





The site is zoned SM-85 and is within the Uptown Urban Center. The east edge of the Seattle Center underlay zone is across Broad St More complete zoning information is included at right.

ZONING ANALYSIS SUMMARY

Site Location: 416 John St
16,966 sf

Site Zoning: SM-85 – Uptown Urban Center

Permitted Uses: 23.48.004 All uses are permitted outright, either as principal or accessory uses, except high-impact uses, heavy manufacturing uses, general manufacturing greater than 25,000 square feet, drive in business, jails, adult motion picture theaters, outdoor storage, surface parking, animal shelters or husbandry, park or pool lots, park and ride lots, work release centers, recycling, solid waste management, or mobile home parks.

Facade Transparency: 23.48.018 Facade transparency and blank facade requirements shall apply to the area of the facade between two (2) feet and eight (8) feet above the sidewalk. A minimum of thirty (30) percent of the width of the street-level facade must be transparent.

Height: 23.48.016 C 85 feet

Height Rooftop features: 23.48.010E The ridge of pitched roofs with a minimum slope of six to twelve may extend 10 feet above the height limit. The ridge of pitched roofs with a minimum slope of 4 to 12 may extend 5 feet above the height limit

Lot Coverage:

Density / FAR: 23.48.016B FAR of four and one half (4.5) is the maximum chargeable floor area permitted for Commercial/Hotel Use. No FAR limit for Residential Use.

Setbacks: 23.48.012 No setbacks required.

Parking: 23.48.034C1a Access to parking and loading shall be from the alley when the lot abuts an alley improved to the standards of Section 23.53.030 C and use of the alley for parking and loading access would not create a significant safety hazard as determined by the Director. If the lot fronts on an alley and an east/west oriented street, parking and loading access may be from the east/west oriented street.

23.54.015 Minimum Parking requirements:

Hotel – one space per 4 rooms

Apartment – one space per dwelling unit

Multifamily and Commercial Zones: 23.54.020F The minimum parking requirements for all uses may be reduced by areas 20% when the use is located within 800 feet of a street with peak transit service headways of 15 minutes or less in each direction.

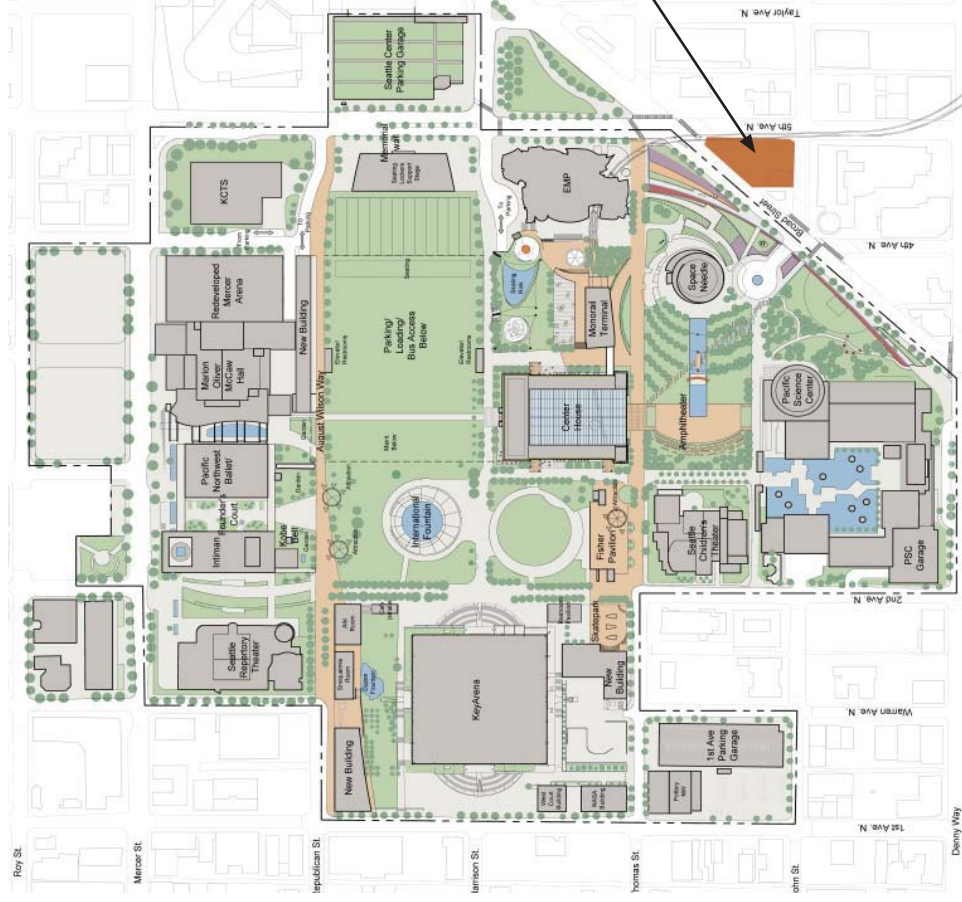
Landscaping:

Green Factor / Street Trees: 23.48.024C Street trees shall be provided in all planting strips. Existing street trees may count toward meeting the street tree requirement. No Green Factor is required in SM-85.

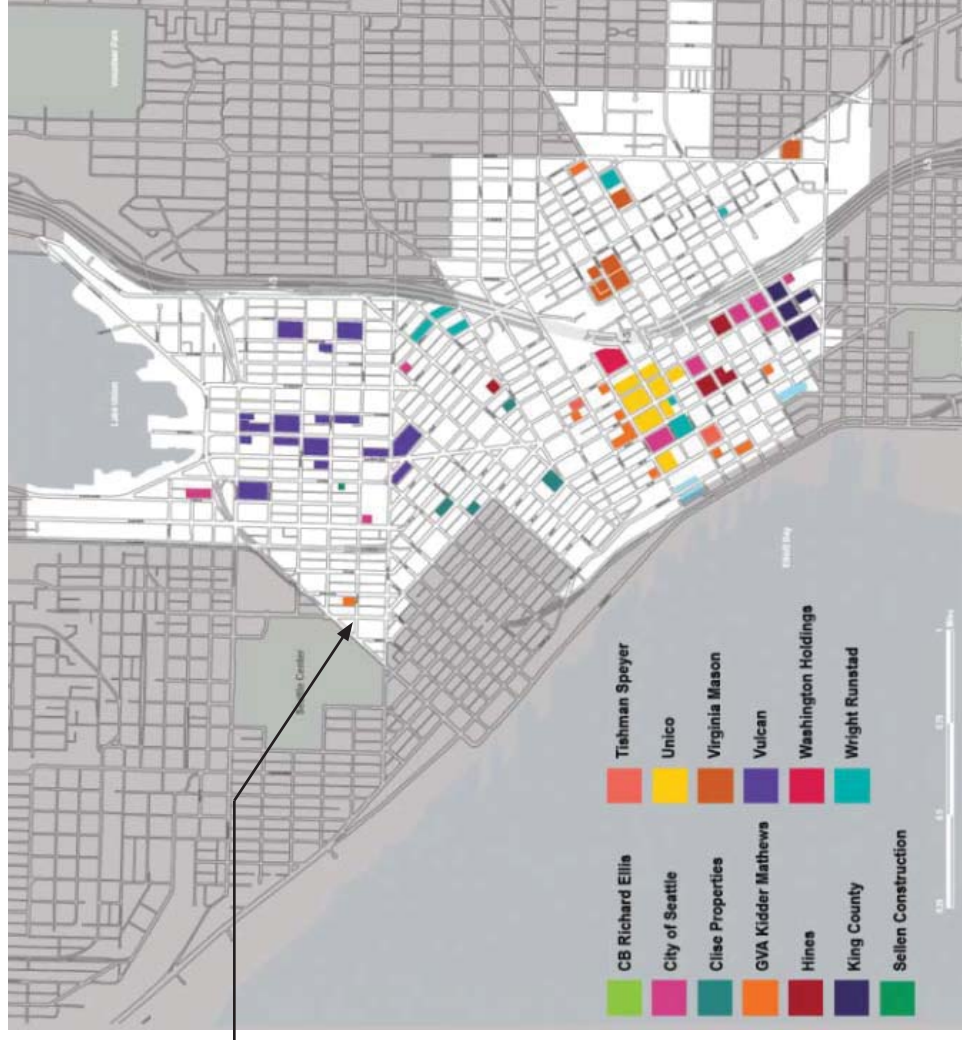
Amenity Space: 23.48.020 All new structures containing more than 20 dwelling units shall provide amenity area on the lot in an amount equivalent to 5 percent of the total gross floor area in residential use. The amenity area shall be available to all residents and may be provided at or above ground level. A maximum of 50 percent of the amenity area may be enclosed. The minimum horizontal dimension for residential amenity area is 15 feet, and no required amenity area shall be less than 225 square feet in size. The exterior portion of required amenity area shall be landscaped and shall provide solar access and seating.

Modulation: Not required in SM-85 (Seattle Mixed)





Seattle Center Century 21 Master Plan



Seattle 2030 Challenge District

Neighborhood Plan

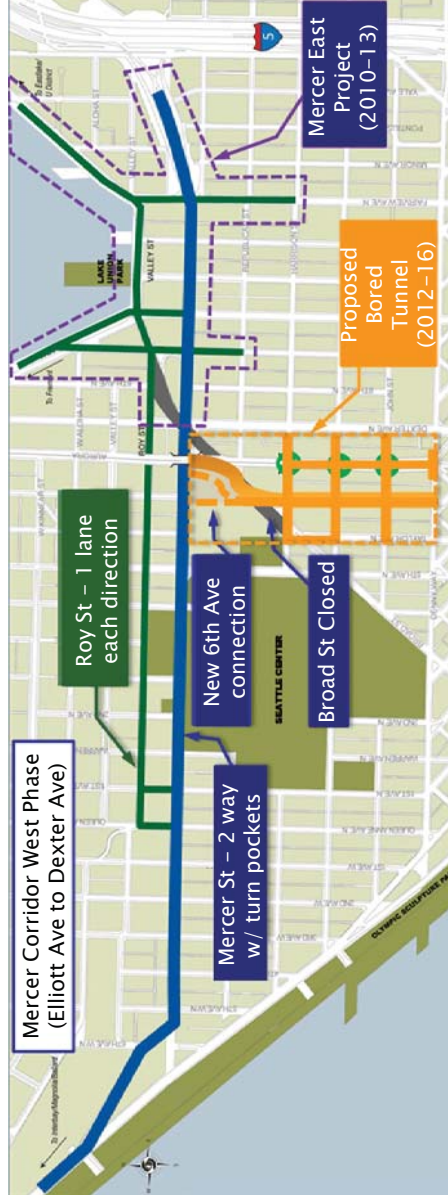
The site is within the district covered by the Uptown Urban Center. The site is illustrated in the images to the left.

This site is not specifically addressed in a particular Neighborhood plan, but flanks the border of multiple proposed development plans. Across Broad St to the northwest lies the border of the 'Century 21 Seattle Center Master Plan'. This plan focuses heavily on reclaiming and unifying open space at the heart of the campus, and making a connection with buildings on the periphery as well as the growing neighborhoods on the Center's edges.

Broad St also serves as a portion of the western border of the Seattle 2030 Challenge District with the site acting as one of the binding properties. The 2030 Challenge district's goal is to develop a public-private effort to promote the ideals of the 2030 Challenge. These ideals include designing all new buildings, developments and major renovations to meet a fossil fuel energy consumption performance standard of 60% below the regional (or country) average for that building type. This goal reduction would be increased to 70% in 2015, 80% in 2020, 90% in 2025, and finally Carbon-neutrality in 2030.

Additionally, this site is located at the edge of the Mercer Corridor West Phase. Broad St will be closed one block northeast of the site directly affecting the traffic pattern of the Broad St / 5th Ave N corridor.

SDOT has an undeveloped plan for an alteration to the traffic pattern along Broad St between John St and 5th Ave N. Access to the site may be limited to a three lanes of traffic (one NE and two SE), two lanes of traffic (two SE bound lanes only) or the street may be permanently closed for this length of Broad St. Access north from alley may be removed as well which will affect Loading and Unloading traffic patterns within the alley.



Mercer Corridor West Phase Redevelopment





VICINITY MAP

- 1. Seattle Monorail
- 2. Seattle Space Needle
- 3. Experience Music Project
- 4. Ride the Ducks
- 5. Fischer Plaza
- 6. McDonald's
- 7. Fun House
(To be demolished – proposed Multi-Family Residential)
- 8. Cafe Aficionado



Seattle Monorail



Seattle Monorail



Seattle Space Needle



Experience Music Project



Ride the Ducks



Fischer Plaza



McDonald's

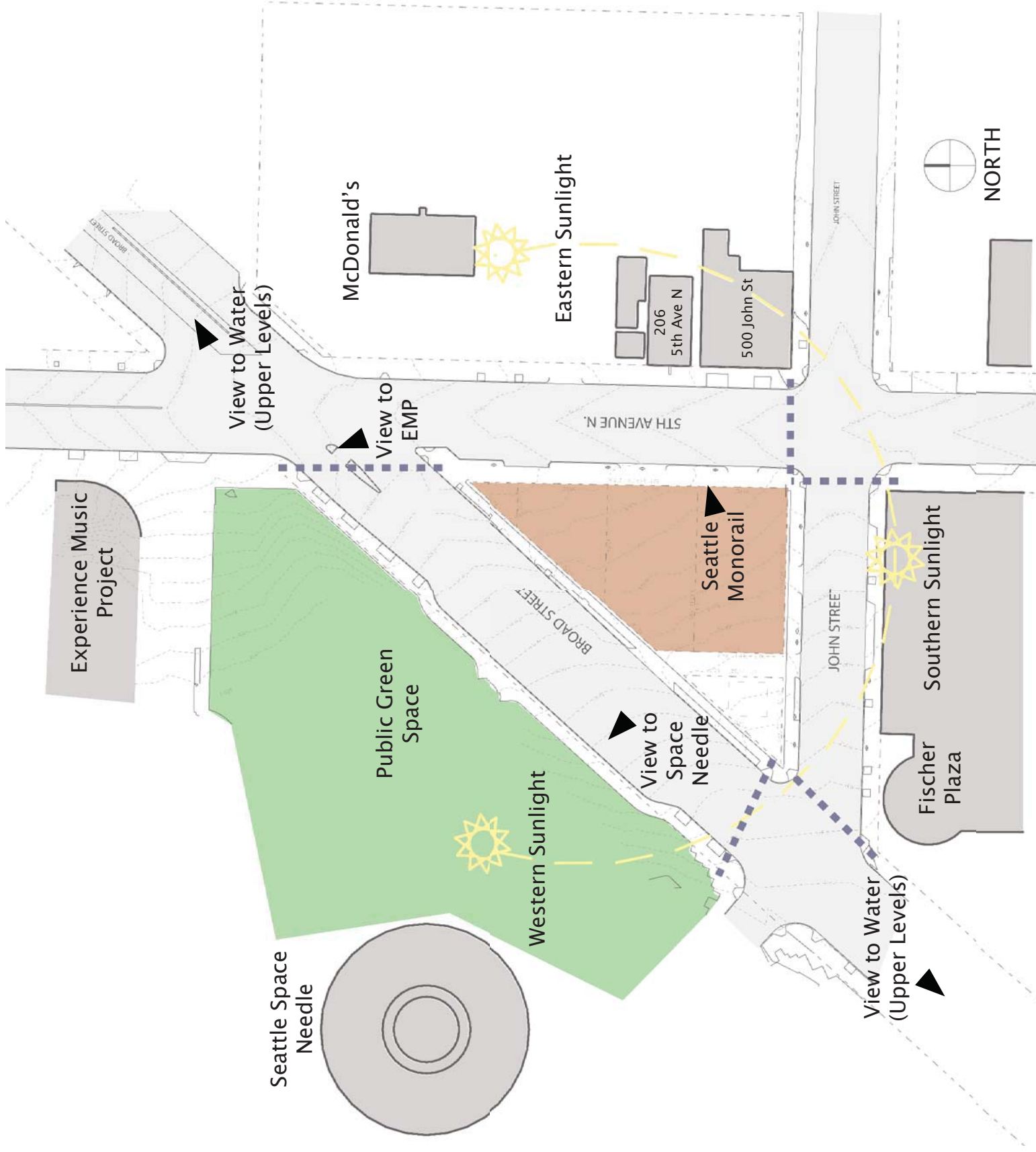


Fun House
(To be demolished)



Cafe Aficionado

JOHNSON BRAUND INC.
 15200 55th Ave. South
 Suite 300
 Seattle, WA 98188
 Phone 206.766.8300
www.johnsonbraund.com



View to Water
(East on Broad St)



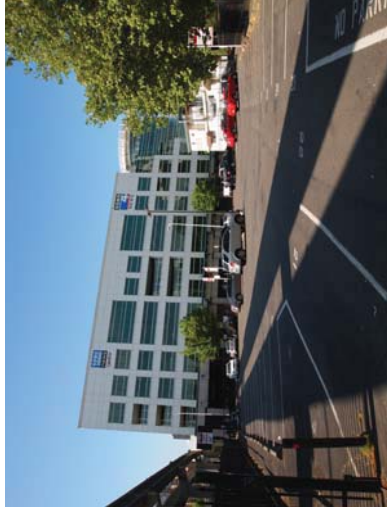
View to EMP



View to Space Needle



View to Monorail



View to Fischer Plaza



View to Water
(West on Broad)

JOHNSON BRAUND INC.

15200 5th Ave. South
Suite 300
Seattle, WA 98188
Phone 206.766.8300

www.johnsonbraund.com



LOOKING EAST (ACROSS 5TH AVE N) FROM SITE



LOOKING NORTH (ACROSS JOHN St) AT SITE



LOOKING SOUTH (ACROSS BROAD St) AT SITE



LOOKING WEST (ACROSS 5TH AVE N) AT SITE

Architecture

The Broad St corridor is predominantly commercial. Many lots along Broad St are irregularly shaped due to the angle of Broad St crossing the city's orthogonal street grid. Often, the irregular lots are underdeveloped, with small retail uses and at grade parking. Examples include the adjacent Café Aficionado, the 'Ride the Ducks' loading lot at the corner of 5th Ave N and Broad St, and the gas station down Broad St to the southwest. A number of mixed use buildings have been built recently along Broad St and 5th Ave N, with large plate uses such as Fischer Plaza, and the Gates Foundation. To the northwest is Seattle Center. The Architectural vernacular ranges from Google (the Seattle Space Needle), to Gothic-esque (the Pacific Science Center) and Experimental (The Experience Music Project). The Architecture immediately to

the east is small in scale and often times limited to one story construction, however further to the east are a number of large scale mixed-use buildings ranging in use from office buildings to hotel and apartment complexes. 5th Avenue North leads to a node of retail and commercial uses roughly three blocks to the north. This area includes the Gates Foundation, a new mixed-use apartment complex with a grocery store at the ground floor, McCaw Hall, the Pacific Ballet and the KCTS television hub. The mix of land uses also includes several open public spaces. Memorial Field is located a block north along 5th Ave N and the Seattle Center public space is located directly adjacent to Memorial field to the west.

