

Design Review Board Package

THE CAMEO 1527 15TH AVENUE SEATTLE, WA 98112

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STRAZZARA

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PLANNING
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PROPOSED DEPARTURE #1:

ALLOW A CURB CUT AND VEHICLE ACCESS TO THE PARKING GARAGE TO REMAIN ON EAST PINE STREET AS CURRENTLY DESIGNED AND PERMITTED, RATHER THAN REQUIRING RELOCATION TO 15TH AVENUE PER SMC 23.47A.015

PROPOSED DEPARTURE #2:

EITHER WAIVE THE REQUIREMENT FOR A DRIVEWAY SIGHT TRIANGLE UNDER SMC 23.54.030, OR, REDUCE THE COMMERCIAL FRONTAGE REQUIREMENT UNDER SMC 23.47A.005 BY TEN (10) FEET TO ALLOW A DRIVEWAY SIGHT TRIANGLE AT THE PROPOSED GARAGE ENTRANCE.

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1 VICINITY PLAN
SCALE: NOT TO SCALE

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TITLE SHEET

T-1

EAST ELEVATIONS

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PARAMOUNT
APARTMENTS

PARAMOUNT
APARTMENTS

EXISTING
BUILDING PERMIT SET (ISSUED 2006)

PROPOSED
NO CHANGE

EAST ELEVATION (15th Avenue)

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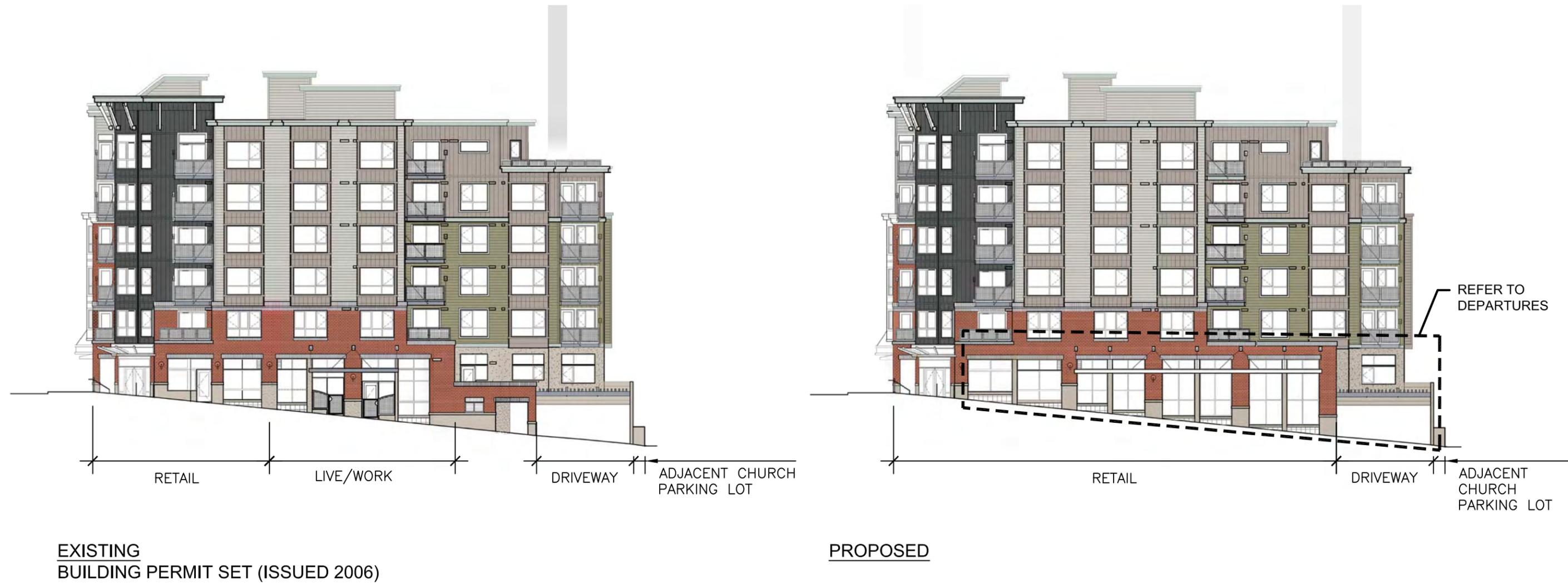
EAST ELEVATIONS

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NORTH ELEVATIONS

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NORTH ELEVATION (E. PINE STREET)

