

# 15th LIVE / WORK

355 15th Ave Seattle WA 98122

Design Review

January 15 2013

CHRIS  
PARDON  
DESIGN

ELEMENTAL  
ARCHITECTURE



# SEATTLE ZONING CODE ANALYSIS

Development Standard Departures			
Development Standard	Design Departure Request	Justification	Board Action
<p><b>23.54.030-G.7.</b></p> <p>Sight triangles are required for two-way entrances into a parking garage or surface parking area.</p>	<p>Convex mirrors at driveway exit, one each side. The driveway, for entry from, and exit to, 15th Avenue is designated two-way. It is 14 feet wide and adjacent to the south property line. Site triangle for exit to south could come from adjacent property, as the front setback for a property zoned R-5000, is 25 feet. Provide convex mirror at both south and north sides of exit from 14 foot drive way.</p>	<p>Minimal disturbance to the existing adjacent property. Both driveway and/or curb cut are appropriately scaled for adjacent residential zoning.</p>	
<p><b>23.54.015, Table A, Line D.</b></p> <p>Parking is not required for this project as it consists of eight (8) Live/Work units, each less than 1,500 SF.</p>	<p>No parking is required.</p>	<p>Parking provided for each resident of the Live/Work units, one (1) space per unit. Each "space" is 8 feet wide by 16 feet deep, and complies with SMC Section 23.54.030. Back up space is a minimum of 24 feet and complies with SMC Section 23.54.030 C. 1.</p>	
<p><b>23.047A.032-A.1.c.</b></p> <p>If access not provided from alley, and site abuts two streets, access is permitted from side street.</p>	<p>Access IN and OUT provided from side street (15th Avenue).</p>	<p>No curb cut on front street (Jefferson Street, per DPD, SMC 23.047.032.C). Residential scale curb cut and driveway width are consistent with the character of 15th Avenue.</p>	
<p><b>23.54.030-D.1.a.</b></p> <p>Driveways less than 100 feet in length that serve 30 or fewer parking spaces shall be a minimum of 10 feet in width for one-way or two-way traffic.</p>	<p>The driveway, for entry and exit from 15th Avenue, is 14 feet wide. Driveway is approximately 47 feet long. Slope of driveway is 15% or less.</p>	<p>Minimal disturbance to the existing adjacent property. The driveway is appropriately scaled for adjacent residential zoning</p>	

### UNIT MATRIX:

UNIT TYPE	I	II	III	IV	V	TYPE	OCC
*GFA - LOWER	435	-	-	435	319	I	R
L1	365	448	448	365	311	I	B
L2	435	521	397	435	435	V	R
L3	443	529	405	443	443	V	R
<b>GFA TOTAL</b>	<b>1282</b>	<b>1498</b>	<b>1250</b>	<b>1282</b>	<b>1189</b>		
<b>TOTAL</b>	<b>1717</b>	<b>1498</b>	<b>1250</b>	<b>1717</b>	<b>1508</b>		

\*FAR EXEMPT

## PROJECT INFORMATION:

ADDRESS: 355 15TH AVE N  
TAX ID NUMBER: 794260-0910  
ZONING: NC1-30  
OVERLAYS: 12TH AVENUE URBAN CENTER  
LOT AREA: 8,688 SF

### PROJECT DESCRIPTION:

**CREATE EIGHT (8) LIVE-WORK ROWHOUSE UNITS.**

### LEGAL DESCRIPTION:

LOT 1, LOT 2 LESS THE WEST 7 FEET OF THE NORTH 1/2  
AND LESS THE WEST 6 FEET OF THE NORTH 14 FEET OF  
THE SOUTH 1/2 AND THE SOUTH 10 FEET OF LOT 3, LESS  
ALLEY, BLOCK 11, SQUIRE PARK ADDITION TO THE CITY  
OF SEATTLE, ACCORDING TO THE PLAT THEREOF,  
RECORDED IN VOLUME 8 OF PLATS, PAGE 6, IN KING  
COUNTY, WASHINGTON.  
SITUATED IN THE CITY OF SEATTLE, KING COUNTY,  
STATE OF WASHINGTON

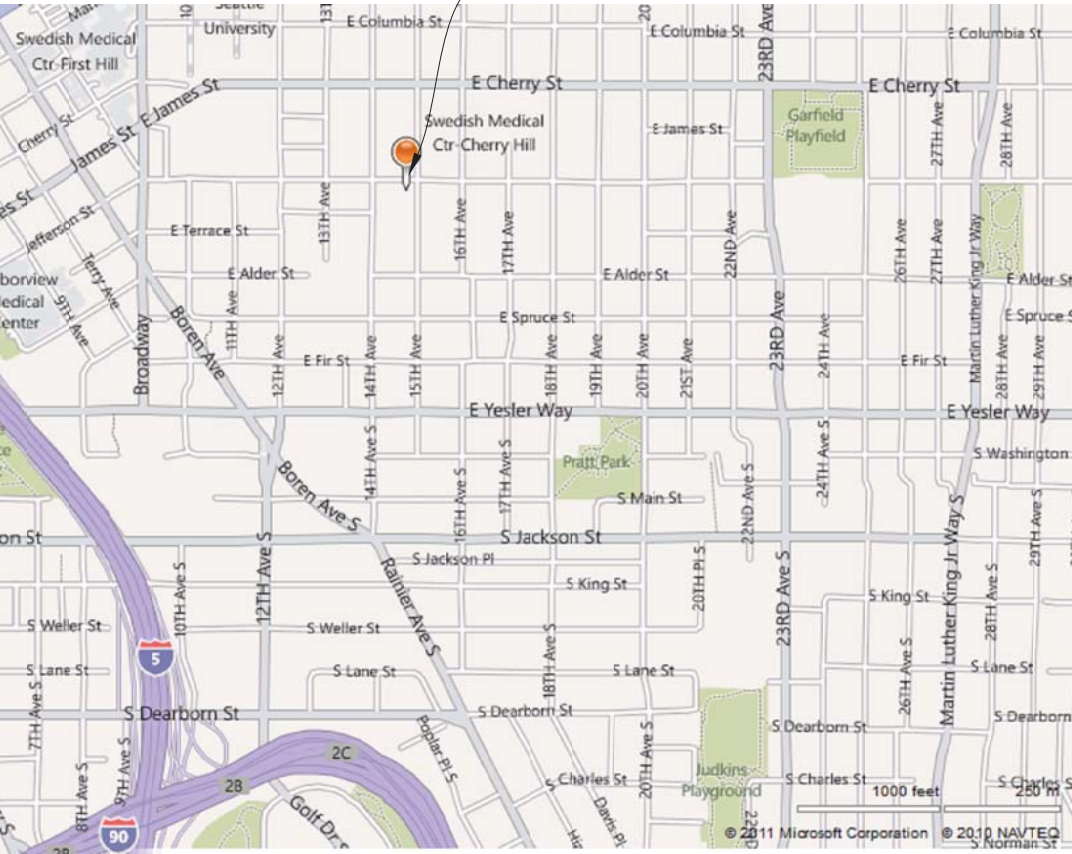
**SUBJECT TO EASEMENTS, RESTRICTIONS, CONDITIONS  
AND COVENANTS OF RECORD, IF ANY.**

## PROJECT TEAM:

**DESIGN:**  
CHRIS PARDO DESIGN: ELEMENTAL ARCHITECTURE  
1520 11TH AVE SUITE G  
SEATTLE WA 98122

**OWNER:**

**LANDSCAPE ARCHITECT:**  
**ANR LANDSCAPE DESIGN**  
**PHONE: 206.818.3610**  
**CONTACT: ANRI RAPELJE**

**VICINITY MAP:**

## LIST OF DRAWINGS:

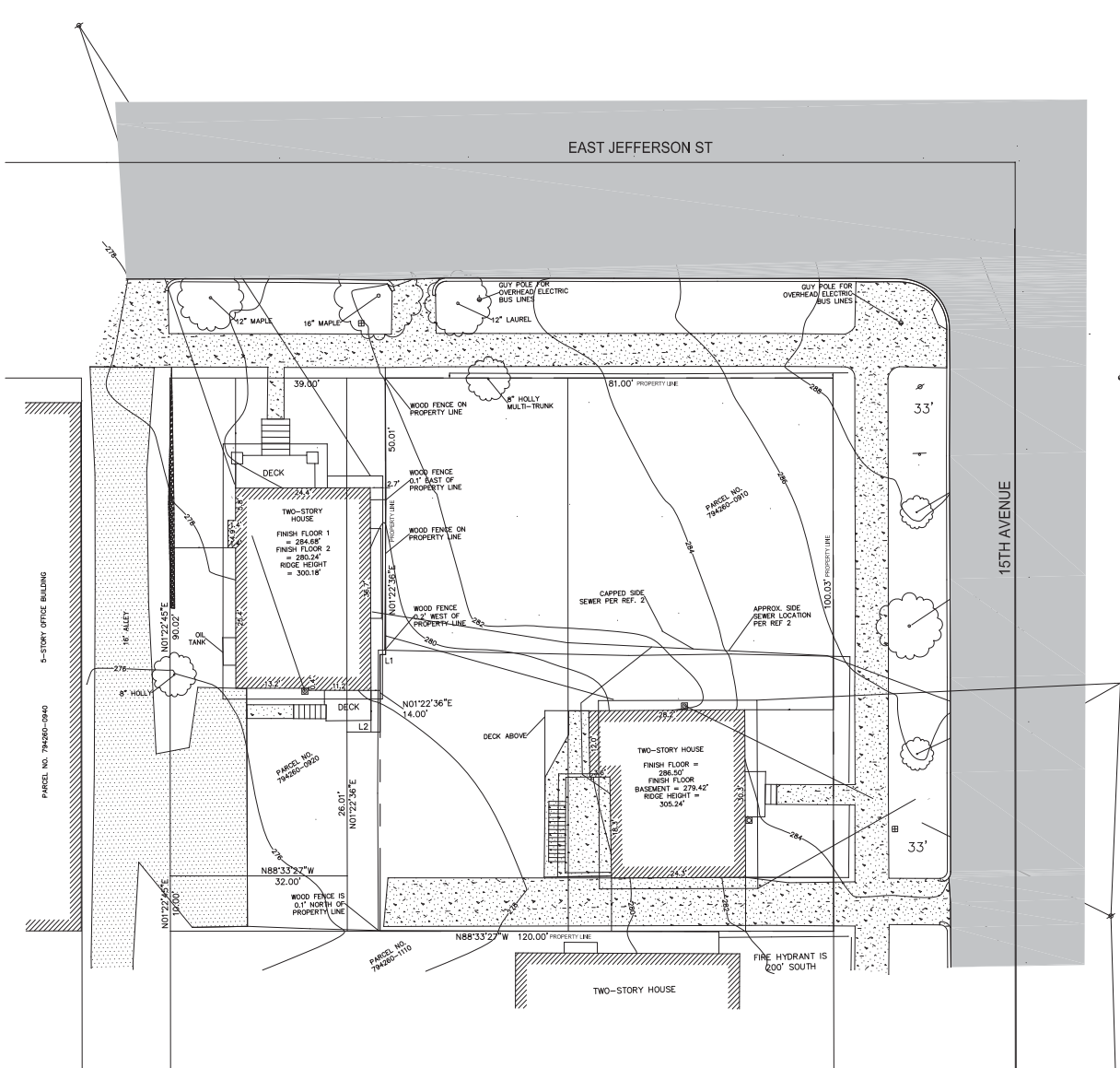
- 1 PROJECT INFORMATION
- 2 SURVEY
- 3 LAND-USE CODE SUMMARY
- 4 DESIGN GUIDELINE RESPONSES
- 5 DESIGN GUIDELINE RESPONSES
- 6 DESIGN GUIDELINE RESPONSES
- 7 DESIGN GUIDELINE RESPONSES
- 8 DESIGN GUIDELINE RESPONSES
- 9 DESIGN GUIDELINE RESPONSES
- 10 DESIGN GUIDELINE RESPONSES
- 11 PARKING & LOWER LEVEL PLAN
- 12 LEVEL ONE FLOOR PLAN
- 13 LEVEL TWO FLOOR PLAN
- 14 LEVEL THREE FLOOR PLAN
- 15 ROOF PLAN
- 16 BUILDING ONE ELEVATIONS
- 17 BUILDING ONE ELEVATIONS
- 18 BUILDING ONE ELEVATIONS
- 19 BUILDING TWO ELEVATIONS
- 20 BUILDING TWO ELEVATIONS
- 21 MATERIALS
- 22 NEIGHBORHOOD CONTEXT
- 23 DEPARTURE REQUESTS
- 24 DEPARTURE REQUESTS
- 25 LAND-USE DIAGRAMS
- 26 LAND-USE DIAGRAMS
- 27 LAND-USE DIAGRAMS

**15TH LIVE WORK**  
355 15TH AVE

## DESIGN REVIEW

Jan 15th 2013

## PROJECT INFORMATION



SURVEY

SCALE: 1/32"=1'-0"





SEATTLE ZONING CODE ANALYSIS

<b>23.47A.004</b> 23.47A.004.G.1	<b>Permitted and prohibited uses.</b> Live-work units are permitted outright subject to provisions of this title. <i>The proposal complies with this section of the Land Use Code.</i>
23.47A.004.G.2 23.47A.004.G.4	<i>Not applicable, site not in a pedestrian-designated zone.</i> Except where expressly treated as a residential use, live-work units shall be deemed a non-residential use. <i>The proposal complies with this section of the Land Use Code.</i>
<b>23.47A.005</b> 23.47A.005.C.1	<b>Street-level uses.</b> Residential uses are generally permitted anywhere in a structure in NC1, NC2, NC3, and C1 zones, except as provided in 23.47A005.C.2 and 23.47A.005.C.3. <i>The proposal complies with this section of the Land Use Code.</i>
23.47A.005.C.2 23.47A.005.C.3	<i>Not applicable, site not in a pedestrian-designated zone.</i> <i>Not applicable, site not in an 85 foot or higher zone.</i>
<b>23.47A.008</b> 23.47A.008.A.1.a 23.47A.008.2	<b>Street-level development standards.</b> Structures in NC zones. <i>Not applicable (no blank facades) as all units have: 1) Windows; 2) Entry ways or doorways; 3) Stairs, stoops, or porticos.</i> <i>The proposal complies with this section of the Land Use Code.</i>
23.47A.008.B.1.a 23.47A.008.B.2.b	Structures with street-level nonresidential uses in NC zones. Transparent areas of façades shall be designed and maintained to allow unobstructed views from the outside into the structure or, in the case of live-work units, into display windows that have a minimum 30-inch depth. <i>The proposal complies with this section of the Land Use Code.</i>
23.47A.008.B.3.a	Nonresidential uses shall extend an average of at least 30 feet and a minimum of 15 feet in depth from the street level façade.
23.47A.008.C 23.47A.008.E	<i>Not applicable, site not in a pedestrian-designated zone.</i> The portion of the live-work unit in which business is conducted must be located between the principal street and the residential portion of the live-work unit. <i>The proposal complies with this section of the Land Use Code.</i>
<b>23.47A.010</b> 23.47A.010.A	<b>Maximum size of nonresidential use.</b> Except as provided in Subsection D of this section, size limits, where specified in Table A of Section 23.47A.004, apply to the total size of a business establishment. <i>The proposal complies with this section of the Land Use Code.</i>
<b>23.47A.012</b> 23.47A.012.A.1.a.	<b>Structure height.</b> The height of a structure may exceed the otherwise applicable height limit by up to 4 feet, subject to subsection 23.47A.012.A.1.c, provided the following conditions are met:
1) i.	Either A floor to floor height of 13 feet or more is provided for non residential uses at street level; or <i>The proposal complies with this section of the Land Use Code.</i>
<b>23.47A.012.C Rooftop Features</b> 2.1.	Open railings, planters, skylights, clerestories, greenhouses, solariums, parapets and firewalls may extend as high as the highest ridge of a pitched roof permitted under subsection 23.47A.012.B or up to 4 feet above the highest applicable height limit, whichever is greater. <i>The proposal complies with this section of the Land Use Code.</i>
4.2.	Coverage of rooftop features, if they include stair enclosures, elevator penthouses and/ or screened mechanical equipment, may not exceed 25 percent. <i>The proposal complies with this section of the Land Use Code.</i>
4.f	Stair and elevator penthouses may extend above the applicable height limit by up to 16 feet. <i>The proposal complies with this section of the Land Use Code.</i>