Landmarks

Landmarks include, as mentioned earlier, the distinct forms of the Seattle Space Needle, the Experience Music Project, the Gates Foundation, the Pacific Science Center and Fischer Plaza.

Topography / Views

Because of the slope of Broad St the site is well situated for views towards downtown, the Puget Sound, the Olympic Peninsula and South Lake Union.

1. LOOKING EAST
(ACROSS 5TH AVE N) FROM SITE



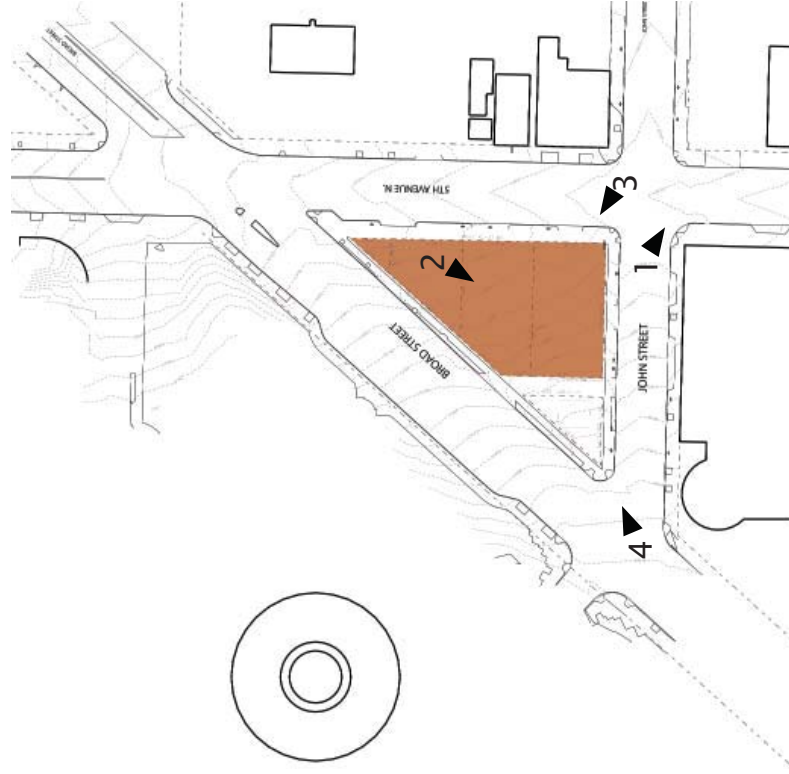
2. LOOKING SOUTH
(ACROSS SITE) AT FISCHER PLAZA



3. LOOKING WEST
(ACROSS 5TH AVE N) AT SITE



4. LOOKING EAST
(CORNER OF JOHN ST & BROAD ST) FROM SITE



Uses

The site is located at the southwest corner of Broad Street and 5th Avenue North. The adjacent west lot is occupied by Café Aficionado/Young's Accounting. This is a wood framed building constructed in 1906. East across 5th Ave N is a brick McDonald's built in 1980, a wood building (the 'Fun House') built in 1947, and a masonry building (L&P Partners) built in 1960. South across John St is Fischer Plaza; a reinforced concrete building constructed in 2001. Northwest across Broad St is the Seattle Space Needle; a reinforced concrete building constructed in 1961.

Topography

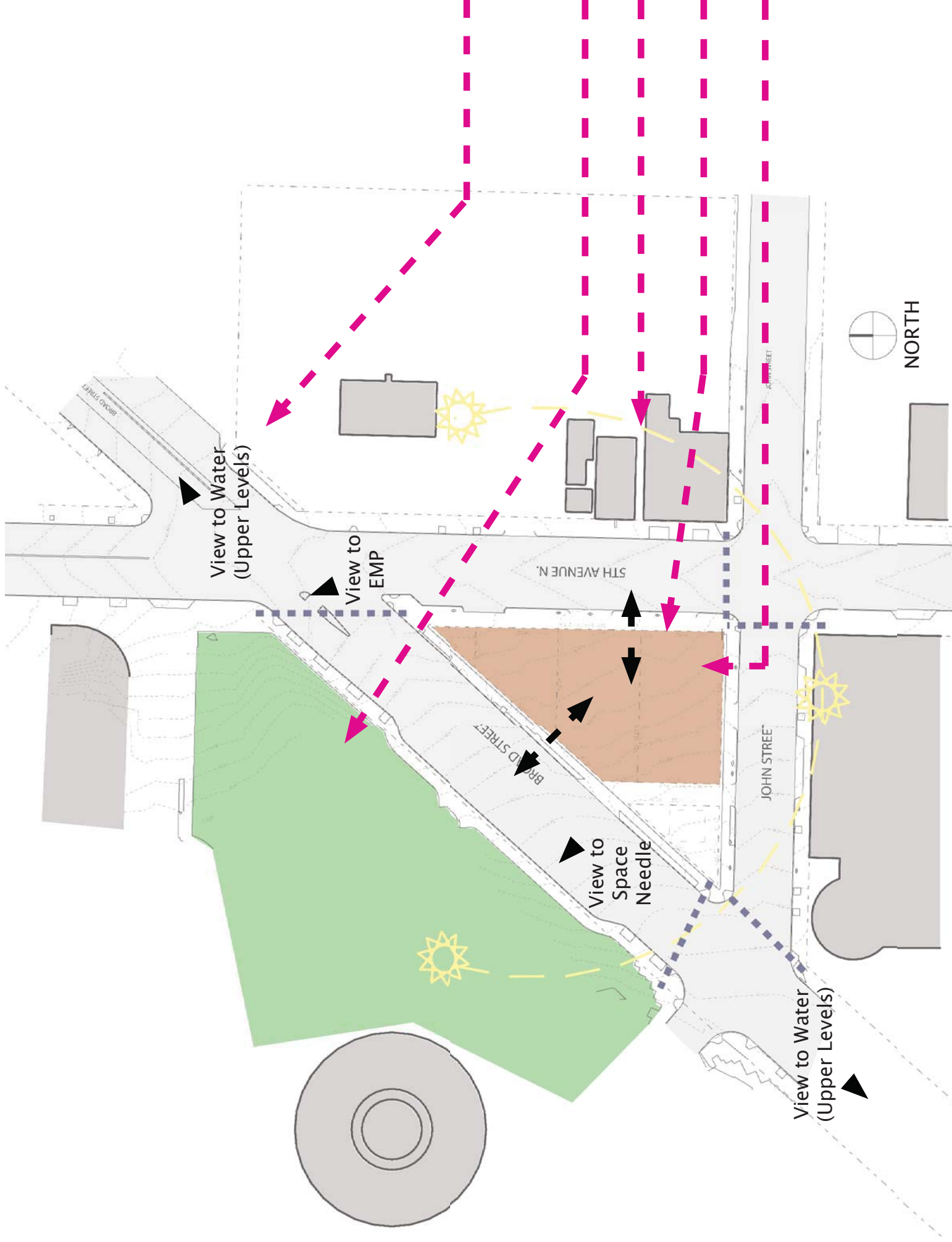
The site rises approximately 8 feet toward the west along Broad St. The top of the site is at the corner of John St and Broad St. The site rises about 4 feet along 5th Ave (going south) and 4 feet along John St (going west).

Access

Streets bind the site on three sides. Broad Street, to the north, is classified as a Principal Arterial. Broad cuts diagonally east separating Belltown from Seattle Center and connects the downtown waterfront to South Lake Union. City to vacate Broad St east of 6th Ave N.

5th Ave N runs along the east property line, becoming a Minor Arterial connecting Belltown/Downtown to Seattle Center.

John St runs along the south property line. This is the least travelled street of the site and acts as the 'back door' to Fischer Plaza populated with garage entries and back of house services.



SITE ANALYSIS SUMMARY

Broad Street Slope and Traffic

- Presents challenges to vehicular entry at street level
- Less walkable than 5th Ave N (approx. 4.5% slope)
- Creates focal point and plinth at corner
- Opportunity for public pedestrian foot traffic at Bar / Restaurant
- Major arterial – to be permanently converted to unidirectional
- High visibility – good opportunity for public entry

5th Ave N Slope and Traffic

- More walkable than Broad St (approx. 1.5% slope)
- Great opportunity for pedestrian entry / hotel taxi drop-off / loading / unloading
- Opportunity for public taxi drop-off / loading / unloading
- Minor arterial – potential of increased traffic due to Broad St closure
- Less visibility than Broad St at grade / 2nd floor due to Monorail

John St Slope and Traffic

- More walkable than Broad St/ Less walkable than 5th Ave N (approx. 3.5% slope)
- Considered 'Back of House'
- Smallest amount of pedestrian/vehicular traffic
- Least visible facade

Views

- Excellent views to west and east (upper floors) – water.
- Excellent views to northwest (Broad St facade) – Space Needle.
- Due west blocked at Main Floor loading dock by existing 2 story building.
- View to east (McDonald's/Fun House) unattractive at main floor.

Amenities

- Green Space northwest across Broad St

Solar Access

- Both the East and West facades have good solar orientation

Building Mass

- Desire to address both Broad St and 5th Ave N

Garage Access

- Broad St entry is inappropriate
- John St entry is best for potential Valet service
- 5th Ave N entry is inappropriate

DESIGN CONSTRAINTS / CHALLENGES

Future Mercer realignment – Mercer West Project

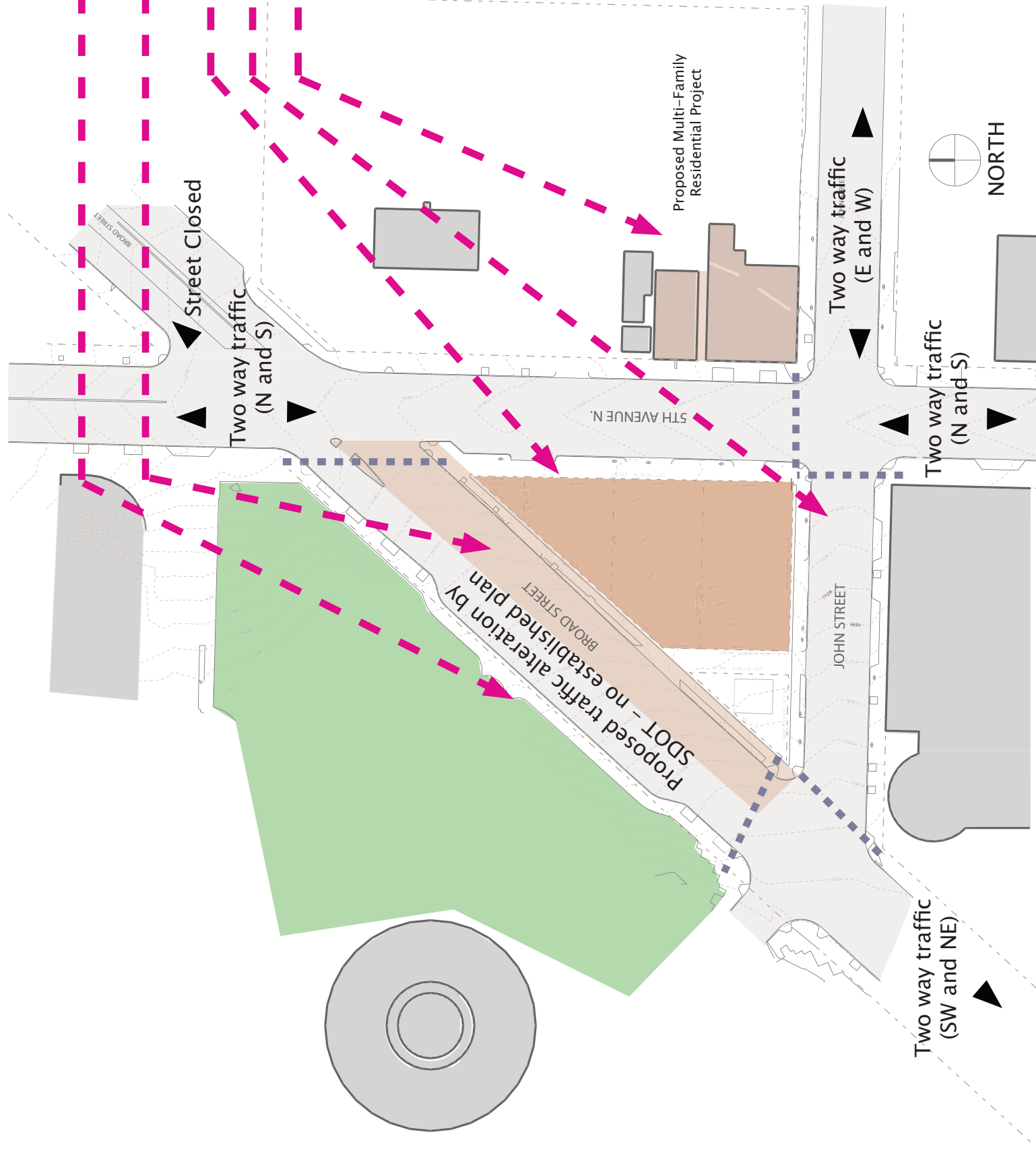
• Vehicular drop-off difficult along Broad St at street level if proposed SDOT design includes unidirectional traffic pattern or full street closure at the project location

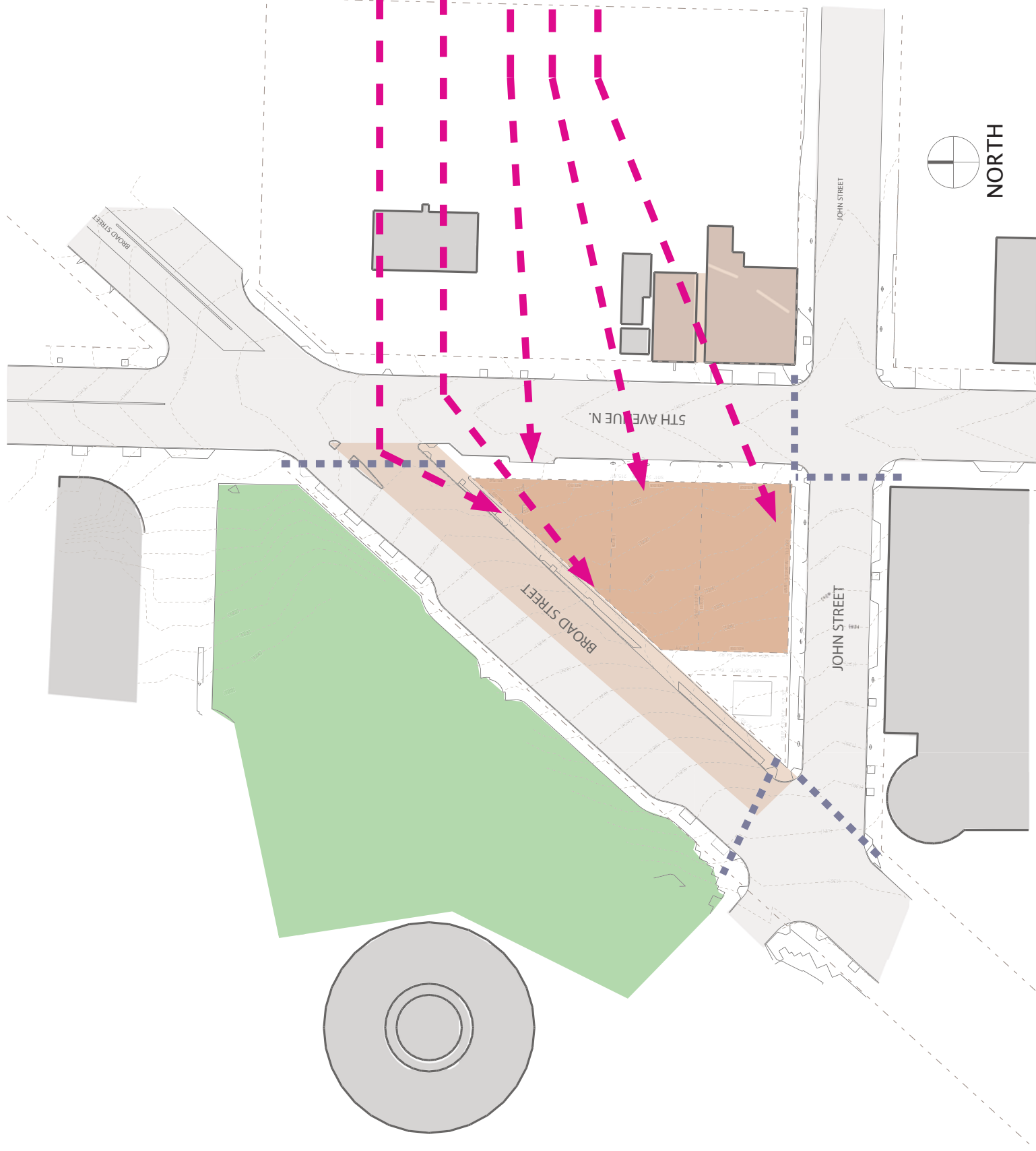
• Decreased visibility of entry from vehicles along Broad if proposed SDOT design includes unidirectional traffic pattern or full street closure at the project location

• Increased Vehicular Traffic along 5th Ave N

• Increased Vehicular Traffic along John St

• Increased Vehicular Traffic due to proposed Residential Development





DESIGN CONSTRAINTS / CHALLENGES

Future Mercer realignment – Mercer West Project

- Vehicular drop-off difficult along Broad St at street level if proposed SDOT design includes unidirectional traffic pattern or full street closure at the project location
- Decreased visibility of entry from vehicles along Broad if proposed SDOT design includes unidirectional traffic pattern or full street closure at the project location
- Increased Vehicular Traffic along 5th Ave N
- Increased Vehicular Traffic along John St
- Increased Vehicular Traffic due to proposed Residential Development

