



STREAM BELMONT
500 BELMONT AVENUE EAST

DESIGN REVIEW

DPD #3012378
SEPTEMBER 7, 2011

4302 SW ALASKA STREET
SEATTLE, WA 98116
206.933.1150
www.nkarch.com



PROJECT DESCRIPTION



SITE LOCATION

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ADDRESSES: 500 Belmont Avenue E
DPD PROJECT #: 3012378 (Old MUP 3006603)
OWNER/APPLICANT: Stream Belmont
AGENT: Nicholson Kovalchick Architects, Jill Burdeen

ZONING AND OVERLAY DESIGNATION

The site is located in the West Slope District, Midrise Zone of the Capitol Hill Urban Center Village. The area of the site is 10,026 sf.

PROJECT HISTORY

The project has an existing entitlement obtained in March 2008 by ARCA Architecture & Planning. We are opting to stay as close within the existing entitlement footprint as possible, with a few minor increases. The revised project has been submitted for a major MUP revision in May 2011. The original plans and elevations have been included in this packet for reference.

EXISTING SITE

The site is on the southwest corner of the intersection Belmont Avenue East and East Republican Street. The site is comprised of three parcels including two existing two-unit residential structures facing Belmont Avenue East and an empty lot on the corner of Belmont and Republican. Each residential structure is accompanied by a curb cut serving two garages. Several mature street trees are adjacent to the site along both Belmont and Republican. The topography of the site has been graded to accommodate the westerly slope of E Republican Street. The resultant site slopes gently to the west before an abrupt slope transitions several feet down to meet the sidewalk along Belmont Avenue.

NEIGHBORING DEVELOPMENT

A surface parking lot, serving the residents of the Lamplighter Condominiums across the street is located on the property directly to the north of the site. Tashkent Park is situated to the northeast and Viceroy Apartments directly abuts the east of the site in a zero lot line condition. Dense multi-family housing projects are located directly across Belmont Avenue and Republican Street. The West Slope District is a densely developed multi-family area with pockets of ground floor commercial, bordered with a higher density commercial area several blocks to the east along Broadway.

DEVELOPMENT OBJECTIVES

The project is a multi-story apartment building containing 65 residential units. Parking for approximately 39 vehicles will be located in a below grade parking garage, which is accessed via a ramp from Belmont Avenue E. The two existing one-story residential structures on site will be demolished. The approximate sizes of the proposed building and its individual uses are as follows:

Number of Residential Units:	65 Units
Number of Parking Stalls:	39 Stalls
Area of Residential Levels:	46,343 Square Feet
Area of Parking Levels:	13,641 Square Feet
Total Building Area:	59,984 Square Feet

MAJOR CHANGES FROM APPROVED MUP

- Increased unit count from 40 to 65
- Decreased parking count from 56 to 39 stalls
- Changed exterior brick of primary building massing to Metal Panel, AEP Span Prestige Series, Cool Metallic Silver
- Changed exterior deck materials to an aluminum deck with glass guard
- Extended building in northeast corner of lot at Levels 2 and above by approximately 75 sf
- Extended building in northwest corner of lot at Levels 3 and above by approximately 293 sf
- Extended building in southwest corner of lot by approximately 100 sf
- Extended portion of north façade towards property line by 7”
- Adjusted modulation along west façade of building
- Relocated lobby to center of building along Belmont Avenue East
- Added a story and increased building height by 8'-10”

MAJOR CHANGE RATIONALE

Relocating the lobby to the center of the building along Belmont is in direct response to the Capitol Hill Neighborhood Design Guidelines D.1 (Pedestrian Open Spaces and Entrances) and D.7 (Personal Safety). It was important to provide entrances that linked the building to the surrounding landscape while also linking the entrances to the pedestrian environment beyond the project perimeter. The original lobby was located at the northwestern end of the site, which pulled the lobby away from the other building entrances, isolating the major public entrance to the northern side of the vehicular entry to the building. Moving the entrance to a more centrally located point in the building linked the main building entry to the other private unit entries while also maintaining a connection to the pedestrian environment. Moreover, bringing the lobby to the center of the building allowed for the core to also be shifted to the center of the building, addressing the boards concerns about the large tower at the north end of the building. Lastly, this lobby location allows the pedestrian entrance to be pulled away from the vehicular entrance which creates a safer pedestrian environment, and distinguishes between the pedestrian and vehicle.

Modifying the exterior of the building at the northeast, northwest and southwest corners creates a much more unified massing, allowing for more organic material changes and a stronger aesthetic statement. Moreover, these changes create the opportunity for clean and streamlined design.

The original project was a concrete structure that was not designed to the fullest zoning capacity, and was seemingly economically infeasible. The proposed project is a wood structure that has increased the overall building capacity, while keeping within the permitted zoning and building code allowances.

PARCEL #: 6848200825, 6848200827, 6848200830
ZONING: MR
OVERLAYS: Capitol Hill Urban Center Village
LOT AREA: 10,026 sf

ZONING CODE PROVISIONS USED IN PREVIOUS MUP

23.45.006 SOLID WASTE & RECYCLING

Required: Size: 200 sf minimum for 51-100 units
Container: Front loading
Access: Direct access from street
Proposed: Size: 200 sf storage
Container: Front loading
Access: Through garage

23.45.054 STRUCTURE WIDTH & DEPTH

Structure Width
• See current code
• Departure granted in original MUP for max width without modulation of 88'-3.5"

Structure Depth
Max Allowed: 65% of lot depth
.65 X 82=53'-4"
Proposed: 75'-8"
• Departure requested for 22'-4" over allowed maximum structure depth
• Departure granted in original MUP for 1,230 sf per floor

23.45.054 MODULATION

- No modulation requirement under current zoning code
- Front and Side Facades: Departure required, but not requested in Original MUP for minimum modulation depth
- Rear Facade: Departure required, but not requested in Original MUP for maximum width of facade without modulation
- Side Facade: Departure granted in original MUP for 47'-3" length of unmodulated facade

23.45.056 SETBACKS

Front Yard Setback
Required: 5'-0" minimum/10'-0" average
Proposed: 6'-0" minimum/11'-7" average at Levels 1-2
10'-6" minimum/11'-3" average at Levels 3-6

Rear Yard Setback
Required: 10'-0" minimum with modulation/15'-0" average
Proposed: 4'-0" minimum/5'-4" average
• Departure requested for 4'-0" minimum/5'-4" average
• No modulation requirement under current zoning code
• Departure granted in original MUP for 41'-7" of a zero lot line condition

Side Yard Setback at North Facade
Required: 8'-0" min at building & 3'-0" min at decks/10'-6" average
Proposed: 5" min at building/1'-6" average
• Departure requested for 5" min at building/1'-6" average
• Departure granted in original MUP for zero lot line condition

Side Yard Setback at South Facade
Required: 8'-0" min at building & 3'-0" min at decks/10'-6" average
Proposed: 4'-2" min at building & 1'-4" min at decks/7'-7" average
• Departure requested for 4'-2" min at building & 1'-4" min at decks/7'-7" avg.
• Departure granted in original MUP for zero lot line condition and for balconies encroaching into 3' balcony setback

23.45.058 OPEN SPACE
• Replaced by amenity space in current code
• Departure required, but note requested in original MUP for location and minimum dimension

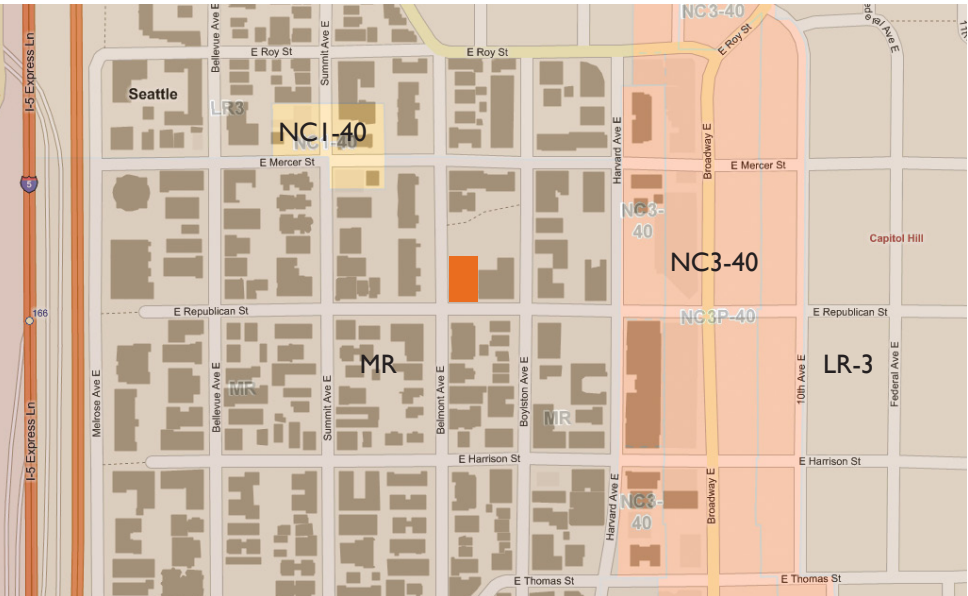
23.45.030 DRIVEWAY
Required: 20' minimum width for two-way traffic
Proposed: 18'
• Departure requested for 18' wide driveway
• Departure granted in original MUP for 18' wide garage entry and access drive

23.45.030.G SIGHT TRIANGLE
Required: 10' X 10' triangle at both sides of driveway
Proposed: 10' X 10' triangle on exit side
10' X 10' triangle on entrance side
• Departure requested for 6" height encroachment into sight triangle on south side of driveway

APPLICABLE CURRENT CODE PROVISIONS

23.45.514 STRUCTURE HEIGHT
Allowed Maximum Structure Height:
- Base Height: 60'-0"
- Additional height allowed per 23.45.514.G: 5'-0"
- Max building height to top of roof: 65'-0"
- 4' additional allowed for parapets: 69'-0"
- 15' additional allowed for stair penthouse: 80'-0"
- 16' additional allowed for elevator penthouse: 81'-0"

Proposed Maximum Structure Height (above average grade line):
- To top of roof: 63'-5.5"
- To top of parapet: 67'-3.5"
- To top of stair penthouse: 73'-4"
- To top of elevator penthouse: 78'-7"



DPD ZONING MAP

23.45.522 AMENITY AREA
Required: 5% of gross floor area in residential use
5% X 46,929 = 2,346 sf required
Proposed: 3,019 sf (6.4%)

23.45.528 STRUCTURE WIDTH
Max Allowed: 150'-0"
Proposed: 115'-4"

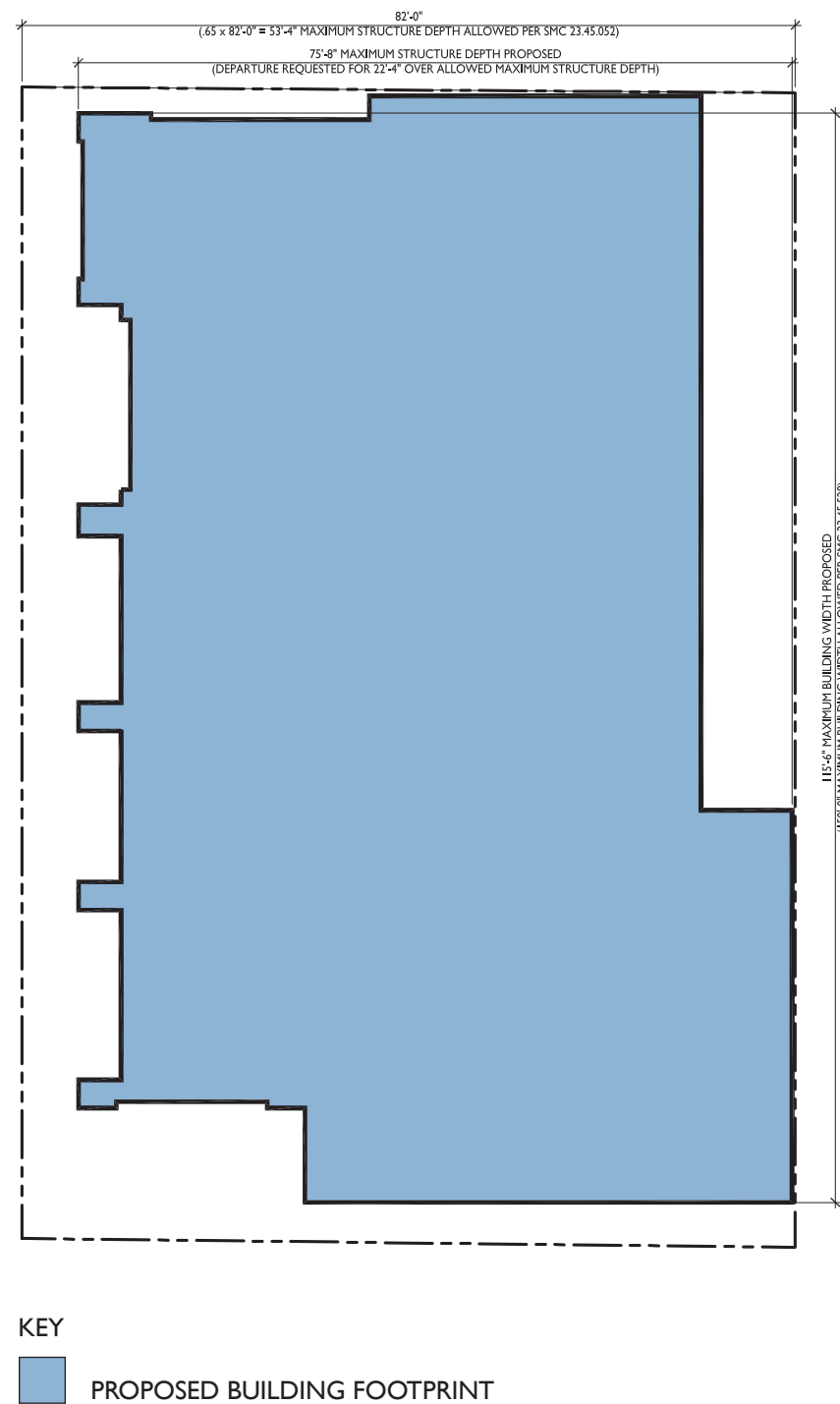
23.54.015 PARKING
No parking is required for uses in commercial zones located in urban centers
• 39 stalls provided

23.86.006 STRUCTURE HEIGHT MEASUREMENT
The height of a structure is the difference between the elevation of the highest point of the structure not excepted from applicable height limits and the average grade level ('average grade level' means the average of the elevation of existing lot grades at the midpoints, measured horizontally, of each exterior wall of the structure or at the midpoint of each side of the smallest rectangle that can be drawn to enclose the structure)