<b>23.47A.013</b> Table A:	<b>Floor area ratio.</b> Maximum Floor Area Ratio (FAR) Outside of the Station Area Overlay District, Total permitted for all uses within a mixed-use structure containing residential and nonresidential uses.
Row 3.	30 feet
Height Limit FAR	2.5 <i>The proposal complies with this section of the Land Use Code.</i>
<b>23.47A.014</b> 23.47A.014.B.	<b>Setback requirements.</b> Setback requirements for lots abutting or across the alley from a residential zone.
1.	A setback is required where a lot abuts the intersection of a side lot line and a front lot line of a lot in a residential zone. The required setback forms a triangular area. (See Exhibit A for 23.47A.014). <i>The proposal complies with this section of the Land Use Code.</i>
2.	A setback is required along any rear or side lot line that abuts a lot in a residential zone, as follows:
a.	Ten feet for portions of structures above 13 feet in height to a maximum of 65 feet; and <i>The proposal complies with this section of the Land Use Code.</i>
5. c.	Fences, bulkheads, freestanding walls and similar structures. Bulkheads and retaining walls used to protect a cut into existing grade may not exceed the minimum height necessary to support the cut or 6 feet, whichever is greater. <i>The proposal complies with this section of the Land Use Code.</i>
<b>23.47A.016</b> 23.47A.016.A.2	<b>Landscaping and screening standards.</b> Landscaping that achieves a Green Factor score of .30 or greater, pursuant to the procedures set forth in Section 23.86.019, is required for any lot with:
a.	development containing more that four new dwelling units: or <i>The proposal complies with this section of the Land Use Code.</i>
23.47A.016.B.1	Street trees are required when any development is proposed, except as provided in subsection 23.47A.016.B.2 and Section 23.53.015. <i>The proposal complies with this section of the Land Use Code.</i>
23.47A.016.D	Screening and landscaping are requirements for specific uses. When there is more than one use that requires screening or landscaping, the requirement that results in the greater amount applies.
1.	Surface parking areas.
a.	Landscaping areas for surface parking areas are summarized in Table C for 23.47A.016. <i>Not applicable as there are less than 20 parking spaces supplied.</i>
<b>23.47A.022</b> 23.47A.022.C	<b>Light and glare standards.</b> To prevent vehicle lights from affecting adjacent properties, driveways and parking areas for more than two (2) vehicles shall be screened from adjacent properties by a fence or wall between five (5) feet and six (6) feet in height, or solid evergreen hedge or landscaped berm at least five (5) feet in height. If the elevation of the lot line is different from the finished elevation of the driveways or parking surface, the difference may substitute for a portion of the required height of the screen so long as the screen itself is a minimum of three (30) feet in height. The Director may waive the requirement for the screening if it is not needed due to changes in topography, agreements to maintain an existing fence, or the nature and location of adjacent used. <i>The proposal complies with the section of the Land Use Code.</i>

<b>23.47A.024</b> 23.47A.024	<b>Amenity area.</b> Not required for live/work units, per DPD Zoning review. <i>The proposal complies with this section of the Land Use Code.</i>
<b>23.47A.030</b> 23.47A.030.A	<b>Required parking and loading.</b> Off-street parking may be required as provided in Section 23.54.015, Required Parking.
23.54.015 D.	Table A. LIVE-WORK UNITS 0 spaces for units with 1,500 square feet or less; <i>The proposal complies with this section of the Land Use Code.</i>
<b>23.47A.032</b> 23.47A.032.A	<b>Parking location and access.</b> Access to parking.
1.	NC zones. The following rules apply in NC zones, except as provided under 23.47A.032.A.2 and 23.47A.032.D:
a.	Access to parking shall be from the alley if the lot abuts an alley improved to the standards of 23.53.030.C, or if the Director determines that the alley access is feasible and desirable to mitigate parking access impacts.
c.	If access is not provided from an alley and the lot abuts two or more streets, access is permitted across one of the side street lot lines as determined through 23.47A.032.C, and curb cuts are permitted pursuant to Section 23.54.030.F.2.a.1). <i>Design departure request. See Sheet G1.2..</i>
23.047A.032.B	Location of parking.
1.	The following rules apply in NC zones, except as provided in subsection 23.47A.032.D.
a.	Parking shall not be located between a structure and a street lot line (Exhibit A for 23.047A.032). <i>The proposal complies with this section of the Land Use Code</i>
<b>23.54.040</b> Table A	<b>Solid waste and recyclable materials storage and access.</b> Non-residential use - 5001-1500 SF = 125 SF shared space. (125/8 = 62.5 SF each unit.) <i>The proposal complies with this section of the Land Use Code.</i>



15TH LIVE WORK  
355 15TH AVE

DESIGN  
REVIEW

Jan 15th 2013

LAND USE  
CODE  
SUMMARY

A-1 RESPONDING TO SITE CHARACTERISTICS.  
AND  
A-2 STREETScape COMPATIBILITY.

THE PROPOSAL ADDRESSES THE DIFFERENT CONTEXT OF THE TWO STREET FRONTAGES IN A MANNER APPROPRIATE TO EACH. THE UNITS FRONTING JEFFERSON STREET ADDRESS THE SITE BY STEPPING EACH UNIT MAIN FLOOR TO SUIT THE TOPOGRAPHY, UNITS EACH RECEIVE A STOOP WITH STEPS, A RAMP AND A PLANTER. THE LOWER FLOORS ARE PREDOMINANTLY STOREFRONT GLAZING. THE UPPER FLOORS RECEIVE A CONSISTENT CLADDING WITH PUNCHED OPENINGS, UNIFYING THE BUILDING TO BETTER ADDRESS THE SCALE OF THE INSTITUTIONAL BUILDINGS ACROSS JEFFERSON. THE CLADDING GIVES WAY TO A THREE STORY GLAZED FAÇADE TO MARK THE CORNER OF 15TH AND JEFFERSON. THE CONNECTED LIVE/WORK UNITS WHICH FRONT 15TH AVENUE ARE TREATED AS INDIVIDUAL UNITS; WITH LESS EMPHASIS PLACED ON THE STOREFRONT GLAZING ON THE GROUND FLOOR. THESE UNITS ALSO STEP TO MATCH THE SLOPE OF THE EXISTING SIDEWALK. DUE TO THE EXTREME EXISTING TOPOGRAPHIC CONDITIONS (6 FEET OF FALL FROM NORTH TO SOUTH AT THE WEST PROPERTY LINE, 8 FEET OF FALL FROM EAST TO WEST AT THE SOUTH PROPERTY LINE, 12 FEET OF FALL FROM NE CORNER TO SW CORNER) THE THROUGH UNITS ALL HAVE A LEVEL BELOW STREET LEVEL, FRONTING AN INTERNAL COURT OR WOONERF.

**A-3 ENTRANCES VISIBLE FROM THE STREET.**

THE PROPOSAL GIVES EACH UNIT AN ENTRY, VISIBLE FROM THE STREET, WITH A DEFINING STOOP CONDITION, FROM THE TWO STREET FRONTAGES. AGAIN, DUE TO THE EXTREME EXISTING TOPOGRAPHIC CONDITIONS (6 FEET OF FALL FROM NORTH TO SOUTH AT THE WEST PROPERTY LINE, 8 FEET OF FALL FROM EAST TO WEST AT THE SOUTH PROPERTY LINE, 12 FEET OF FALL FROM NE CORNER TO SW CORNER) THE THROUGH UNITS ALL HAVE A LEVEL BELOW STREET LEVEL, AT THE WOONERF. EACH THROUGH UNIT ALSO HAS AN ENTRY FROM THIS LOWER (INTERNAL) LEVEL. AS THE CONNECTED LIVE/WORK UNITS ARE SPECULATIVE AND FOR SALE THE USE OF THESE DOORS WILL NEED TO BE FLEXIBLE ENOUGH TO SUIT THE BUYERS. SOME STREET FRONT DOORS MAY, THEREFORE, BE USED FOR ACCESS TO THE WORK SIDE OF UNIT. IT IS ALSO POSSIBLE THAT SOME WILL CHOOSE TO USE THE WOONERF LEVEL DOOR AS ACCESS TO THEIR WORKSPACE.

**A-4 HUMAN ACTIVITY.**

EACH UNIT HAS A DEFINED ENTRY WITH A STOOP CONDITION, VISIBLE FROM THE STREET. ENTRIES ARE RECESSED FROM JEFFERSON STREET (AN ARTERIAL) FOUR (4) FEET. ENTRIES ARE RECESSED FROM 15TH AVENUE SOUTH (A RESIDENTIAL STREET) TWO (2) FEET. THE CORNER UNIT HAS ONE OF EACH ENTRY CONDITION. THE ENTRIES, AND STOOPS, AND STOREFRONT GLAZING ARE DESIGNED TO ENCOURAGE A CONNECTION FROM THE PASSERBY TO THE WORKSPACE IN EACH UNIT. ONCE AGAIN, DUE TO THE EXTREME EXISTING TOPOGRAPHIC CONDITIONS (6 FEET OF FALL FROM NORTH TO SOUTH AT THE WEST PROPERTY LINE, 8 FEET OF FALL FROM EAST TO WEST AT THE SOUTH PROPERTY LINE, 12 FEET OF FALL FROM NE CORNER TO SW CORNER) ALL THROUGH UNITS ALL HAVE A LEVEL BELOW STREET LEVEL, AT THE WOONERF. EACH THROUGH UNIT ALSO HAS AN ENTRY AND AN UPWARD ACTING, FULLY GLAZED, GARAGE DOOR, FROM THIS LOWER (INTERNAL) LEVEL. THIS TRANSPARENCY IS DESIGNED ENCOURAGE A VARIETY OF USES AND EXPERIENCES FROM THE WOONERF LEVEL.



**NORTH ELEVATION**



**SIGNAGE AT COMMERCIAL SPACES ON JEFFERSON**



#### A-5 RESPECT FOR ADJACENT SITES.

THE PROPOSED PROJECT OCCUPIES A PARCEL THAT IS ADJACENT TO A VARIETY OF ZONES AND EXISTING CONDITIONS, FROM MAJOR INSTITUTIONS TO SINGLE FAMILY. TO THE EXTENT POSSIBLE, IN AN EIGHT-UNIT PROJECT, THE PROPOSAL ATTEMPTS TO MEDIATE BETWEEN THE ZONES AND FIT INTO THE SEAM BETWEEN THE VARIOUS CONDITIONS AND USES. AS ADDRESSED IN A-1 AND A-2, THE FAÇADE AND MASSING OF THE UNITS FRONTING JEFFERSON STREET ARE LINKED TO FORM A MORE CONTINUOUS EXPRESSION. THE FAÇADE AND MASSING OF THE UNITS FACING 15TH AVENUE ARE GIVEN AN EXPRESSION OF INDIVIDUAL LIVE/WORK UNITS TO RESPOND TO THE GRAIN OF THE SINGLE-FAMILY DEVELOPMENT ON 15TH AVENUE.

THE WEST ELEVATION IS AN OPAQUE PROPERTY LINE WALL, 36 FEET DEEP, RESPONDING TO THE ADJACENT NC2-40 ZONE. THE ADJACENT SINGLE-FAMILY STRUCTURE IS CURRENTLY BEING USED FOR PROFESSIONAL OFFICES. IT SHOULD BE NOTED THAT THE EXISTING STRUCTURE WEST OF THIS NC-40 ZONE IS A FULL LOT CONCRETE BUILDING THAT HAS FIVE (5) STORIES OF OFFICES OVER TWO (2) STORIES OF PARKING.

THE FULL HEIGHT OF THE SOUTH ELEVATION IS HELD BACK FROM THE PROPERTY LINE BY 10 FEET. THE EXISTING CURB CUT IS USED TO ACCESS A DRIVEWAY (EXISTING) TO THE WOONERF (PER EDG). THE MASSING OF THIS FAÇADE IS BROKEN INTO ELEMENTS THAT EXPRESS THE PROTOTYPICAL COMPOSITION OF THE CONNECTED LIVE/WORK UNITS STYLE.

THE CENTER OF THE SITE IS A RECESSED OPEN SPACE, A WOONERF, WHICH IS ALSO USED FOR OPEN SPACE FOR THE RESIDENTS AS WELL AS ACCESS AND PARKING. THE SPACE IS PARTIALLY EXCAVATED, MATCHING EXISTING GRADE AT THE SW, AND PROVIDING ACCESS TO THE LOWER LEVEL OF THE LIVE/WORK UNITS, TO THE NORTH AND TO THE EAST. THIS RECESSED OPEN SPACE, COUPLED WITH THE ALIGNMENT OF THE BUILDINGS, HELPS DISTANCE THE BUILDING MASS FROM THE ADJACENT SINGLE-FAMILY DEVELOPMENT ON 15TH.

#### A-8 PARKING AND VEHICLE ACCESS.

VEHICLE ACCESS (DRIVEWAY: IN AND OUT) IS FROM THE EXISTING CURB CUT AND 10-FOOT WIDE DRIVEWAY OFF OF 15TH AVENUE AT THE SOUTH PROPERTY LINE. THIS CONFIGURATION MINIMIZES THE WIDTH OF THE VEHICLE ACCESS.

THE CENTER OF THE SITE IS DESIGNED AS A WOONERF. IT IS DEVELOPED AS A RECESSED OPEN SPACE, WHICH IS ALSO USED AS A COMMON OPEN SPACE FOR THE RESIDENTS AS WELL AS ACCESS AND PARKING. THIS SPACE IS PARTIALLY EXCAVATED, MATCHING EXISTING GRADE AT THE SW, AND PROVIDING ACCESS TO THE LOWER LEVEL OF THE LIVE/WORK UNITS, TO THE NORTH AND TO THE EAST. THIS RECESSED OPEN SPACE IS APPROXIMATELY 8-FEET LOWER THAN THE STREET LEVEL, ALLOWING LIGHT AND VIEW FROM THE WORKSPACE WITHOUT LOOKING DIRECTLY AT VEHICLE PARKING.

PARKING IS EXTERIOR, ON THE PERVIOUS SURFACE OF THE WOONERF, NESTLED UNDER ELEVATED DECKS OFF OF THE MAIN FLOOR, ADJACENT TO A GROUND LEVEL SPACE IN EACH UNIT.



SCALE OF BUILDINGS IN AREA



VIEW OF WOONERF FROM ABOVE





CORNER OF 15th & JEFFERSON

**A-10 CORNER LOTS.**

AS DISCUSSED IN A-1 AND IN A-2, THE DESIGN OF THE CONNECTED LIVE/WORK UNITS RECOGNIZES THE THREE DIFFERENT SITE CONDITIONS: THE NORTH FAÇADE IS A CONTINUOUS SCRIM, TO RESPOND TO THE MAJOR INSTITUTIONAL BUILDINGS ACROSS JEFFERSON STREET; AND THE EAST FAÇADE IS AN INDIVIDUAL LIVE/WORK UNIT EXPRESSION, TO RESPOND TO THE SINGLE-FAMILY BUILDINGS ON 15TH AVENUE. WHERE THE BUILDING TURNS THE CORNER THE MORE SOLID EXPRESSION OF THE NORTH FAÇADE GIVES WAY TO A MORE GLASSY FAÇADE OF THE TWO CORNER UNITS, RECOGNIZING THE CORNER CONDITION AND MEDIATING BETWEEN THE TWO STREET ELEVATION.



# B-1 HEIGHT, BULK, AND SCALE COMPATIBILITY.

THE PROPOSAL IS COMPATIBLE WITH THE HEIGHT, BULK AND SCALE OF THE SURROUNDING BUILDINGS.

THIS PARCEL IS A LONE 30 -FOOT HEIGHT LIMITED (NC1-30) PARCEL IN THE IMMEDIATE AREA. THE SITE IMMEDIATELY TO THE WEST IS (NC2-40), WITH A 65+ FOOT TALL BUILDING TO THE WEST OF IT. THE SITES ACROSS JEFFERSON ARE DEVELOPED TO 65 FEET (MIO65). TO THE EAST AND SOUTH IS SINGLE-FAMILY (SF500) WHICH COMES WITH A 30-FOT (PLUS 5-TEET FOR A SLOPED ROOF, OR 35-FOOT HEIGHT LIMIT.

THE PROPOSED PROJECT IS 30 FEET, PLUS 4 FEET FOR A 13-FOOT FLOOR-TO-FLOOR HEIGHT AT THE GROUND FLOOR, PLUS 4 FEET FOR PARAPETS, ETC. OR 5 FEET FOR SLOPED ROOF, WHICH EVER IS GREATER. THE TOTAL ALLOWABLE HEIGHT IS 38 FEET. THE PROPOSAL WILL BE THIS TALL, AT CERTAIN LOCATIONS, AS ALLOWED BY CODE, PER SMC 23.86.006, AND DR 4-2012.

THE PROPOSED PROJECT, BASED ON THE PREFERRED ALTERNATIVE FROM THE EDG, PLACES THE BUILDING MASS AT THE EDGE OF THE SITE, ADJACENT TO THE STREET FRONTAGES, LEAVING THE SW QUADRANT OF THE PARCEL AT GRADE. THIS KEEPS THE BULK OF THE BUILDING FURTHER AWAY FOR THE SINGLE-FAMILY DEVELOPMENT THAN OTHER POSSIBLE OPTIONS.

THE PROJECT IS SMALLER IN SCALE THAN THE INSTITUTIONAL BUILDINGS IN THE IMMEDIATE AREA. IT IS SHORTER THAN THE EXISTING OFFICE BLOCK ON THE SAME SIDE OF JEFFERSON.

IT IS 4 OR 5 FEET TALLER THAT WHAT IS ALLOWED ON THE SINGLE-FAMILY LOTS. THE UNITS FACING 15TH ARE 16 FEET WIDE CONNECTED UNITS, WHICH IS GENERALLY NARROWER THAN THE TYPICAL SINGLE FAMILY HOME IN THE IMMEDIATE AREA.



