

14TH & REPUBLICAN

DRB RECOMMENDATION MEETING

DPD#3012837 APRIL 17, 2013



REVIEW ITEMS:

- 1 - UPPER SETBACK L4 NORTH FACADE [DEPARTURE]
- 2 - NORTH FACADE WINDOWS
- 3 - GARAGE DOOR [DEPARTURE]
- 4 - FACADE OF TWO-STORY MASS
- 5 - WALK-UP ENTRIES ON 14TH AVE
- 6 - MAIN ENTRY ON 14TH AVE
- 7 - RAILING SYSTEM

UPPER SETBACK L4 NORTH FACADE [DEPARTURE]

A-5 Respect for Adjacent Sites

Buildings should respect adjacent properties by being located on their sites to minimize disruption of the privacy and outdoor activities of residents in adjacent buildings.

Initial Recommendation:

The Board restated its disinclination to approve the facade length departure due to the overscaled massing on the north elevation. The fourth floor should be set back 15 feet from the property line with the fourth floor one continuous wall east of the portion of the third and fourth floors that projects forward of the cedar siding wall.

B-1 Height, Bulk, and Scale Compatibility

Projects should be compatible with the scale of development anticipated by the applicable Land Use Policies for the surrounding area and should be sited and designed to provide a sensitive transition to nearby, less intensive zones. Projects on zone edges should be developed in a manner that creates a step in perceived height, bulk, and scale between anticipated development potential of the adjacent zones.

Initial Recommendation:

In spite of modifications to the massing in response to earlier guidance, the revisions to the north elevation remained problematic for the Board. As the massing relates closely to the departure request for greater facade length along the north side, the Board requested increased depth to the upper level (15 feet from the property line) for the (entire) length of the north facade east of the two-story projecting bay before considering a recommendation of approval for the departure.



Previous North Elevation

UPPER SETBACK L4 NORTH FACADE [DEPARTURE]



North Elevation

Design Team Response:

- 1** Fourth floor north facade set back 15 feet from property line
- 2** Fourth floor east facade set back from the building face

UPPER SETBACK L4 NORTH FACADE [DEPARTURE]



Previous East Elevation

UPPER SETBACK L4 NORTH FACADE [DEPARTURE]



East Elevation

Design Team Response:

- 1** Fourth floor north facade set back 15 feet from property line
- 2** Fourth floor east facade set back from the building face

UPPER SETBACK L4 NORTH FACADE [DEPARTURE]



View of L4 North Facade Setback



View of L4 North Facade Setback [Previous Design]

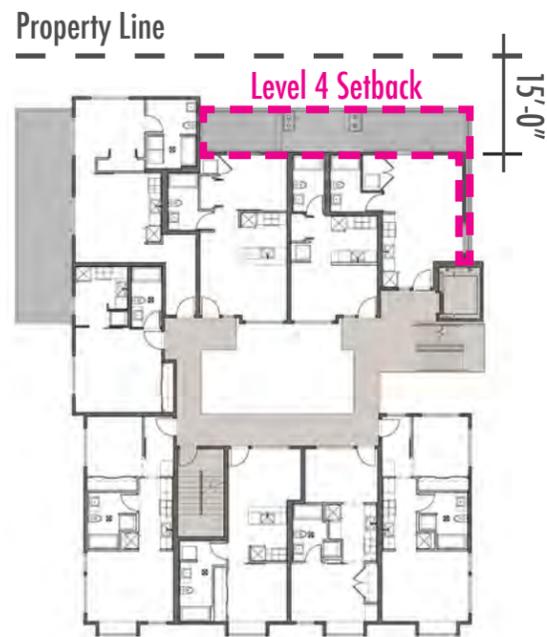
Design Team Response:

- 1** Fourth floor north facade set back 15 feet from property line
- 2** Fourth floor east facade set back from the building face

UPPER SETBACK L4 NORTH FACADE [DEPARTURE]



Level 4 Floor Plan [Previous Design]



Level 4 Floor Plan

DEPARTURE
SMC 23.45.527.B.1 - Maximum Façade Length Required/Allowed

Requirements:
The maximum façade length of all portions of facades within 15 feet of a lot line that is neither a rear lot line nor a street or alley lot line shall not exceed 65 percent of the length of that lot line.

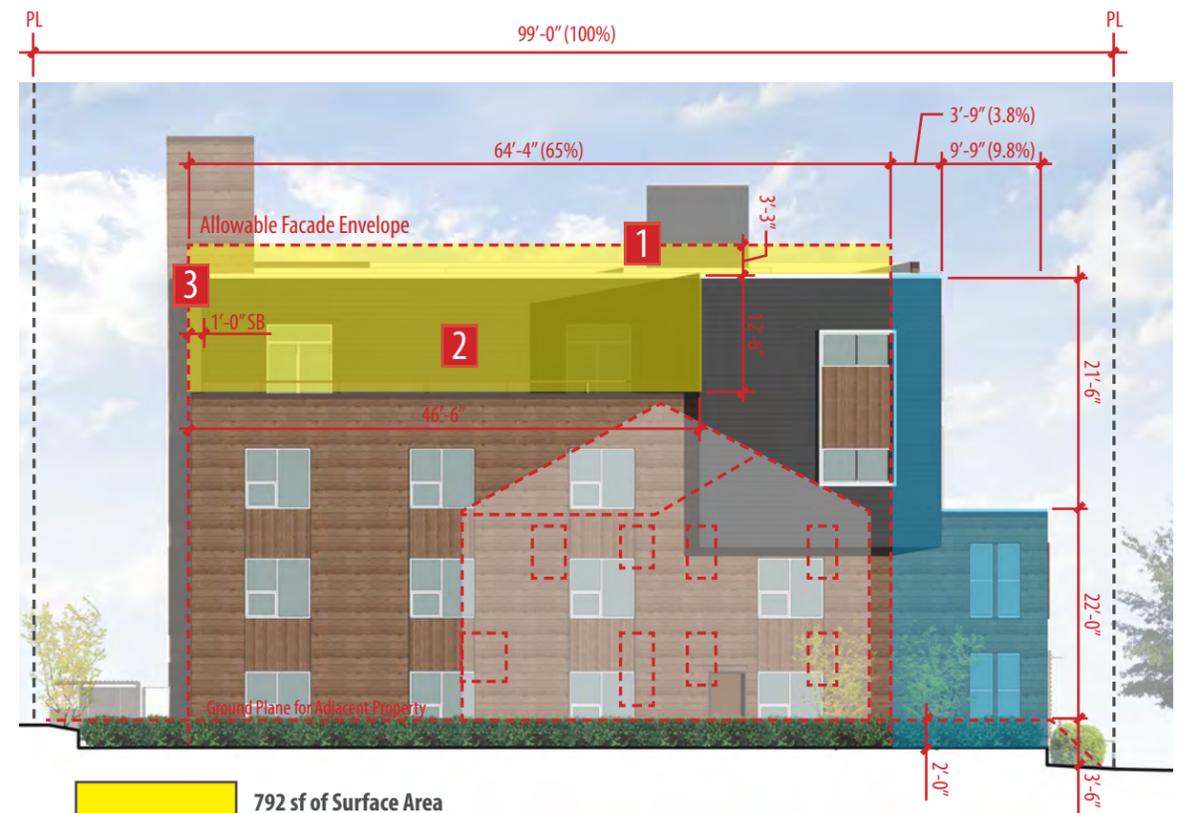
Proposed:
An additional 13'-6" of façade length on the north interior lot line for a façade length of 78.6% of the total lot line length.

The proposed design better serves neighbors to the North of the project by providing more solar access than a strictly code-compliant scheme. The design also maintains the East-West view corridor through the middle of the site and the walk-up units along 14th Avenue East, both of which reflect the street/block pattern along 14th. Subscribing to the 65% facade length limit would require the project to (a) re-orient the North West ground floor units to the North-South direction eliminating walk-up access from 14th and (b) shift the two-story mass at the North West corner to the South blocking the current view corridor. This departure allows the project to better meet the following design guidelines:

1. A-2 Streetscape Compatibility
2. A-3 Entrances Visible from the Street
3. A-4 Human Activity
4. A-7 Residential Open Space
5. C-1 Architectural Context
6. D-1 Pedestrian Open Space

Response to Design Board Guidance:

- 1** Building height has been held down 3-3" below maximum allowable building height.
- 2** 67% of 4th Floor along North facade has been set back 15'-0" from property line.
- 3** The East facade of the 4th Floor (26 LF), North of the elevator core, has been set back 1'-0" to create a plane shift where materials intersect.



North Facade Departure Sketch

NORTH FACADE WINDOWS

A-5 Respect for Adjacent Sites

Buildings should respect adjacent properties by being located on their sites to minimize disruption of the privacy and outdoor activities of residents in adjacent buildings.

Initial Recommendation:

The north wall lacks sensitivity to the scale of the adjacent residential building. The large windows of the bedrooms on the north side should be scaled down to a more compatible size.

C-2 Architectural Concept and Consistency

Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept. Buildings should exhibit form and features identifying the functions within the building. In general, the roofline or top of the structure should be clearly distinguished from its facade walls.

Initial Recommendation:

Changes in the vertical plane warrant changes in materials. For the most part, the proposal provides some consistency with this idea; however, on the north and east elevation this doesn't occur. Per guidance from A-5, the upper portion of the fourth floor north facade should be set back 15 feet from the property line with the portion of the grey fiber cement board east of the project bay continuous so that the northeast corner does not project forward to meet the cedar siding plane. On the east elevation, a change in material should represent a dimensional change in the vertical plane.



View of North Facade looking Southwest



View of North Facade looking Southwest
[Previous Design]

Design Team Response:

- 1** Fourth floor north facade set back 15 feet from property line
- 2** Scale of windows along north facade have been reduced to be more sensitive to adjacent building
- 3** Changes in materiality are represented by dimensional change in the vertical plane

GARAGE DOOR [DEPARTURE]

A-8 Parking and Vehicle Access

Siting should minimize the impact of automobile parking and driveways on the pedestrian environment, adjacent properties, and pedestrian safety.

Initial Recommendation:

See C-5 guidance.

C-5 Structured Parking Entrances

The presence and appearance of garage entrances should be minimized so that they do not dominate the street frontage of a building.

Initial Recommendation:

The Board found the position and the design of the garage door problematic as it lacked a strong kinship to the south facade. The Board recommended that the applicant raise the height of the door, align the door with the bay windows above it, and provide an architectural element that relates to the physical form of the bays or overall elevation. Simply, the garage door should appear better integrated with the larger elevation.



Previous South Elevation

GARAGE DOOR [DEPARTURE]



Design Team Response:

- 1** Garage door has been recessed 12" from south building facade
- 2** Garage recess has been raised to align with top of adjacent bay window, thereby increasing the apparent height of the door
- 3** Garage door is aligned with east edge of bay windows above
- 4** Addition of vertical siding element between windows to relate to garage door accent

South Elevation

GARAGE DOOR [DEPARTURE]



Garage Entry looking North
[Previous Design]



Garage Entry looking North

DEPARTURE

SMC 23.45.536.D.3.b - Screening of Parking by Garage Doors

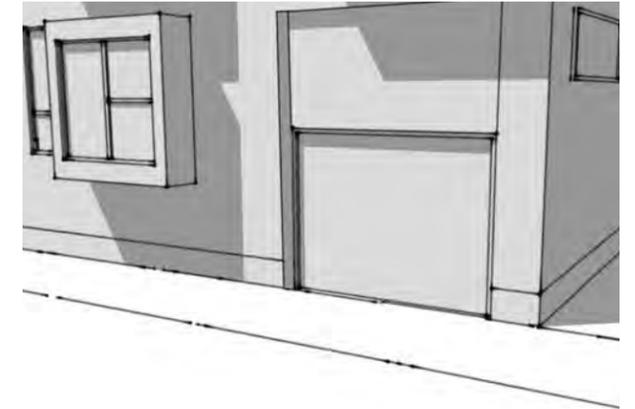
Requirements:

Garage doors facing the street shall be set back at least 15 feet from the street lot line, and shall be no closer to the street lot line than the street-facing façade of the structure.

Proposed:

Place the garage door 8'-5" from the street lot line.

If the project garage door was located per code (SMC 23.45.536.D.3.b Screening of Parking by Garage Doors), it would be set back 7.5' from the face of the building to meet the 15' minimum setback. The proposed departure proposes locating the door set back 1' from the building face, which is set back 15' from the back of sidewalk. This orientation promotes safety, as it would eliminate a 6.5' additional setback (recess) into the building and is architecturally consistent with adjacent garage access that is typically set flush with the building face.



Proposed Configuration



Code-Compliant Configuration

Design Team Response:

- 1** Garage door has been recessed 12" from south building facade
- 2** Garage recess has been raised to align with top of adjacent bay window, thereby increasing the apparent height of the door
- 3** Garage door is aligned with east edge of bay windows above

FACADE OF TWO-STORY MASS

C-3 Human Scale

The design of new buildings should incorporate architectural features, elements, and details to achieve a good human scale.

Initial Recommendation:

The design of the two units resembling townhouses facing 14th Avenue East lack warmth and intimacy. The scale matches the larger four story portion of the west facade on the other side of the entry portal. Suggestions by the Board included separating the entries and reducing the size of the apertures. The deep recessed entry, which imitates the larger portal, is unwelcoming. The designer should reduce the scale of this mass yet continue to maintain its relationship to the larger composition.



Previous West Elevation

FACADE OF TWO-STORY MASS



West Elevation

Design Team Response:

- 1** Depth of recessed entry has been reduced from 6 feet to 2 feet
- 2** Scale of windows reduced and further articulated to separate from language of the other two masses along west facade
- 3** Change in cladding from horizontal to vertical between second floor windows creates a horizontal band that references overall west facade
- 4** See page 15 for further revisions at unit entries

FACADE OF TWO-STORY MASS



View of Two-Story Mass looking Northeast



View of Two-Story Mass looking Northeast [Previous Design]

Design Team Response:

- 1 Depth of recessed entry has been reduced from 6 feet to 2 feet
- 2 Scale of windows reduced and further articulated to separate from language of the other two masses along west facade
- 3 Change in cladding from horizontal to vertical between second floor windows creates a horizontal band that references overall west facade
- 4 See page 15 for further revisions at unit entries

WALK-UP ENTRIES ON 14TH AVE

C-3 Human Scale

The design of new buildings should incorporate architectural features, elements, and details to achieve a good human scale.

Initial Recommendation:

The design of the two units resembling townhouses facing 14th Avenue East lack warmth and intimacy. The scale matches the larger four story portion of the west facade on the other side of the entry portal. Suggestions by the Board included separating the entries and reducing the size of the apertures. The deep recessed entry, which imitates the larger portal, is unwelcoming. The designer should reduce the scale of this mass yet continue to maintain its relationship to the larger composition.



View of Walk-up Entries looking East



View of Walk-up Entries looking East [Previous Design]

Design Team Response:

- 1** Depth of recessed entry has been reduced from 6 feet to 2 feet
- 2** Individual stairs to each unit entry
- 3** Individual canopies further define unit entry
- 4** Transom windows over doors
- 5** Raised planter for additional privacy at entry

MAIN ENTRY ON 14TH AVE

A-6 Transition between Residence and Street
For residential projects, the space between the building and the sidewalk should provide security and privacy for residents and encourage social interaction among residents and neighbors.