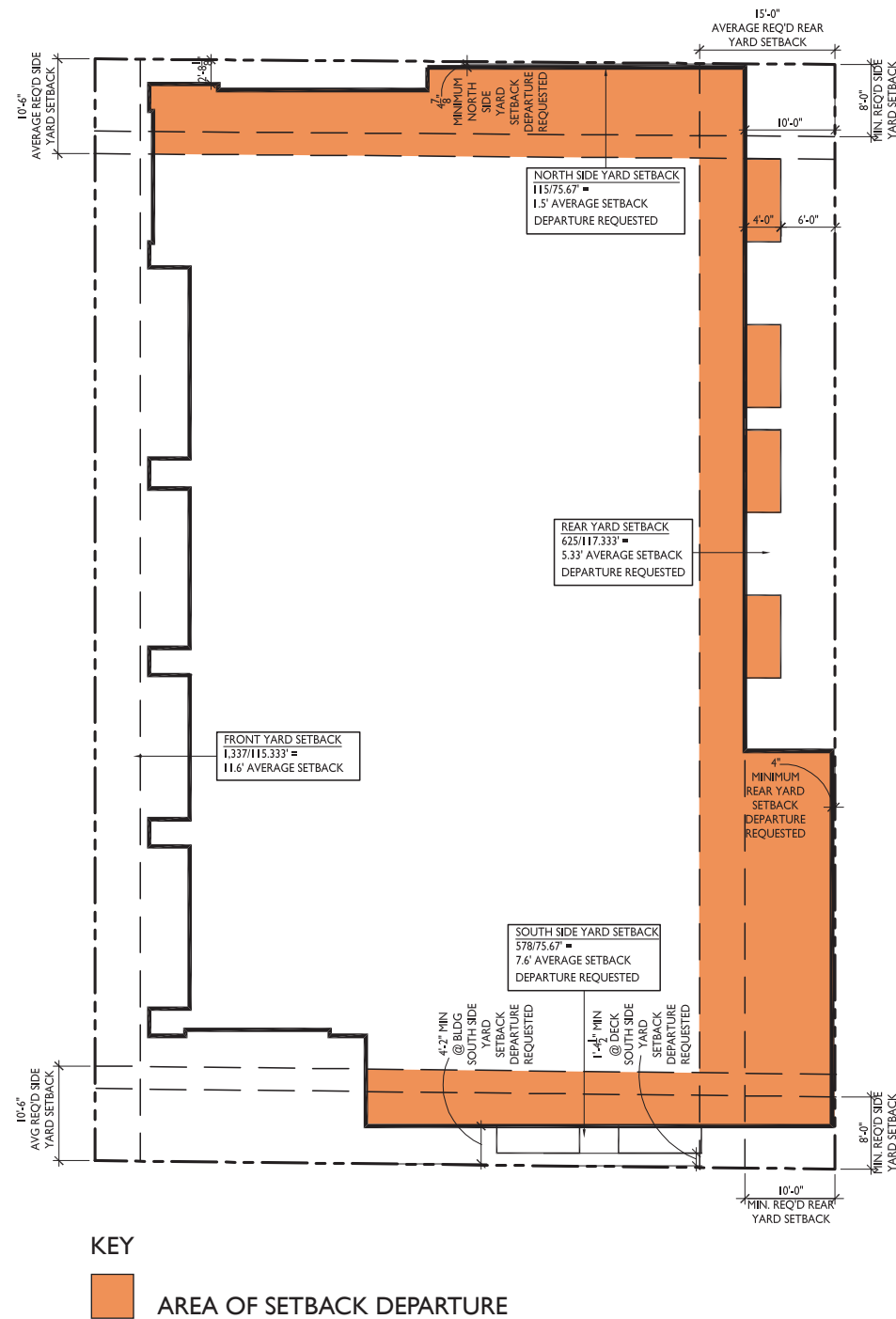
DEPARTURE MATRIX

		MIDRISE ZONE (MR)				
	Development Standard	Requirement	Proposed	Departure Amount	Reason for Departure	Design Review Guidelines
1	Structure Depth (SMC 23.45.052)	Maximum structure depth allowed = 65% of lot depth = .65x82' = 53'-4" allowed	75'-8"	22'-4"	The proposed massing is deeper in order to address the zero lot line condition to the East. Greater depth allows for more desirable living spaces within the units. The building steps back towards the northeast, to give some relief along the East property line. We did not increase the building depth beyond the departure granted in the original MUP	A-1 Site Characteristic A-2 Streetscape B-1 Hght, Bulk, Scale C-2 Arch Concept
2	Rear Setback (SMC 23.45.056)	Minimum = 10' Average = 15' (no modulation req'd under current code)	Minimum = 4" Average = 5'-4"	Minimum = 9'-8" Average = 9'-8"	The proposed departure is in response to the zero lot line condition of the building to the east. Again, the building steps back towards the North, to provide some relief for the units.	A-1 Site Characteristic B-1 Hght, Bulk, Scale C-2 Arch Concept
3	Side Setback (SMC 23.45.056)	Min @ Bldg = 8'-0" Min @ Decks = 3'-0" Average = 10'-6"	North Façade: Min @ Bldg = 5" Average = 1'-6" South Façade: Min @ Bldg = 4'-2" Min @ Decks = 1'-4" Average = 7'-7"	North Façade: Min @ Bldg = 7'-7" Average = 9'-0" South Façade: Min @ Bldg = 3'-10" Min @ Decks = 1'-8" Average = 2'-11"	Whether the parcel to the north remains a parking lot or is developed at a future date the design addresses both situations. If the lot remains a parking lot, the building has addressed the blank wall by adding detail to the façade linking it to the rest of the building. If the lot is developed the almost zero lot line condition benefits the units in the proposed project as their views to the east and west will be preserved. The setback along the south property line continues the zero lot line condition at the rear setback to obscure the blank wall of the building to the east.	A-1 Site Characteristic A-2 Streetscape A-4 Human Activity A-6 Transition B-1 Hght, Bulk, Scale C-2 Arch Concept
4	Driveways (SMC 23.45.030)	Minimum width for 2-way Driveway = 20'-0"	Minimum width for 2-way Driveway = 18'-0"	Minimum width for 2-way Driveway = 2'-0"	This departure was granted in the original MUP, and was encouraged by the Design Review Board. The 18'-0" wide driveway decreases the visual impact of the vehicular entrance, which is only accessed by 39 parking stalls	A-2 Streetscape A-6 Transition C-2 Arch Concept C-5 Pkng Entrances
5	Sight Triangle (SMC 23.54.030.G)	Triangle at both sides of driveway = 10'x10' No obstructions in vertical spaces between 32" and 82" from ground	Triangle at both sides of driveway = 10'x10' Obstruction in the entrance side Triangle = 6"+ plant height	Obstruction in the entrance side Triangle = 6" + plant height	Both sides of the driveway have the required site triangle; however there is a planter which extends 6" into the obstruction limit on the entrance side. This planter aligns with all other planters along Belmont Ave E and exists to minimize the vehicular entrances impact along the street.	A-2 Streetscape A-6 Transition C-2 Arch Concept C-5 Pkng Entrances E-3 Landscaping