EAST ELEVATION

# C-1 ARCHITECTURAL CONTEXT.

THE PROJECT IS SMALLER IN SCALE THAN THE INSTITUTIONAL BUILDINGS, MOSTLY NORTH OF JEFFERSON, IN THE IMMEDIATE AREA. THE PROJECT COMBINES THE UPPER LEVEL FACADES BEHIND A SCRIM, TO ESTABLISH A DIALOGUE WITH THE CONTEXT OF LARGER NEIGHBORING BUILDINGS.

THE PROJECT IS SHORTER IN HEIGHT THAN THE EXISTING OFFICE BLOCK ON THE SAME SIDE OF JEFFERSON. IT DOES STAND UP TO THE ALLOWABLE HEIGHT TO REINFORCE THE SCALE OF THE SOUTH SIDE OF JEFFERSON, BETWEEN 15TH AND 14TH.

THE PROJECT IS ABOUT 4 OR 5 FEET TALLER THAN WHAT IS ALLOWED ON THE SINGLE-FAMILY LOTS. THE UNITS FACING 15TH STEP WITH THE EXISTING GRADE AND ARE DESIGNED AS 16 FEET WIDE CONNECTED LIVE/WORK UNITS WHICH, THOUGH GENERALLY NARROWER THAN THE TYPICAL SINGLE FAMILY HOME IN THE IMMEDIATE AREA, ESTABLISHES A COMPATIBLE GRAIN WITH THESE BUILDINGS.

# C-2 ARCHITECTURAL CONCEPT AND CONSISTENCY.

AS DESCRIBED IN EARLIER SECTIONS OF THE DESIGN REVIEW RESPONSES, THE PROJECT IS DESIGNED TO RESPOND TO, AND BE COMPATIBLE WITH, A NUMBER OF DIFFERENT ARCHITECTURAL CONTEXTS. THE DESIGN DOES THIS BY STARTING WITH A TYPICAL UNIT PLAN AND SECTION, AND FITTING IT TO THE EXISTING TOPOGRAPHY AND STREETScape. THE FACADES OF THE UNITS ARE THEN TREATED IN A DIFFERENT FASHION TO RESPOND TO THE CONTEXT WHILE STILL REMAINING PART OF A UNIFIED WHOLE. THE SELECTION OF MATERIALS AND FINISHES IS KEPT TO A MINIMUM NUMBER, FOR A CALM AND CONSISTENT RENDER OF THE FACADES.



UNITS OPEN INTO SHARED WOONERF SPACE



### C-3 HUMAN SCALE.

ELEMENTS OF THE DESIGN ARE SCALED TO HUMAN PROPORTIONS. ON THE STREET SIDES, PLANTERS, STOOPS, HANDRAILS, ENTRY DOORS AND LIGHTING FIXTURES ARE DESIGNED AND DETAILED TO CREATE A SMALLER, MORE PEDESTRIAN SCALE, COMPOSITION. ON THE WOONERF SIDE, THE GRIDDED WINDOW WALLS, DOORS, DECKS AND RAILS CREATE A THREE DIMENSIONAL TRACERY THAT REFLECTS THE SCALE OF THE OCCUPANTS.

### C-4 EXTERIOR FINISH MATERIALS.

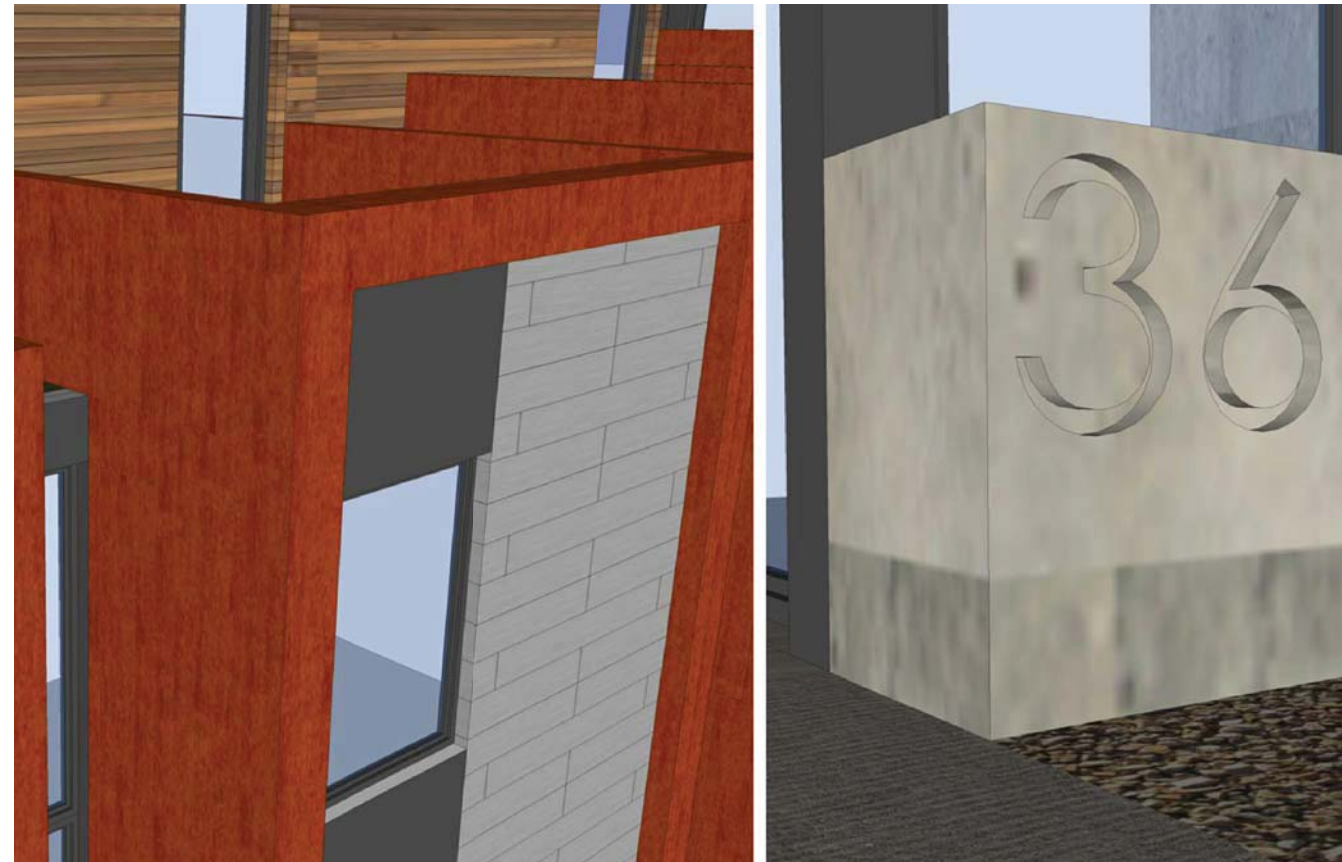
CAST-IN-PLACE CONCRETE  
ALUMINUM WINDOWS  
HORIZONTAL FLUSH METAL SIDING  
VERTICAL CORTEN STANDING SEAM SIDING  
POWDER COATED ALUMINUM RAILINGS  
CONCRETE PAVERS  
FLAT ALUMINUM COMPOSITE PANEL

### D-1 PEDESTRIAN OPEN SPACES AND ENTRANCES.

AS ILLUSTRATED IN A-4, EACH UNIT HAS A DEFINED ENTRY WITH A STOOP CONDITION, AT THE STREET LEVEL. ENTRIES AND STOREFRONT GLAZING ARE RECESSED FROM JEFFERSON STREET (AN ARTERIAL) FOUR (4) FEET. THERE IS A 1'-0" STEEL PLATE PLANTER INCORPORATED BETWEEN THE BACK OF SIDEWALK AND THE 3'-0" ADA COMPATIBLE WALKING SURFACE TO THE ENTRY.

ENTRIES ARE RECESSED FROM 15TH AVENUE SOUTH (A RESIDENTIAL STREET) SIX (6) FEET AND THE STOREFRONT GLAZING IS SETBACK TWO (2) FEET. THERE IS A 2'-0" STEEL PLATE PLANTER INCORPORATED BETWEEN THE BACK OF SIDEWALK AND THE STOREFRONT GLAZING.

DUE TO THE EXTREME EXISTING TOPOGRAPHIC CONDITIONS (6 FEET OF FALL FROM NORTH TO SOUTH AT THE WEST PROPERTY LINE (PL), 8 FEET OF FALL FROM EAST TO WEST AT THE SOUTH PROPERTY LINE, 12 FEET OF FALL FROM NE CORNER TO SW CORNER) ALL THROUGH UNITS ALL HAVE A LEVEL BELOW STREET LEVEL, ORGANIZED AROUND A PRIVATE SPACE, SHARED BY ALL UNITS, DESIGNED AS A WOONERF. A WOONERF IS FROM THE DUTCH WORD FOR A "LIVING STREET". THE CONCEPT IS THAT PEDESTRIANS, BICYCLISTS, AND MOTOR VEHICLES SHARE A STREET, WITH PEDESTRIANS AND BICYCLES HAVING PRIORITY OVER MOTOR VEHICLES. THE CONCEPT IS USUALLY APPLIED TO A NARROW RESIDENTIAL STREET, WITH NO DIFFERENTIATION BETWEEN SURFACES; NO CURBS, NO STRIPES, AND NO SEPARATION BETWEEN USES. IN OUR CASE WE ARE CREATING A SHARED COMMON SPACE FOR THE BENEFIT OF ALL HOMEOWNERS. IT IS A PLACE TO WALK, TO PLAY, TO RIDE BIKES, TO SIT IN THE SUN, AND ONLY INCIDENTALLY, PARK A CAR. IT IS AN ESSENTIAL COMPONENT OF THE DESIGN. AS WE ARE NOT REQUIRED BY CODE TO PROVIDE PARKING (EACH LIVE/WORK UNIT IS LESS THAN 1500 SF), THE USE OF THE SPACE CAN BE FLEXIBLE AND ADAPTABLE. AS A MATTER OF THE MARKETABILITY, WE HAVE ELECTED TO ACCOMMODATE ONE CAR PER UNIT, OR EIGHT (8) "SPACES". THE USE OF PERMEABLE UNIT PAVERS WILL ALLOW US TO DESIGN A UNIFORM "RUG" FOR THIS SPACE, PERHAPS WITH A SUBTLE PLAID PATTERN TO AID IN THE USE OF THE SPACE, SO PARKING DOES NOT COME TO DOMINATE THE ALLOCATION OF FOOTPRINT OVER TIME. THE DISTRIBUTION AND POSITION OF PARKING WILL BE NEGOTIATED BY THE HOMEOWNERS. THE WOONERF ALSO ALLOWS THE INCORPORATION OF PLANTING (TREES) INTO THE PATTERN WHERE PRACTICAL.



FINISH MATERIALS AT UPPER LEVEL / CAST-IN-PLACE ELEMENT



CAST-IN-PLACE WALL ELEMENTS



## D-2 BLANK WALLS.

THE STREET FACE FACADES ARE PREDOMINANTLY STOREFRONT GLAZING, ALLOWING VIEWS INTO, AND OUT OF, THE GROUND LEVEL SPACES.

THERE ARE A FEW LOCATIONS IN THE PROJECT THAT ARE LIMITED, BY BUILDING CODE, IN ALLOWABLE OPENINGS.

THE WEST FACING PROPERTY LINE WALL IS A BANK WALL, RENDERED IN A MINIMALIST COMPOSITION OF TWO MATERIALS CONSISTENT WITH THE REQUIRED FIRE RATING.

THE INTERIOR WALLS AT THE EAST/WEST STAIR SLOT BETWEEN BUILDINGS, IS LIMITED TO 15% GLAZING DUE TO THE DISTANCE FROM THE ADJACENT BUILDING. ONE SIDE HAS INCIDENTAL GLAZING FOR THE LIVE/WORK UNIT, WHILE THE OTHER SIDE IS A MINIMALIST COMPOSITION OF TWO MATERIALS CONSISTENT WITH THE REQUIRED FIRE RATING.

THE SOUTH FACING WALL OF THE EAST BUILDING IS LIMITED TO 25% GLAZING DUE TO THE PROXIMITY OF THE PROPERTY LINE. THIS WALL HAS INCIDENTAL GLAZING FOR THE LIVE/WORK UNIT, IN A MINIMALIST COMPOSITION MATERIALS CONSISTENT WITH THE REQUIRED FIRE RATING AND PRIVACY OF THE ADJACENT SINGLE-FAMILY HOME.

## D-7 PERSONAL SAFETY AND SECURITY.

THE CONNECTED LIVE/WORK UNITS ARE DESIGNED WITH AMPLE GLAZING ON THE STREET SIDE, ESPECIALLY AT GROUND LEVEL, TO ENCOURAGE “EYES ON THE STREET”. THE WOONERF (INTERNAL) SIDE OF THE LIVE/WORK UNITS HAVE GLAZING ON ALL FLOORS TO CREATE VIEWS OUT INTO THE NEIGHBORHOOD AND TO FOSTER A CONNECTION TO THE SHARED MULTI-FUNCTION SPACE.

## D-9 COMMERCIAL SIGNAGE.

THE CONNECTED LIVE/WORK UNITS ARE DESIGNED WITH AMPLE STOREFRONT GLAZING ON THE STREET SIDE. A TYPICAL LOCATION FOR BUSINESS (COMMERCIAL) SIGNAGE IS INTEGRATED INTO THE STOREFRONT DESIGN.

## D-10 COMMERCIAL LIGHTING.

AS ILLUSTRATED IN D-9, THE CONNECTED LIVE/WORK UNITS ARE DESIGNED WITH AMPLE STOREFRONT GLAZING ON THE STREET SIDE. A TYPICAL LOCATION FOR LIGHTING, OF BOTH THE STOOPS AND ANY COMMERCIAL SIGNAGE, IS INTEGRATED INTO THE STOREFRONT DESIGN.

## D-11 COMMERCIAL TRANSPARENCY.

AS ILLUSTRATED IN D-9, THE CONNECTED LIVE/WORK UNITS ARE DESIGNED WITH AMPLE STOREFRONT GLAZING ON THE STREET SIDE. A TYPICAL GROUND FLOOR CONFIGURATION OF A UNIT IS 16'-0" FEET, CENTERLINE TO CENTERLINE, WITH A CEILING HEIGHT OF 12'-0", OR 192 SQUARE FEET. THE DESIGN WILL INCORPORATE A FULL HEIGHT WINDOW WALL AND GLAZED ENTRY DOOR AS WIDE AND AS TALL AS THE STRUCTURE WILL ACCOMMODATE.



TRANSPARENCY AT LIVE / WORK UNITS



TRANSPARENCY AT LIVE / WORK UNITS



**D-12            RESIDENTIAL ENTRIES AND TRANSITIONS.**

EACH UNIT HAS TWO ENTRIES.

AS ILLUSTRATED IN A-4, EACH UNIT HAS A DEFINED ENTRY WITH A STOOP CONDITION, AT THE STREET LEVEL. ENTRIES AND STOREFRONT GLAZING ARE RECESSED FROM JEFFERSON STREET (AN ARTERIAL) FOUR (4) FEET. ENTRIES ARE RECESSED FROM 15TH AVENUE SOUTH (A RESIDENTIAL STREET) SIX (6) FEET AND THE STOREFRONT GLAZING IS SETBACK TWO (2) FEET. THE ENTRIES, AND STOOPS, AND STOREFRONT GLAZING ARE DESIGNED TO PROVIDE A TRANSITION FROM THE SIDEWALK (PUBLIC) TO THE WORKSPACE (SEMI-PRIVATE) IN EACH UNIT.

DUE TO THE EXTREME EXISTING TOPOGRAPHIC CONDITIONS (6 FEET OF FALL FROM NORTH TO SOUTH AT THE WEST PROPERTY LINE, 8 FEET OF FALL FROM EAST TO WEST AT THE SOUTH PROPERTY LINE, 12 FEET OF FALL FROM NE CORNER TO SW CORNER) ALL THROUGH UNITS ALL HAVE A LEVEL BELOW STREET LEVEL, AT THE WOONERF. EACH THROUGH UNIT ALSO HAS AN ENTRY AND AN UPWARD ACTING, FULLY GLAZED, GARAGE DOOR, FROM THIS LOWER (INTERNAL) LEVEL, WHICH WILL SERVE AS A PRIMARY RESIDENTIAL ENTRY FROM THE MORE PRIVATE SIDE OF THE PROJECT.

AS THE PROJECT IS A SPECULATIVE PROJECT, DESIGNED TO ATTRACT A BUYER WHO WANTS TO LIVE AND WORK IN THE SAME UNIT, A NUMBER OF CONFIGURATIONS NEED TO BE POSSIBLE. IN A SIMILAR FASHION, THE ENTRY CONFIGURATION ALSO NEEDS TO BE FLEXIBLE.



**WOONWERF, BALCONIES AT PRIVATE SIDE OF PROPERTY**

**E-2            LANDSCAPING TO ENHANCE THE BUILDING AND/OR SITE.**

THE PROJECT INCORPORATES A COORDINATED SERIES OF LANDSCAPE ELEMENTS; STREET TREES, STOOPS AND PLANTERS, A WOONERF, AND GREEN ROOFS INTEGRATED INTO THE DESIGN.

