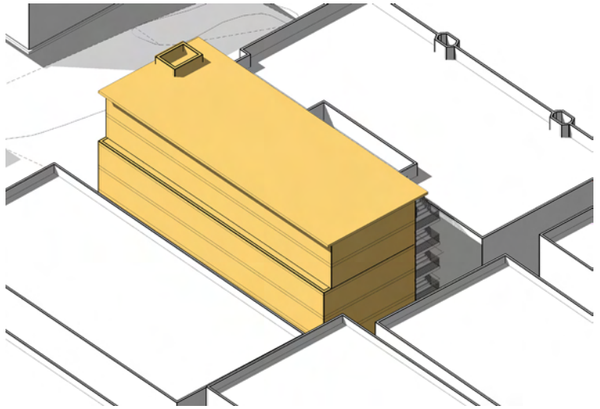


SOUTHEAST CORNER

OPTION B

42 Units
373 SF / unit residential unit average
0 Vehicle parking stalls provided
10 Bicycle parking stalls provided

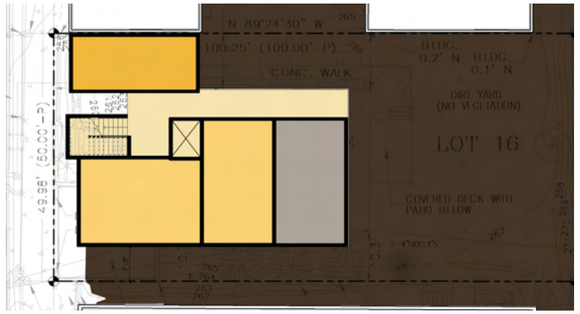
DISTINGUISHING FEATURES
Strong circulation element with modulated building massing

PROS

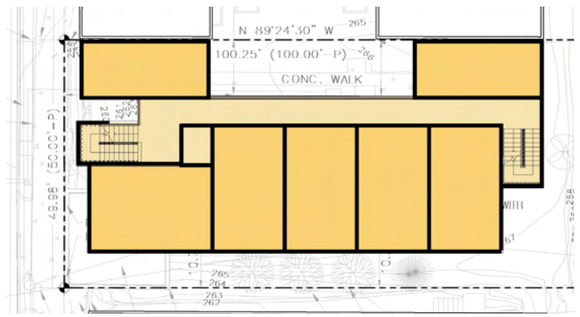
- Blank walls of the north building are masked
- Building mass somewhat reduced at the street
- Separation between the north building windows and circulation is incressed

CONS

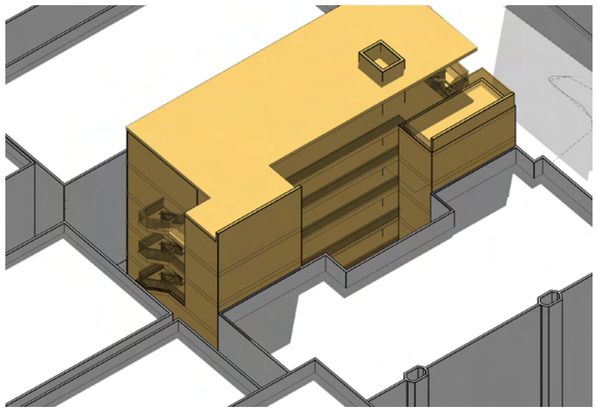
- Roof top amenities are limited due to development objectives
- Does not maintain the strong building presence to the street like adjacent developments