Initial Recommendation:

Characterized as awkward and uninviting, the entry sequence from the 14th Avenue right of way into the courtyard troubled the Board. A redesign should eliminate the concrete wall for more landscaping, turn the steps 90 degrees to be perpendicular to the west facade, position the gate further into the courtyard and design a more porous appearing gate. These actions ought to create a more welcoming entry into the courtyard.

D-3 Retaining Walls

Retaining walls near a public sidewalk that extend higher than the eye level should be avoided where possible. Where higher retaining walls are unavoidable, they should be designed to reduce their impact on pedestrian comfort and increase the visual interest along the streetscape.

Initial Recommendation:

See guidance for A-6.



Main Entry on 14th looking East



Main Entry on 14th looking East [Previous Design]

Design Team Response:

- 1** Entry stair rotated to be perpendicular to 14th Avenue sidewalk
- 2** Concrete retaining wall removed and replaced with additional planting at ground plane
- 3** Perforated steel gate replaced with more transparent mesh infill and height lowered by 2'

RAILING SYSTEM

A-7 Residential Open Space

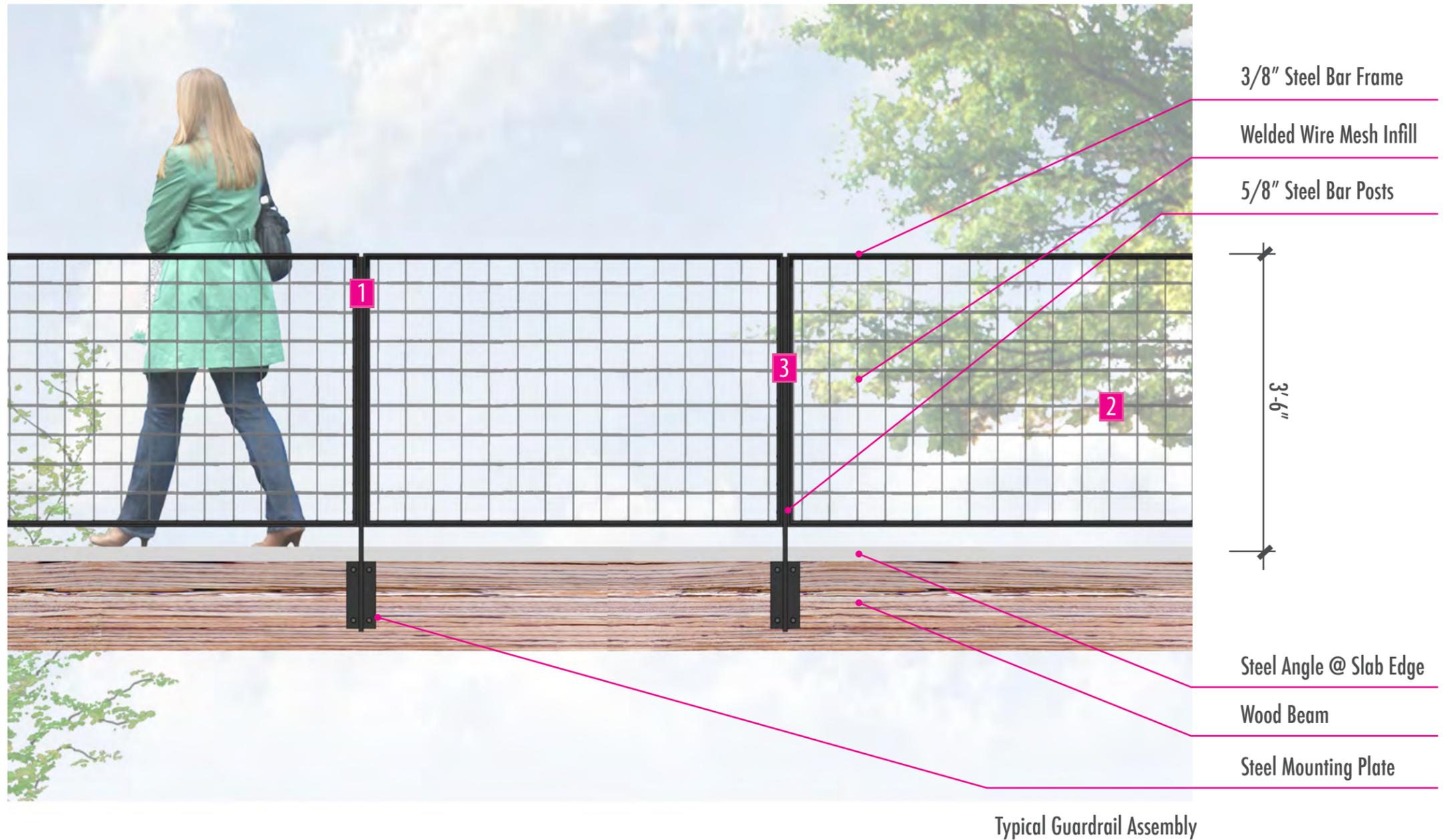
Residential projects should be sited to maximize opportunities for creating usable, attractive, well-integrated open space.

Initial Recommendation:

By the February Recommendation meeting, the applicant had reorganized the edges of the courtyard to reduce the amount of stairs and upper level exterior walkways impeding into the court. In spite of these efforts, the Board requested that the applicant continue to reduce the visual clutter created by the materials comprising the railings and walkways. The busyness of the selected materials obstructs the important view created through the courtyard. Staff note: Consider using glass railings, a more planar underside of the walkways, and potentially fewer posts.

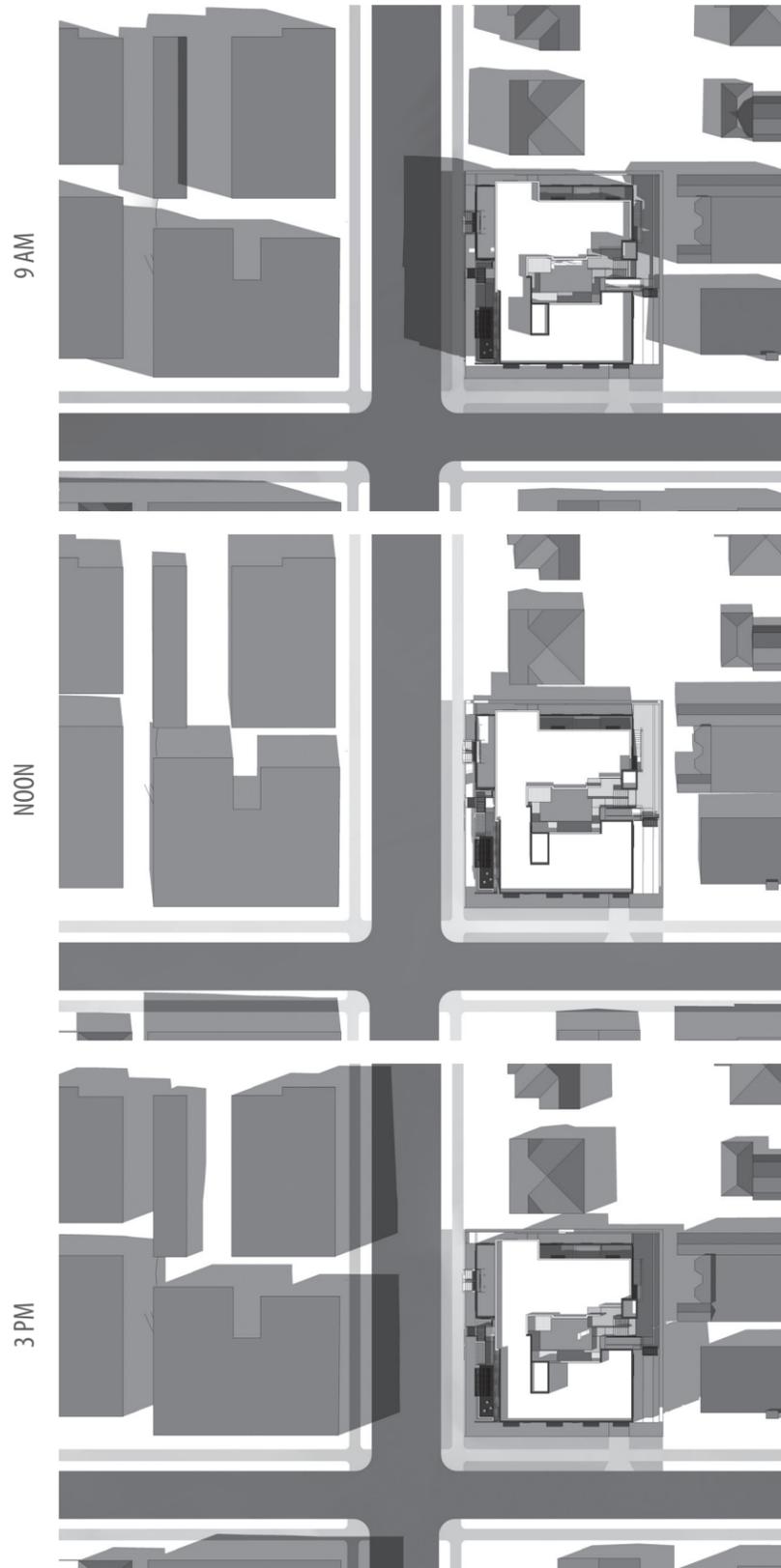
Design Team Response:

- 1** Wider spacing of support posts, thereby reducing number of posts
- 2** Thinner wire mesh infill with larger opening size creates higher level of transparency
- 3** Replaced steel tube posts with steel bar to provide a thinner profile

