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Presentation to the City of Seattle
Design Review Board Recommendation Meeting
DPD #3011428

888 WESTERN AVENUE
COLMAN TOWER

November 23, 2010

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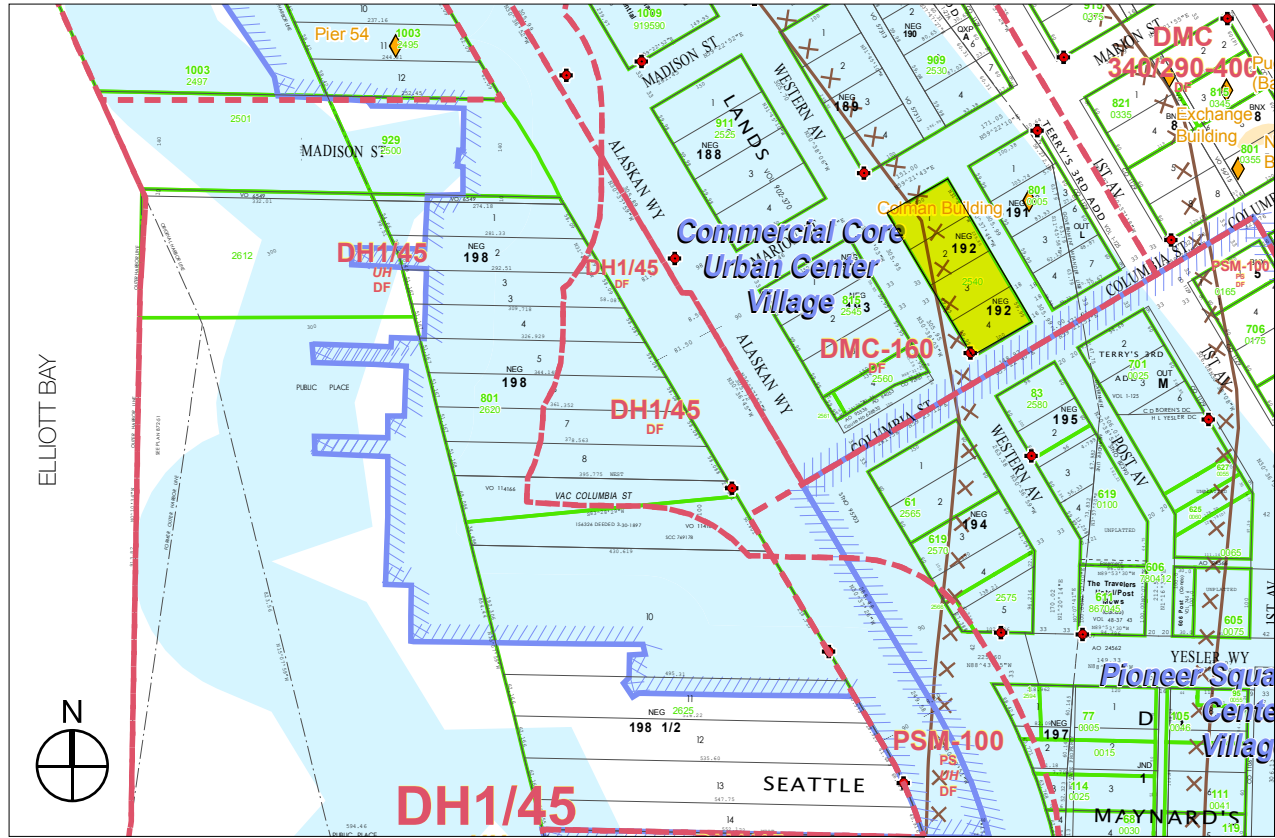
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Zoning & Overlays: Downtown Mixed Commercial DMC 160 Zone.

- The site is within the Commercial Core Urban Center Village.

Allowable Height Limit: 23.49.008.

- The base and maximum height limit is 160'.

Floor Area Ratio (F.A.R.): N/A in Residential Uses

Common Recreation Area: 23.49.10.B

- Common Recreation Area is required in an area equivalent to five (5) percent of the total gross floor area in residential use. The common recreation area shall be available to all residents and may be provided at or above ground level.
- A maximum of fifty (50) percent of the common recreation area may be enclosed.
- Common recreation area that is provided as open space at street level shall be counted as twice the actual area in determining the amount provided to meet the common recreation area requirement.
- For lots abutting designated green streets, up to fifty (50) percent of the common recreation area requirement may be met by contributing to the development of a green street.

Continuous street-front overhead weather protection:

- Overhead weather protection is required unless the building is set back more than 5', abuts an open space, has a 2' landscaped area, or is at driveways. It must be <8' from the wall or 2' from the curb, and 10' to 15' above the sidewalk.

Parking Requirements: 23.49.019

- No parking is required for residential projects in downtown zones.

STREET TYPES

Sidewalk & Alley Widths: 23.49.022 MAP 1B, 1F

- Western: Class II Pedestrian Street, Minor Arterial
Map 1C requires a 12' wide sidewalk, so a 2' dedication into the property is being provided to satisfy this requirement (up to height of 16').
- Marion: Green Street
Map 1C requires a "variable" sidewalk width, so the required sidewalk width should be maintained.
- Post Avenue: Map 1C requires a 12' sidewalk width, however, SDOT will not allow intrusion into Row.
- Columbia Street: Class II Pedestrian Street, Principal Arterial
Map 1C requires a 12' sidewalk width

