

ARTHOUSE - ELLIOTT & BATTERY



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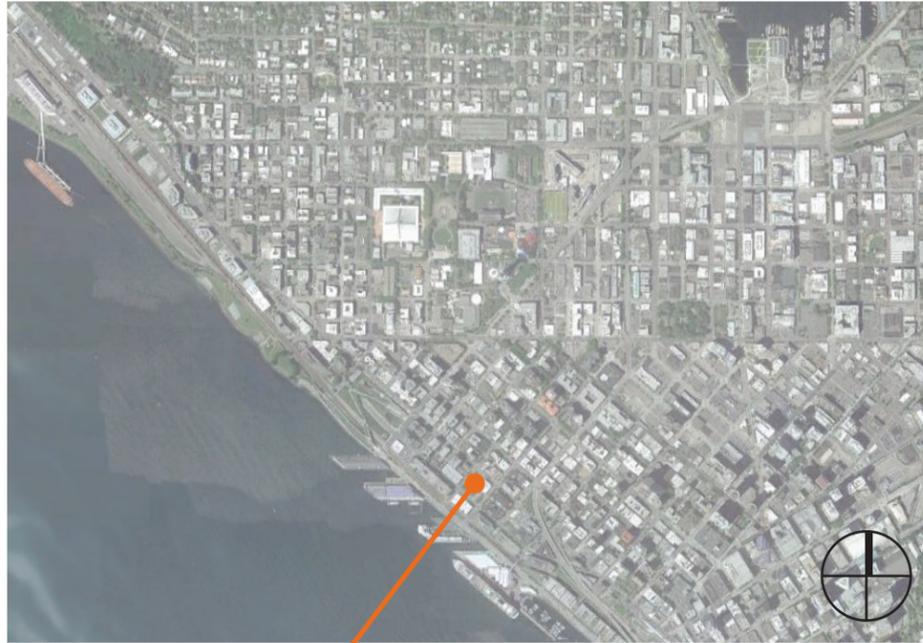
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EARLY DESIGN GUIDANCE DPD #3012499

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PROJECT DESCRIPTION



SITE LOCATION

ADDRESS: 2334 Elliott Avenue
DPD PROJECT #: 3012499
OWNER: 55 Battery, LLC
APPLICANT: Nicholson Kovalchick Architects
CONTACT: Tom Steidl

DEVELOPMENT OBJECTIVES

- Reinforce the contextual rhythm (scale, massing, building modulation) of the streetscape along Elliott & Battery.
- Maximize scenic views of Elliott Bay from both residential spaces and common outdoor spaces.
- Take advantage of the topography to provide natural pedestrian and vehicular access points into the building.
- Incorporate sustainable design strategies to preserve resources.
- Provide indoor spaces at-grade that engage passers-by.

The proposed project is a 7-story apartment building consisting of 5 wood-framed floors organized around a courtyard above a podium housing additional residential units, parking, live/work units and residential amenity spaces. The project site is located in a DMR/C 85/65 zone in the Belltown Urban Center Village. The site slopes approximately 22' from the northeast (at the alley) to the southwest (the intersection of Elliott & Battery). Pedestrian access to the 2-story building lobby will likely occur at the corner with individual pedestrian access points to the 'work' portion of the live/work units from either Elliott or Battery. The lobby and amenity spaces along with live/work and residential units will line the 2-level garage. Separate vehicular access points to each level are proposed, one from the alley and one from either Elliott or Battery.

The upper most floors will have unobstructed views of Elliott Bay once above the Art Institute Building and the common landscaped courtyard having partial views if it is oriented toward Elliott (Option 3). Another design option proposes that the courtyard face northeast; while it would be located at a different elevation, this feature could be used to develop the alley façade (Option 2). A second landscaped deck, either on the roof or on the highest residential floor, will also be provided. A third, code-compliant option, explores maximizing the buildable density of the site. All schemes intend to incorporate sustainable design principals.

Number of Residential Units:	Approximately 145
Number of Parking Stalls:	Approximately 83

Gross Residential Floor Area:	Approximately 106,000 SF
Gross Indoor Amenity Floor Area:	Approximately 5,000 SF
Gross Parking Area:	Approximately 25,000 SF
<u>Total Area:</u>	<u>Approximately 136,000 SF</u>

EXISTING SITE

- 21,600 square feet (180' x 120')
- Currently vacant lot
- Grade change +/- 22' from NE corner (high) to SW corner (low)

ZONING AND OVERLAY DESIGNATION

Zoning: DMR/C 85/65

Zoning Overlays:

- Belltown Urban Center Village
- Downtown Fire District
- Archaeological Buffer Area

Street Designations:

- Elliott Avenue: Class II Pedestrian Street, Principal Arterial
- Battery Street: Class II Pedestrian Street, Access Street

ADJACENT TO SITE

- East (shared side property line): 6 -Story, 92 Unit '2300 Elliott Avenue Apartments' c. 1990
- North (across 16'-wide improved alley): 2-story plus basement, 15,000 sf 'Mars Hill Fellowship Church' c. 1949 & surface parking lot and separate 7,200 sf commercial surface parking lot.
- West (across 66'-wide Battery Street): 6-story, 118 Unit 'Elliott Bay Plaza Apartments' c. 1990
- South (across 66'-wide Elliott Street): 3-story commercial office over 4-story garage, 276,000 sf Seattle Art Institute Building c. 1983

NEIGHBORING DEVELOPMENT

The neighborhood is located at the southwestern edge of the Belltown Urban Center Village, between the First Avenue Commercial District and the waterfront. The immediate vicinity is defined by several mid-rise multi-family residential structures that step down the steeply sloping topography as one approaches Elliott Bay. Elliott and Western constitute principal arterials that parallel the waterfront while circulation on Battery diminishes as it approaches the water. Further to the northwest, numerous high-rise residential towers define the area while to the southeast, the Alaskan Way viaduct emerges from the Battery Street tunnel creating a psychological barrier between the site and Downtown.

The project site is bounded by what were once commercial warehouse structures dating from the first half of the 20th Century and surface parking lots directly across the alley to the north. Directly to the east (sharing a side property line) and west (across Battery) are 6-story, wood-frame over podium apartment buildings organized around internal courtyards. Directly across Elliott to the south are a line of commercial office buildings that house the Seattle Art Institute and Seattle World Trade Center. There is a limited amount of retail spaces on the immediate surrounding blocks. The Belltown P-Patch is a block away on Elliott and the Seattle Empire Laundry Building is on the opposite side of the block at the intersection of Western and Bell.



① 81 VINE STREET



② A-I LAUNDRY BUILDING



③ FIRST AVE HISTORIC BUILDINGS



④ ALASKAN WAY VIADUCT



⑤ BELLTOWN P-PATCH



⑥ GROWING VINE STREET



⑦ RAILROAD AT ALASKAN WAY



⑧ BELL STREET PEDESTRIAN BRIDGE



⑨ ALASKAN WAY



⑩ 2300 ELLIOTT AVENUE APARTMENTS

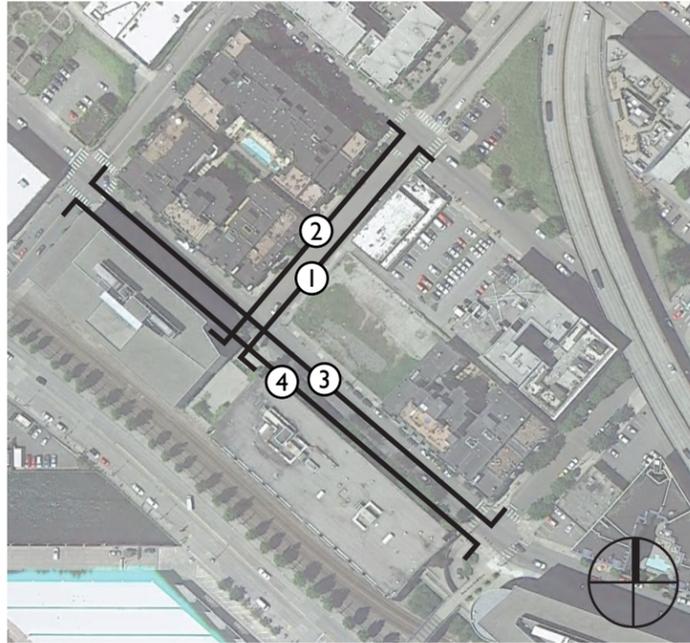


⑪ ELLIOTT BAY PLAZA APARTMENTS



⑫ ART INSTITUTE

STREETSCAPE



① BATTERY STREET LOOKING SOUTHEAST



② BATTERY STREET LOOKING NORTHWEST



③ ELLIOTT AVENUE LOOKING NORTHEAST



④ ELLIOTT AVENUE LOOKING SOUTHWEST



OPPORTUNITIES & CONSTRAINTS

The site is perched on a steep slope a block above the waterfront in a section of Belltown marked by former industrial buildings and densely-spaced mid-rise apartment buildings built in the past two decades. As noted in the Design Guidelines, Belltown is Seattle’s densest residential community and features a high concentration of jobs and retail activities that create a mixed-use community with ample pedestrian activity. The diversity in social and cultural fabric creates the condition for an enhanced built environment through architecture, public art and street amenities. Additionally, the Design Guidelines call for new buildings to “not appear to have been constructed in a past era.” Considering all of these factors, the project presents an opportunity to reinforce the existing pattern of dense mid-rise apartment buildings in a modern architectural style. By introducing an enhanced element of design, the project would further improve the streetscape along Elliott and Battery.

The site slopes steeply to the southwest, creating the potential to maximize views of Elliott Bay and to maximize solar exposure in common outdoor spaces. Additionally, the existing natural topography around the perimeter of the site would accommodate pedestrian and vehicular access to multiple levels of the proposed building directly from the street.

Wedged between the end of the Alaskan Way Viaduct and the row of Seattle World Trade Center buildings, the site can feel disconnected from the waterfront and downtown. Battery slopes steeply uphill toward Western and First Avenues. Due to these constraints, there is less pedestrian traffic in the immediate area than in other parts of Belltown. This could be why there is a noticeable lack of retail on the surrounding blocks. With the exceptions of partially glazed lobby entrances, the adjacent apartment buildings feature long, blank facades at street level along Elliott. The Design Guidelines call for “reinforcing existing retail concentrations” within Belltown, suggesting that the project should look to activate the street though uses other than retail at the ground floor. Multiple setback restrictions come into play at various heights across the site that would create an interesting building mass, but complicate the ability to “stack” units and vertical circulation.

- Opportunity to continue the existing pattern of mid-rise apartment and office buildings on the immediately surrounding blocks.
- Opportunity to mass the building to take advantage of views of Elliott Bay and solar exposure to the southwest.
- Opportunity to use the topography to create at-grade pedestrian and vehicular access points to multiple floors.
- The various view corridor, height limits and setbacks provide an opportunity to create a building that varies in height and mass.
- However, these setbacks and setbacks make stacking units difficult and create multiple outdoor roofs and decks over living spaces that complicate waterproofing.
- Potential difficulties and inefficiencies result from accessing the upper parking level from the alley due to topography.
- Limited retail in area may require locating residential amenity spaces to activate the street.

EXISTING SITE PLAN



DESIGN GUIDELINE PRIORITIES (BELLTOWN URBAN CENTER VILLAGE)

- A-1 – Respond to the physical environment
 - The proposed project will maximize views from residential units and common spaces and use the topography to its advantage by creating multiple entries to lobby and live/work spaces.
- B-1 – Respond to the neighborhood context &
 - B-3 – Reinforce the positive urban form & architectural attributes from the immediate area.
 - To compliment adjacent mid-rise apartment buildings, the proposed project will be similar in scale and massing and will include similar building features, such as balcony projections and façade modulation to respond to rhythms and fenestration patterns.
- C-1 – Promote pedestrian interaction
 - The Belltown Design Guidelines call for “reinforcing existing retail concentrations”. As there is very limited retail in the immediate vicinity, the project would enliven pedestrian interaction by locating live/work units and specially programmed indoor amenity spaces at street level. These spaces would vary in size and depth and would be open, inviting and lend a human scale to the street.
- C-6 – Develop the alley facade
 - Main building facades will “turn the corner” into the alley and voluntary building setbacks will provide opportunities for landscaping and concealed service and utility spaces.
- D-1 – Provide inviting and usable open space &
 - D-2 – Enhance the building with landscaping
 - Preferred option features a landscaped common courtyard and landscaped roof deck, both oriented to Elliott Bay to capture views and sunlight. Entries to the live/work units will be partially setback to accommodate terraced landscape areas along the sidewalk.



① CORNER OF BATTERY & ELLIOTT



② VIEW OF SITE FROM ALLEY



③ ALLEY

PARCEL #: 0654000305
LOT AREA: 21,600 SF
ZONING: DMR/C 85/65
OVERLAYS: Belltown Urban Center Village
 Downtown Fire District
 Archaeological Buffer Area
STREET DESIGNATIONS: Elliott Ave: Class II Pedestrian Street, Principal Arterial
 Battery St: Class II Pedestrian Street, Access Street

- the parking, excluding garage doors, is screened from view at street level
- the street facade is enhanced by architectural detailing, landscaping, etc.