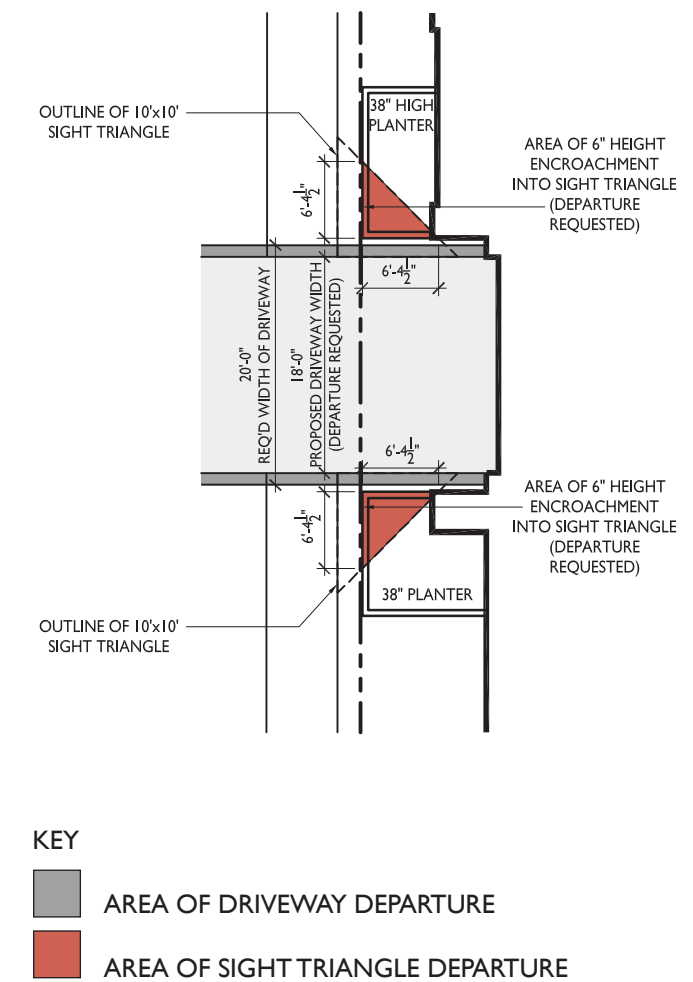
STRUCTURE DEPTH DEPARTURE #1



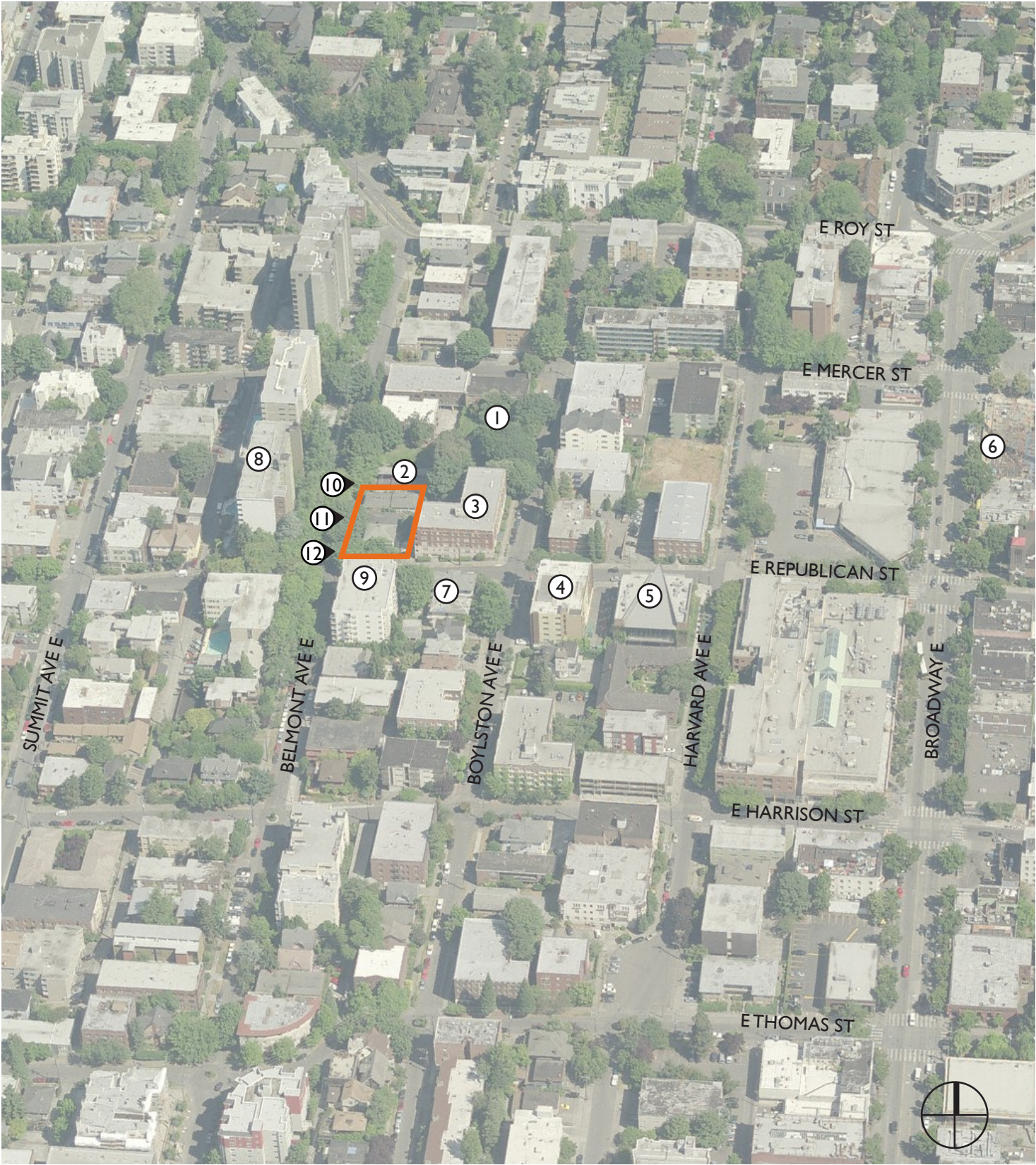
SETBACK DEPARTURES #2-3



DRIVEWAY AND SIGHT TRIANGLE DEPARTURES #4-5



SITE CONTEXT



① TASHKENT PARK



② ADJACENT PARKING LOT



③ VICEROY APARTMENTS



④ VERTIGO CONDOMINIUMS



⑤ CAPITOL HILL LIBRARY



⑥ BRIX CONDOS ON BROADWAY



⑦ COMMUNITY HOUSE



⑧ LAMPLIGHTER CONDOS



⑨ BELMONT COURT APARTMENTS



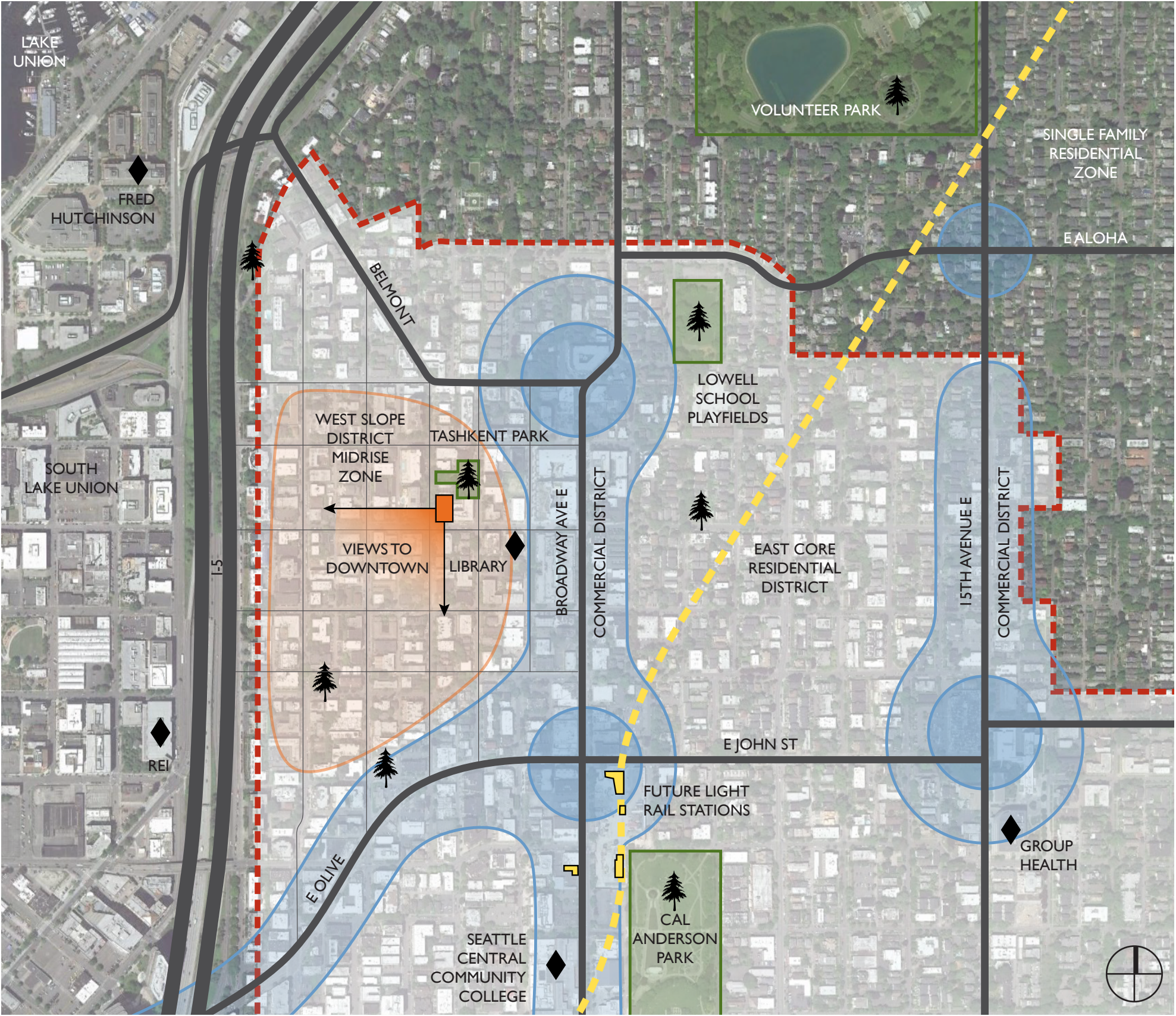
⑩ EXISTING SITE - 508 BELMONT



⑪ EXISTING SITE - 504 BELMONT



⑫ EXISTING SITE - 502 BELMONT



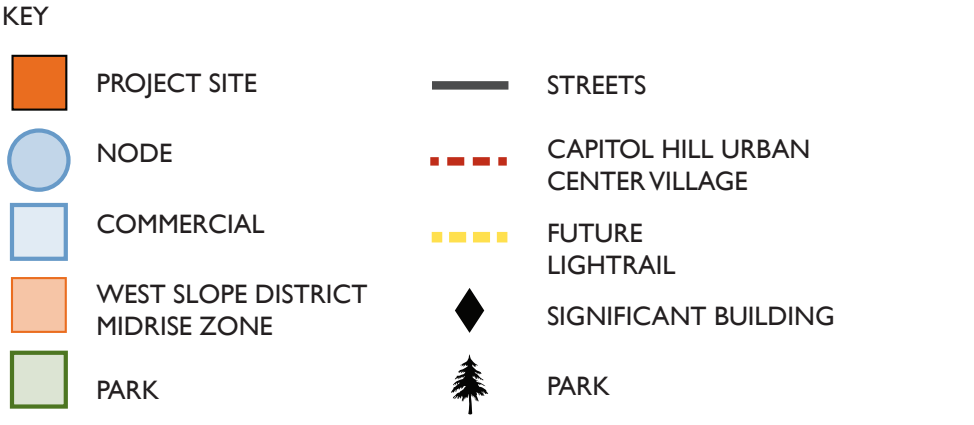
OPPORTUNITIES & CONSTRAINTS

The site is located in the West Slope District Midrise Zone, one of two primary residential areas in the Capitol Hill neighborhood. The dense residential development of the West Slope District provides for well-established neighborhood character predominately comprised of multi-family brick structures. Neighborhood development goals include ensuring that new infill development enhances this existing character. Several large-scale residential projects are in close proximity to the site along Belmont Avenue East including the Lamplighter and Highlander Condominiums directly across the street.

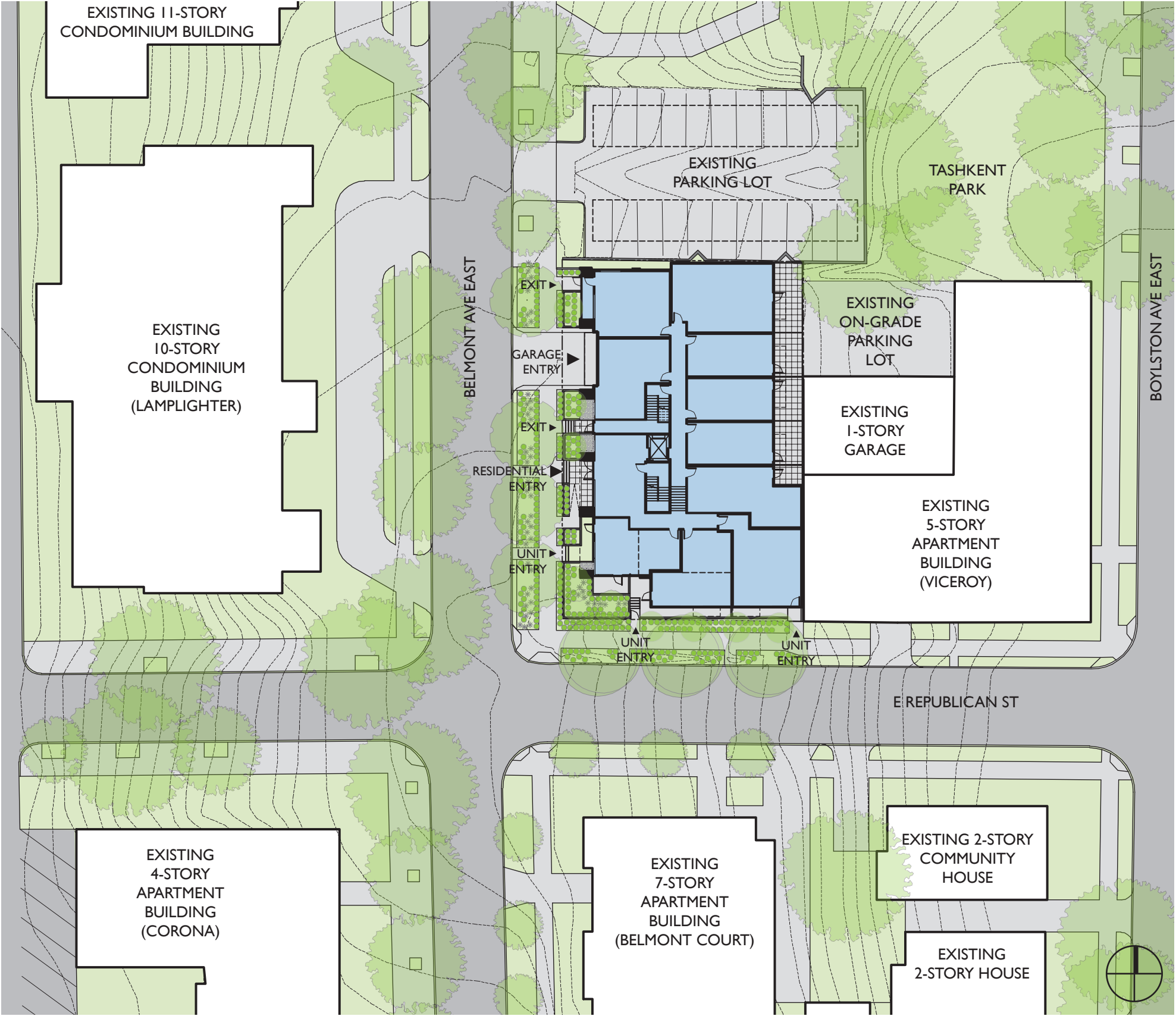
The immediate neighborhood context is bounded by major arterials: Belmont and Roy Street to the north, Broadway to the east of the site, and Olive Way to the South. To the west, I-5 creates a distinct edge to Capitol Hill. The neighborhood is well served by public transit. A future lightrail station is currently under construction at the corner of Broadway and E John Street. This future station is within walking distance of the site and creates a pedestrian relationship to public transportation.

The urban commercial district along Broadway borders the West Slope District three blocks to the east. Broadway offers walkable neighborhood commercial amenities and pedestrian-oriented activities. This section of Broadway has seen recent development including increases in both commercial and residential density. Farther east, the 15th Avenue commercial district offers additional amenities. Community resources include the Capitol Hill Branch Library located one block to the east of the site and Seattle Central Community College, approximately one-half mile to the south.

Numerous parks and amenity areas surround the site. Tashkent Park is directly to the northeast, providing opportunities for park views for residents. Views of downtown and the Olympic Mountains can be seen looking southwest from the site. Additionally, several mature street trees border the site along both Belmont and Republican. The site is also within blocks of Thomas Street Mini Park and the planned park at Summit and E John Street. At a broader scale, the site is located equidistant from Volunteer Park to the north and Cal Anderson Park to the south.



SITE PLAN



DESIGN GUIDELINE PRIORITIES

- A-1 Responding to Site Characteristics**
The siting of buildings should respond to specific site conditions and opportunities such as non-rectangular lots, location on prominent intersections, unusual topography, significant vegetation and views or other natural features.
- A-2 Streetscape Compatibility**
The siting of buildings should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.
- A-3 Entrances Visible from the Street**
Entries should be clearly identifiable and visible from the street.
- A-4 Human Activity**
New development should be sited and designed to encourage human activity along the street.
- A-6 Transition Between Residence and Street**
For residential projects, the space between the building and the sidewalk should provide security and privacy for residents and encourage social interaction among residents and neighbors.
- B-1 Height, Bulk and Scale**
Projects should be compatible with the scale of development anticipated by the applicable Land Use Policies for the surrounding area and should be sited and designed to provide a sensitive transition to near-by, less-intensive zones.
- C-2 Architectural Concept and Consistency**
Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept. Buildings should exhibit form and features identifying the functions within the building.
- C-5 Structure Parking Entrances**
The presence and appearance of garage entrances should be minimized so that they do not dominate the street frontage of a building.
- D-2 Blank Walls**
Buildings should avoid large blank walls. Where blank walls are unavoidable, they should receive design treatment to increase pedestrian comfort and interest.
- E-3 Landscape Design to Address Special Site Characteristics**
The landscape design should take advantage of special on-site conditions such as high-blank front yards, steep slopes, view corridors, or existing significant trees and off-site conditions such as greenbelts, ravines, natural areas, and boulevards.

RESPONSE TO DESIGN PRIORITIES

(B-1) Similar Height, Bulk and Scale to Surrounding Multi-family Context

(C-2) Horizontal Wood Siding;Visually Connecting Stacking Units

(C-2) Brick Frame Envelopes the 2-story Spaces at Ground Level

(C-2/C-5) Recessed Building at Garage Entry; De-emphasing Vehicular Entry

(A-2/C-5) 18'-0" Decreased Driveway Width Flanked by Planters

(A-3) Prominent Lobby with Large Glazed Wall to Emphasize Pedestrian Entry

(A-3) Wide Staircase at Residential Entry

(A-2/A-4/C-2) Multi-Story Street-Related Units

(A-2/A-4) Direct Pedestrian Entrances and Patios along Sidewalk

(D-2) Large Glazed Wall at Ground Related Units

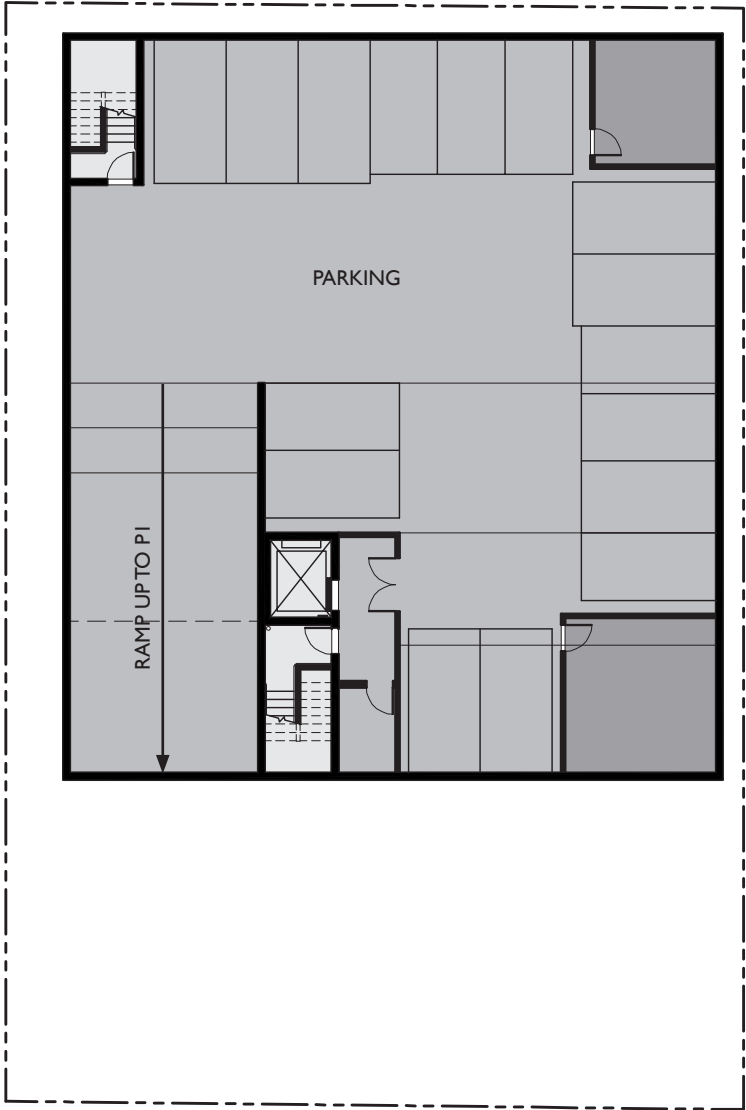
(A-1/E-3) Existing Mature Street Trees to be Replaced per City of Seattle Request

(A-4/A-6) Low Planter Walls Encourage Human Interaction while also Providing a Safety Buffer