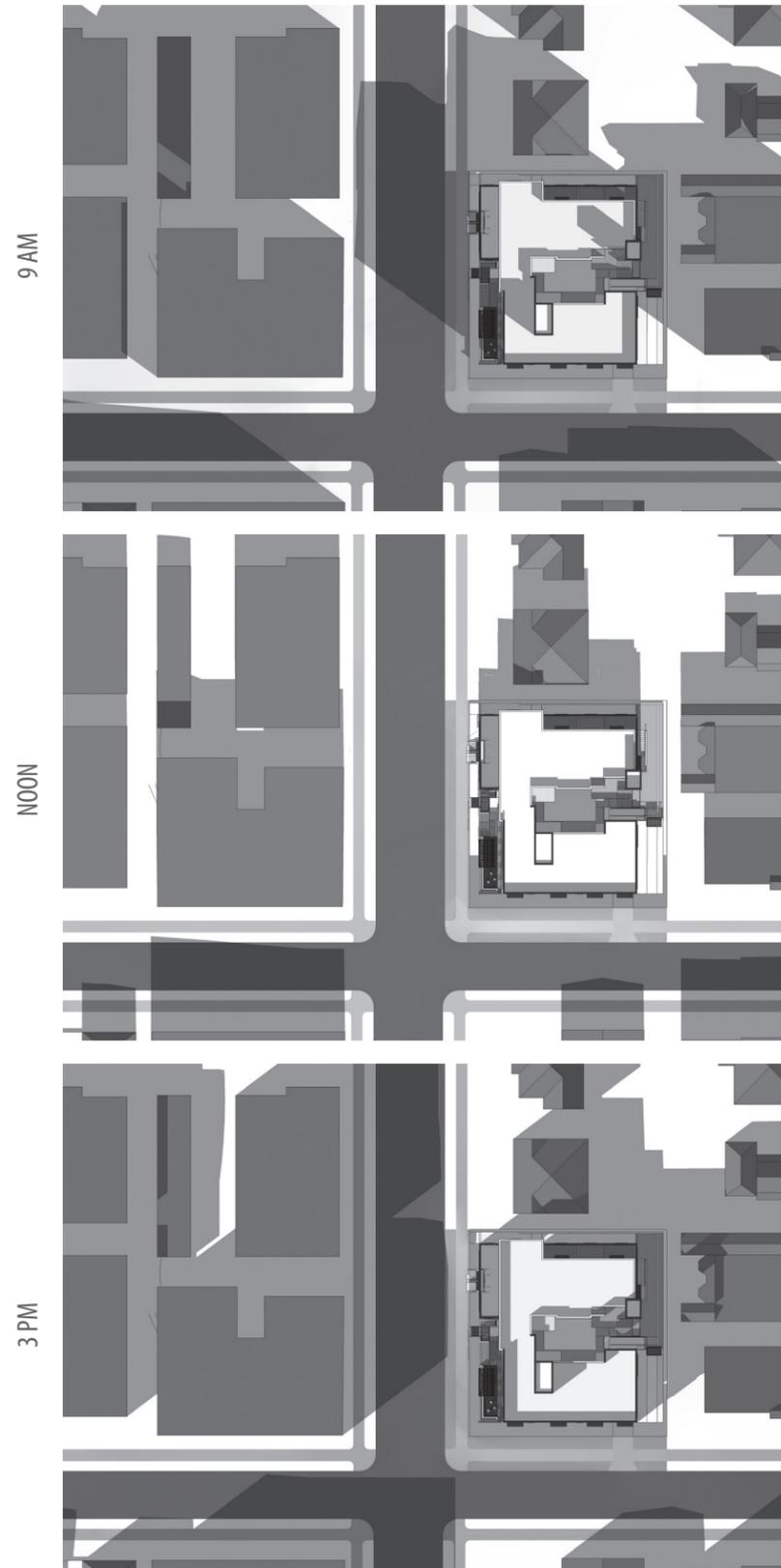


SHADOW STUDIES

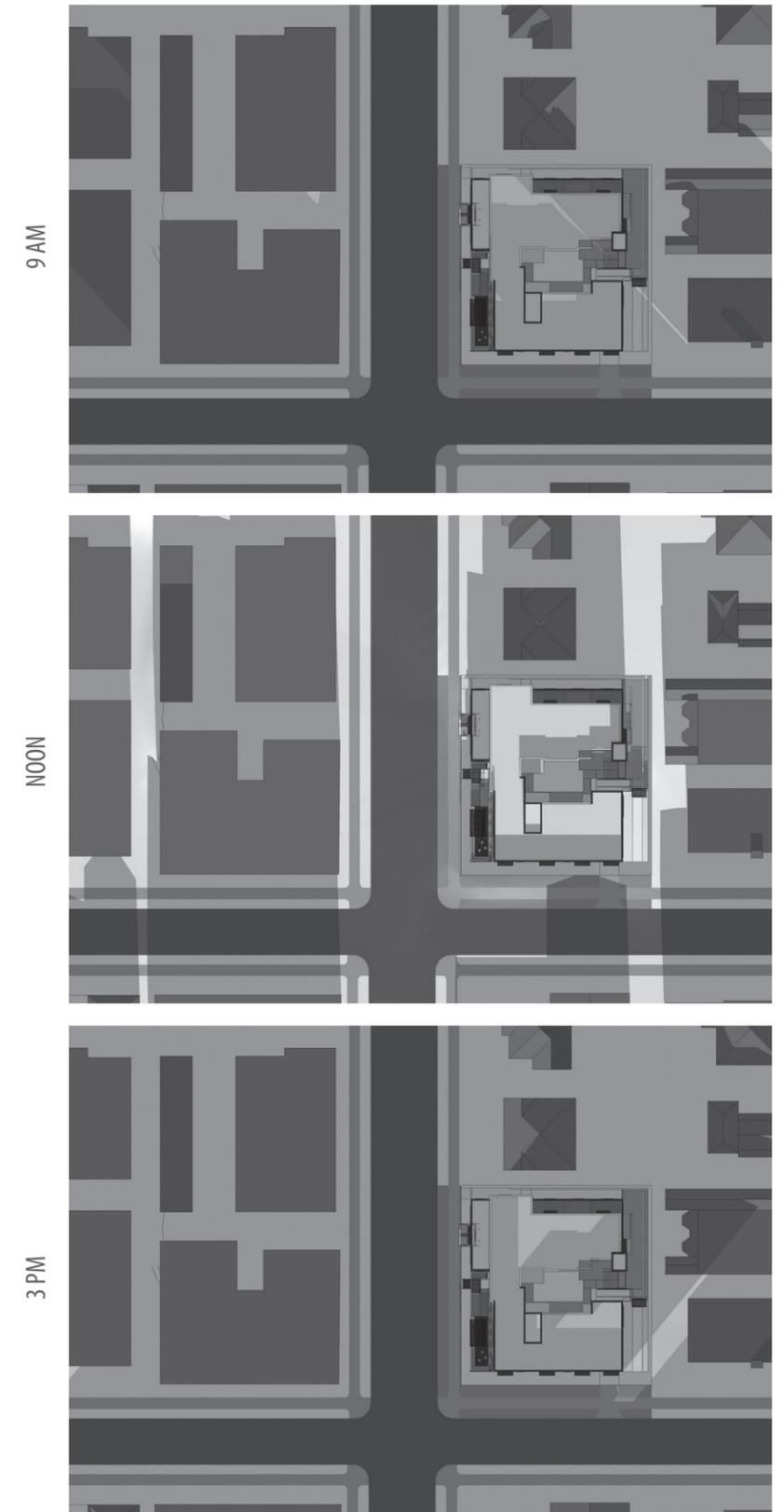
SUMMER SOLSTICE



EQUINOX



WINTER SOLSTICE





14TH & REPUBLICAN

DRB RECOMMENDATION SUBMITTAL

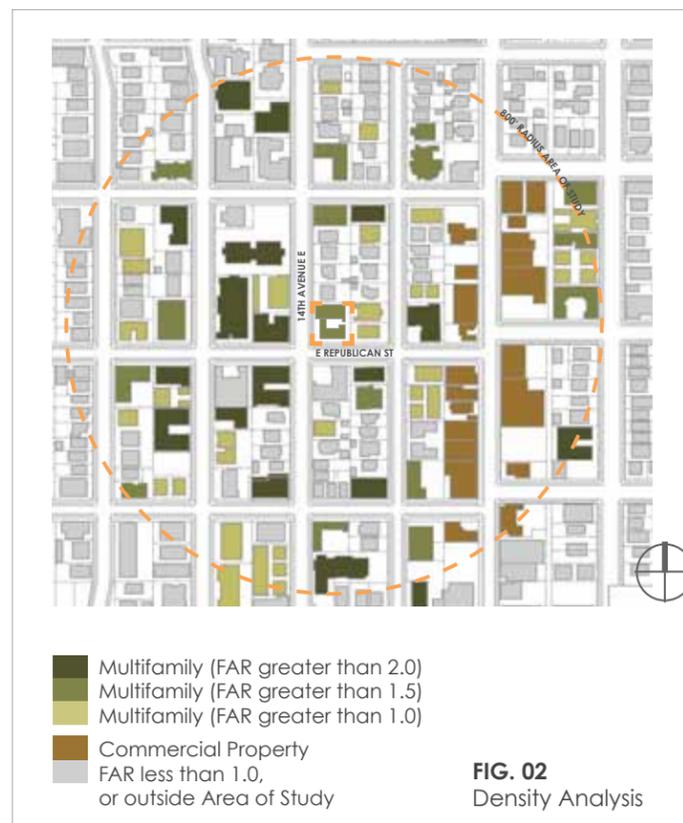
DPD #3012837 FEBRUARY 6, 2013

SITE ANALYSIS

The building site is located within the LR-3 zone at the intersection of 14th and Republican, between the commercially zoned corridors of 15th Avenue and Broadway.

Design cues for the building modulation are based upon an analysis of the neighborhood figure-ground study, illustrating the distinction between the two street patterns. The study revealed that building modulation tends to reflect the orientation of the parcels, with longer facades along Republican Street and shorter facades along 14th Avenue. A typical pattern of larger multifamily buildings, located primarily on the corners of intersections, emerges. Mid-block pattern consists of a mixture of smaller-scale multifamily, townhomes and some single family homes, with nodes of larger multifamily buildings at street intersections. (FIG. 01)

A density analysis illustrates, that the project proposed density will be appropriate, given the existing neighborhood density mix. Upon completion, 17 buildings in the area will have a higher FAR than the proposed project including 2 buildings at the project intersection. (FIG. 02)



NEIGHBORHOOD CONTEXT

The architectural context of the neighborhood, in regards to multifamily development, consists of an eclectic mix of early- to mid-20th century buildings along with some contemporary projects.

As illustrated in the street block pattern analysis, buildings along Republican typically have long, unmodulated facades. Vehicular access for these buildings is typically located off Republican. (Images 1,2)

In contrast, buildings along 14th Avenue typically have shorter façades, or are modulated to reflect the narrower street pattern. (Images 4,5)

In regards to pedestrian open space and entrances, there is a strong presence of raised entries and courtyards in the neighborhood, often elevated anywhere from 2 to 8 feet above street level. (Image 6)

Another contextual cue the project draws upon is the architectural element of the portal. Neighborhood examples consist of portals and framed entries in both traditional and contemporary expressions. (Image 3)



DESIGN STRATEGY:
COMPOSITIONAL CONCEPT

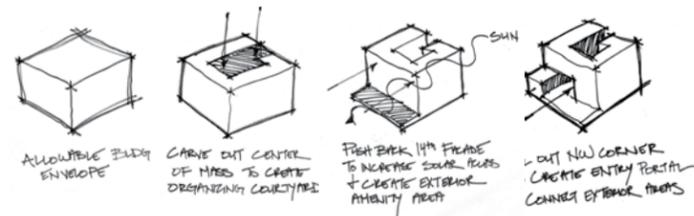


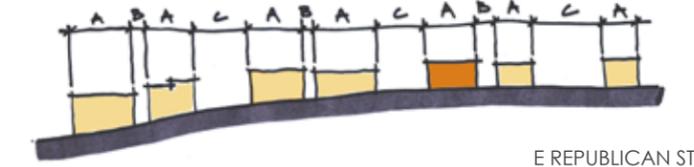
FIG 01: Preliminary Massing Study

Evolution Of Major Building Massing Modulation



FIG 03: EDG Ground Level Plan

FIG 04: EDG Levels 3-4 Plan



E REPUBLICAN ST



14TH AVE E

FIG 02: Street Pattern Study

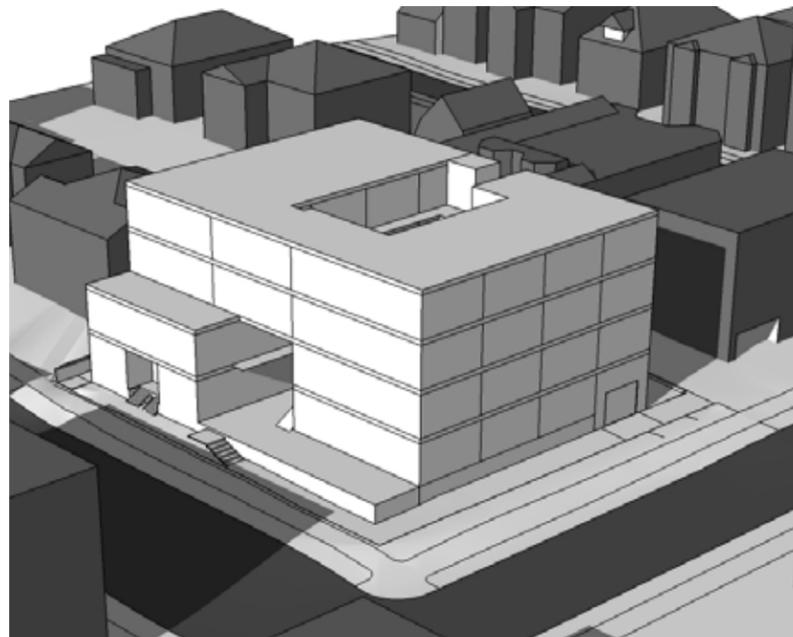


FIG 05: EDG Massing Concept

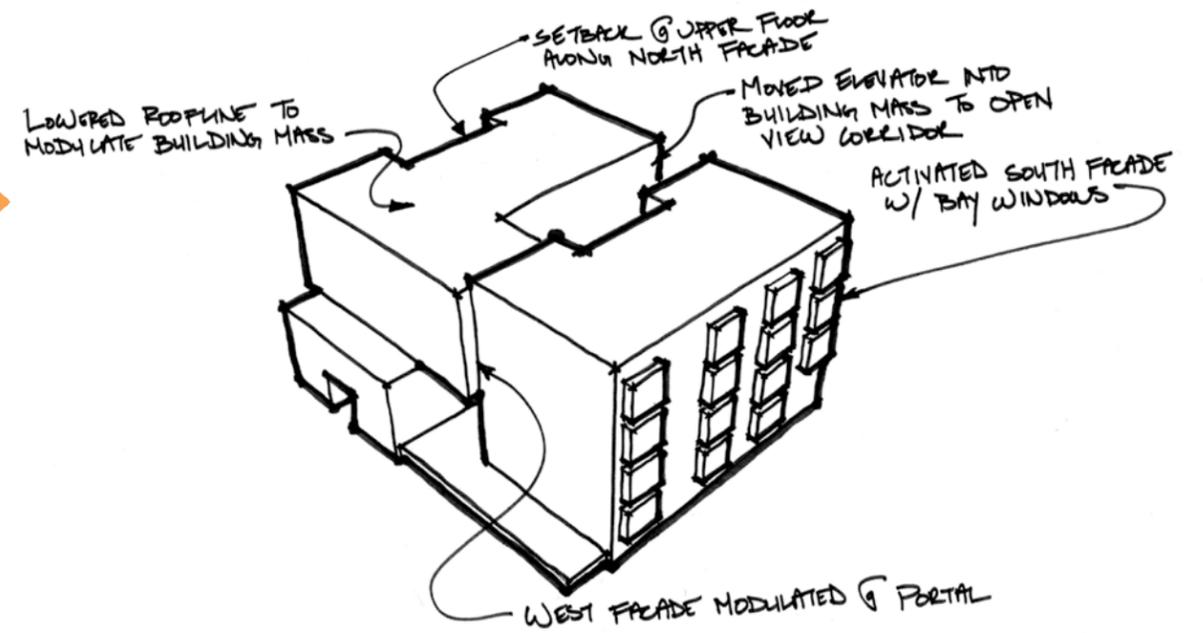


FIG 06: Final Massing Concept

DESIGN REVISIONS OVERVIEW:
MAIN DESIGN GUIDANCE THEMES



• Revised circulation system and consolidated vertical circulation core widens the central view corridor and creates improved visual and physical flow through the site.

• A section of the 4th floor of the North facade has been pushed back from the property line to reduce the apparent mass of the building and provide a greater level of modulation. Also, the building height has been held down below the maximum building height allowed.

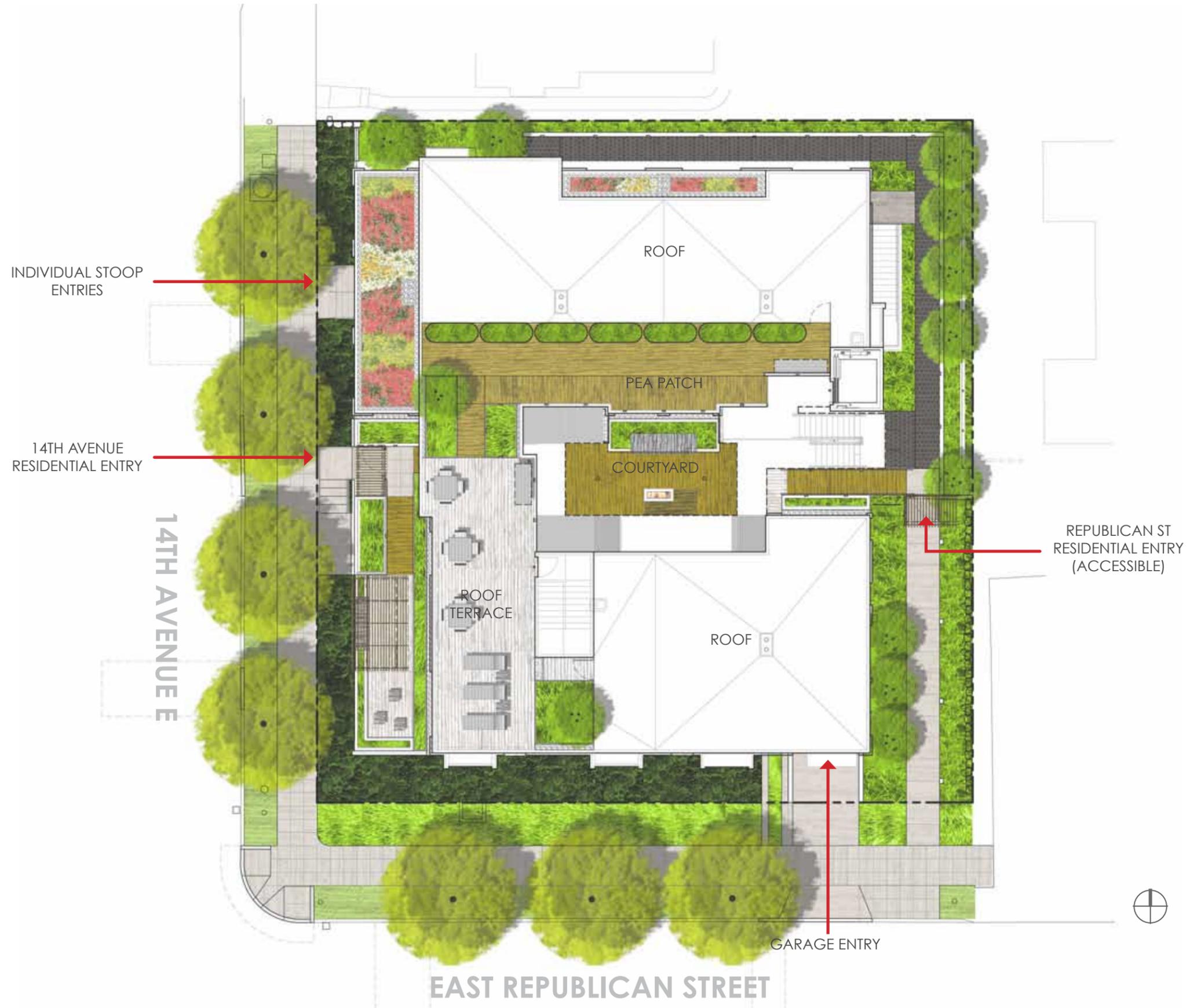
• Articulation of the portal bridge element and roofline, through a stepback of the Southwest building mass, creates a visual break in the West facade, consistent with the existing streetscape characteristics.