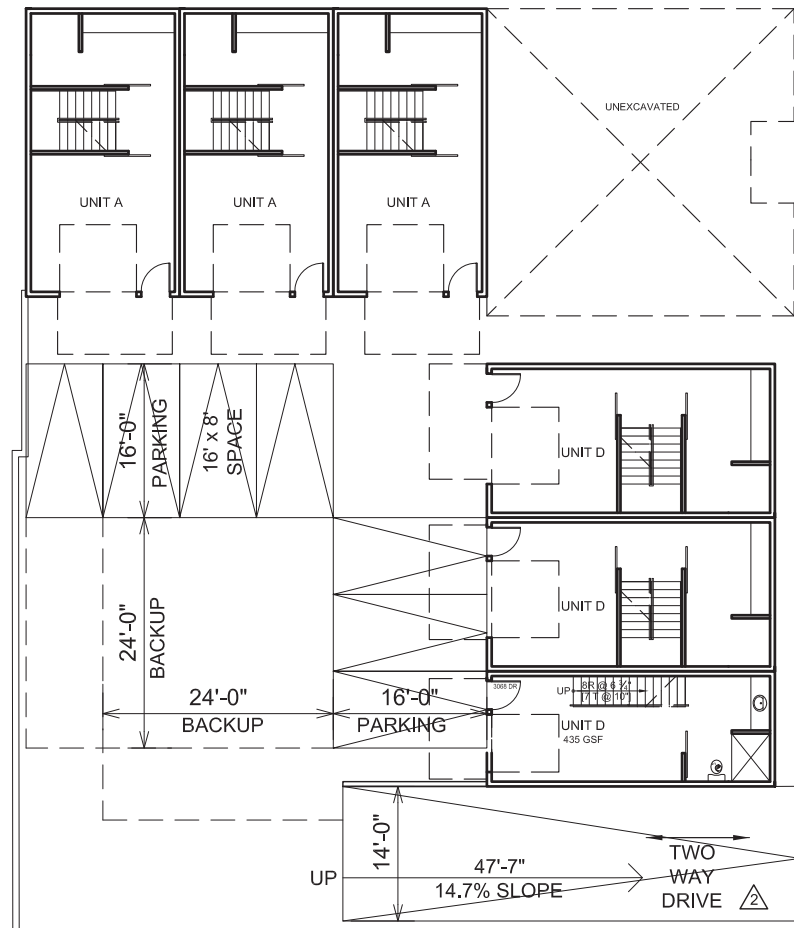
STREET TREES WILL BE PROVIDED ON BOTH 15TH AVENUE AND EAST JEFFERSON STREET, PER DPD AND SDOT. WHERE TREES EXIST, THEY WILL BE PROTECTED DURING CONSTRUCTION, WHERE THEY NEED TO BE ADDED, THEY WILL BE COORDINATED WITH SDOT.

AS ILLUSTRATED IN A-4, EACH UNIT HAS A DEFINED ENTRY WITH A STOOP CONDITION, AT THE STREET LEVEL. ENTRIES AND STOREFRONT GLAZING ARE RECESSED FROM JEFFERSON STREET (AN ARTERIAL) FOUR (4) FEET. THERE IS A 1'-0" STEEL PLATE PLANTER INCORPORATED BETWEEN THE BACK OF SIDEWALK AND THE 3'-0" ADA WALK TO THE ENTRY.

ENTRIES ARE RECESSED FROM 15TH AVENUE SOUTH (A RESIDENTIAL STREET) SIX (6) FEET AND THE STOREFRONT GLAZING IS SETBACK TWO (2) FEET. THERE IS A 2'-0" STEEL PLATE PLANTER INCORPORATED BETWEEN THE BACK OF SIDEWALK AND THE STOREFRONT GLAZING.

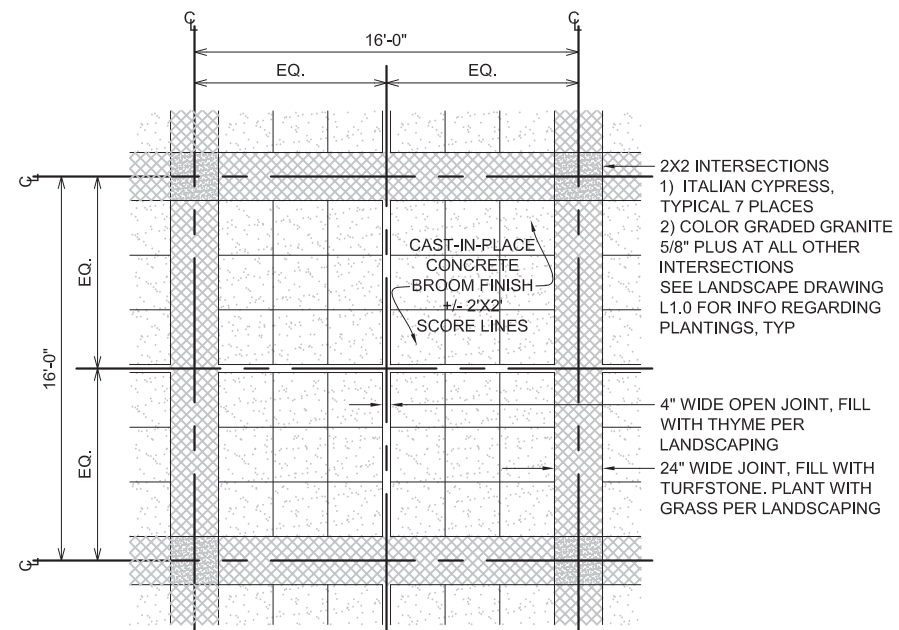
DUE TO THE EXTREME EXISTING TOPOGRAPHIC CONDITIONS (6 FEET OF FALL FROM NORTH TO SOUTH AT THE WEST PROPERTY LINE, 8 FEET OF FALL FROM EAST TO WEST AT THE SOUTH PROPERTY LINE, 12 FEET OF FALL FROM NE CORNER TO SW CORNER) ALL THROUGH UNITS ALL HAVE A LEVEL BELOW STREET LEVEL, ORGANIZED AROUND A SPACE DESIGNED AS A WOONERF. AS ILLUSTRATED IN D-6, A WOONERF IS FROM THE DUTCH WORD FOR A "LIVING STREET". THE CONCEPT IS THAT PEDESTRIANS, BICYCLISTS, AND MOTOR VEHICLES SHARE A STREET, WITH PEDESTRIANS AND BICYCLES HAVING PRIORITY OVER MOTOR VEHICLES. WHILE THIS CONCEPT IS TYPICALLY APPLIED TO A NARROW RESIDENTIAL STREET, WITH NO DIFFERENTIATION BETWEEN SURFACES AND NO SEPARATION BETWEEN USES. IN THE CASE OF THIS PROJECT WE ARE CREATING A SHARED COMMON SPACE FOR THE BENEFIT OF ALL HOMEOWNERS; A SHARED PLACE TO WALK, TO PLAY, TO RIDE BIKES, TO SIT IN THE SUN, AND ONLY INCIDENTALLY, PARK A CAR. AS WE ONLY CHOOSE TO ACCOMMODATE EIGHT (8) CARS (ONE PER UNIT), THE USE OF THE SPACE CAN BE FLEXIBLE AND ADAPTABLE. THE USE OF PERMEABLE UNIT PAVERS WILL ALLOW US TO DESIGN A UNIFORM "RUG" FOR THIS SPACE, PERHAPS WITH A SUBTLE PLAID PATTERN TO AID IN THE USE OF THE SPACE, SO PARKING DOES NOT COME TO DOMINATE THE ALLOCATION OF FOOTPRINT OVER TIME. THE WOONERF ALSO ALLOWS THE INCORPORATION OF PLANTING (GRASSES, SHRUBS, AND/OR TREES) INTO THE PATTERN WHERE PRACTICAL.

LAST, ALL UNITS HAVE A SIGNIFICANT PORTION OF EACH ROOF PLANTED IN AN IRRIGATED GREEN ROOF SYSTEM



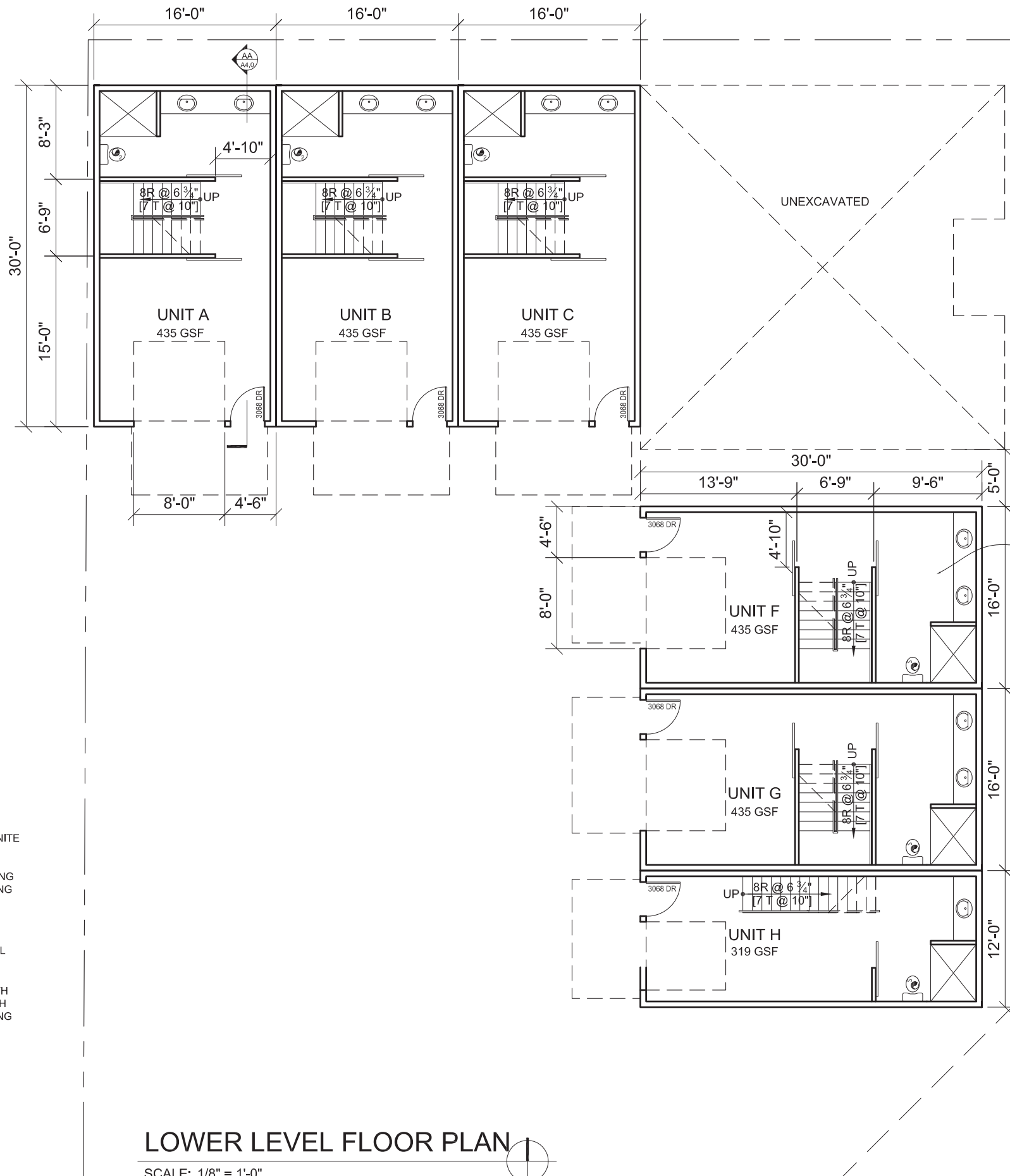
## PARKING PLAN

SCALE: 1"=10'



## WOONERF PAVING PATTERN DETAIL

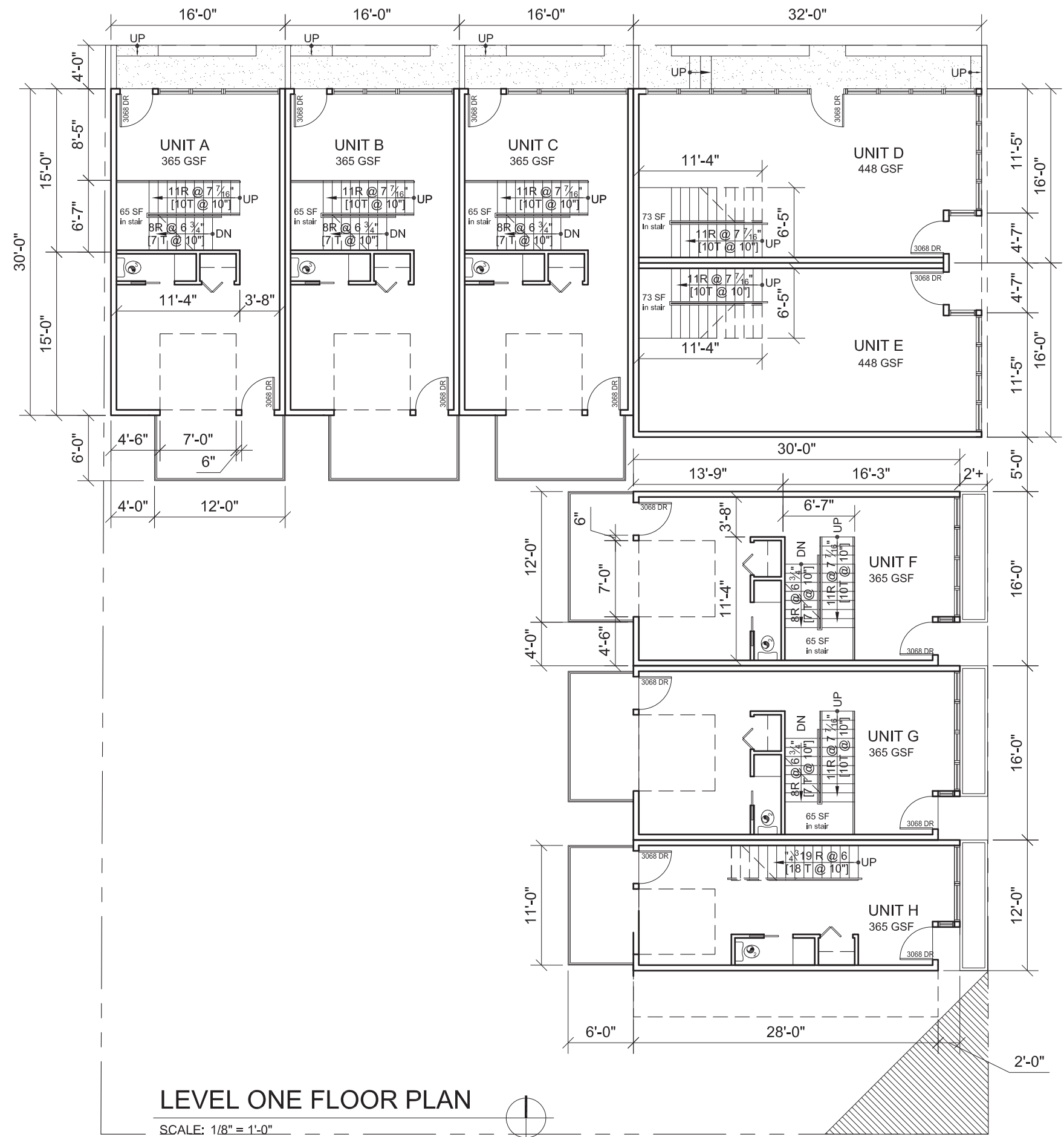
SCALE: 1/4" = 1'-0"