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EAST ELEVATIONS

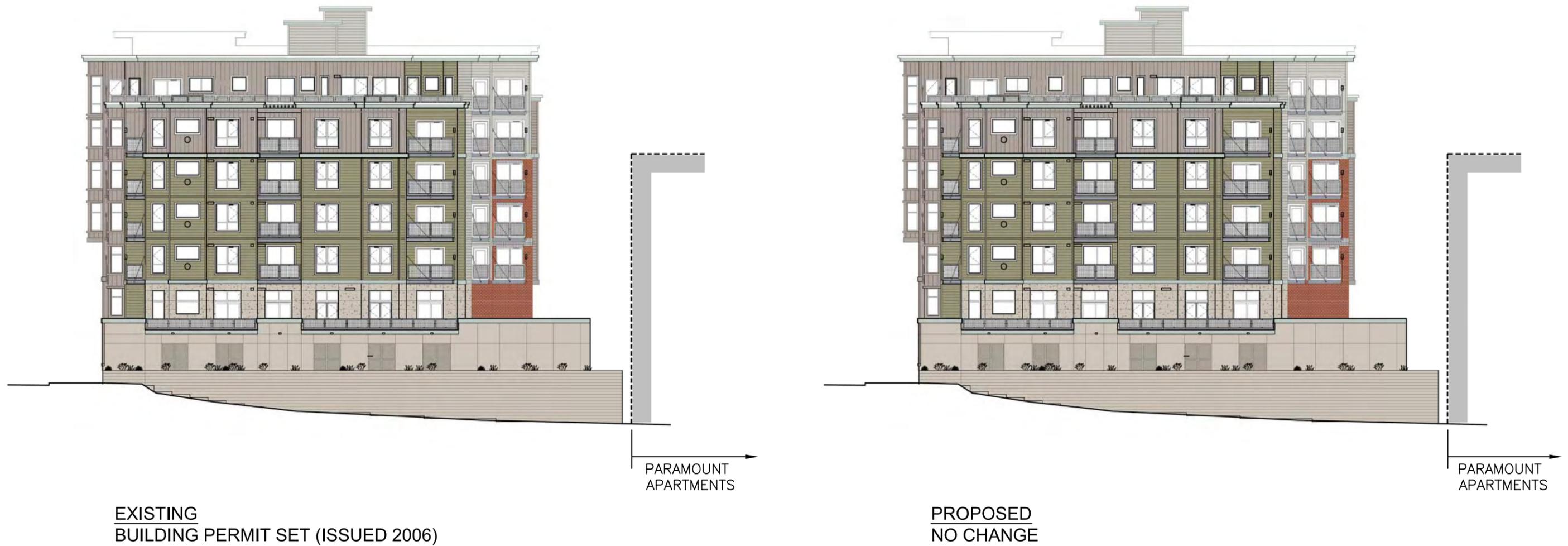
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WEST ELEVATIONS

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WEST ELEVATION

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WEST ELEVATIONS

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SOUTH ELEVATIONS

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PARAMOUNT APARTMENTS IN FOREGROUND

EXISTING
BUILDING PERMIT SET (ISSUED 2006)



PARAMOUNT APARTMENTS IN FOREGROUND

PROPOSED
NO CHANGE

SOUTH ELEVATION

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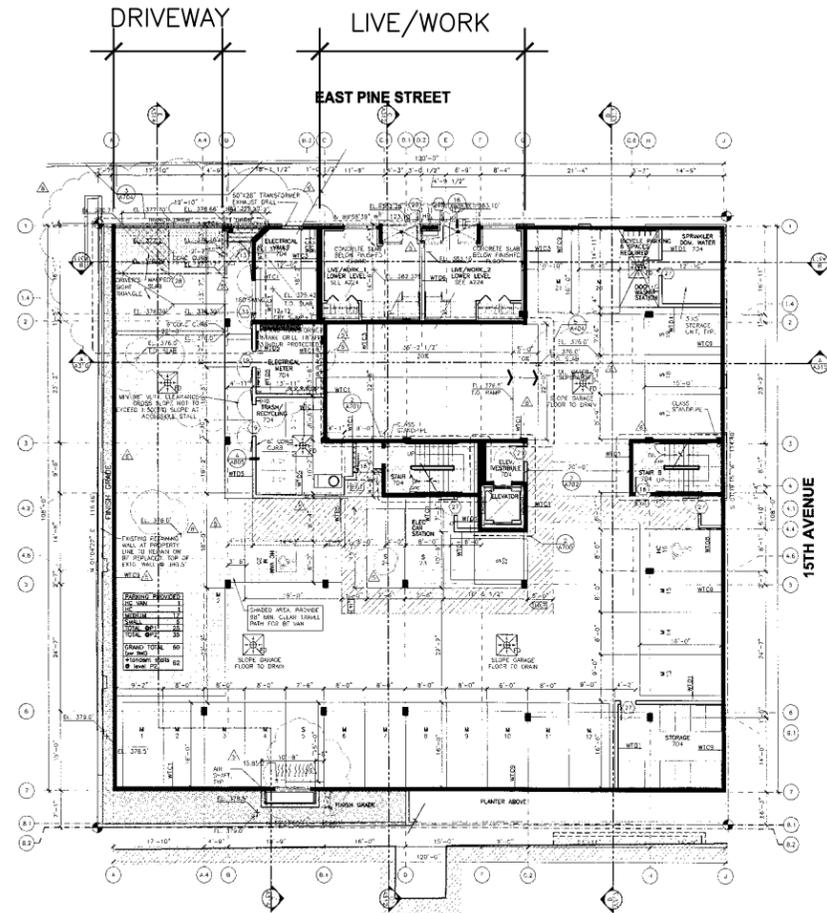
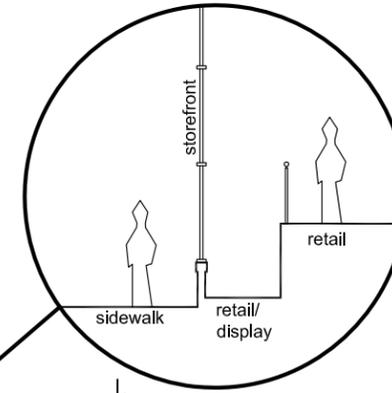
SOUTH ELEVATIONS