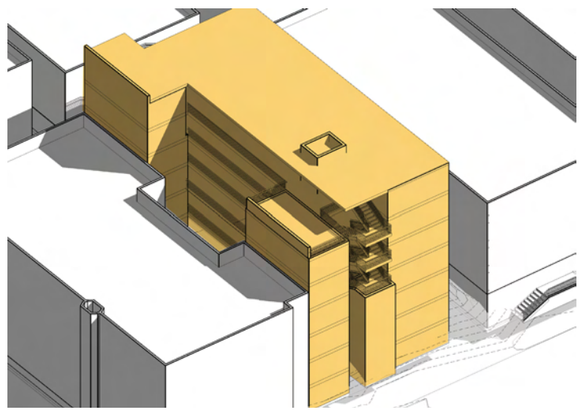
GROUND FLOOR



TYPICAL FLOOR



NORTHEAST CORNER



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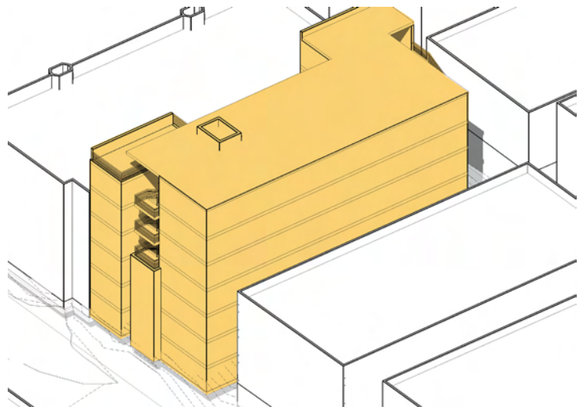
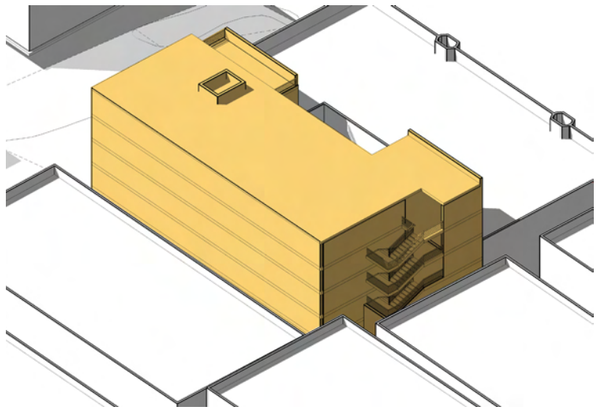


SOUTHWEST CORNER



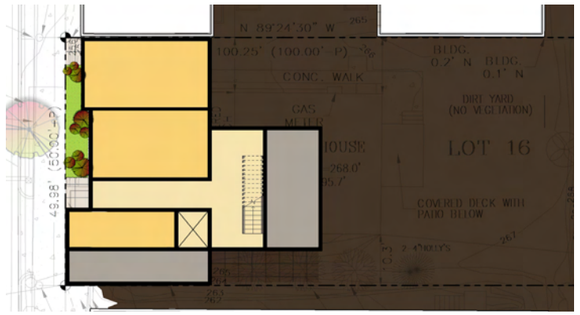
SOUTHEAST CORNER

46 Units
285 SF / unit residential unit average
0 Vehicle parking stalls provided
12 Bicycle parking stalls provided

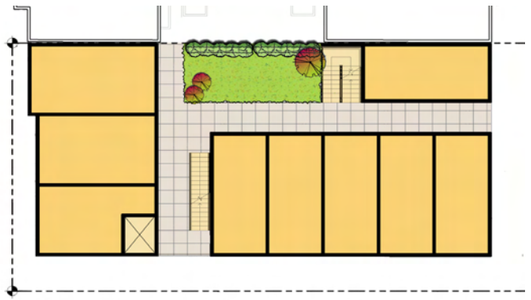
DISTINGUISHING FEATURES
Large roof top deck amenity with reduced street facing building scale

PROS
- Blank walls of the north building are masked
- Maintains the strong building presence to the street like adjacent developments
- Roof deck amenity at view side full width
- Separation between the north building windows and circulation is increased
- More units face the view side

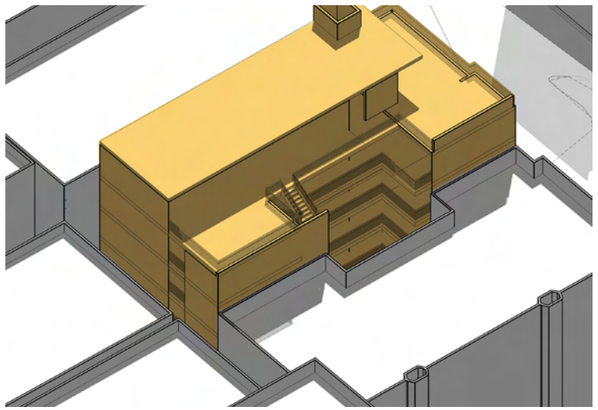
CONS
- Proximity to eastern lot reduced
- Appears bulky but fits context
- Reduced potential to activate the ground level