• Bay windows have been added to the South elevation providing facade modulation and greater interaction at street level.

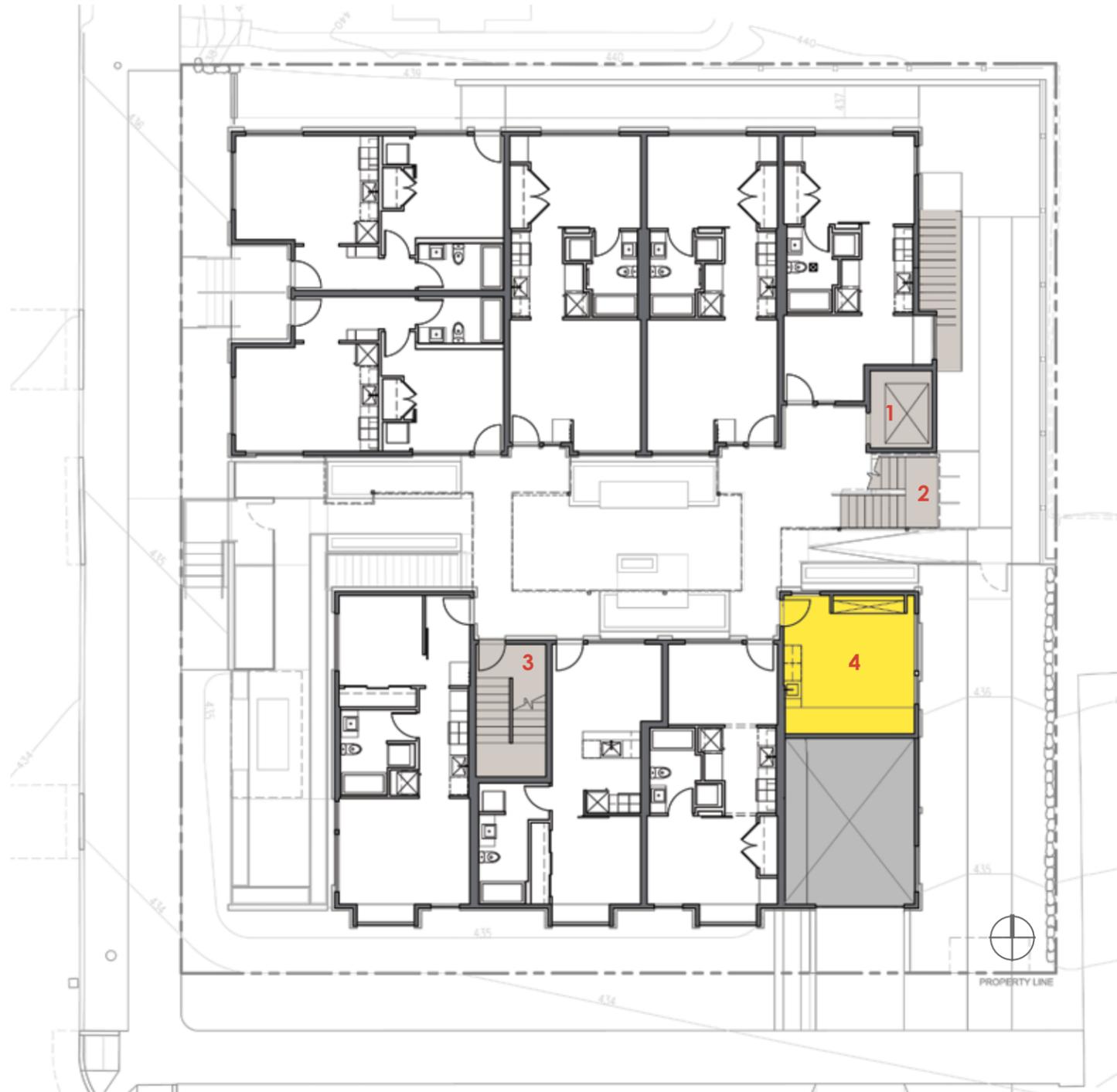
• The trellis structure has been reduced in scale, providing a comfortable relationship to the sidewalk, while also defining the corner amenity space.

• Thoughtfully designed landscaping visually softens the bulk of the plinth, while maintaining a strong transition of public to private.

SITE PLAN



PLAN LEVEL 1



PLAN LEVEL 2

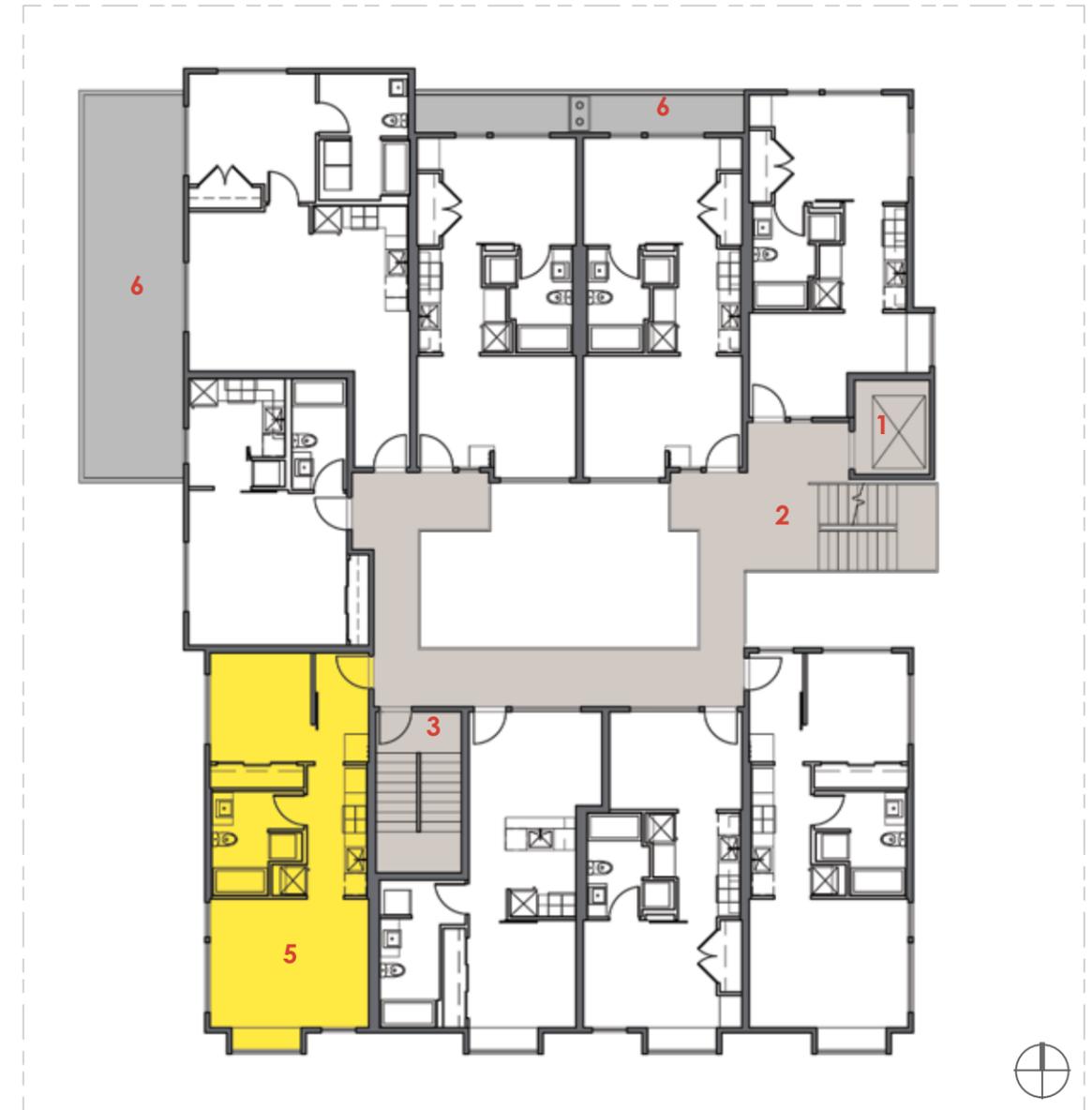


- 1 ELEVATOR
- 2 EXTERIOR STAIRCASE
- 3 INTERIOR STAIRCASE
- 4 AMENITY ROOM
- 5 COURTYARD ONE-BED UNIT

PLAN LEVEL 3

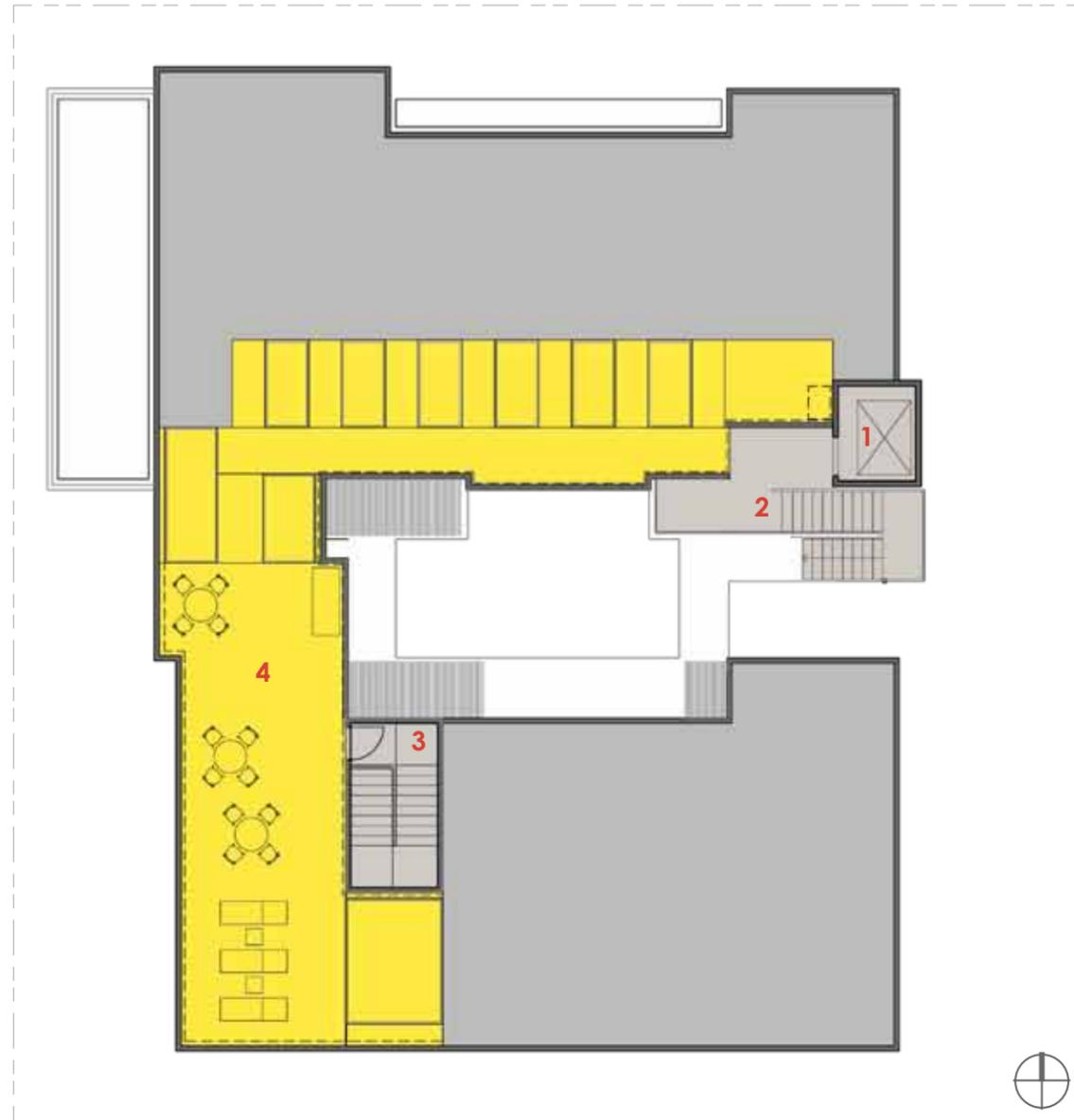


PLAN LEVEL 4

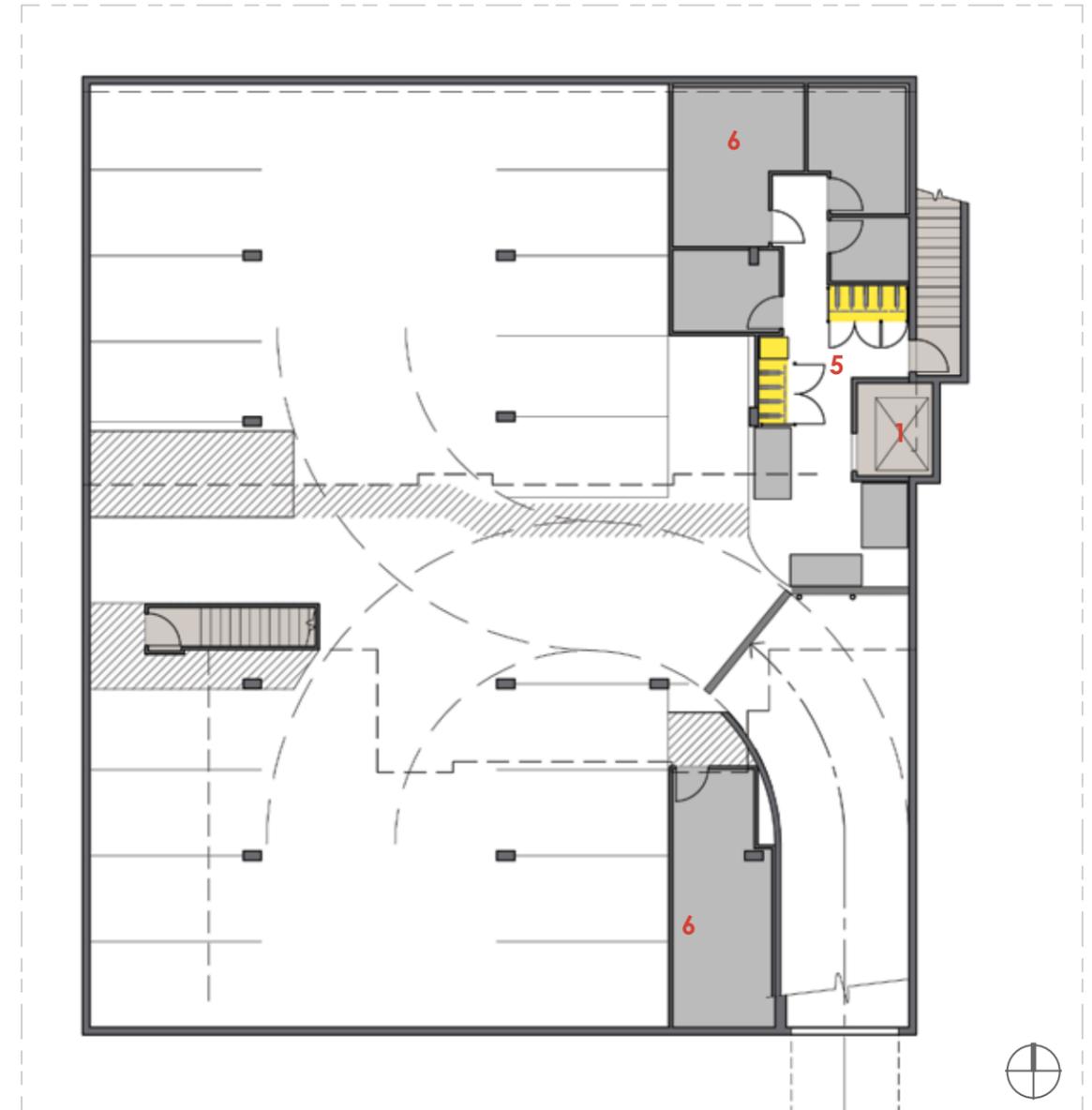


- 1 ELEVATOR
- 2 EXTERIOR STAIRCASE
- 3 INTERIOR STAIRCASE
- 4 STUDIO UNIT
- 5 CORNER ONE-BED UNIT
- 6 GREEN ROOF

ROOFTOP LEVEL



PARKING LEVEL



- 1 ELEVATOR
- 2 EXTERIOR STAIRCASE
- 3 INTERIOR STAIRCASE
- 4 ROOFTOP AMENITY AREA
- 5 SECURED BIKE STORAGE
- 6 BUILDING UTILITIES