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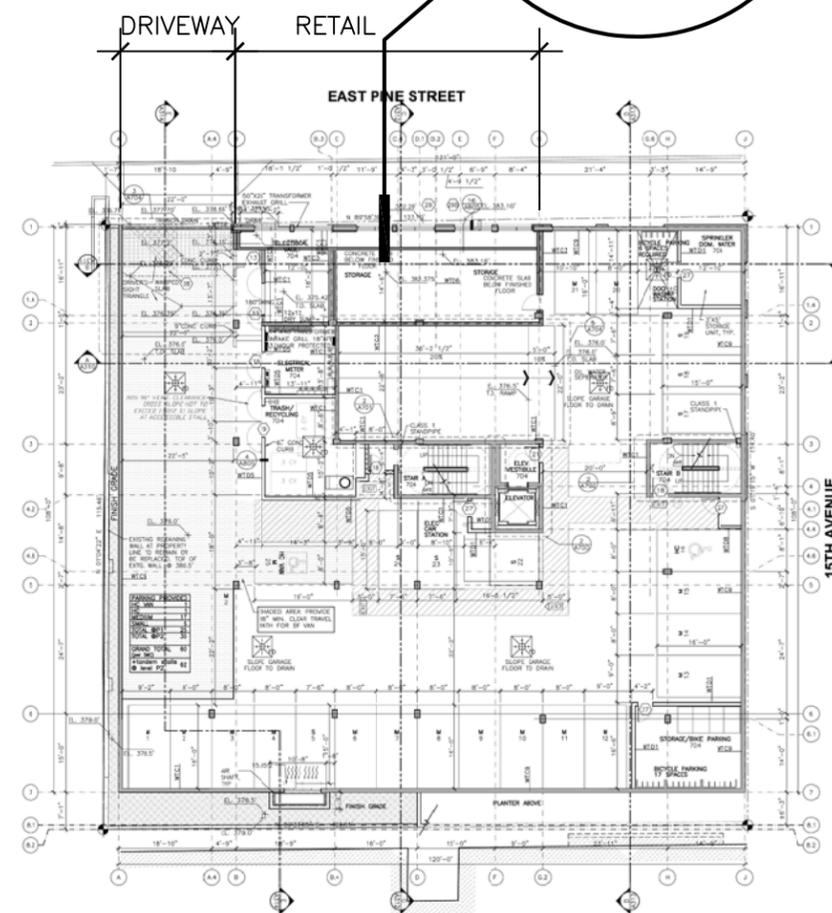
GRADE PLANE PLAN - PARKING LEVEL 1

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EXISTING
BUILDING PERMIT SET (ISSUED 2006)



PROPOSED

GRADE PLANE PLAN - PARKING LEVEL 1

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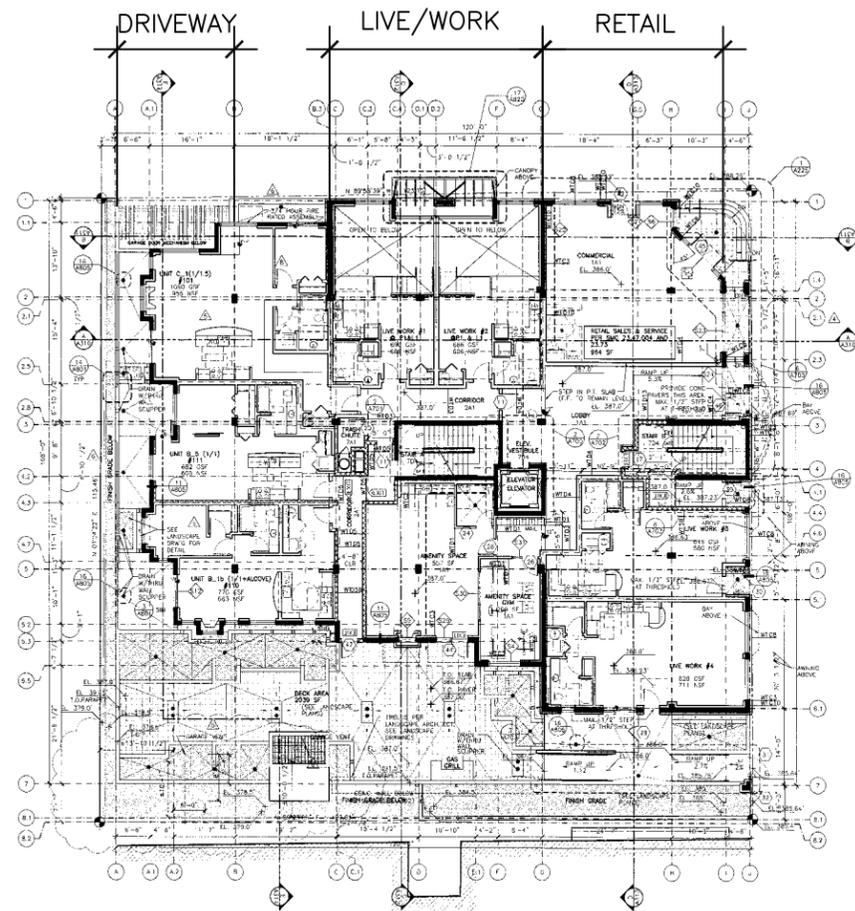
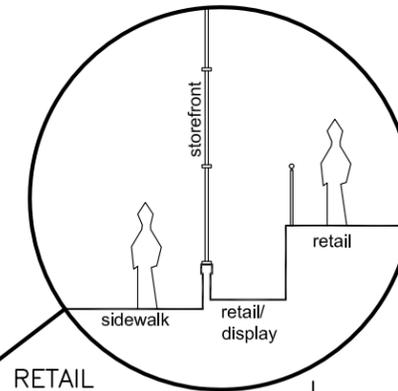
GRADE PLANE PLAN

