

8TH AVENUE APARTMENTS

4527, 4531, 4535, 4539, 4543 8TH AVENUE NE



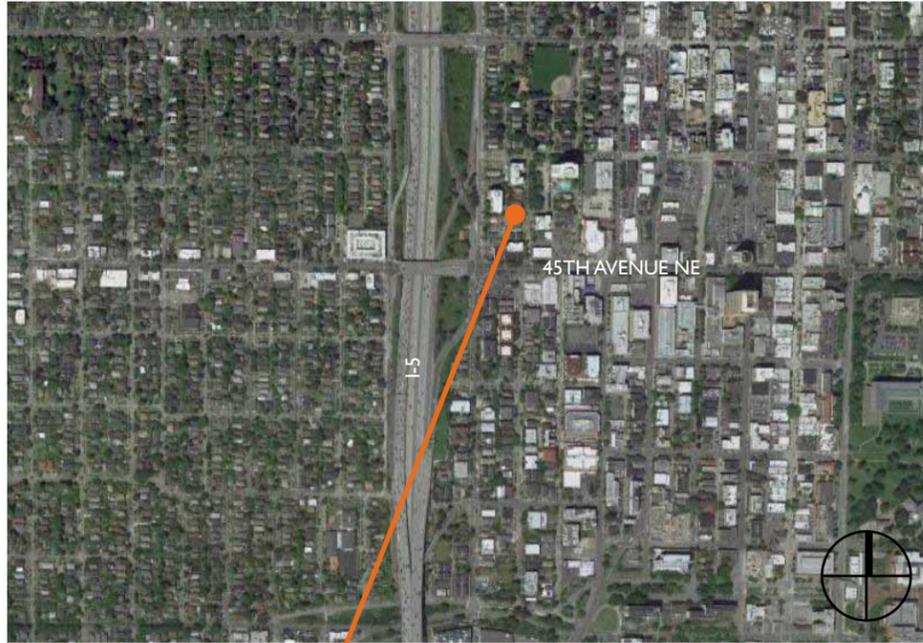
EARLY DESIGN GUIDANCE

DPD #3012547

4302 SW ALASKA STREET
SEATTLE, WA 98116
206.933.1150
www.nkarch.com



PROJECT DESCRIPTION



SITE LOCATION

ADDRESS: 4527, 4531, 4535, 4539, 4543 8th Avenue NE
DPD PROJECT #: 3012547
OWNER: Intracorp
APPLICANT: Nicholson Kovalchick Architects
CONTACT: Jill Burdeen

DEVELOPMENT OBJECTIVES

The property owner's objective is to construct a new 7-story apartment building with approximately 152-164 units and 90-100 parking stalls. Additionally, the intention is to provide housing for students and young professionals that encourages social interaction, while incorporating sustainable design strategies to preserve resources.

Potential departure requests from development standards would be to develop a building with an overall structure width greater than 150 feet, as well as an overall structure depth greater than 75% of the lot depth, to allow an above grade parking structure within the rear setback along the alley, and to allow the below grade parking structure to encroach slightly into the front yard setback.

PROJECT PROGRAM

Number of Residential Units:	Approximately 152-164
Number of Parking Stalls:	Approximately 90-100
Area of Residential Levels:	Approximately 84,000-94,000 sf
Area of Parking Level:	Approximately 28,000 sf
Total Area:	Approximately 117,000-123,500 sf

EXISTING SITE

The site is composed of 5 tax parcels located mid block along 8th Avenue NE, between NE 45th and NE 47th. The parcel is rectangular and measures 202 feet wide by 107 feet deep. An alley at the west side of the site connects to NE 45th Street and NE 47th Street.

The site currently contains 2 single-family residences and 3 triplexes, as well as surface parking along the alley.

The site slopes from the highest point on the northwest corner to the lowest point on the southeast corner. There is little vegetation on the site.

ZONING AND OVERLAY DESIGNATION

The parcel is zoned MR and is in the University District Northwest Urban Center Village. The surrounding area is zoned NC3-65 to the South and NC3-85 to the East and MR to the North and West. Per the DPD's GIS map, this area is a Frequent Transit Corridor.

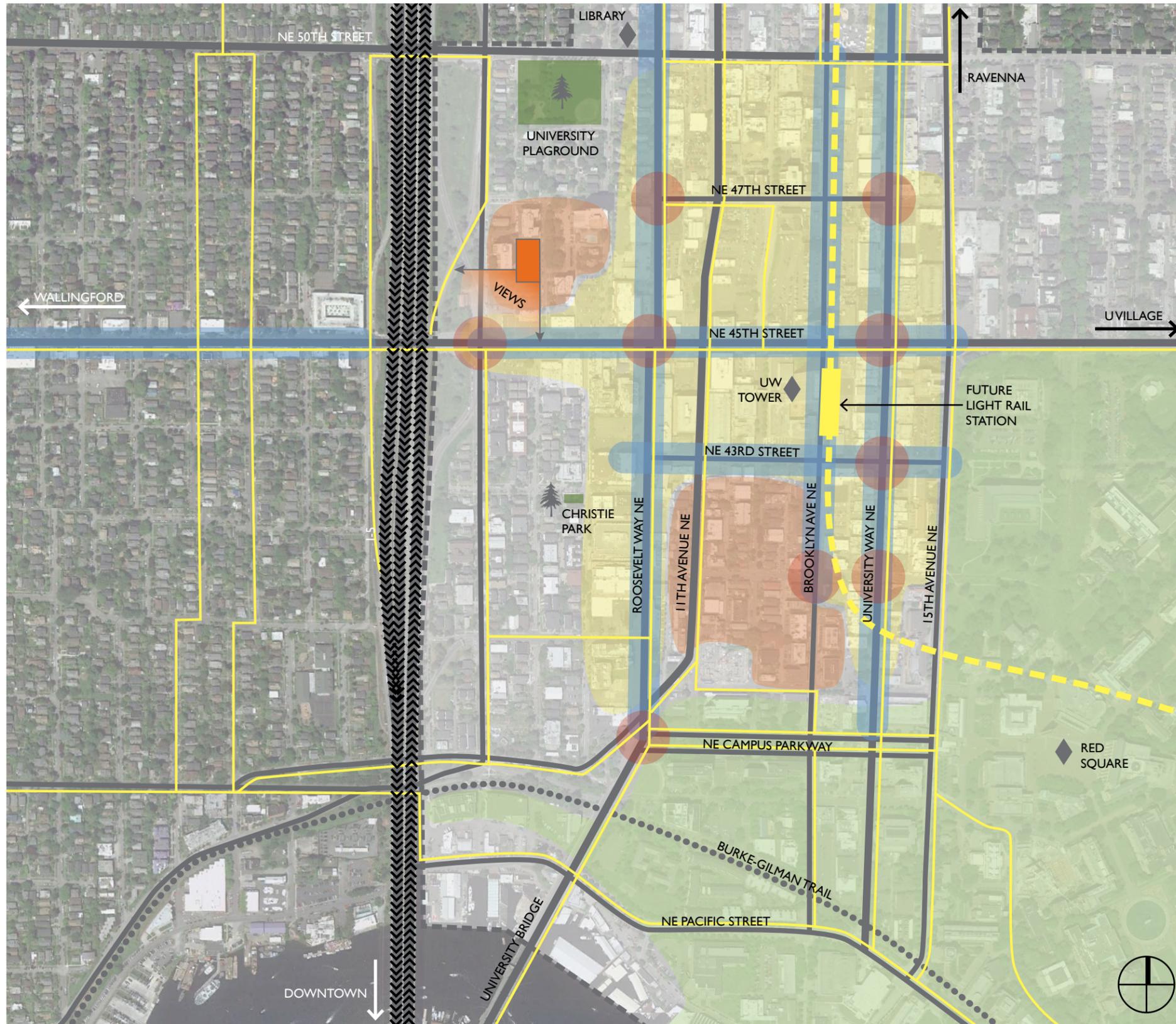
NEIGHBORING DEVELOPMENT

The proposed site is located within the University District, which is largely comprised of single-family homes, townhouses and mid-size to large apartment/condominium buildings. Additionally, there are several commercial pockets and streets located south and east of the project. NE 45th Street, which is located a half block south of the project, is a major arterial. Interstate-5, the main north-south transportation route/corridor through Washington State, is located a little over one block to the west of the project site. The project site is within walking distance of the University of Washington, and is in a pedestrian friendly neighborhood.

The University District is a diverse neighborhood with a wide array of building typologies. In the immediate vicinity of the proposed project there are single-family houses, townhomes, mid-size condominiums and a couple of high-rise buildings. Additionally, there are numerous commercial buildings as well as a movie theatre.

Across the alley, to the west, is a 10-story Seattle Housing Authority apartment complex. Directly across the street to the east lies the tallest building in the vicinity, the 24-story University Plaza Condominium. The University Plaza is a community of condominiums built in the mid-1970's and is one of only a few high-rise condominiums located within Seattle's neighborhoods. Directly north of the project is the recently constructed Duncan Place Condominium Building. This 63-unit brick building includes an eastern facing entry courtyard, accessed off of 8th Avenue NE. There are two single-family structures south of the project site, which were built in 1908, and appear to be mirrored versions of one another. Directly south of the single-family houses is a 4-unit townhouse project built in 2006.

One block to the east is one of Seattle's Landmark movie theaters, Metro Cinemas. There are several other retail centers and small one-story commercial buildings located south of the project location, along NE 45th Street. NE 45th Street is a major arterial which generates a large amount of vehicular traffic, due to its direct access to both I-5 to the west and The University of Washington to the east.



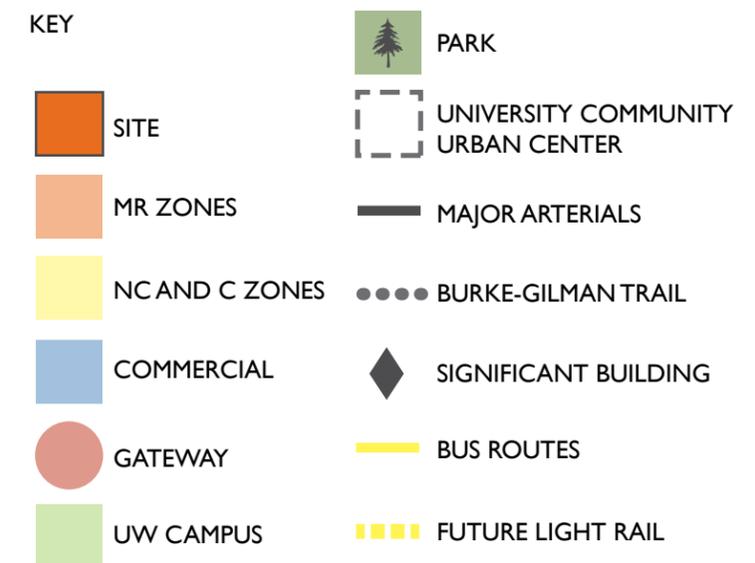
OPPORTUNITIES & CONSTRAINTS

The site is located in the MR zone in the University District Northwest Urban Center Village, one of the two primary urban villages represented within the University Community Urban Center (UCUC). The University Community Guidelines acknowledge the diversity of the University District, from the architecture, to the building typology. The site is located amongst several of the tallest buildings in the vicinity. Directly across the street, to the east, is the 24-story University Plaza Condominium Tower, one of only a few high-rise condominiums located within Seattle's neighborhoods. Across the alley to the west lies a Seattle Housing Authority 10-story apartment building. Additionally, there are numerous buildings in each direction of similar scale located within blocks of the project site.