- Curb cuts:**
- When a lot abuts an alley, alley access required
 - Maximum curb cut width for arterial lots: 23'
 - 2 curb cuts allowed for arterial lot frontage > 160'-320'
 - Maximum of 2 curb cuts for one way traffic at least 40 feet apart, or 1 curb cut for two way traffic permitted on each street front
 - No curb cut shall be located within 40 feet of an intersection
 - Sight Triangles for driveways > 22': 10'X10' triangle on exit side

Bicycle Parking Requirements: 1 space for every 2 units (after first 50 spaces are provided, additional spaces are required at 1/2 the ratio)

23.49.024 VIEW CORRIDOR REQUIREMENTS
 Minimum upper level setback on Battery St 50 feet above the sidewalk: 30 feet

23.49.158 COVERAGE
 Portions of structures at an elevation of 0-65 Feet: 100% Coverage Permitted
 Portions of structures at an elevation of 66-85 Feet: 65% Coverage Permitted

23.49.162 STREET FAÇADE REQUIREMENTS
 Minimum Facade Height: 15 feet
 Facade Setback Limits apply to the portion of the facade 15 feet or less in height:

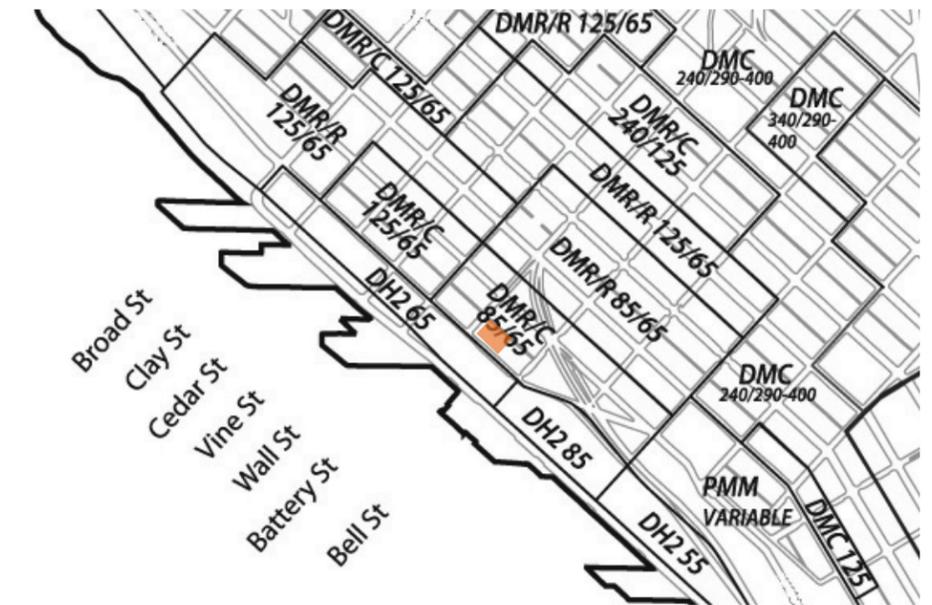
- Maximum area of all setbacks along each street frontage determined by multiplying an averaging factor of 20 by the width of the street frontage of the structure
- Maximum width of any setback area exceeding a depth of 15 feet: 80 feet or 30% of the lot frontage on that street, whichever is less
- Maximum setback of the facade at intersections: 10 feet for a length of 20 feet along each street

Minimum Facade Transparency: 30% (25% if street frontage slope > 7 1/2%)
 Maximum Blank Façade Limits: 70% (75% if street frontage slope > 7 1/2%)

- Blank facades shall be limited to segments 30 feet wide, except garage doors
- Garage door width maximum: width of the driveway plus 5 feet
- Blank segments shall be separated by transparent areas at least 2 feet wide
- Requirements apply to facade 2-8 feet above the sidewalk (4-8 feet if street frontage slope > 7 1/2%)
- Requirements do not apply to portions of structures in residential use

Landscaping Requirements

- Street trees shall be required on all streets abutting a lot
- Minimum landscaped area in the R.O.W.: 1 1/2 X length of property line
- 20% of areas on the street property line that are not covered by a structure, which have a depth of 10 feet or more from the street property line and are larger than 300 square feet, shall be landscaped



DPD ZONING MAP

23.49.164 MAXIMUM WALL DIMENSIONS
 Maximum wall length of portion of structure 66-125 feet in elevation: 120 feet

- Maximum wall length measured for portions of a structure that are separated by at least 20 feet at all points

23.49.166 SIDE SETBACK REQUIREMENTS
 Required Side Setbacks not on Street Side Lot Lines above 65 Feet: 20 feet

23.53.030 ALLEY IMPROVEMENTS

- Required Minimum Right-of-Way Widths for Existing Alleys: 20 feet

23.53.035 STRUCTURAL BUILDING OVERHANGS
 Structural Building Overhangs:

- Minimum vertical clearance: 8 feet at sidewalk or 26 feet at alley
- Maximum horizontal projection: 3 feet and no closer than 8 feet to alley centerline
- Minimum glass/open portions: 50% of the vertical surfaces
- Maximum length: 15 feet reduced at 45 degrees to maximum of 9 feet in length
- Minimum horizontal separations as prescribed by SMC
- Maximum dimensions for decorative projections: 1 foot horizontal (3 feet at roof); 2.5 feet vertical; 30% of area of facade

23.54.040 SOLID WASTE AND RECYCLABLE MATERIALS STORAGE AND ACCESS
 Minimum Shared Storage Space: 575 sf plus 4 sf for each unit above 100

- Minimum storage area reduced by 15%, if min. horizontal dimension is 20 feet

23.49.008 STRUCTURE HEIGHT
 Base and Maximum Height Limits: 85' (residential)/ 65' (commercial)

- Additional 4' height allowed for parapets, open railings, planters, etc.
- Additional 15' height allowed for stair penthouses, mechanical equipment, covered or enclosed common recreation areas, etc.
- Additional height allowed for elevator penthouses as prescribed by SMC
- Maximum combined coverage of all rooftop features: 35% of roof area

23.49.010.B COMMON RECREATION AREA

- Minimum Area: 5% of the total gross floor area in residential use
- Available to all residents and may be provided at or above ground level
- Maximum 50% of the common recreation area may be enclosed
- Minimum horizontal dimension of 15 feet, except landscaped setback areas at street level with minimum horizontal dimension of 10 feet
- No required common recreation area shall be less than 225 square feet
- Common recreation area that is provided as open space at street level shall be counted as twice the actual area

23.49.011 FAR
 FAR: 1 (base)/ 4 (maximum)

- Not included in chargeable floor area: residential, live-work units, floor area below grade, parking accessory to residential uses
- Mechanical equipment: 3 1/2% deduction in chargeable gross floor area

23.49.018 OVERHEAD WEATHER PROTECTION AND LIGHTING
 Continuous overhead weather protection required along entire street frontage except where façade is farther than 5 feet from the street property line, is separated by a landscaped area at least 2 feet in width, or at driveways and loading docks

- Minimum horizontal dimension: 8 feet from the building wall or must extend to a line 2 feet from the curb line, whichever is less
- Height: lower edge must be between 10-15 feet above the sidewalk
- Adequate lighting for pedestrians shall be provided

23.49.019 PARKING
 Parking Quantity: No minimum requirement
 Parking Location within Structures at street level may be permitted if:

- at least 30% of the street frontage of any street level parking area, excluding garage doors, is separated from the street by other uses
- the facade of the separating uses satisfies the transparency and blank wall standards for Class I Pedestrian Streets

OPTION 1 - MAXIMIZE BUILDING ENVELOPE

DISTINGUISHING FEATURES

- 9-stories of Type-I Construction throughout (per the building code, the lowest level is considered a basement floor).
- Maximizes buildable envelope.
- Live/Work units at-grade along Battery w/ lobby entrance mid-block on Elliott (similar to Option 2).
- Upper garage level access from Battery (similar to Option 2).

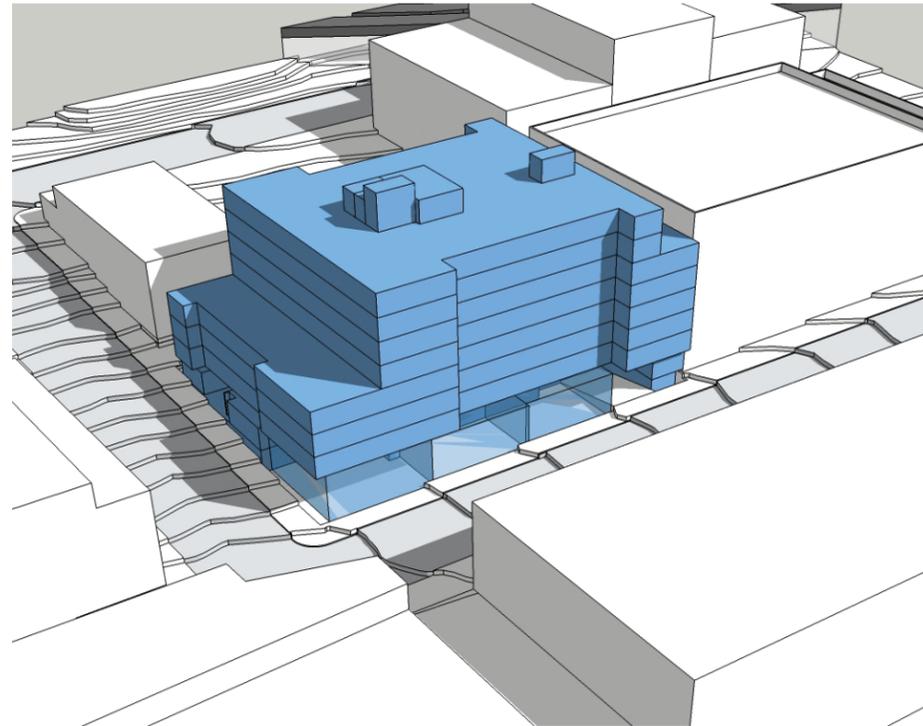
- 118 Dwelling Units
- 4 Live/Work Units
- 77 Parking Stalls

PROS

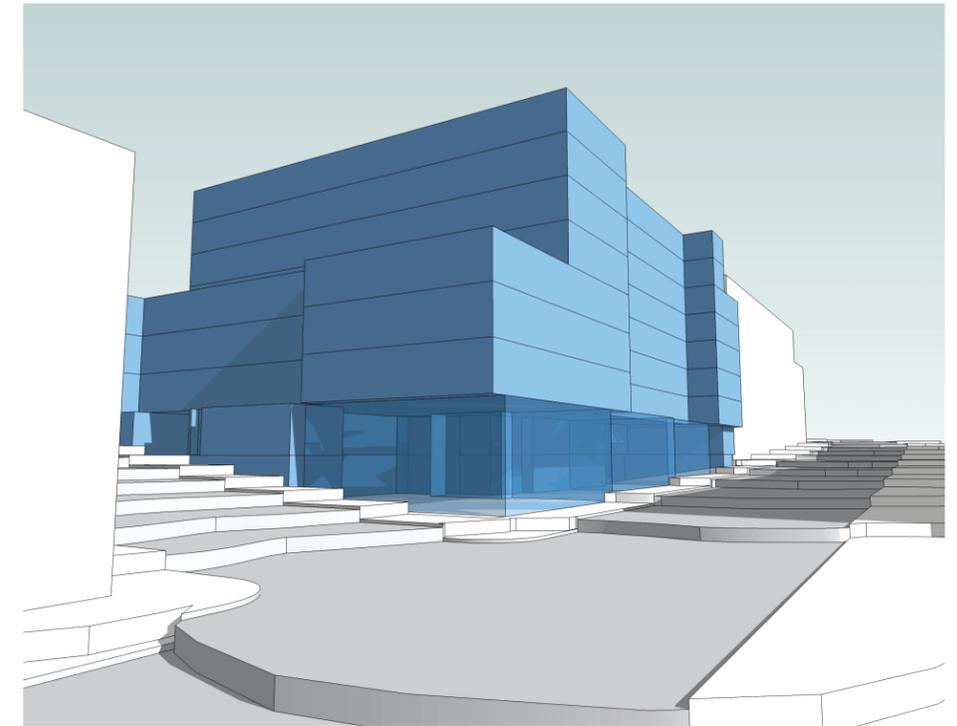
- Provides greatest amount of residential floor area of 3 options presented.
- Views from roof deck of Elliott Bay, Downtown and Belltown.
- Increased efficiency of residential floor area.