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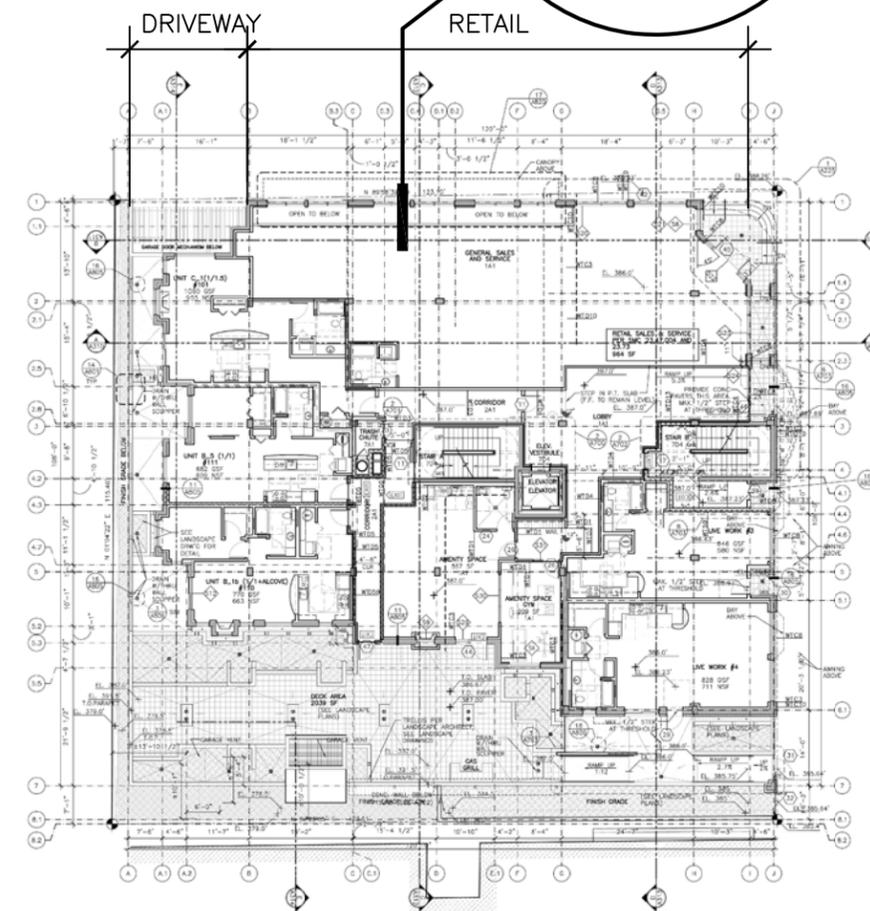
GRADE PLANE PLAN - LEVEL 1

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EXISTING
BUILDING PERMIT SET (ISSUED 2006)



PROPOSED

GRADE PLANE PLAN - LEVEL 1

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GRADE PLANE PLAN

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DEPARTURE #1

ALLOW A CURB CUT AND VEHICULAR ACCESS TO THE PARKING GARAGE TO REMAIN ON EAST PINE STREET AS CURRENTLY DESIGNED AND PERMITTED, RATHER THAN REQUIRING RELOCATION TO 15TH AVENUE PER SMC 23.47A.015

RATIONALE #1A LANDSCAPE COURT (PREVIOUSLY APPROVED)

Access from 15th Ave. would require a 100-foot-long speed ramp (see rationale #1B) from street level to the first parking level. This ramp would eliminate the majority of the at-grade landscape court amenity area, which is currently designed (and previously approved by the Board) for street-level access. A street ramp at this location would negatively impact the immediately adjacent residential windows of the existing Paramount Apartment next door.