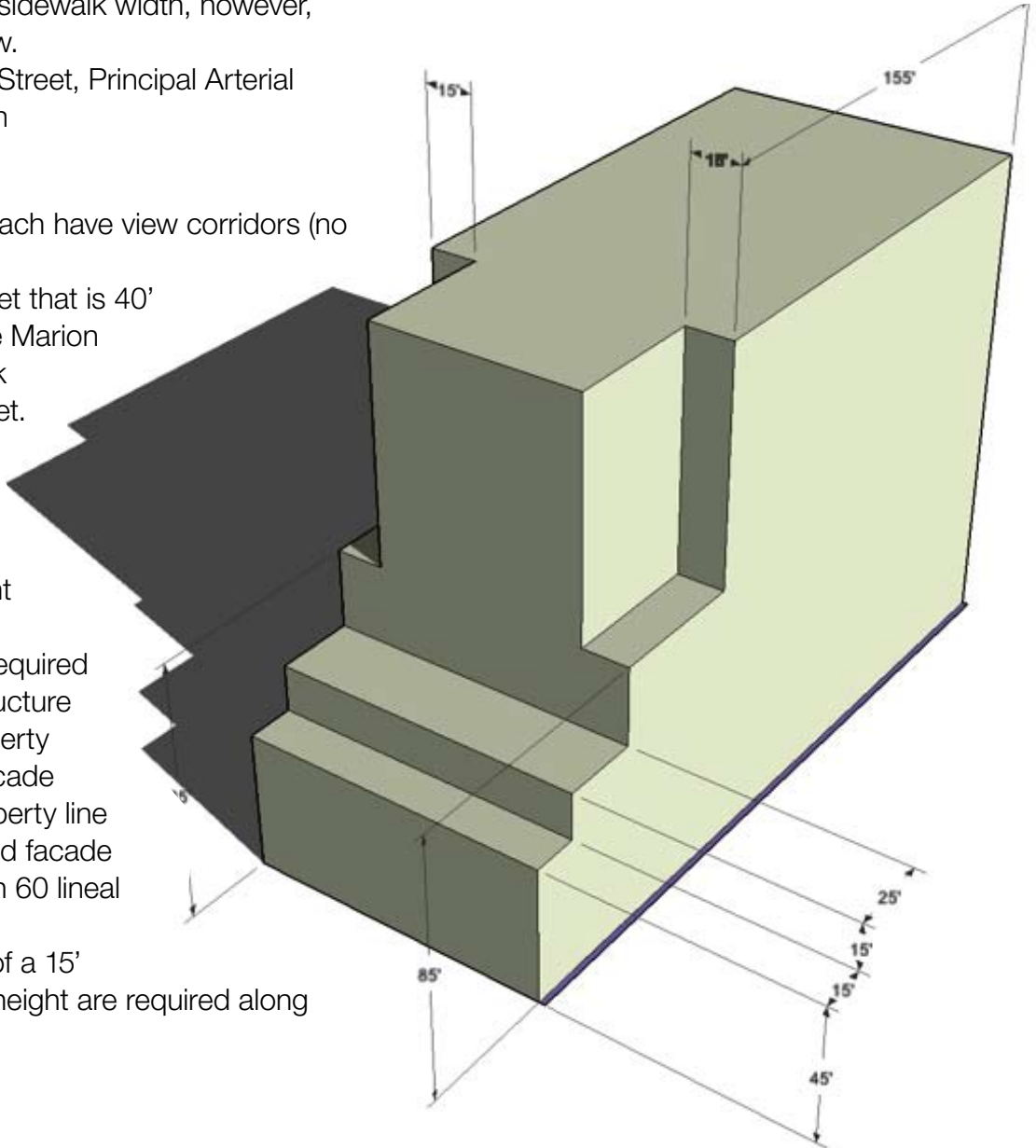
View Corridors: 23.49.024

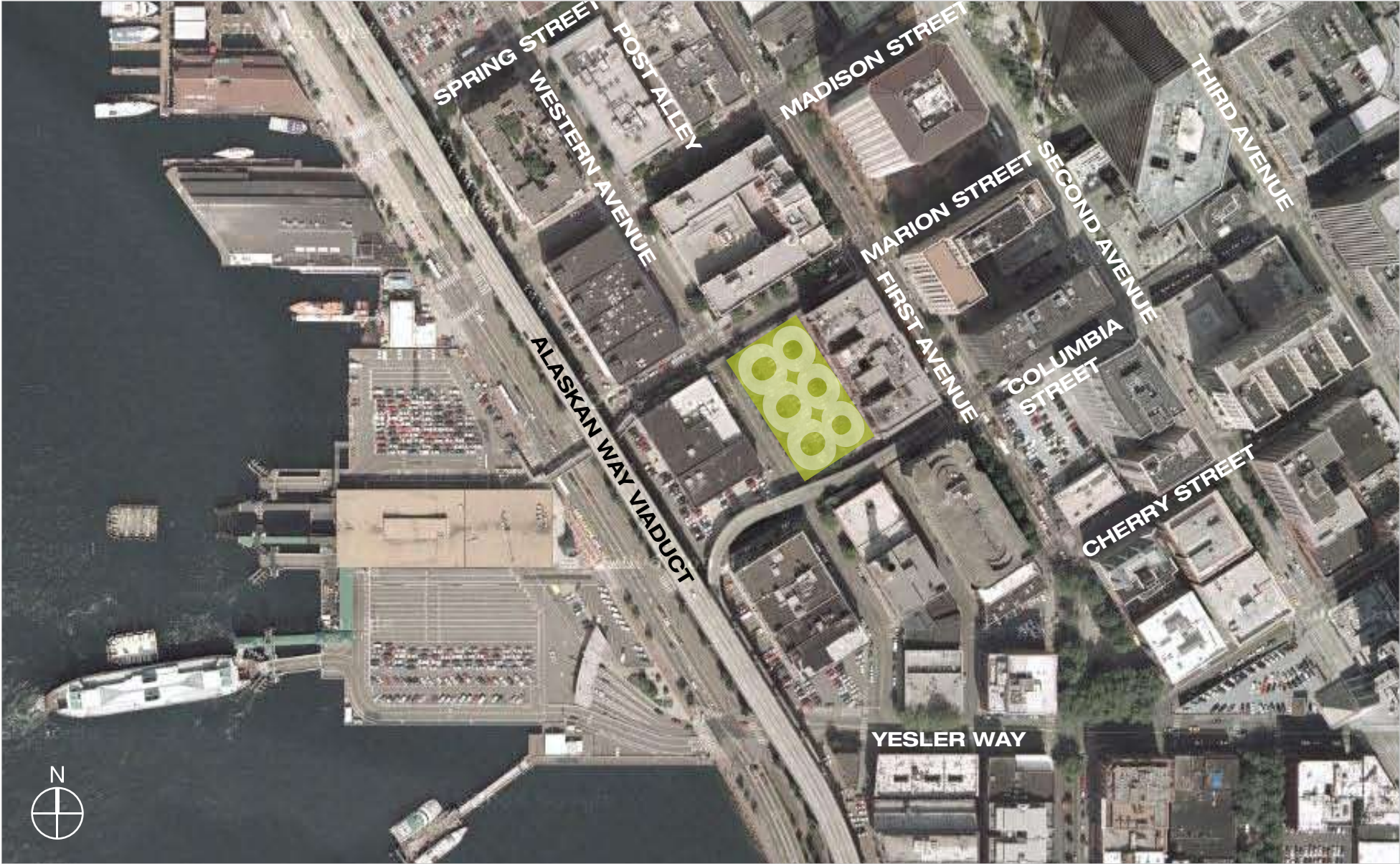
- Marion Street and Columbia Street each have view corridors (no S.B. required on Columbia).
- A setback is required on Marion Street that is 40' wide above a height of 60' above the Marion Street sidewalk elevation. No setback requirement exists on Columbia Street.

Upper Level Development

standards: 23.49.058

- 100% coverage is allowed on the developable lot area up to 160' height except for:
 - >> Part B — Facade Modulation is required above 85' for any portion of a structure located within 15' of a street property line. The maximum length of a facade that is within 15' of the street property line is 155 linear feet. The 15' recessed facade length is required to be more than 60 lineal feet.
 - >> Part F — Upper Level Setbacks of a 15' continuous setback above a 45' height are required along green streets (Marion).





SITE DEVELOPMENT AREA:

- This full block is platted at 240' x 100'

GENERAL:

- Average area per floor 17,267 sf
- Total building area 276,277 sf

RESIDENTIAL UNITS FOR THE YOUNG, URBAN PROFESSIONAL:

- 208 apartment units
- Average unit size (including micro units) is 704 sf
- 183,046 sf of total residential
- 16 stories

Unit Type	Studio	Open 1	1 Bed	2 Bed
# of units	18	130	20	40
% of unit type	9%	62%	10%	19%

RETAIL USES:

- 8,017 sf on the first floor
- 1,401 sf on the second floor (*relating to pedestrian bridge*)
- 9,418 sf total of retail use