CONS

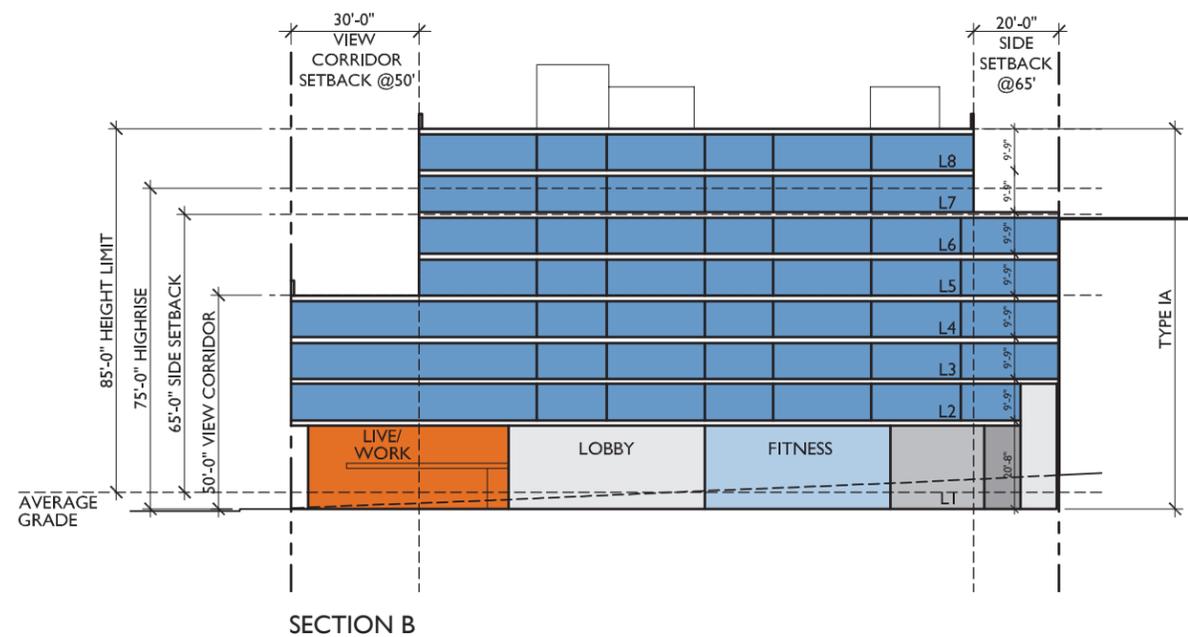
- Substantially increased cost of Type-I and highrise construction with limited increases in floor area.
- Limited façade modulation without courtyard.
- Greatest amount of shade cast of adjacent properties of 3 options presented.
- Reduced surface area reduces ability to bring natural light deeper into each floor plate.
- Large average unit sizes.
- Rubbish removal requires moving up ramp.
- By not entering the garage directly from the adjoining grade, inefficiencies are created.



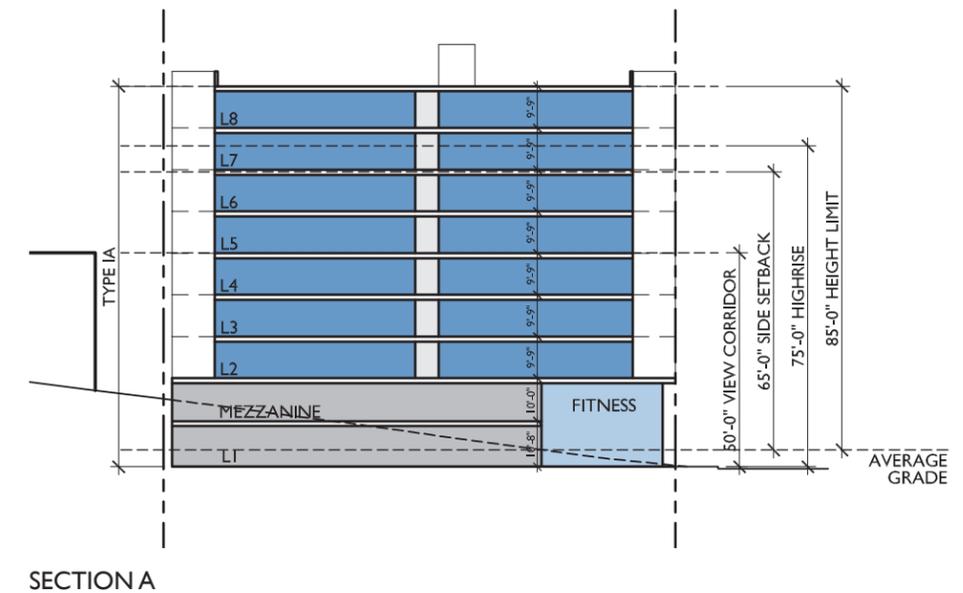
AERIAL VIEW



STREETVIEW

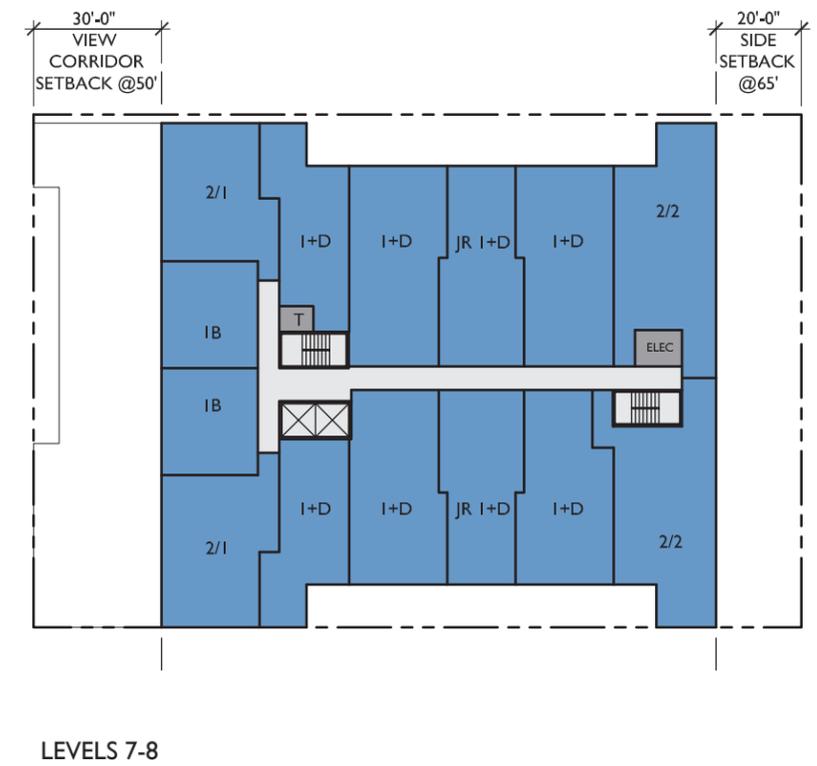
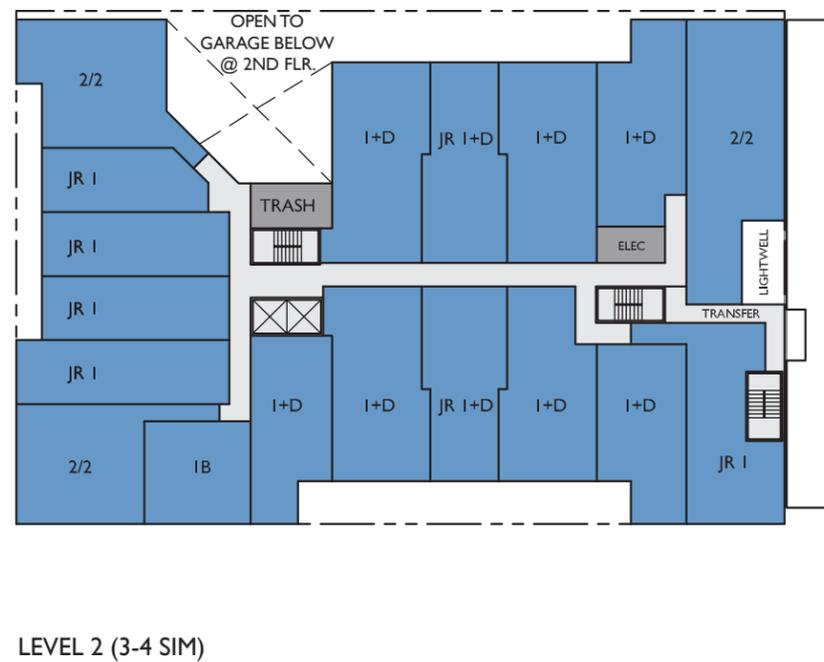
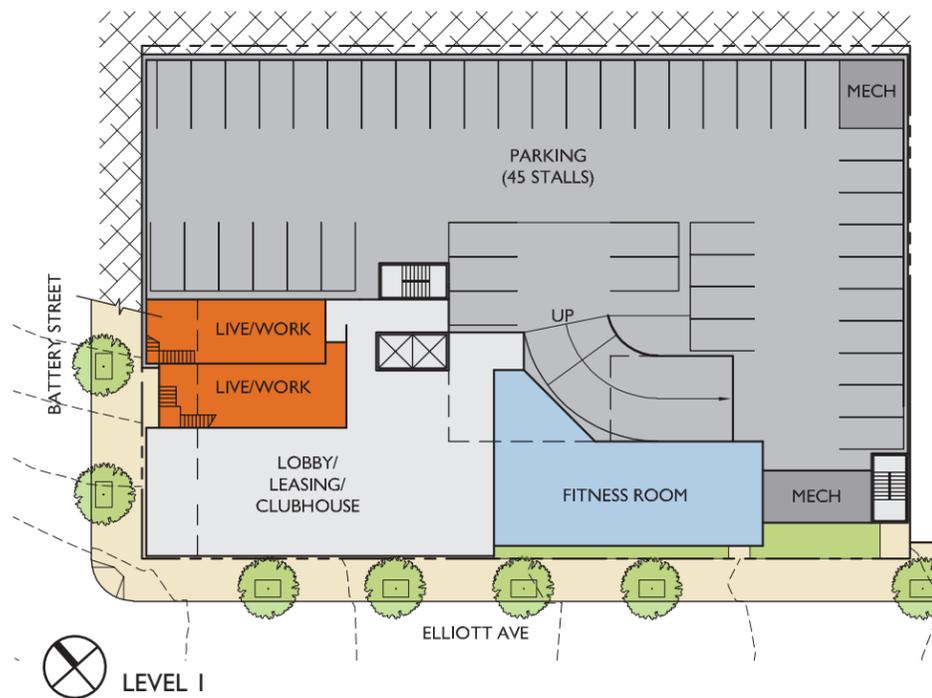
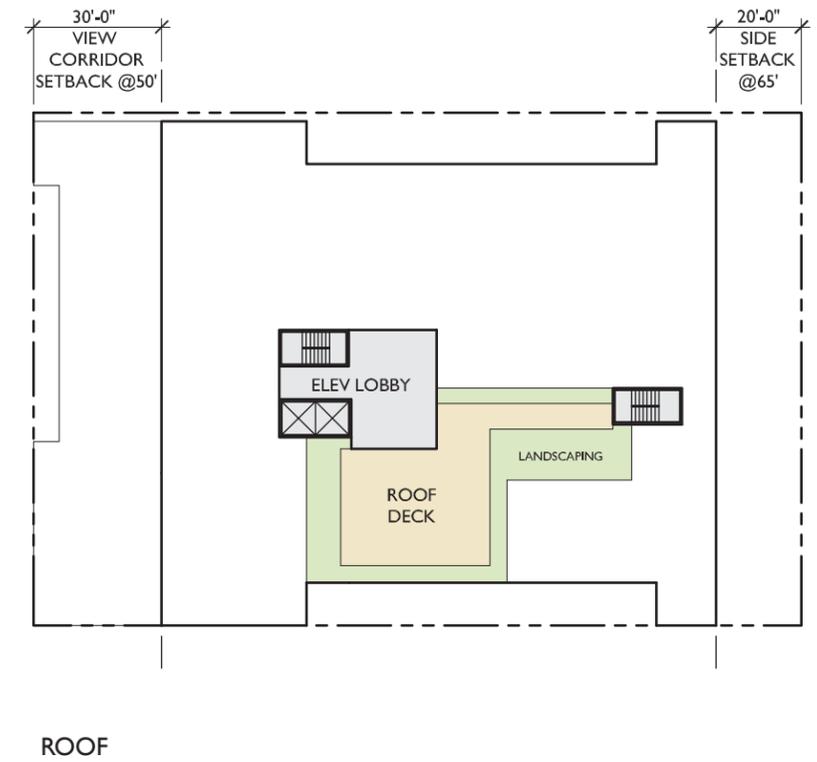
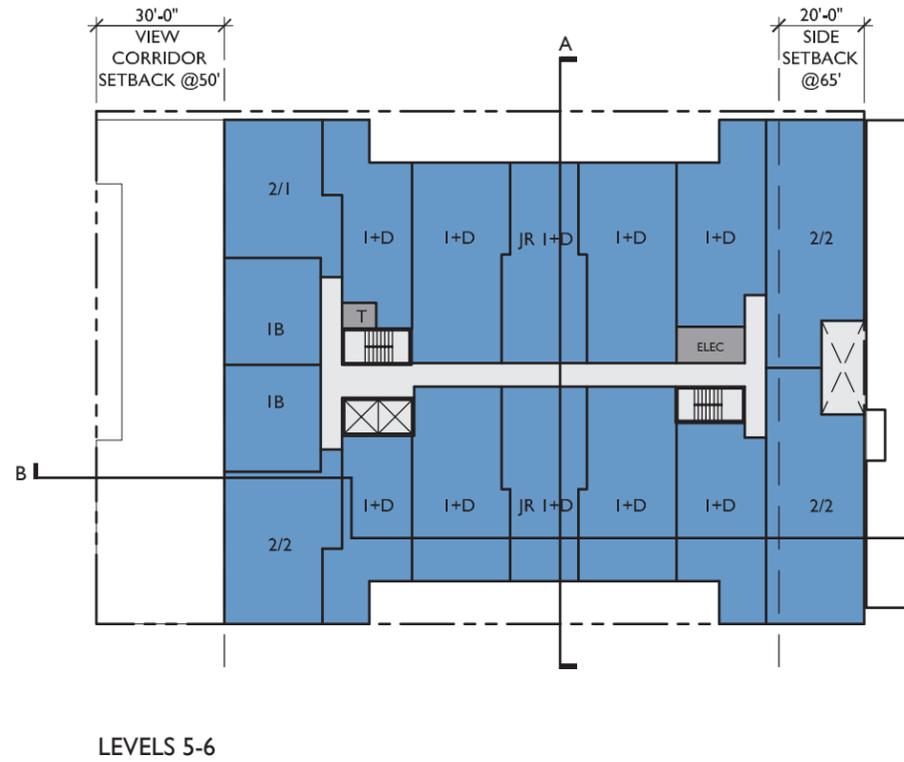
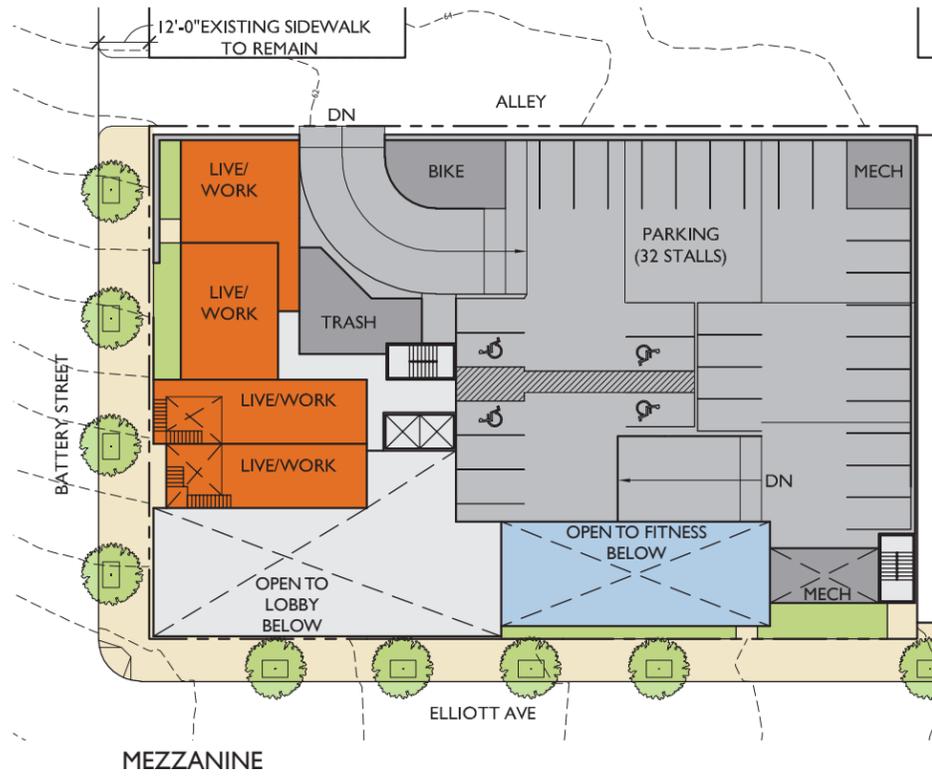