The site is located within walking distance of the University of Washington, as well as numerous commercial districts and 2 movie theatres. The neighborhood is vastly pedestrian friendly and well served by public transportation. A future light rail station is planned several blocks to the east.

The site is largely bound by major arterials. Major streets include I-5 two blocks to the west and NE 45th Street to the south. I-5 is the main north-south transportation corridor through Washington State and NE 45th provides direct access to both I-5 and the University of Washington.

Views of downtown can be seen looking southwest from the project site.



IMMEDIATE SITE CONTEXT



① UNIVERSITY PLAZA CONDOS



② UNIVERSITY PLAYGROUND



③ UNIVERSITY PUBLIC LIBRARY



④ DUNCAN PLACE CONDOS



⑤ SINGLE FAMILY STRUCTURES



⑥ GRANDVIEW PLAZA CONDOS



⑦ SHA UNIVERSITY WEST



⑧ BLUE MOON TAVERN



⑨ WEST UNIVERSITY CENTER



⑩ I-5



⑪ NE 45TH ST AND 7TH AVE NE



⑫ VIEW TOWARD DOWNTOWN



① ELLIPSE APARTMENTS



② SUN PLAZA CONDOS



③ UNIVERSITY HOUSE APARTMENTS



④ ACACIA COURT APARTMENTS



⑤ PLACE APARTMENTS



⑥ HELIX APARTMENTS



⑦ LA TERRAZZA CONDOS



⑧ KENNEDY APARTMENTS



⑨ TRINITY APARTMENTS



⑩ TRINITY APARTMENTS



⑪ WATERTOWN HOTEL



⑫ COLLEGE PLACE APARTMENTS



⑬ UW WEST CAMPUS HOUSING



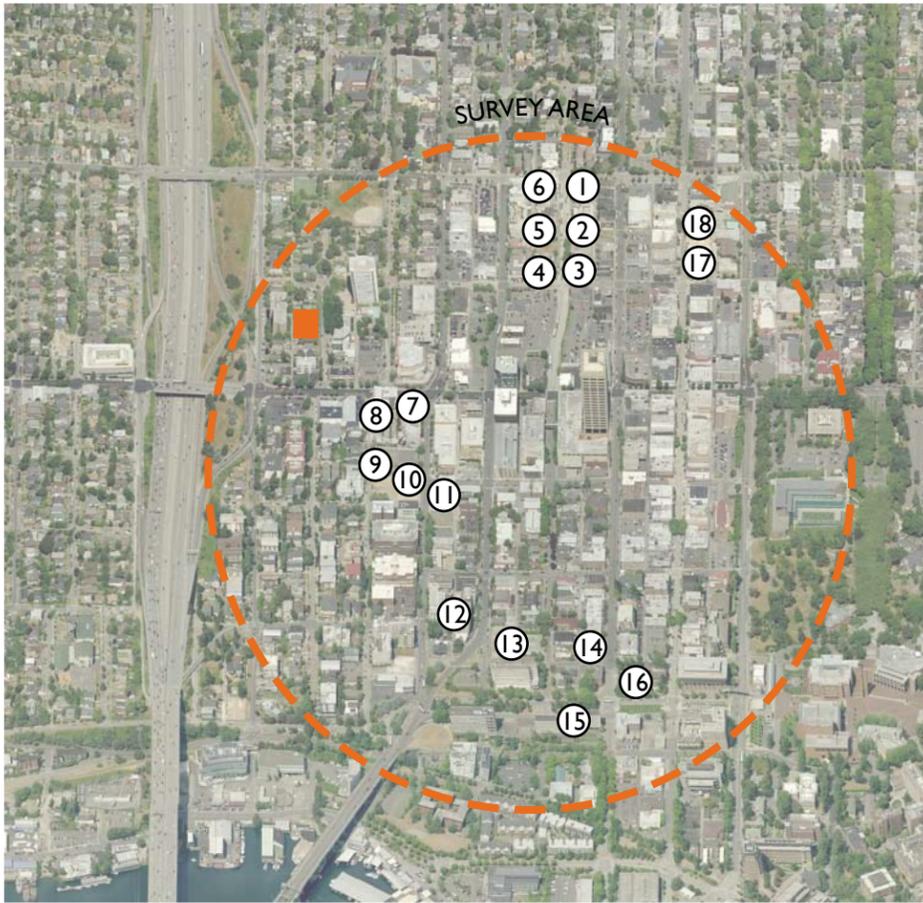
⑭ LEVERE APARTMENTS



⑮ UW LANDER HALL



⑯ UW POPLAR HALL

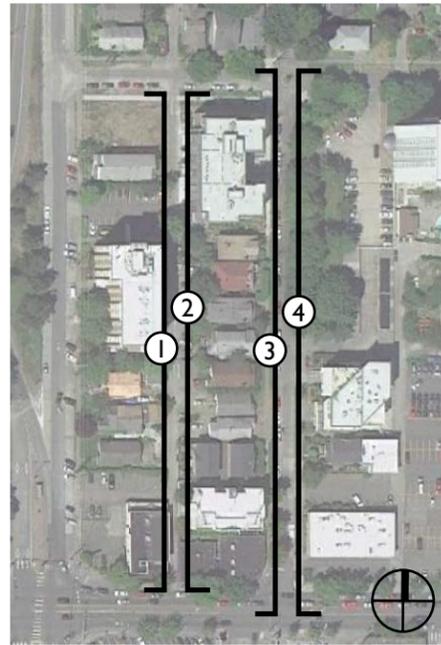


⑰ WILSONIAN APARTMENTS



⑱ LOTHLORIEN APARTMENTS

STREETSCAPE



① ALLEY LOOKING WEST



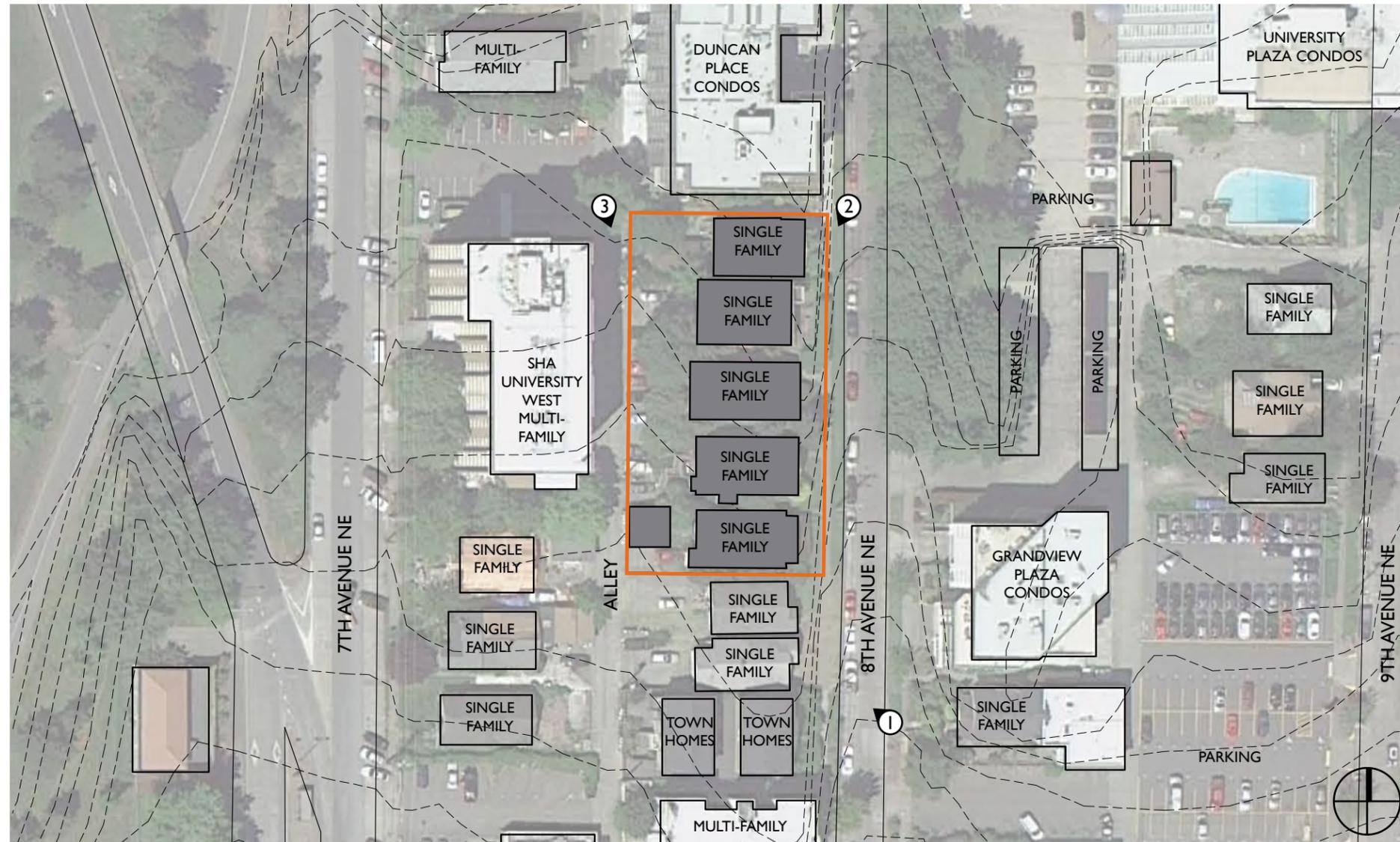
② ALLEY LOOKING EAST



③ 8TH AVENUE NE LOOKING WEST



④ 8TH AVENUE NE LOOKING EAST



RELEVANT DESIGN GUIDELINE PRIORITIES

* Denotes Relevant 'University Community Design Guideline' Priorities

- A-2*** Streetscape Compatibility
 - Location of lobbies and courtyards will be important, and should reinforce pedestrian activity.
- A-3*** Entrances Visible from the Street
 - Building entrances visible from the street encouraging pedestrian interaction.
 - Residential entries promote visual access and security.
 - Courtyard will have at least one entry from the street.
 - Units facing the Courtyard will have porches or stoops associated with the units into the courtyard.
- A-7*** Residential Open Space
 - Preferred project proposals provide an exterior landscaped ground-level courtyard that offers light, air, and sustainable landscape elements to the units and amenity spaces providing for the comfort, health and recreation of residents.
 - Ground-level open space will increase privacy and reduce visual impacts to neighboring properties.
 - Ground-level open space will provide a landscaped front yard, providing a transition between public and private realms.
 - Rooftop decks will integrate sustainable landscape design elements.
- A-8** Parking and Vehicular Access
 - Vehicular access will be located off the alley, minimizing the impact of automobile parking and driveways on the pedestrian environment, adjacent properties and pedestrian safety.
 - Underground parking will be provided in all design options, minimizing the impacts of driveways and parking lots.
- B-1*** Height, Bulk, and Scale Compatibility
 - All schemes break up the facades utilizing either a courtyard or by breaking up the project into 2 buildings.
 - Exterior exposure will be provided in all schemes and will be maintained through to the lobbies.
 - The project will focus on Scale Compatibility with neighboring projects.
- C-4*** Exterior Finish Materials
 - Project materials will be durable and attractive.
 - Project materials will comply with University Community Design Guidelines.
- D-1*** Pedestrian Open Spaces and Entrances
 - Pedestrian entryways will attempt to link the building to the sidewalk and exterior courtyard/walkway and will provide personal safety.
 - Ground level open space provided will improve the quality of the pedestrian environment.
- E-2** Landscaping to Enhance the Building and/or Site
 - Landscape design will enhance the character of the building and link the project to the neighborhood.