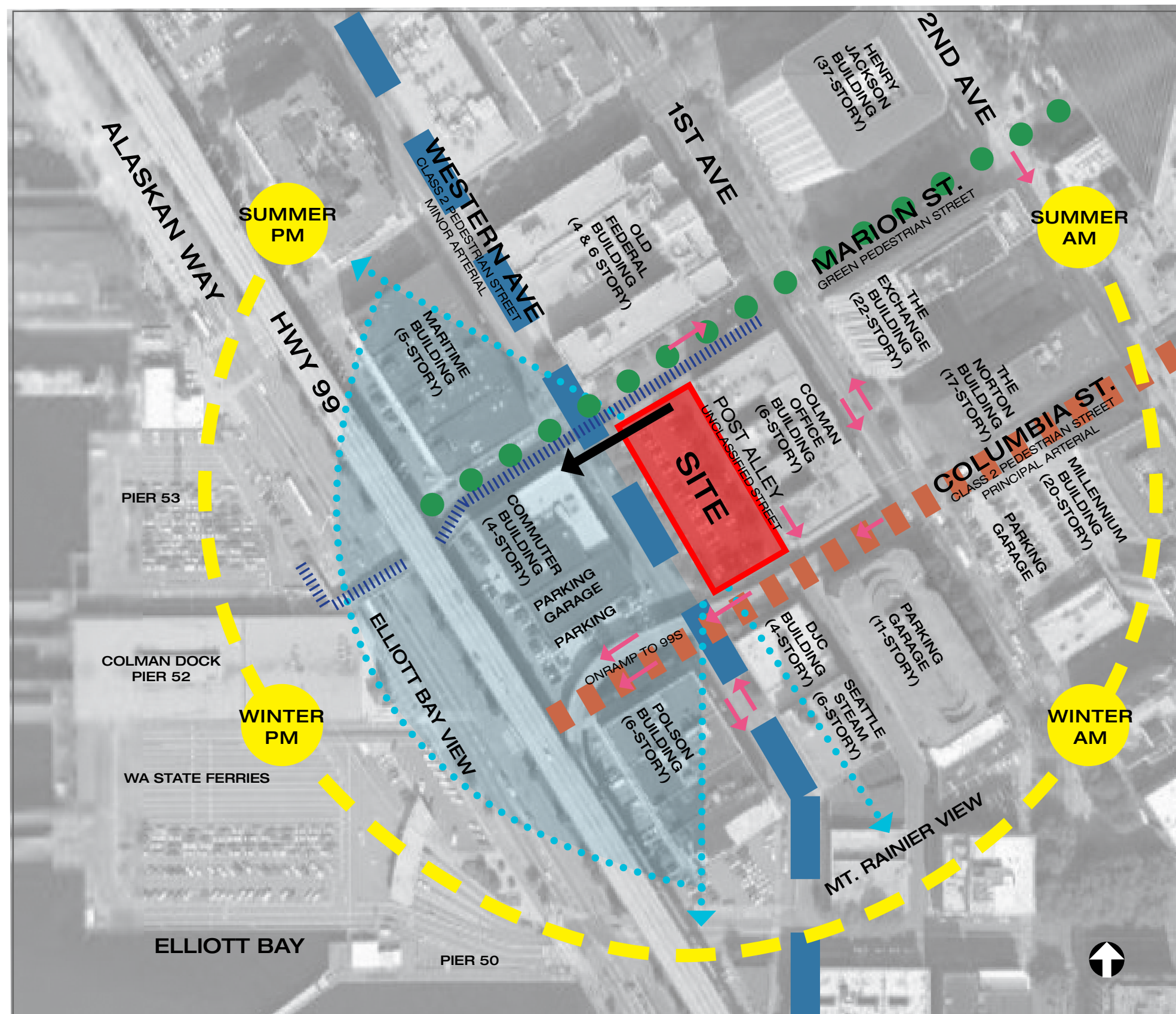
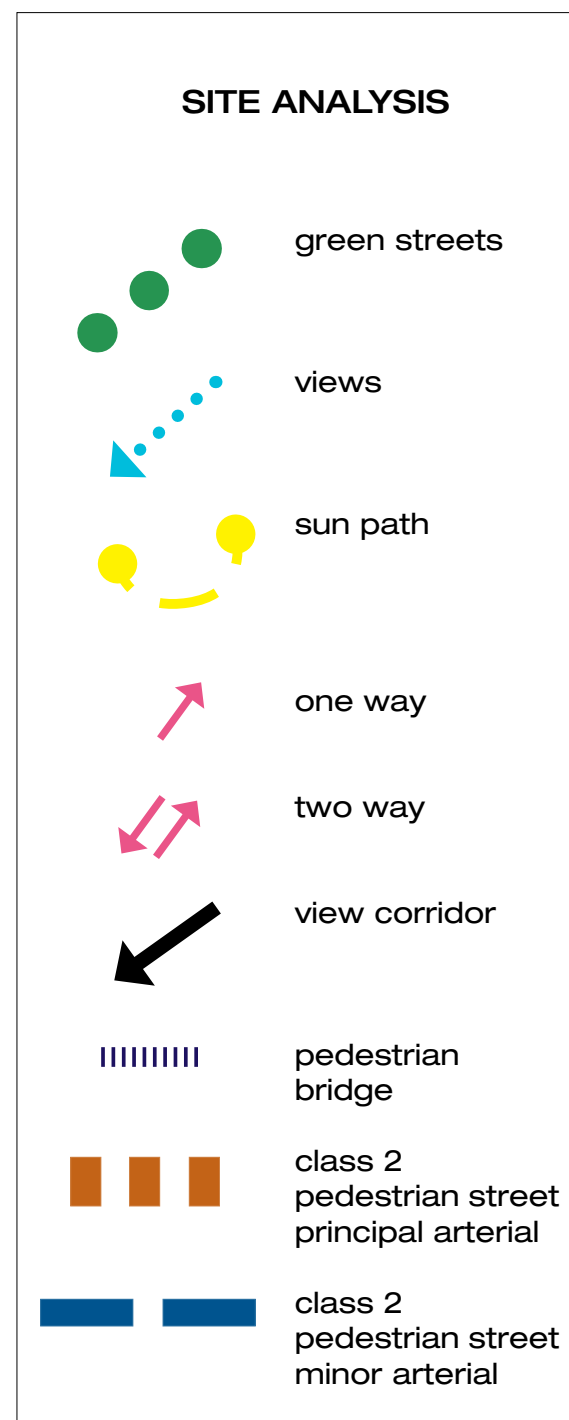
PARKING:

- Approx. 124 parking stalls
- 0.6 stalls per unit

DEPARTURE REQUESTS:

- Façade Modulation
- Parking Separation
- Green Street Setback
- Post Ave. Sidewalk Width
- Overhead Weather Protection
- Blank Façade
- Façade Setback







MARITIME BUILDING



FEDERAL BUILDING



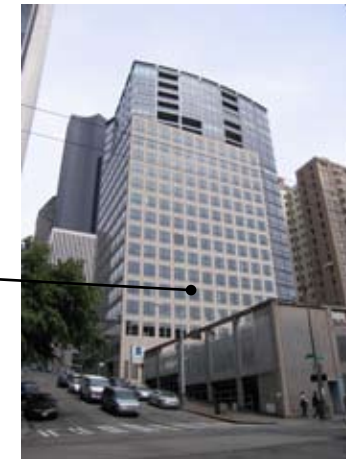
COLMAN BUILDING



NORTON BUILDING



COMMUTER BUILDING



MILLENNIUM TOWER



GARAGE



HIGHWAY 99 VIADUCT



COLMAN DOCK



PARKING GARAGE



POLSON BUILDING

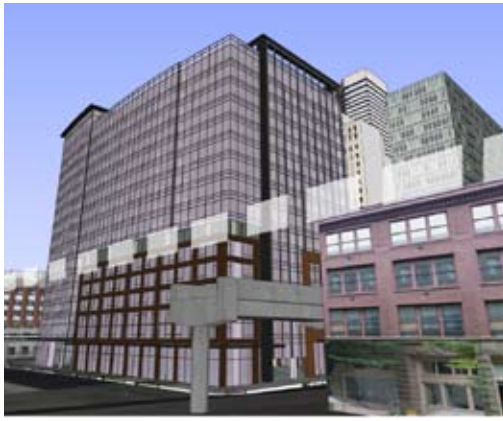


DJC BUILDING





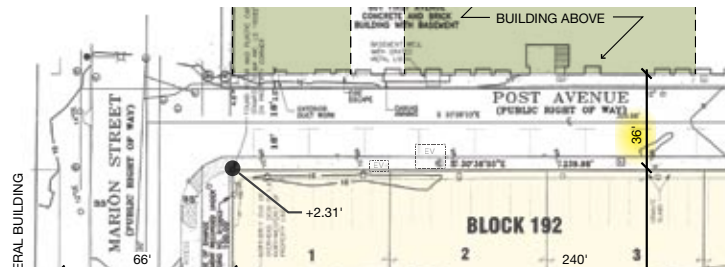
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A. Site Planning and Massing

A-1 Respond to the Physical Environment. *Develop an architectural concept and compose the building's massing in response to geographic conditions and patterns of urban form found beyond the immediate context of the building site.*

At the Early Design Guidance Meeting, the Board discussed that the preferred option (Option 3) better fits into the neighborhood context and best holds the corners of the block and street edges. The Board acknowledged the unusual condition of the pedestrian bridge and the challenge of integrating it into the site and building design. The Board also supported establishing datum lines on the façade design that respond to the nearby buildings and using these lines to delineate the building base.

B. Architectural Expression: Relating to the Neighborhood Context

B-1 Respond to the Neighborhood Context – *Develop an architectural concept and compose the major building elements to reinforce desirable urban features existing in the surrounding neighborhood.*

At the Early Design Guidance Meeting, the Board noted that there isn't much precedent for the proposed three-story "cube" design that would connect the pedestrian bridge and the main building. The Board expressed support for the setback shown at the north side of the block to provide a plaza space and landscaping.