SECTION B



SECTION A

OPTION I - MAXIMIZE BUILDING ENVELOPE



OPTION 2 - 5 OVER 2 COURTYARD SCHEME

DISTINGUISHING FEATURES

- 5-stories of wood-framed apartments over 2 level podium (per the building code, the lowest level is considered a basement floor) without extra residential floor below the podium.
- Courtyard oriented away from Elliott Bay
- Live/Work units at-grade along Battery w/ lobby entrance mid-block on Elliott.
- Upper garage level access from alley.
- Common exterior deck on roof.

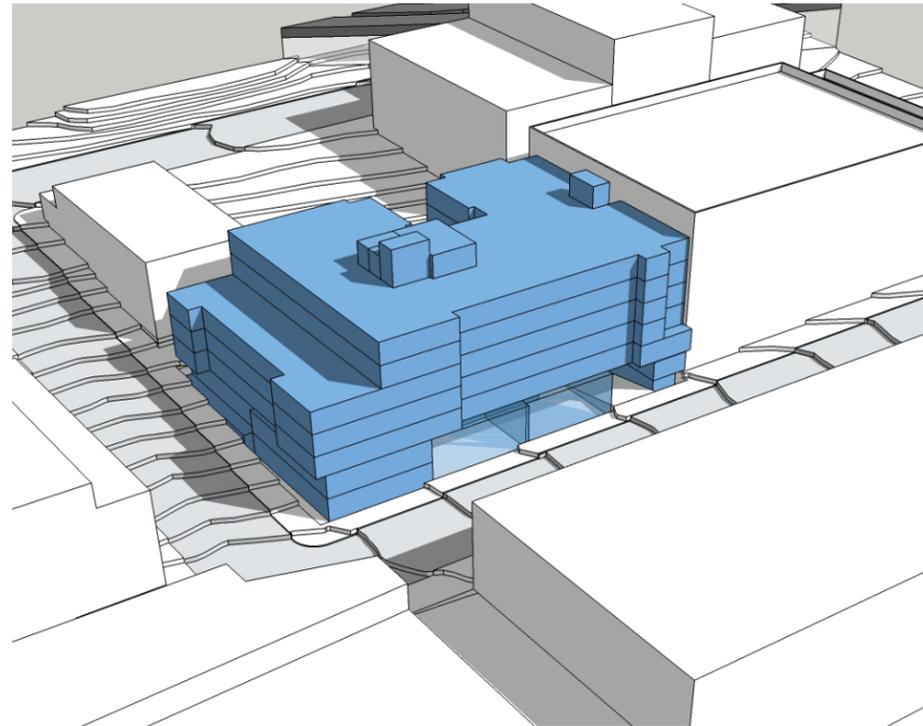
- 97 Dwelling Units
- 5 Live/Work Units
- 73 Parking Stalls

PROS

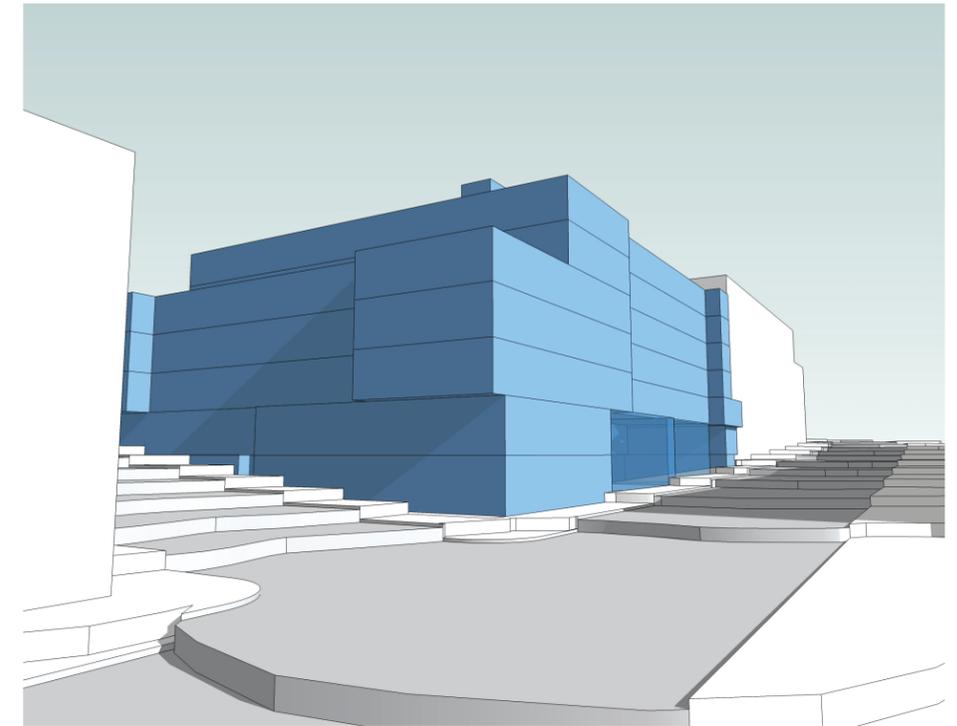
- Views from roof deck of Elliott Bay, Downtown and Belltown.
- Minimizes shade and shadow cast on adjacent properties.
- Courtyard orientation would create added opportunities to develop the alley façade per Guideline C-6.

CONS

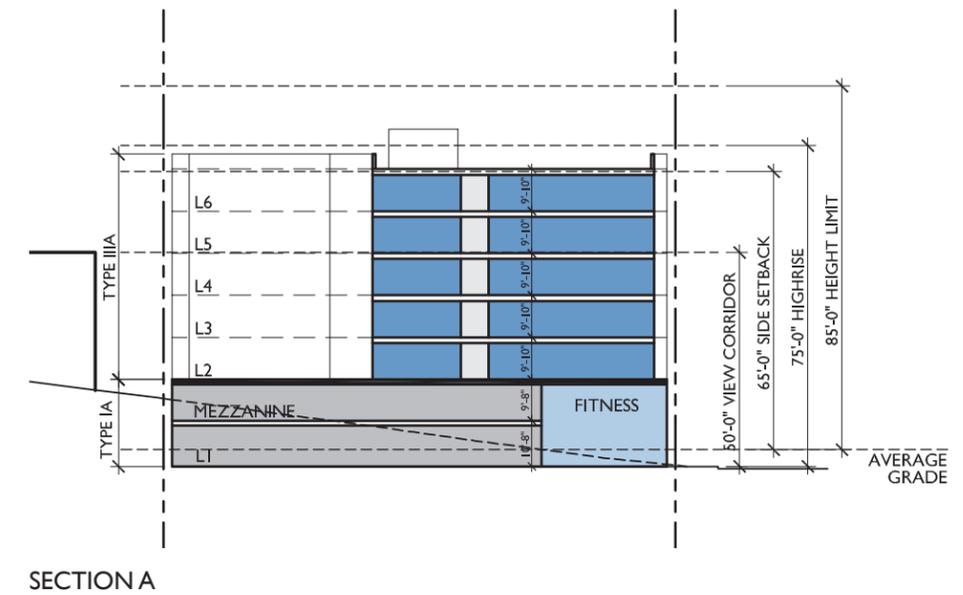
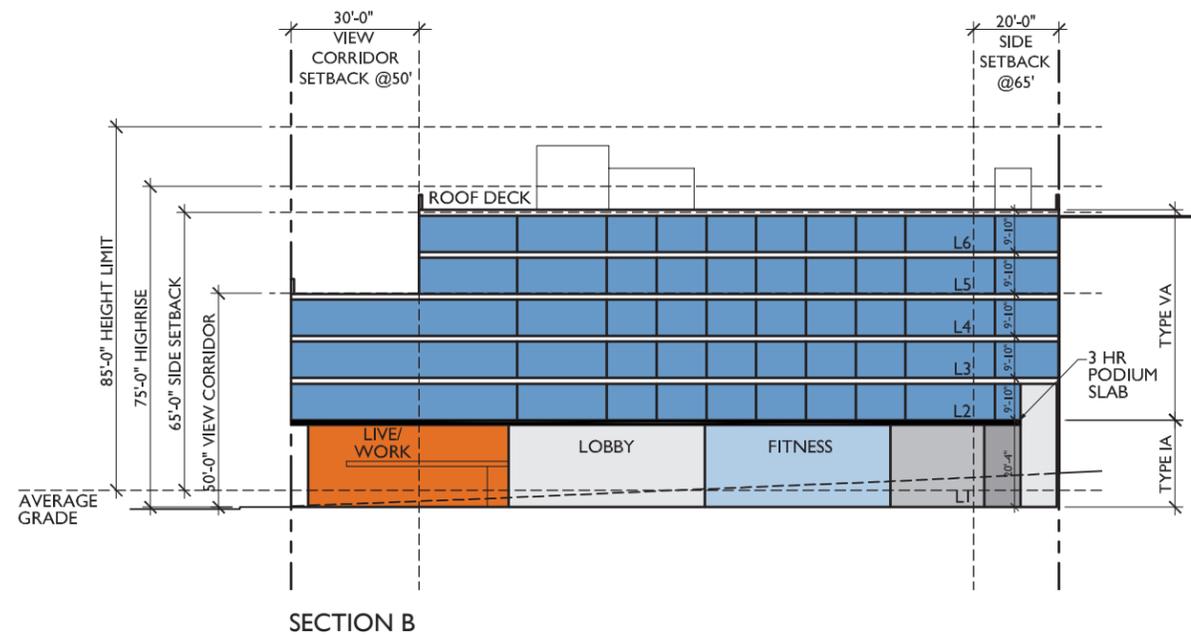
- Smaller portion of residential floor area has views toward Elliott Bay.
- Rubbish removal requires moving up ramp.
- Limited modulation of façade along Elliott Ave.
- Least dense scheme of the 3 options.
- Large amount of floor area dedicated to garage ramping (greatest of the three options)
- Departure required for driveway from Battery
- Courtyard often in shade