GROUND FLOOR



TYPICAL FLOOR
(Courtyard level 2 only)



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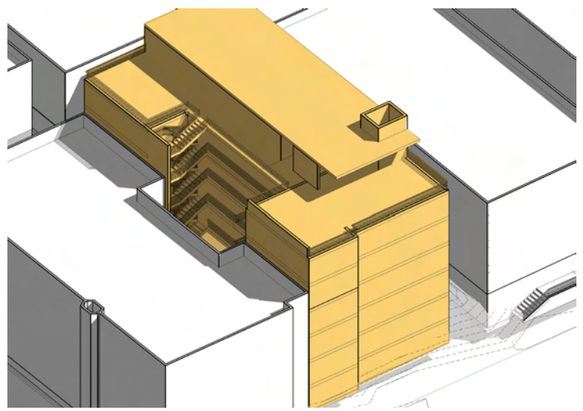

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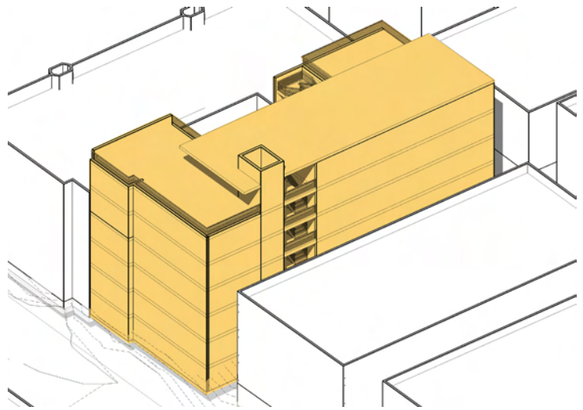
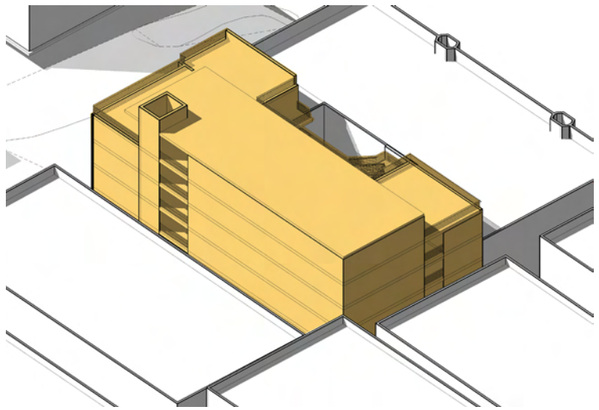
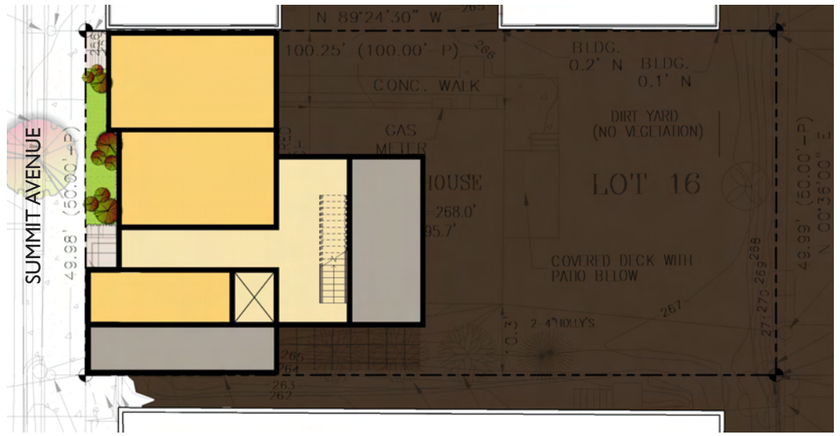