B-3 Reinforce the Positive Urban Form & Architectural Attributes of the Immediate Area. *Consider the predominant attributes of the immediate neighborhood and reinforce desirable siting patterns, massing arrangements, and streetscape characteristics of nearby development.*

At the Early Design Guidance Meeting, the Board discussed the importance of Post Avenue and how the treatment of this façade and its street level design is critical to create a more vibrant street.

The width of the existing sidewalk along Post Avenue is six feet, while the requirement is 12 feet. The Board would like to see landscaping, street trees, minimum street width and strong retail bay frontage to encourage activation. The Board is supportive of the request to SDOT to modify the street width reduction and integrate adequate sidewalk width, landscaping and street trees and a loading area. The Board agreed that having a wider sidewalk is more critical than having a wider roadway.

B-4 Design a Well-Proportioned & Unified Building. *Compose the massing and organize the publicly accessible interior and exterior spaces to create a well-proportioned building that exhibits a coherent architectural concept. Design the architectural elements and finish details to create a unified building, so that all components appear integral to the whole.*

At the Early Design Guidance Meeting, the Board encouraged a simplified architectural concept and form that is unified and not overly busy and instead responds to the more simple building massing of the historic buildings in the immediate context.



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COLMAN TOWER | EDG DIRECTION / DESIGN GUIDELINE PRIORITIES

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C. The Streetscape: Creating the Pedestrian Environment

C-1 Promote Pedestrian Interaction. Spaces for street level uses should be designed to engage pedestrians with the activities occurring within them. Sidewalk-related spaces should be open to the general public and appear safe and welcoming.

At the Early Design Guidance Meeting, the Board discussed Post Avenue and how to best enliven and take advantage of this unusually narrow and historic street to become a more animated and usable pedestrian and retail street. See also B-3. The Board noted a preference for generous landscaping rather than overhead weather protection along Post Avenue. They indicated potential support for a departure from overhead weather protection if it would be in direct conflict with the provision of street trees and generous landscaping accommodated for on Post Avenue. See also the discussion of the ‘cube’ feature under C-2.

C-2 Design Facades of Many Scales. Design architectural features, fenestration patterns, and materials compositions that refer to the scale of human activities contained within. Building facades should be composed of elements scaled to promote pedestrian comfort, safety, and orientation.

At the Early Design Guidance Meeting, the Board expressed support for the proposed notch on the east side of the building which alleviates the possible canyon created by two tall building on either side of the narrow Post Avenue.

The Board found the proposed curved west façade problematic due to its lack of architectural precedent in the area for such a gesture. Instead, the façade should be flat and strive for simplicity. The Board suggested that the modern portion of the building (above the datum line) should recede behind the historic building base (which references the historic context).

The Board requested that the proposed “cube” feature be as transparent as possible and eliminate the proposed cube design concept “B” with the angled facade. The cube design should be simple and avoid adding new angles and forms that are incongruent with the neighborhood context. Of paramount concern should be the connection between the pedestrian bridge, the subject building and the movement from the bridge to ground level – this circulation function should be clearly expressed in the cube design. The Board recognized the challenge of locating retail use at the second level. The Board encouraged the provision of seating and open space at the second level abutting the pedestrian bridge. The Board also agreed that the design of this connection should consider the light and shadow of the area below the bridge and endeavor to use translucent materials where possible.

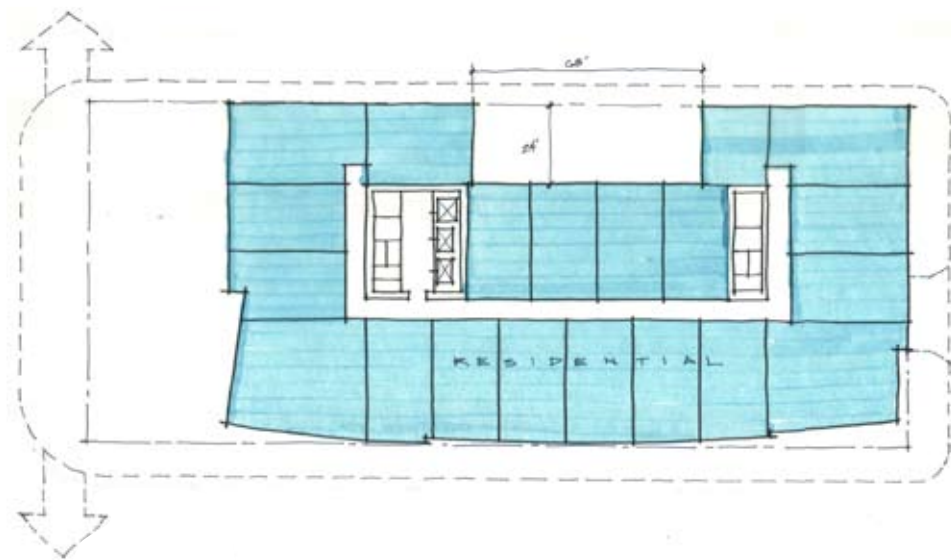
The datum lines of the nearby historic buildings should be acknowledged. The Board cautioned against too much curtain wall and glass as being out of context. Punched windows should be considered in the design of the building base.

C-3 Provide Active—Not Blank—Facades. Buildings should not have large blank walls facing the street, especially near sidewalks.

At the Early Design Guidance Meeting, the Board noted that on Western Avenue the residential units at the parking levels could be shifted to the curtain wall area so that the parking levels could be behind the punched masonry grid block area. It seems like screening the parking behind the grid is more effective than screening behind the spandrel glass. See also E-2.



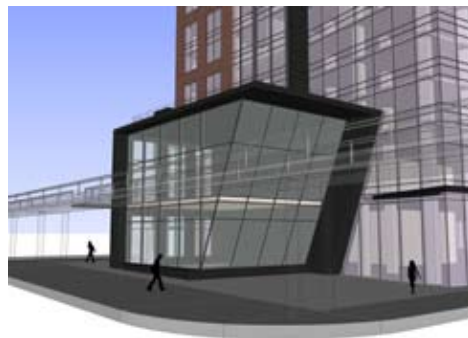
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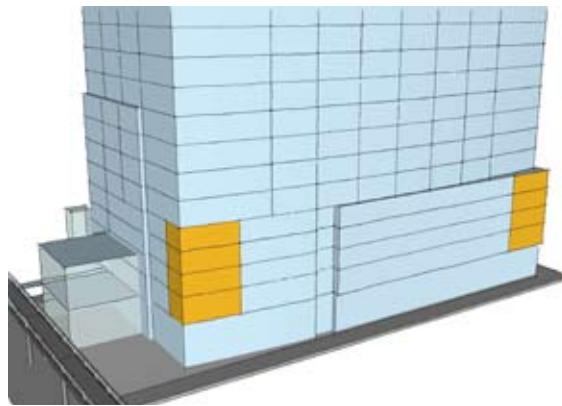
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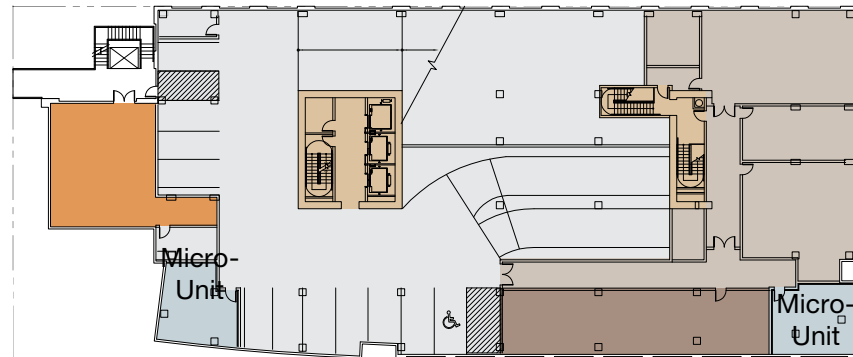
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D. Public Amenities: Enhancing the Streetscape & Open Space

D-2 Enhance the Building with Landscaping. *Enhance the building and site with substantial landscaping—which includes special pavements, trellises, screen walls, planters, and site furniture, as well as living plant material.*

At the Early Design Guidance Meeting, the Board focused on the Post Avenue streetscape and the Marion Street open space/plaza area. Both are prime opportunities for the integration of interesting landscaping and hardscaping to create visual interest at the pedestrian level but also as viewed from the pedestrian bridge. See also C-1.