WEST ELEVATION



SOUTH ELEVATION



EAST ELEVATION



NORTH ELEVATION



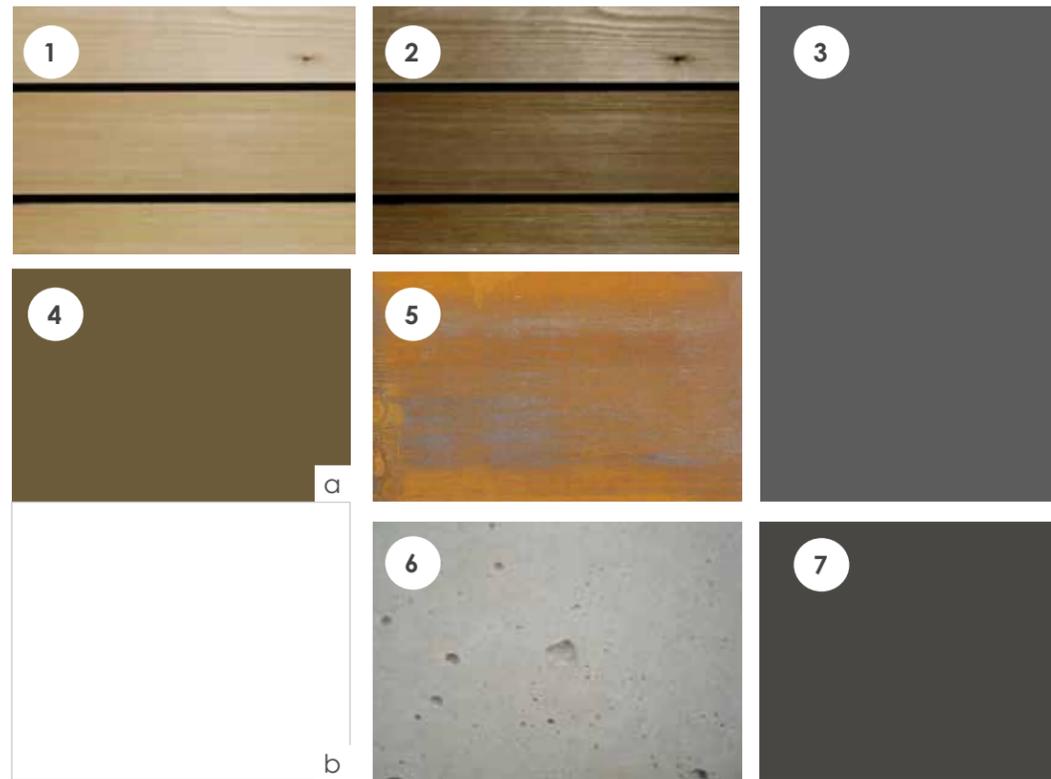
COURTYARD SECTIONS



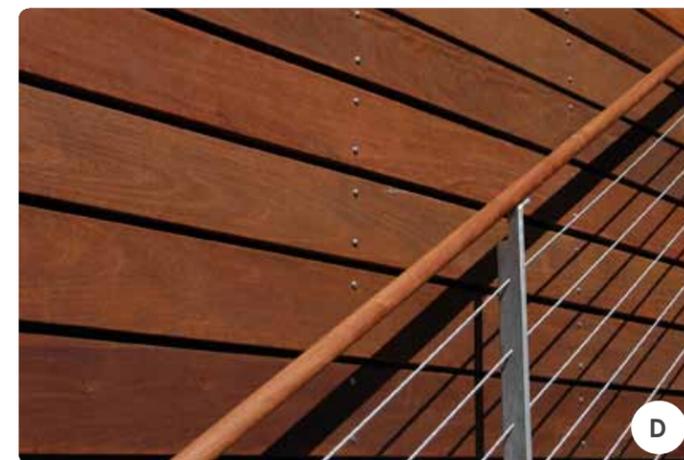
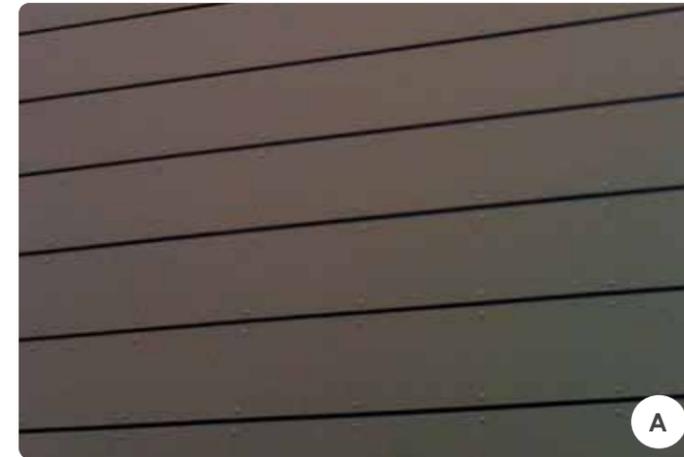
FIG 01: Section through Courtyard, Looking North



FIG 02: Section through Courtyard, Looking South



ARCHITECTURAL MATERIAL PALETTE



MATERIALS:

1. Cedar Siding, Clear Sealer
2. Stained Cedar Siding
3. Painted Fiber Cement Board
4. Prefinished Metal Flashing
 - a. Dark Bronze (Cool Kynar 500)
 - b. Regal White (Cool Kynar 500)
5. Mild Steel
6. Concrete
7. Painted Steel at Handrails/Balcony Structure

PRECEDENT IMAGES:

- A. Painted Fiber Cement Board Rain Screen
- B. Vinyl Window System
- C. Metal and Concrete Stair/Railing System
- D. Cedar Rain Screen Siding

PROJECT RENDERING



View from SW Corner at the Intersection of 14th and Republican



View from SE Corner on Republican Street

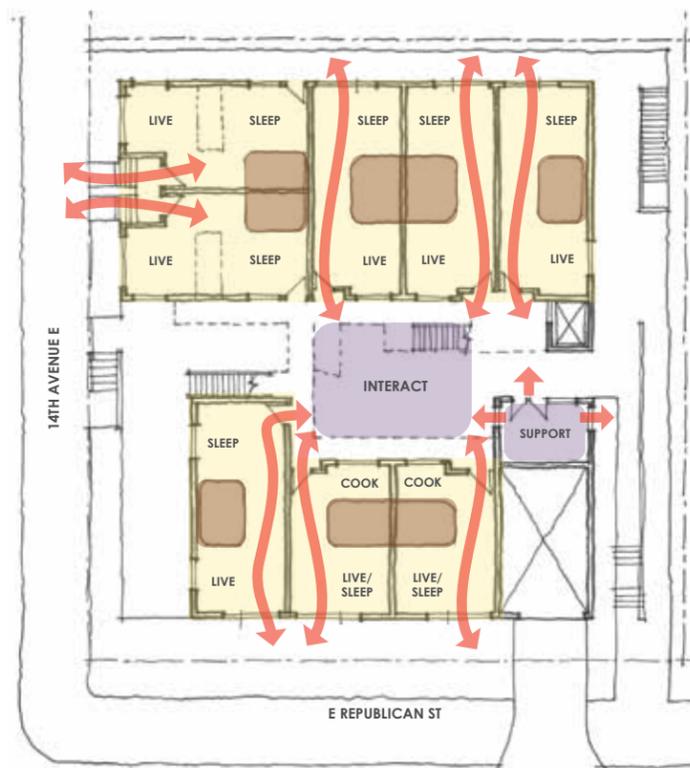
PROJECT RENDERING



View from NW Corner on 14th Avenue East

DESIGN STRATEGY:
COURTYARD

EVOLUTION OF COURTYARD SITE DESIGN



Organizing all units around a central courtyard with exterior entrances, allows for maximum access to air and light to all tenants. The courtyard space is designed to be a vibrant amenity to the building tenants. A two-story portal bridge along 14th Avenue, is designed as a landscaped buffer zone, creating a gradual transition from public to private, while allowing views through the site.

The courtyard layout promotes the projects main design themes of:

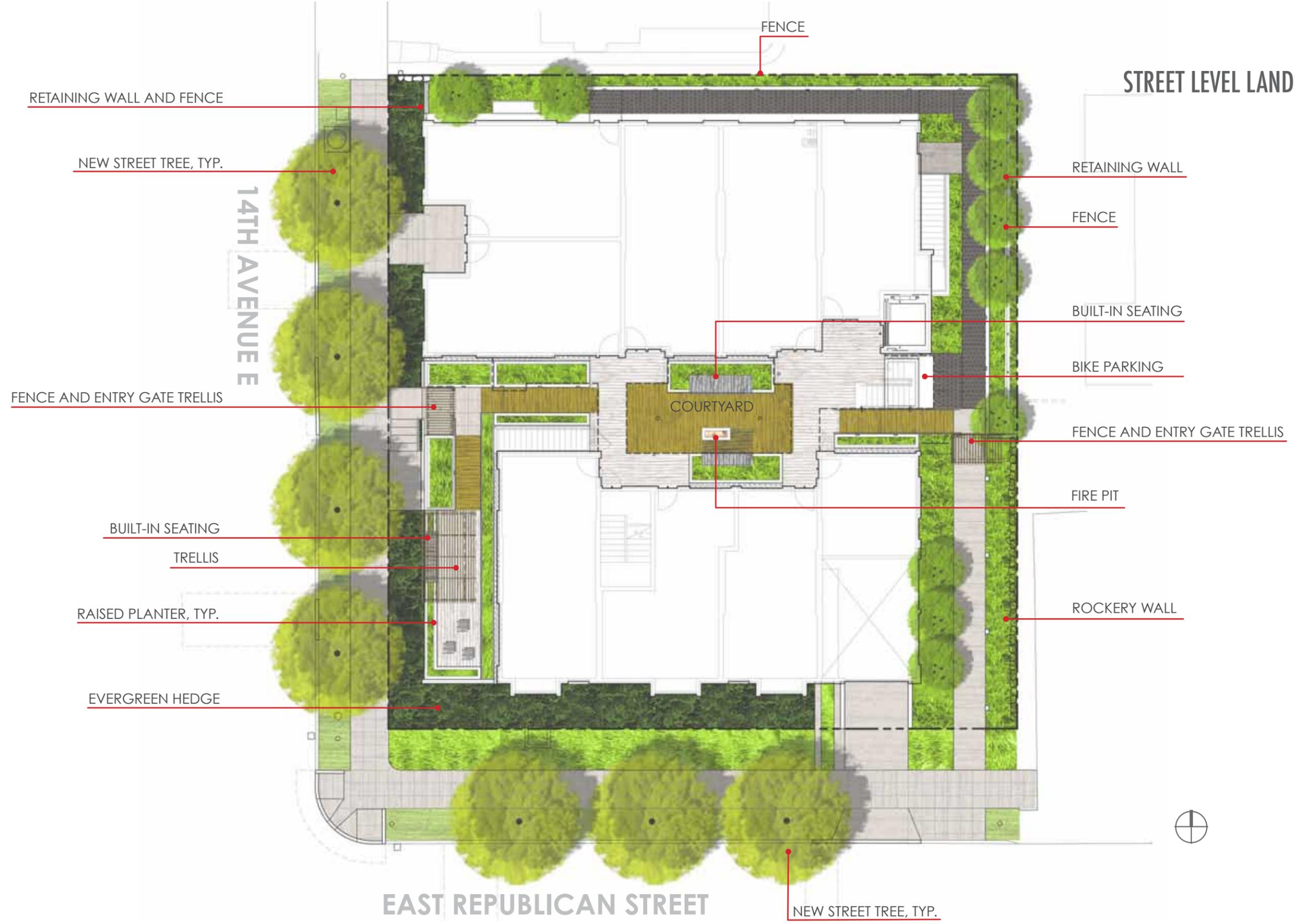
- Quality Ground Level Open Space
- Fostering a Sense of Community
- A Commitment to Sustainable Design

FIG 01: EDG Conceptual Site Design Study



FIG 02: Section Through Courtyard, Looking North

STREET LEVEL LANDSCAPE PLAN



RETAINING WALL AND FENCE

NEW STREET TREE, TYP.

14TH AVENUE E

FENCE AND ENTRY GATE TRELLIS

BUILT-IN SEATING

TRELLIS

RAISED PLANTER, TYP.

EVERGREEN HEDGE

FENCE

RETAINING WALL

FENCE

BUILT-IN SEATING

BIKE PARKING

COURTYARD

FENCE AND ENTRY GATE TRELLIS

FIRE PIT

ROCKERY WALL

EAST REPUBLICAN STREET

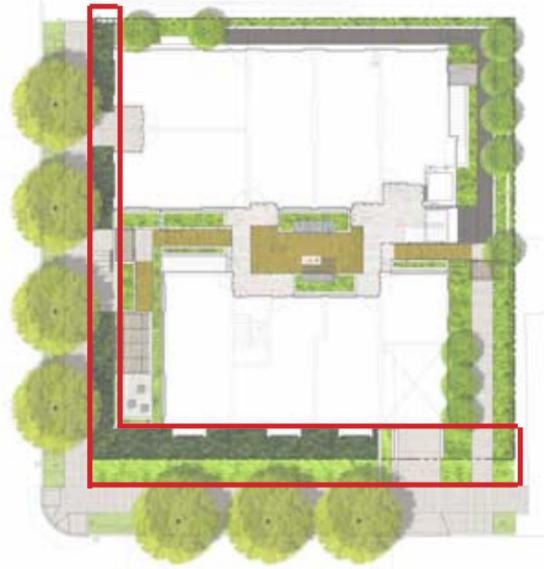
NEW STREET TREE, TYP.