① SOUTHEAST CORNER OF SITE



② EAST EDGE OF SITE ALONG 8TH AVENUE NE



③ WEST EDGE OF SITE ALONG ALLEY

ZONING ANALYSIS

PARCEL #: 2097700076, 8812400200-0210-0220-0230
ZONING: MR
OVERLAYS: University District Northwest Urban Center Village
LOT AREA: 21,632 SF

23.45.504 PERMITTED USES

Permitted outright: Residential

23.45.510 FLOOR AREA RATIO

Base FAR: 3.2
 Maximum FAR: 4.25
 Maximum FAR per sustainable design and affordability incentives (SMC 23.45.516, SMC 23.45.526, SMC 23.58A.014)

23.45.514 STRUCTURE HEIGHT

Allowed Maximum Structure Height:

- Base Height: 60'-0"
- Maximum bonus height per incentives: 75'-0"
- 4' additional allowed for parapets: 79'-0"
- 15' additional allowed for stair penthouse: 90'-0"
- 16' additional allowed for elevator penthouse: 91'-0"

23.86.006 STRUCTURE HEIGHT MEASUREMENT

The height of a structure is the difference between the elevation of the highest point of the structure not excepted from applicable height limits and the average grade level ('average grade level' means the average of the elevation of existing lot grades at the midpoints, measured horizontally, of each exterior wall of the structure or at the midpoint of each side of the smallest rectangle that can be drawn to enclose the structure)

23.45.518 SETBACK REQUIREMENTS

Front setback:

- 7' average, 5' minimum
- No setback required if a courtyard abuts street, and the courtyard is minimum 30% width of abutting street frontage or 20' whichever is greater, and minimum 20' deep measured from street

Rear setback:

- 10' if abutting an alley

Side setback from interior lot line:

- For portions 42' high or less, 7' average setback and 5' minimum setback
- For portions higher than 42', 10' average setback and 7' minimum setback

Additional setbacks:

- 10' minimum required separation between principal structures at any two points on different interior facades
- Projections permitted in setbacks to various depths for:
- Cornices, eaves, roofs, et al.; garden and bay windows; unenclosed decks, porches, balconies, or steps; ramps or bridges; underground structures; fences; bulkheads and retaining walls; arbors; et al.

23.45.522 AMENITY AREA

Required: 5% of gross floor area in residential use
 Option 1: 5% X 87,948 sf = 4,397 sf required
 Option 2: 5% X 101,125 sf = 5,056 sf required
 Option 3: 5% X 86,899 sf = 4,344 sf required

General requirements:

- All units shall have access to private or common amenity area
- No more than 50% of the amenity area may be enclosed, and this enclosed area shall be provided as common amenity area
- No minimum horizontal dimension for private amenity areas, except 10' at non-street side lot lines

Requirements for apartments, rowhouses, and townhouses:

- No common amenity area shall be less than 250 sf in area, and common amenity areas shall have a minimum horizontal dimension of 10'
- Min. 50% of common amenity area at ground level shall be landscaped
- Seating, lighting, outdoor protection, art, et al. shall be provided
- Common amenity area req'd at ground level will be accessible to all units

23.45.524 LANDSCAPING REQUIREMENTS

Green Factor score minimum 0.5 required

23.45.528 STRUCTURE WIDTH AND DEPTH FOR MIDRISE LOTS GREATER THAN 9,000 SF

Max Width Allowed: 150'-0"
 Max Depth Allowed: 75% of the depth of the lot
 - Exception per SMC 23.45.528.B.2 (Courtyard at street), as long as lot coverage not increased

23.54.015 REQUIRED PARKING

No parking is required for uses in multi-family zones located in urban centers
 Bicycle long-term parking: 1 per 4 units.

Curb cuts:

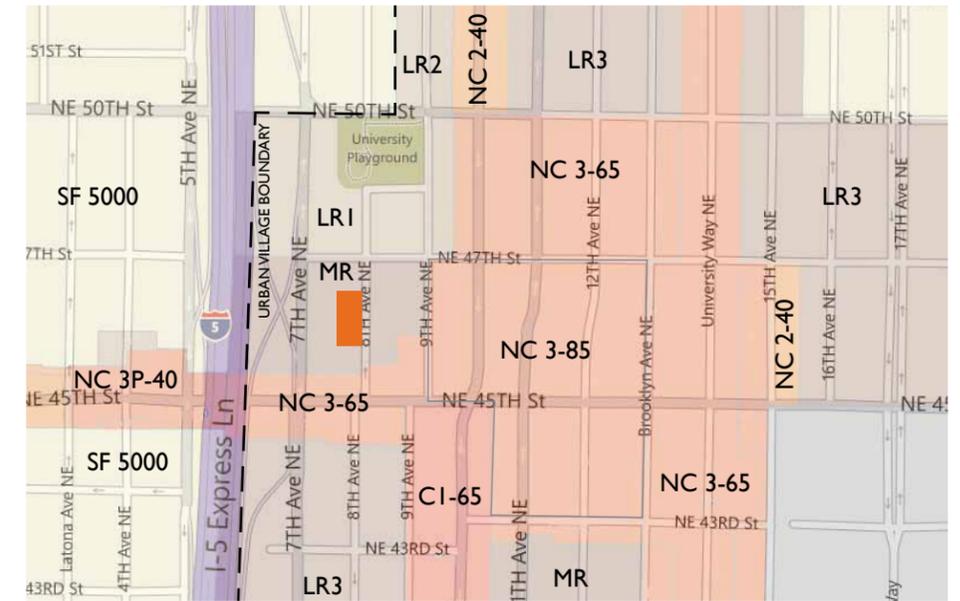
- 8th Avenue NE is classified as a nonarterial per SMC 11.18.010.
- 3 curb cuts allowed for non-arterial lot frontage > 160'-320'
- Maximum curb cut width: 10' or 20' substituted for 2 curb cuts

Sight Triangle:

- Driveways < 22' wide: 10'x10' sight triangle required on each side
- Driveways ≥ 22' wide: 10'x10' sight triangle required on exit side

23.54.040 SOLID WASTE & RECYCLABLE MATERIALS STORAGE AND ACCESS

More than 100 units:
 - 575 SF, plus 4 SF for each additional unit above 100
 - Min. storage area may be reduced 15% if min. horizontal dimension is 20'

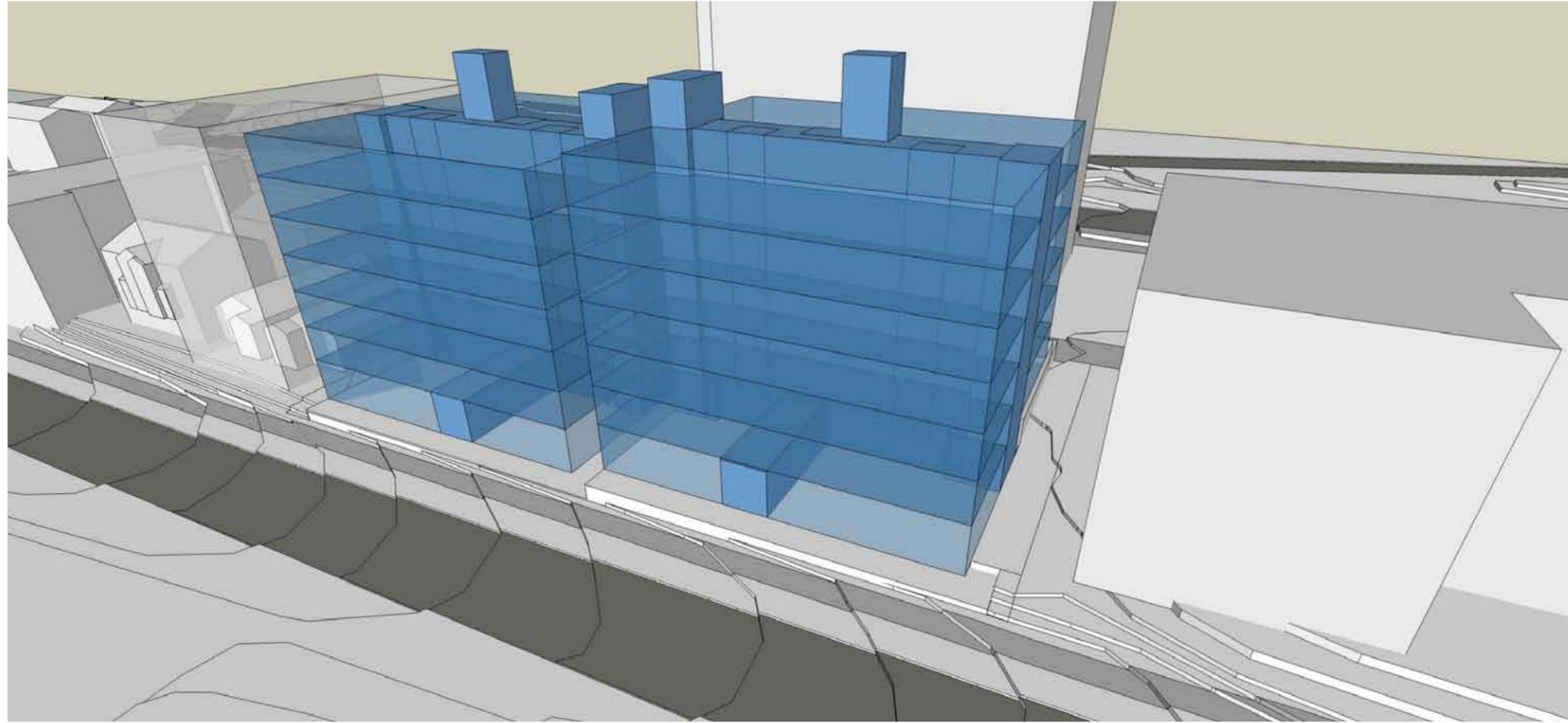


DPD ZONING MAP

REQUESTED DEPARTURE	OPTION 1	OPTION 2	OPTION 3
#1 STRUCTURE WIDTH		X	X
#2 STRUCTURE DEPTH		X	X
#3 REAR SETBACK		X	X
#4 FRONT SETBACK		X	X