D-5 Provide Adequate Lighting. *To promote a sense of security for people downtown during nighttime hours, provide appropriate levels of lighting on the building facade, on the underside of overhead weather protection, on and around street furniture, in merchandising display windows, and on signage.*

At the Early Design Guidance Meeting, the Board members noted they would like to see more information on the proposed lighting plan, particularly as it relates to the pedestrian bridge level and on the sidewalk/plaza area below the bridge.

D-6 Design for Personal Safety & Security. *Design the building and site to enhance the real and perceived feeling of personal safety and security in the immediate area.*

At the Early Design Guidance Meeting, the Board emphasized the importance of creating a safe and visually accessible area below the pedestrian bridge.

E. Vehicular Access & Parking: Minimizing the Adverse Impacts

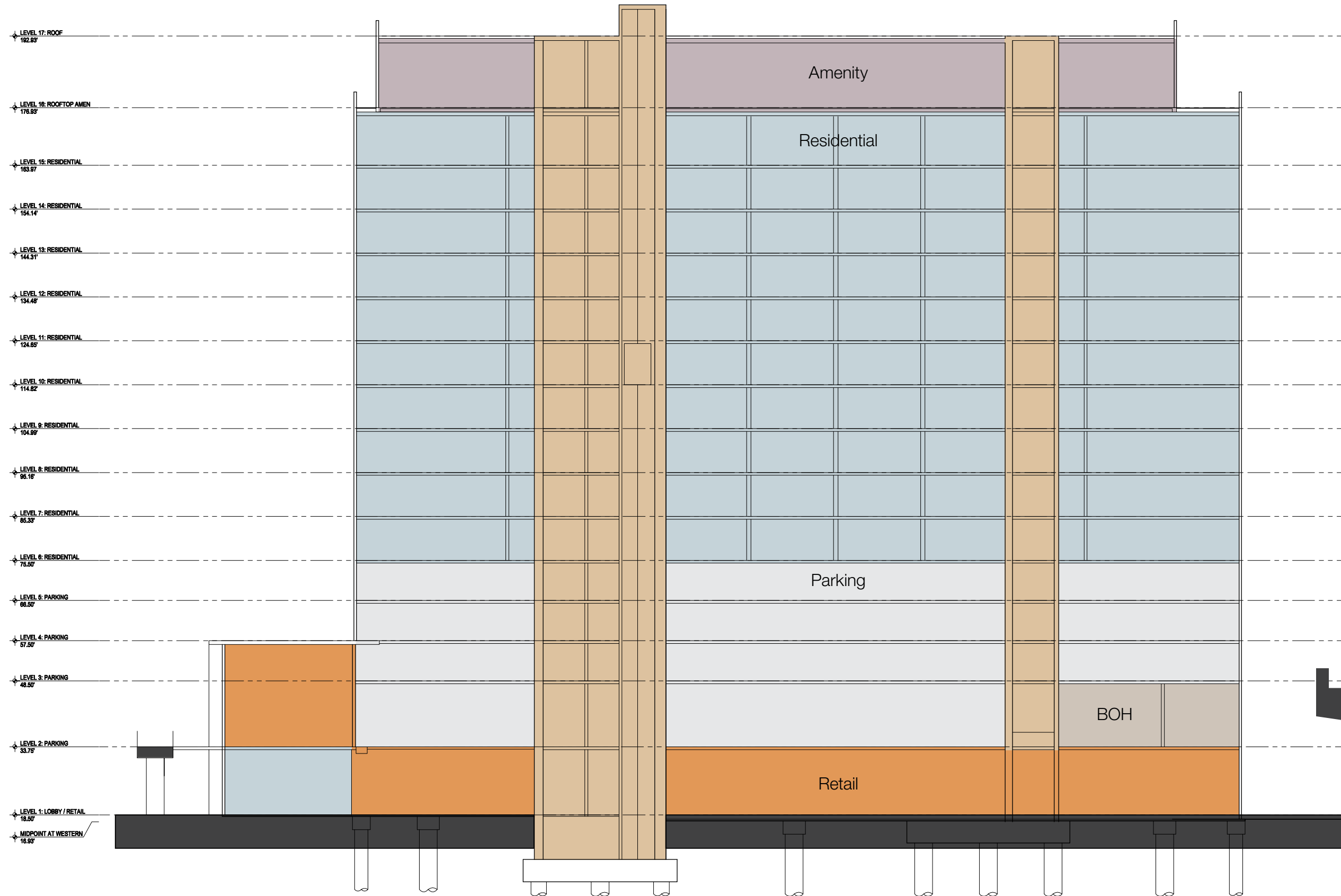
E-1 Minimize Curb Cut Impacts. *Minimize adverse impacts of curb cuts for the safety and comfort of pedestrians.*

At the Early Design Guidance Meeting, the Board agreed that incorporating vehicular access from Columbia Street was appropriate. The Board would like to better understand how and where resident loading will occur and whether the proposed loading zone on Post Avenue is viable.

E-2 Integrate Parking Facilities. *Minimize the visual impact of parking by integrating parking facilities with surrounding development. Incorporate architectural treatments or suitable landscaping to provide for the safety and comfort of people using the facility as well as those walking by.*

At the Early Design Guidance Meeting, the Board was supportive of the proposal to include studio units at the corners of the parking levels thereby enlivening the facades at the corner locations with transparency and visual access to the lighting in those units beyond what the parking screening includes. Ultimately, per C-3, the Board supported locating all of the garage-level units at the curtain-wall at the northwest corner of the building, fronting Western Avenue and wrapping around to the North Plaza. The Board looks forward to learning more about how the cars are screened. The spandrel glass may be insufficient in terms of creating an overly blank wall.





KEY

- Residential
- Retail
- Amenity
- Circulation
- Storage
- BOH
- Parking
- Mechanical



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COLMAN TOWER | BUILDING SECTION – LOOKING EAST

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EXISTING COLMAN BUILDING

KEY

- Residential
- Retail
- Amenity
- Vertical Circulation
- Storage
- BOH
- Parking
- Mechanical



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COLMAN TOWER | GROUND FLOOR PLAN