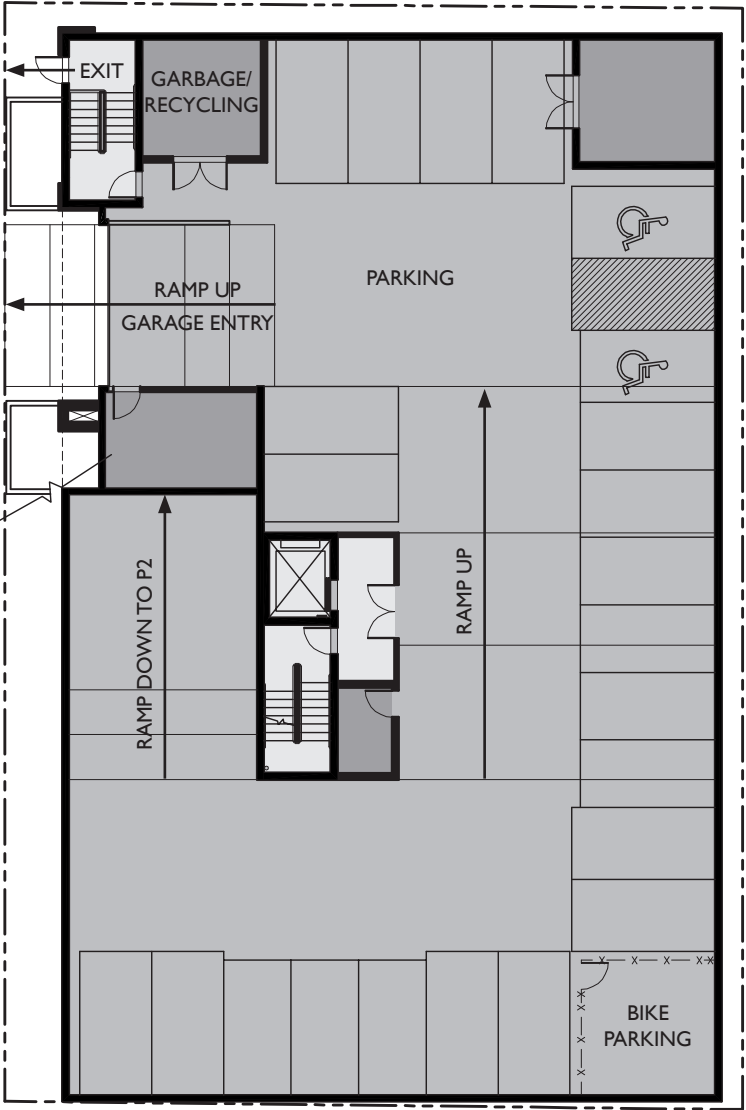


PROPOSED FLOOR PLANS

LEVEL P2



LEVEL P1



LEVEL I

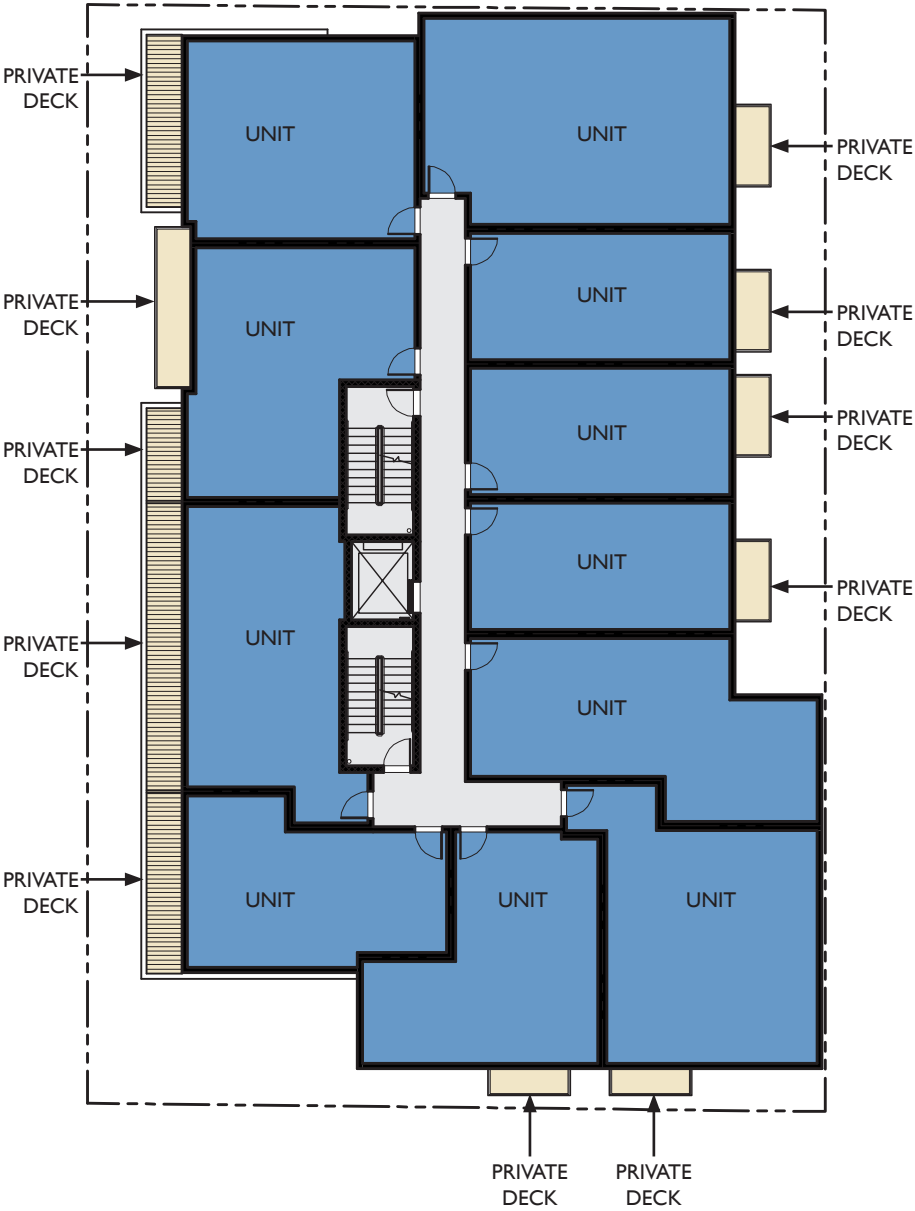


BELMONT AVENUE E

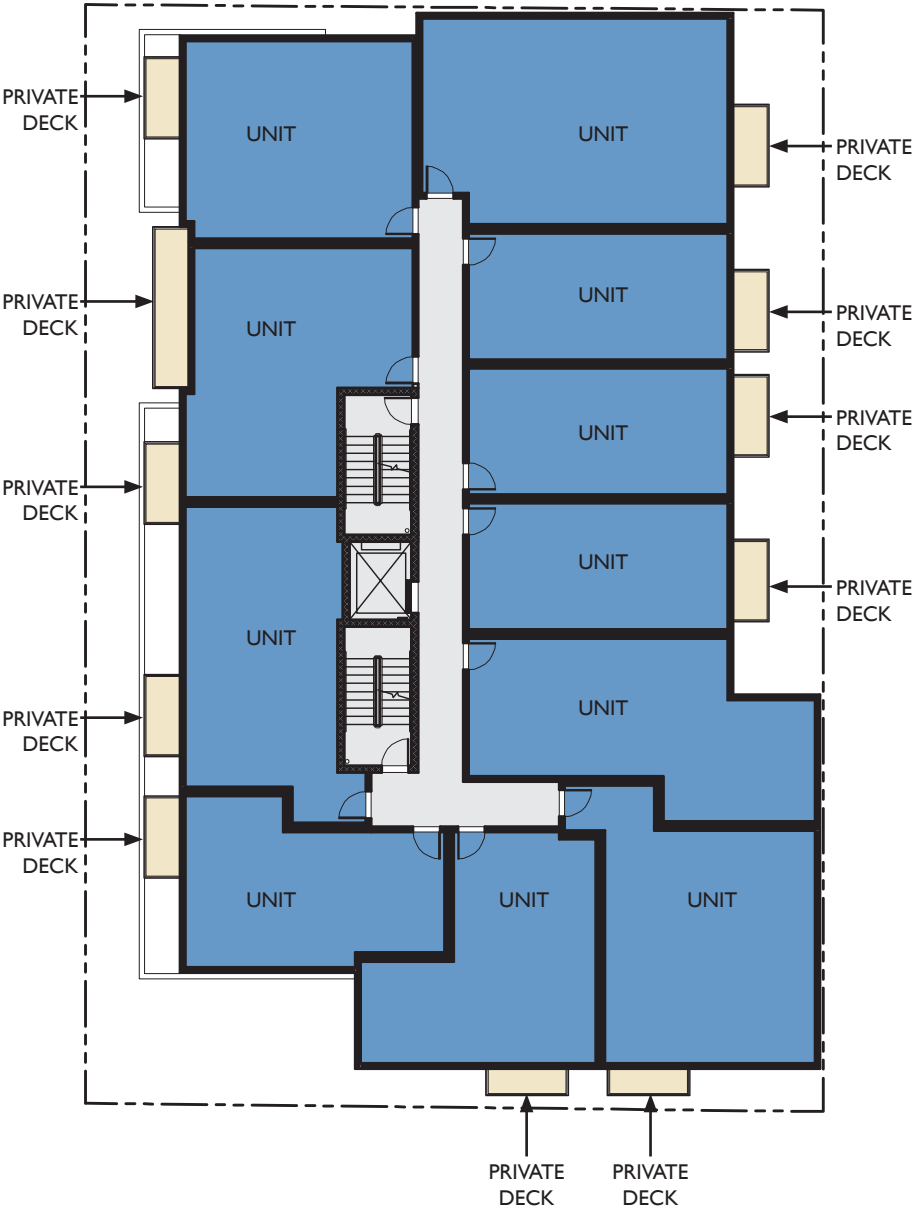
E REPUBLICAN ST



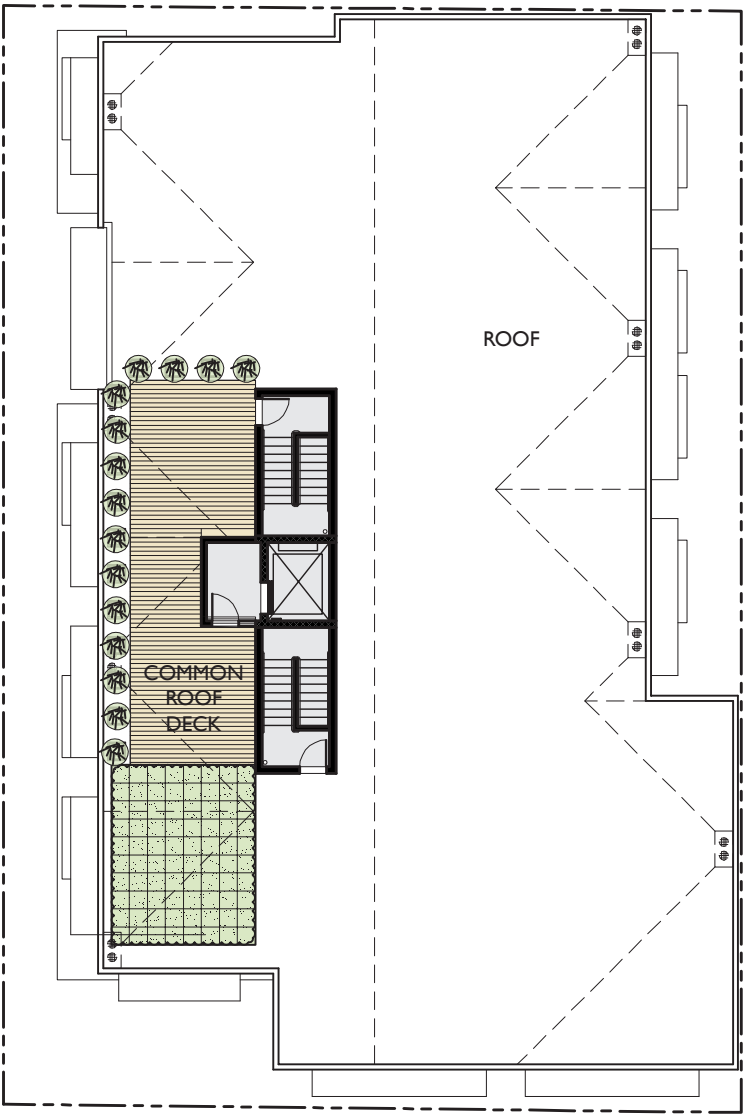
LEVEL 2



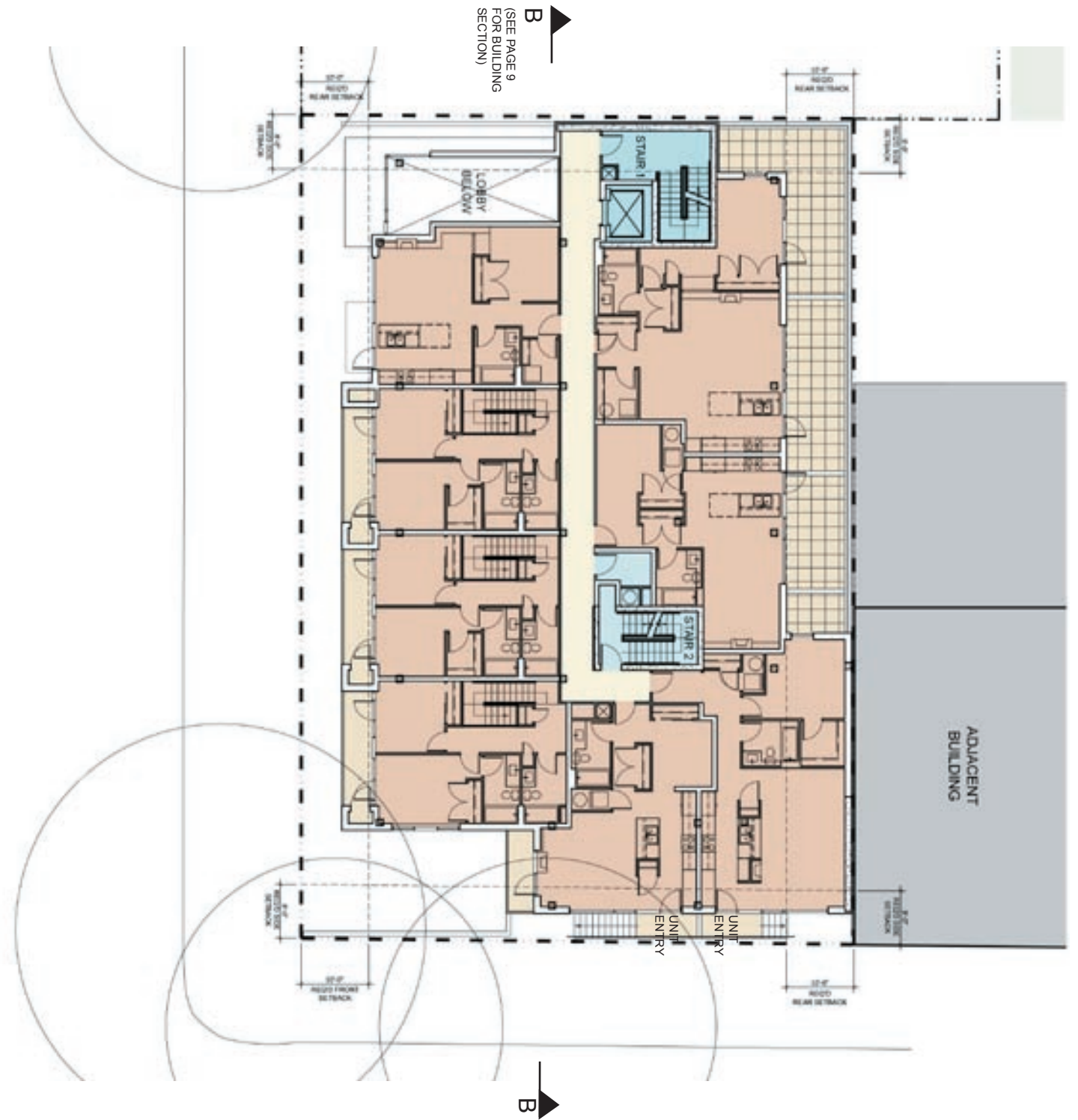
LEVEL 3-6



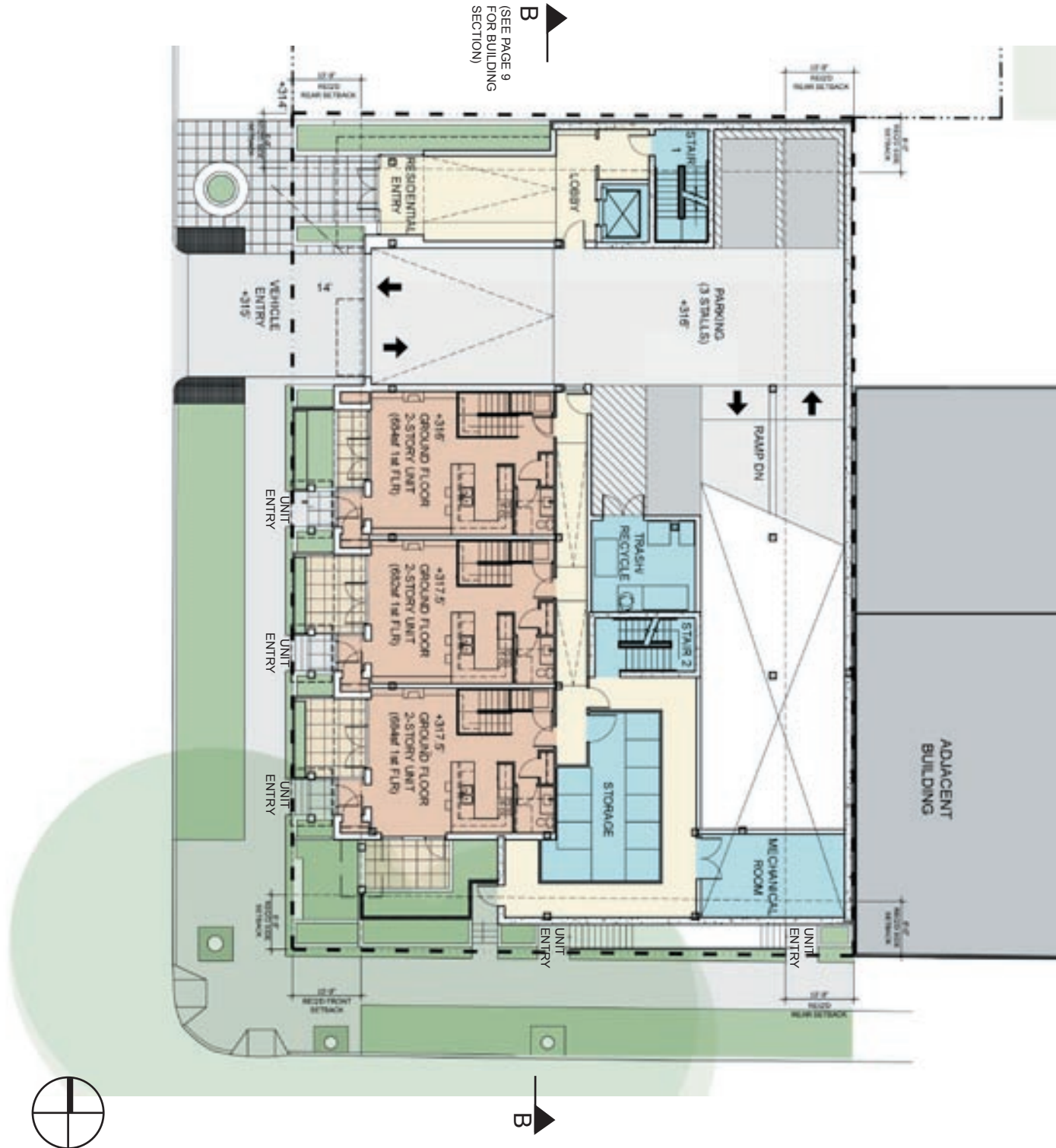
ROOF



LEVEL 2



LEVEL I

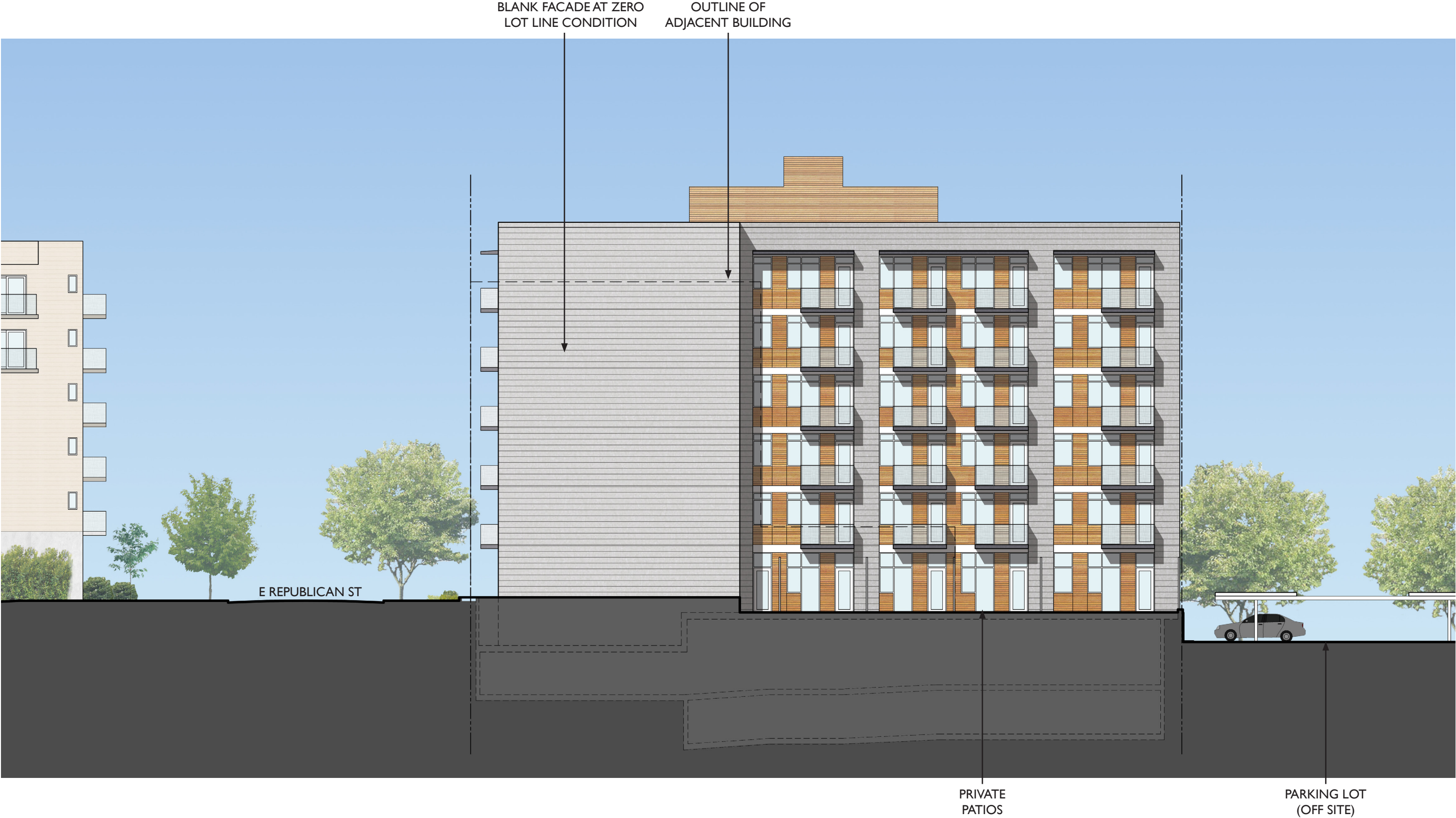


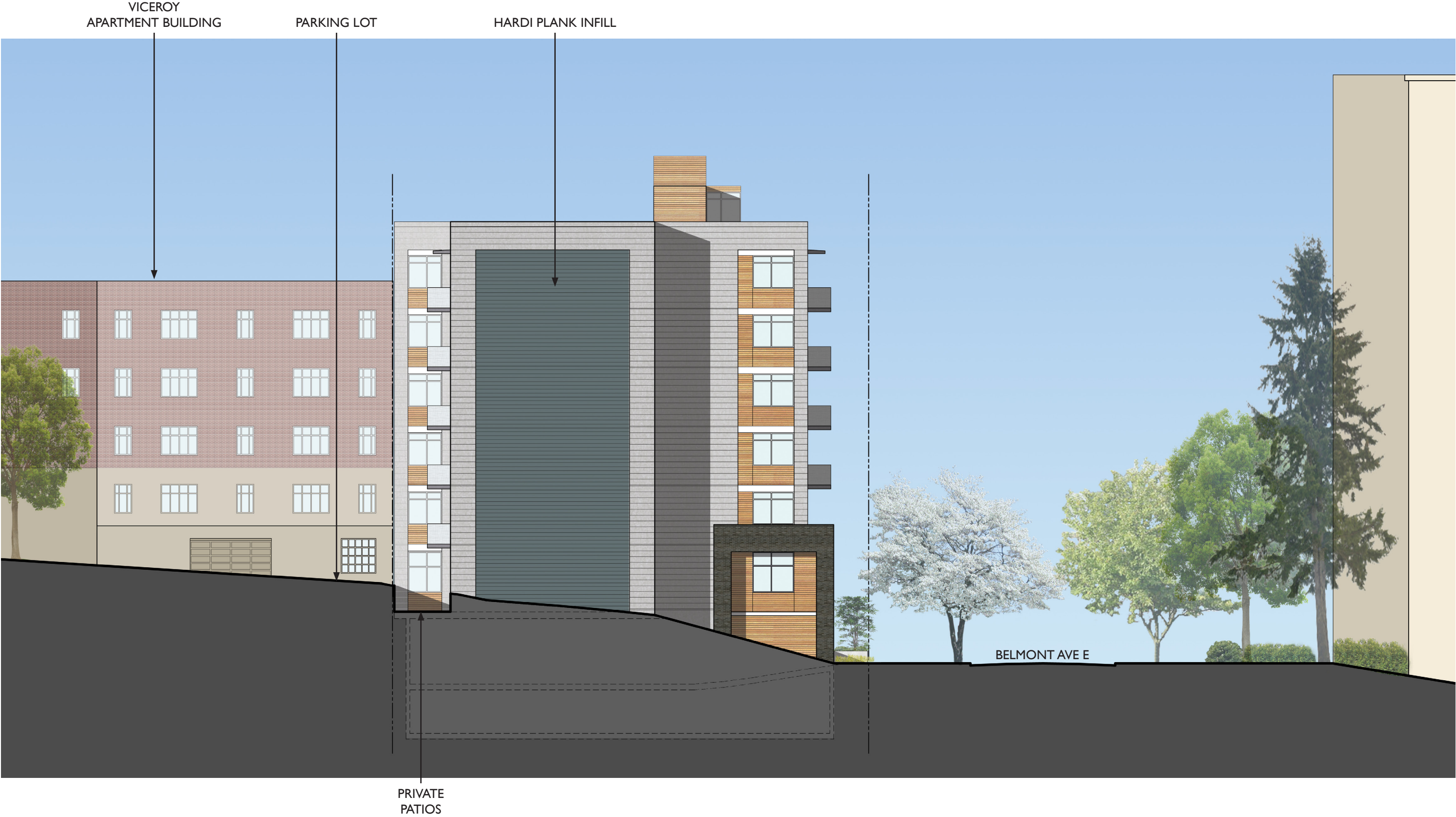
SCHEME A PROPOSED WEST ELEVATION





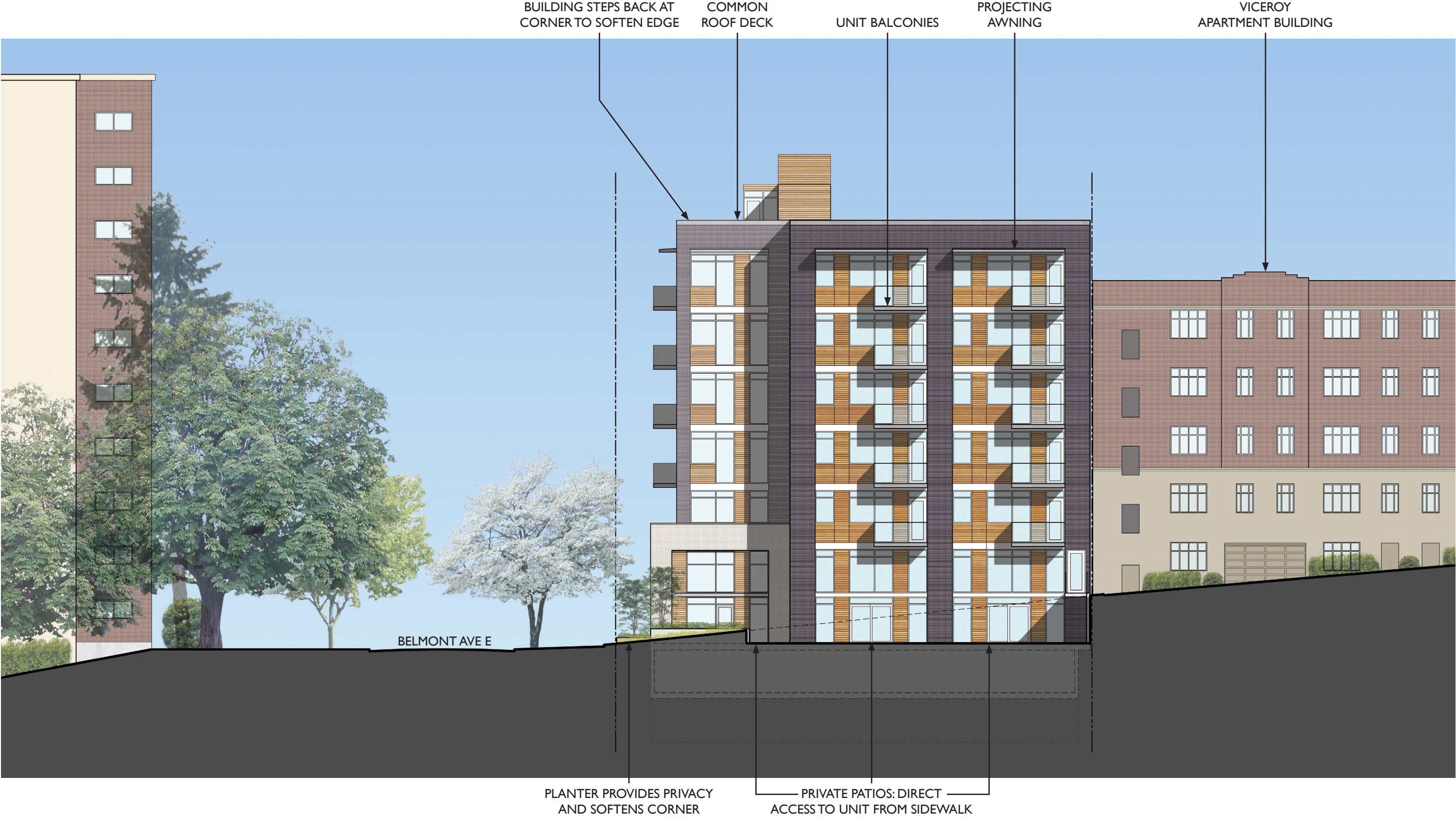
SCHEME A PROPOSED EAST ELEVATION



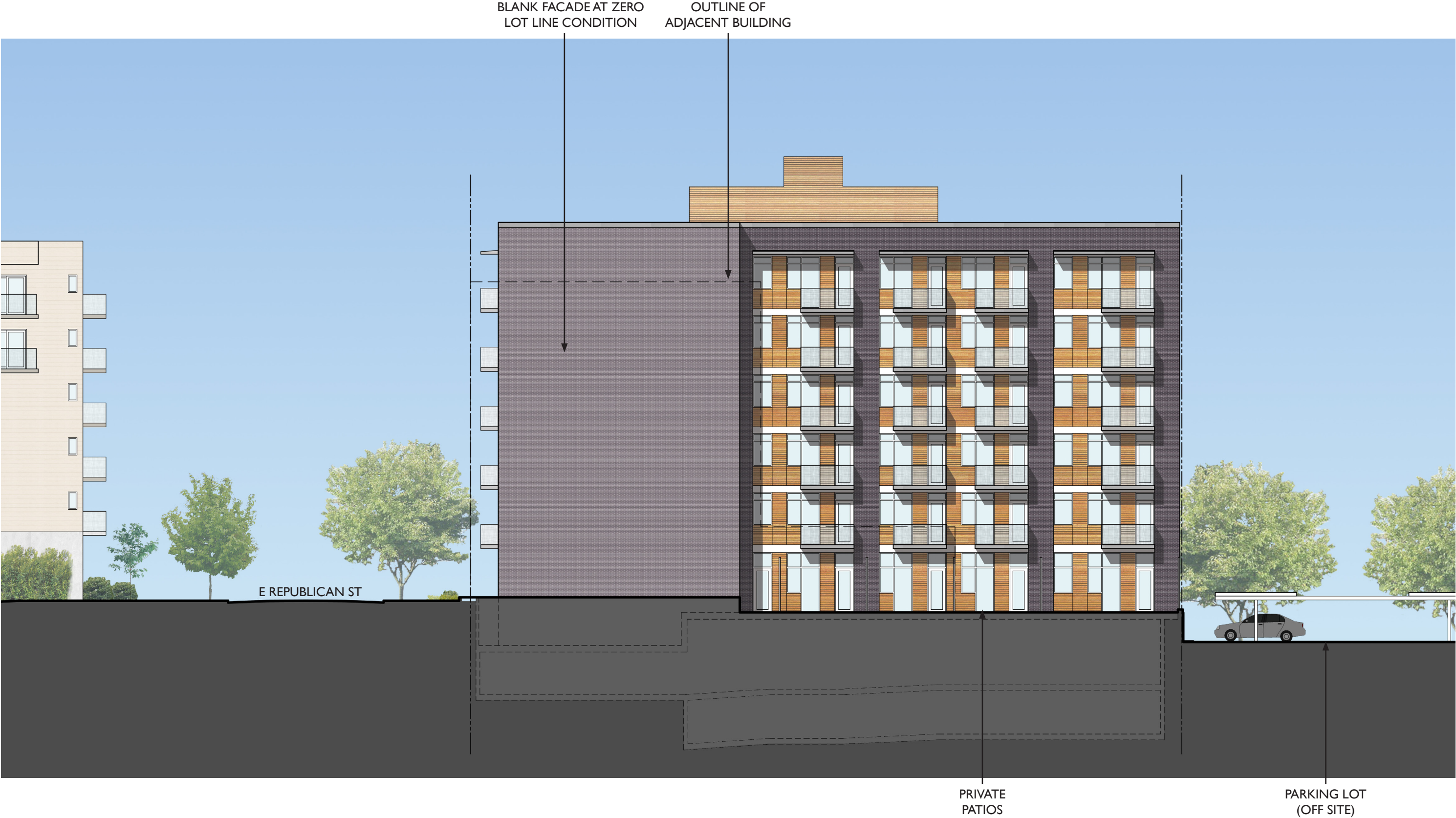


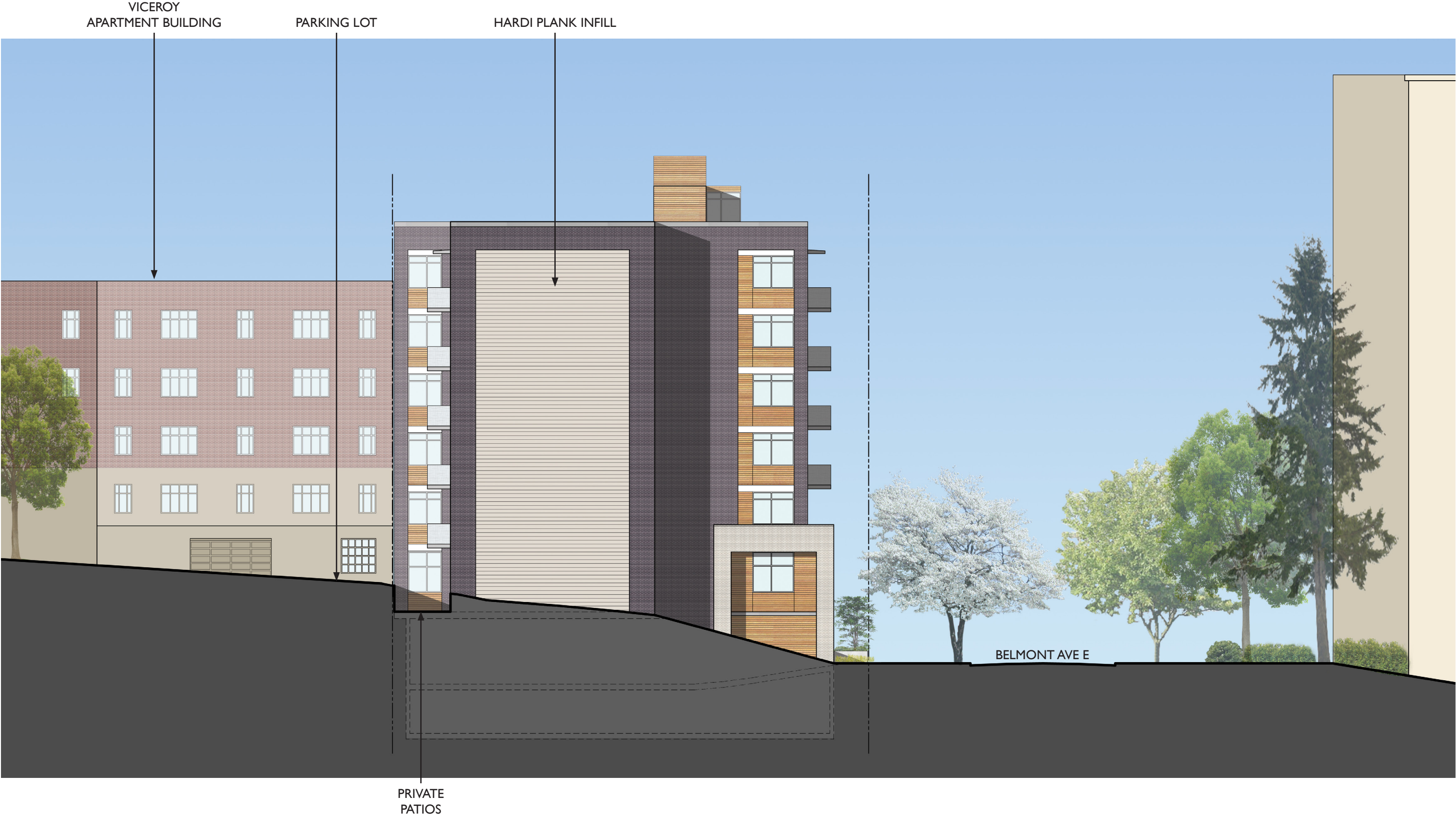