MR ZONING CODE	REQUIREMENT	PROPOSED	DEPARTURE AMOUNT	REASON FOR DEPARTURE	DESIGN REVIEW GUIDELINES
#1 STRUCTURE WIDTH SMC 23.45.058	Maximum structure width allowed = 150'-0"	OPTION 2: 182'-0" OPTION 3: 182'-0"	OPTION 2: 32'-0" OPTION 3: 32'-0"	A wider and deeper building allows us to provide an entry courtyard, which is highly encouraged by the University Community Design Guidelines. The courtyard provides opportunities for social interaction, creates a connection to the pedestrian environment, and generates an opportunity for more light and air to be introduced into the units flanking the courtyard. Additionally, the courtyard is an improved urban response to the standard larger side yards that would be required in order to meet the maximum allowed overall structure width. The building has been pulled away from the front property line, in addition to the courtyard provided, diminishing the impact of the building on the pedestrian environment.	A-2 Streetscape A-3 Entrances A-7 Open Space B-1 Hght, Bulk, Scale D-1 Pedestrian Space E-2 Landscaping
#2 STRUCTURE DEPTH SMC 23.45.528	Maximum structure depth allowed = 75% of lot depth = .75x107.1' = 80'-4" allowed	OPTION 2: 87'-0" OPTION 3: 83'-0"	OPTION 2: 6'-8" OPTION 3: 2'-8"	See the above response to Structure Width.	A-2 Streetscape A-3 Entrances A-7 Open Space B-1 Hght, Bulk, Scale D-1 Pedestrian Space E-2 Landscaping
#3 REAR SETBACK SMC 23.45.518	Min @ Alley = 10'-0"	OPTION 2: 0' @ Level I 13' above Level I OPTION 3: 0' @ Level I 13' above Level I	OPTION 2: 10'-0" @ Level I OPTION 3: 10'-0" @ Level I	Providing access to the parking off the alley minimizes the impact of the vehicular traffic on the pedestrian environment, adjacent properties and pedestrian safety. The parking is pulled all the way to the property line in order to provide adequately sized parking stalls without compromising the unit sizes/layouts and to provide an adequately sized courtyard.	A-2 Streetscape A-3 Entrances A-7 Open Space A-8 Parking D-1 Pedestrian Space E-2 Landscaping
#4 FRONT SETBACK SMC 23.45.518	Min @ Level I = 5' Avg @ Level I = 7' Min above L I = 5' Avg above L I = 7'	OPTION 2: Min/Avg @ Level I = 0'/0' Min/Avg above LI = 7'/11'-6" OPTION 3: Min/Avg @ Level I = 0' Min/Avg above LI = 11'/12'-11"		In order to provide adequately sized parking below grade on this site, the below grade parking garage would have to extend to the east property line. Due to the sloping topography of the site, the parking garage would slightly extend above grade at the south end of the site. Landscaping would be provided to soften the walls edge.	A-8 Parking E-2 Landscaping

OPTION I (ZONING COMPLIANT - 2 BUILDINGS)



EAST FACADE



NORTHEAST AERIAL VIEW



SOUTHEAST AERIAL VIEW

DISTINGUISHING FEATURES

- 154 apartment homes.
- Studio, one bedroom, and two bedroom units.
- 93 parking stalls.
- 2 identical buildings separated by required cluster setback.
- 2 lobbies provided, each located in the center of each building street façade.
- Central courtyard provided between buildings.
- Parking access to both LI and PI parking levels located off alley.
- Roof top deck provided for amenity space.

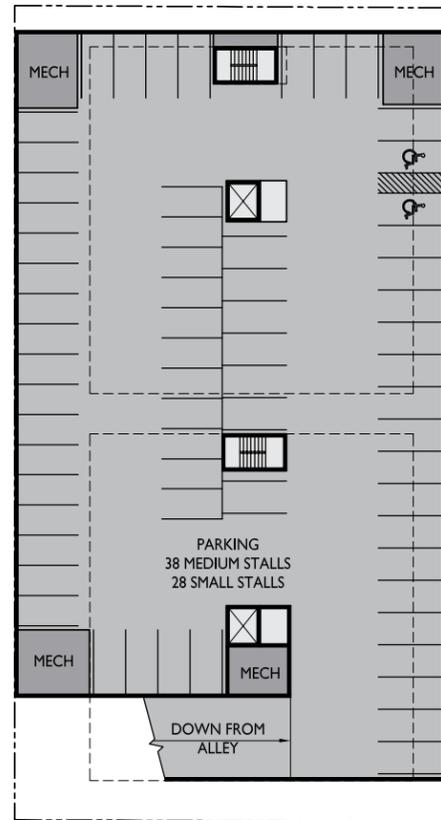
PROS

- 2 buildings create an opportunity for stepping the buildings.
- 2 buildings create a design opportunity to develop 2 distinctly different buildings.
- Scheme provides significantly greater Rear Yard Setback than is required.
- Views from roof deck of Downtown.

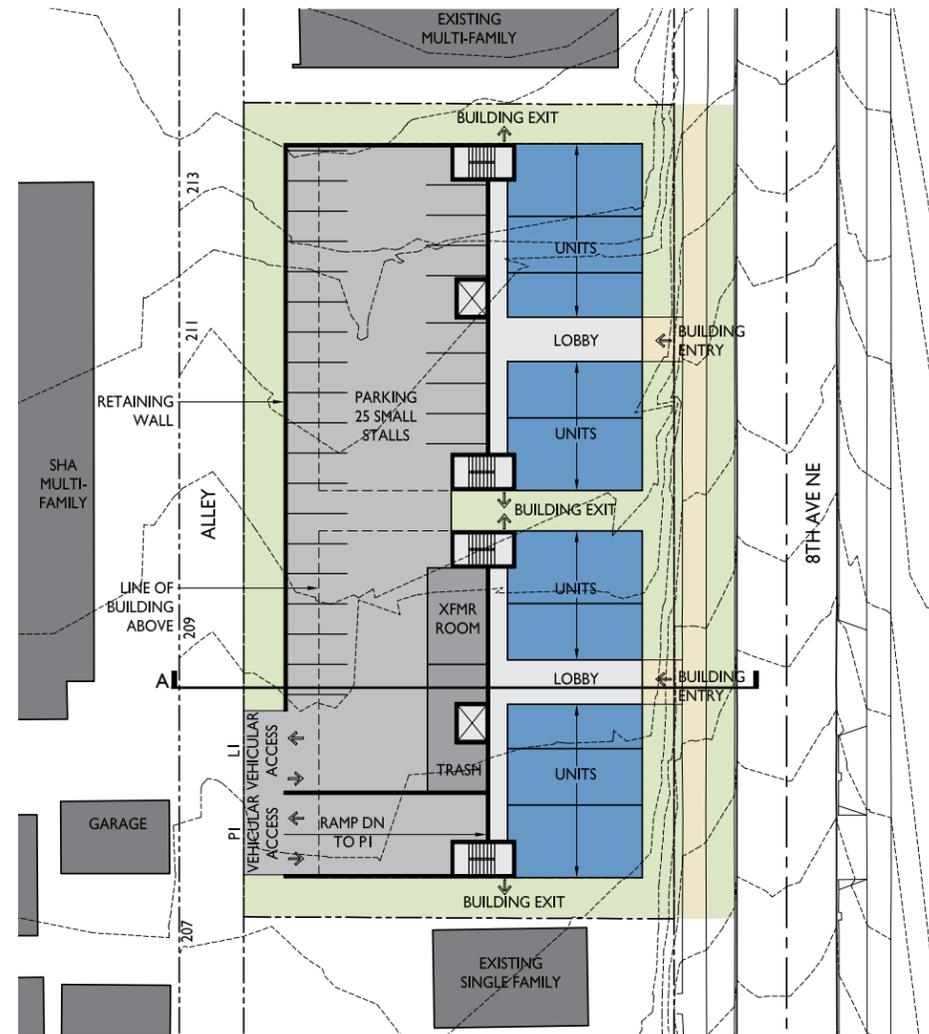
CONS

- Because gap between buildings is narrow, it is difficult to perceive the building as 2 separate buildings.
- The street façade has zero modulation.
- Central courtyard is not spacious.
- Increased construction cost because 2 lobbies and 2 cores have to be provided, one for each building.
- Increased construction cost due to the additional story and increased exterior surface area.

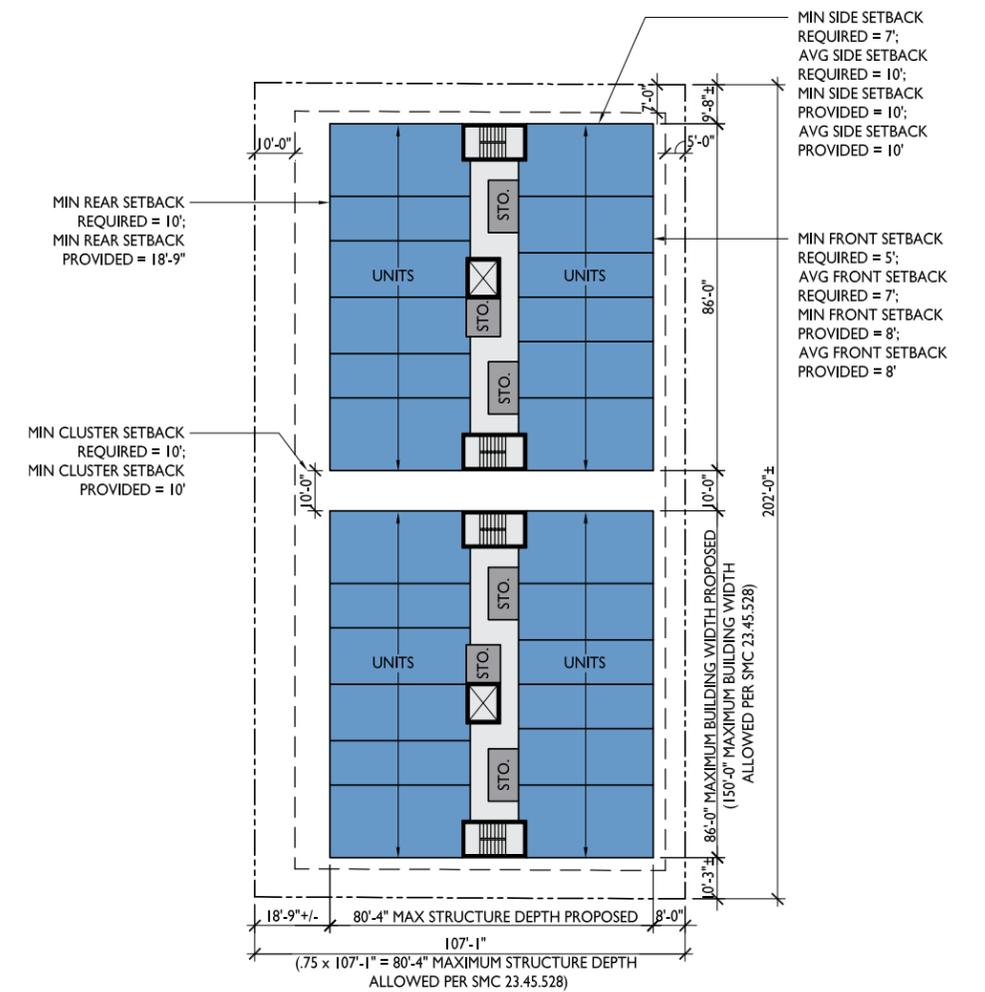
OPTION I (ZONING COMPLIANT - 2 BUILDINGS)