GARAGE ENTRANCE ON 15TH AVE ELIMINATE AT-GRADE LANDSCAPE COURT



WITHOUT RAMP ON 15TH AVE



WITH RAMP ON 15TH AVE

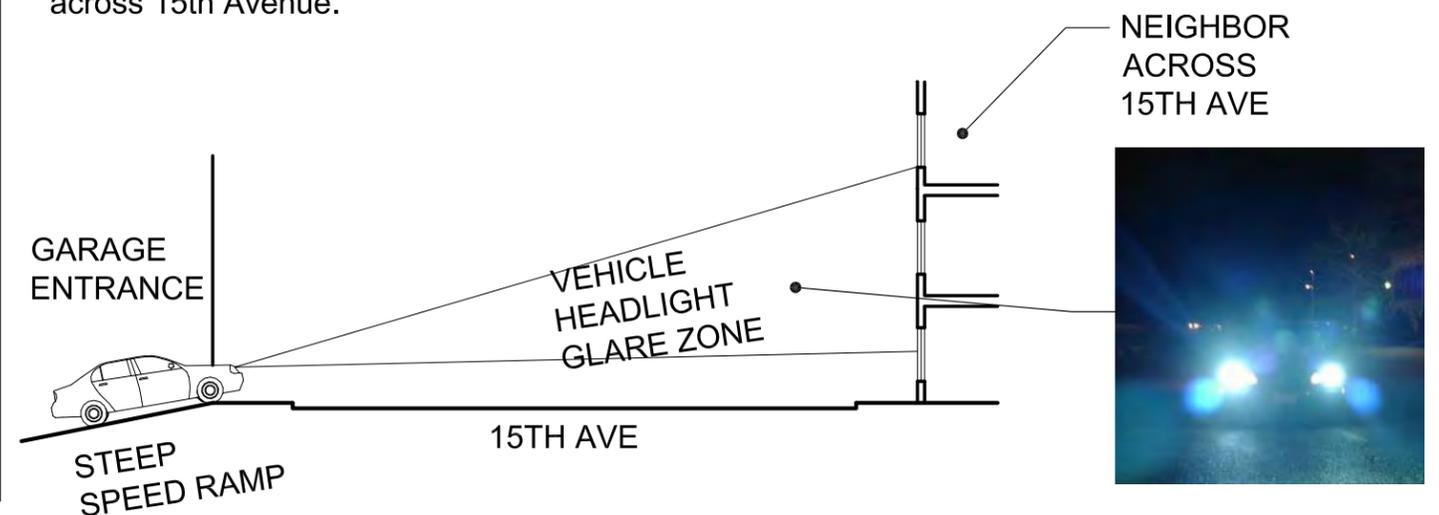
RATIONALE #1B SITE SLOPE

The site slopes significantly from east (high) to west (low). Access from 15th would require a significant reconfiguration of the design to incorporate a 100-foot-long speed ramp from street level to the first parking level.



RATIONALE #1C LIGHT POLLUTION

The steep ramp slope and location on 15th would also create a light-pollution and glare problem from car headlights shining into the upper-story windows of the newer apartment project located across 15th Avenue.



RATIONALE #2 METRO QUEUING AREA ALONG 15TH AVE.

Metro currently uses the curb parking strip along 15th Avenue directly adjacent to the site as a bus layover queuing area. Locating a curb cut on 15th would impact this function dramatically, displacing buses laying over between routes, and possibly requiring Metro approval.

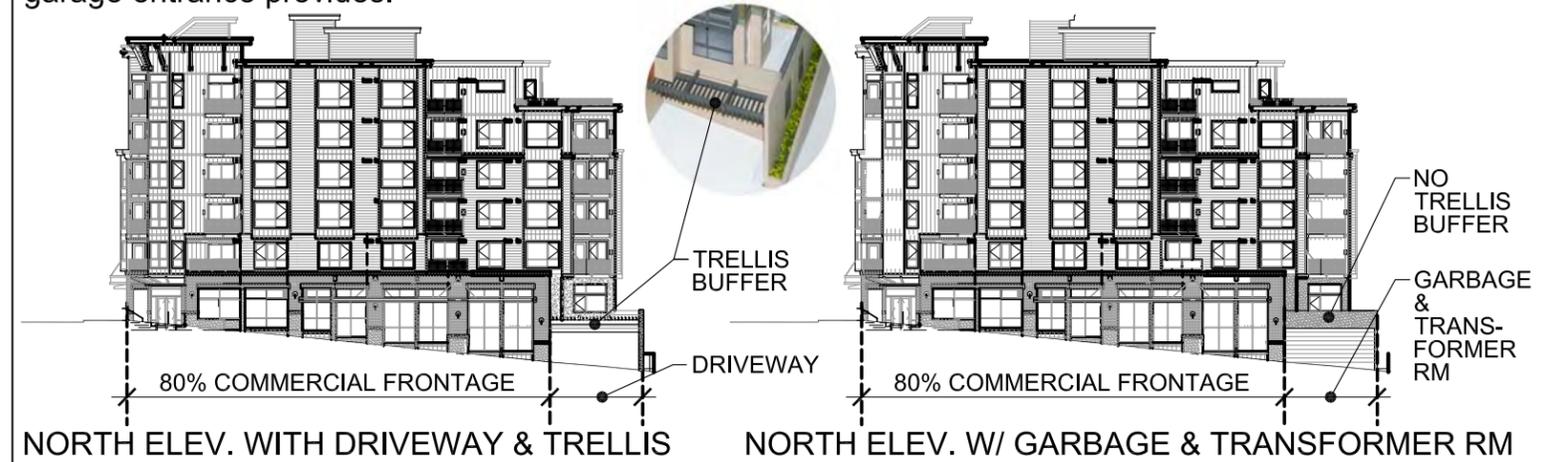


RATIONALE #3 PLAZA RETAIL

Even if the garage access was to be located on 15th, the garbage and electrical transformer rooms located at garage level would have to retain at-grade access for utility service on East Pine Street. There would be no additional commercial frontage possible at this location. These functions are accessed through the garage entrance in the current design, and screened by the garage entrance doorway and trellis. Moving the garage entrance to 15th would require relocation of the garbage and transformer rooms directly adjacent to the sidewalk along East Pine St, resulting in no improvement to the sidewalk environment and a loss of the pavement and trellis buffer the garage entrance provides.

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RATIONALE #4 POOR RETAIL ENVIRONMENT

The neighboring property to the west has a large surface lot located directly adjacent to this project and the sidewalk at E Pine St. This parking lot is not located at sidewalk level, but a few feet below (with retaining walls and fences at the boundary). The church located on this site is a designated historic landmark, so this arrangement is unlikely to change at any point in near-or medium-term future. The neighboring properties on 15th Avenue, on the other hand, are apartment buildings both adjacent and across the street, with significant resident foot traffic in and out of them on 15th. We feel that the impact on the pedestrian environment of having the main parking entrance for this project on East Pine St is less than on 15th Avenue.