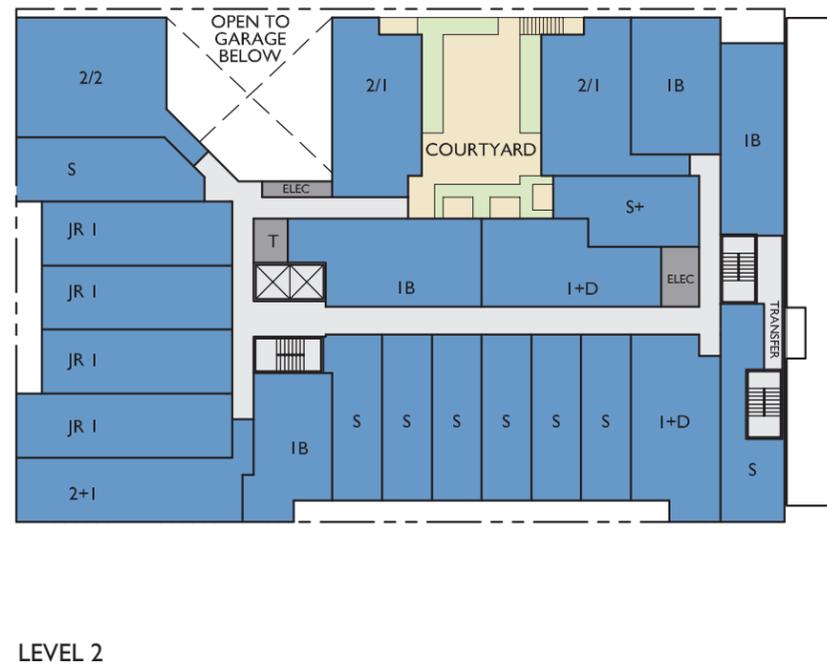
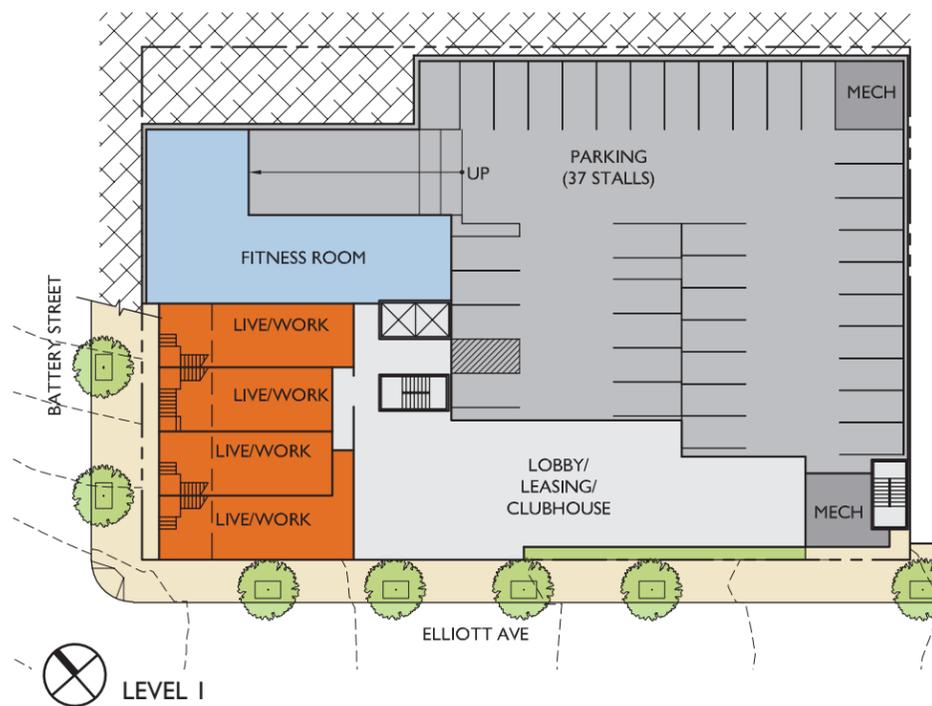
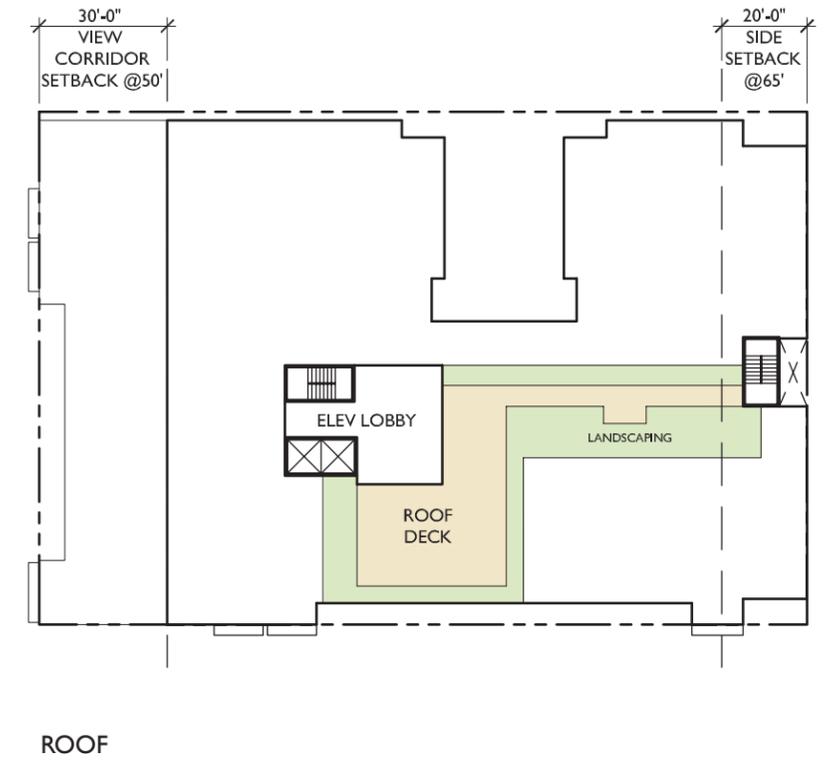
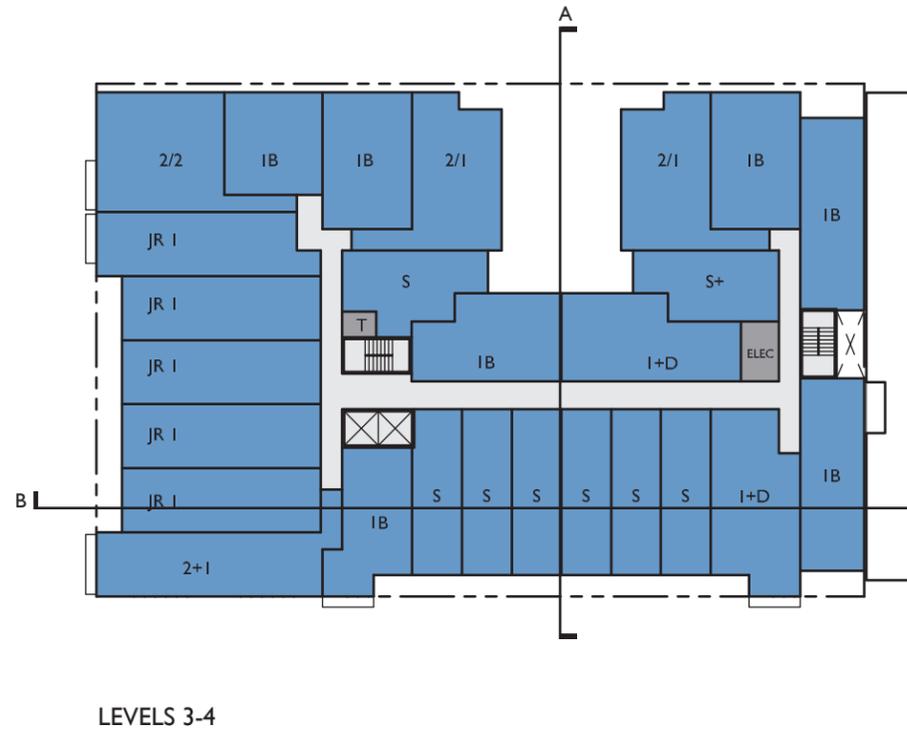
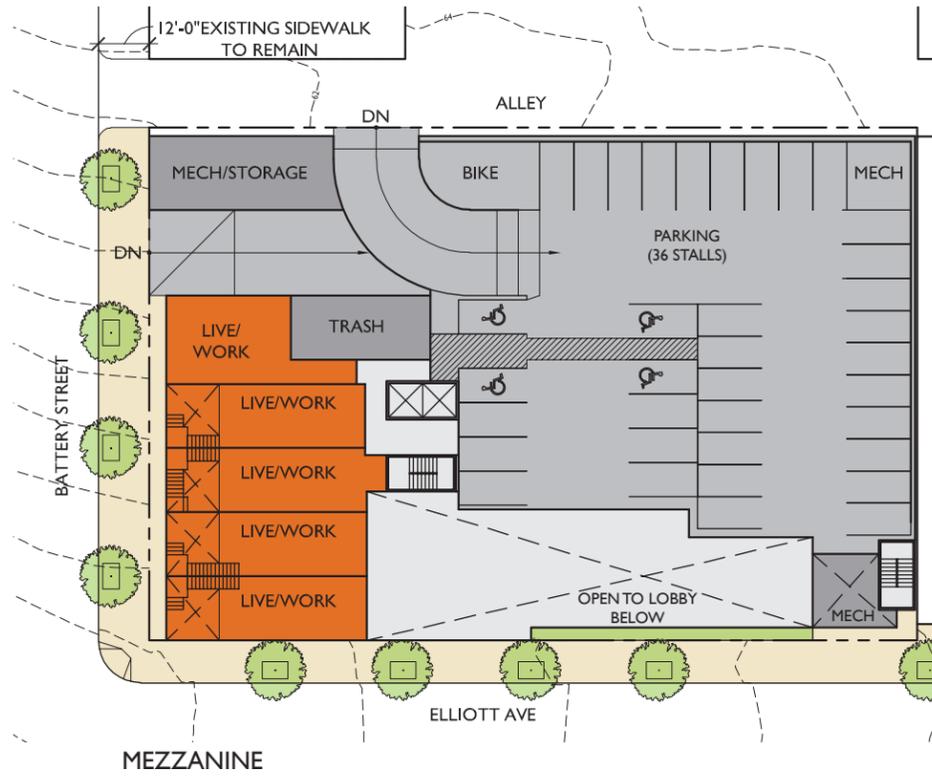
AERIAL VIEW



STREETVIEW



OPTION 2 - 5 OVER 2 COURTYARD SCHEME



OPTION 3 – 5 OVER 3 COURTYARD SCHEME (PREFERRED)

DISTINGUISHING FEATURES

- 5-stories of wood-framed apartments over 3 level podium (per the building code, the lowest level is considered a basement floor) with full floor of residential units below the podium.
- Courtyard oriented toward Elliott Bay
- Double height lobby entry at corner with Live/Work units at-grade along Elliott.
- Garage access from Elliott and alley, following previously approved SIP.
- Roof deck and common indoor amenity spaces carved into upper floor.