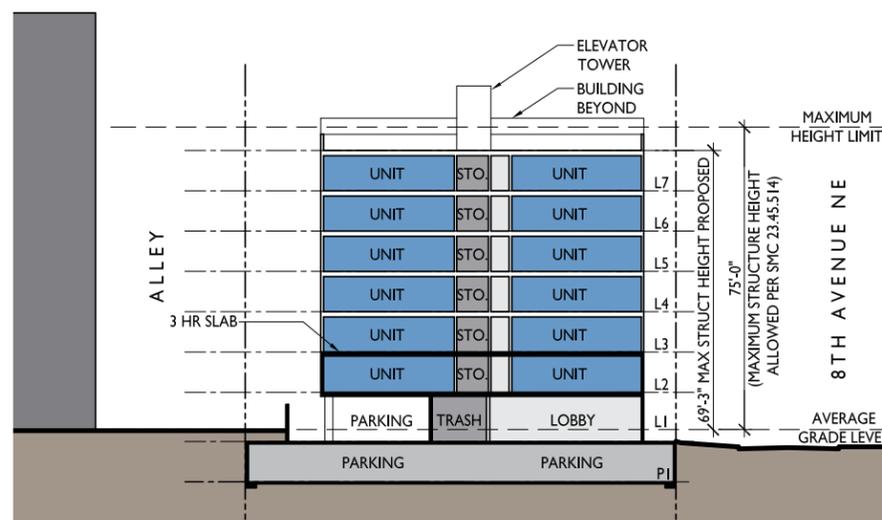
LEVEL P1



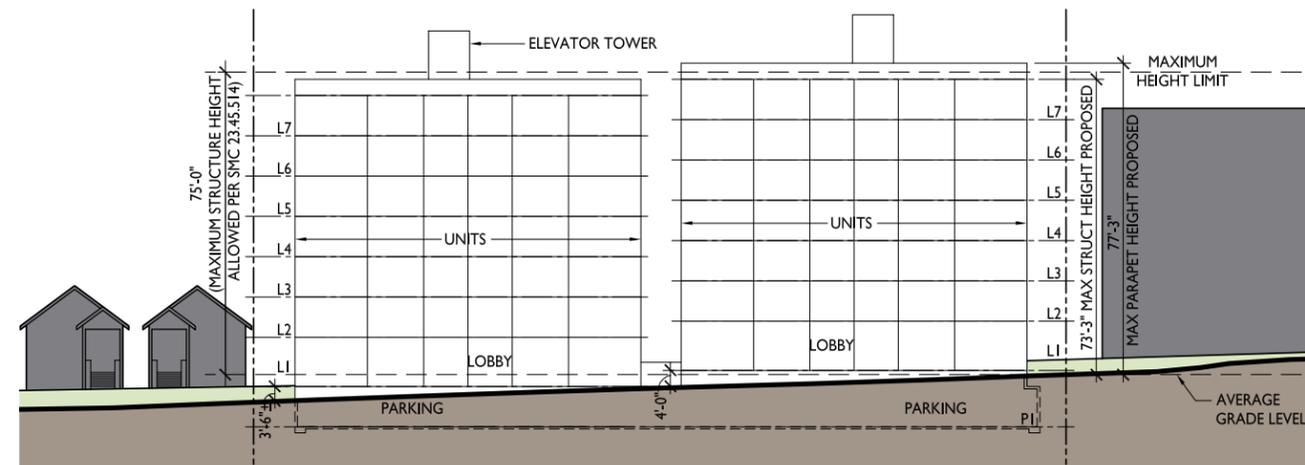
LEVEL I



LEVELS 2-7

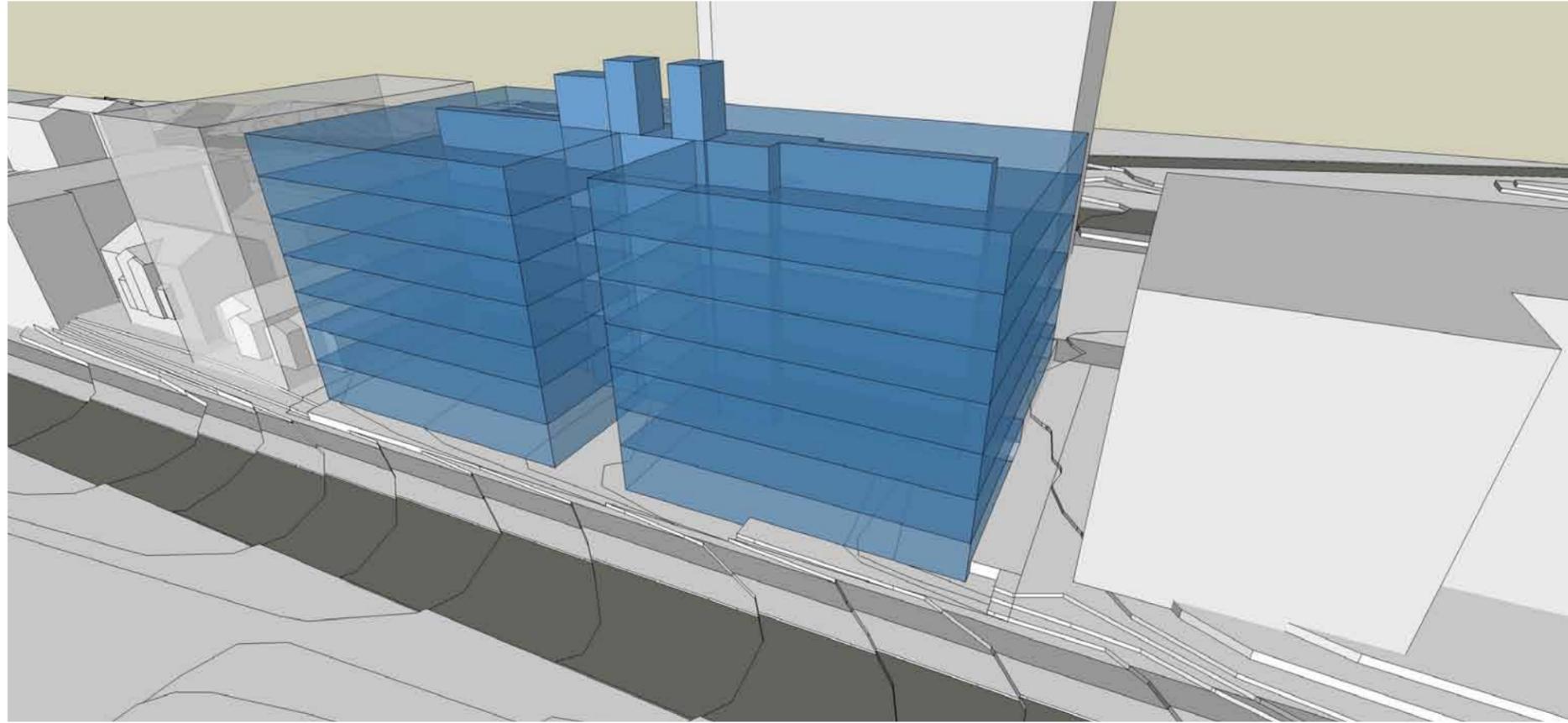


SECTION A



EAST ELEVATION

OPTION 2 (COURTYARD SCHEME)



EAST FACADE



NORTHEAST AERIAL VIEW



SOUTHEAST AERIAL VIEW

DISTINGUISHING FEATURES

- 164 apartment homes.
- Studio, one bedroom, and two bedroom units.
- 101 parking stalls.
- Entry courtyard at ground level, located in the middle of the site.
- Courtyard faces east, towards the park-like landscaped right-of-way across the street, belonging to the University Plaza Condominiums.
- Parking access to both LI and PI parking levels located off alley.
- Roof top deck provided for amenity space.

PROS

- Courtyard provides visual relief at ground level and creates strong pedestrian connection to the neighborhood and emphasizes human scale.
- Entry courtyard at Level 1 creates a sense of entry.
- Entry courtyard creates a natural place for gathering.
- Courtyard creates a modulated building façade.
- Courtyard provides a social opportunity for residents.
- Scheme provides greater average Front Yard Setback than is required.
- Structure is below allowable height limit.
- Massing is similar to Duncan Place Condominiums to the north.
- Single core layout produces a more efficient building.
- Scheme is more economical to build than the 2-building scheme.
- Views from roof deck of Downtown.

CONS

- Courtyard isn't as wide as Option 3
- Exceeds allowable overall structure width.
- Exceeds allowable overall structure depth.
- Parking provided at LI, along the alley, encroaches into the rear setback.
- Below grade parking provided at PI encroaches into the front setback

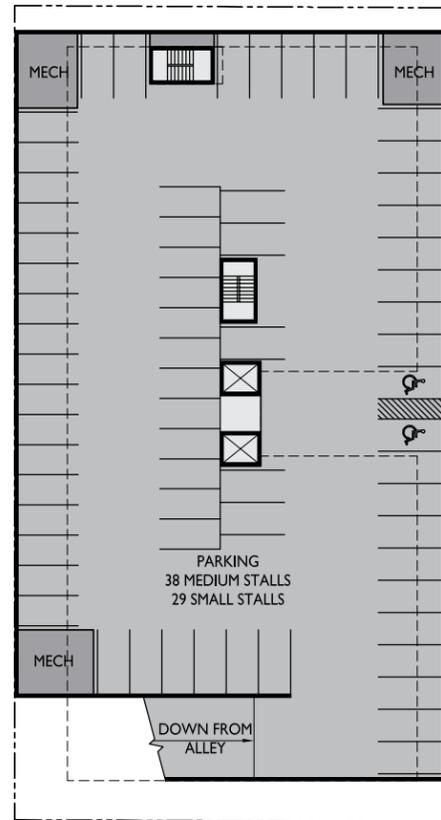
REQUESTED DEPARTURES

- Structure Width: 32'-0" wider than the 150' allowed structure width.
- Structure Depth: 7'-8" deeper than the 80'-4" allowed structure depth.
- Rear Yard Setback: Parking at Level 1 encroaches 10' into the 10' required rear yard setback, creating a zero lot line condition at Level 1, only.
- Front Yard Setback: Parking at Level PI encroaches 7' into the 7' required front yard setback (only along the south end of the building)