RESPONSES / SOLUTIONS

- Proposed Restaurant with outdoor seating and Public entrance located at corner of Broad St and 5th Ave N
- Proposed Hotel ‘Secondary’ Entrance along Broad St
- Proposed drop-off entry along 5th Ave N
- Proposed Hotel Main Entrance along 5th Ave N
- Proposed tandem entry and exit for vehicles at Valet station along John St

UPTOWN NEIGHBORHOOD DESIGN GUIDELINES

- Context and Site**
 CS1. Natural Systems and Site Features
 CS2. Urban Pattern and Form
 Respond To Site Characteristics (former A-1)
 Streetscape Compatibility (former A-2)
 Corner Lots (former A-10)
 Height, Bulk, and Scale Compatibility (former B-1)
 CS3. Architectural Context and Character
 Architectural Context (former C-1)
- Public Life**
 PL1. Public Space
 Streetscape Compatibility (former A-2)
 Landscaping to Reinforce Design Continuity with
 Adjacent Sites (former E-1)
- PL2. Walkability**
 Pedestrian Open Spaces and Entrances (former D-1)
 Street-level Interaction
 Entrances Visible from the Street (former A-3)
 Human Activity (former A-4)
 Transition Between Residence and Street (former A-6)
- PL4. Active Transportation**

DESIGN GUIDELINES FOR MULTIFAMILY AND COMMERCIAL BUILDINGS

- A. Site planning**
 A-1 Responding to site characteristics
 A-2 Streetscape compatibility
 A-3 Entrances visible to the street
 A-4 Human activity
 A-5 Respect for adjacent sites
 A-6 Transition between residence and street
 A-7 Residential open space
 A-8 Parking and vehicle access
 A-9 Location of parking on commercial street fronts
 A-10 Corner lots
- B. Height, Bulk Scale**
 B-1 Height, Bulk, and Scale compatibility
- C. Architectural elements and materials**
 C-1 Architectural context
 C-2 Architectural concept and consistency
 C-3 Human Scale
 C-4 Exterior finish material
 C-5 Structured parking entrances
- D. Pedestrian environment**
 D-1 Pedestrian open spaces and entrances
 D-2 Blank walls
 D-3 Retaining walls
 D-4 Design of parking lots near sidewalks
 D-5 Visual impacts of parking structure
 D-6 Screening of dumpsters, utilities, and service areas
 D-7 Personal safety and security
 D-8 Treatment of alleys
 D-9 Commercial signage
 D-10 Commercial lighting
 D-11 Commercial transparency
 D-12 Residential entries and transitions

- E. Landscaping**
 E-1 Landscaping to reinforce design continuity with
 adjacent sites
 E-2 Landscaping to enhance the building and/or site
 E-3 Landscape design to address special site conditions

Note: Intended points of focus highlighted in orange

KEY PROJECT ELEMENTS – RESPONSE TO DESIGN GUIDELINES

- Full lot development from property line to property line with building height and scale to match adjacent Fischer Plaza – **CS2, CS3, DC2, and B1**
- Proposed outdoor seating and planting strip on Broad St at grade– **PL1, PL3, DC3, A2, A3, A4, C3, D1, E1 and E2**
- Hotel Entrance at Broad St and 5th Ave N with additional Public Entrance along Broad St at proposed Restaurant feature – **PL2, PL3, A2, A3, C3, and D1**
- Proposed Parking and Vehicle access on least walkable street – **DC1 and A8**
- Use of contextually appropriate material and lighting that respond to the colors and styles of the Space Needle and the EMP – **DC4, A1, A2, C4, D10**
- Proposed roof top open space – **DC3, D1, E1, and E2**
- Proposed 2' alley development easement to allow for increase in alley width at main floor – **D8**



UPTOWN NEIGHBORHOOD DESIGN GUIDELINE – A-4 HUMAN ACTIVITY

Board Comment / Guidance:

'The Board observed that the proposal is in a highly pedestrian area which is expected to get even more heavily pedestrian in the future. Broad St will get more highly pedestrian and 5th Ave N past the site [to the East] is a fairly significant bike route.

The Board asked for some response to the new, wide sidewalk, green landscaped Broad St planned for the post viaduct era. There could be a softening with landscape or street furniture. Opportunity for building modulation is very limited. A workable approach might be the facade / right of way seam where landscaping could be incorporated and a building entry expressed.

The Board observed that when looking at the site from a broad, external perspective it is most observable from Seattle Center. From Fifth Ave it is hidden by the Monorail. From the south the site is hidden by the Fischer building. From the north the site narrows to a point. The Board indicated they were interested in the building's response to Broad St. They called for an expression looking back at Seattle Center; possibly over the entry.

The Board observed that the courtyard location could have many forms. It should respond to site conditions, namely buildings on two sides and an 'amazing open space' at Seattle Center on another. It asked, what are the options for the courtyard? What direction should it face?

KEY FOCUS OF GUIDANCE:

1. Site located in a highly pedestrian area – the project should respond to this and anticipate the increase of pedestrian traffic
2. Adjacent street width to be reduced and sidewalk width increased allowing for a softening of the streetscape through landscaping or street furniture
3. The project should respond to the adjacent green space across Broad St at Seattle Center
4. The project facade should explore the relationship between the Broad St facade and the adjacent Seattle Center

UPTOWN NEIGHBORHOOD DESIGN GUIDELINE – A-9 LOCATION OF PARKING ON COMMERCIAL STREET FRONTS

Board Comment / Guidance:

'When considering the best place for vehicular entry, the Board encouraged the applicants to consider the alley. If an internal auto court with street access is to be created the Board indicated the street to use for access would be John St with one possible configuration having an entry from John St and an exit to the alley.

The Board indicated if a curb side drop off area is to be incorporated, Broad St should be considered as a strong candidate because 5th Ave N is complicated by the presence of the overhead Monorail with support pillars in the right of way along the proposed site. John St would be another possibility for curb side drop off. The Board asked that this issue be studied and a resolution shown at the next Design Review meeting.'

KEY FOCUS OF GUIDANCE:

1. The project should consider the alley for vehicular entrance. If this is not possible then access should be from John St with a possible exit at the alley
2. If a curb site drop-off is incorporated Broad St should be considered. John St would be another consideration with 5th Ave N being the least desirable from the Board's standpoint

UPTOWN NEIGHBORHOOD DESIGN GUIDELINE – C-4 EXTERIOR FINISH MATERIALS

Board Comment / Guidance:

'The Board encourages the applicants to be bold in their overall design. A transportation influenced theme, relating to the Monorail, would be appropriate. A floating mass over a light base might be good. The Board called for futuristic looking elements.'

KEY FOCUS OF GUIDANCE:

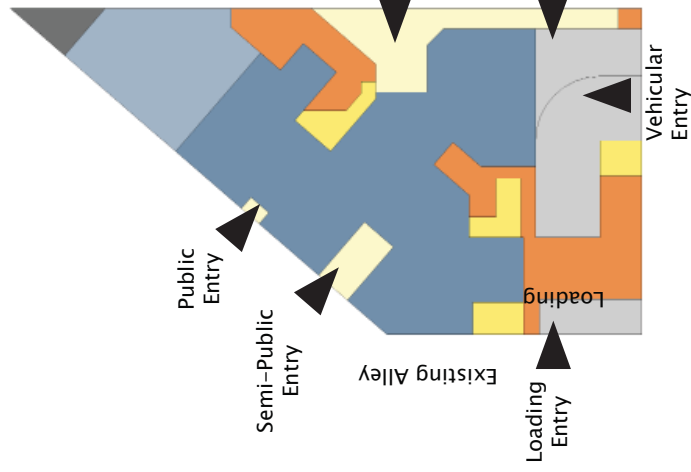
1. The project should explore the possibility of a light / transparent base with a larger mass above
2. Materials should be 'futuristic'



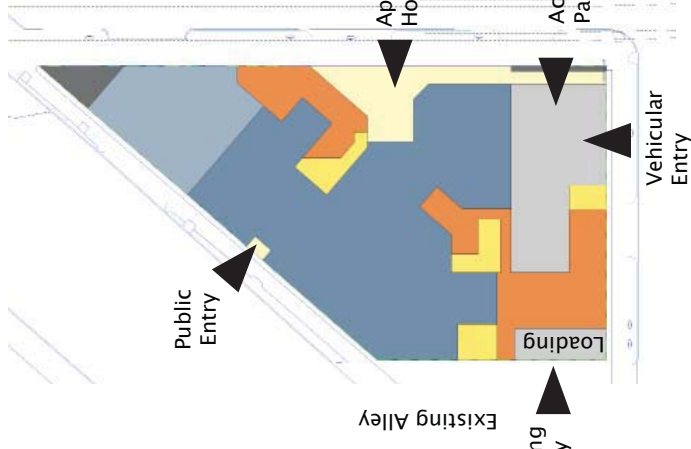
JOHNSON BRAUND INC.

15200 55th Ave. South
Burien, WA 98148
Seattle, WA 98188
Phone 206.766.8300

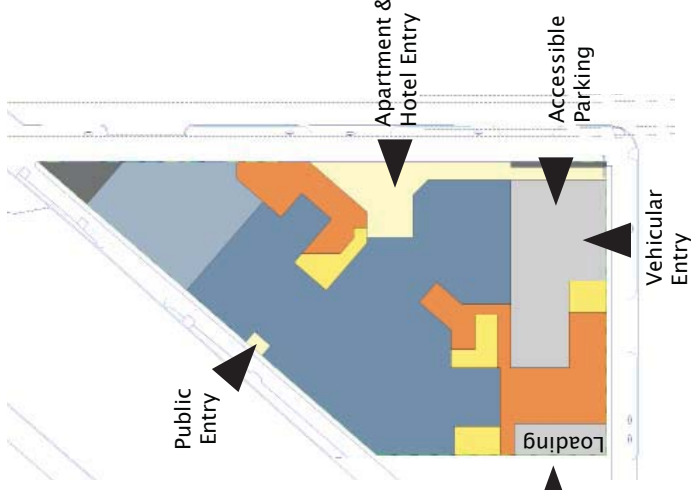
www.johnsonbraund.com



Option 1 Main Floor Plan (Preferred)



Option 2 Main Floor Plan



Option 3 Main Floor Plan

Option 1
 Hotel space is located throughout most of the site with the corner of Broad St and 5th Ave N dedicated to a proposed semi-public dining space. Access to the hotel is located along 5th Ave N with potential valet drop-off / Accessible Van Parking entry located adjacent to the property line at the corner of John St and 5th Ave N. Loading and Unloading would be accessed from the alley with the Loading berth located within the building footprint. Both Hotel and Residential uses will enter through the main lobby with designated key card access per floor for each use.

Advantages

- Public Amenity space at Main Floor
- Rooms have ample natural light
- Entry is protected from high-traffic street
- Creates a strong street edge along both Broad St and 5th Ave N
- Hotel / Apartment Amenity space at roof
- Ample Passenger Load/Unload space
- Stronger facade on 5th Ave N / More responsive facade on Broad St

Challenges

- Control of the public entry by the hotel staff
- Reduced floor space allotted to hotel use
- Loss of rentable space on 2nd floor directly above Loading berth

Option 2

Similar to Option 1 on the main floor with an opening at upper levels on John St

Advantages

- Public Amenity space at Main Floor
- Rooms have ample natural light
- Entry is protected from high-traffic street
- Creates a strong street edge along both Broad St and 5th Ave N
- Hotel / Apartment Amenity space at roof
- Ample Passenger Load/Unload space
- Stronger facade on Broad St / More responsive facade on 5th Ave N

Challenges

- Control of the public entry by the hotel staff
- Reduced floor space allotted to hotel use
- Loss of rentable space on 2nd floor directly above Loading berth

Option 3

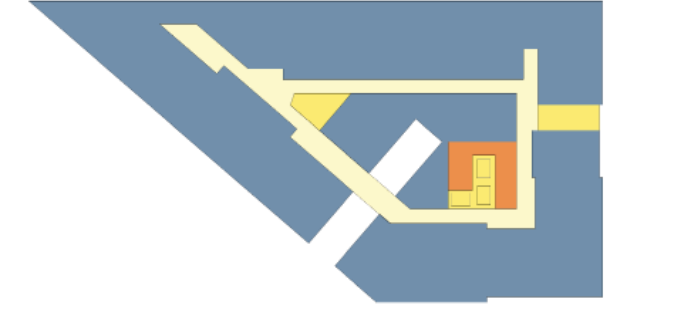
Similar to Option 2 with a bridge connection along 5th Ave N.

Advantages

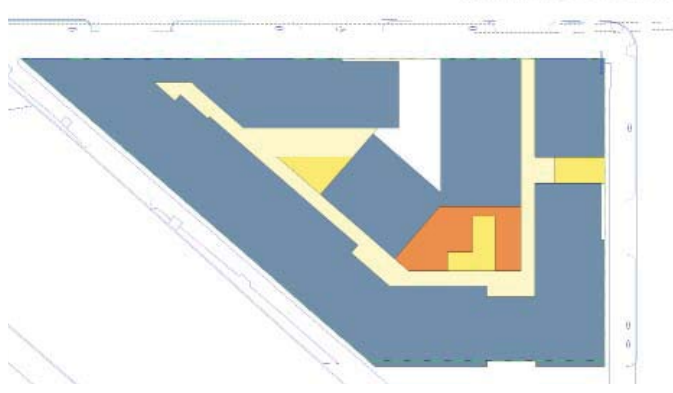
- Public Amenity space at Main Floor
- Rooms along the perimeter have ample natural light
- Entry is protected from high-traffic street
- Creates a strong street edge along both Broad St and 5th Ave N
- Hotel / Apartment Amenity space at roof

Challenges

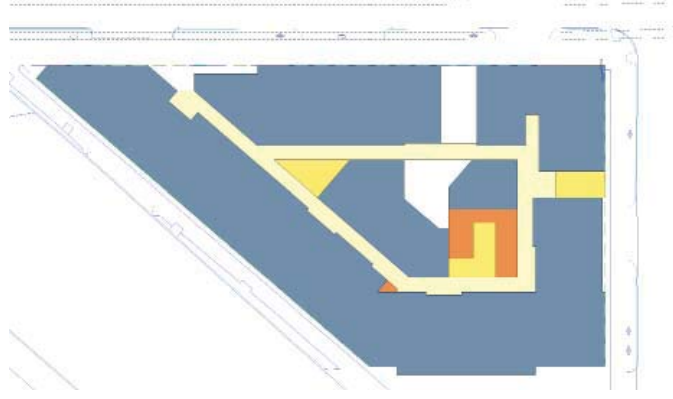
- Control of the public entry by the hotel staff
- Reduced floor space allotted to hotel use
- Loss of rentable space on 2nd floor directly above Loading berth



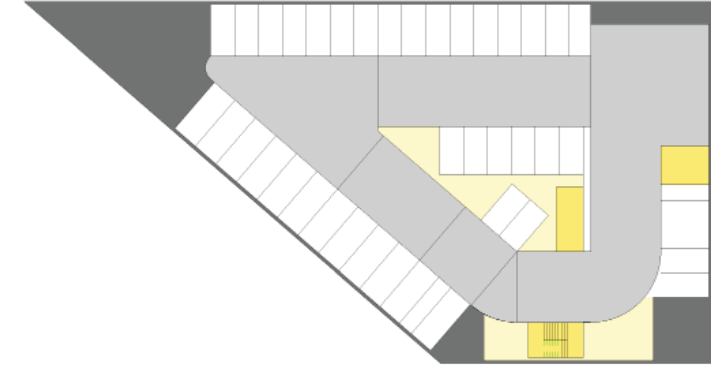
Option 1 Typical Upper Floor Plan (Preferred)



Option 2 Typical Upper Floor Plan



Option 3 Typical Upper Floor Plan



Option 1 & 3 Parking Level 1 (Preferred)

Objectives

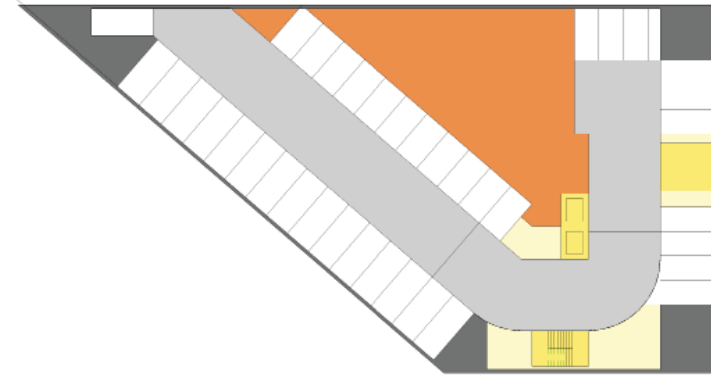
The project's objective is to construct a well-designed building that creates excellent urban short term and extended stay housings; contributing to the character of its surroundings by:

- Adding to the activity and character of the Broad St / 5th Ave N corridor
- Holding the corner of Broad St and 5th Ave N; acting as an anchor for the southeast portion of Seattle City Center
- Creating attractive residential units that provide easy access to transit
- Making light-filled units that create great spaces for urban living

Desired Uses:

Approximate Structure Size:

The zoning allows a 85-foot building height with the highest occupiable floor at 75 feet above grade for 'Low-Rise' development designation. The design intent is to use the full buildable height of 85 feet as defined by "Low-Rise" development for one story of hotel public space, four stories of

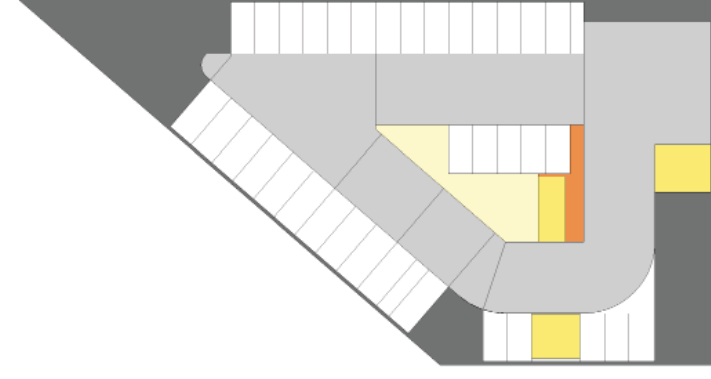


Option 1 & 3 Parking Level 2 (Preferred)

hotel use, two and a half residential stories and an occupiable roof deck. The hotel portion will meet the maximum FAR of 4.5, as required by 23-48.016B.