PROPOSED SITE

ADJACENT CHURCH SURFACE PARKING LOT

CURBCUT W/ DRIVEWAY

CHURCH TRUCK LOADING ZONE & GARBAGE COLLECTION AREA

BLANK WALL

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DEPARTURE #1 & RATIONALES

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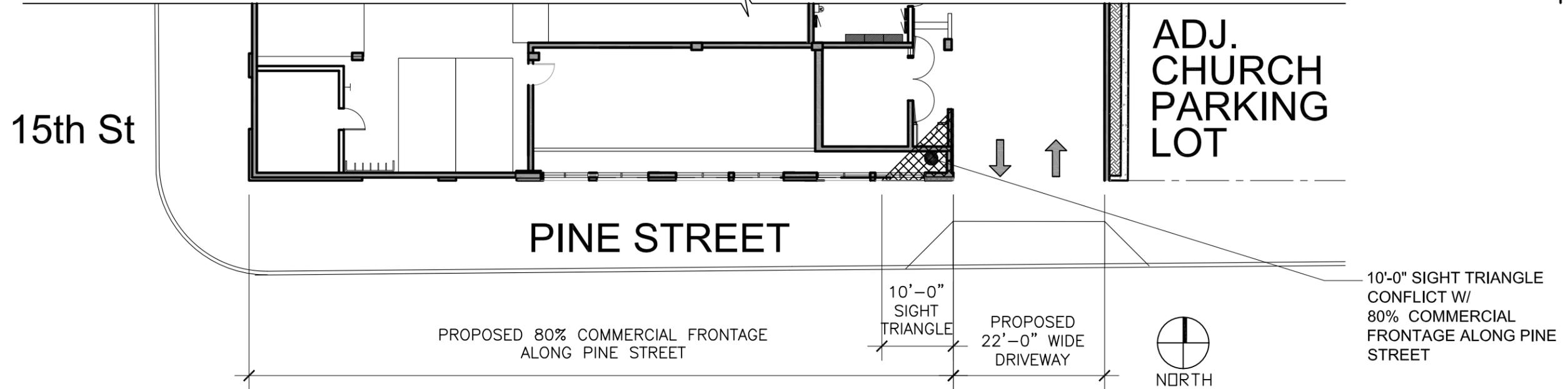
DEPARTURE #2

EITHER WAIVE THE REQUIREMENT FOR A DRIVEWAY SIGHT TRIANGLE UNDER SMC 23.54.030, OR, REDUCE THE COMMERCIAL FRONTAGE REQUIREMENT UNDER SMC 23.47A.005 BY TEN (10) FEET TO ALLOW A DRIVEWAY SIGHT TRIANGLE AT THE PROPOSED GARAGE ENTRANCE.

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PARTIAL FLOOR PLAN AT GRADE



PARTIAL NORTH ELEVATION AT GRADE



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DEPARTURE #2

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RATIONALE #1A SIGHT TRIANGLE RETAINED

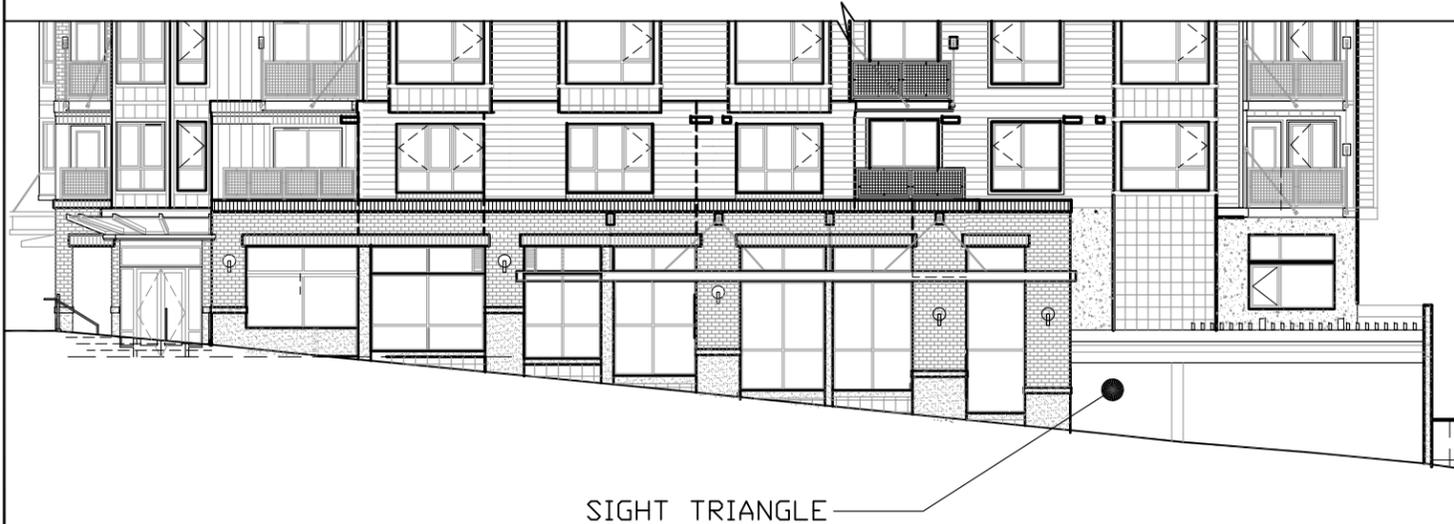
The site dimension along East Pine Street is short enough (123 feet), that there is not physically enough space to accommodate the required driveway width (22 feet), a ten-foot sight triangle, and enough length for 80% commercial frontage (98+ feet) as required. Either the site triangle or commercial frontage requirement has to give in order for all of this to fit. We leave it at the Board's discretion whether:

A) The sight triangle retained with a reduction in commercial frontage, or

ELEVATION W/ 80% COMMERCIAL FRONTAGE



ELEVATION W/ SIGHT TRIANGLE



RATIONALE #1B SIGHT TRIANGLE WAIVED

B) The sight triangle waived (as is so often the case on tight urban sites) with safety measures (mirrors, flashers, etc.) implemented instead.