SOUTHWEST CORNER

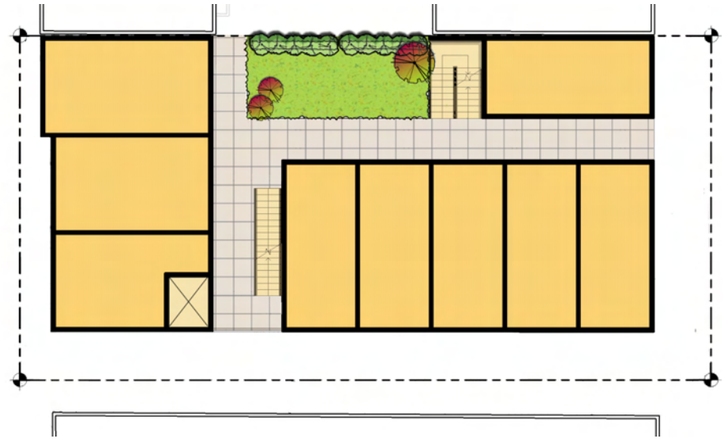


SOUTHEAST CORNER

OPTION C - PREFERRED

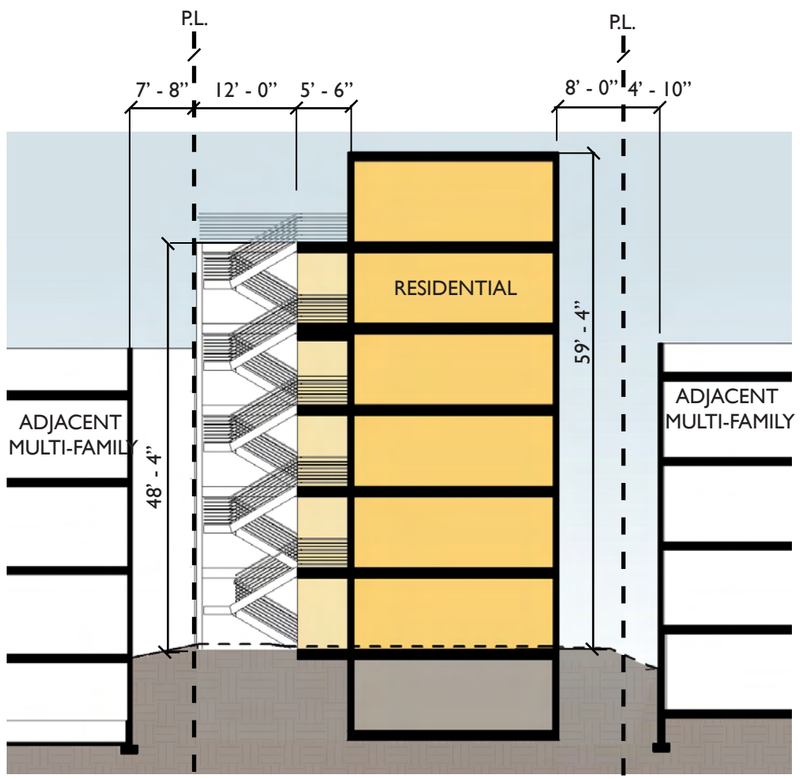


GROUND LEVEL

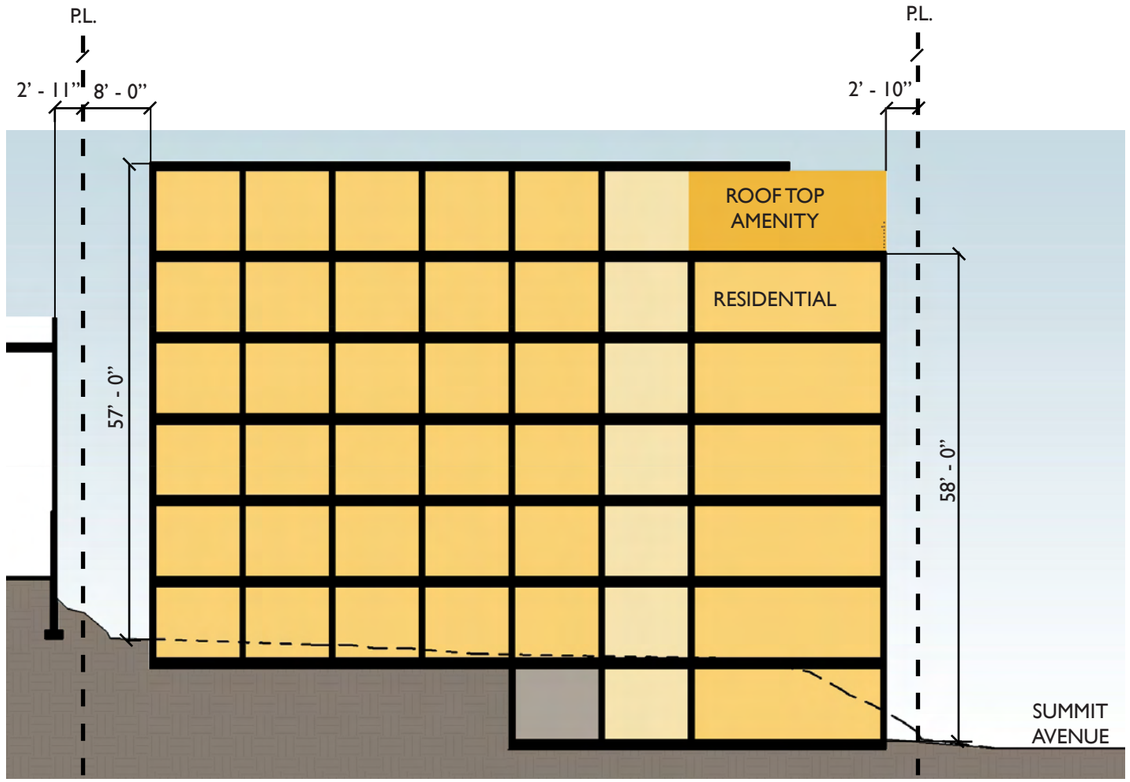


TYPICAL FLOOR (Courtyard level 2 only)