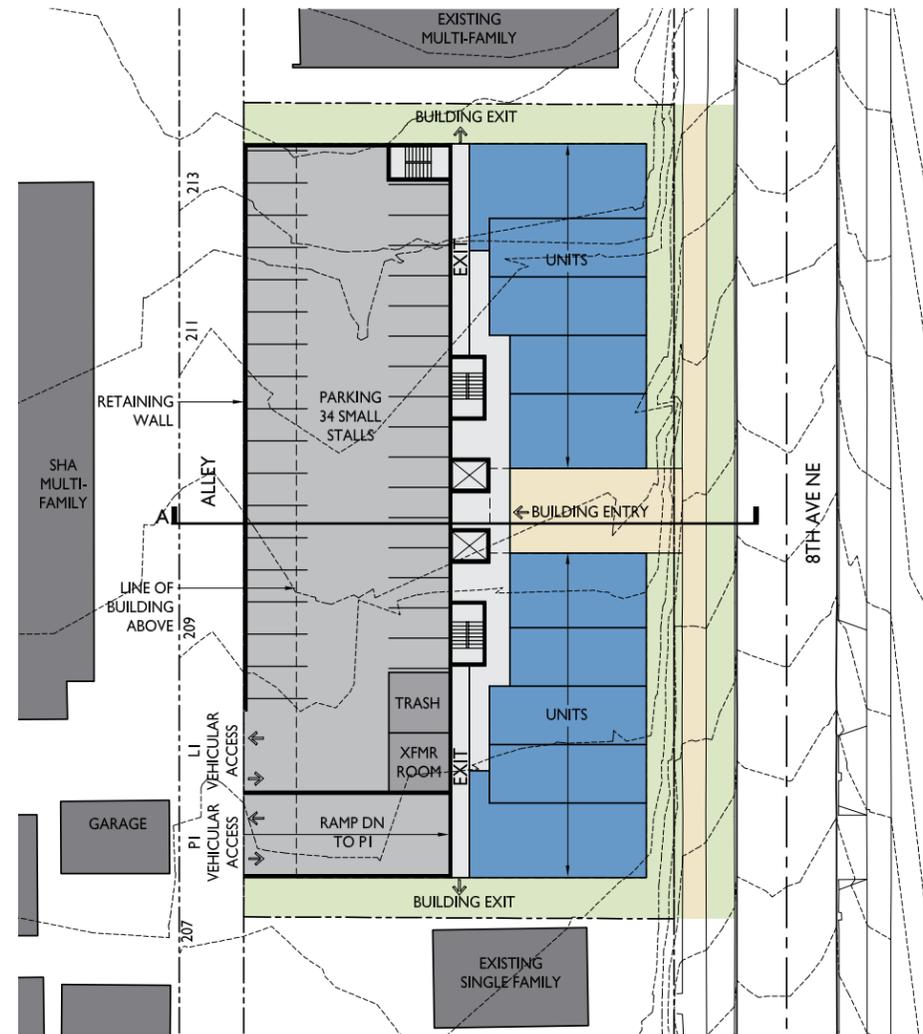
SIGNIFICANT UNIVERSITY COMMUNITY DESIGN GUIDELINES

- A-2: Streetscape Compatibility. The courtyard creates a pedestrian friendly environment, promoting safety while encouraging interaction.
- A-3: Entrances Visible from the Street. The courtyard identifies the building entrance, promoting visual access and security.
- A-7: Context. The ground-level courtyard provides open space for the project, is visible and accessible to the public and improves the quality of the residential environment.
- D-1: Pedestrian Open Spaces and Entrances. The ground-related open space improves the quality of the pedestrian and residential environment.

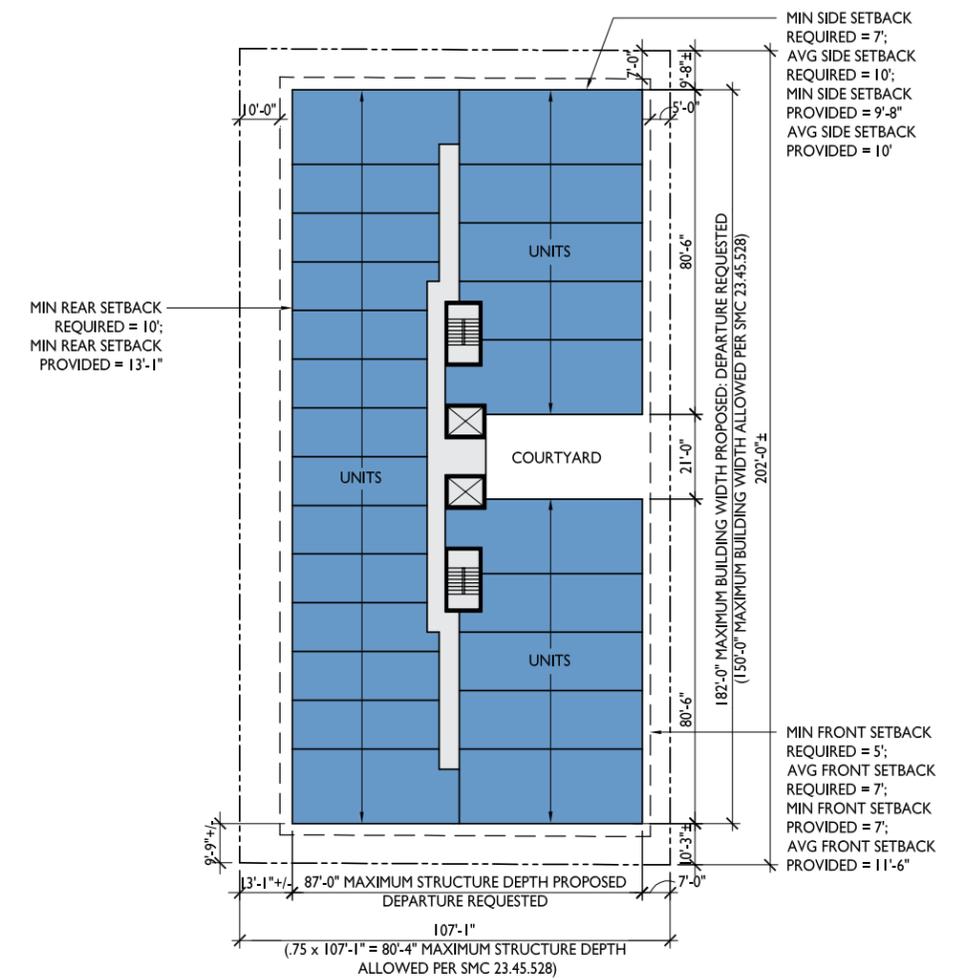
OPTION 2 (COURTYARD SCHEME)



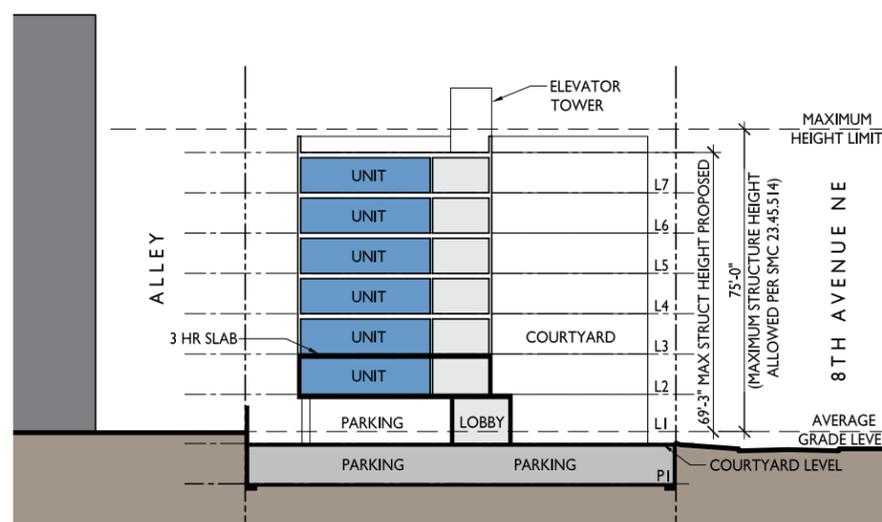
LEVEL P1



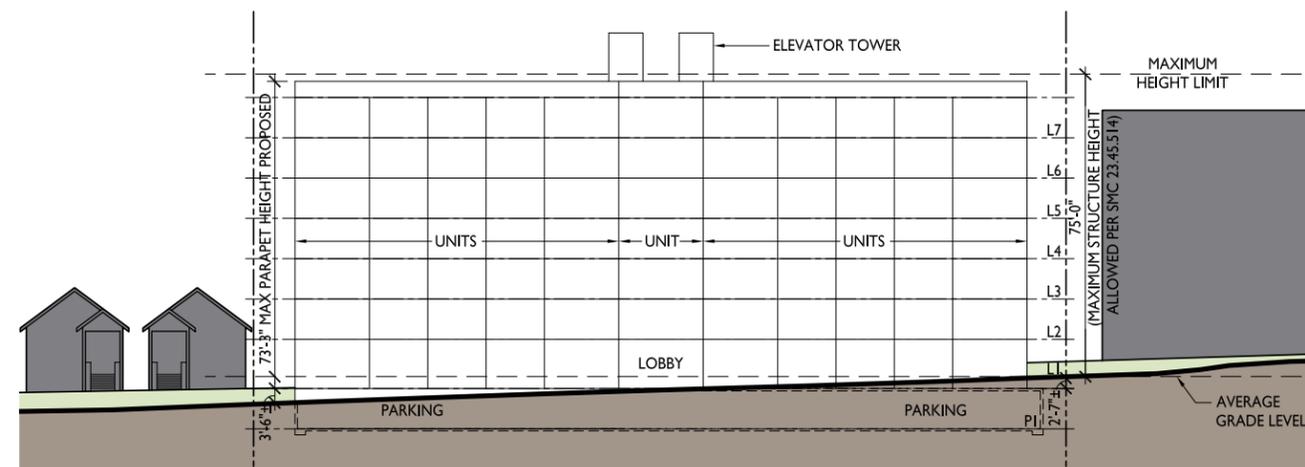
LEVEL I



LEVELS 2-7



SECTION A



EAST ELEVATION

OPTION 3 (PREFERRED SCHEME)



EAST FACADE



NORTHEAST AERIAL VIEW



SOUTHEAST AERIAL VIEW

DISTINGUISHING FEATURES

- 152 apartment homes.
- Studio, one bedroom, and two bedroom units.
- 93 parking stalls.
- Grand entry courtyard at ground level, located in the middle of the site.
- Courtyard faces east, towards the park-like landscaped right-of-way across the street, belonging to the University Plaza Condominiums.
- Parking access to both LI and PI parking levels located off alley.
- Roof top deck provided for amenity space.

PROS

- Courtyard provides visual relief at ground level and creates strong pedestrian connection to the neighborhood and emphasizes human scale.
- Entry courtyard at Level 1 creates a sense of entry.
- Entry courtyard creates a natural place for gathering.
- Entry courtyard provides opportunity for lush landscaping
- Courtyard creates a distinctly modulated building façade.
- Courtyard provides a social opportunity for residents.
- Scheme provides significantly greater Front Yard Setback than is required.
- Structure is below allowable height limit.
- Massing is similar to Duncan Place Condominiums to the north.
- Single core layout produces a more efficient building.
- Scheme is more economical to build than the 2-building scheme.
- Views from roof deck of Downtown.
- Courtyard area provided greater than standard courtyard requirements.
- Front yard setback also provided, in addition to the courtyard

CONS

- Depth of courtyard is less than 20'
- Exceeds allowable overall structure width and depth
- Parking provided at LI, along the alley, encroaches into the rear setback.
- Below grade parking provided at Level PI encroaches into front setback.