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CONVEX MIRROR



FLASHERS &
AUDIBLE ALARM

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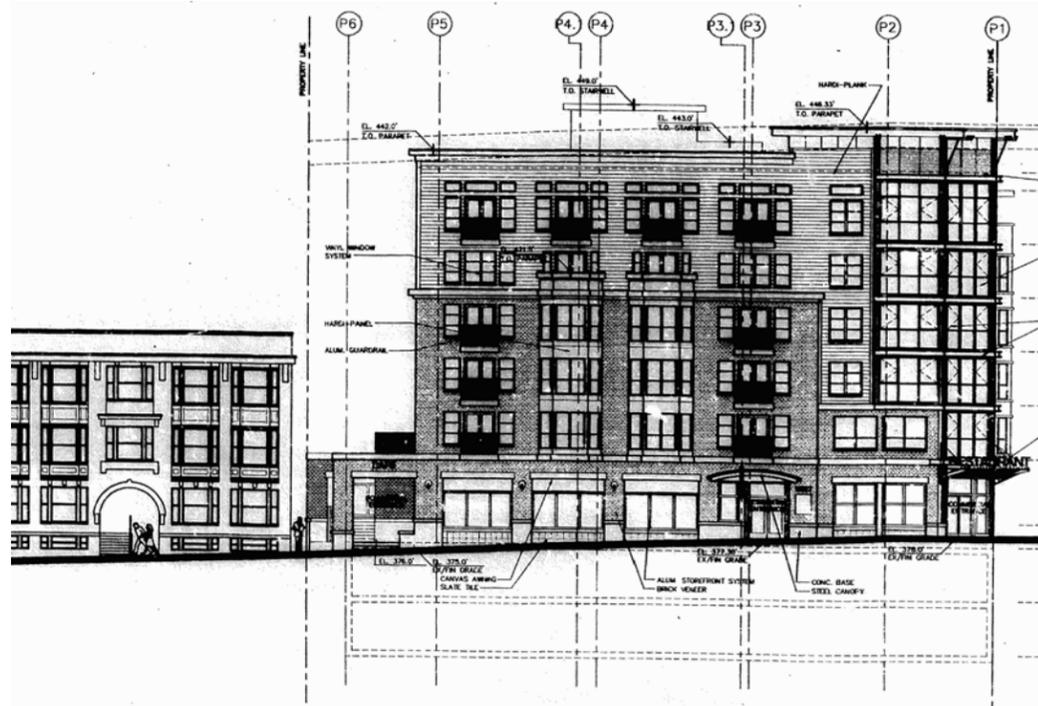
DEPARTURE #2 RATIONALES