ROOF LANDSCAPE PLAN



STREET EDGE PLANTINGS



CORNUS KOUSA 'STARLIGHT'
Starlight Dogwood



ILEX CRENATA 'CONVEXA'
Convex Leaf Japanese Holly (Hedge)



HYDRANGEA QUERCIFOLIA 'PEE WEE'
Oakleaf Hydrangea



RHUS AROMATIC 'GRO-LOW'
Gro-Low Sumac



ACHILLEA MILLEFOLIUM
Yarrow



CAREX MORROWII
Morrow's Sedge



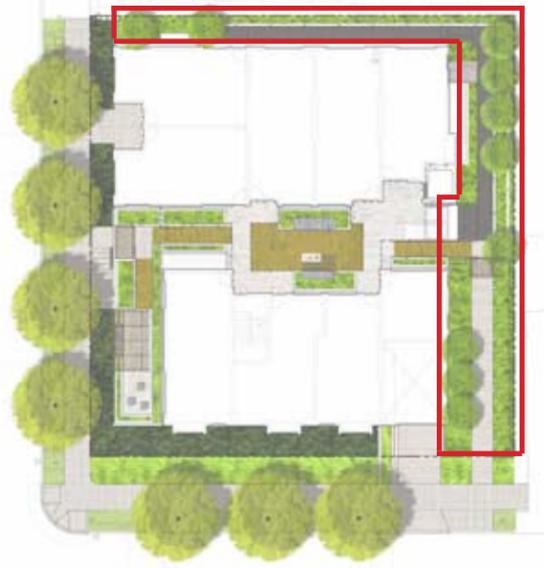
DESCHAMPSIA CESPITOSA
Tufted Hairgrass



SEDUM X 'AUTUMN JOY'
Autumn Joy Sedum

PLANTING PALETTE

GROUND LEVEL PLANTINGS



ACER CIRCINATUM 'MONROE'
Cutleaf Vine Maple



HYDRANGEA MACROPHYLLA 'AMETHYST'
Mophead Hydrangea



ROSA RUGOSA 'ALBA'
Rugosa Rose



SPIRAEA DOUGLASII
Western Spiraea



POLYSTICHUM MUNITUM
Western Swordfern



GAULTHERIA SHALLON
Salal

PLANTING PALETTE



HAKONECHLOA MACRA
'ALL GOLD'
Golden Japanese Forest Grass



CAREX ELATA 'AUREA'
Bowle's Golden Sedge



BLECHNUM SPICANT
Deer Fern



PANICUM VIRGATUM
'ROTSTRAHLBUSCH'
Switchgrass

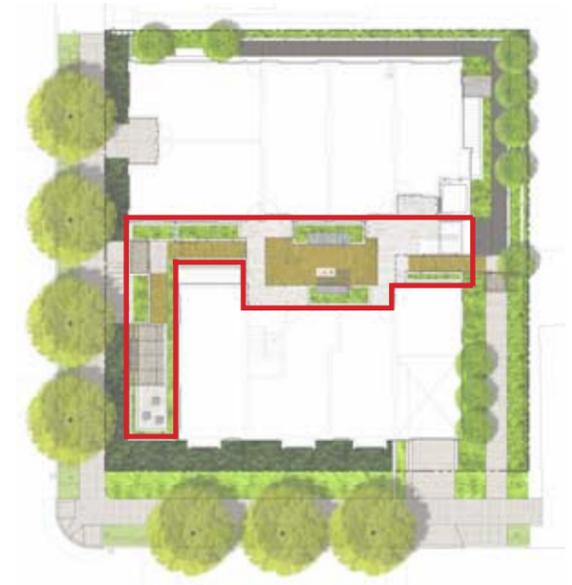


HELICHTOTRICHON SEMPERVIRENS
Blue Oat Grass



HOSTA 'AUGUST MOON'
August Moon Hosta

COURTYARD PLANTINGS



ACER CIRCINATUM 'PACIFIC FIRE'
Pacific Fire Vine Maple



PANICUM VIRGATUM
'ROTSTRAHLBUSCH'
Switchgrass



SEDUM X 'AUTUMN JOY'
Autumn Joy Sedum



LIRIOPE SPICATA
Creeping Lilyturf

ROOFTOP PLANTINGS



SEDUM RUPESTRE 'ANGELINA'
Angelina Sedum



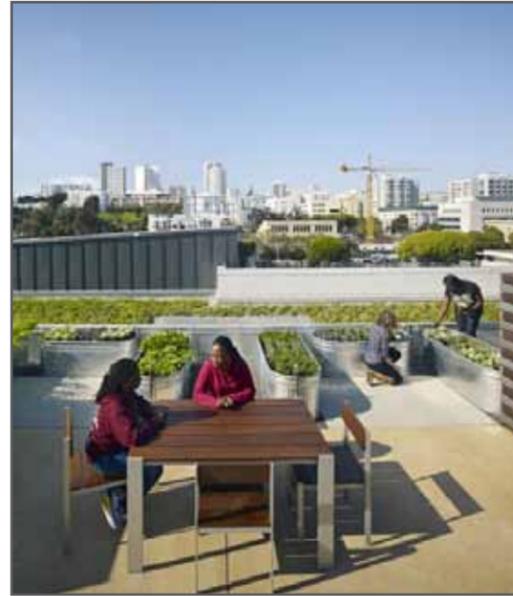
Green Roof Mix



AMENITIES



Courtyard Firepit



Rooftop P Patch

RAILINGS & FENCES



Steel Railings



Perforated Steel Gates

TRELLIS STRUCTURE



Wood Fence



LANDSCAPE MATERIALS PALETTE

FURNISHINGS



Bike Rack

SEATING



Movable Seating



Short Term Bike Parking

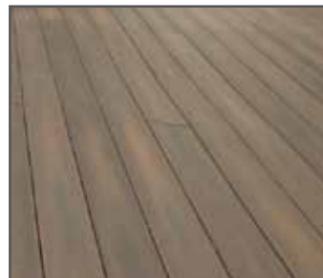


Built-In Seating

PAVING



Pedestal Paving



Composite Decking



Concrete Paving



Permeable Pavers

PLANTERS & WALLS



Courtyard Planter



Concrete Wall



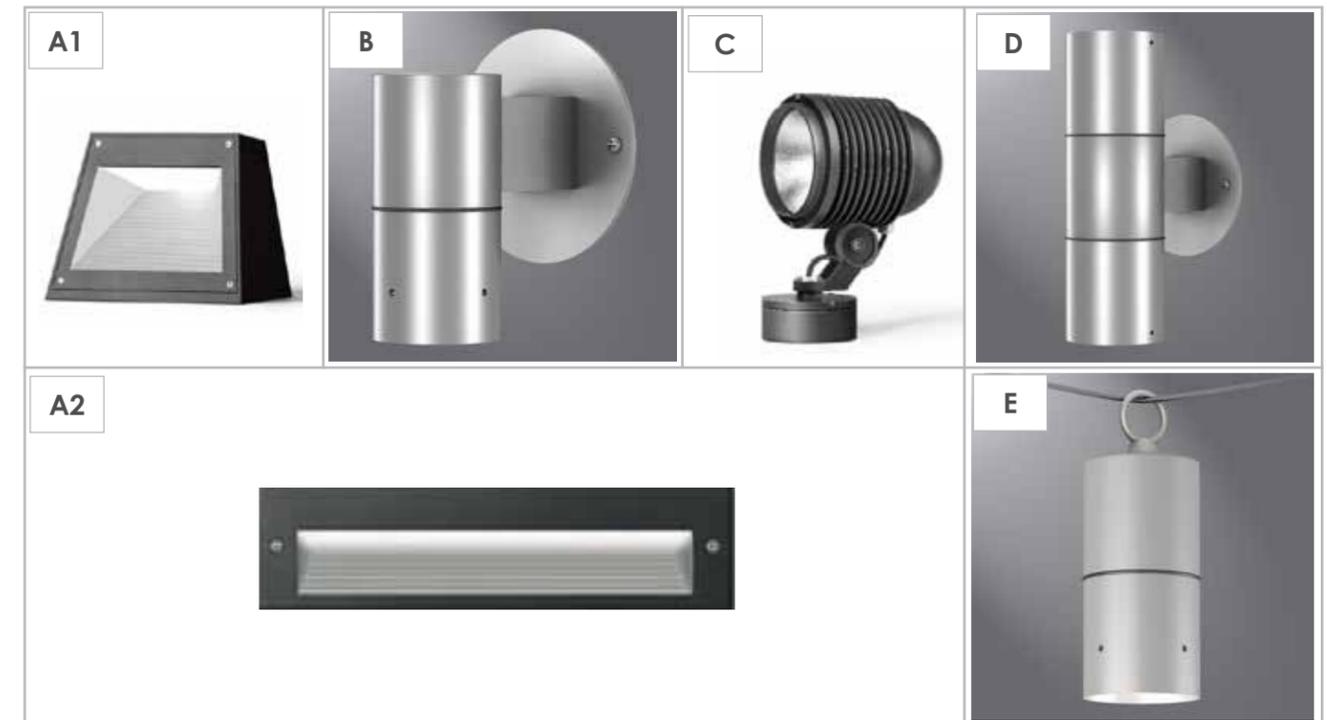
Steel Planter



Ground Level Lighting Diagram

EXTERIOR LIGHTING DIAGRAM

- A. PATH LIGHTING
 - 1. Path Light
 - 2. Wall/Step Light
- B. ENTRY LIGHTING
 - Sconce, Typ.
- C. ACCENT LIGHTING
 - Uplight, Typ.
- D. STAIR LIGHTING
 - Column Mounted Spot
- E. COURTYARD LIGHTING
 - Catenary, Typ.



Fixture Selections



Nighttime View of Southwest Corner

BOARD PRIORITY DESIGN GUIDELINES:

SITE PLANNING

A-2 STREETScape COMPATIBILITY

The siting of buildings should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.
(see Guidance A-3)

A-3 ENTRANCES VISIBLE FROM THE STREET

Entries should be clearly identifiable and visible from the street.

EDG Recommendations: The Board requests greater interaction between the proposed building and the E. Republican St. streetscape. Entrances and stoops would create a more active street front and begin to reduce the scale of the south façade.

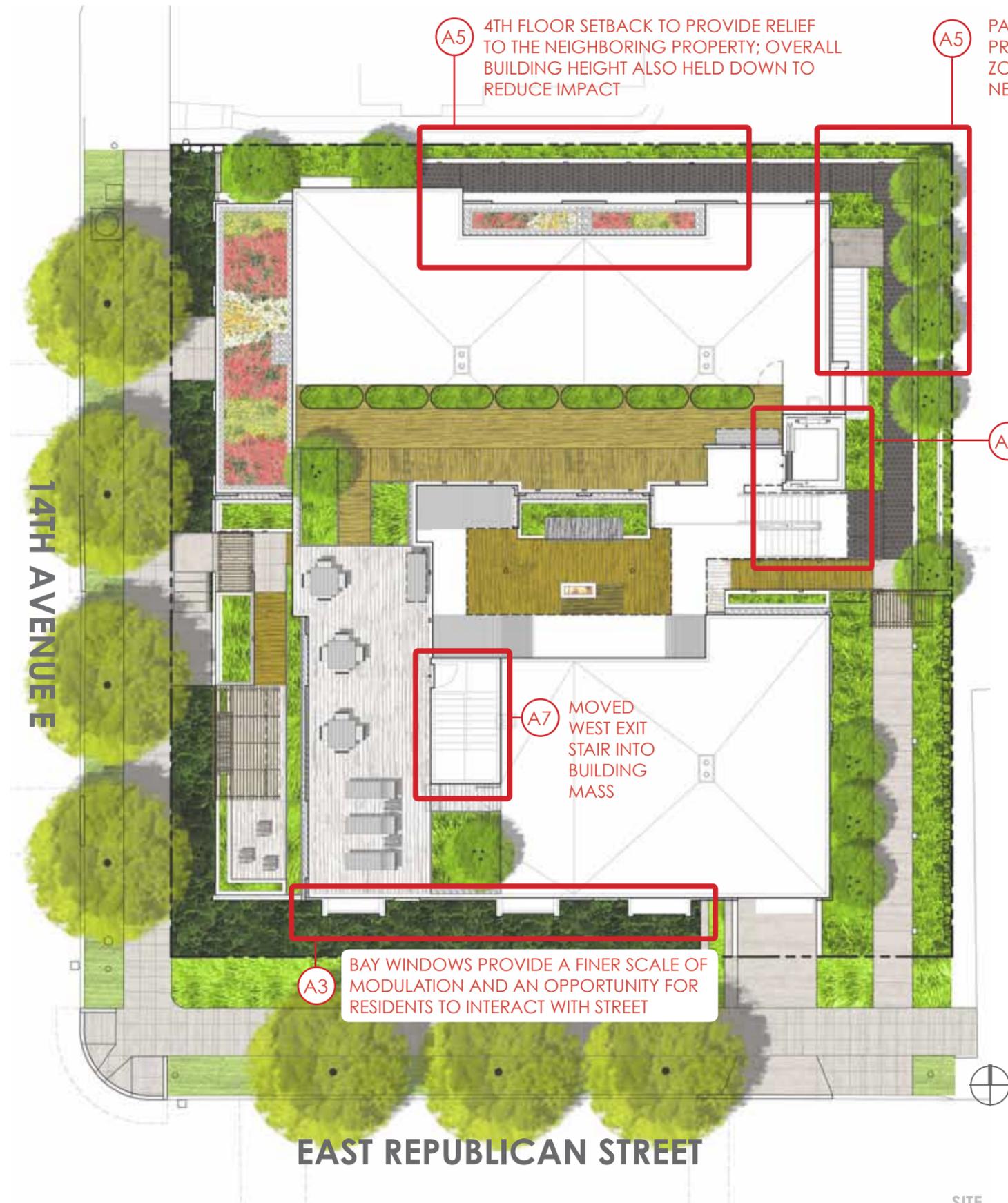
A-4 HUMAN ACTIVITY

New development should be sited and designed to encourage human activity on the street.
(see Guidance A-3)

A-5 RESPECT FOR ADJACENT SITES

Buildings should respect adjacent properties by being located on their sites to minimize disruption of the privacy and outdoor activities of residents in adjacent buildings.

EDG Recommendations: Noting the length and height of the north façade's impact upon the neighbors, the Board expressed its disinclination to grant the departure request related to this façade. Reconsideration of the departure would require greater setbacks and/or significant modification to the height and modulation of the wall. The Board asked for the game court area at the northeast corner to be reconsidered, providing passive landscaping to reduce noise impacts upon the neighbors.(see Guidance E-2)



A5 4TH FLOOR SETBACK TO PROVIDE RELIEF TO THE NEIGHBORING PROPERTY; OVERALL BUILDING HEIGHT ALSO HELD DOWN TO REDUCE IMPACT

A5 PASSIVE GREENSPACE PROVIDING A BUFFER ZONE TOWARDS THE NEIGHBORING PROPERTIES

A7 ELEVATOR MOVED NORTH INTO BUILDING MASS AND HAS BEEN GANGED WITH EAST STAIR

A7 MOVED WEST EXIT STAIR INTO BUILDING MASS

A3 BAY WINDOWS PROVIDE A FINER SCALE OF MODULATION AND AN OPPORTUNITY FOR RESIDENTS TO INTERACT WITH STREET

RESPONSE:

A series of individual bay windows along the south façade facing Republican Street provide modulation and greater interaction at street level. The bay windows with window seats provide a substantial place where one feels encouraged to sit, read, lounge, and watch the street. The bay windows are placed at a height where a resident feels comfortable being seen from the public sidewalk without compromising privacy.

The building along north property line has been modulated through a stepping of the facade, 4th floor setback, and a reduction of overall building height. This will reduce the overall scale of the building towards the north.