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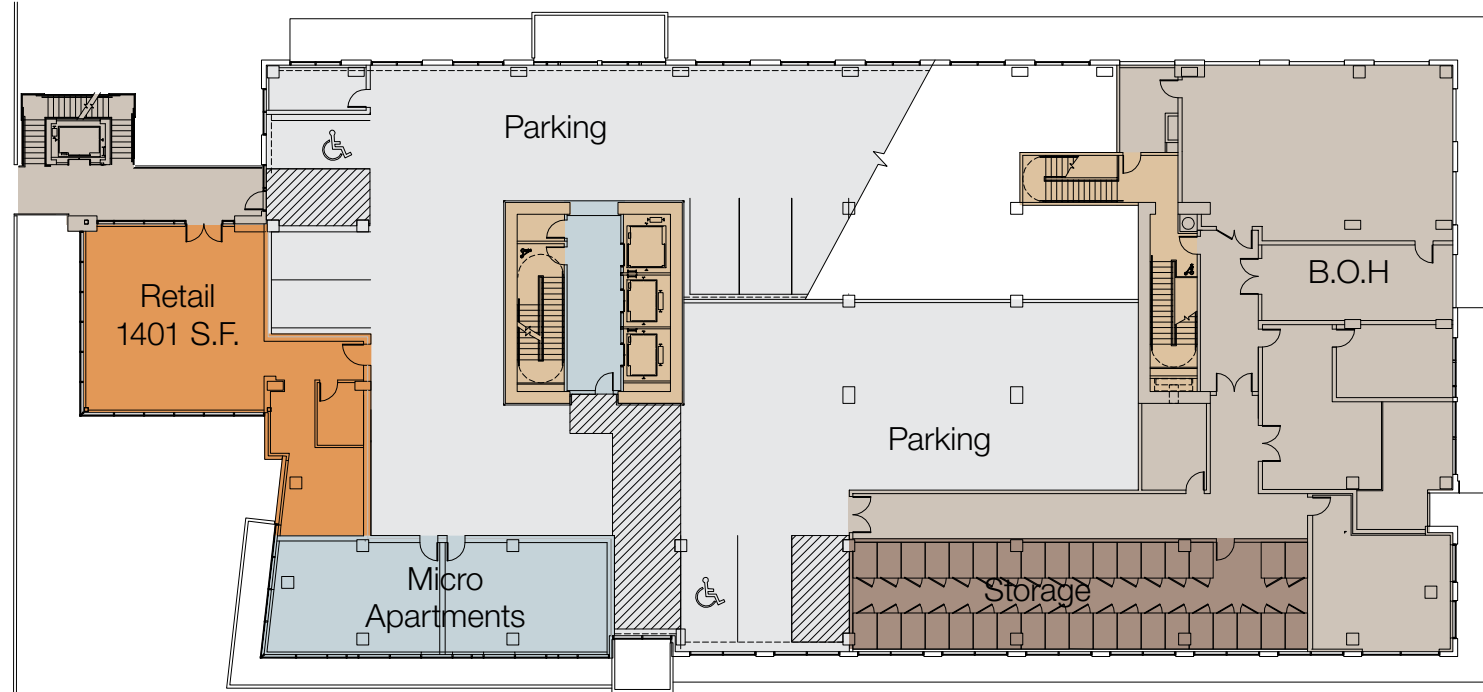
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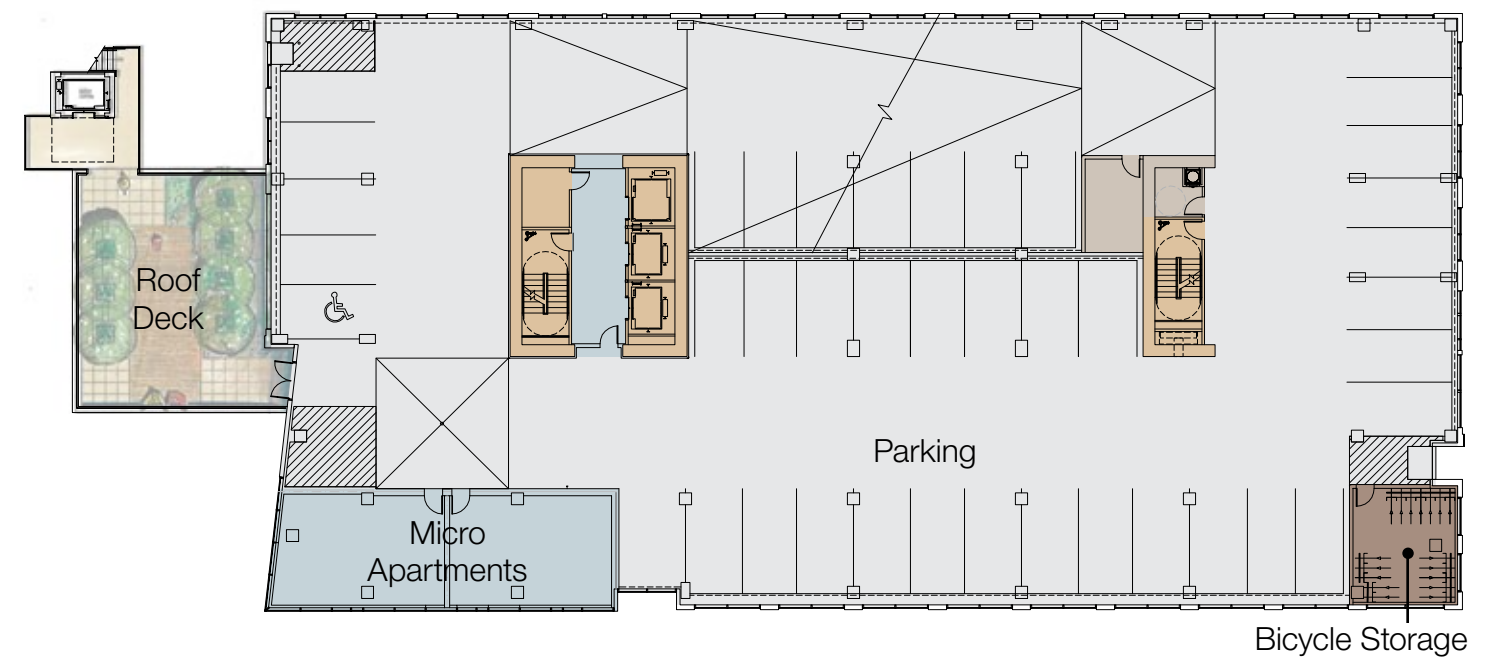
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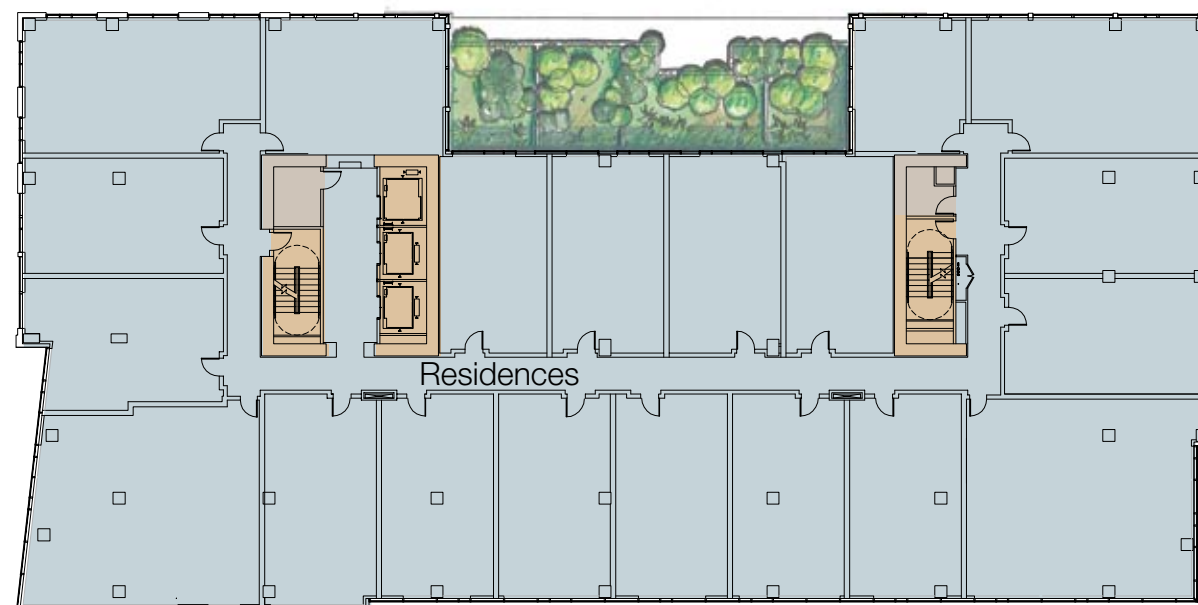




LEVEL 2



LEVEL 4 (LEVELS 3 & 5 SIMILAR)



LEVEL 6 RESIDENTIAL



LEVELS 7 – 15 RESIDENTIAL



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COLMAN TOWER | LEVELS 2, 4, 6, AND 7 – 15