REQUESTED DEPARTURES

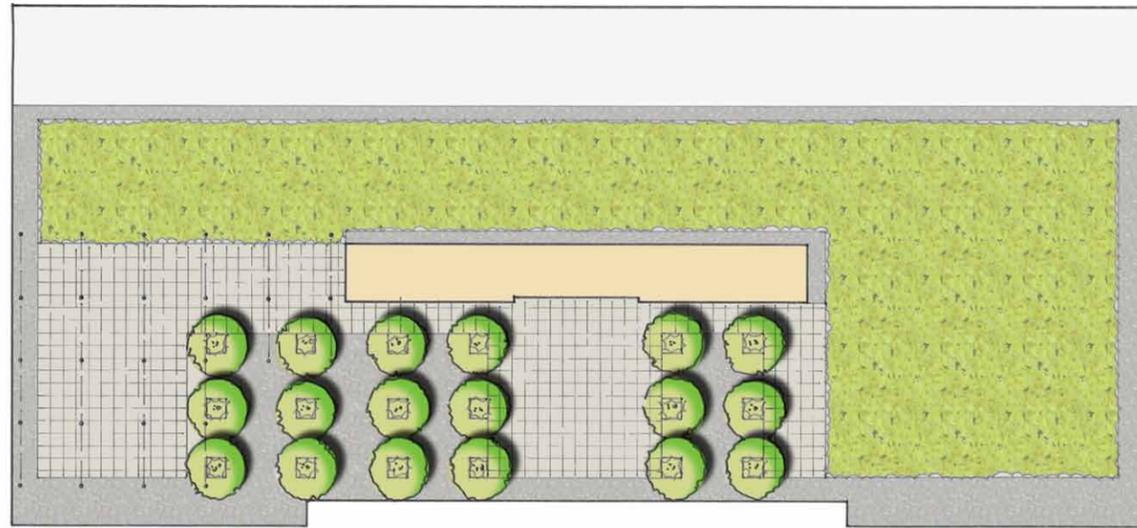
- Structure Width: 32'-0" wider than the 150' allowed structure width.
- Structure Depth: 2'-8" deeper than the 80'-4" allowed structure depth.
- Rear Yard Setback: Parking at Level I encroaches 10' into the 10' required rear yard setback, creating a zero lot line condition at Level I, only.
- Front Yard Setback: Parking at Level PI encroaches 7' into the 7' required front yard setback (only along the south end of the building)

SIGNIFICANT UNIVERSITY COMMUNITY DESIGN GUIDELINES

- A-2: Streetscape Compatibility. The courtyard creates a pedestrian friendly environment, promoting safety while encouraging interaction.
- A-3: Entrances Visible from the Street. The courtyard identifies the building entrance, promoting visual access and security.
- A-7: Context. The ground-level courtyard provides open space for the project, is visible and accessible to the public and improves the quality of the residential environment.
- D-1: Pedestrian Open Spaces and Entrances. The ground-related open space improves the quality of the pedestrian and residential environment.