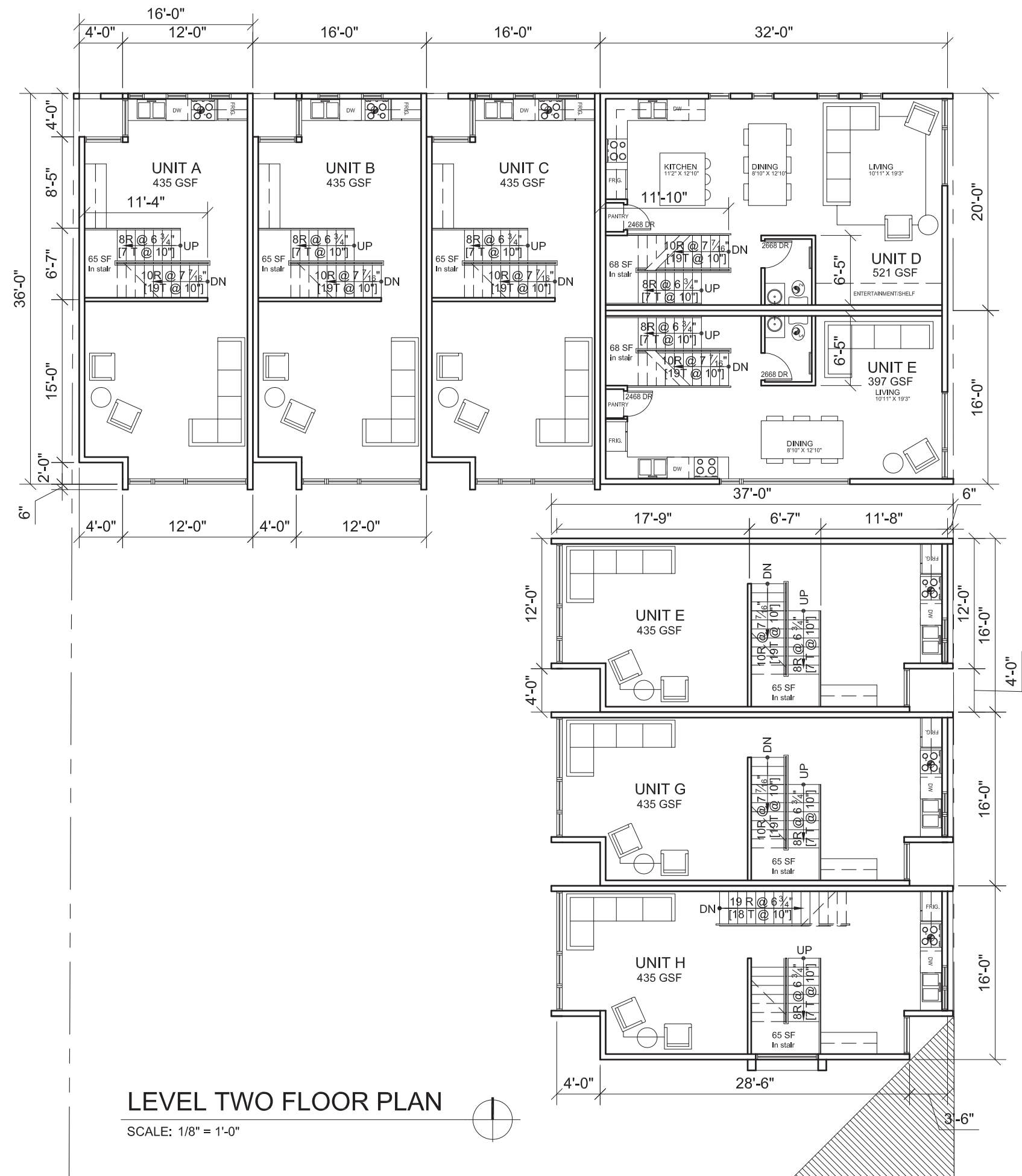


## LOWER LEVEL FLOOR PLAN

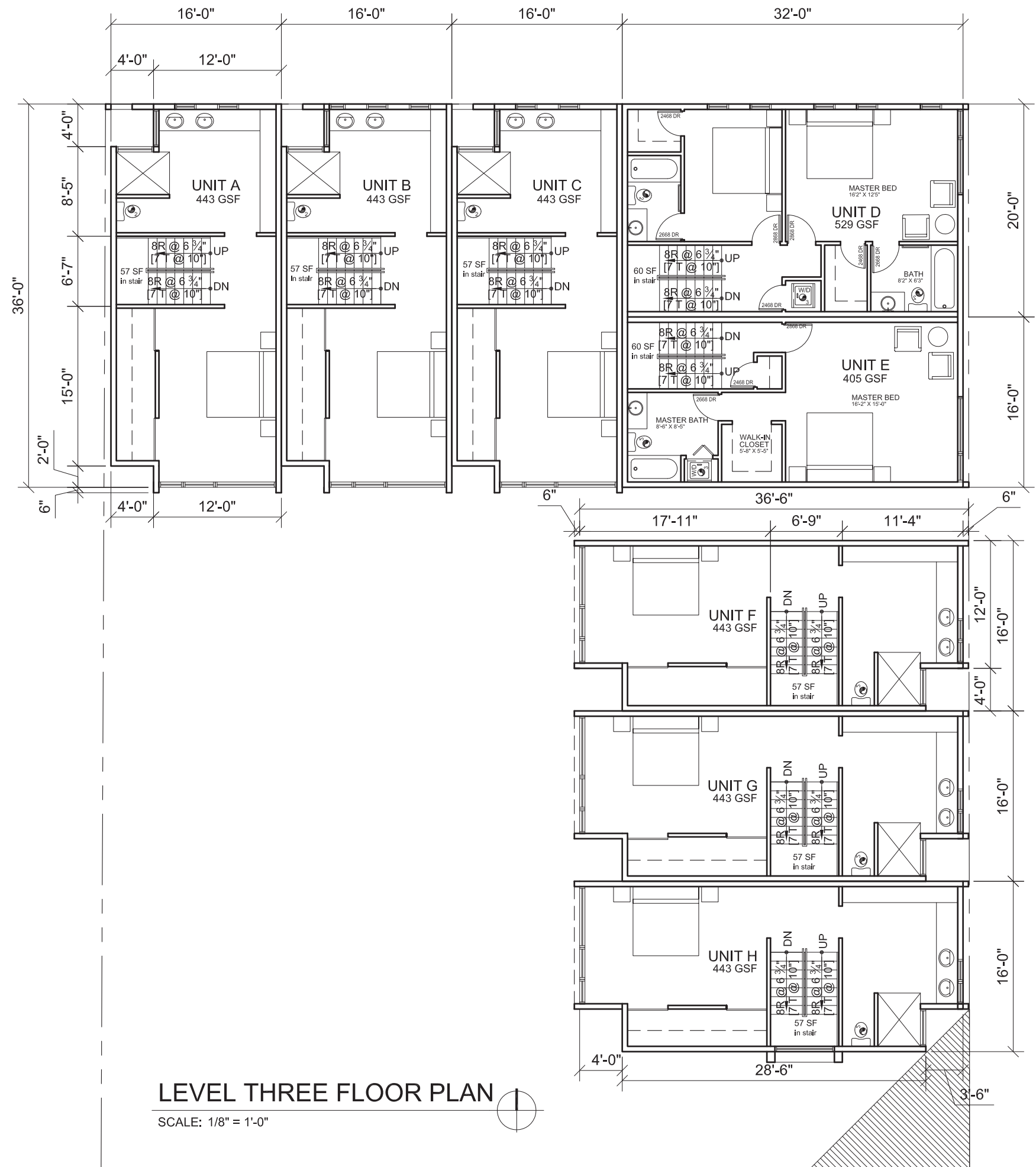
SCALE: 1/8" = 1'-0"

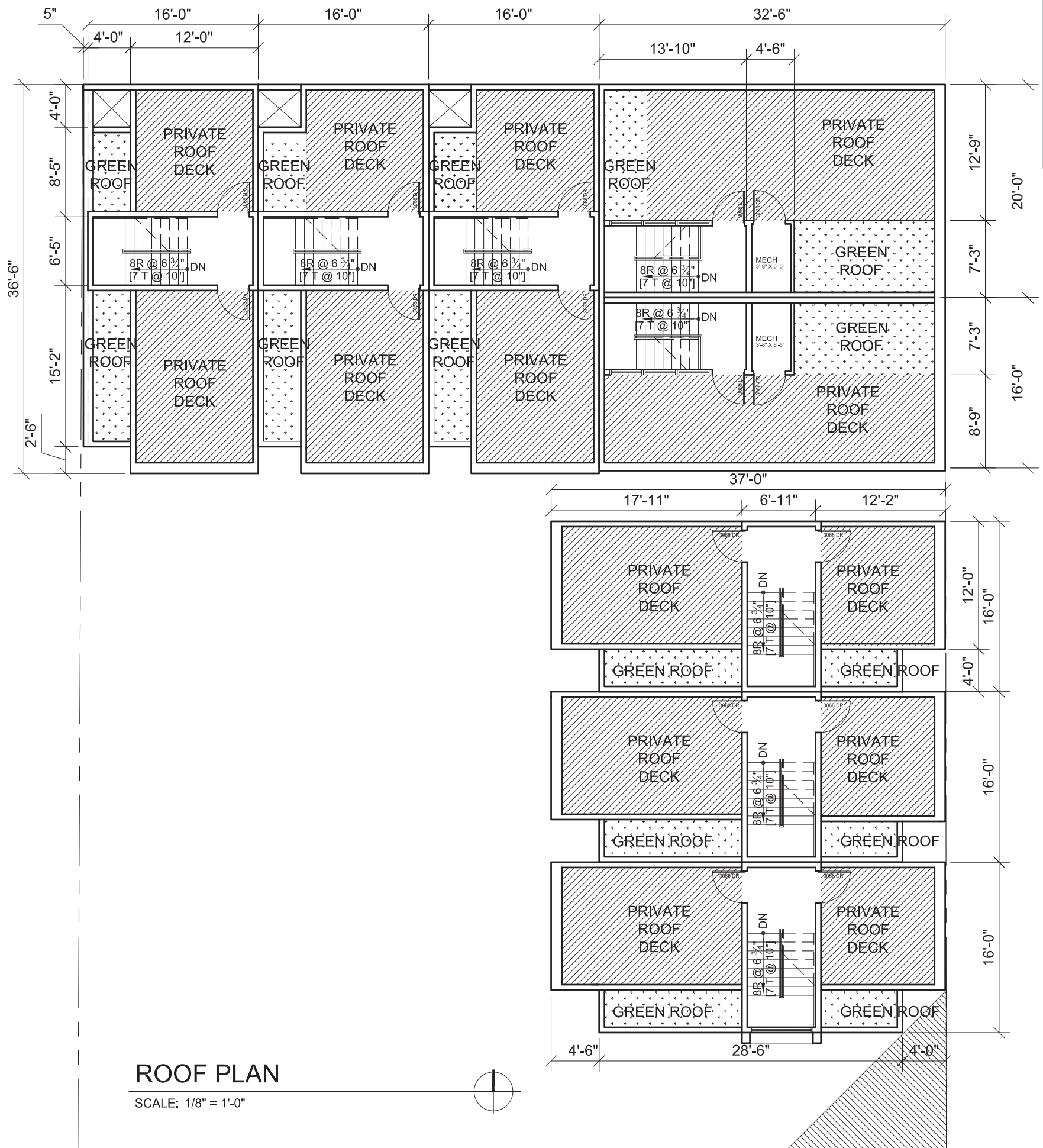












ROOF PLAN

SCALE: 1/8" = 1'-0"

ROOF PLAN



EXTERIOR FINISH SCHEDULE				
TAG	MATERIAL	MANUFACTURER	COLOR/ FINISH	NOTES
C001	24" CORTEN STANDING SEAM	T.B.D.	-	1
C002	ALUMINUM PANELS	T.B.D.	CHARCOAL	2
C003	ALUMINUM PANELS	T.B.D.	GREY	2
C004	CEDAR PLANKS	-	-	-
C005	CONCRETE		FAIR-FACED	

NOTES:

1. PROVIDE 1/4" RELIEF AT ALL SEAMS

ELEVATION NOTES:

1. ALL EXPOSED CONCRETE SHALL HAVE 1 COAT PERMEABLE SILFANE SILOXIDE SEALER.

2. ALL DECK RAILINGS SHALL BE 36" A.F.F. MIN. WITH 4" MAX. INFILL. RAILING SHALL BE PROPERLY SECURED TO DECK AND RATED TO WITHSTAND A MIN. 200 LB PULL-OUT, PER CODE.

3. ALL OVER-FLOW SCUPPERS SHALL BE PLACED 2" ABOVE LOWEST DRAINAGE AREA AT DECKS.



BLDG 1 NORTH ELEVATION

SCALE: 1/8" = 1'-0"

EXTERIOR FINISH SCHEDULE				
TAG	MATERIAL	MANUFACTURER	COLOR/ FINISH	NOTES
C001	24" CORTEN STANDING SEAM	T.B.D.	-	1
C002	ALUMINUM PANELS	T.B.D.	CHARCOAL	2
C003	ALUMINUM PANELS	T.B.D.	GREY	2
C004	CEDAR PLANKS	-	-	-
C005	CONCRETE		FAIR-FACED	

NOTES:

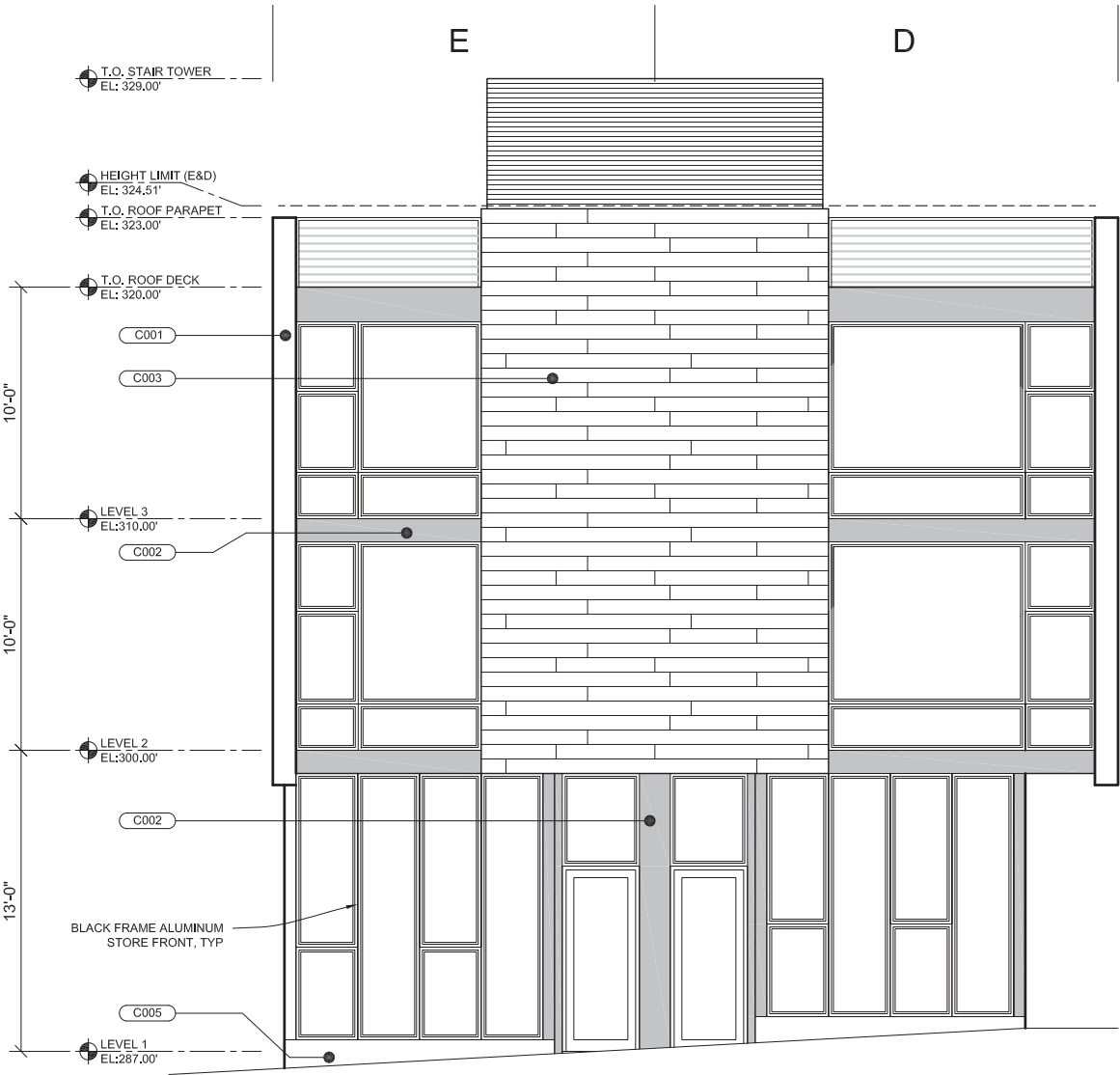
1. PROVIDE 1/4" RELIEF AT ALL SEAMS

ELEVATION NOTES:

1.ALL EXPOSED CONCRETE SHALL HAVE 1 COAT PERMEABLE SILFANE SILOXIDE SEALER.