Hotel:

This highly visible site is well suited to hotel use. The design intent is to use the geometry and the slope of the site to best advantage for an active pedestrian environment. Hotel use will wrap the corner along 5th Ave N and Broad St in the preferred scheme the intent is to activate the corner of the lot with a semi-public dining/bar space open to the hotel guests and the general public (approx. 2,140 sf). Floors two through five are intended to house approximately 26 units and maximize the floor plate (103 units proposed). Our proposal for vehicular and pedestrian entry has vehicular entry from John St, main pedestrian entry from 5th Ave, and secondary pedestrian entry from Broad St; seeing these as the most successful traffic options for the site. This maximizes the pedestrian visibility on 5th Ave N as well as Broad St while



Option 2 Parking Level 1

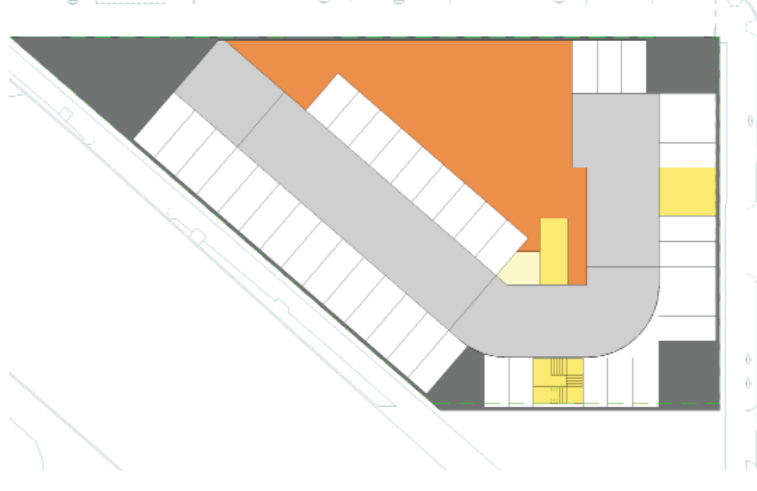
capitalizing on the low traffic flow along John St for vehicular entry and exit.

Residential:

Residential units are proposed for floors six, seven, and a portion of the roof (67 units proposed, 26 per floor plate). The design intent will be to have light-filled urban units with immediate access to the Seattle City Center, and a building massing that defines an edge of the Seattle City Center underlay district. The residential units would also be arranged in such a way that allows both Hotel and Resident use to a proposed roof top deck (11,450 sf allowable area, 2,100 sf proposed). Both Hotel and Residential uses will enter through the main lobby with designated key card access per floor for each use.

Access and Parking:

Our strong preference is to access the parking from John St Given the vehicular speed and traffic pattern of Broad St it is best used for public space or hotel services rather than for garage entry. The

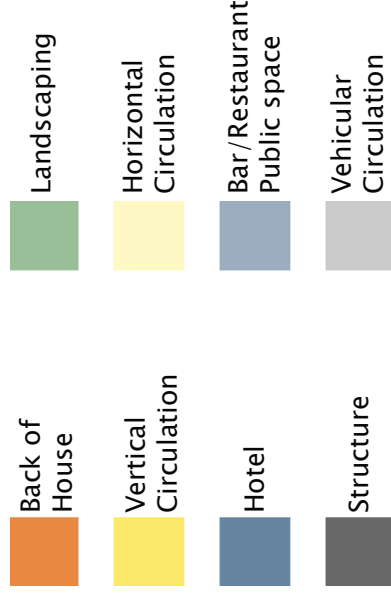


Option 2 Parking Level 2

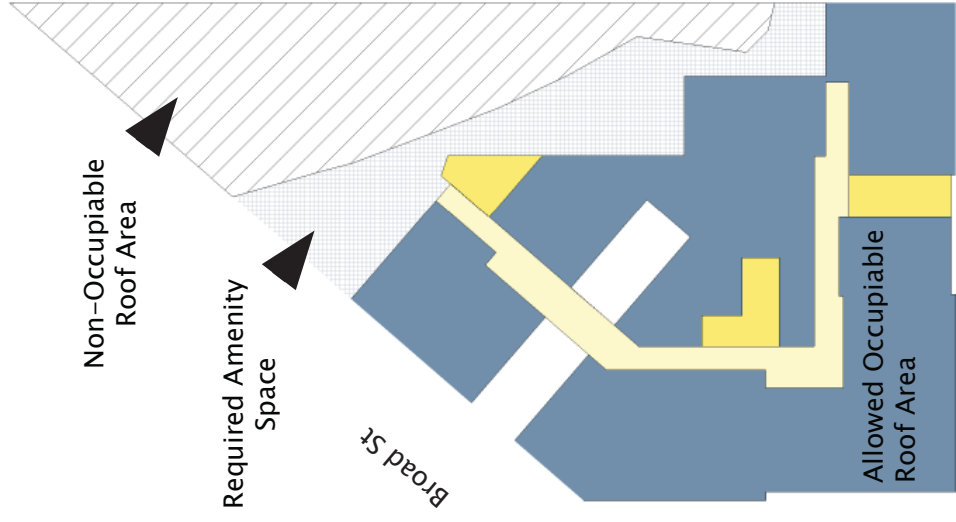
preferred parking in the diagram above indicates an entry on the southeast corner of the site, ramping at a 20% maximum slope with double loaded parking. Potential valet parking would be available to all hotel guests with a residential self-park option. One full level and at least an additional half level are needed to meet the current parking requirements (74 proposed, 74 required after transit reduction).

Potential Design Departures:

We intend to provide a 20% drive aisle slope. This would be a departure from the requirements of SMC 23.54.030.



Option 1
Roof Plan



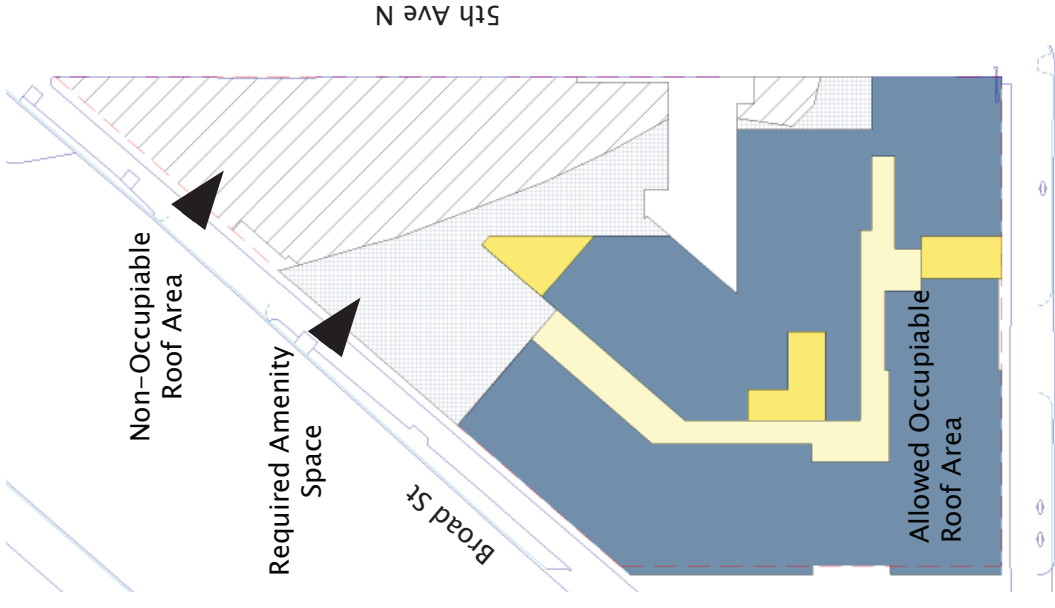
John St

Roof Top Development

Per SMC section 23.48.020 this project is required to provide Residential Amenity Space based on 5% of the floor area devoted to Residential use. The proposed square footage of each floor plate approximately 15,722 (31,445 sf total) with 5% of the square footage being 2,067 sf.

Based off of the 'Sliding Ladder' ruling established by the Seattle Fire Marshall and Seattle Department of Planning and Development occupiable floor area at the roof level is based off of a measurement of 75 feet above the adjacent

Option 2
Roof Plan



John St

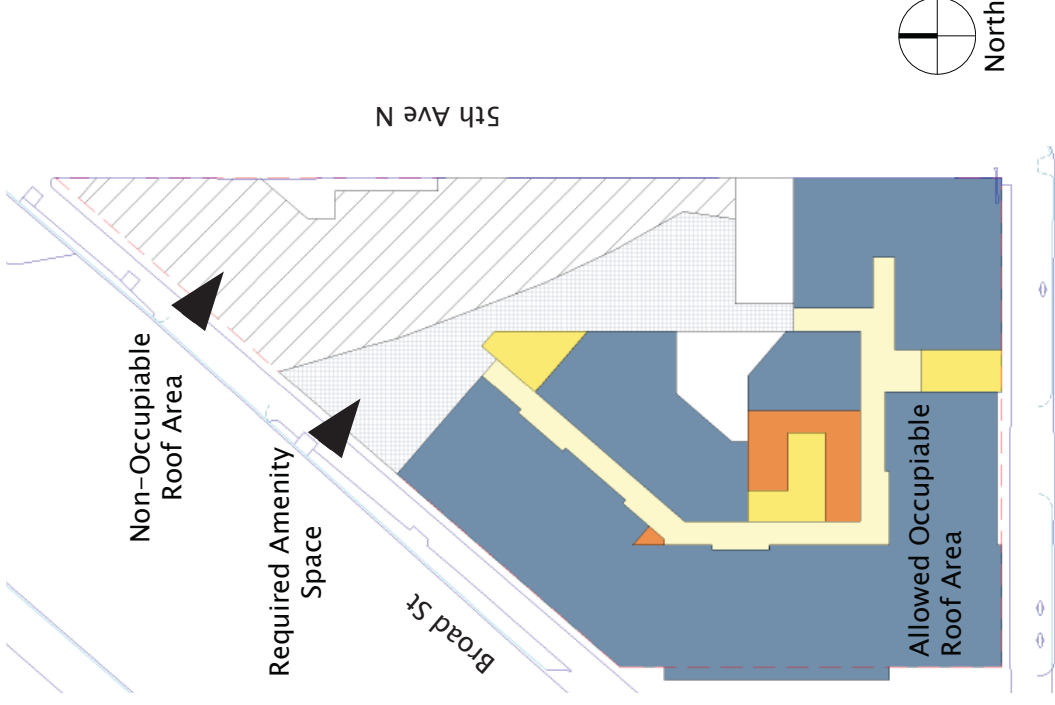
grade at all points of the site. The diagrams above represent the delineation of occupiable and non-occupiable areas based on the proposed massing options included in this submittal.

The approximate square footage of allowable occupiable roof area is 1,450 sf. It is our intention to meet or exceed the square footage requirements of the Residential Amenity Space and to make this portion of the roof accessible to both the Residential occupants and Hotel guests.

Building Massing

Double-loaded corridors look out at the perimeter

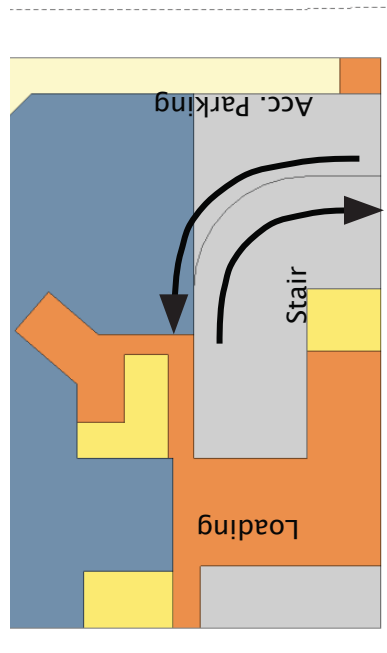
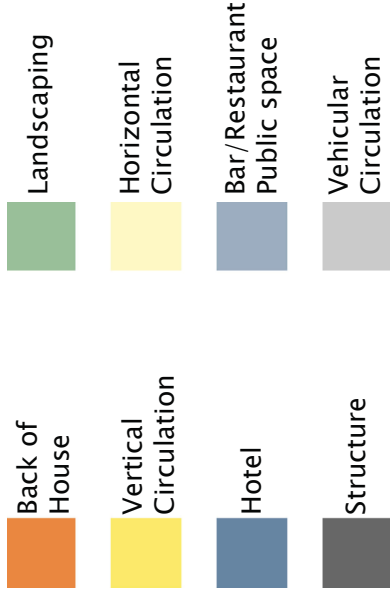
Option 3
Roof Plan



John St

for a large portion of the site. The angle of the site and the building mass provides frontage on Broad St, 5th Ave N and John St.

Units vary in size and number of rooms from Studios, to 1 Bedrooms, and Suites. In all three options views to the green space adjacent to the Space Needle are available to the units along Broad St Units located on upper floors have views of the Puget Sound, Olympic Peninsula, and South Lake Union. The unit at the corner of 5th Ave N and Broad St will have light from multiple directions and views down 5th Ave N to the north and Broad St to the east.



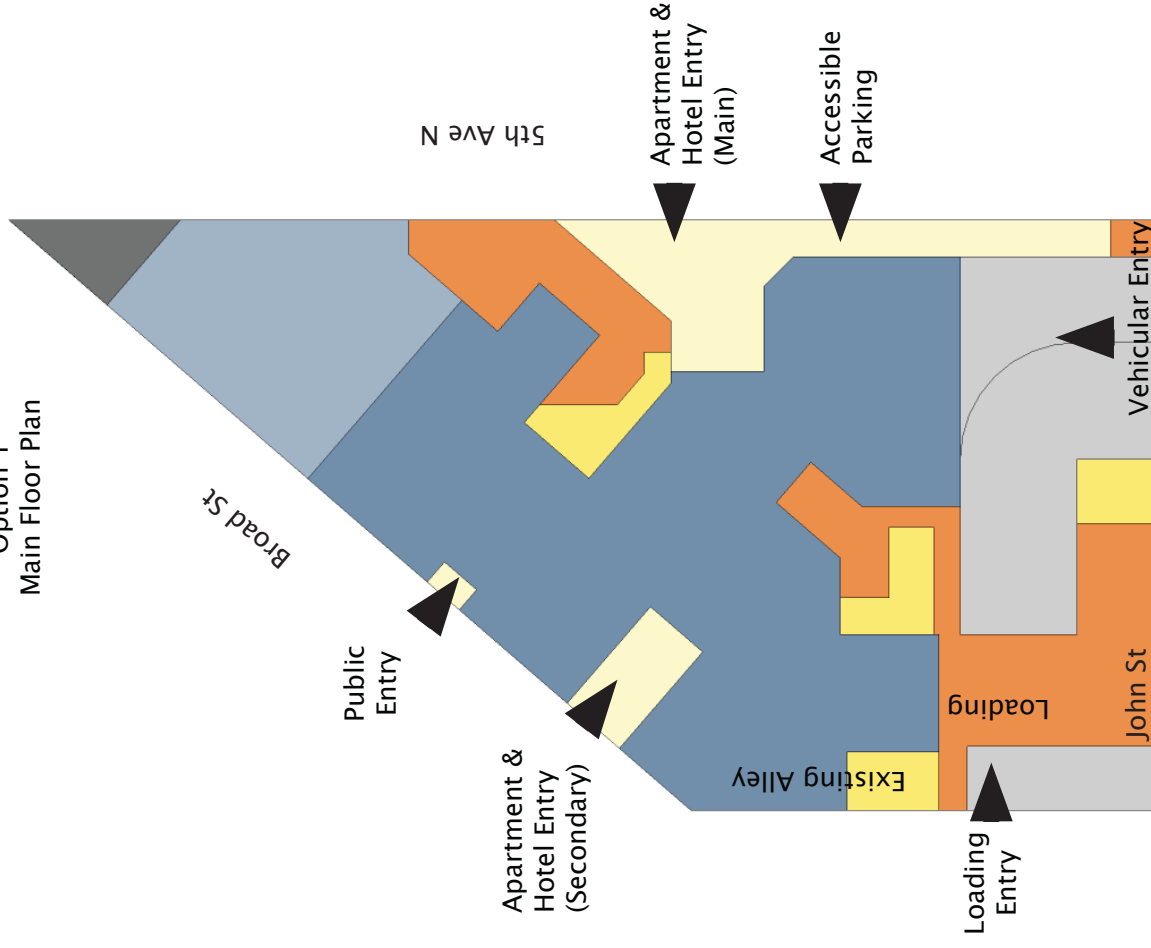
Entry Sequence
(Code Compliant and Preferred Option)

One enters the site off of John St through one lane. The entry aisle is used for potential valet drop-off, direct garage access by the Residential occupants, and access to the Accessible Van stall. Once a patron exits their vehicle they will access the main Hotel lobby by way of the ramp/stairs adjacent to the Accessible Van stall. Residential occupants will access their floors from the elevator bay in the garage.

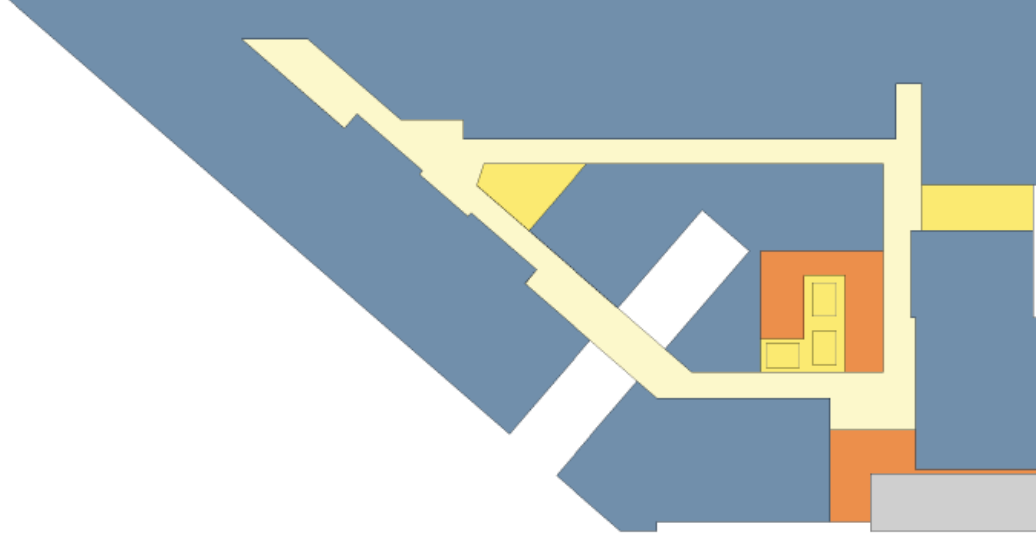
Accessible Van Stall users may exit out on to John St once completing a three-point turn / 'U-turn'. Residential occupants may exit directly on to John St from the garage. Hotel guests may exit on to John St upon receiving their vehicle at the Valet stand. This is the preferred option as exiting on to John St will cause the least amount of traffic congestion due to its minimal use as an arterial.