SCHEME B PROPOSED WEST ELEVATION





SCHEME B PROPOSED EAST ELEVATION







WEST ELEVATION



SOUTH ELEVATION

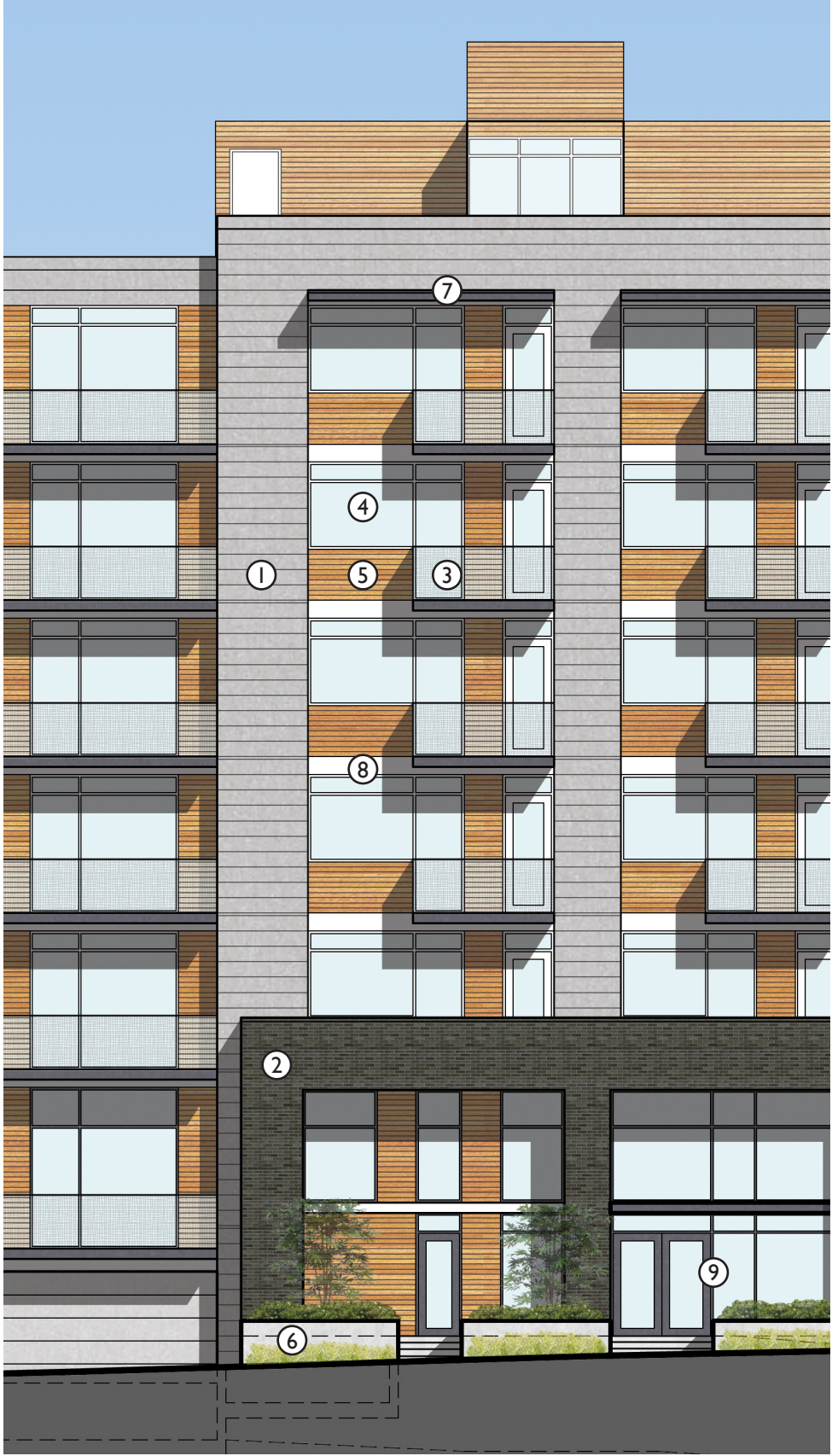


NORTH ELEVATION



EAST ELEVATION

SCHEME A PROPOSED EXTERIOR PALETTE



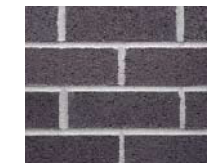
MATERIALS

- ① Flat Metal Panel Siding:
AEP Span Prestige Panel, Cool Metallic Silver
- ② Brick Veneer:
Dark Iron Spot
- ③ Metal Deck with Transparent Glass Guardrails
- ④ Vinyl Window/Door System
- ⑤ Horizontal Cedar Planking:
(1X6) Stained with Sikkins Cetol 2/3, Light Natural with
Satin Finish, 1/4" Gap
- ⑥ Cast-In-Place Concrete Planter with
Cementitious Coating
- ⑦ Metal Bris Soleil to match Metal Deck
- ⑧ White Metal Panel
- ⑨ Storefront Window/Door System



SCHEME B PROPOSED EXTERIOR PALETTE

MATERIALS



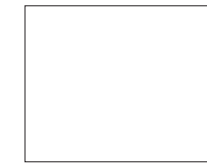
① Brick Veneer:
Ebony



② Brick Veneer:
Limestone



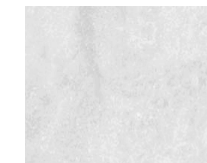
③ Metal Deck with Transparent Glass Guardrails



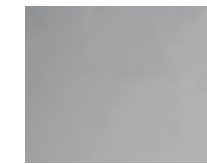
④ Vinyl Window/Door System



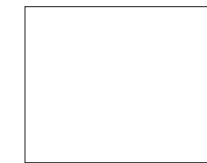
⑤ Horizontal Cedar Planking:
(1X6) Stained with Sikkins Cetol 2/3, Light Natural with
Satin Finish, 1/4" Gap



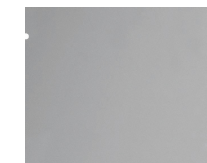
⑥ Cast-In-Place Concrete Planter with
Cementitious Coating