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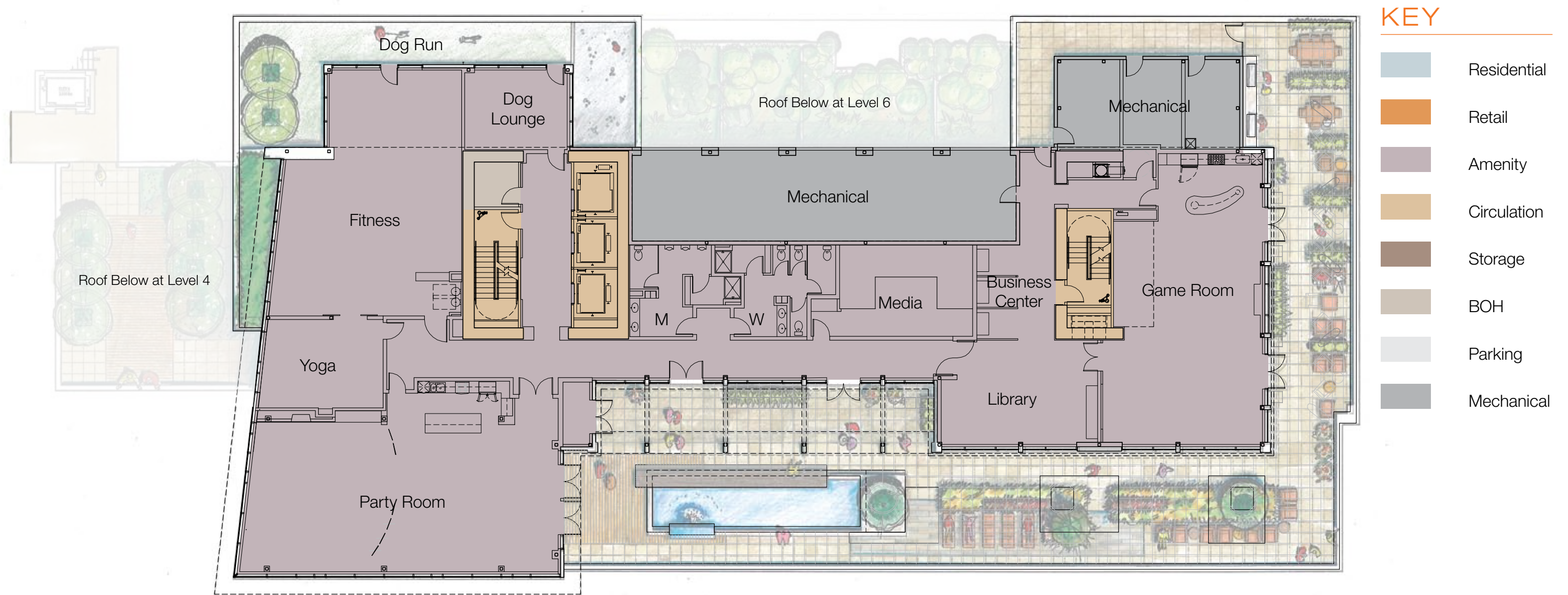
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COLMAN TOWER | VIEW LOOKING SOUTH AT POST AVENUE

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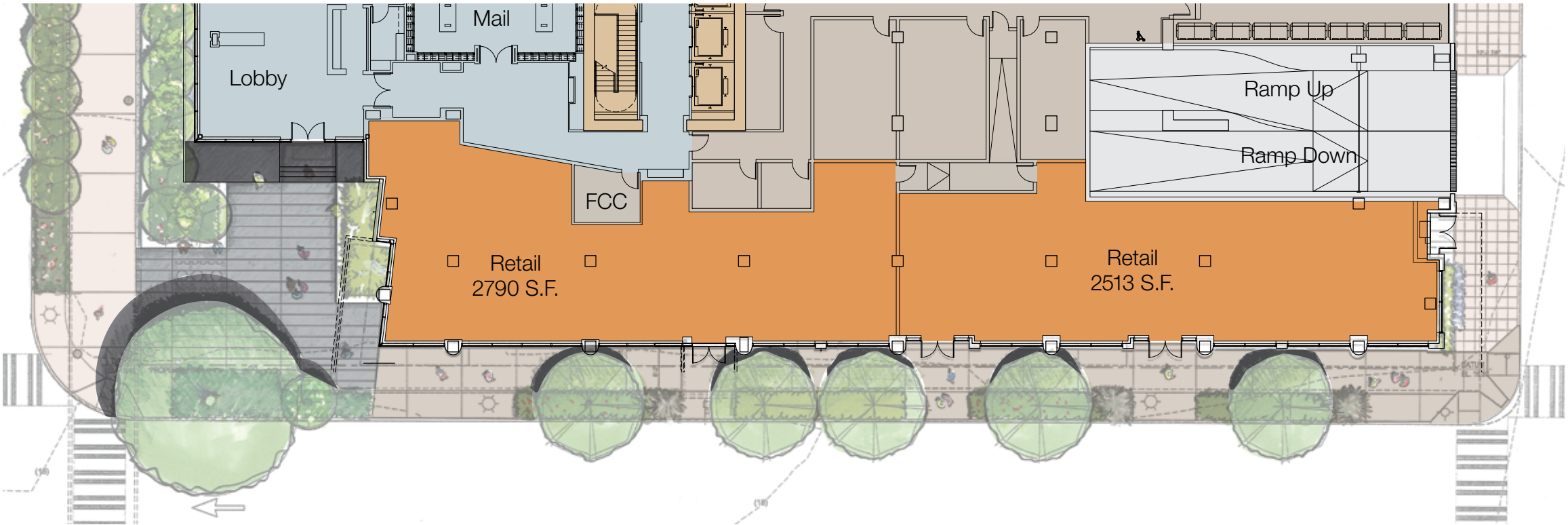
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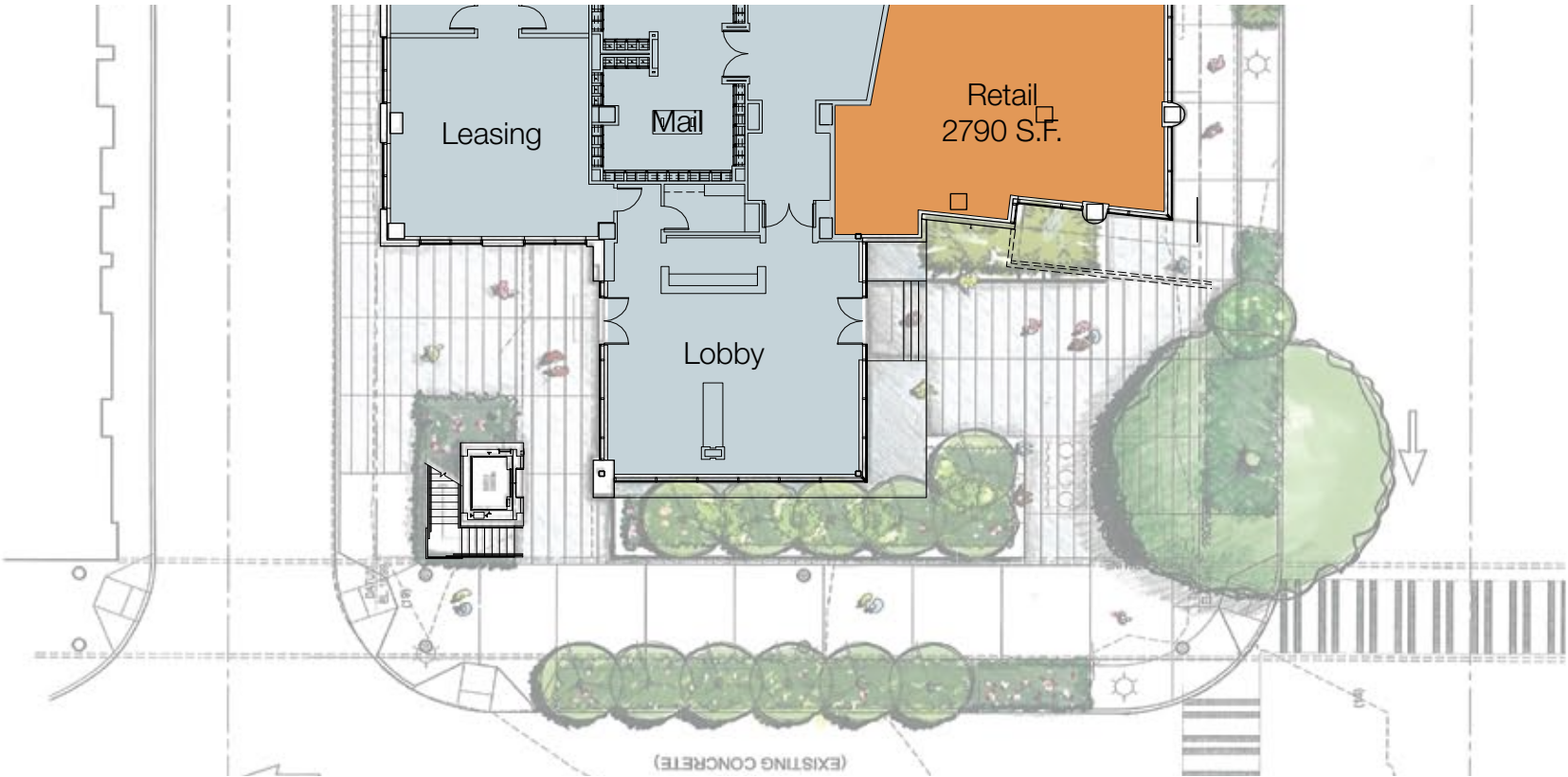
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PLAN