A-10

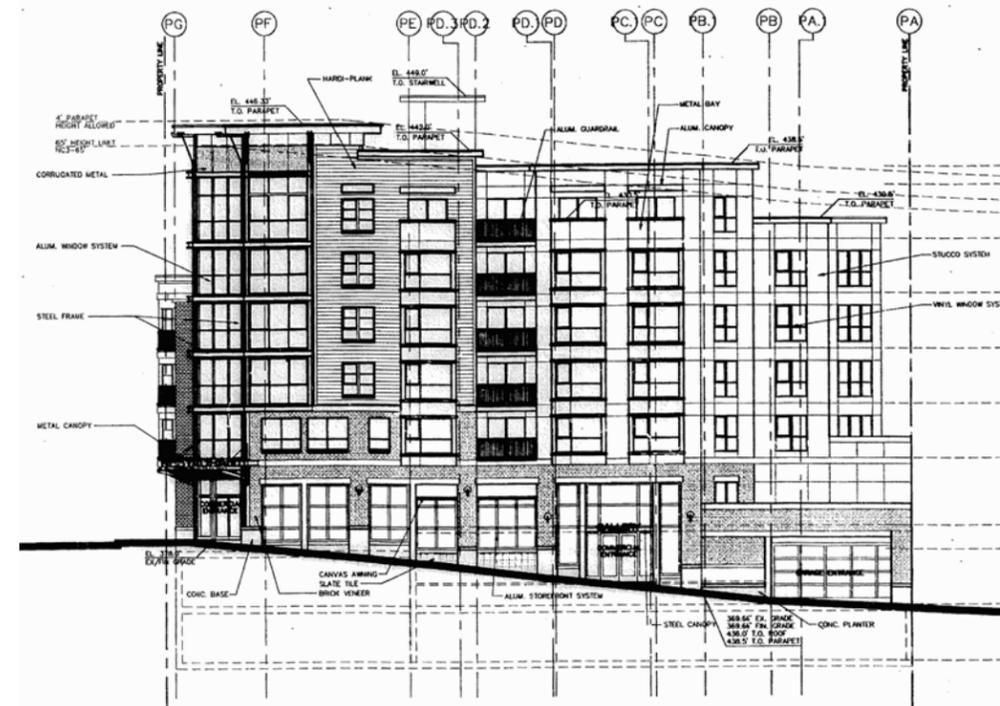
REFERENCE MUP ELEVATIONS

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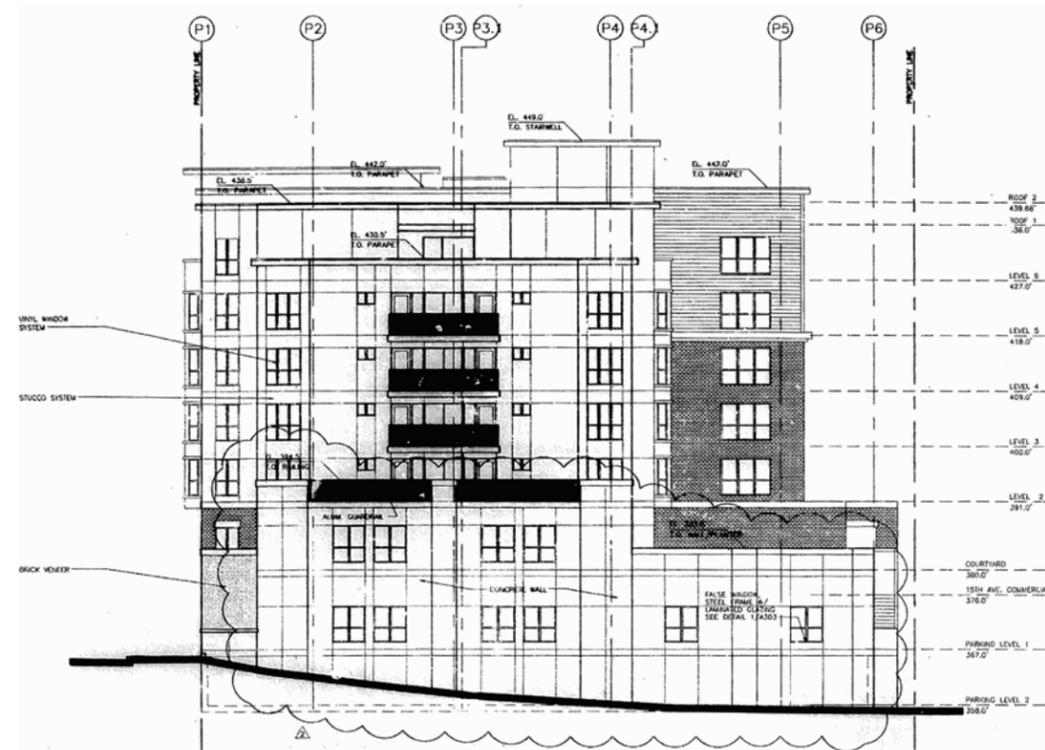
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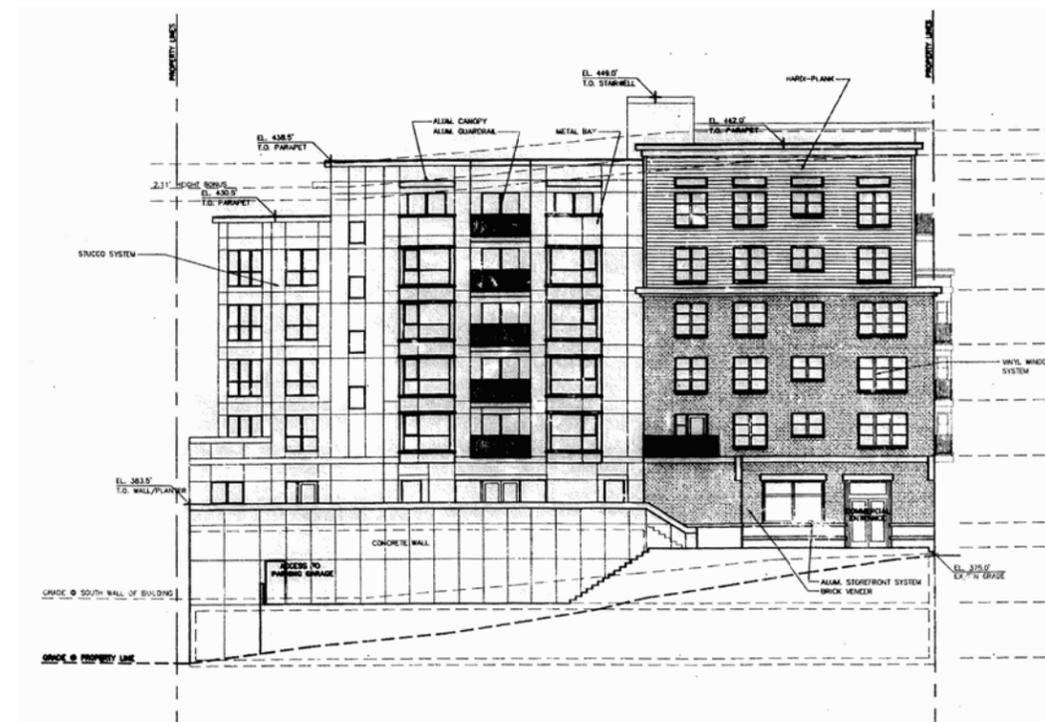
EAST ELEVATION



NORTH ELEVATION



WEST ELEVATION



SOUTH ELEVATION

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KEY COMPONENTS

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