OPTION 3 (PREFERRED SCHEME) LANDSCAPE



ROOF PLAN - OPTION 2



SITE PLAN/ ROOF PLAN - OPTION I



CREEPING MAHONIA



FEATHER REED GRASS



RHODODENDRON



DAPHNE



KOUSA DOGWOOD



STYRAX



DEER FERN



LEATHERLEAF MAHONIA



SWORD FERN



JAPANESE MAPLE

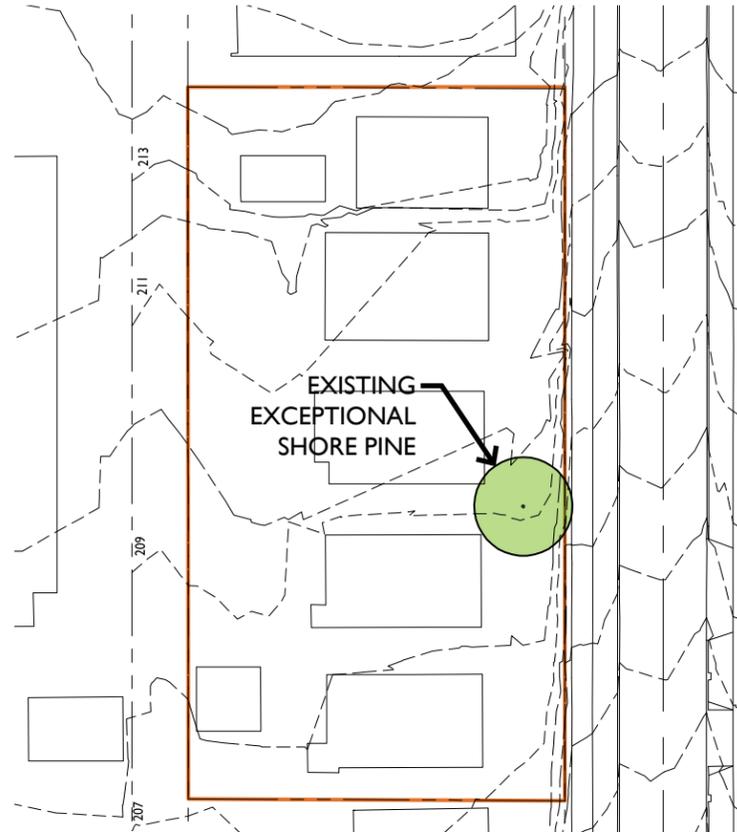


PIERIS

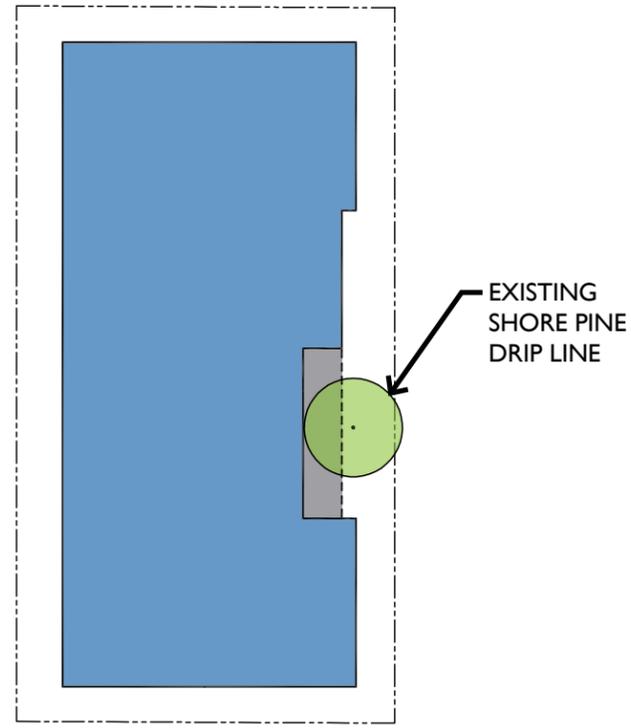


VINCA

EXISTING TREE IMPACTS

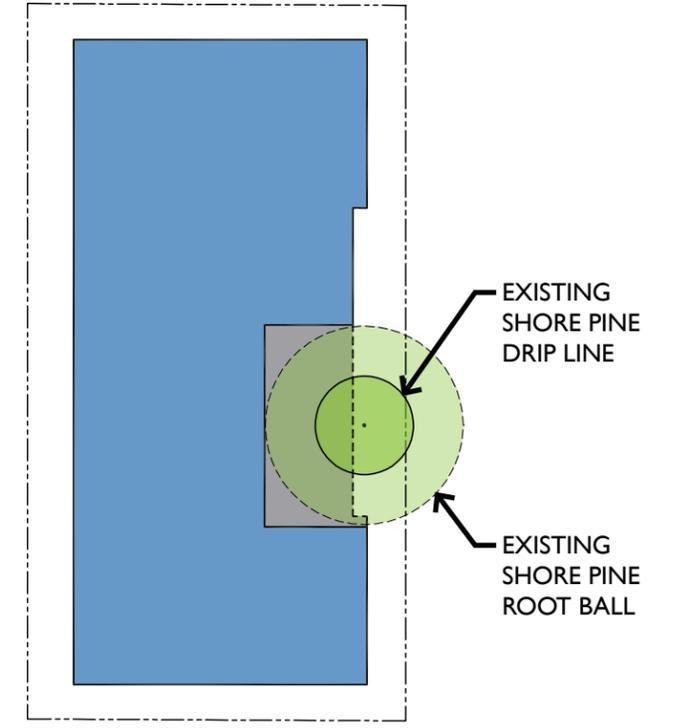


MINIMUM PER CODE



LEVELS 1-7

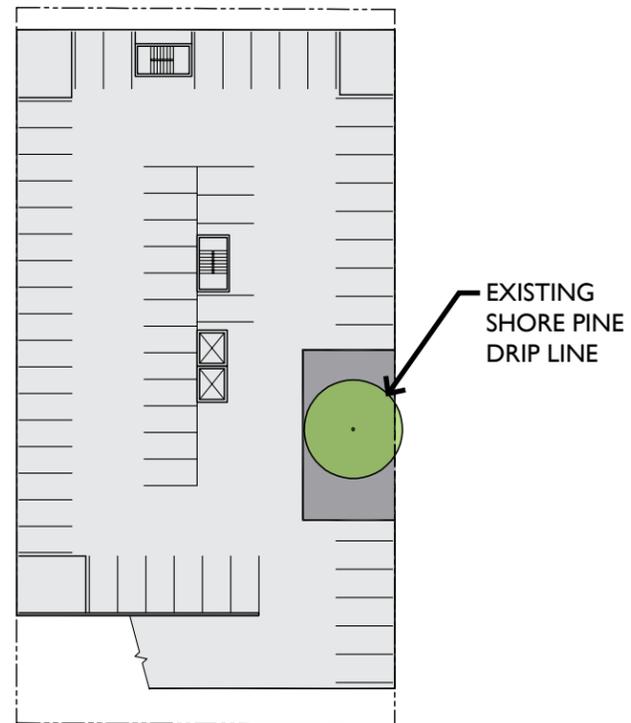
RECOMMENDED BY ARBORIST



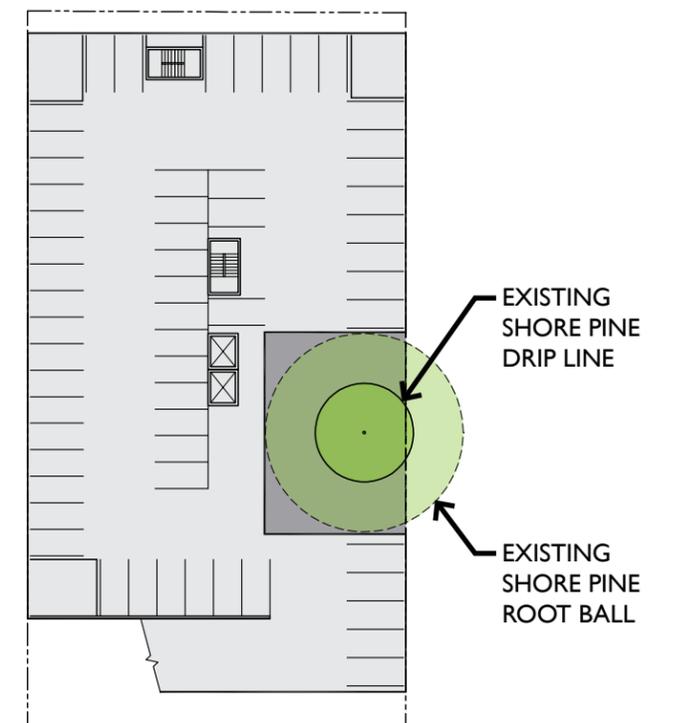
LEVELS 1-7



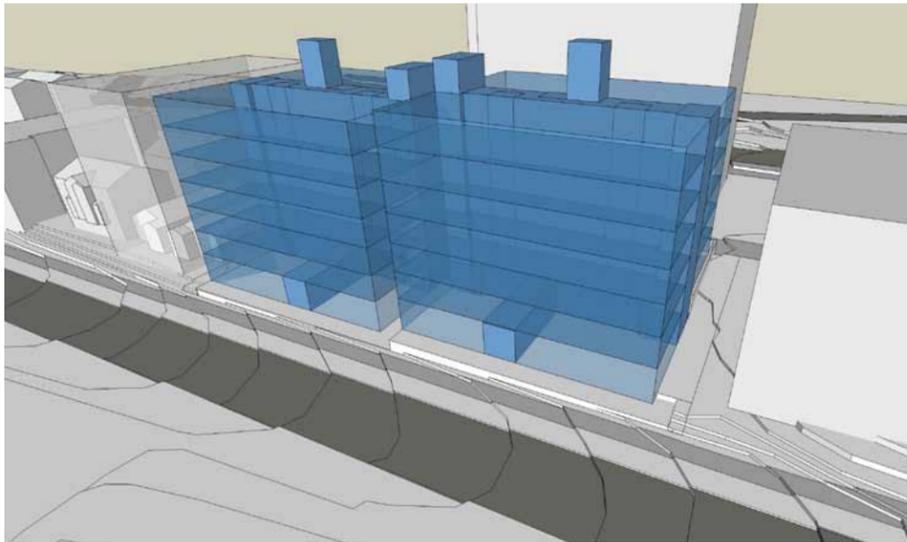
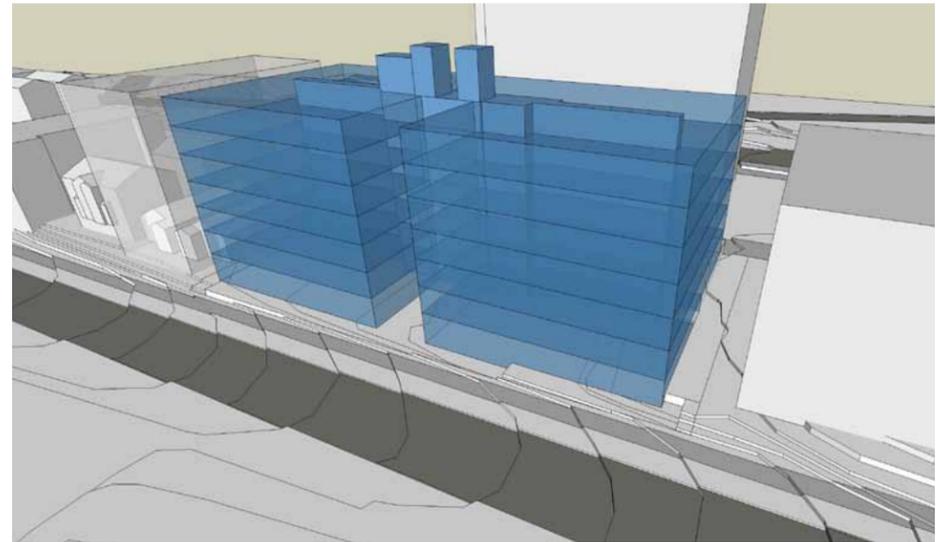
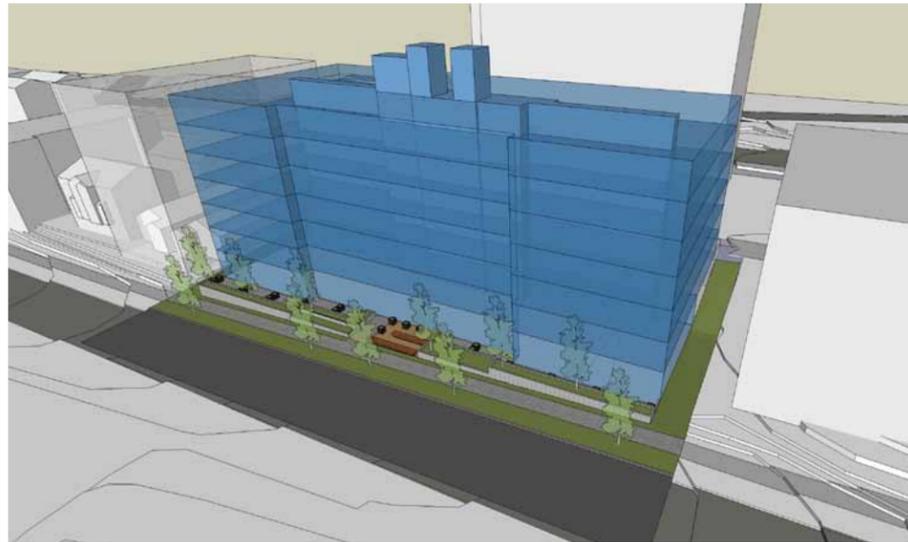
EXISTING SHORE PINE
PHOTO CREDIT: HOLLY IOSSO, TREE SOLUTIONS



LEVEL P1



LEVEL P1

OPTION 1 ZONING COMPLIANT	OPTION 2 COURTYARD	OPTION 3 PREFERRED SCHEME
		
DISTINGUISHING FEATURES	DISTINGUISHING FEATURES	DISTINGUISHING FEATURES
<ul style="list-style-type: none"> · 154 apartment homes, 93 parking stalls · Two buildings separated by required cluster setback · Two lobbies provided · Central courtyard provided between buildings · Parking access located off alley · Rooftop decks provided for amenity space 	<ul style="list-style-type: none"> · 164 apartment homes, 101 parking stalls · Entry courtyard in the middle of the site · Courtyard faces east, towards the park-like landscaping of the University Plaza Condos · Parking access located off alley · Rooftop deck provided for amenity space 	<ul style="list-style-type: none"> · 152 apartment homes, 93 parking stalls · Grand entry courtyard at ground level, located in the middle of the site · Courtyard faces east, towards the park-like landscaping of the University Plaza Condos · Parking access located off alley · Rooftop deck provided for amenity space
REQUESTED DEPARTURES	REQUESTED DEPARTURES	REQUESTED DEPARTURES
	<ul style="list-style-type: none"> #1 Structure Width #2 Structure Depth #3 Rear Setback #4 Front Setback 	<ul style="list-style-type: none"> #1 Structure Width #2 Structure Depth #3 Rear Setback #4 Front Setback

PRECEDENTS



KOPVAN OOST - MECANOO



UW POPLAR HALL- MAHLUM



RADIAN APARTMENTS - ERDY MCHENRY ARCHITECTS



ONE HANCOCK SQUARE - ERDY MCHENRY ARCHITECTS



APARTMENT BUILDING DE ZILVERREIGER - MECANOO



SIOBHAN DAVIES DANCE STUDIOS - SARAH WIGGLESWORTH



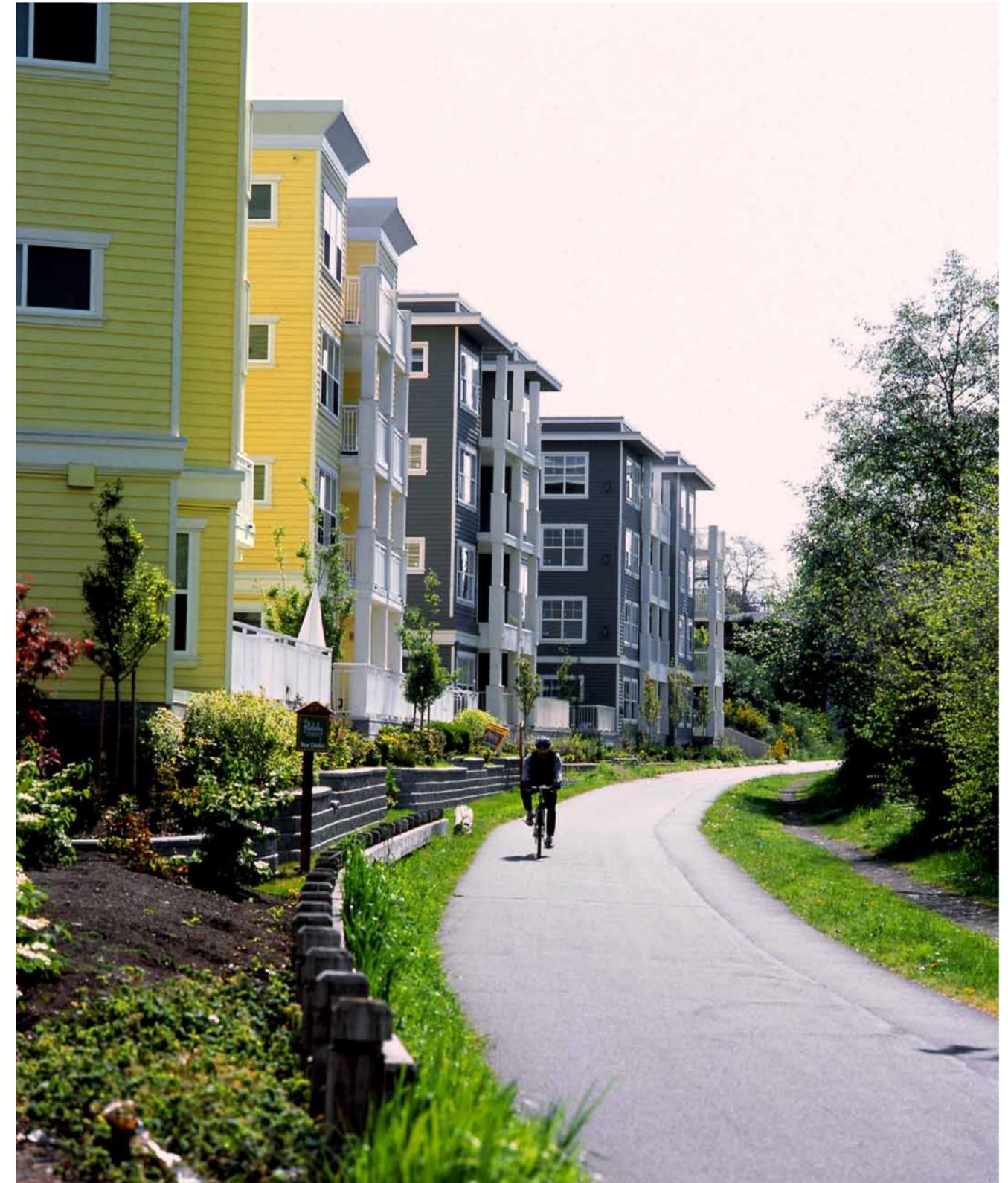
THE HAYES



SIDNEY



EXPO 62



BLAKELEY COMMONS

RECENT NK PROJECTS



WESTLAKE VILLAGE



222 VIEW APARTMENTS



CHELAN RESORT SUITES



THE DAKOTA



SALVEO - LEED H PLATINUM



H2O APARTMENTS - LEED H MIDRISE PILOT GOLD TARGET



BROADSTONE KOI APARTMENTS - LEED NC CERTIFIED TARGET



OLIVEWAY MIXED-USE APARTMENTS - LEED NC SILVER TARGET



MIST APARTMENTS - LEED NC SILVER TARGET