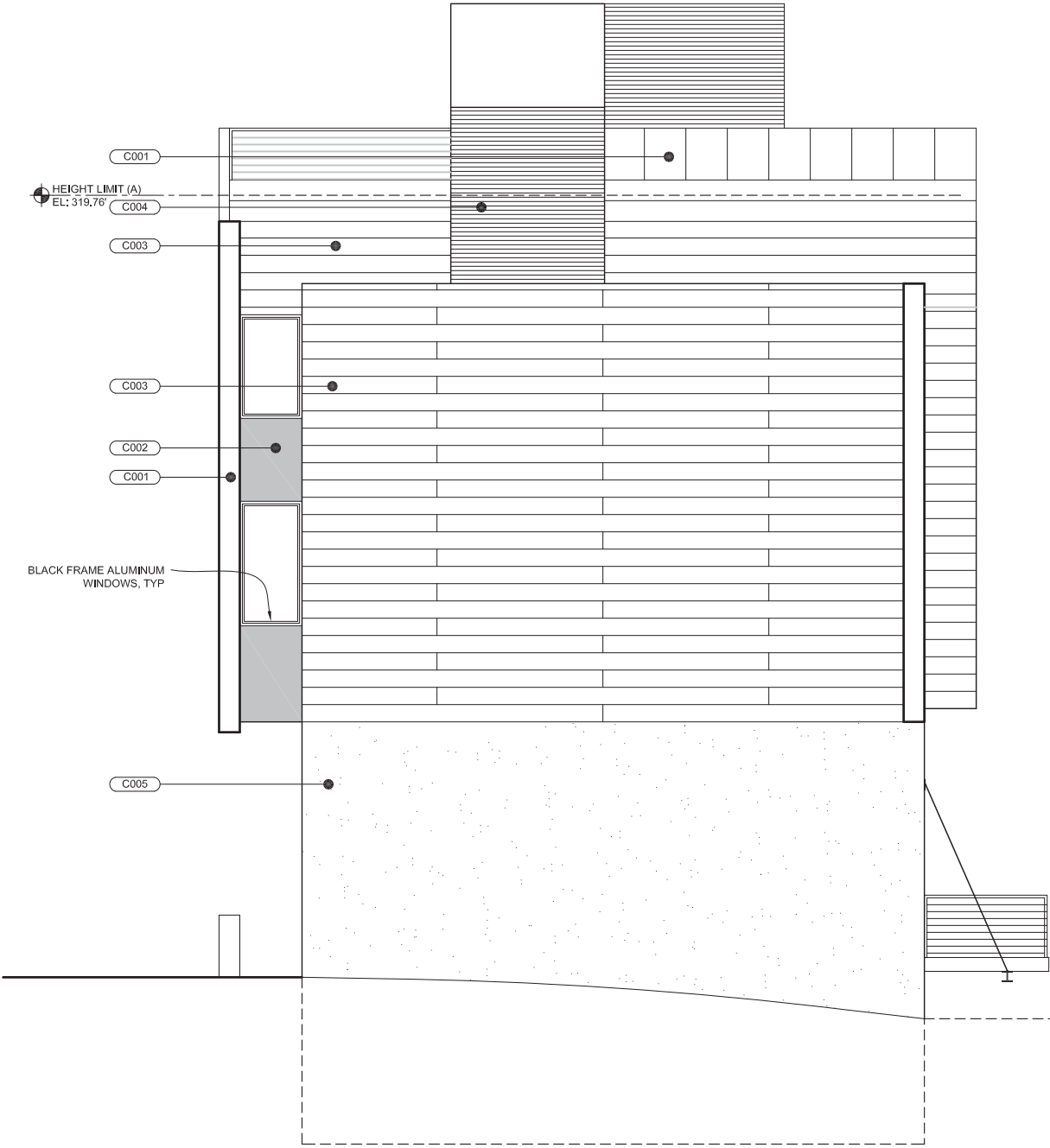
2.ALL DECK RAILINGS SHALL BE 36" A.F.F. MIN, WITH 4" MAX. INFILL, RAILING SHALL BE PROPERLY SECURED TO DECK AND RATED TO WITHSTAND A MIN. 200 LB PULL-OUT, PER CODE.

3. ALL OVER-FLOW SCUPPERS SHALL BE PLACED 2" ABOVE LOWEST DRAINAGE AREA AT DECKS.



BLDG 1 EAST ELEVATION

SCALE: 1/8" = 1'-0"



BLDG 1 WEST ELEVATION

SCALE: 1/8" = 1'-0"



EXTERIOR FINISH SCHEDULE				
TAG	MATERIAL	MANUFACTURER	COLOR/ FINISH	NOTES
C001	24" CORTEN STANDING SEAM	T.B.D.	-	1
C002	ALUMINUM PANELS	T.B.D.	CHARCOAL	2
C003	ALUMINUM PANELS	T.B.D.	GREY	2
C004	CEDAR PLANKS	-	-	-
C005	CONCRETE		FAIR-FACED	

NOTES:

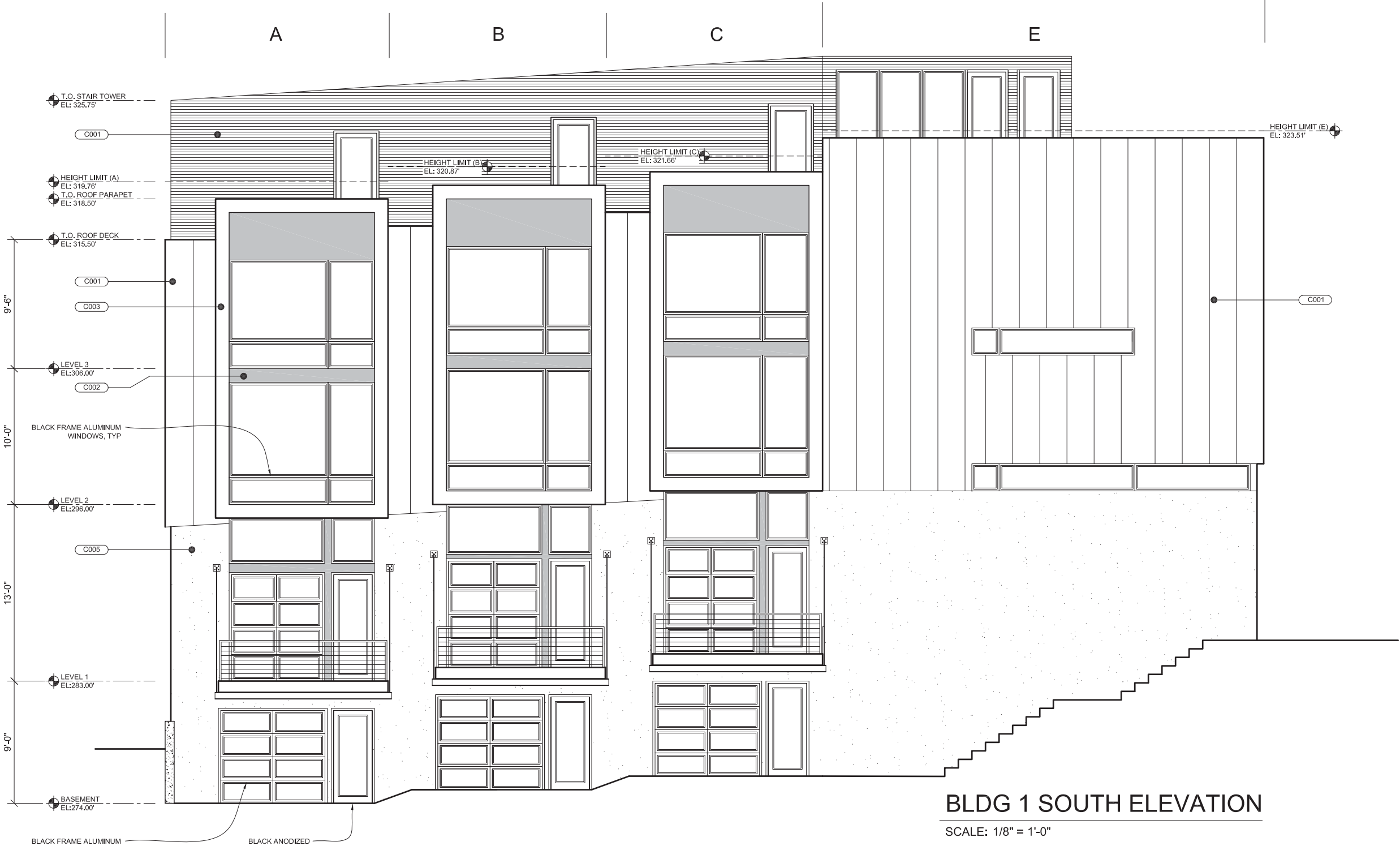
1. PROVIDE 1/4" RELIEF AT ALL SEAMS

ELEVATION NOTES:

1.ALL EXPOSED CONCRETE SHALL HAVE 1 COAT PERMEABLE SILFANE SILOXIDE SEALER.

2.ALL DECK RAILINGS SHALL BE 36" A.F.F. MIN. WITH 4" MAX. INFILL. RAILING SHALL BE PROPERLY SECURED TO DECK AND RATED TO WITHSTAND A MIN. 200 LB PULL-OUT, PER CODE.

3. ALL OVER-FLOW SCUPPERS SHALL BE PLACED 2" ABOVE LOWEST DRAINAGE AREA AT DECKS.



BLDG 1 SOUTH ELEVATION

SCALE: 1/8" = 1'-0"

EXTERIOR FINISH SCHEDULE				
TAG	MATERIAL	MANUFACTURER	COLOR/ FINISH	NOTES
C001	24" CORTEN STANDING SEAM	T.B.D.	-	1
C002	ALUMINUM PANELS	T.B.D.	CHARCOAL	2
C003	ALUMINUM PANELS	T.B.D.	GREY	2
C004	CEDAR PLANKS	-	-	-
C005	CONCRETE		FAIR-FACED	

NOTES:

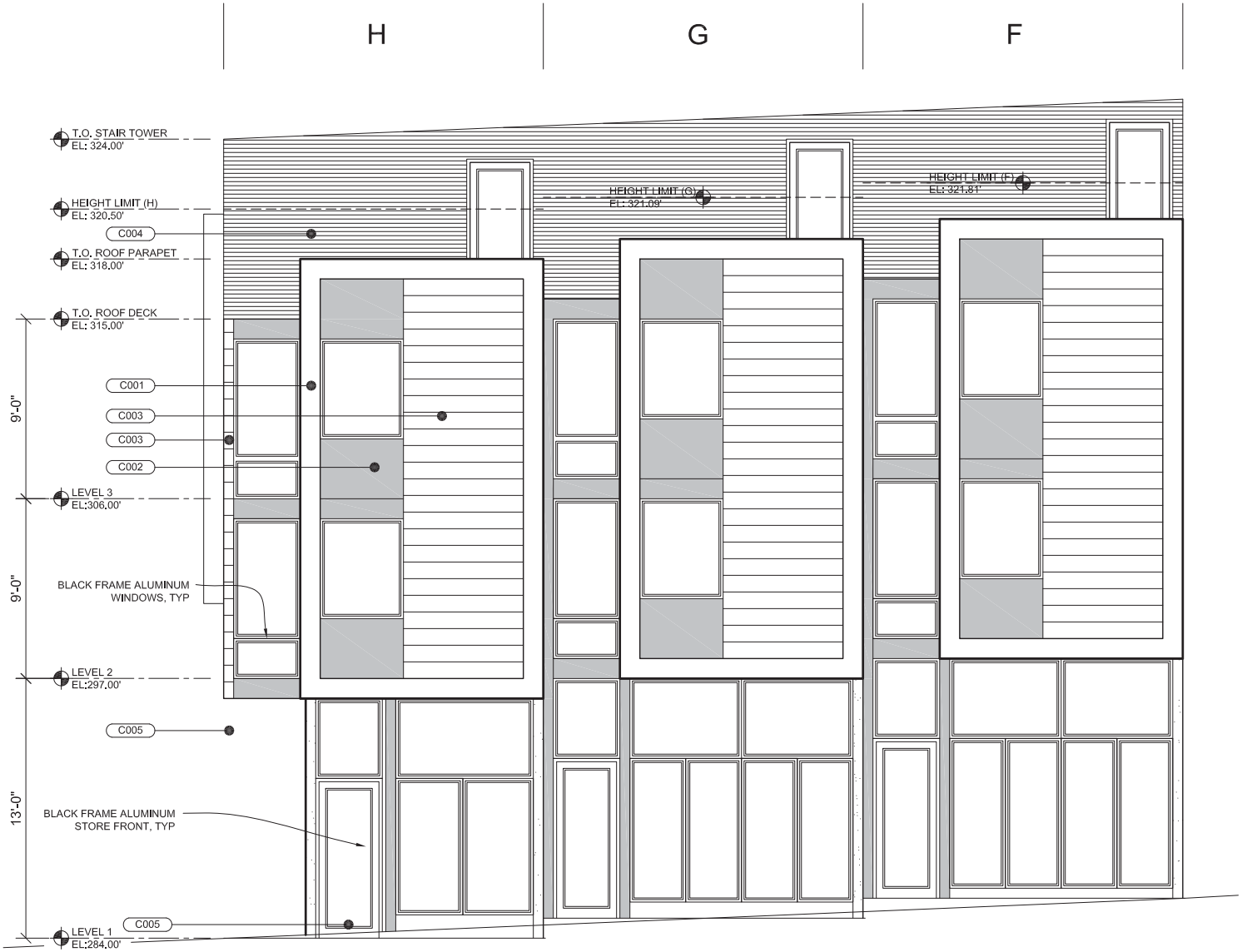
1. PROVIDE 1/4" RELIEF AT ALL SEAMS

ELEVATION NOTES:

1.ALL EXPOSED CONCRETE SHALL HAVE 1 COAT PERMEABLE SILFANE SILOXIDE SEALER.

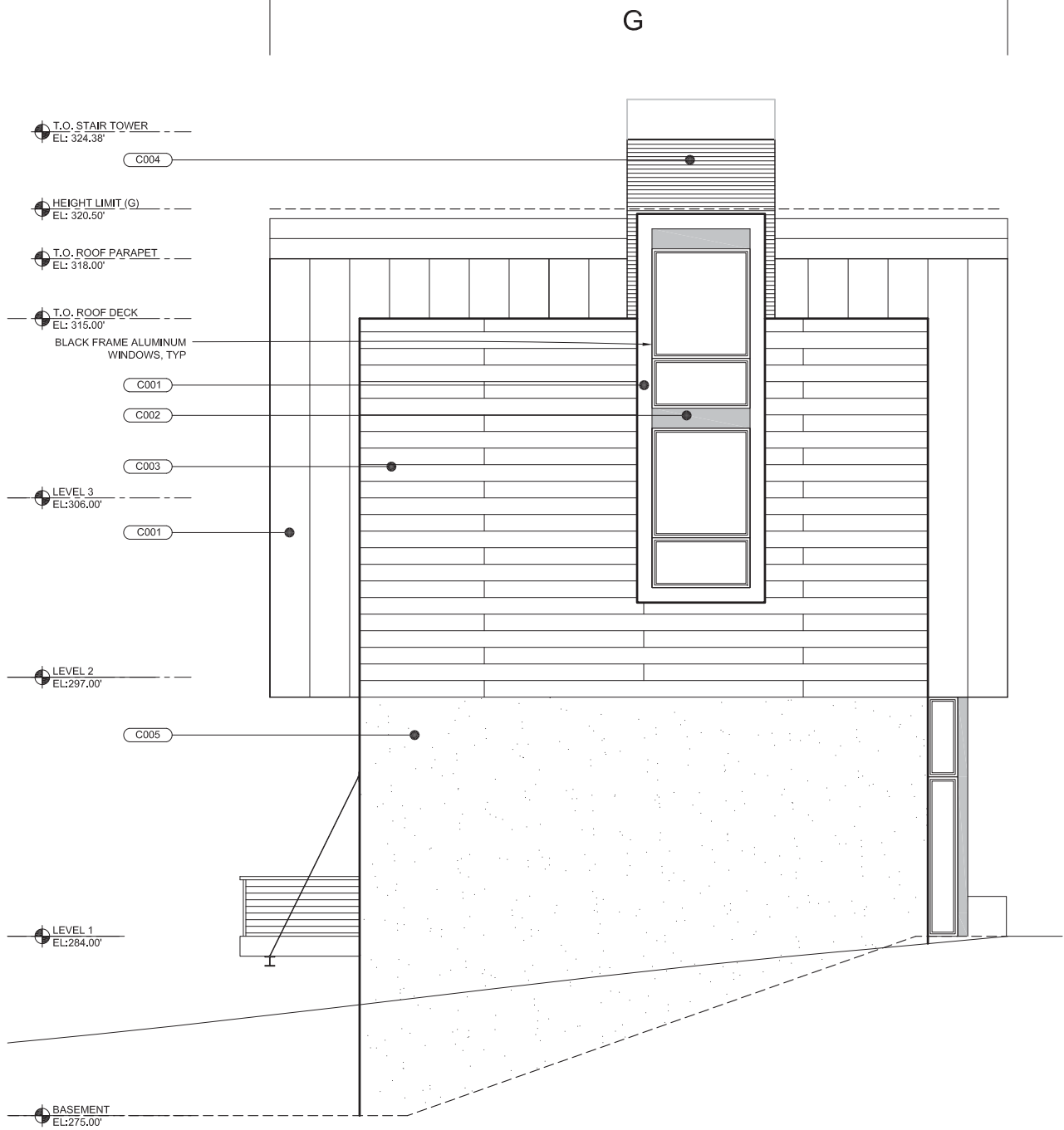
2.ALL DECK RAILINGS SHALL BE 36" A.F.F. MIN. WITH 4" MAX. INFILL. RAILING SHALL BE PROPERLY SECURED TO DECK AND RATED TO WITHSTAND A MIN. 200 LB PULL-OUT, PER CODE.