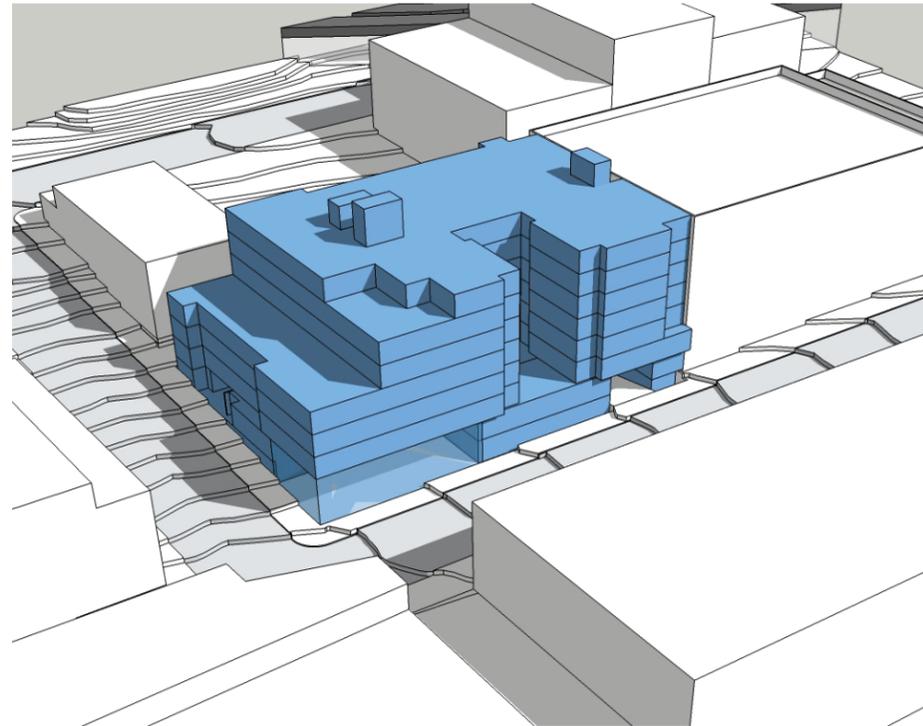
- 141 Dwelling Units
- 5 Live/Work Units
- 83 Parking Stalls

PROS

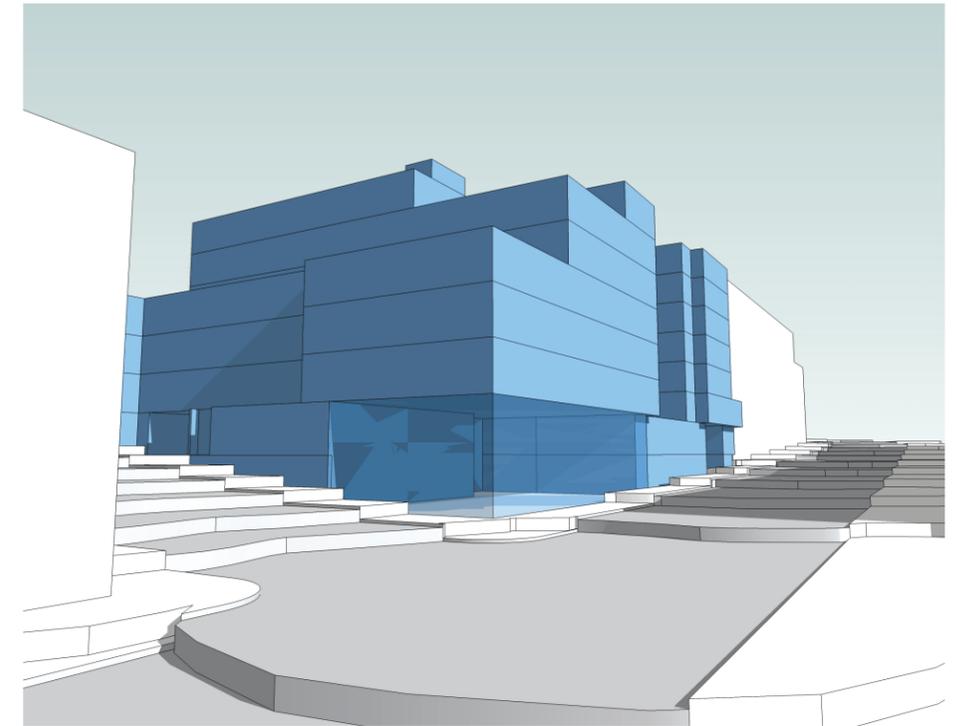
- Provides added density without added expense of Type I and highrise construction throughout.
- Courtyard orientation maximizes views of Elliott Bay and solar exposure.
- Courtyard orientation breaks up building mass as viewed from Elliott Ave. and waterfront.
- Uses topography strategically for garage access, reducing internal ramping and adding garage efficiency.
- Eroded top floor creates ample indoor common amenity space with Bay views.

CONS

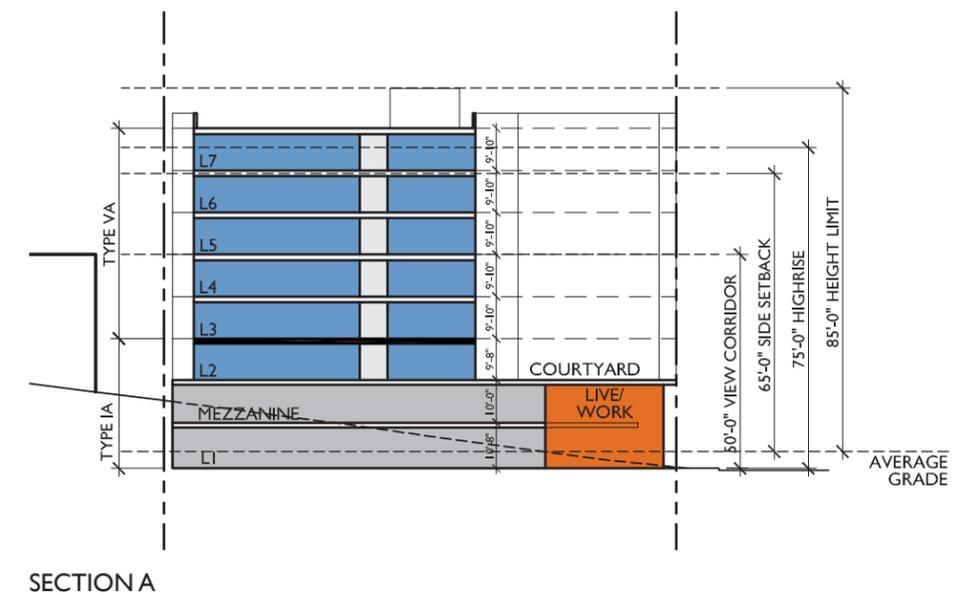
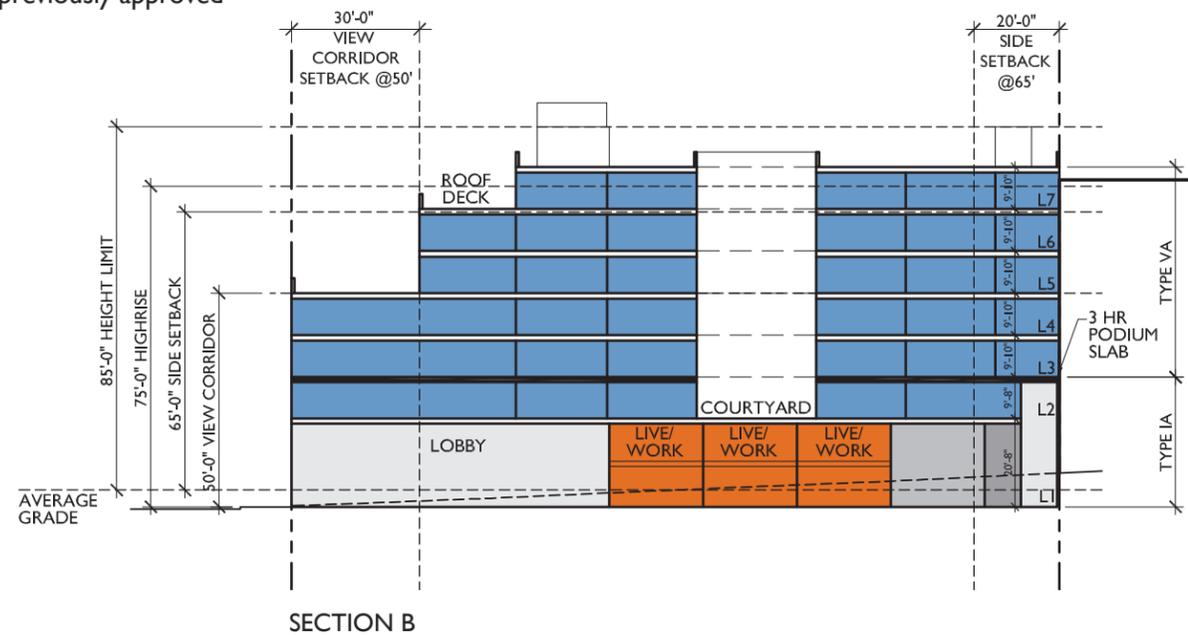
- Extra residential floor adds to shading of properties located across alley.
- Departure from setback above 65' height limit.
- Rubbish removal requires moving up ramp.
- Departure for garage access from Elliott to follow previously approved SIP.