The 'game court' area has been replaced with a space supporting more passive uses. The space is a balance of circulation, plantings, and seating. See Response E-2.

A-7 RESIDENTIAL OPEN SPACE

Residential projects should be sited to maximize opportunities for creating usable, attractive, well-integrated open space.

EDG Recommendations: The Board felt that the design of the vertical circulation system cluttered the courtyard, diminishing its usefulness. They noted that the number of staircases appears to be redundant. The Board asked for the relocation of the elevator to widen the view corridor through the complex and to produce a better connection between the rear of the complex and the courtyard. The stairs and elevator should be ganged (with the elevator against a blank wall) rather than spread along the perimeter of the court.

RESPONSE:

In response to the Boards concerns, the circulation system has been revised, in order to create a better flow through the courtyard space. The vertical circulation has been revised such that (a) the elevator has been moved north so it sits tighter within the building (b) the elevator and East exit stairs have been ganged and (c) the West exit stairs have been enclosed within the building mass. These design changes enhance the view corridor through the courtyard and provide additional solar access. The building portal is now an uncluttered, carefully landscaped buffer zone, allowing for a transition between public and private realm.

Great attention has been paid to a smooth integration of various attractive open spaces that will enhance the experience of the tenants and maximize the transparency of the site and its interaction with the streetscape at large.



FIG 01: View through Courtyard, looking East



FIG 02: View through Courtyard, looking West

BOARD PRIORITY DESIGN GUIDELINES:

HEIGHT, BULK & SCALE

B-1 HEIGHT, BULK, AND SCALE COMPATIBILITY

Projects should be compatible with the scale of development anticipated by the applicable Land Use Policies for the surrounding area and should be sited and designed to provide a sensitive transition to nearby, less intensive zones. Projects on zone edges should be developed in a manner that creates a step in perceived height, bulk, and scale between anticipated development potential of the adjacent zones.

EDG Board Recommendation: The Board recommended several changes to the massing to provide relief to the neighbors and create a better relationship to the larger urban patterns established in the neighborhood. These include the following: 1) stepping back the building mass at the upper floors along the north façade or shifting the entire volume away from the north property line, 2) stepping back or eliminating the bridge above the entry portal, 3) varying the cornice or roof line along the west and south façades, 4) reducing the plinth height by another foot or 18 inches, and 5) introducing modulation along the Republican St. facade.

FIG 01: Perspective View of NE Corner



FIG 02: Perspective View of SW Corner

RESPONSE:

The design team has incorporated several changes to the overall building massing, in order to provide relief to the neighbors and create a better relationship to the larger urban patterns established in the neighborhood.

- 1.** 45% of the 4th floor North façade has been pulled back 11'-0" from property line to reduce the apparent mass of the building and provide a greater level of facade modulation. Also, the building height has been held down 3'-3" below the max. building height allowed.
- 2.** The building mass above the entry portal has been articulated from the overall West façade with a 2'-0" modulation from the South building mass.
- 3.** The roof line along the West façade has been articulated by a 1'-0" step between the South and North building masses.

- 4.** As a response to the Boards concern about the plinth height, the design team studied lowering the plinth height further. If we were to lower the plinth below the current elevation, we would then need to include all partially-below grade area in our FAR calculation, which would eliminate our ability to provide parking as reducing the amount of residential area is not an acceptable option for the project. In addition, we are constrained by allowable garage ramp slope limits. Thoughtfully designed landscaping will reduce the plinth's visual impact, while providing a pleasant transition from the public to private realm.
- 5.** A series of individual bay windows are proposed for the South façade facing Republican Street, providing modulation and interaction with the streetscape. Sliding screens along the West and East facade provide dynamic elements that further articulate the facade(s).

C-1 ARCHITECTURAL CONTEXT

New buildings proposed for existing neighborhoods with a well-defined and desirable character should be compatible with or complement the architectural character and siting pattern of neighboring buildings.

EDG Board Recommendation: Noting their appreciation for the architect's close analysis of the site's urban context, the Board responded with several observations. The larger apartment buildings in the neighborhood tend to be three or three and a half floors. The open entry portals shown on the context photographs are typically for vehicular parking and the pedestrian entrances telescope to a finer grain beyond the gate or portal.

RESPONSE:

The entry portal balances the needs of providing a pedestrian entry to the project while providing a proper view corridor through the site. The portal serves as a distinguishing element of the transition from public to private, providing a zone of compression that opens into the central courtyard. Incorporation of an entry gate and articulation of the ground plane with softscape and hardscape provides a finer of grain definition beyond the larger mass of the portal.



FIG 01: View through Courtyard, looking East



FIG 02: Sliding Cedar Screens in Open and Closed Position



C-2 ARCHITECTURAL CONCEPT AND CONSISTENCY

Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept. Buildings should exhibit form and features identifying the functions within the building. In general, the roofline or top of the structure should be clearly distinguished from its facade walls.

EDG Board Recommendation: The Board reaffirmed this guideline's importance.

RESPONSE:

The design team has developed a series of finely detailed elements, such as sliding cedar screens and bay windows, creating a tactile and visually interesting facade. The overall siting of the building is expressed with an emphasize on exterior open space encouraging ground level pedestrian activity.

BOARD PRIORITY DESIGN GUIDELINES:

ARCHITECTURAL ELEMENTS & MATERIALS

C-3 HUMAN SCALE

The design of new buildings should incorporate architectural features, elements, and details to achieve a good human scale.

EDG Board Recommendation: The Board felt that the trellis at the SW corner should be reduced in scale introducing greater intimacy to the corner open space. (See E-2 for further guidance.)

RESPONSE:

The trellis structure has been reduced in scale and relocated in order to provide a comfortable relationship to the streetscape while defining the corner amenity space.



FIG 01: View Corner Amenity Trellis, looking South

C-4 EXTERIOR FINISH MATERIALS

Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.

EDG Board Recommendation: The Board noted its support for a modern interpretation of historic precedent and encourages the use of high quality materials.

RESPONSE:

The design team has developed a warm, contemporary pallet with a combination of cedar siding and painted fiber cement board. Custom movable sun screens on the west and east facade, create visual interest and provide a dynamic expression.



FIG 02: View of Entry Walk from East Republican Street

C-5 STRUCTURED PARKING ENTRANCES

The presence and appearance of garage entrances should be minimized so that they do not dominate the street frontage of a building.

EDG Board Recommendation: The Board noted its likely support for the garage door departure request.

RESPONSE:

Further detail and the departure request is provided in the Departure Request section of the submittal.



FIG 01: Street Section 14th Ave E, Looking North



FIG 02: Street Section 14th Avenue East looking North



FIG 03: Street Section Republican St, Looking East

D-3 RETAINING WALLS

Retaining walls near a public sidewalk that extend higher than eye level should be avoided where possible. Where higher retaining walls are unavoidable, they should be designed to reduce their impact on pedestrian comfort and to increase the visual interest along the streetscapes.

EDG Board Recommendation: The site grades and the parking garage combine to place the building higher above the sidewalk grade than what the Board wishes. This acts to create an uncomfortable relationship between the building and the streetscape.

RESPONSE:

Typical relationship between sidewalk grades and ground floor elevation in surrounding district ranges from 18" to more than 48". The proposed ground floor elevation for our building fits well within this range and the surrounding neighborhood. If we were to lower the plinth below the current elevation, we would then need to include all partially-below grade area in our FAR calculation, which would eliminate our ability to provide parking as reducing the amount of residential area is not an acceptable option for the project. In addition, we are constrained by allowable garage ramp slope limits. The proposed ground floor elevation will actually be lower and have a greater setback than the existing grade on the site, thereby enhancing the pedestrian experience of the site.

D-1 PEDESTRIAN OPEN SPACES AND ENTRANCES

Convenient and attractive access to the building's entry should be provided. To ensure comfort and security, paths and entry areas should be sufficiently lighted and entry areas should be protected from the weather. Opportunities for creating lively, pedestrian-oriented open space should be considered.

EDG Board Recommendation: See Board guidance for the courtyard in Section A7.

D-7 PERSONAL SAFETY AND SECURITY

Project design should consider opportunities for enhancing personal safety and security in the environment under review.

EDG Board Recommendation: The board noted that a concept lighting plan is required for the Recommendation meeting.

RESPONSE:

A full lighting plan has been provided in the Design section of the submittal.

BOARD PRIORITY DESIGN GUIDELINES

LANDSCAPING



FIG 01: View of SW Corner Amenity Space

E-2 LANDSCAPING TO ENHANCE THE BUILDING AND/OR SITE.

Landscaping, including living plant material, special pavements, trellises, screen walls, planters, site furniture, and similar features should be appropriately incorporated into the design to enhance the project.

EDG Board Recommendation: The Board requested for the game court area at the northeast corner to be replaced with passive landscaping to reduce noise impacts upon the neighbors. The Board felt, that the proposed trellis along 14th Ave E. appears monumental and out of scale as it attempts to echo the two-story volume to the north of the entry portal. The board felt that the trellis should be reduced in scale introducing greater intimacy to the corner open space. The Board was concerned that the raised patio would seem to be the province of the adjacent unit and less of an amenity for all of the residents given the transparency along the 14th Ave facade.

RESPONSE: In response to the Board's concern over noise impact on the neighbors, the former 'game court' area has been replaced with a space supporting more passive uses, such as reading or enjoying coffee. (See A-5)

The trellis at the South West corner of the building has been reduced to a single story volume more in keeping with the scale of the space. The trellis location has also been adjusted to 'miss' ground floor unit windows and enhance privacy for adjacent units. Taller raised planters (2' above paving finished grade) with vertically oriented plantings provide additional separation between private and semi-public space. (See C-3)

The amenity space is envisioned as a passive semi-private space to be shared by the residents of the community, creating a visual connection to the streetscape while preserving a comfortable sense of separation and privacy via grade change and continuous planted evergreen hedge.

SMC 23.41.012.A - Development Standard Departures

Departures may be allowed if an Applicant demonstrates that departures from Land Use Code requirements would result in a development that better meets the intent of the adopted Design Guidelines.

SMC 23.45.527.B.1 - Maximum Façade Length Required/Allowed

Requirements:

The maximum façade length of all portions of façades within 15 feet of a lot line that is neither a rear lot line nor a street or alley lot line shall not exceed 65 percent of the length of that lot line.

Proposed:

An additional 13'-6" of façade length on the north interior lot line for a façade length of 78.6% of the total lot line length.

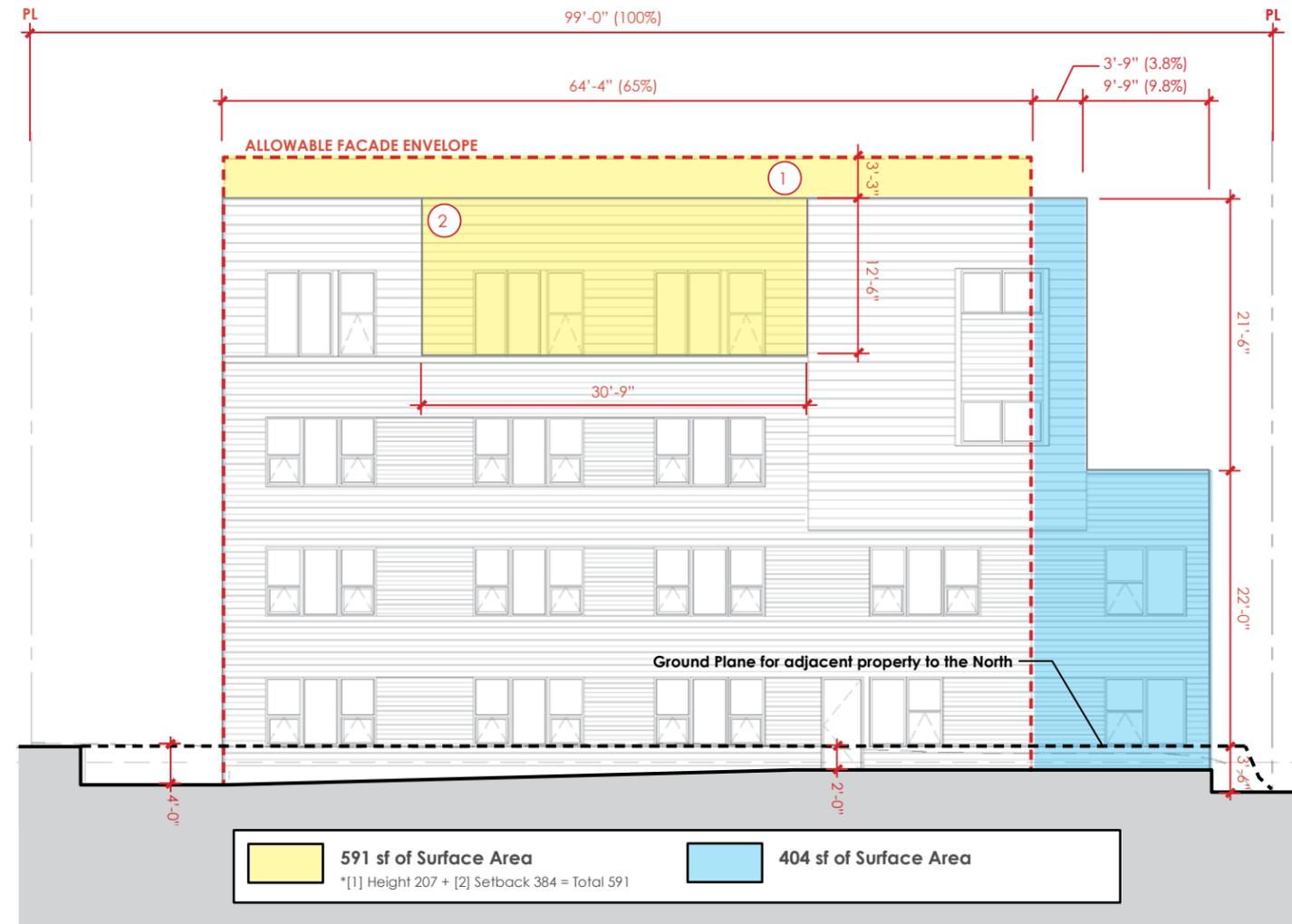


FIG 01: North Facade Departure Sketch

How it Better Meets the Intent of Adopted Design Guidelines:

Maintains the proposed East-West view corridor through the middle of the site and maintain the proposed walk-up units along 14th Avenue East. The view corridor and walk-up units reflect the street/block pattern along 14th (diagrams on pages 2 & 4). Subscribing to the 65% facade length limit would require the project to (a) re-orient the North West ground floor units to the North-South direction eliminating walk-up access from 14th and (b) shift the two-story mass at the North West corner to the South blocking the current view corridor. This departure allows the project to better meet the following design guidelines:

1. A-2 Streetscape Compatibility, p. 28
2. A-3 Entrances Visible from the Street, p. 28
3. A-4 Human Activity, p. 28
4. A-7 Residential Open Space; p. 29
5. C-1 Architectural Context; p. 31
6. D-1 Pedestrian Open Space; p. 33

Response to Design Board Guidance:

- 1 Building Height has been held down 3'-3" below maximum allowable height.
- 2 45% of 4th Floor along North facade has been set back 11'-0" from property line.