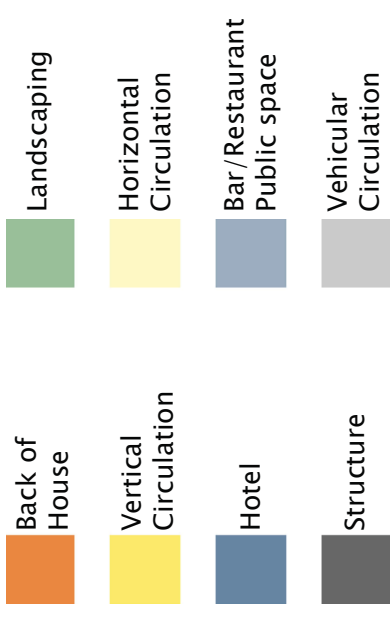
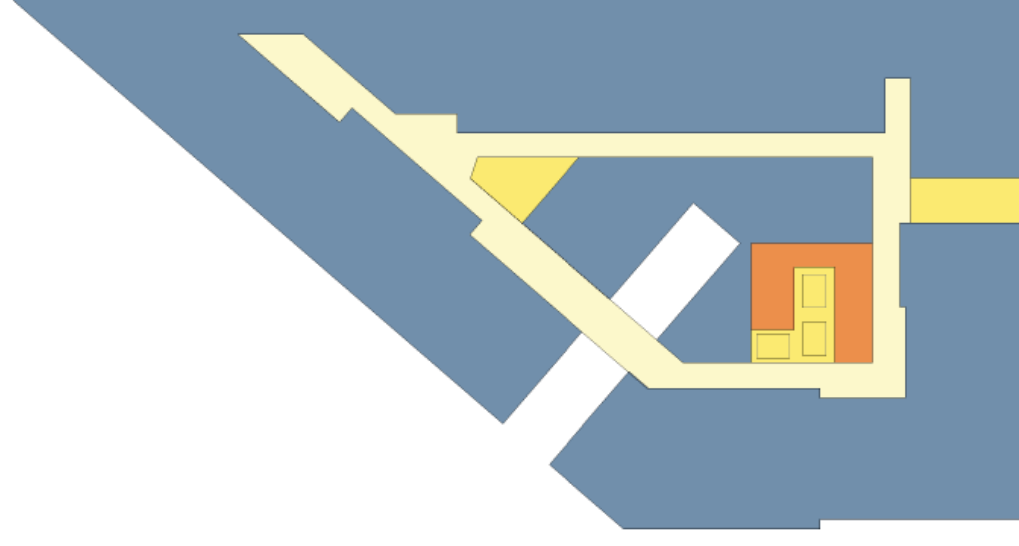
Option 1
Main Floor Plan



Option 1
Second Floor Plan



Option 1
Typical Upper Floor Plan



Description: Street Level

Hotel space is located throughout most of the site with the corner of Broad St and 5th Ave N dedicated to a dining / bar space. Pedestrian access to the hotel is located along 5th Ave N with curb-side drop-off. 'Secondary' Pedestrian entry is located along Broad St to respond to pedestrian traffic coming from Seattle Center. John St will act as the sole point of vehicular entry. Loading and Unloading will be accessed from the alley with the Loading berth located within building footprint.

Responses to Board Guidance:

Street Level

1. Anticipation of increased pedestrian traffic through proposal of additional entries along Broad St
2. Proposal of outdoor / sidewalk furniture to allow for softening of the streetscape
3. Entry and seating along Broad St to respond to adjacent open space at Seattle Center
4. Proposed recession of facade at Broad St with

glass bridge at upper levels to promote a visual connection to Seattle Center

5. Vehicular access to the garage and for Valet drop-off is located on John St
6. Proposal of a highly transparent main floor facade that allows for views in to and through the building

Option 1 Massing (View south from Broad St Facade)

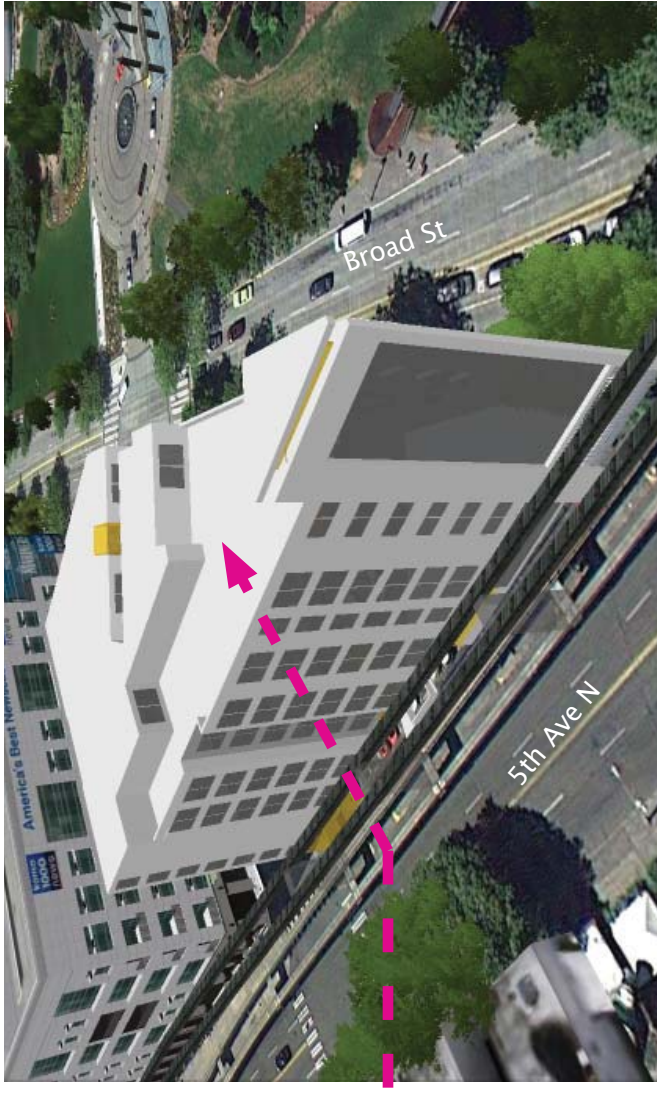


Fully Glazed Stairwell

Rooftop amenity space

Transparency along Main Floor Facade

Option 1 Massing (View southwest from corner of 5th Ave N & Broad St)



Option 1 Massing (View west from 5th Ave N Facade)



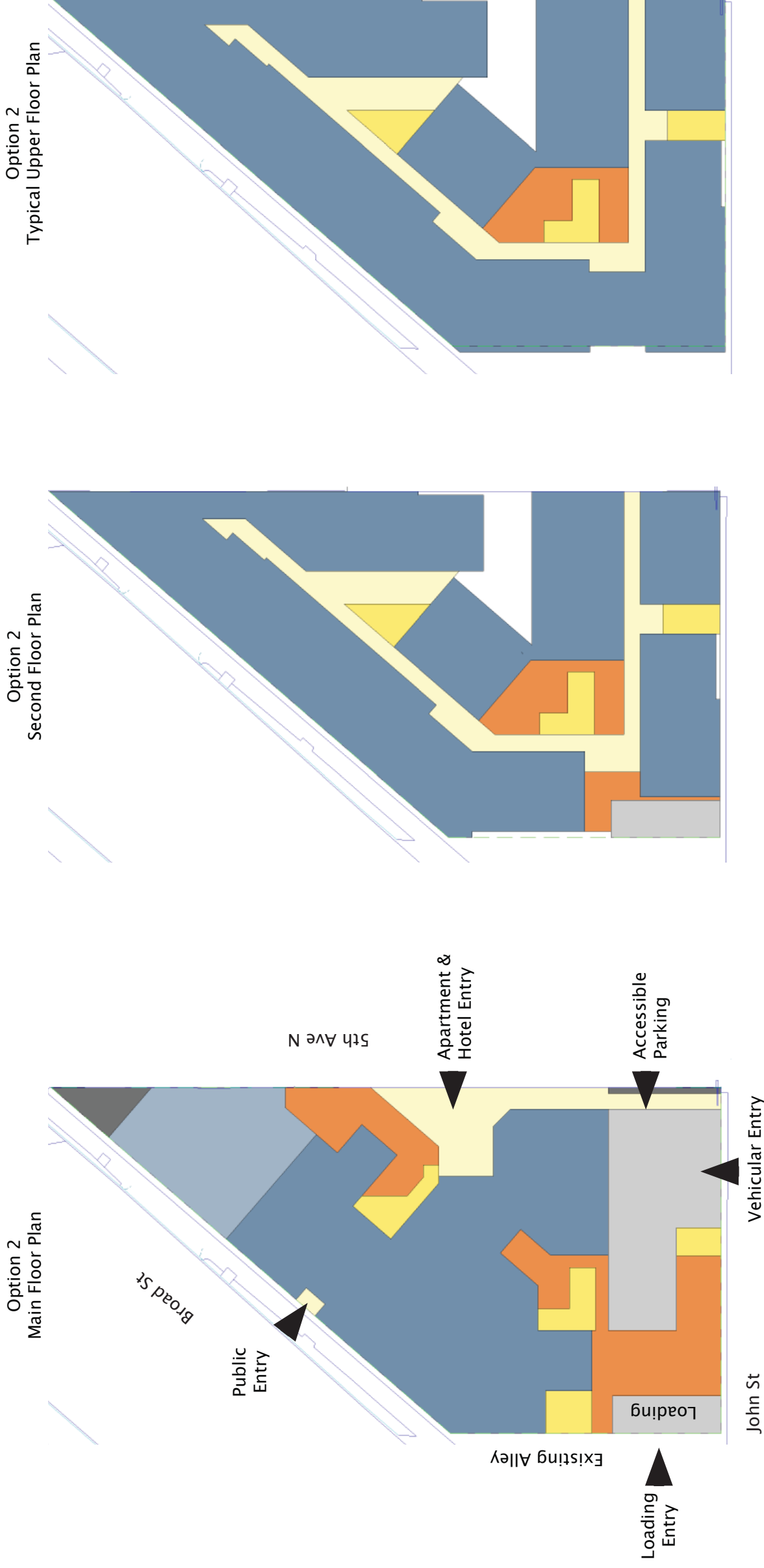
Responses to Board Guidance:

Building Massing

1. Proposed recession of facade at Broad St with glass bridge at upper levels to promote a visual connection to Seattle Center
2. Proposed Roof top amenity space to allow for views to the adjacent Seattle Center and create a visual connection between the building and the Space Needle

3. Proposal of a highly transparent main floor facade that allows for views in to and through the building

4. Proposed bold color scheme to respond to the adjacent Architectural elements of the Space Needle and the EMP



Description: Street Level

The street level for option 2 is similar to option 1 with the hotel 'Back of House' located along John St and the existing alley. This option has similar proposed public space. Pedestrian entry would be from 5th Ave N with Vehicular access from John St. Potential valet drop-off / Accessible Van parking entry would be accessed from John St. Loading and Unloading would be accessed from the alley with the Loading berth located within the footprint of the building.

Responses to Board Guidance:

Street Level

1. Anticipation of increased pedestrian traffic through proposal of additional public entry to restaurant space along Broad St
2. Proposal of outdoor / sidewalk furniture to allow for softening of the streetscape
3. Entry and seating along Broad St to respond to adjacent open space at Seattle Center

4. Vehicular access to the garage and for Valet drop-off is located on John St

5. Proposal of a highly transparent main floor facade that allows for views in to and through the building

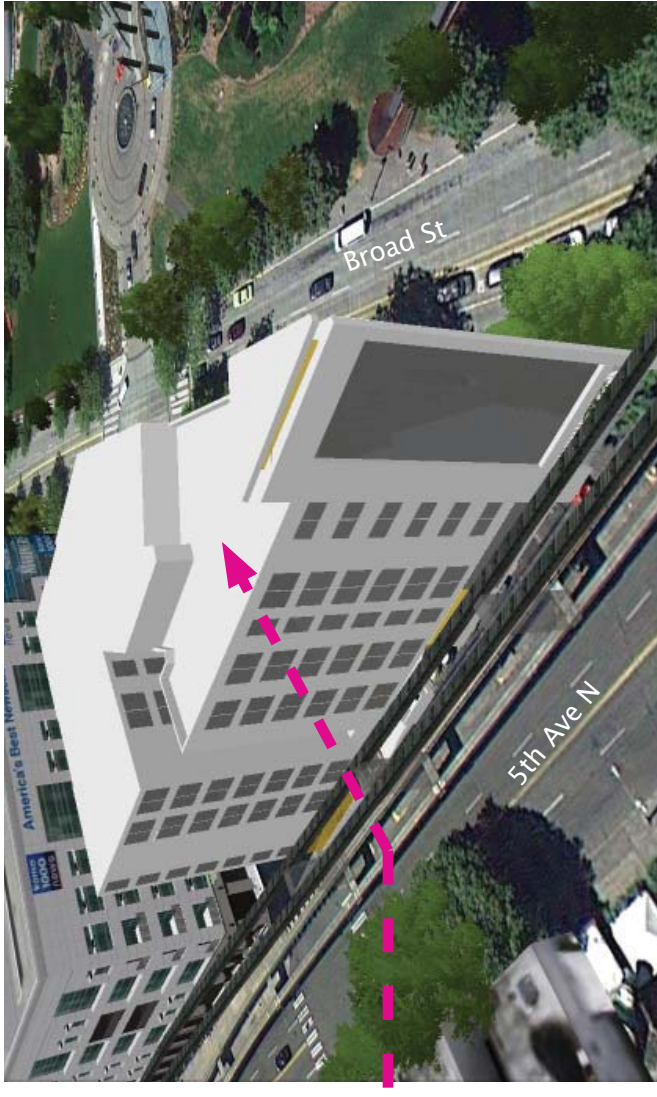
Option 2 Massing (View south from Broad St Facade)



Rooftop amenity space

Transparency along Main Floor Facade

Option 2 Massing (View southwest from corner of 5th Ave N & Broad St)



Option 2 Massing (View west from 5th Ave N Facade)

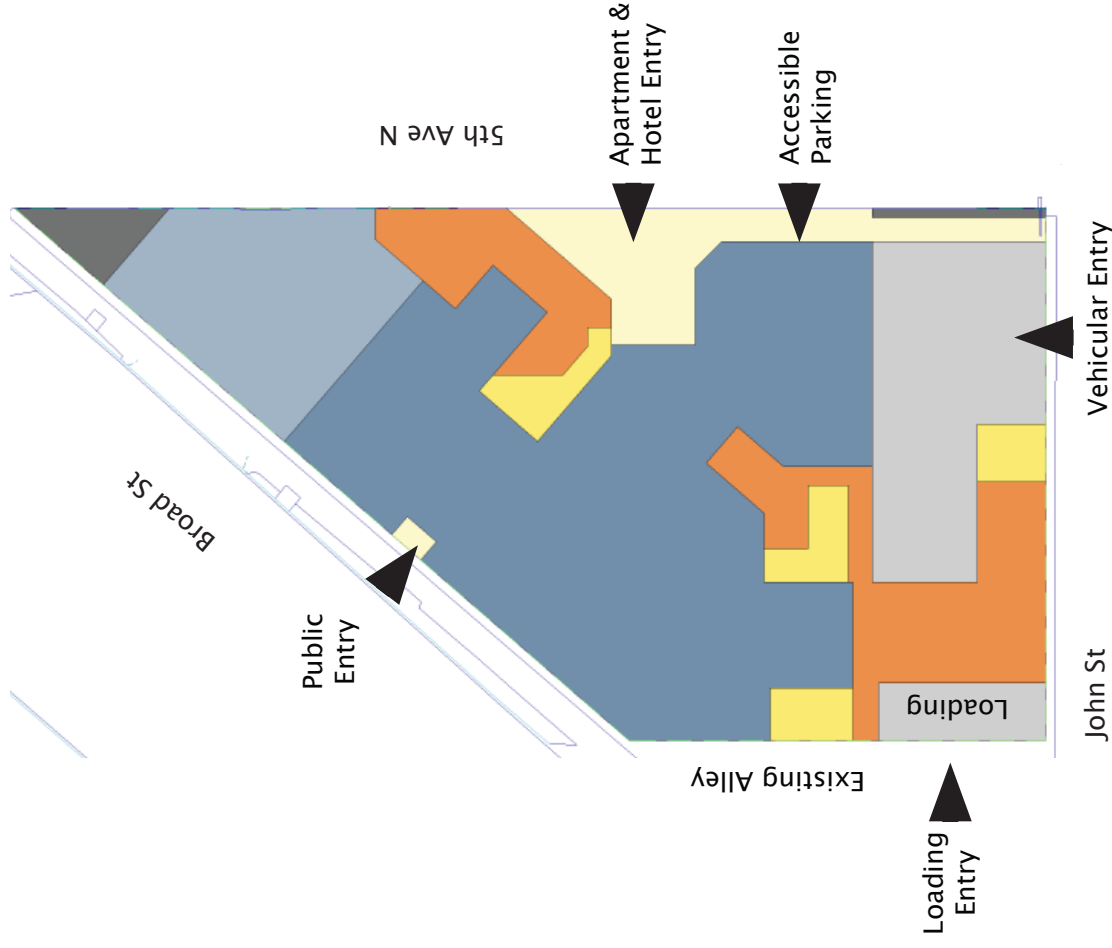


Responses to Board Guidance:

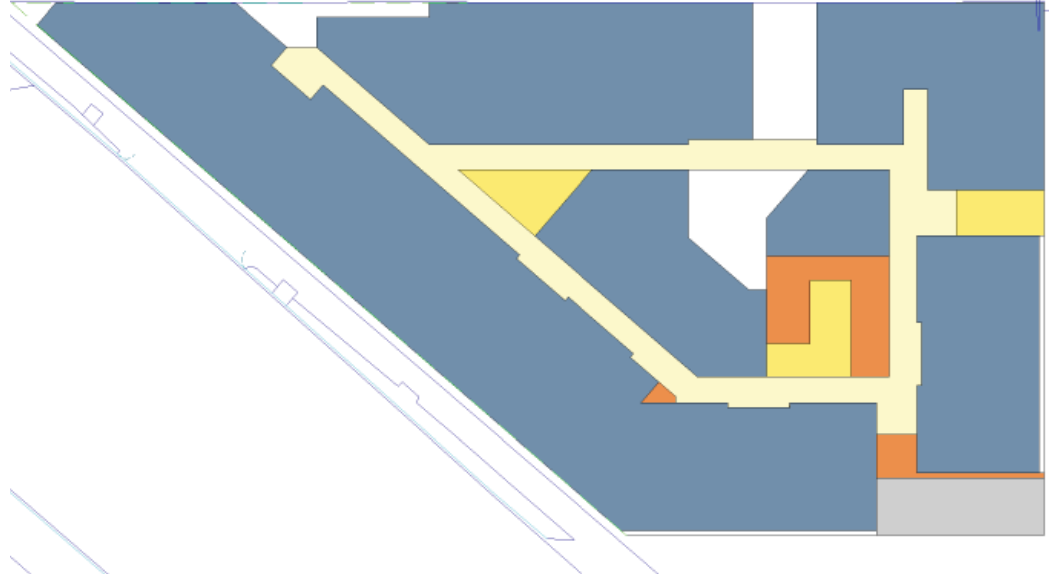
Building Massing

1. Proposed Roof top amenity space to allow for views to the adjacent Seattle Center and create a visual connection between the building and the Space Needle
2. Proposal of a highly transparent main floor facade that allows for views in to and through the building
3. Proposed bold color scheme to respond to the adjacent Architectural elements of the Space Needle and the EMP
4. Recess in facade at main floor to indicate public entry

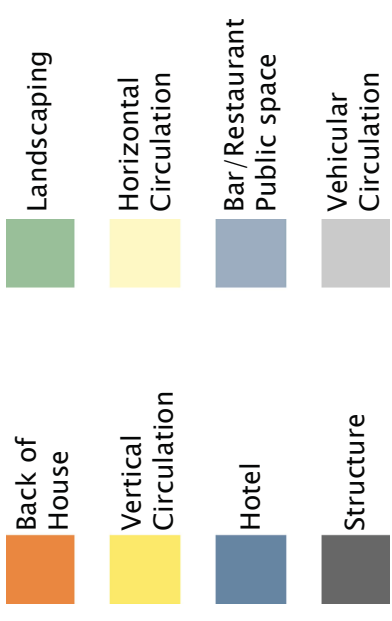
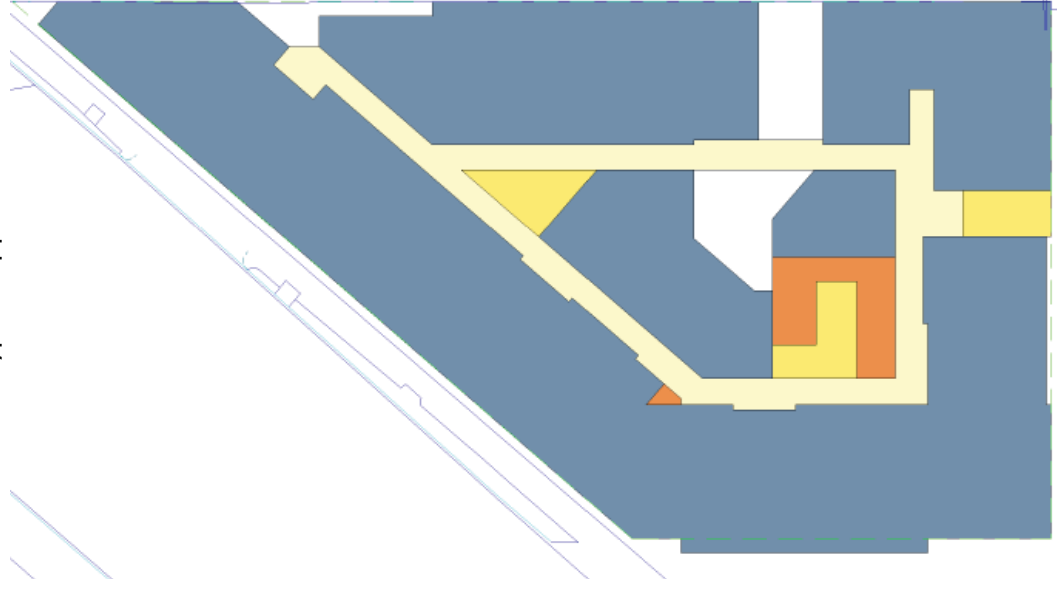
Option 3
Main Floor Plan



Option 3
Second Floor Plan



Option 3
Typical Upper Floor Plan



Description: Street Level

The street level for option 3 is similar to options 1 and 2 with the hotel 'Back of House' located along John St and the existing alley. This option also includes proposed public space at the corner of 5th Ave N and Broad St Loading and Unloading would be accessed from within the confines of the property. Vehicular entry would be from John St with Potential valet drop-off / Accessible Van Parking entry located adjacent to the structure of the Seattle Monorail. Pedestrian Access would be from 5th Ave N

Responses to Board Guidance:

Street Level

1. Anticipation of increased pedestrian traffic through proposal of additional public entry to restaurant space along Broad St
2. Proposal of outdoor / sidewalk furniture to allow for softening of the streetscape
3. Entry and seating along Broad St to respond to adjacent open space at Seattle Center

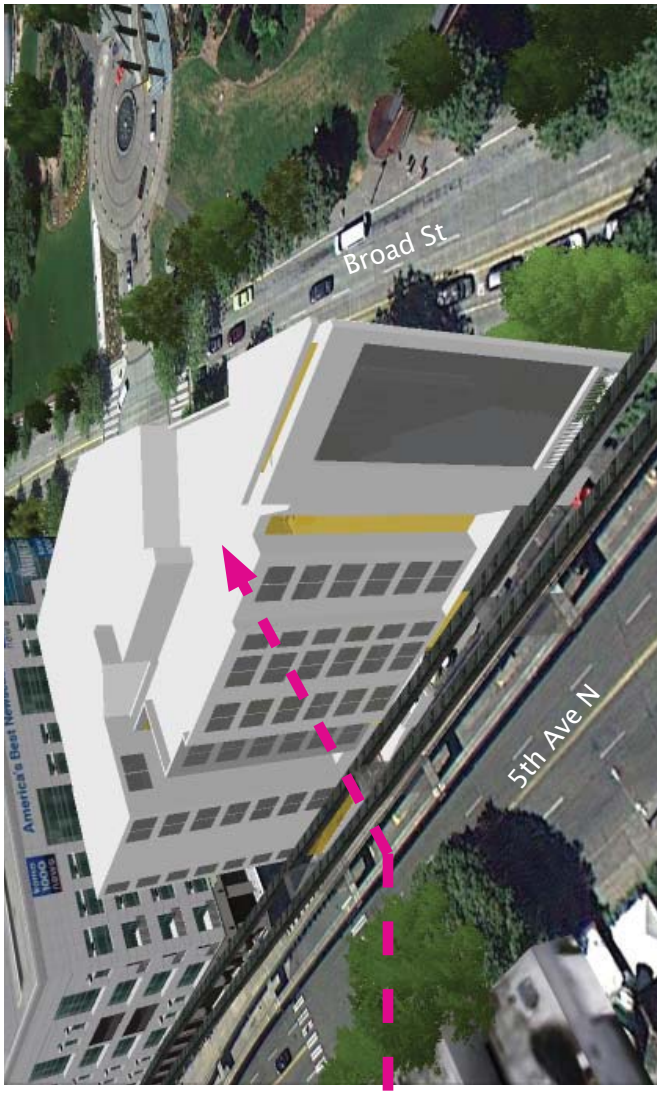
4. Vehicular access to the garage and for Valet drop-off is located on John St

5. Proposal of a highly transparent main floor facade that allows for views in to and through the building

Option 3 Massing (View south from Broad St Facade)



Option 3 Massing (View southwest from corner of 5th Ave N & Broad St)



Rooftop amenity space

Transparency along Main Floor Facade

Option 3 Massing (View west from 5th Ave N Facade)



Fully Glazed Stairwell

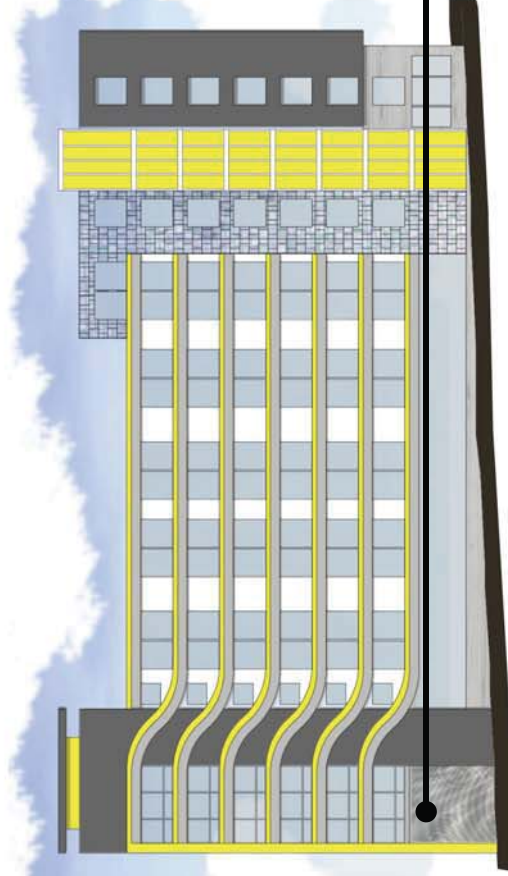
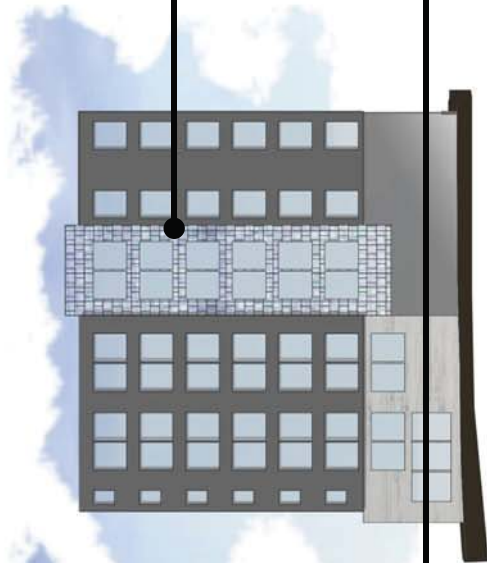
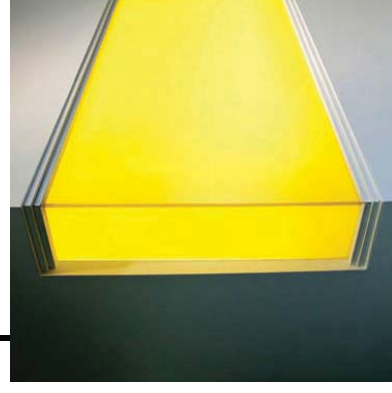
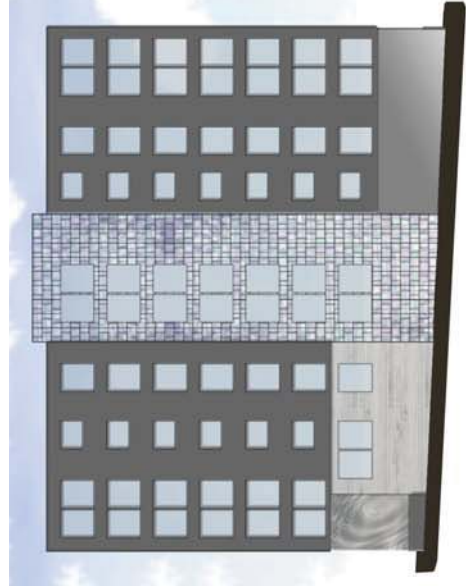
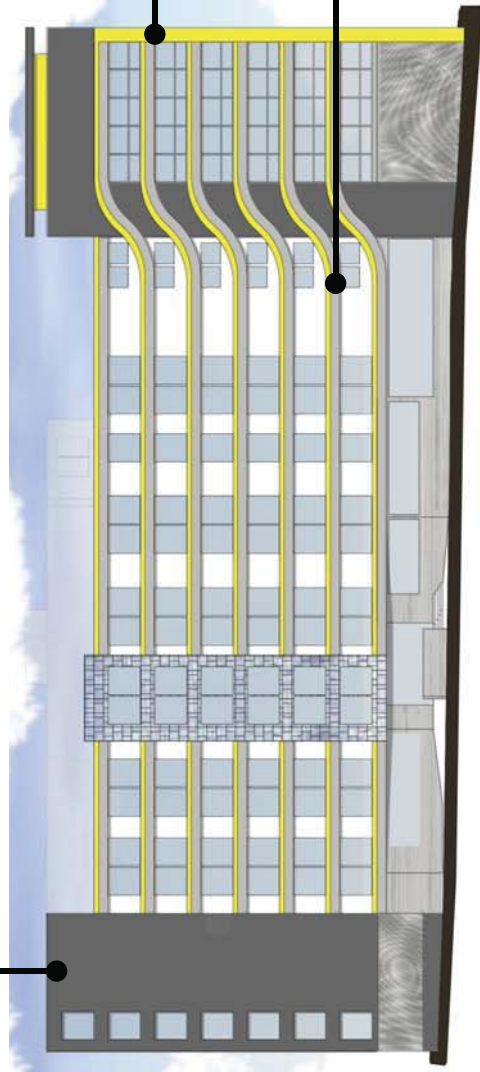
Responses to Board Guidance:

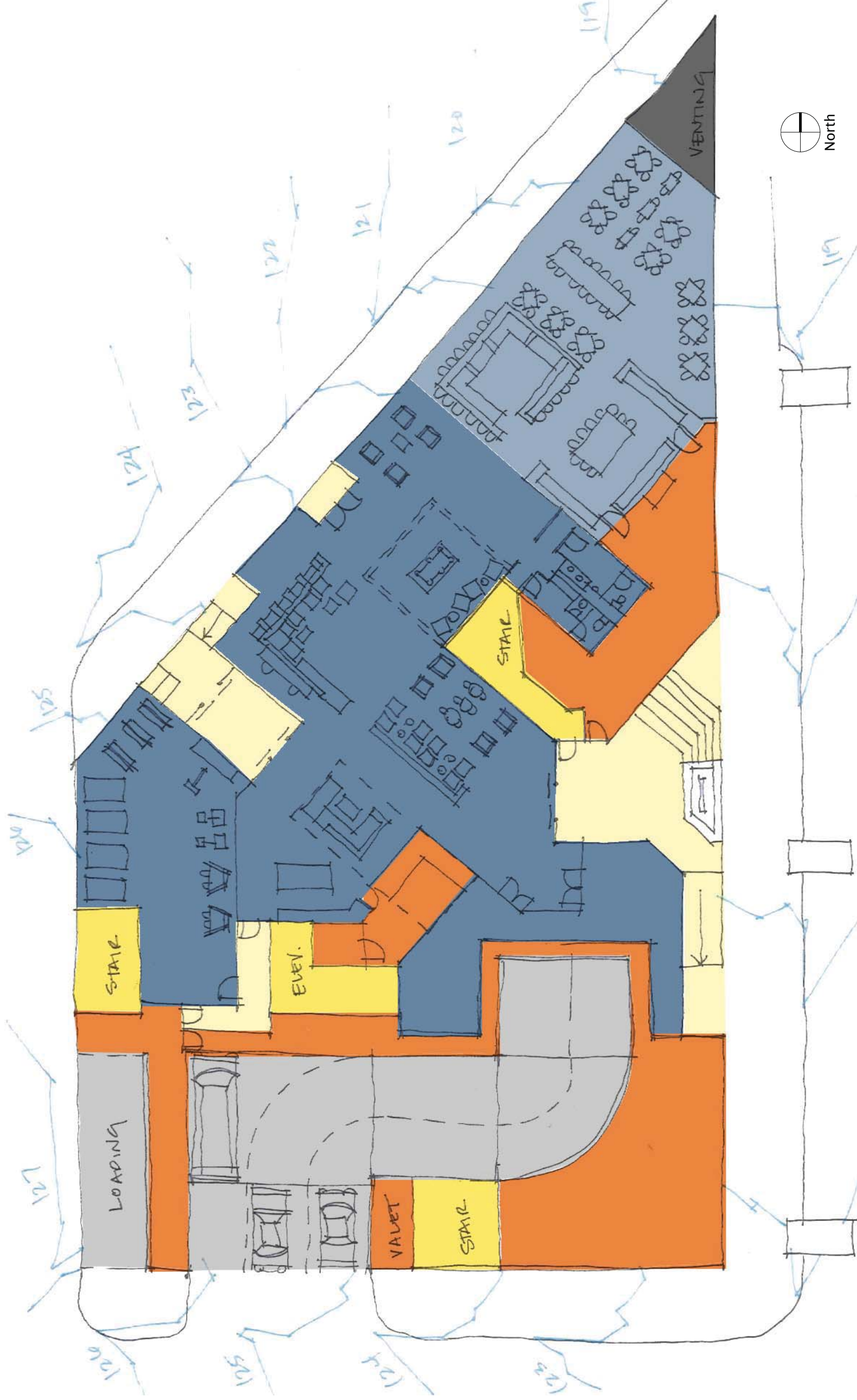
Building Massing

1. Proposed recession of facade at 5th Ave N with glass bridge at upper levels to allow views to surrounding landscape
2. Proposed Roof top amenity space to allow for views to the adjacent Seattle Center and create a visual connection between the building and the Space Needle
3. Proposal of a highly transparent main floor facade that allows for views in to and through the building
4. Proposed bold color scheme to respond to the adjacent Architectural elements of the Space Needle and the EMP