ROOF DECK



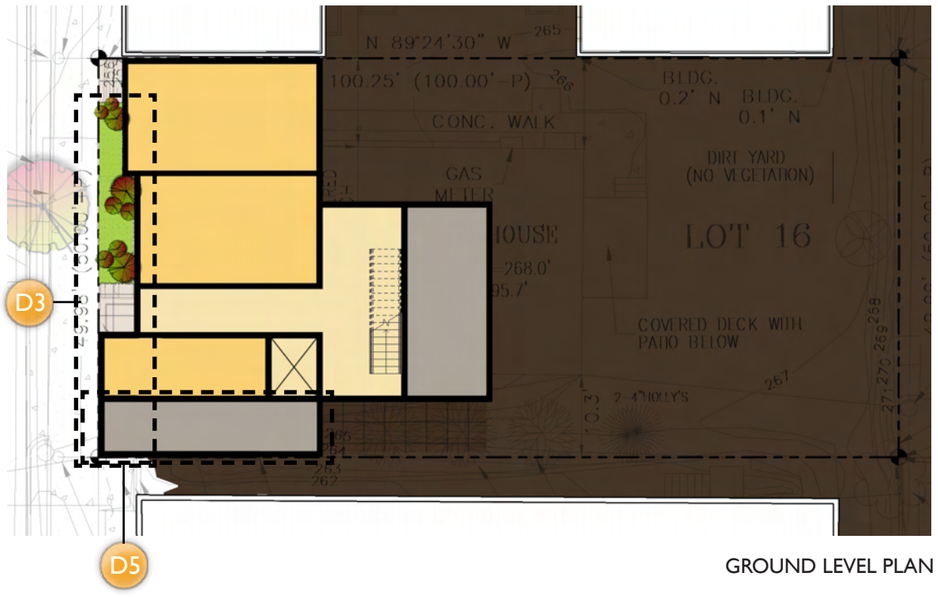
SECTION LOOKING WEST



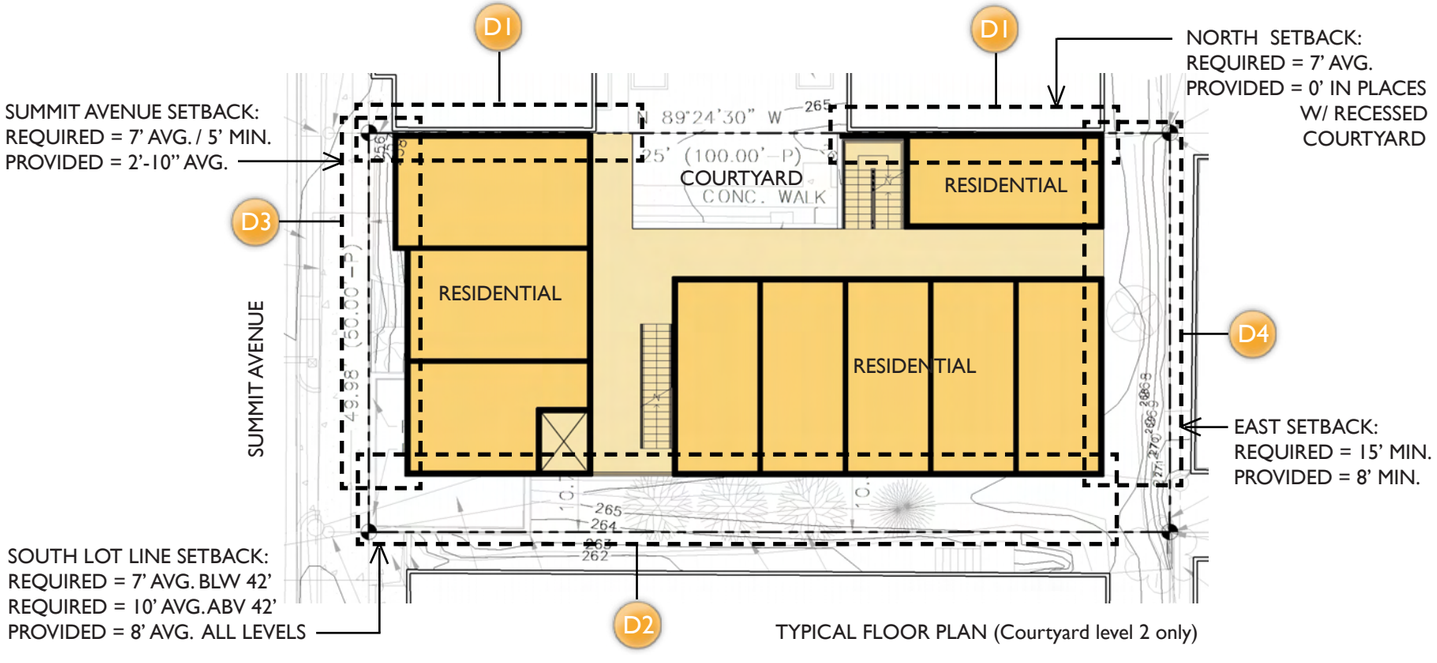
SECTION LOOKING SOUTH

OPTION C - DEPARTURE REQUESTS

DEPARTURE NUMBER		LAND USE CODE SECTION	ITEM	CODE REQUIREMENT	DEPARTURE REQUESTED	DESIGN RATIONALE
1	D1	SMC 23.45.518	Side setbacks	7' Average setback required with a min. of 5' from +0' to +42' above grade.	Zero lot line side setback at the north property line.	Existing adjacent building maintains a zero lot line blank facades in two areas. In effort to mitigate these facades the design proposes a similar condition while maintaing an open courtyard at the recessed area.
2	D2	SMC 23.45.518	Side setbacks	10' Average setback required with a min. of 5' from +42' above grade to height limit	8' Average setback at the south property line.	Similar height, bulk, and scale (i.e. full height walls with no modulation) are present in most nearby buildings including south building facing project site. Design proposal is consistent with architectural context.
3	D3	SMC 23.45.518	Front setbacks	7' Average setback required with min. 5'	2'-10" front setback at front property line.	The streetscape along Summit maintains a strong building presence near the sidewalk. The proposed development seeks to mimic this design rational
4	D4	SMC 23.45.518	Rear Setbacks	Minimum 15' rear setback	8'-0" rear setback at east property line.	Design proposes alignment of East facade with existing building to north. All adjacent buildings are minimal in all side / rear setbacks due to lack of an existing alley. Design proposal is consistent with architectural context.
5	D5	SMC 23.45.518	Side setbacks	7' Average setback required with a min. of 5' from +0' to +42' above grade.	Zero lot line side setback at the southwest corner. (Level 1 only)	The project proposes using the massing of the existing garage at the southwest corner. This will allow for a strong pedestrian entry at the street while providing a convient fully enclosed refuse recycle room. Alternatively the refuse will be open air and screened via fencing at this location.