⑦ Metal Bris Soleil to match Metal Deck



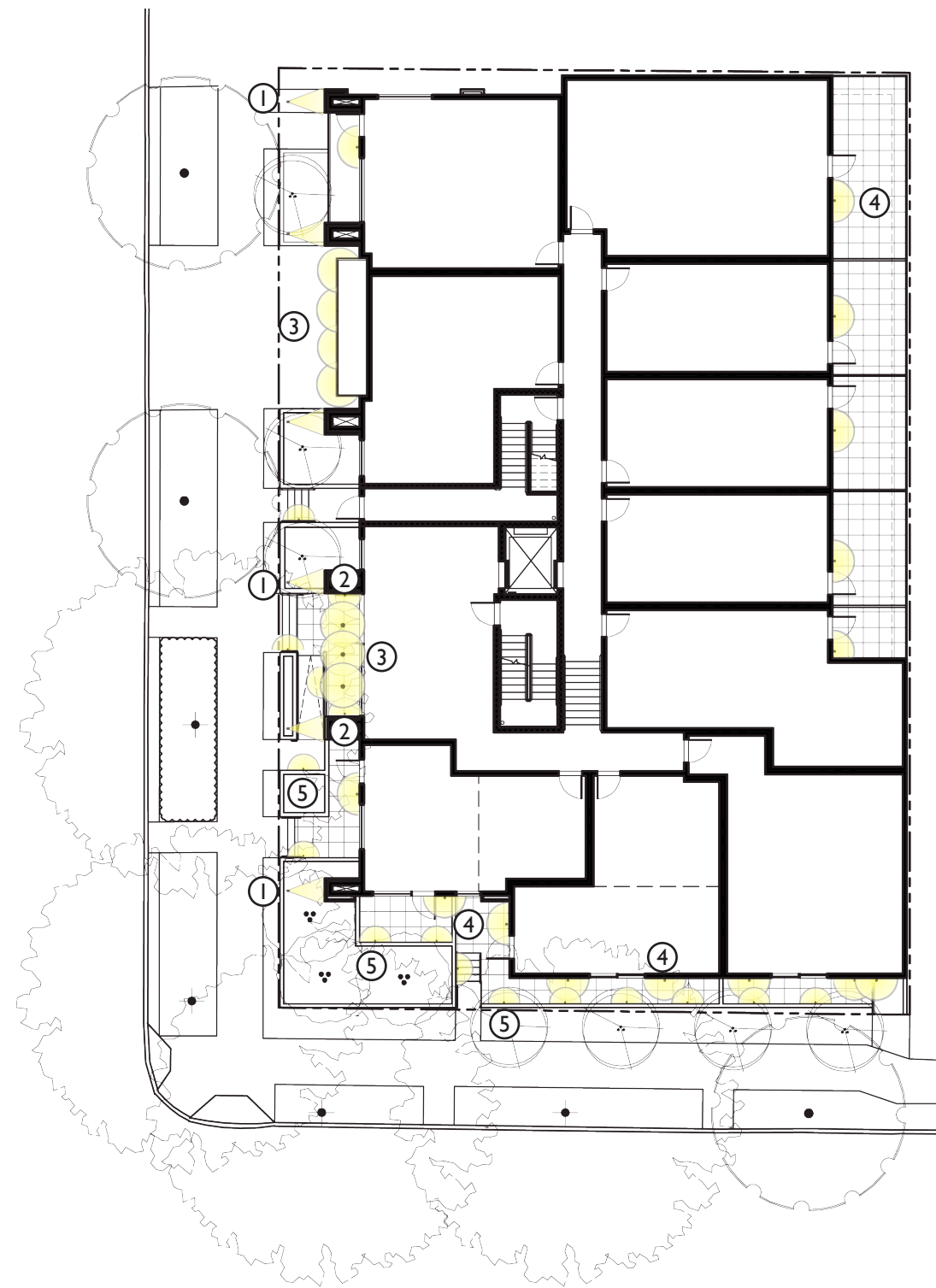
⑧ White Metal Panel



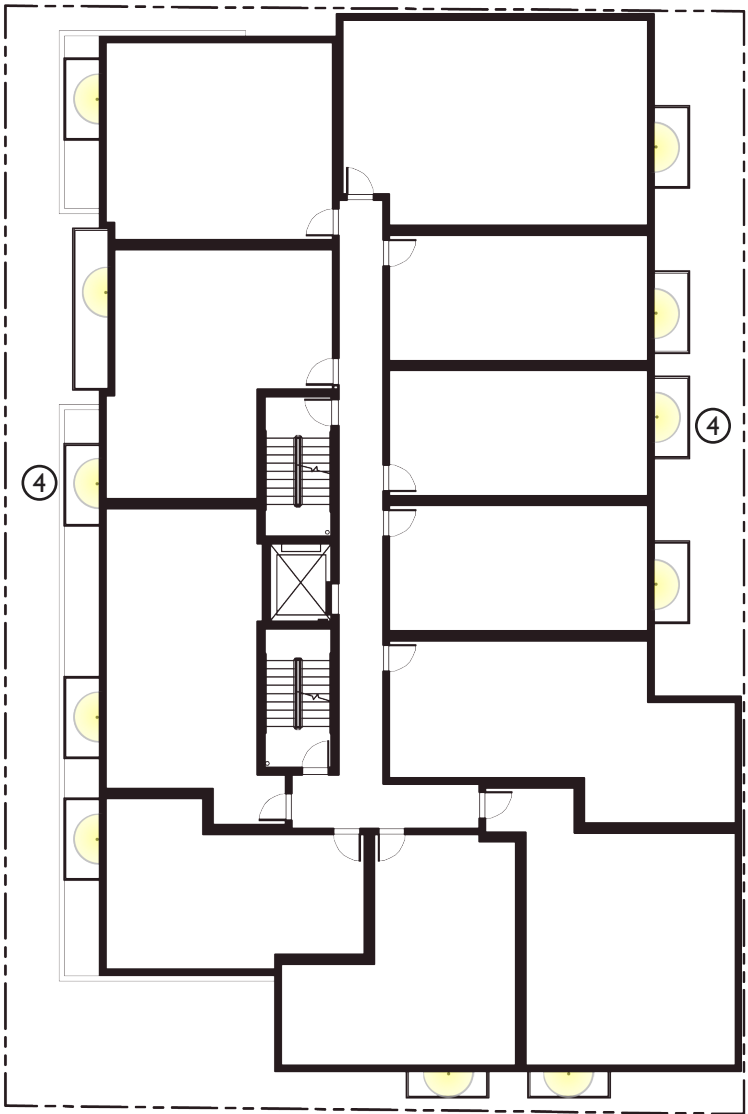
⑨ Storefront Window/Door System

PROPOSED LIGHTING PLAN

STREET LEVEL LIGHTING PLAN



RESIDENTIAL LEVEL LIGHTING PLAN



LIGHT FIXTURES



① Level I Uplighting
Bega Small Scale Floodlight



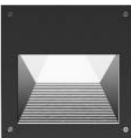
② Entry Area
Prescolite Litebox Recessed Wall Washer



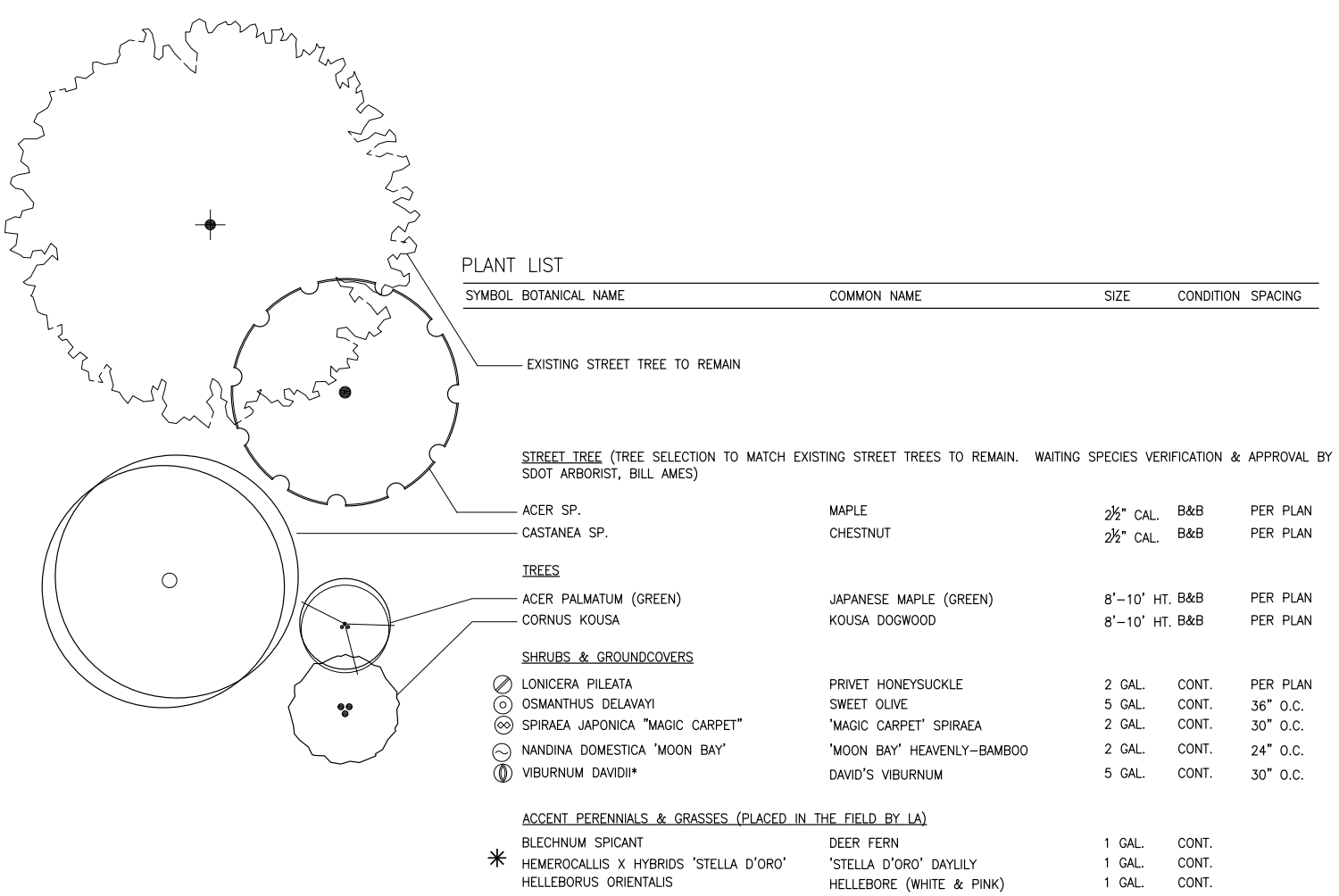
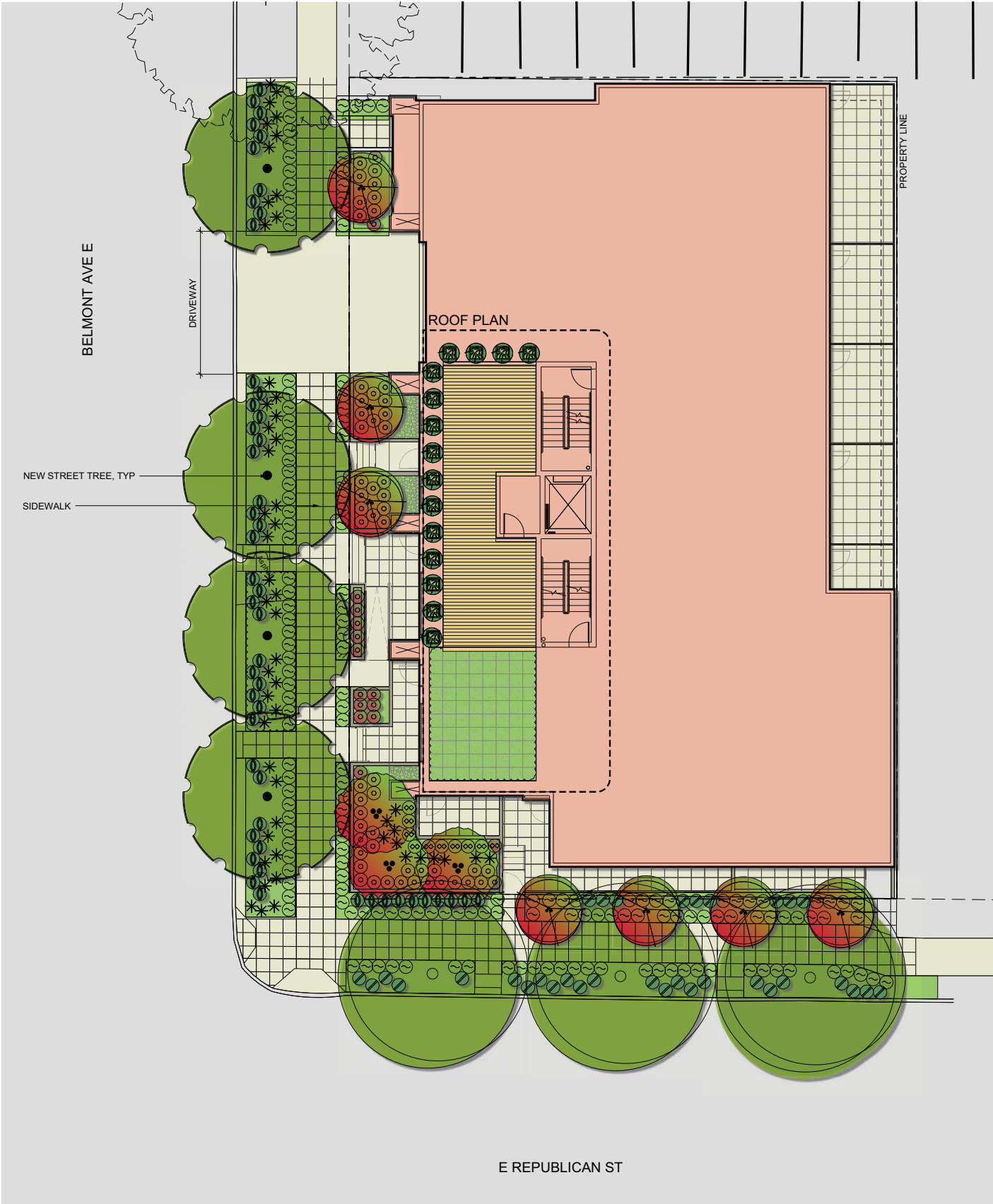
③ Garage Entry, Entry Area
Prescolite Litebox Recessed Downlight with Black Baffle



④ Unit Entry Doors, Private Decks
Prescolite Liteforms Wall Mount Cylinder



⑤ Common Exterior Walkway Footlights, LI Private Patios
Bega Recessed Wall with Linear Spread Diffuser



- (A-1/E-3) Existing Mature Street Trees to be Replaced per City of Seattle Request
- (C-2) Brick Frame Envelopes the 2-story Spaces at Ground Level
- (A-3) Prominent Lobby with Large Glazed Wall to Emphasize Pedestrian Entry
- (A-2/A-4/C-2) Multi-Story Street-Related Units
- (D-2) Large Glazed Wall at Ground Related Units
- (A-3) Wide Staircase at Residential Entry
- (A-4/A-6) Low Planter Walls Encourage Human Interaction while also Providing a Safety Buffer
- (A-2/A-4) Direct Pedestrian Entrances and Patios along Sidewalk



STREETSCAPE: BELMONT AVENUE EAST



STREETSCAPE: E REPUBLICAN AVENUE



NORTHEAST COURTYARD

ATTACHMENT B: RESPONSE TO GUIDELINES

A-1 RESPONDING TO SITE CHARACTERISTICS

The siting of buildings should respond to specific site conditions and opportunities such as non-rectangular lots, location on prominent intersections, unusual topography, significant vegetation and views or other natural features.

Early Design Guidance:

- The Board strongly supports the intention of the applicant to preserve the mature street trees which contribute to the distinctive character of the neighborhood.

Response:

- The intention was to preserve the existing street trees, however the City arborist and a private arborist both recommend the removal of all 4 of the existing street trees after observing the amount of decay and damage imposed upon them. The lanscape architect has proposed larger than standard caliper trees, that have been approved by the City arborist.

A-2 STREETScape COMPATIBILITY

The siting of buildings should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.

Capitol Hill-specific supplemental guidance:

- Vehicle entrances to buildings should not dominate the streetscape.
- Orient townhouse structures to provide pedestrian entrances to the sidewalk.
- For buildings that span a block and ‘front’ on two streets, each street frontage should receive individual and detailed site planning and architectural design treatments to complement the established streetscape character.

Early Design Guidance:

- The Board also supported the concept of ground level units with connection to the street on Belmont Avenue East and recommended that the applicant consider making a similar connection from any units on East Republican taking cue from the existing porches on the street. The Board noted that to achieve the street connection for these units, a lower and more transparent form of privacy screen should be employed instead of fences or high concrete privacy walls.

Design Review:

- The Board noted the lower and more transparent privacy screen for the ground level units in the form of metal planters with garden type plantings. They were also pleased with the added unit entrances for the units facing on East Republican and agreed that they should be enhanced.

Response:

- The original MUP was granted a departure for narrowing the driveway width. The departure was encouraged by the Board in order to lessen the impact of the vehicular entrance on the street/sidewalk. The current design preserves the 18’-0” driveway for the same reason. Additionally, planters have been added, flanking either side of the driveway, and are therefore located within the sight triangles in order to deemphasize the vehicular access.

- The revised design proposes multi-story ground-related units along both Belmont Avenue E and E Republican Street. The unit along Belmont Ave E is buffered by a set of stairs and a planter in order to give the occupant privacy, however the porch is not blocked by a high fence or wall which encourages a connection to the passers by. The units along E Republican are sunken below grade, creating a natural private situation, however a connection to the sidewalk is still maintained through front porches/patios. The southwestern unit has direct access to the patio and unit through a set of stairs while the southeastern unit has bridge access directly into the second level of the unit from the sidewalk. The southwestern unit also has a sunken patio which provides an opportunity for the occupant to make a connection with the public, while also maintaining the occupant’s privacy.

A-3 ENTRANCES VISIBLE FROM THE STREET

Entries should be clearly identifiable and visible from the street.

Early Design Guidance:

- The Board indicated that they would like to see a more open and shallow entry to the lobby and elevator area than proposed in the preferred alternative and that the design should engage the corner location. The applicant should consider providing greater visibility, height and access to light for the entry.

Design Review:

- At the Recommendation Meeting, the Board responded very positively to the re-designed building. They were pleased to see that the lobby entrance was relocated to the north end of the building as proposed at the second EDG meeting and features a prominent 2-story glazed façade. This location adjacent to the vehicle entrance is bracketed by low concrete planter which the Board insisted should remain.

Response:

- The pedestrian entrance on Belmont Avenue E is emphasized by a prominent 2-story glazed facade and is flanked by a wide set of stairs and an accessible ramp creating greater visibility, height and access to light, as requested during the EDG meeting. The current design pulls the lobby entrance away from the vehicular entrance for safety and practical design reasons.

A-4 HUMAN ACTIVITY

New development should be sited and designed to encourage human activity along the street.

Response:

- The multi-story ground-related units along Belmont Ave E and E Republican have a strong connection to the street/sidewalk. The units have glass or transparent railings or low planter walls between the private space and the sidewalk as well as direct access from the sidewalk which naturally encourages human interaction of passers by.

A-6 TRANSITION BETWEEN RESIDENCE AND STREET

For residential projects, the space between the building and the sidewalk should provide security and privacy for residents and encourage social interaction among residents and neighbors.

Response:

- See A-2 and A-4 above.