3. ALL OVER-FLOW SCUPPERS SHALL BE PLACED 2" ABOVE LOWEST DRAINAGE AREA AT DECKS.



BLDG 2 EAST ELEVATION

SCALE: 1/8" = 1'-0"



BLDG 2 SOUTH ELEVATION

SCALE: 1/8" = 1'-0"



EXTERIOR FINISH SCHEDULE				
TAG	MATERIAL	MANUFACTURER	COLOR/ FINISH	NOTES
C001	24" CORTEN STANDING SEAM	T.B.D.	-	1
C002	ALUMINUM PANELS	T.B.D.	CHARCOAL	2
C003	ALUMINUM PANELS	T.B.D.	GREY	2
C004	CEDAR PLANKS	-	-	-
C005	CONCRETE		FAIR-FACED	

NOTES:

1. PROVIDE 1/4" RELIEF AT ALL SEAMS

ELEVATION NOTES:

1. ALL EXPOSED CONCRETE SHALL HAVE 1 COAT PERMEABLE SILFANE SILOXIDE SEALER.

2. ALL DECK RAILINGS SHALL BE 36" A.F.F. MIN. WITH 4" MAX. INFILL. RAILING SHALL BE PROPERLY SECURED TO DECK AND RATED TO WITHSTAND A MIN. 200 LB PULL-OUT, PER CODE.

3. ALL OVER-FLOW SCUPPERS SHALL BE PLACED 2" ABOVE LOWEST DRAINAGE AREA AT DECKS.

15TH LIVE WORK  
355 15TH AVE

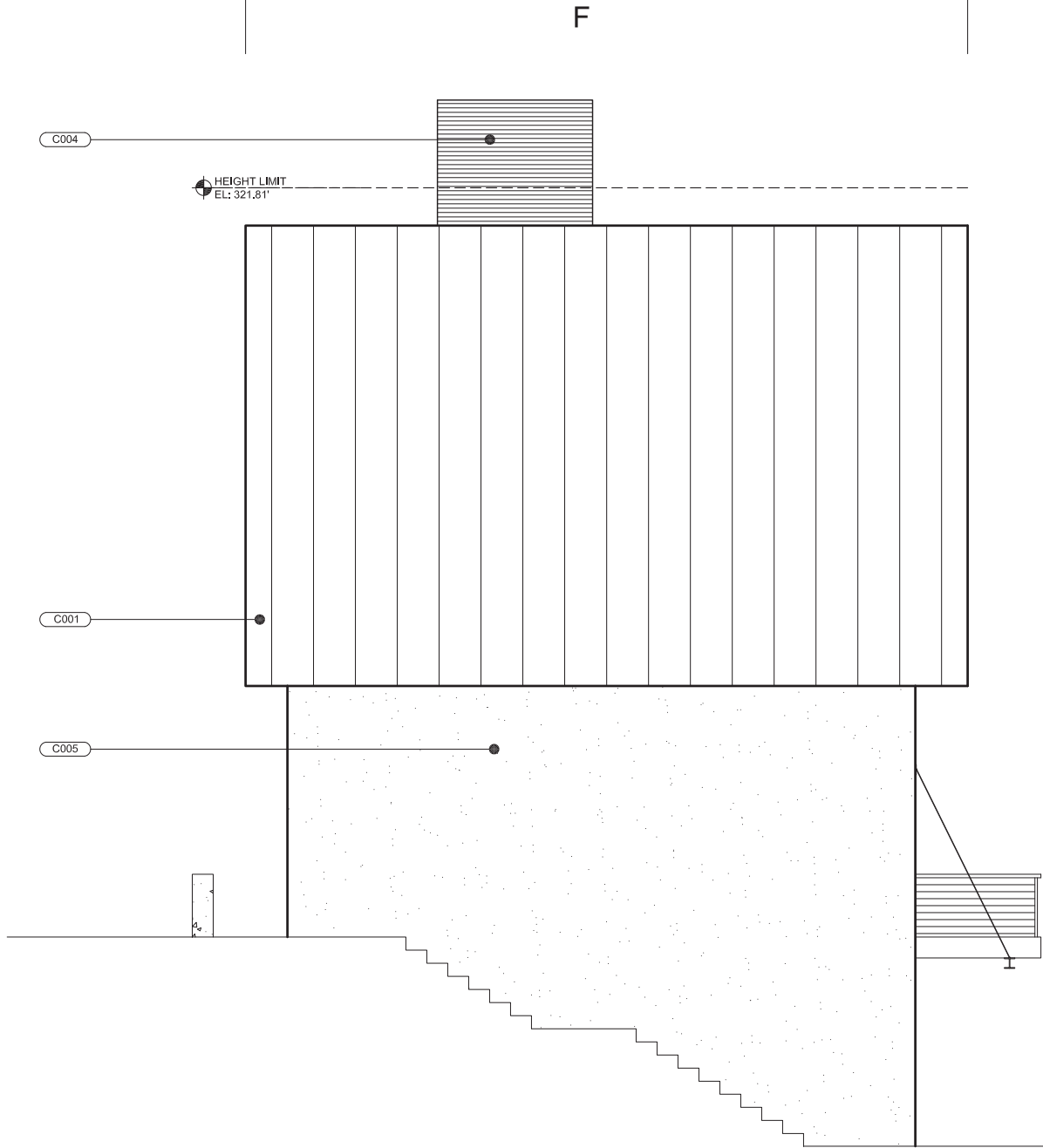
DESIGN  
REVIEW

Jan 15th 2013



BLDG 2 WEST ELEVATION

SCALE: 1/8" = 1'-0"



BLDG 2 NORTH ELEVATION

SCALE: 1/8" = 1'-0"

BUILDING  
TWO  
ELEVATIONS



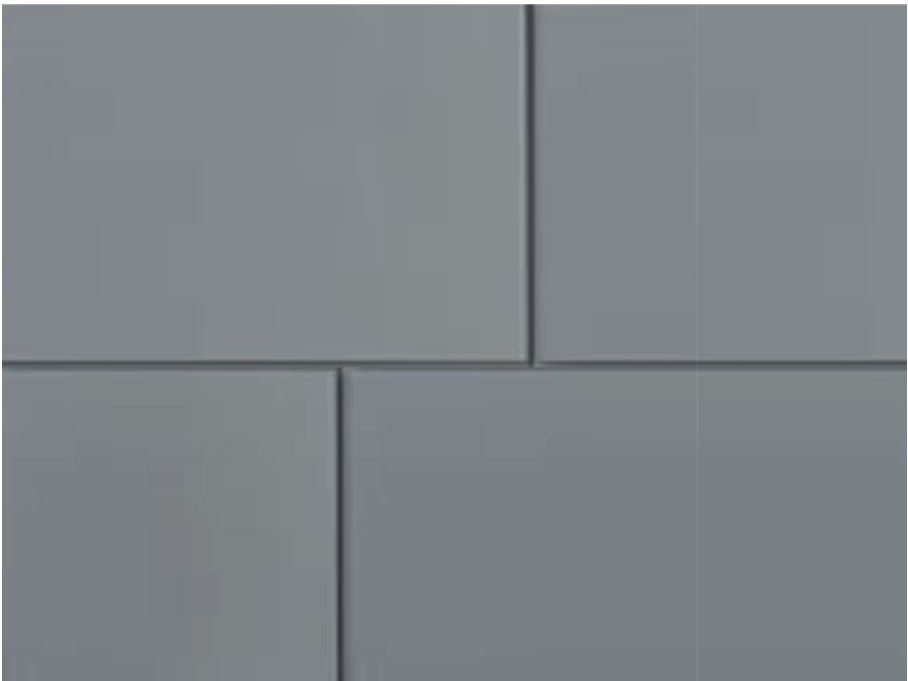
CONCRETE



ALUMINUM STOREFRONT



STAINED CEDAR



ALUMINUM PANEL SIDING



STANDING SEAM CORTEN STEEL SIDING





BUILDING ADJACENT SITE ON JEFFERSON STREET



PARKING STRUCTURE ACROSS INTERSECTION OF 15TH AVENUE AND JEFFERSON STREET



NEW CONSTRUCTION ACROSS JEFFERSON STREET



15TH AVENUE & JEFFERSON STREET CORNER BUILDING SITE





## DEPARTURE REQUEST

SCALE: 1/32" = 1'



## DESIGN DEPARTURE REQUESTS - GENERAL

Allow two-way access to and from Woonerf for project at 355 15th Avenue. Two-way access via 14-foot driveway from 15th Avenue.

The space (used for resident parking) is designed as a Woonerf, per SMC 23.84A.044-"W". The emphasis is on pedestrian access and recreational use, with sufficient hard landscape surface provided for incidental parking. The Woonerf is paved to allow for infiltration of runoff, and is planted with Italian Cypress to define a series of outdoor rooms. As the individual row house units are each less than 1500 SF, no parking is required per SMC 23.54.015-D.

SMC 23.84A.044-"W". "Woonerf" means a common space shared by pedestrians, bicyclists and vehicles, used for vehicular access, in which amenities such as trees, planters, and seating serve to impede vehicular movement and provide opportunities for outdoor use by occupants of abutting structures. A woonerf is intended and designed to prioritize pedestrian movement and safety, through features such as pavers and pervious ground surfaces that slow vehicular movement.

### Background

Property at the southwest corner of 15th Avenue and Jefferson Street has an existing curb cut on 15th Avenue, plus an existing 10-foot driveway on the south property line. Parcel is zoned NC1-30, as is the adjacent parcel to the west (1415 Jefferson Street).

### Benefit

The relationship between adjacent zones is the crucial issue here. The scale of the proposed access is more compatible with the existing driveways on 15th Avenue. The existing ten (10) foot curb cut on 15th can be re-used. The existing ten (10) foot wide driveway can be widened to 14-feet. Two-way in and out (from 15th) will minimize traffic impact on neighbors. Limiting vehicles to residents further reduces trips. Creating off-street parking, even when not required by Code, minimizes the on street parking impact on neighborhood. Small-scale live-work spaces are not likely to generate many neighborhood trips (minimum number of customer service uses).

I zoning.

## VEHICLE ACCESS

Section  
23.047A.032-A.1.c.

### Required

If access not provided from alley, and site abuts two streets, access is permitted from side street.

### Provided

Access IN and OUT provided from side street (15th Avenue).

### Benefit

No curb cut on front street (Jefferson Street, per DPD, SMC 23.047.032.C). Residential scale curb cut and driveway width are consistent with the character of 15th Avenue.

## DRIVEWAY

Section  
23.54.030-D.2.a.2

### Required

The minimum width of driveways for two-way traffic shall be 22 feet and the maximum width shall be 25 feet.

### Provided.

The driveway, for entry and exit from 15th Avenue, is 14 feet wide. Driveway is approximately 47 feet long. Slope of driveway is 15% or less.

### Benefit

Minimal disturbance to the existing adjacent property. The driveway is appropriately scaled for adjacent residential zoning.

## CURB CUT

### Required

One curb cut greater than 10 feet but in no case greater than 20 feet in width may be substituted for each two curb cuts permitted by subsection 23.54.030.F.1.a. Curb cut to be width of driveway.

### Provided.

Reuse existing 10 feet wide curb cut.

### Benefit

Minimal disturbance to the existing adjacent property. The driveway is appropriately scaled for adjacent residential zoning.





**SIGHT TRIANGLE**

23.54.030-G.7.

**Required**

Sight triangles are required for two-way entrances into a parking garage or surface parking area.

**Provided**

Convex mirrors at driveway exit, one each side. The driveway, for entry from, and exit to, 15th Avenue is designated two-way. It is 14 feet wide and adjacent to the south property line. Site triangle for exit to south could come from adjacent property, as the front setback for a property zoned R-5000, is 25 feet. Provide convex mirror at both south and north sides of exit from 14 foot drive way.

**Benefit**

Minimal disturbance to the existing adjacent property. Both driveway and/or curb cut are appropriately scaled for adjacent residential zoning.

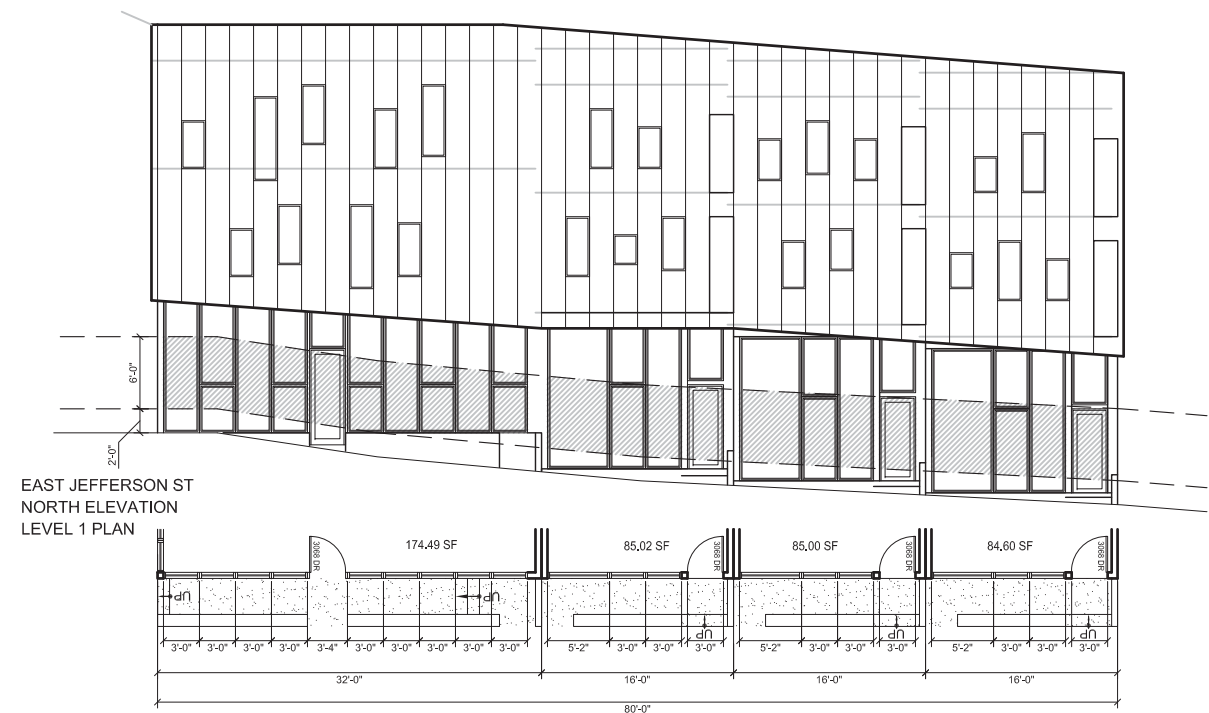
**DEPARTURE REQUEST**

SCALE: 1/32" = 1'

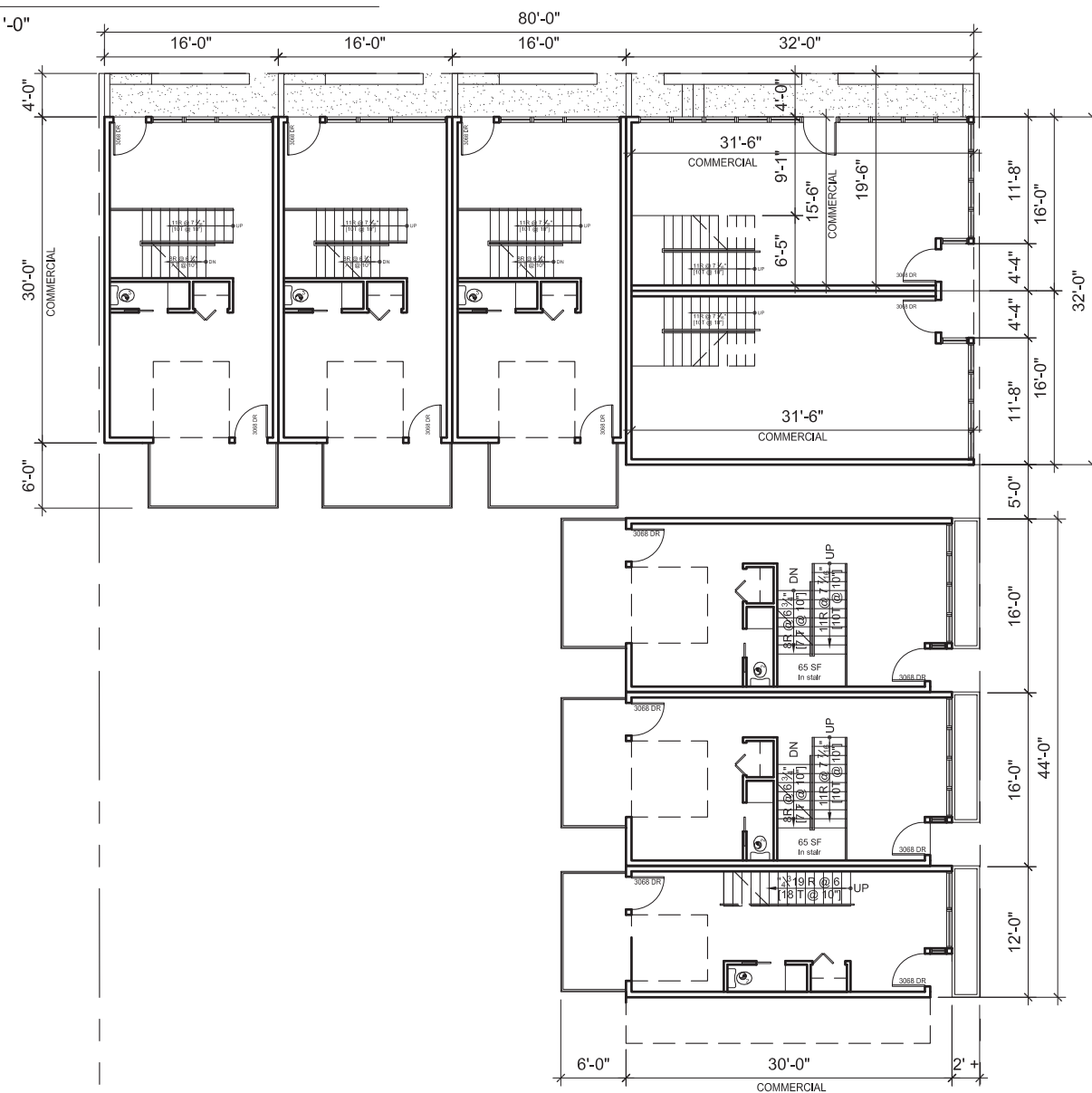




an 15th 2013



SCALE: 1/8"=1'-0"



PER SMC 23.47A.008

STREET-FACING FACADES ARE REQUIRED TO BE 60% TRANSPARENT BETWEEN 2' & 8'  
15TH AVE (SOUTH EAST): 44'-0" X 6' = 264 SF REQ. X .6 = 158.4 SF REQUIRED  
236.0 SF PROVIDED

15TH AVE (NORTH EAST):      32'-0" X 6' = 192 SF REQ. X .6 = 115.2 SF REQUIRED  
169.0 SF PROVIDED

EAST JEFFERSON ST (NORTH)     $80'-0" \times 6' = 480 \text{ SF REQ.}$   $\times .6 = 288.0 \text{ SF REQUIRED}$   
429.1 SF PROVIDED

BLANK FACADES MAY NOT EXCEED 20'-0"  
 LONGEST BLANK FACADE = E. JEFFERSON: 1'-8"  
 15TH AVE: 1'-8"

FOR ADDITIONAL INFORMATION & DIMENSIONS SEE FLOOR PLANS AND ELEVATIONS

SCALE: 1/8"=1'-0"

PER SMC 23.47A.008.B.3.a NONRESIDENTIAL USES SHALL EXTEND AN AVERAGE OF AT LEAST 30 FEET AND A MINIMUM OF 15 FEET IN DEPTH FROM THE STREET LEVEL FAÇADE.