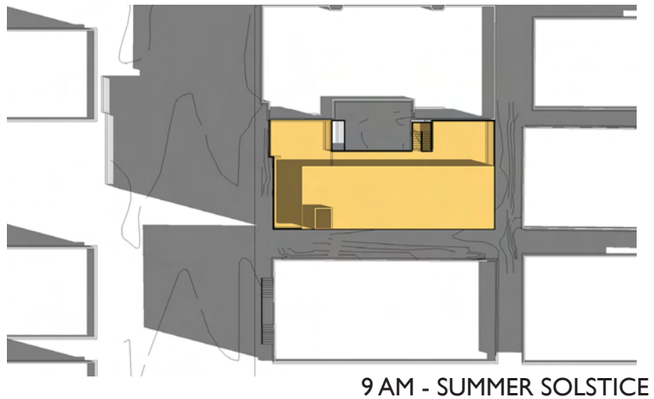


GROUND LEVEL PLAN

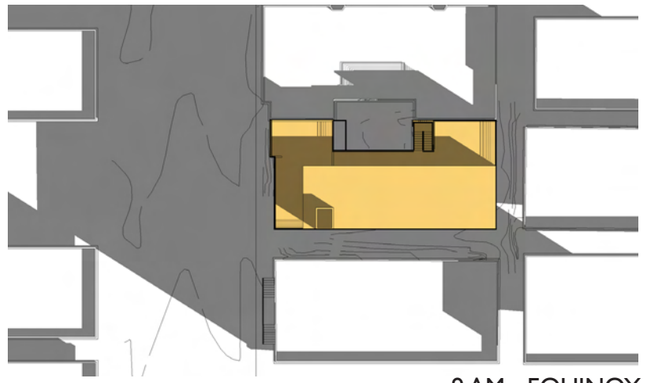


TYPICAL FLOOR PLAN (Courtyard level 2 only)

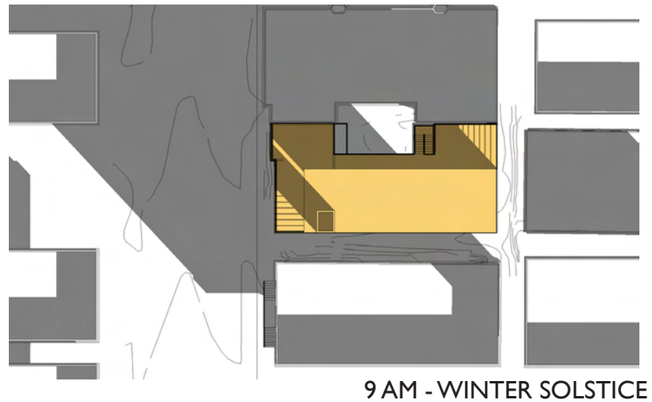
STUDY: SHADOW CASTING - PREFERRED SCHEME