CONCEPT DESCRIPTION:
HORIZONTAL MOVEMENT
WARM GLOWING BEACON
WARPING EFFECT
TRANSPARENCY AT BASE

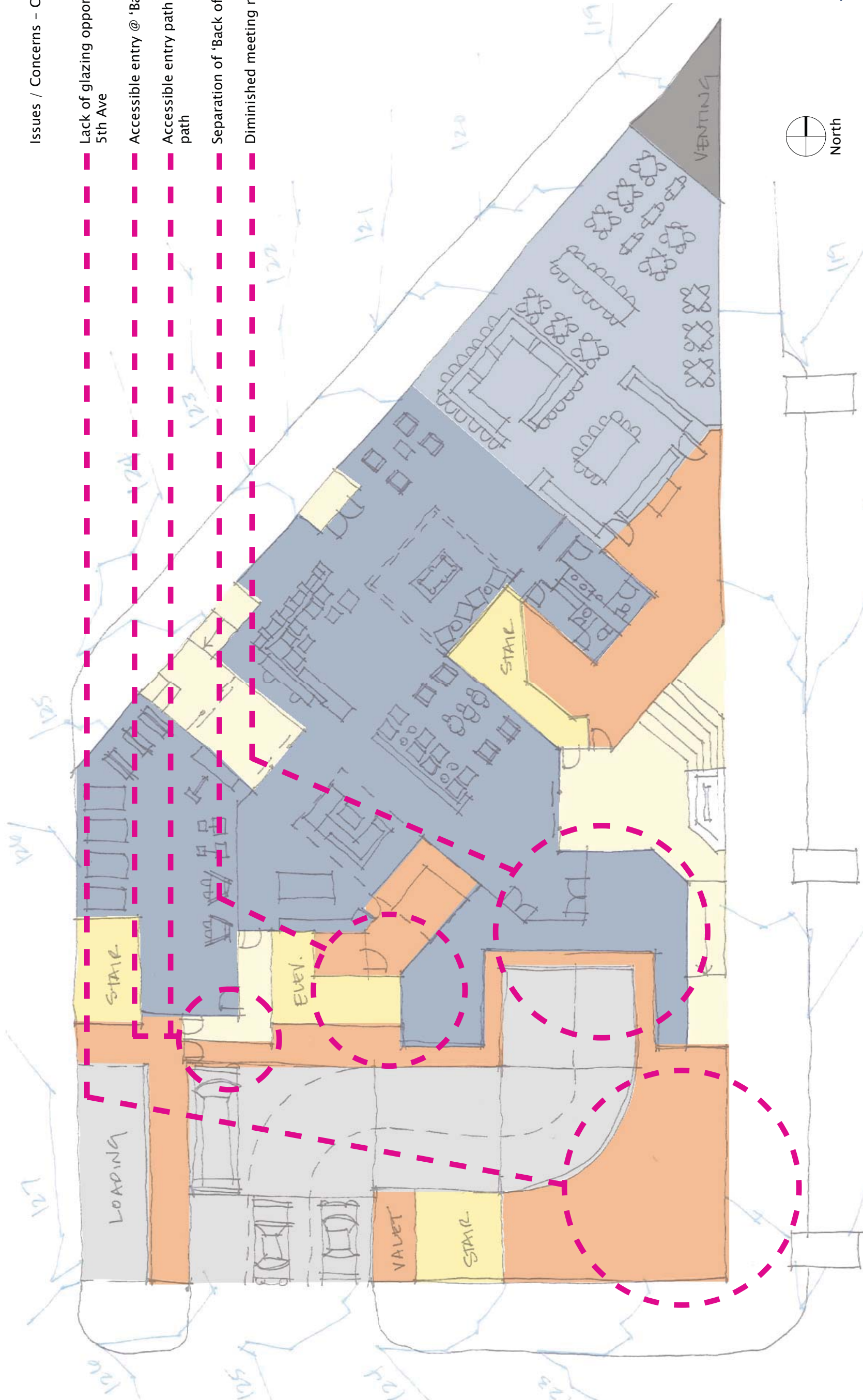


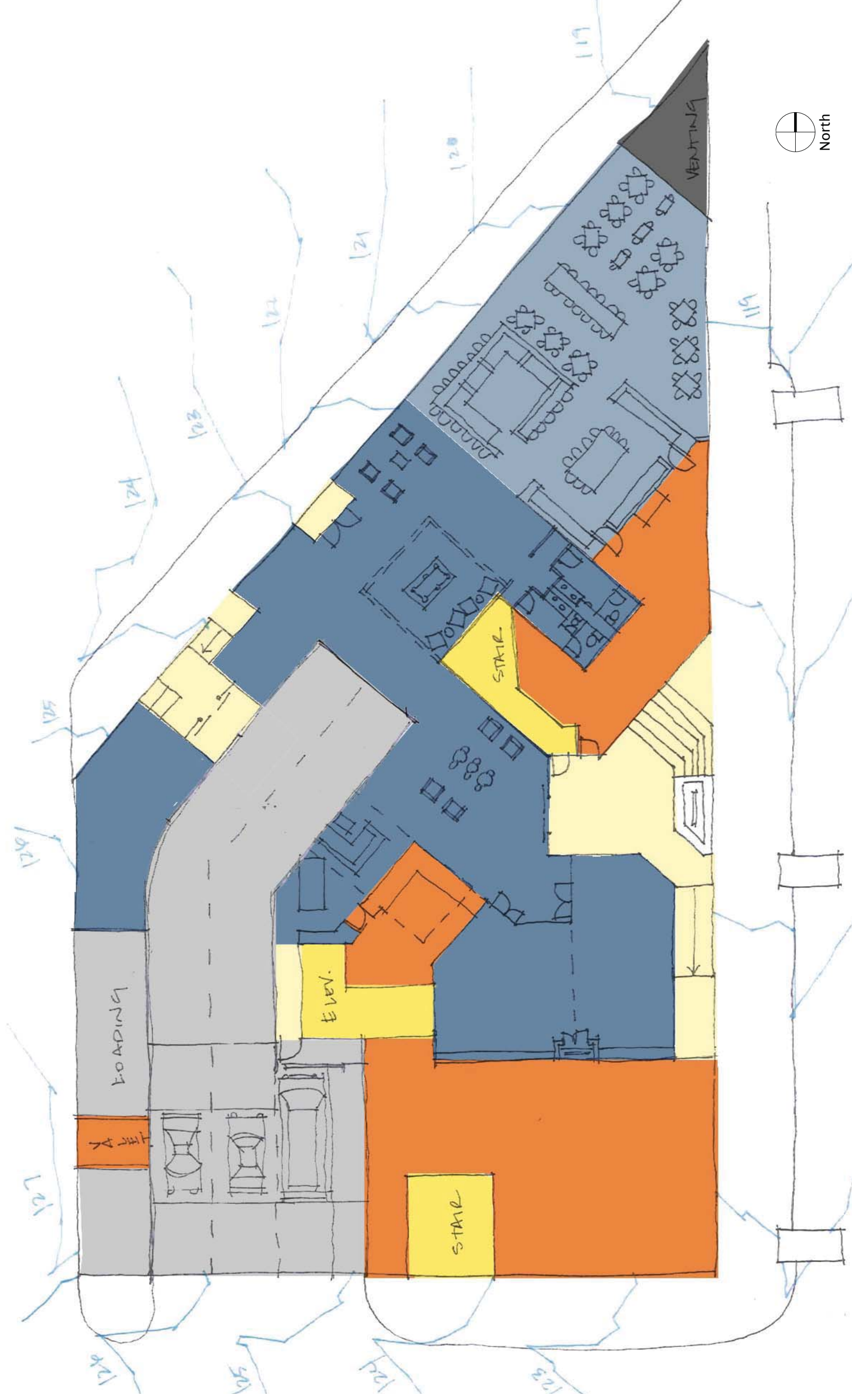


North

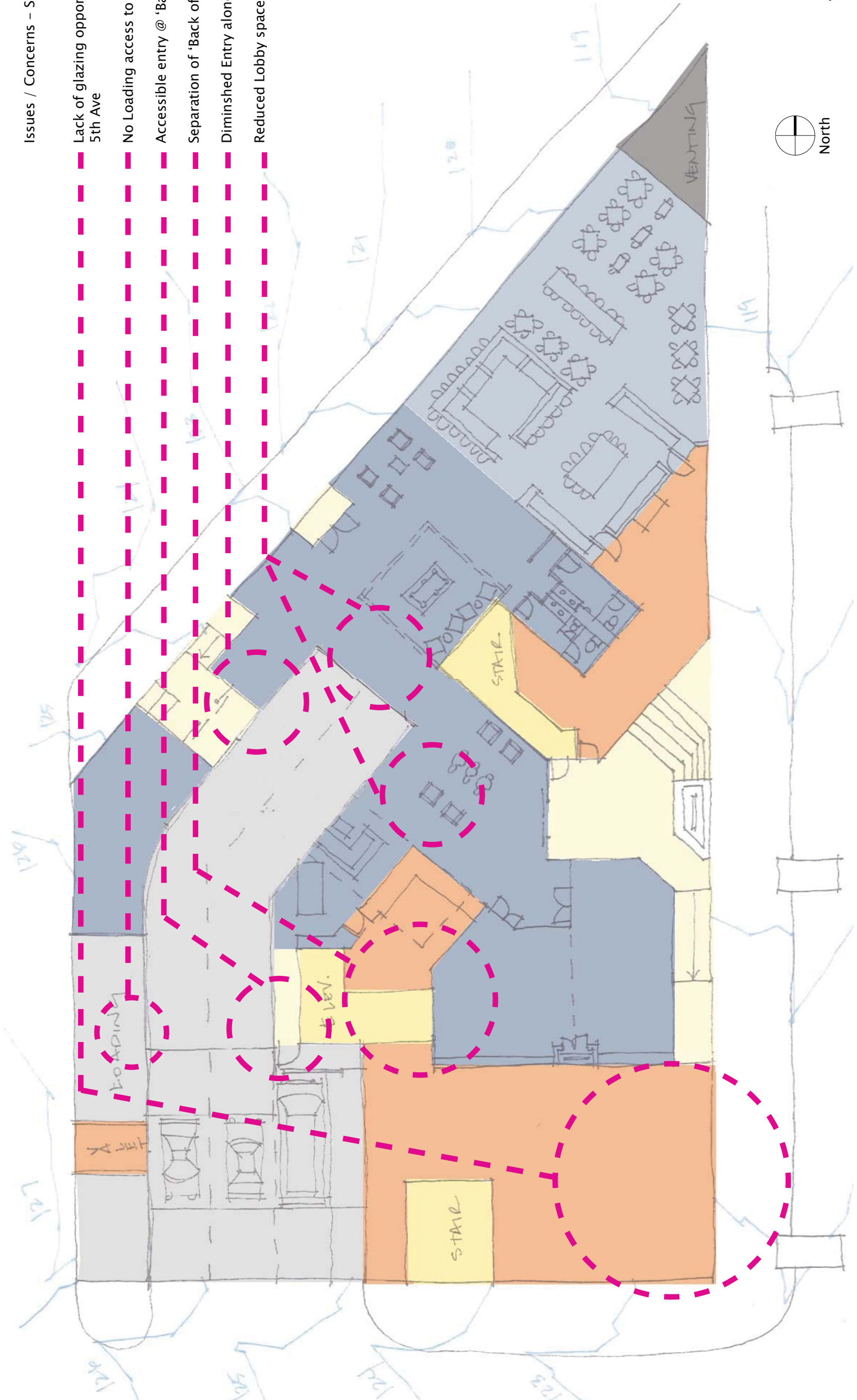
Issues / Concerns - Curved entry option

- Lack of glazing opportunity @ corner of John St - 5th Ave
- Accessible entry @ 'Back of House'
- Accessible entry path crosses loading circulation
- Separation of 'Back of House' from host
- Diminished meeting room space





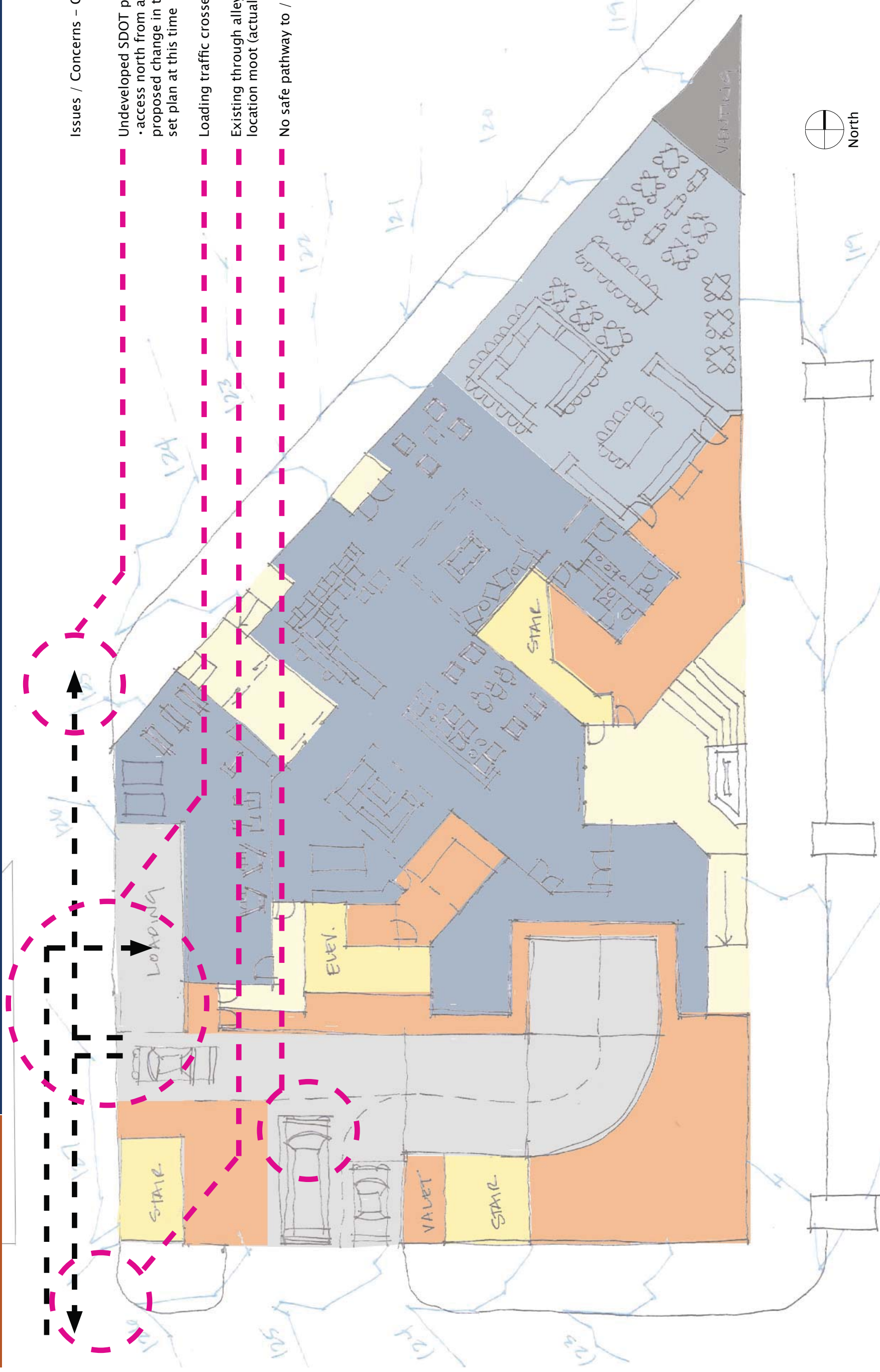
North



Issues / Concerns - Straight entry option

- Lack of glazing opportunity @ corner of John St - 5th Ave
- No Loading access to building / 'Back of House'
- Accessible entry @ 'Back of House'
- Separation of 'Back of House' from host
- Diminished Entry along Broad St
- Reduced Lobby space due to drive aisle





Issues / Concerns - Curved exit option

Undeveloped SDOT plan for Broad St.

- access north from alley may be removed due to proposed change in traffic pattern - SDOT has no set plan at this time

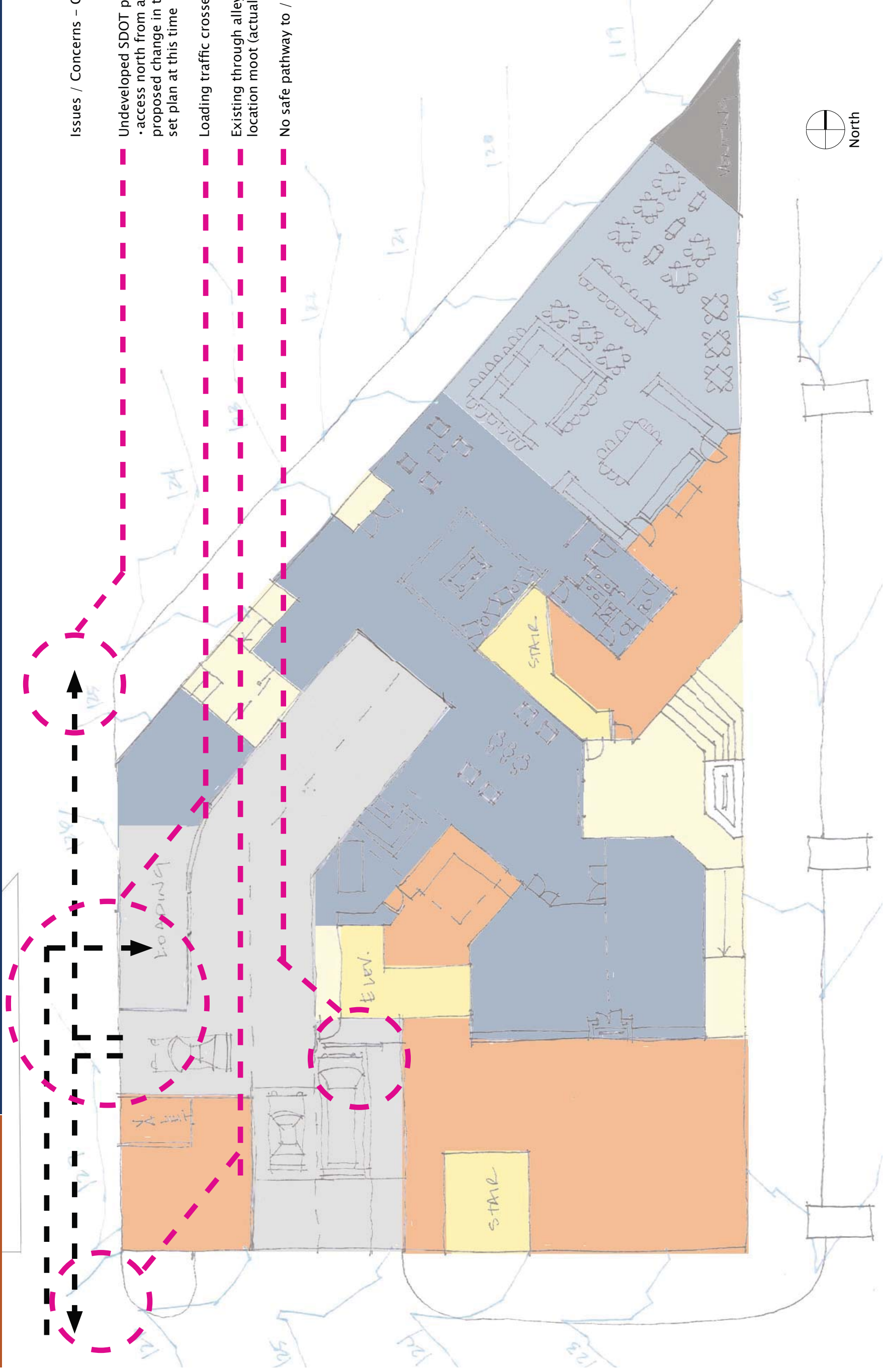
Loading traffic crosses exiting traffic

Existing through alley on to John makes exit location moot (actually promotes traffic issues)

No safe pathway to / from Accessible stall



North



Issues / Concerns - Curved exit option

Undeveloped SDOT plan for Broad St.