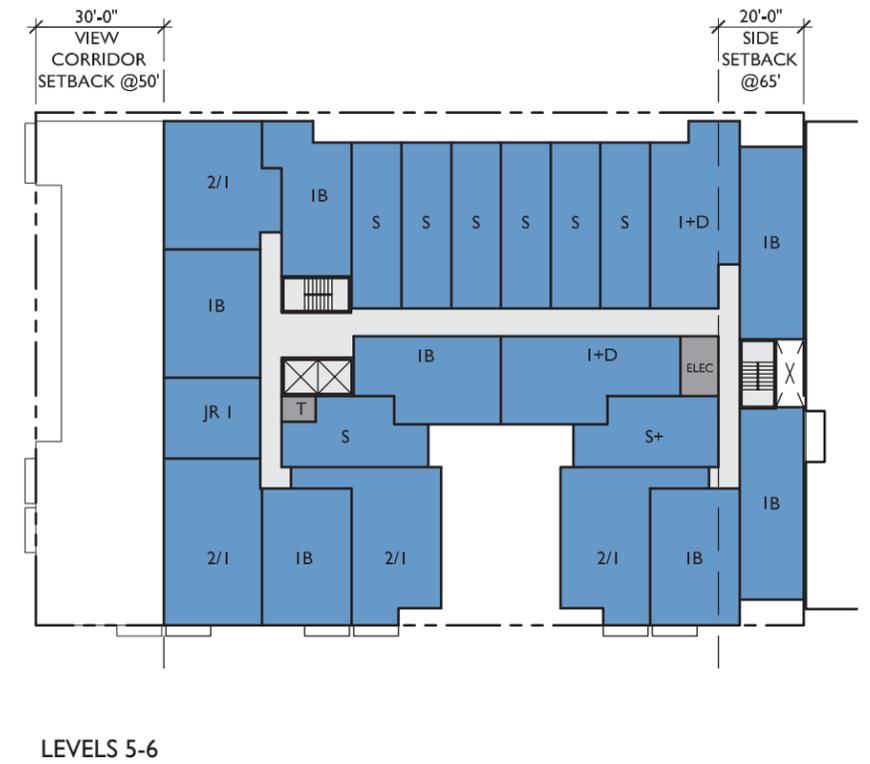
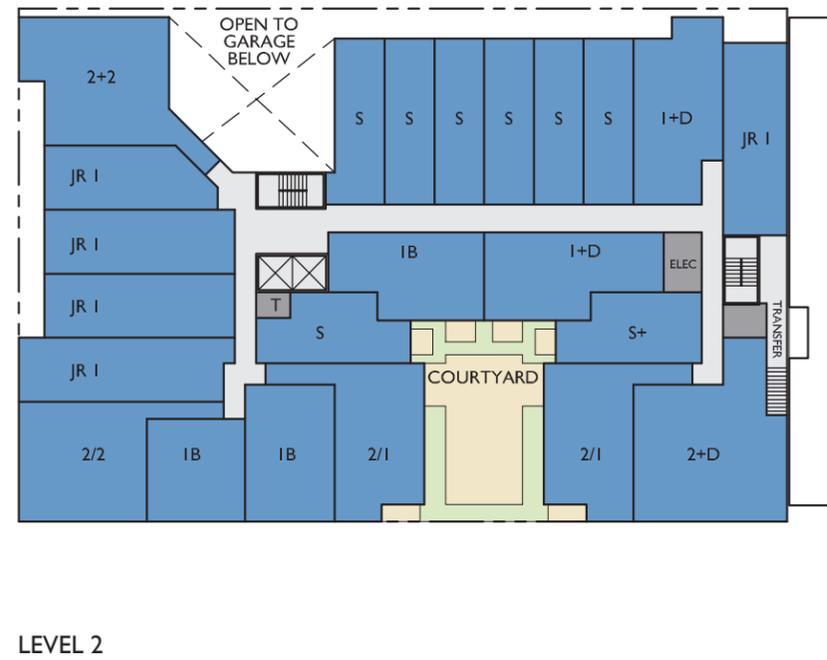
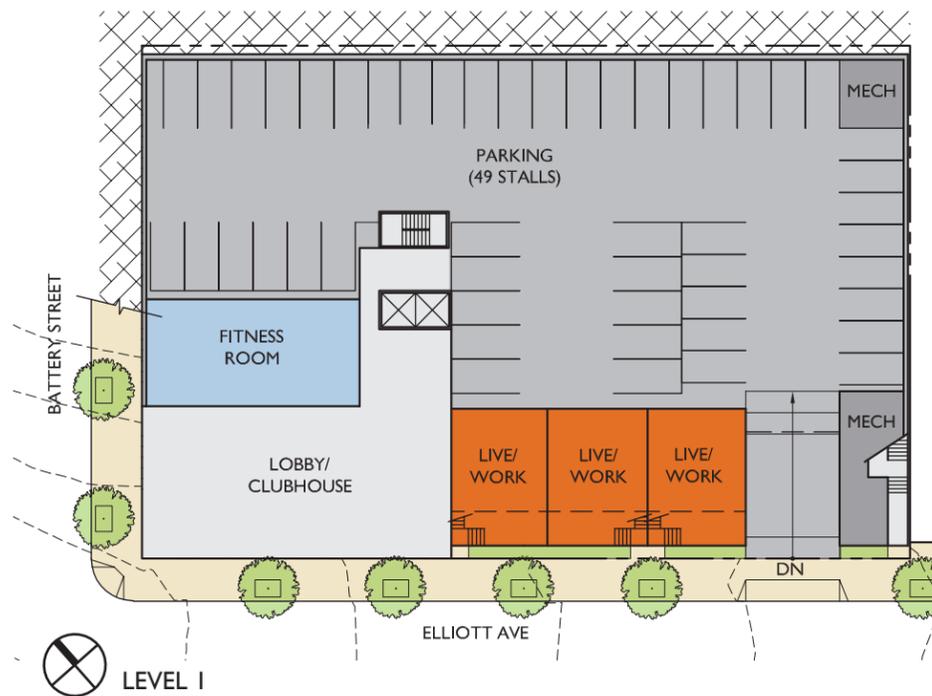
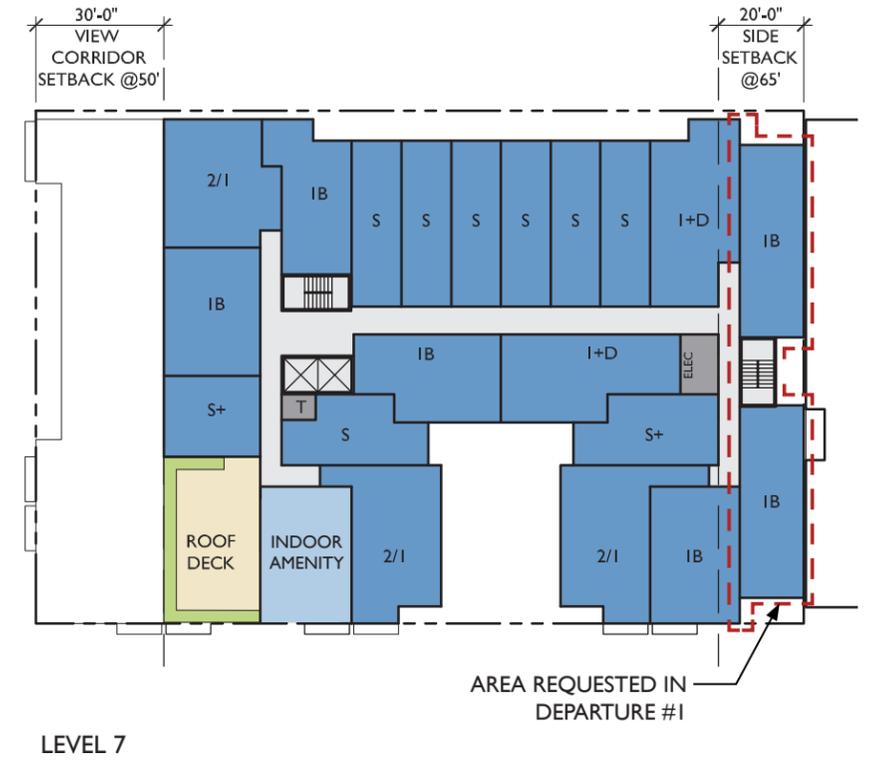
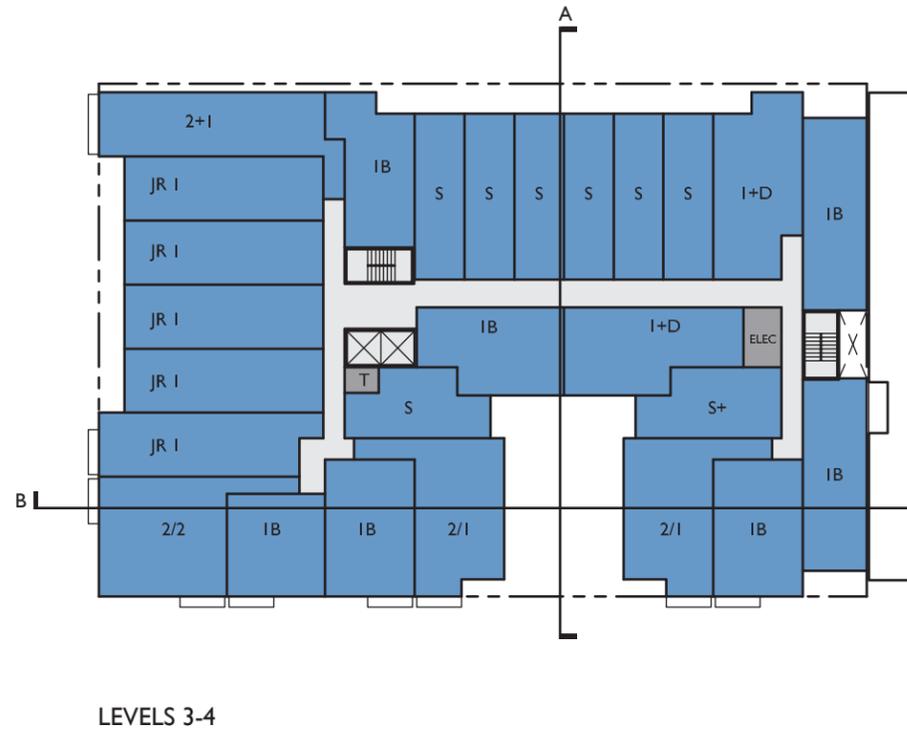
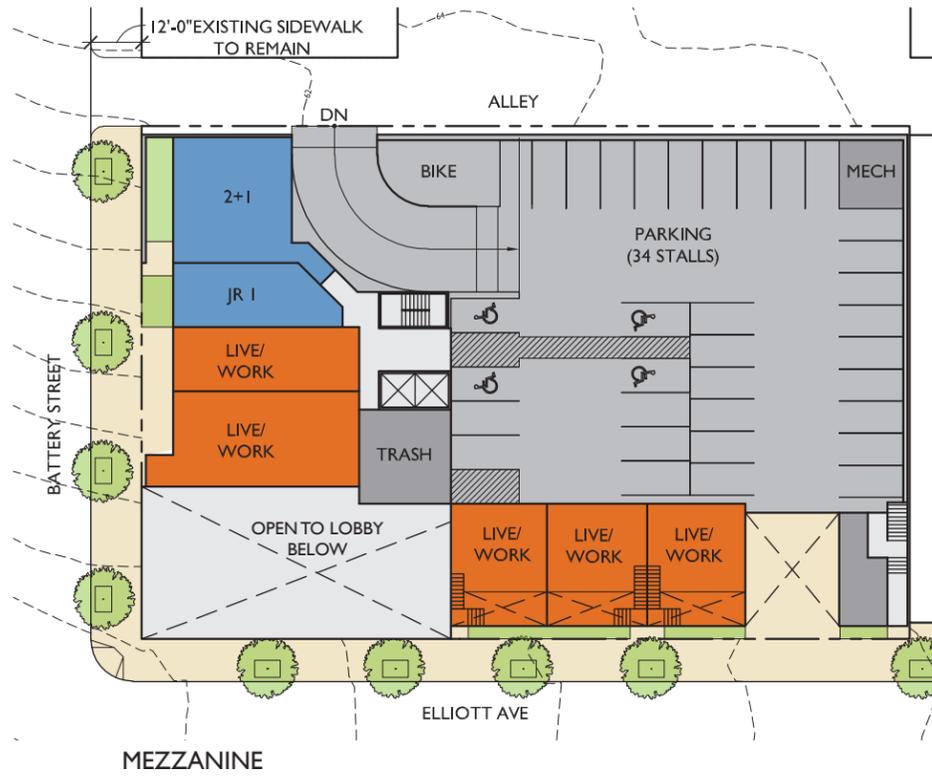
AERIAL VIEW



STREETVIEW



OPTION 3 – 5 OVER 3 COURTYARD SCHEME (PREFERRED)



DEPARTURES

DEPARTURE RATIONALE

Two main factors drive the departure requests:

- Provisions in the Land Use Code to create stepped, articulated massing by way of view corridors, setbacks and lot coverage limits above a certain height and limits on façade length.
- The existing site topography.

The departures requested would aid in the overall goals of the Land Use Code to establish a project with varied massing and articulated facades that add, not detract, from the neighborhood while enhancing the dwelling units and common spaces within the project. Departures #1, #2, and #3 request methods to vary the façade in plane by providing projections into the required sideyard and over the right of ways. Departure #4 requests the ability to exceed the maximum façade length while acknowledging that steps will be taken to vary the façade to avoid the appearance of a monolithic building. Departure #5 proposes to use the roof created by the required view corridor as an outdoor amenity space for residents by allowing open railing and balcony projections into the setback. Departure #6 requests that the existing site topography be used in a common-sense manner; as the alley runs the length of the highest side of the site, the project requests that one garage driveway be from one of the streets fronting the project in lieu of additional internal ramping. Departure #7 requests that a portion of the roof deck be enclosed so that the views of Elliott Bay could be enjoyed year round by all of the building's residents although, by definition, enclosing this space would exceed the 50/50 indoor to outdoor ratio mandated by the code.

ZONING DEPARTURES	OPTION 1	OPTION 2	OPTION 3
1 Side setbacks over 65'			X
2 Lot Coverage ≤ 65%			X
3 Bay window & balcony projections over ROWs		X	X
4 Wall Length Dimension over 120'		X	X
5 Balcony and open railing projections into view corridor		X	X
6 Vehicle access to lower garage level from street		X	X
7 Allow additional enclosed common recreation area			X

DEPARTURE #1: Side setback of 20' required from common property line above 65' (23.49.166)

The intent of the DMR zones in the surrounding area was to create a uniform base height of 65' with narrow towers of various heights rising above, stepped back from the edge of the property. Options 2 and 3 propose mid-rise projects that do not step back to higher towers. Allowing the upper most floor of Option #3 to encroach vertically into the sideyard setback by +/- 8' plus parapet would

help reinforce this lower massing "base" as the project will better align with the top of the adjacent apartment building.

DEPARTURE #2: 65% coverage permitted between 65' and 85' (23.49.158, Table A).

If Departure #1 is granted, the addition of floor area within the 20' sideyard setback on Level 7 of Option 3 will increase the lot coverage above 65' to approximately 72%. Similar to and in conjunction with Departure #1, the addition of this floor area will reinforce the lower massing "base" and help the project to better align with the adjacent existing apartment building.

DEPARTURE #3: Bay window and balcony projections over the avenue, street & alley (23.53.035)

To increase the articulation of the facades, allow for structural projections of bay windows and balconies into right of ways at selected points around the building. Request is to increase bay width projection from 9' to 12' and allow 90-degree corners in lieu of 45 degree corners that are more Victorian in nature and would likely not match desired architectural style. Balcony projections in alley would not be closer than 8' to the center line and bay window projections over the street would not exceed 36" from the main façade.

DEPARTURE #4: Wall length dimensions are not to exceed 120' above 65' in elevation (23.49.164).

Given the length of the site is 180', after the view corridor setback of 30' and sideyard setback of 20', 130' of building length above 65' is left. Accounting for the request made in Departure #1, the maximum wall length proposed would be 150' above 65' on a building façade opposite the courtyard (either facing the alley in Option 3 or Elliott Ave. in Option 2). Both Options 2 & 3 feature articulated recesses and facade projections to break-up the facade on all exposed elevations.

DEPARTURE #5: Allow 42" high open guardrails and planter boxes to project vertically into the view corridor setback and balconies with open railings to project horizontally into the view corridor setback (23.49.024.B)

The roof area created by the required view corridor setback would benefit from roof decks for use by the building occupants. The units fronting the view corridor would further benefit from having small, open balconies attached to the setback building façade.

DEPARTURE #6: Provide vehicle access to the lower garage level directly from the right of way. Upper garage level to be accessed directly from the alley (23.49.019.H.1).

Due to the site topography, the alley is located at the high point of the site, approximately 2 floor levels above the low-point of the site at the intersection of Elliott and Battery. The parking would logically be located on the lowest two levels of the building, away from the street and tucked in against the hillside. Using the existing natural topography to access the garage would eliminate

the need for internal ramping but would require one access from Elliott. The requirements set forth in 23.54.030 allow two driveways. The two adjacent apartment buildings feature two driveways directly from the street and no alley access.

DEPARTURE #7: To allow enclosed common area provided in excess of the minimum required to be excluded when calculating the 1:1 ratio of outdoor common area to enclosed common area (23.49.010.B.2).