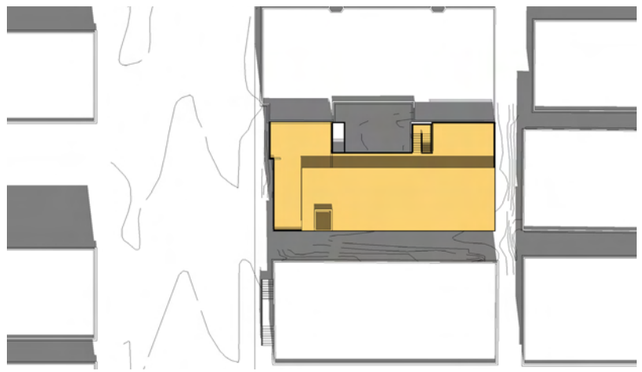
9 AM - SUMMER SOLSTICE



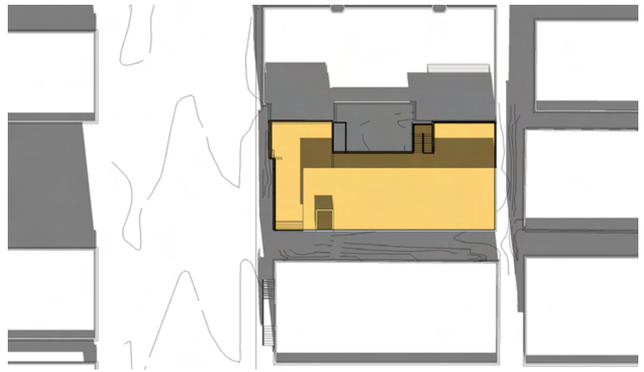
9 AM - EQUINOX



9 AM - WINTER SOLSTICE



NOON - SUMMER SOLSTICE



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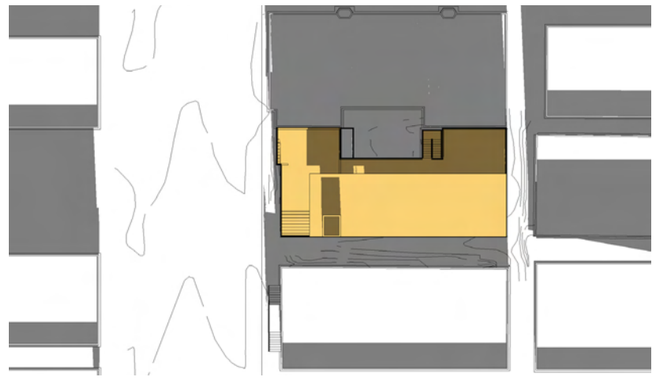

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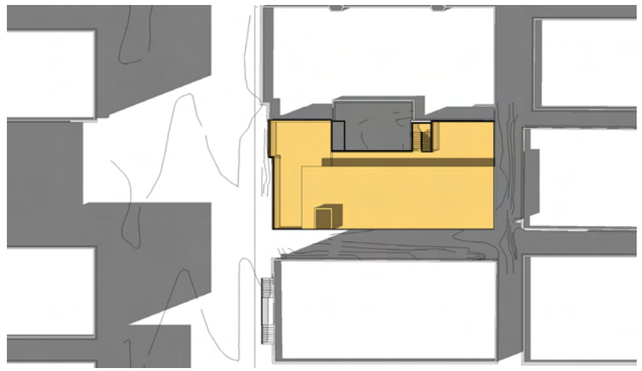

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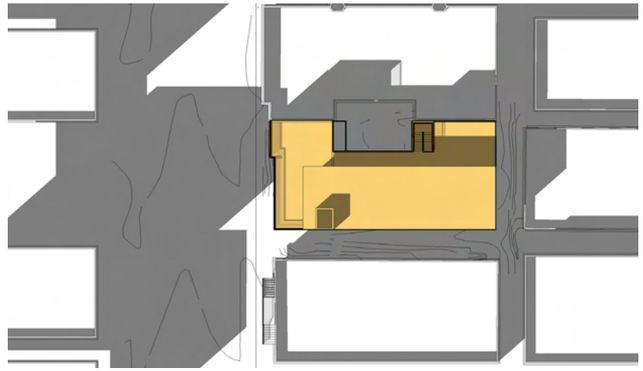

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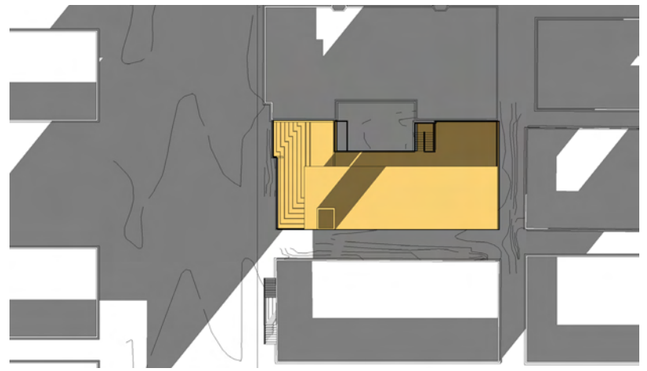

3 PM - WINTER SOLSTICE

PAST PROJECTS by grouparchitect

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