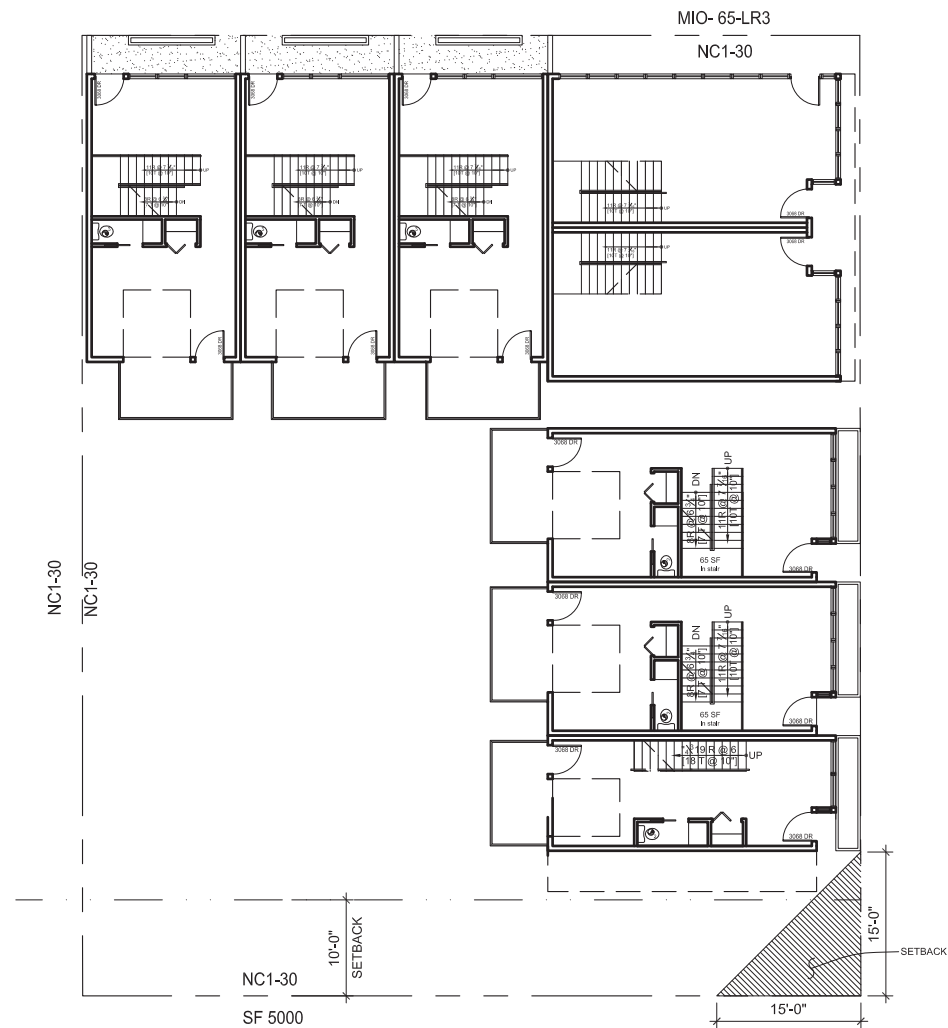
NON-RESIDENTIAL DEPTH: AVE. 30', MIN 15' REQUIRED

PROVIDED: E. JEFFERSON AVG: 30'  
MIN: 30'

PROVIDED:	15TH AVE	AVG: 30'
		MIN: 30'

## LAND USE DIAGRAMS





## DIAGRAM: SETBACKS

SCALE: 1" = 10'-0"

PER SMC 23.47A.014.B.1 A SETBACK IS REQUIRED WHERE A LOT ABUTS THE INTERSECTION OF A SIDE LOT LINE AND A FRONT LOT LINE OF A LOT IN A RESIDENTIAL ZONE. THE REQUIRED SETBACK FORMS A TRIANGULAR AREA.

SETBACK PROVIDED.

PER SMC 23.47A.014.B.2.a A SETBACK IS REQUIRED ALONG ANY REAR OR SIDE LOT LINE THAT ABUTS A LOT IN A RESIDENTIAL ZONE, AS FOLLOWS: TEN FEET FOR PORTIONS OF STRUCTURES ABOVE 13 FEET IN HEIGHT TO A MAXIMUM OF 65 FEET.

10'-0" SETBACK REQUIRED, 11'-0 PROVIDED.

## DIAGRAM: F.A.R.

SCALE: 1" = 30'-0"

PER SMC 23.47A.13 PERMITTED F.A.R. IS 2.5

LOT AREA 8688 SF

LEVEL	GFA	FAR
LL	(2,610)	N/A
L1	2,986	.34
L2	3,524	.41
L3	3,592	.41
ROOF	347	.04

TOTAL 10,449 1.20

F.A.R. 2.5 ALLOWED  
F.A.R. 1.22 PROPOSED

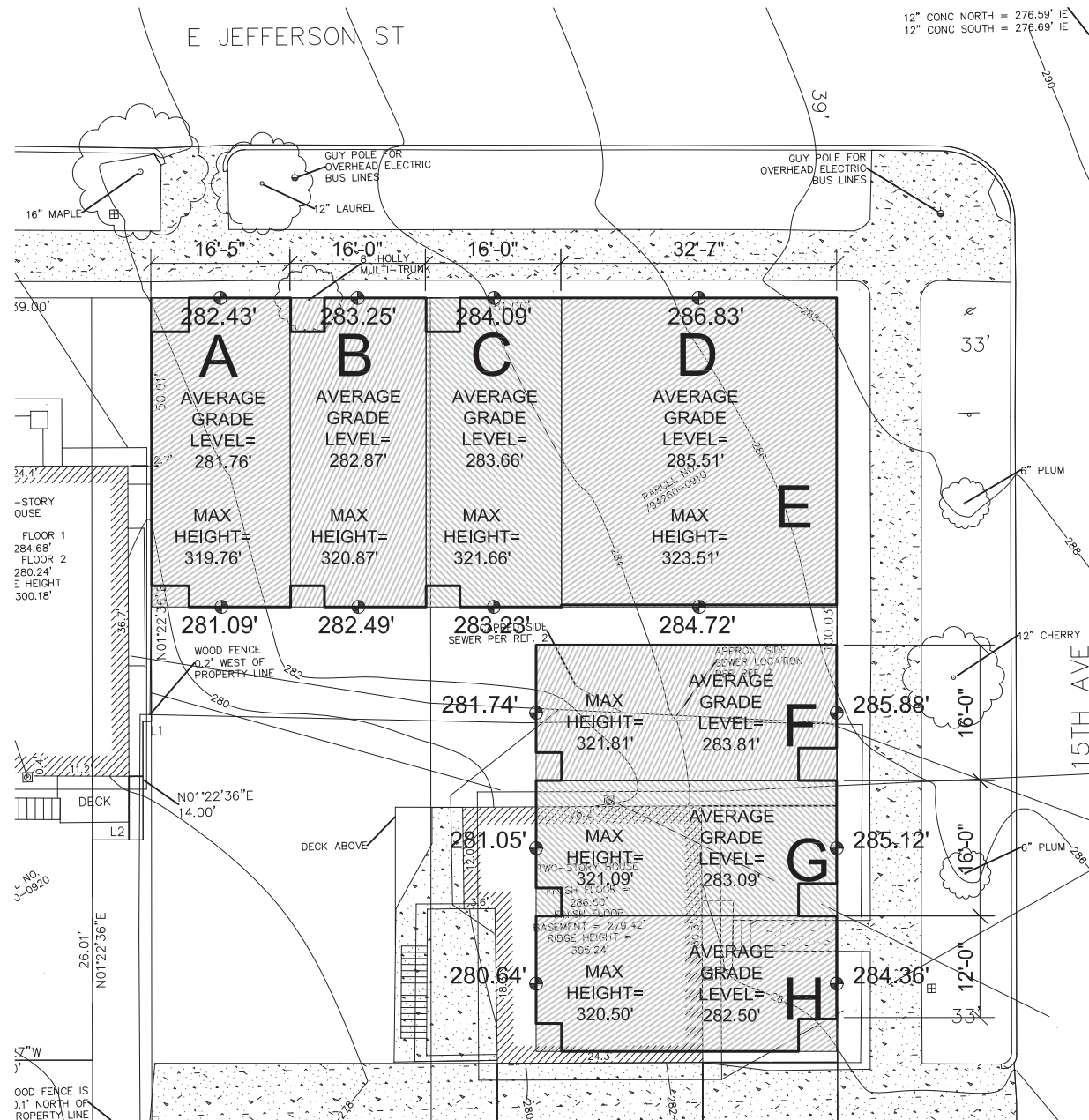
ROOF TOP LEVEL  
347 SF  
FAR: .04

LEVEL 3  
3,592 SF  
FAR: .41

LEVEL 2  
3,524 SF  
FAR: .41

LEVEL 1  
2,986 SF  
FAR: .34

(UNFINISHED)  
LOWER LEVEL  
2,610 SF  
NOT COUNTED IN FAR



PER SMC 23.86.006 AND DR 4-2012  
MEAN ELEVATION  
+30' HEIGHT  
+ 4' PARAPET HEIGHT  
+ 4' PER SEC 23.47A.012.A.1.a  
+38' ALLOWED ABOVE AVERAGE GRADE LEVEL

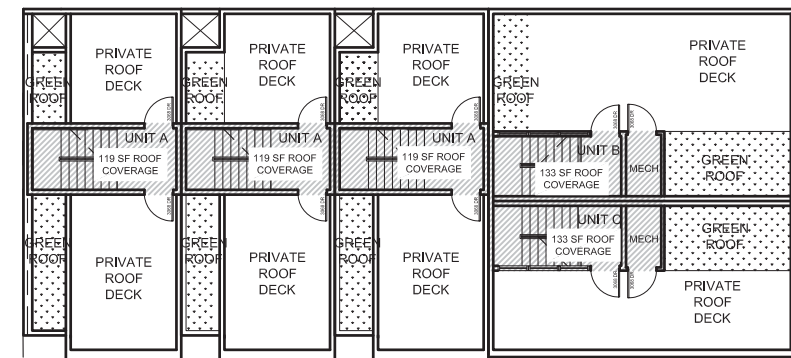
DIAGRAM: HEIGHT CALCULATION

SCALE: 1" = 20'-0"

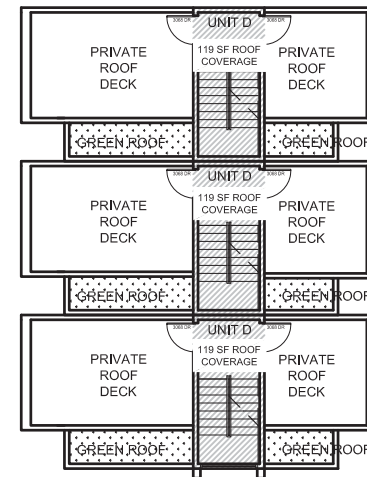


PER SMC 23.47A.012.C.4 - THE FOLLOWING  
ROOFTOP FEATURES MAY EXTEND UP TO 16 FEET  
ABOVE THE APPLICABLE HEIGHT LIMIT, AS LONG  
AS THE COMBINED TOTAL COVERAGE OF ALL  
FEATURES GAINING ADDITIONAL HEIGHT LISTED IN  
THIS SUBSECTION 23.47A.012.C.4 DOES NOT  
EXCEED 20 PERCENT OF THE ROOF AREA, OR 25  
PERCENT OF THE ROOF AREA IF THE TOTAL  
INCLUDES STAIR OR ELEVATOR PENTHOUSES OR  
SCREENED MECHANICAL EQUIPMENT

OVERALL ROOF AREA: 4550 SF  
ROOF COVERAGE: 980 SF (22%)



OVERALL ROOF AREA: 2868 SF  
ROOF COVERAGE: 623 SF (22%)



OVERALL ROOF AREA: 1682 SF  
ROOF COVERAGE: 357 SF (22%)

DIAGRAM: ROOF TOP COVERAGE

SCALE: 1" = 20'-0"



PER SMC 23.54.040 - SOLID WASTE AND  
RECYCLABLE MATERIALS STORAGE AND ACCESS:

COMMERICAL USE 5,001-15,000 SQUARE FEET : 125  
SQUARE FEET

REQUIRED: 125/8 = 15.62 SQFT  
PROVIDED: 15.62 SQFT PER UNIT

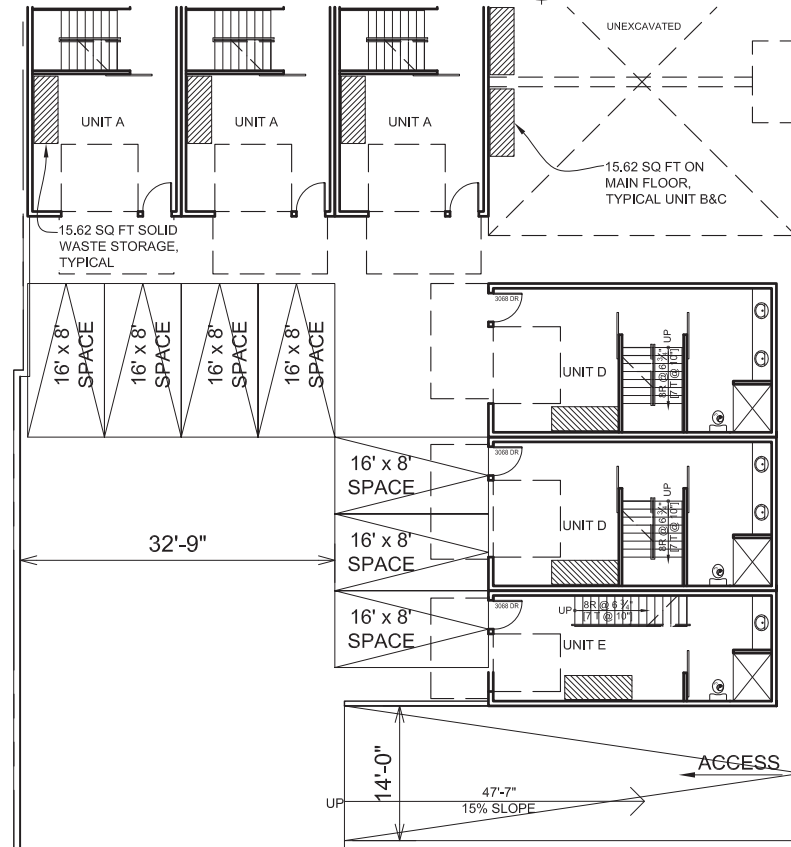


DIAGRAM: SOLID WASTE STORAGE AND ACCESS

SCALE: 1" = 20'-0"