The intent of 23.49.010.B.2 is to ensure an acceptable ratio of enclosed to unenclosed common recreation area. The project is required to have 5% of gross residential floor area provided as common recreation area, of which a maximum of 50% is permitted to be enclosed. In Option 3, an area equivalent to 5% of the gross floor area is being provided as common recreation area, 50% of which is being provided as unenclosed area. Option 3 however provides an additional amount of enclosed common recreation area equal to 1.4% of the project's gross residential floor area, thus creating approximately a 60/40 ratio of enclosed to unenclosed space. Permitting the additional enclosed common recreation area would allow for an ample indoor recreation space to be attached to the outdoor roof deck, providing views of Elliott Bay that could be enjoyed by residents throughout the year.

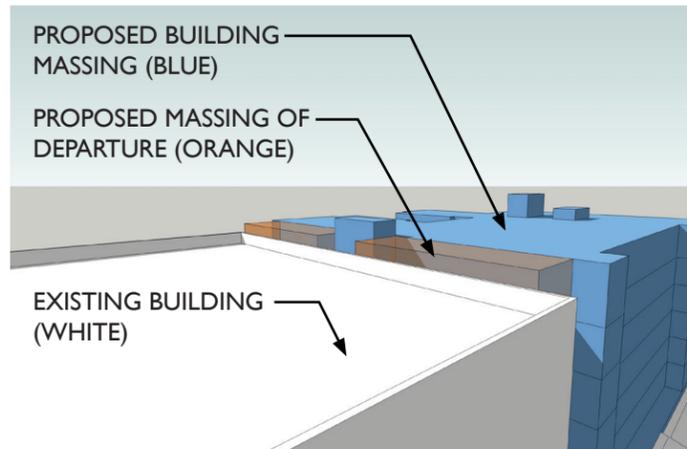
LEVEL 1 & MEZZANINE AREA COMPARISON

DESIGN OPTION (GARAGE ACCESS POINTS)	GARAGE AREA	STALLS	AREA/STALL	USABLE AREA	UNITS
Option 1 (Alley access only)	25,930 sf	77	337 sf	8,264 sf	4 live/work
Option #2 (Alley/Battery)	23,586 sf	73	323 sf	10,424 sf	5 live/work
Option #3 (Alley/Elliott)	24,664 sf	83	297 sf	11,022 sf	5 live/work 2 residential

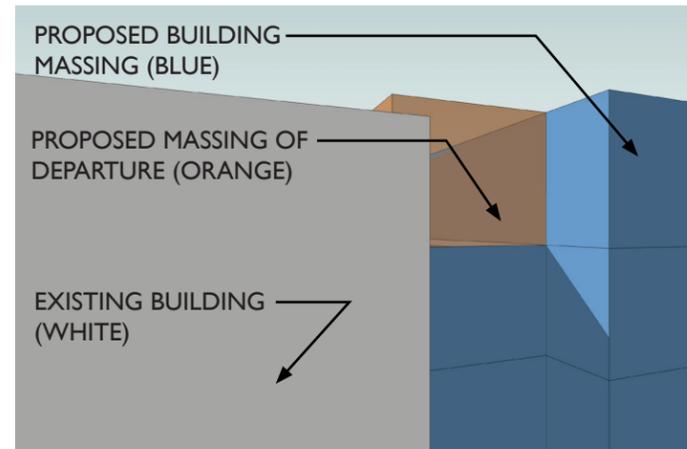
PARKING STALLS PER UNIT RATIOS

Unit count shown in Option 3 (Preferred): 141

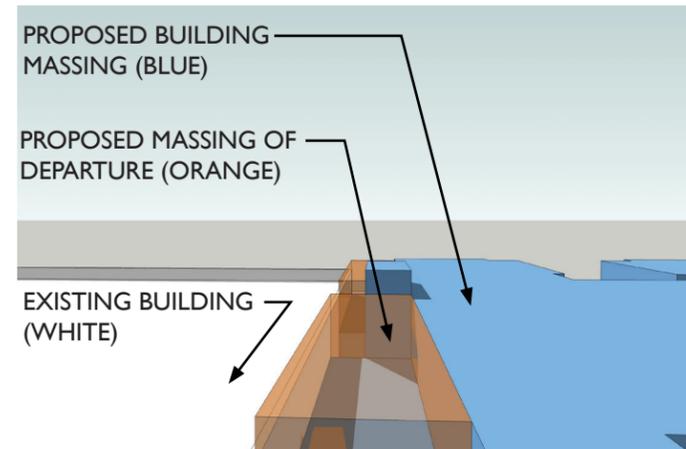
UNIT COUNT SHOWN IN OPTION 3 COMBINED WITH GARAGE SHOWN IN:	STALLS	STALL/UNIT RATIO
Option 1	77	.54
Option 2	73	.51
Option 3	83	.59



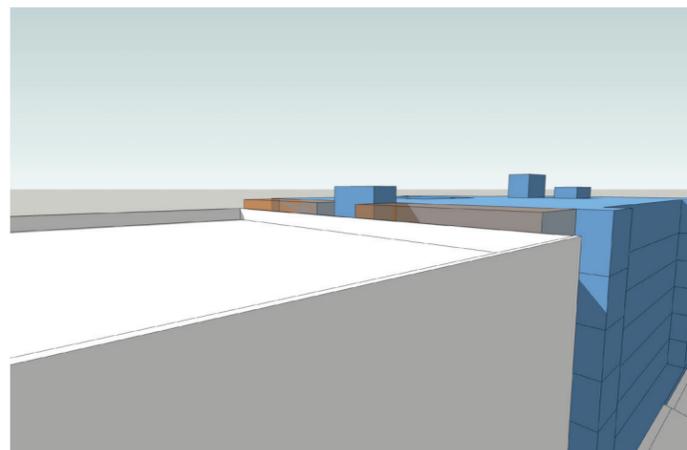
Ⓐ EXISTING LEVEL 6 ≈ +124'-6"



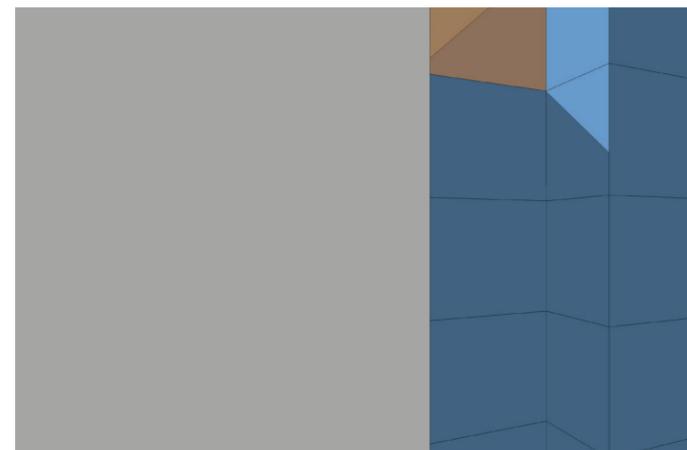
Ⓑ EXISTING LEVEL 3 ≈ +100'-6"



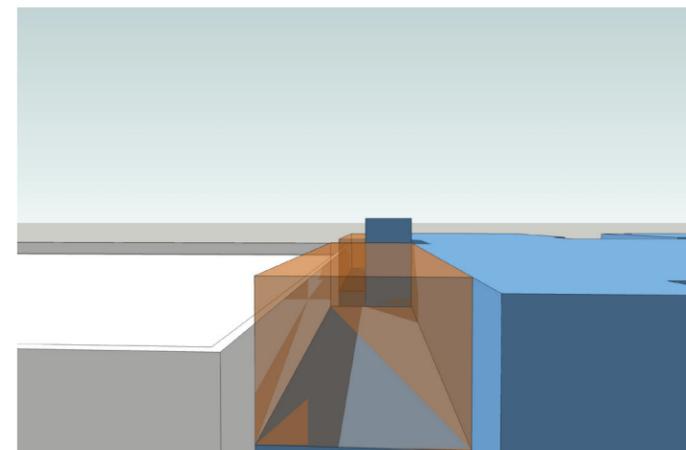
Ⓒ FUTURE LEVEL 6 ≈ +124'-6"



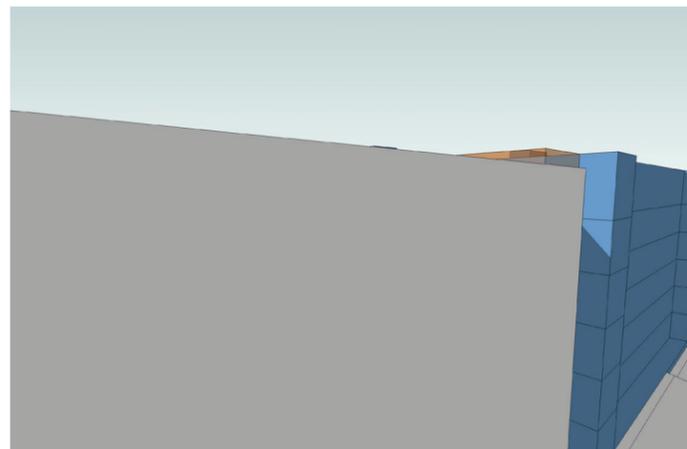
Ⓐ EXISTING LEVEL 5 ≈ +114'-6"



Ⓑ EXISTING LEVEL 2 ≈ +86'-6"



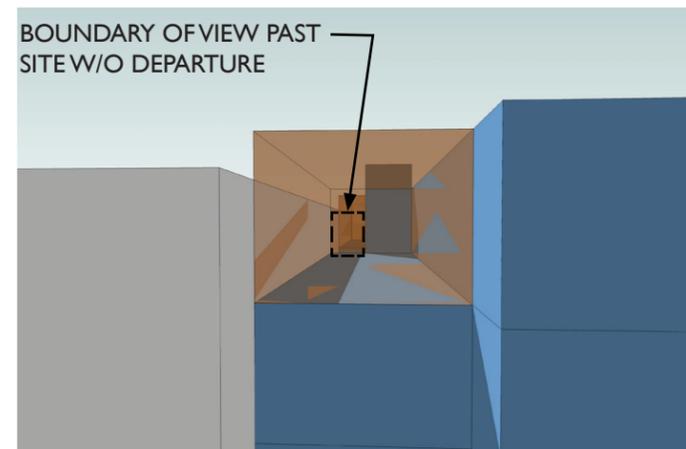
Ⓒ FUTURE LEVEL 5 ≈ +114'-6"



Ⓐ EXISTING LEVEL 4 ≈ +104'-6"



Ⓑ EXISTING LEVEL 1 ≈ +72'-6"



Ⓒ FUTURE LEVEL 4 ≈ +104'-6"



SUMMARY OF 3 DESIGN OPTIONS

Option 3 is the preferred scheme as it could increase residential density by using conventional construction types that are compatible with the surrounding mid-rise apartment buildings. By providing one additional residential floor of Type-I construction below the podium, this scheme uses the sloping site to its advantage; the lowest floor will be considered a basement per the building code and the upper residential floors will be raised slightly, increasing views of Elliott Bay. The orientation of the courtyard toward Elliott breaks up the building mass and creates views for units located deep within each floor plate. Further eroding the top floor creates an outdoor common roof deck and indoor amenity space with ample views of the bay while keeping the building under the highrise limit.

AESTHETIC DIRECTION & MATERIALS

The immediate area around the site is defined by the former warehouse buildings once associated with waterfront industry, the hard-edged concrete and glass style of the World Trade Center buildings and the emergence of the Alaskan Way Viaduct from the Battery Tunnel. The project seeks to utilize the palate of industrial materials and respond to the existing character of the neighborhood, but at a residential scale with bold elements that would add an artistic spark, similar to other recently completed residential projects in the neighborhood. This material palate will likely include metal panel systems, exposed concrete, steel-framed balconies and large windows. Vivid colors and interesting material textures would be used strategically to enliven the streetscape and create a distinctive sense of place.



AESTHETIC VIGNETTE A - NEO MID-CENTURY MODERN: EMPHASIZING HORIZONTAL BUILDING LINES & SOFTENED WITH WOOD ACCENTS.



AESTHETIC VIGNETTE B - ERODED ENVELOPE: RESIDENTIAL FLOOR PLANS DICTATE CUTS INTO THE DARK-COLORED ENVELOPE AT BALCONIES THAT REVEAL A VIVID-COLORED INNER CORE.



AESTHETIC VIGNETTE C - VERTICAL FRAMES: EMPHASIZING VERTICAL BUILDING LINES, PROJECTIONS ARE ARTICULATED AS 'PICTURE-FRAMES' WITH LARGE GLAZED OPENINGS.



H2O APARTMENTS - LEED H MIDRISE PILOT GOLD TARGET



222 VIEW APARTMENTS



BROADSTONE KOI APARTMENTS - LEED NC CERTIFIED TARGET



OLIVE WAY MIXED-USE APTS - LEED NC SILVER TARGET



WESTLAKE VILLAGE



MIST APARTMENTS - LEED NC SILVER TARGET

ARTHOUSE - ELLIOTT & BATTERY - DPD #3012499

EARLY DESIGN GUIDANCE