- access north from alley may be removed due to proposed change in traffic pattern - SDOT has no set plan at this time

Loading traffic crosses exiting traffic

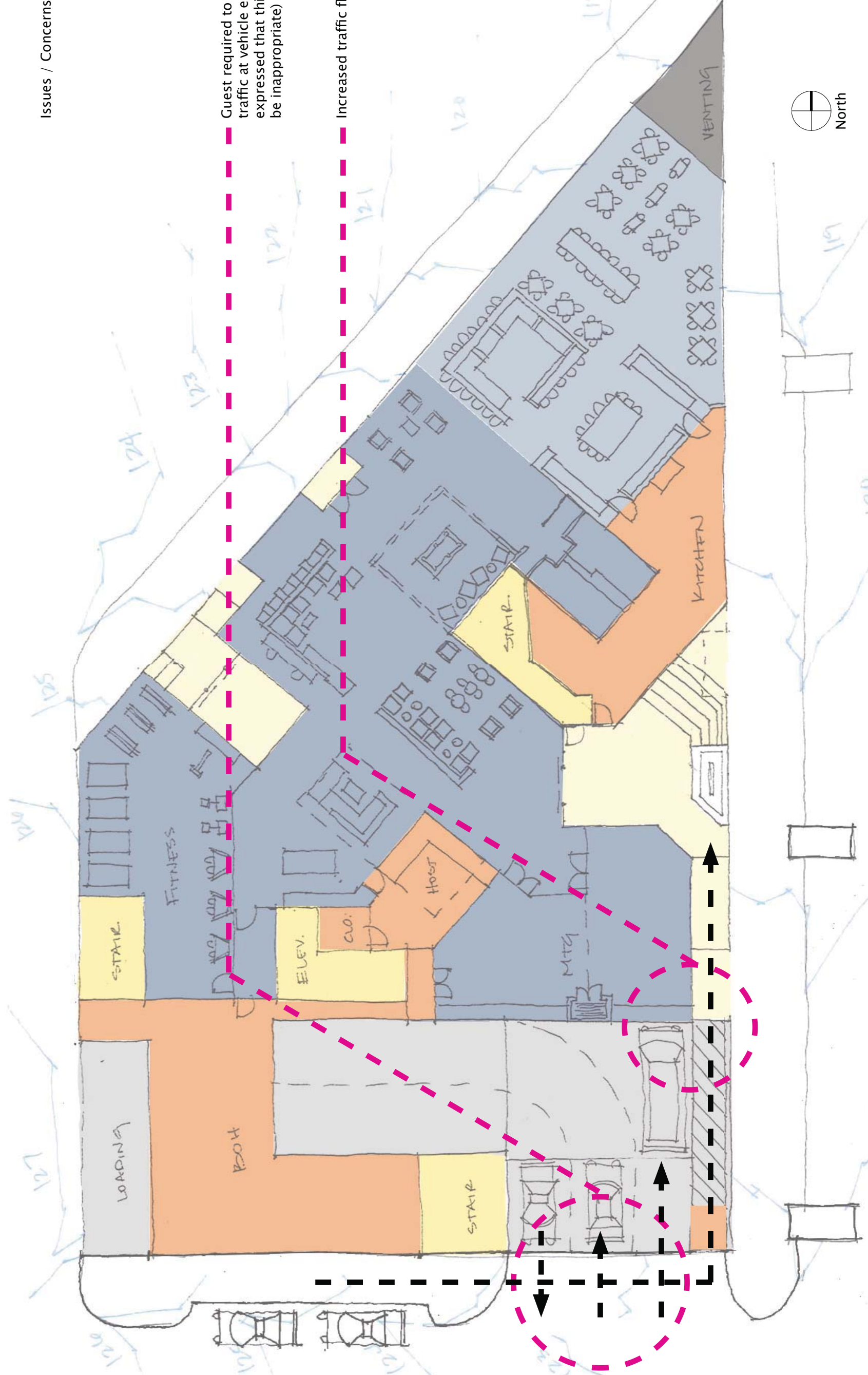
Existing through alley on to John makes exit location moot (actually promotes traffic issues)

No safe pathway to / from Accessible stall



North

Issues / Concerns - John St drop-off option

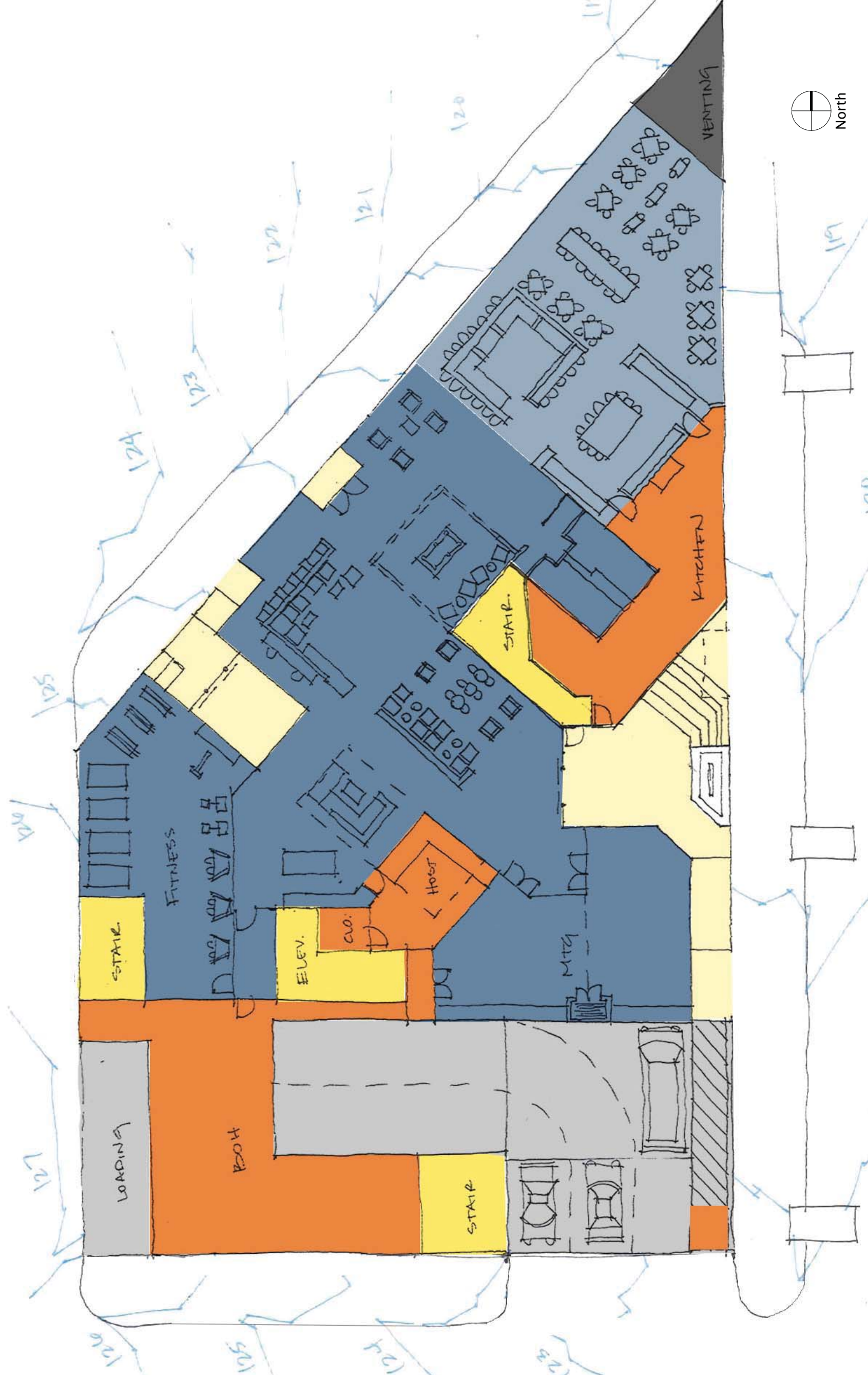


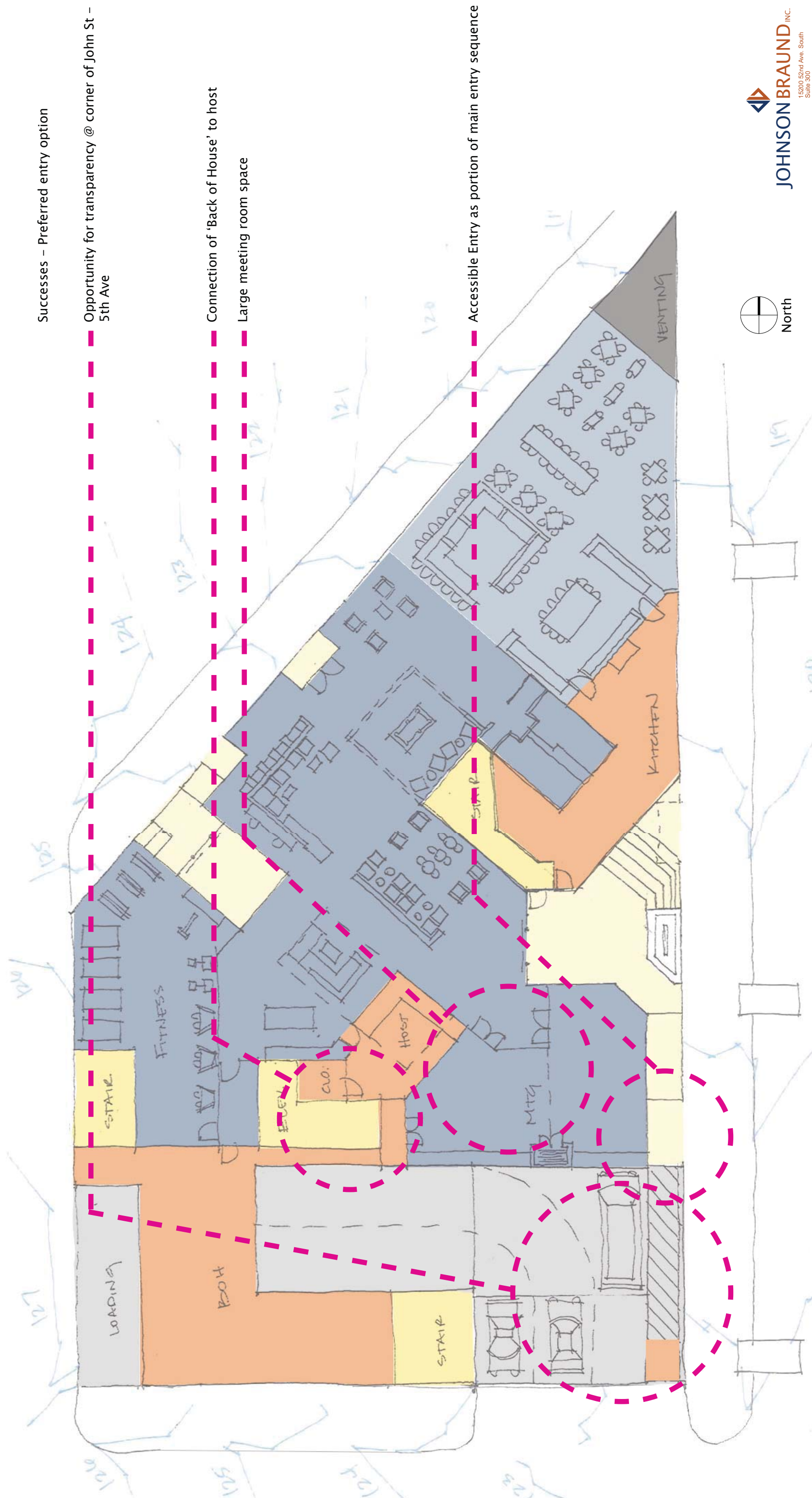
Guest required to cross in-bound and outgoing traffic at vehicle entry (the Board previous expressed that this type of entry sequence would be inappropriate)

Increased traffic flow at Accessible Entry



North





Successes - Preferred entry option

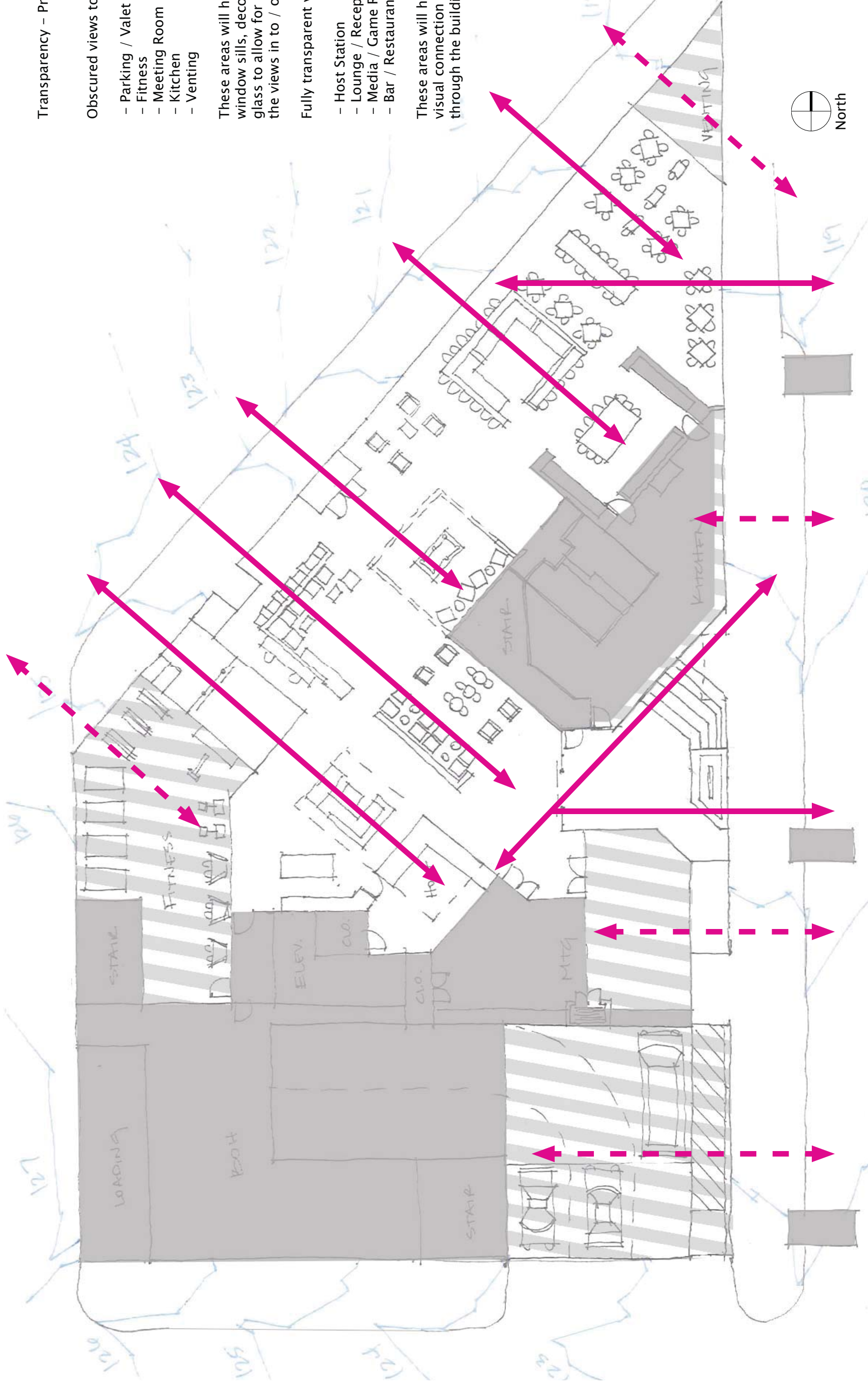
Opportunity for transparency @ corner of John St - 5th Ave

Connection of 'Back of House' to host

Large meeting room space

Accessible Entry as portion of main entry sequence





Transparency – Preferred entry option

Obscured views to ‘less-public’ services:

- Parking / Valet area
- Fitness
- Meeting Room
- Kitchen
- Venting

These areas will have translucent scrim, higher window sills, decorative metal banding or fritted glass to allow for light transmission while reducing the views in to / out of the space

Fully transparent views to ‘public’ spaces:

- Host Station
- Lounge / Reception
- Media / Game Room
- Bar / Restaurant

These areas will have full height glass allowing a visual connection from the street edge into and through the building