SMC 23.45.536.D.3.b - Screening of Parking by Garage Doors

Requirements:

Garage doors facing the street shall be set back at least 15 feet from the street lot line, and shall be no closer to the street lot line than the street-facing façade of the structure.

Proposed:

Place the garage door 7'-5" from the street lot line.

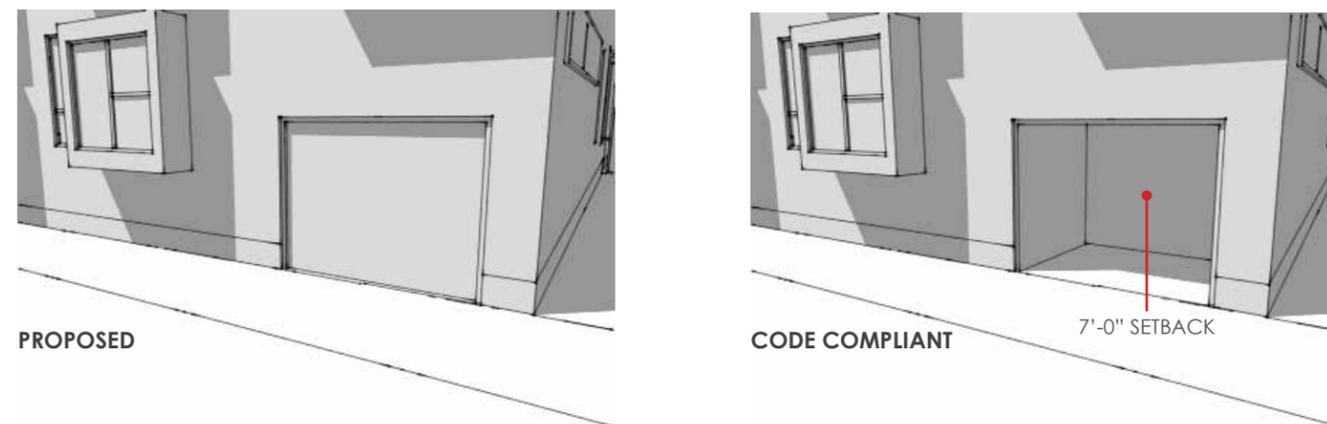


FIG 02: Garage Door Departure Sketch

How it Better Meets the Intent of Adopted Design Guidelines:

If the project garage door was located per code (SMC 23.45.536.D.3.b Screening of Parking by Garage Doors), it would be set back 7.5' from the face of the building to meet the 15' minimum setback. The proposed departure proposes locating the door along the building face, which is set back 14' from the back of sidewalk. This orientation promotes safety, as it would eliminate a 7.5' additional setback (recess) into the building and is architecturally consistent with adjacent garage access that is typically set flush with the building face. This departure allows the project to better meet the following design guidelines:

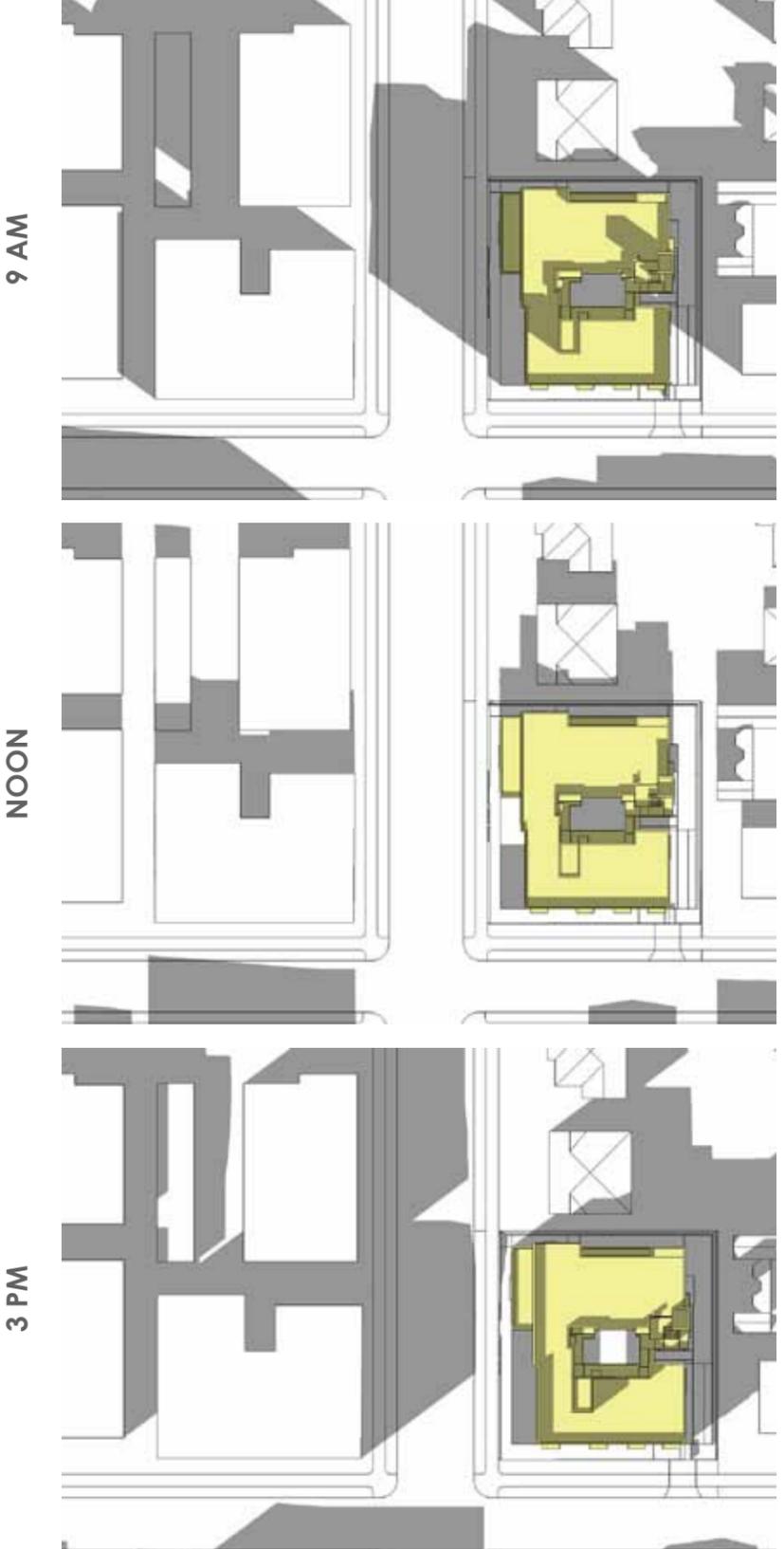
1. A-2 Streetscape Compatibility, p. 28
2. A-3 Entrances Visible from the Street, p. 28
3. A-6 Transition between Residents and Street

SHADOW STUDIES (RECOMMENDATION MEETING)

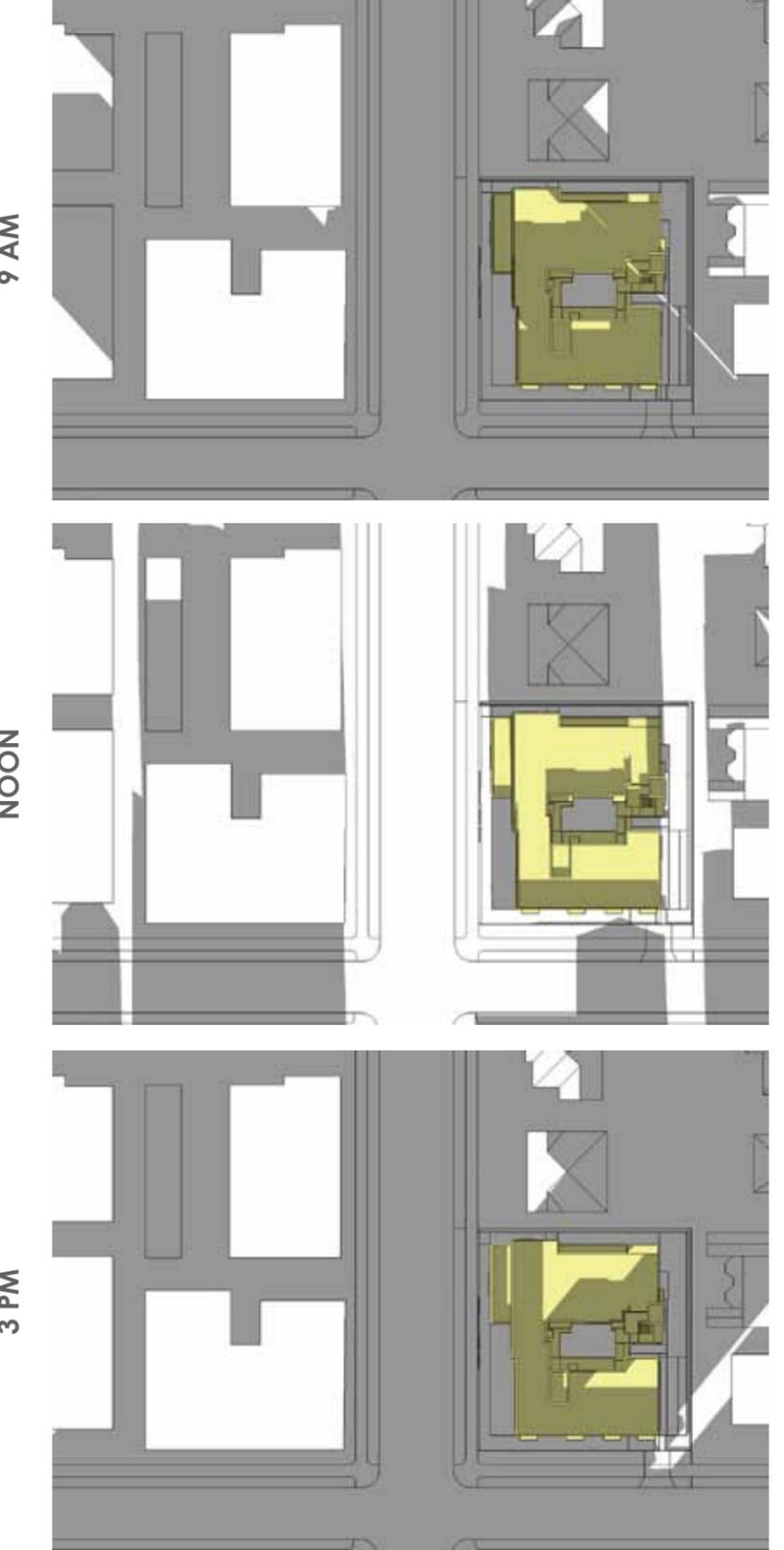
SUMMER SOLSTICE



EQUINOX



WINTER SOLSTICE



PROJECT RENDERING



View from SW Corner at the Intersection of 14th and Republican



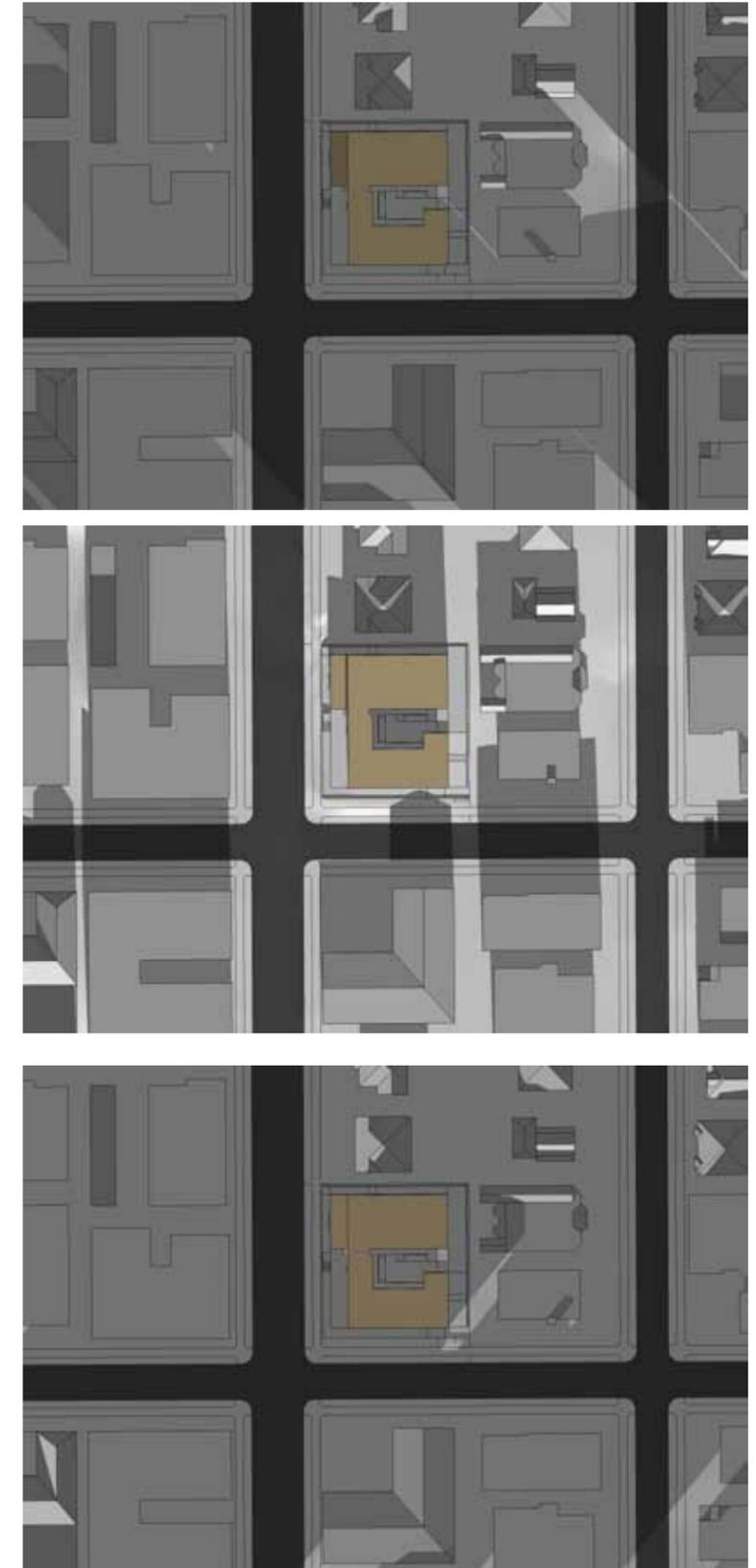
View from SE Corner on Republican Street

PROJECT RENDERING



View from NW Corner on 14th Avenue East

WINTER SOLSTICE



EQUINOX



SUMMER SOLSTICE



9 AM

NOON

3 PM

9 AM

NOON

3 PM

9 AM

NOON

3 PM

EXISTING SITE ANALYSIS

Setback Requirements:

Front Setback: 5' min.
 Rear Setback: 15' min. with no alley
 Side Setback for facades 40' or less in length: 5'
 Side setback for facades greater than 40' in length: 5' min., 7' average.

Existing Lot Elevation:

Along Republican, the sidewalk sits 4'-5' below the site.
 Along 14th Ave, the sidewalk sits 4' below the site at the southwest corner, and slopes up to 2-1/2' below the site at the northwest corner. The entire site slopes up 2-1/2' from the southwest corner to the northeast corner.