B-1 HEIGHT, BULK AND SCALE

Projects should be compatible with the scale of development anticipated by the applicable Land Use Policies for the surrounding area and should be sited and designed to provide a sensitive transition to near-by , less-intensive zones.

Early Design Guidance:

- The Board is willing to entertain the requests for departures for structure width and depth and is looking forward to a design whose bulk and scale fits well with the character and scale of the neighborhood.

Design Review:

- The Board agreed that the departures contribute to the design of the building and the scale is appropriate for an urban multi-family structure in this neighborhood.

Response:

- For the most part, the overall building massing, modulation and form have been derived from the original MUP. As was noted in the Design Recommendation meeting, the scale of this building is appropriate for this urban setting. Currently, there is a 5-story concrete multi-family structure with a zero lot line condition to the east; a 6-story vinyl-sided multi-family structure to the south; a 10-story multi-family structure to the west; and a parking lot directly north of the project site. This building is of similar height, bulk and scale to all those building surrounding it, with the exception of the parking lot to the north, which, if developed, could have a similar scale.

C-2 ARCHITECTURAL CONCEPT AND CONSISTENCY

Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept. Buildings should exhibit form and features identifying the functions within the building.

Early Design Guidance:

- The Board looks forward to reviewing a more detailed, high quality material and color palette.

Design Review:

- At the Recommendation meeting, a color and materials board was presented. The main building body is proposed to be a red brick blend with a charcoal brick for the base. The vinyl residential widows are shown as a pewter grey color. There was considerable concern expressed about the corrugated siding on the stairwell just nine inches from the north property line and the ability of some future development to build adjacent to this. As the siding is simply attached to the all

concrete stairwell structure the Board agreed that this material would be appropriate as long as it could be removed if necessary.

Response:

- For the most part, the overall building massing, modulation and form have been derived from the original MUP. The building design consists of large overall vertical elements, comprised of windows, decks and horizontal wood siding, visually connecting stacking units. The vertical elements are flanked by ‘columns’ making up the base building. This vocabulary is carried out on all four sides of the structure. At the base of the structure, the vertical elements are broken up by a heavy brick frame, flanking the ground related entrances (the lobby, building exit, and residential units), accentuating the pedestrian entrances to the building and deemphasizing the vehicular entrance.
- The current design proposes either a flat-profiled grey metal panel for the main building body and black brick for the base frame or a dark brick for the main building and a light brick for the base frame. In both schemes, both the base and the main building are accented by horizontal wood paneled siding, metal bolt on decks, glass deck rails and vinyl residential windows.

C-5 STRUCTURED PARKING ENTRANCES

The presence and appearance of garage entrances should be minimized so that they do not dominate the street frontage of a building.

Early Design Guidance:

- The Board strongly agreed that the vehicular access to the site should be visually minimized and cause as little disruption to pedestrian circulation around the site as possible. The Board would encourage the applicant to reduce the width of vehicle entrance on Belmont Avenue East and would entertain a departure request for a narrower than required garage entrance.

Design Review:

- The Board noted that the vehicle entrance is reduced to 18 feet and approved the departure request.

Response:

- The original MUP was granted a departure for narrowing the driveway width. The departure was encouraged by the Board in order to lessen the impact of the vehicular entrance on the street/sidewalk. The current design preserves the 18’-0” driveway for the same reason. Additionally, planters have been added, flanking either side of the driveway, and are therefore located within the sight triangles in order to deemphasize the vehicular access.

D-2 BLANK WALLS

Buildings should avoid large blank walls. Where blank walls are unavoidable, they should receive design treatment to increase pedestrian comfort and interest.

Early Design Guidance:

- The Board noticed that the proposed design for the south facing exterior wall of the ground-related units presented a blank wall adjacent

to the entry. The Board feels that there should be visual relief for this wall and suggested introducing upper level windows into this unit which would also afford views to the southwest.

- The Board also encouraged the applicant to consider an alternative design to the prominent stairwell portion on the north side of the proposed structure. As proposed, it presents an uninterrupted blank wall from ground level to roof. At the next meeting, the Board would like to review a design that pulls the stairwell into the main structure so as not to be so prominent.

Design Review:

- The Board was pleased to see the redesign of the south-facing wall of the ground level units with the added windows and additional access to a private patio space where the former lobby entrance was located. The remainder of the corner space has been landscaped and benches provided for pedestrian respite.
- The north stairwell has been pulled more into the bulk of the building and a corrugated siding is proposed. See discussion above.

Response:

- There is no longer a blank wall situation at the south facing exterior wall of the ground-related units. All the ground-related units and entrances have large amounts of glazing, including the units that fall below grade. The below-grade units have a sunken patio in front of them, creating an opportunity to add glazing to the façade as well as creating a private patio space where the unit occupant can have private exterior space as well as connect with passers by.
- While there is no longer a stair along the north end of the building, the building still has an almost zero lot line condition along the north property line, similar to the location of the wall in the original MUP. The units along the north property line are pushed north in order to gain larger window opportunities for views to the east and west. This creates better opportunities within the units for higher quality living space. If the lot to the north remains a parking lot, the building as designed is innocuous to the parking lot, and if the lot is developed in the future, the almost zero lot line condition benefits the units in the proposed project as their views to the east and west will be preserved.

E-3 LANDSCAPE DESIGN TO ADDRESS SPECIAL SITE CONDITIONS

The landscape design should take advantage of special on-site conditions such as high-blank front yards, steep slopes, view corridors, or existing significant trees and off-site conditions such as greenbelts, ravines, natural areas, and boulevards.

Early Design Guidance:

- The Board placed a high value on the retention of the existing mature street trees. See A-6.

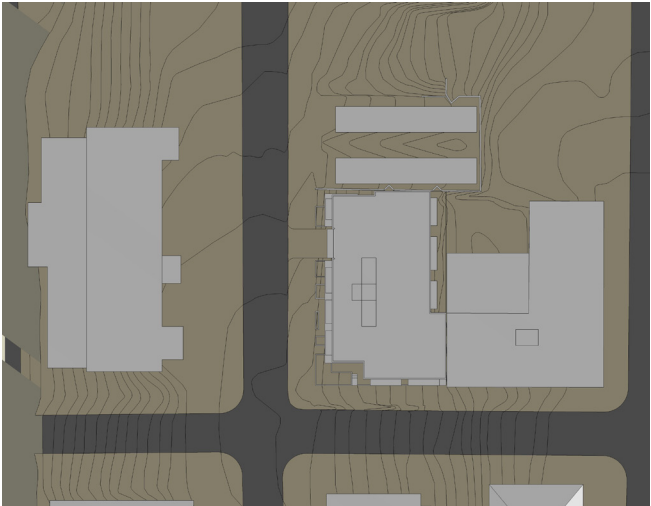
Design Review:

- The Board was pleased with the landscape design and applicants’ assurance that the existing mature street trees should be protected as recommended.

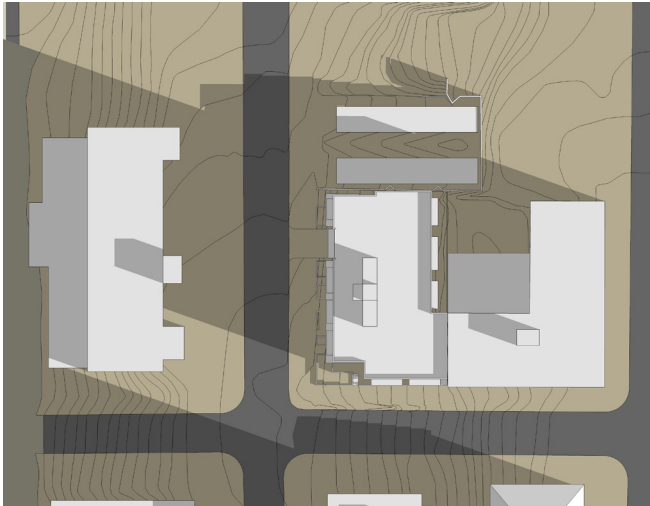
Response:

- The intention was to preserve the existing mature street trees. However, the City arborist and private arborist have both recommended the removal of the trees after observing the amount of decay and damage imposed upon them. The current landscape design emphasizes the pedestrian entrances and deemphasizes the vehicular entrance.

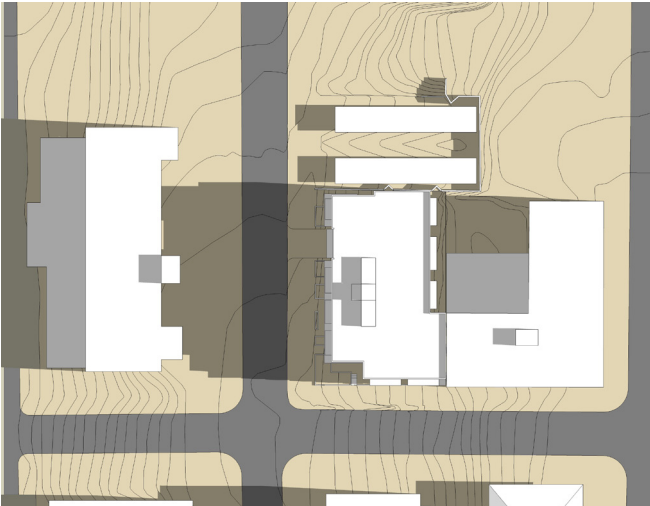
SHADOW STUDY



DECEMBER 21 - 8 AM



MARCH 21 - 8 AM



JUNE 21 - 8 AM



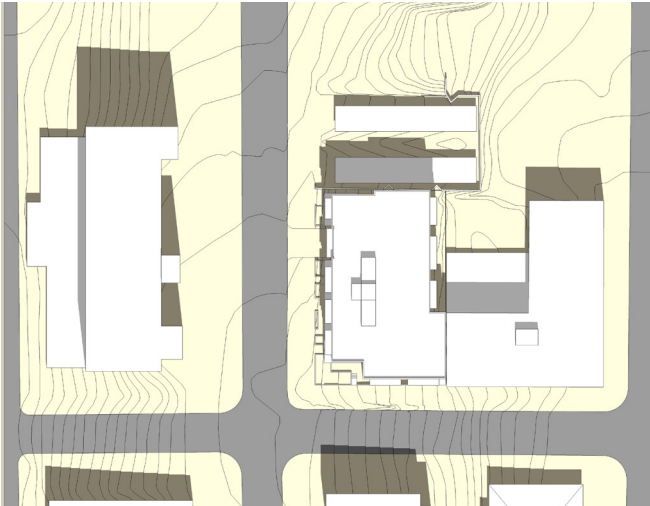
SEPTEMBER 21 - 8 AM



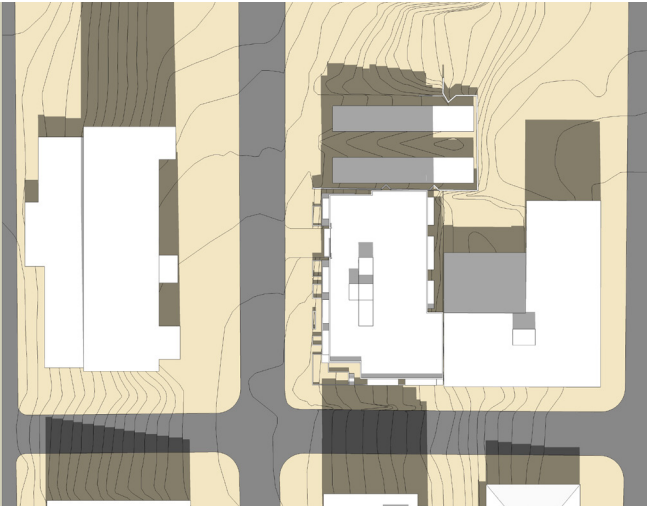
DECEMBER 21 - 12 PM



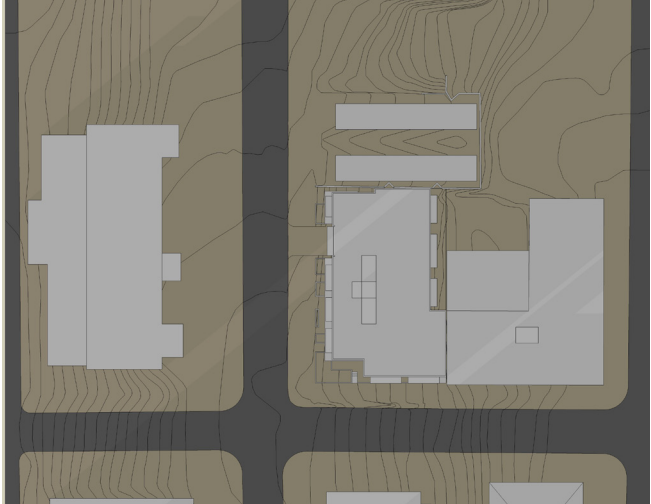
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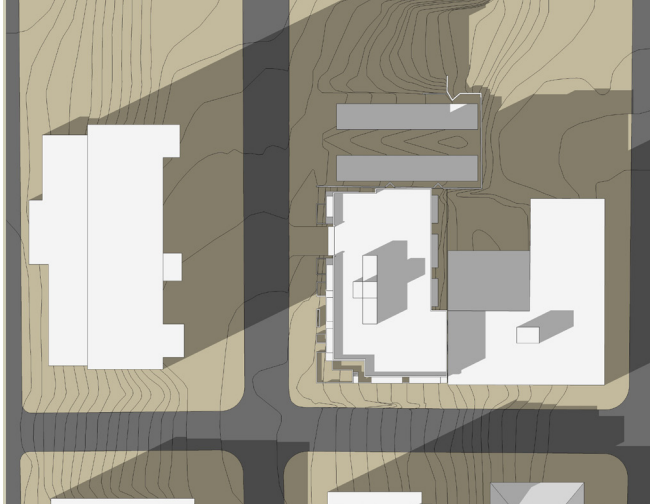
JUNE 21 - 12 PM



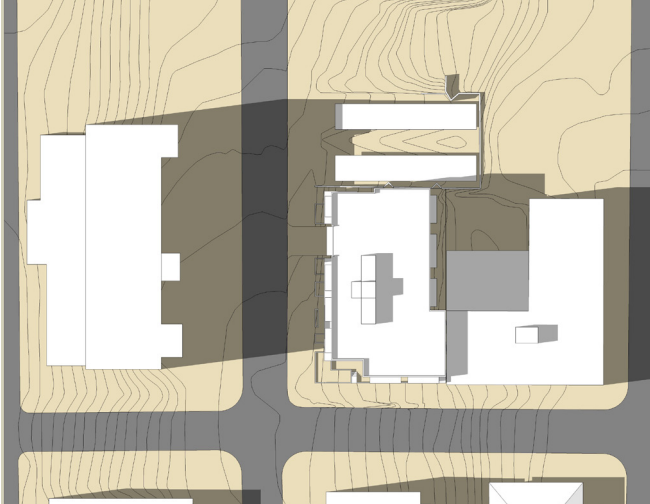
SEPTEMBER 21 - 12 PM



DECEMBER 21 - 4 PM



MARCH 21 - 4 PM



JUNE 21 - 4 PM



SEPTEMBER 21 - 4 PM



WESTLAKE VILLAGE



SALVEO - LEED H PLATINUM



CHELAN RESORT SUITES



THE DAKOTA



222 VIEW APARTMENTS



H2O APARTMENTS - LEED H MIDRISE PILOT GOLD TARGET



BROADSTONE KOI APARTMENTS - LEED NC CERTIFIED TARGET



OLIVEWAY MIXED-USE APARTMENTS - LEED NC SILVER TARGET



MIST APARTMENTS - LEED NC SILVER TARGET

STREAM BELMONT - DPD #3012378

DESIGN REVIEW