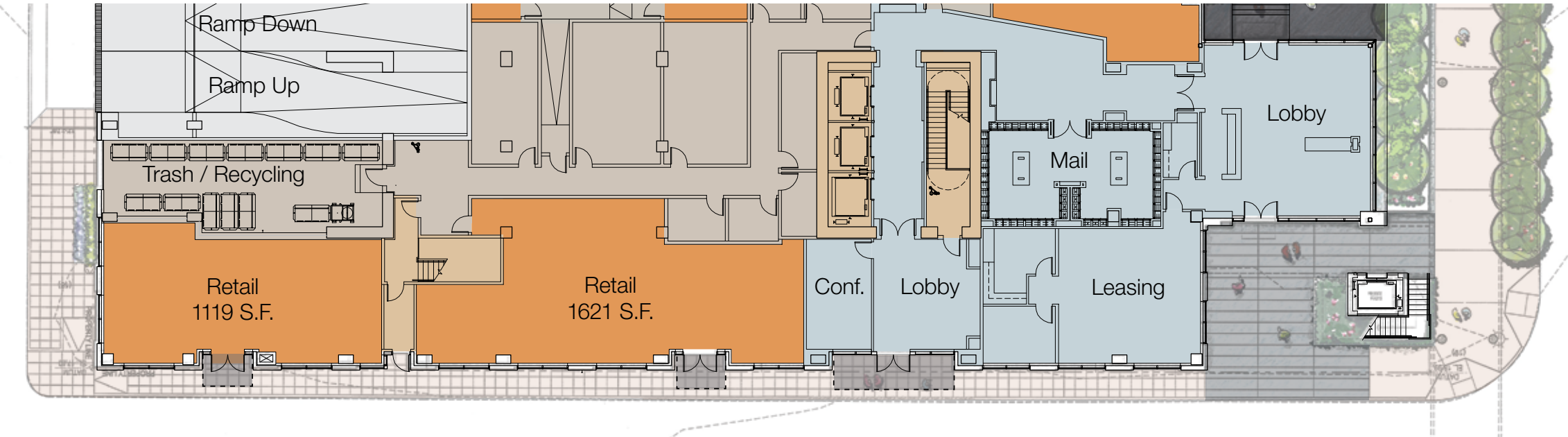
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PLAN



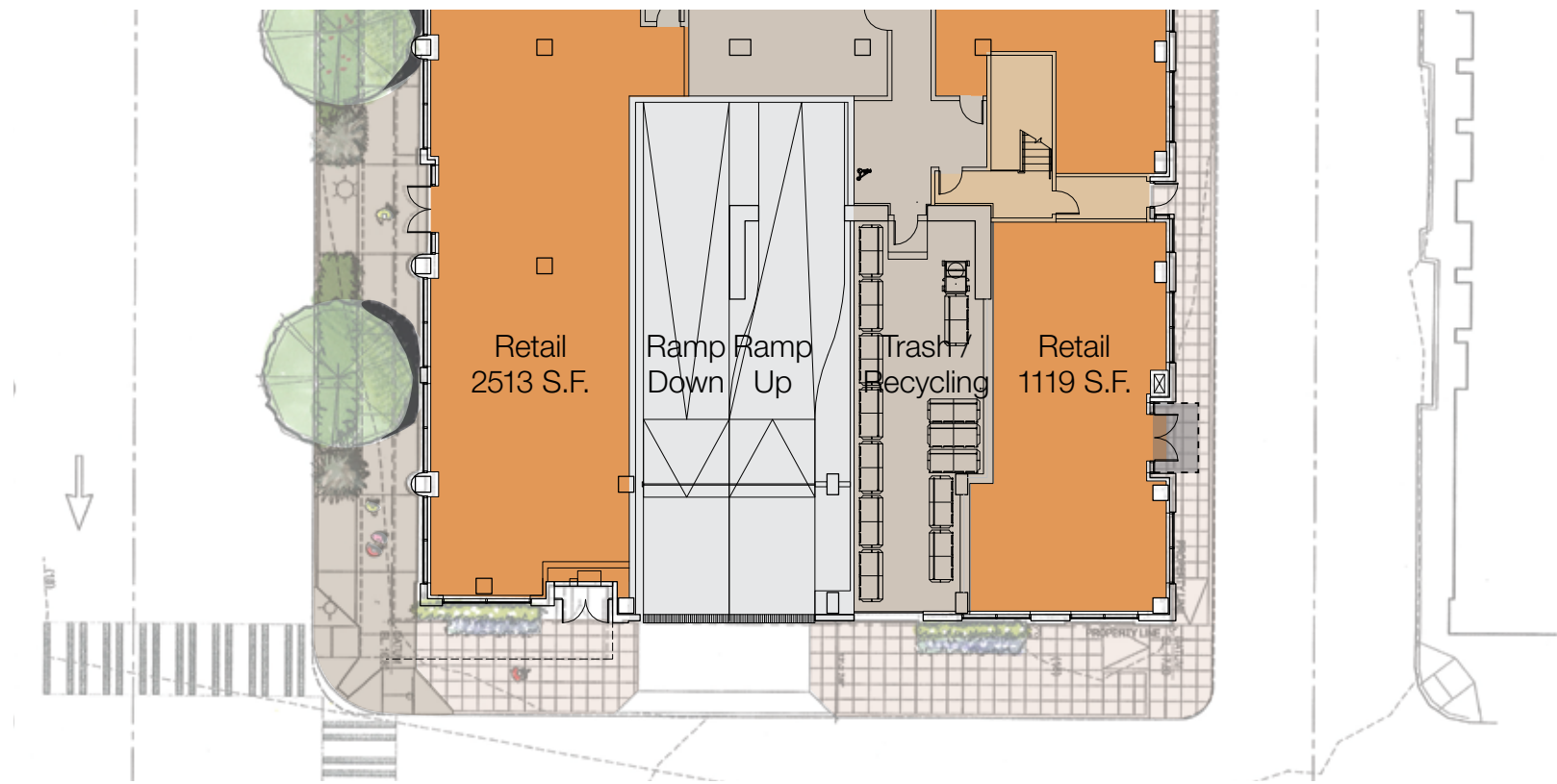
ELEVATION



PLAN



ELEVATION



PLAN



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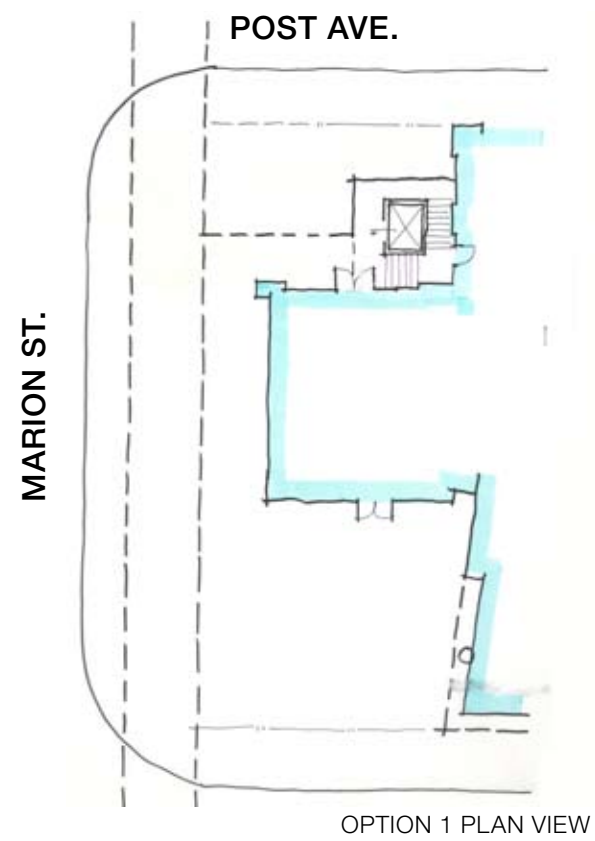
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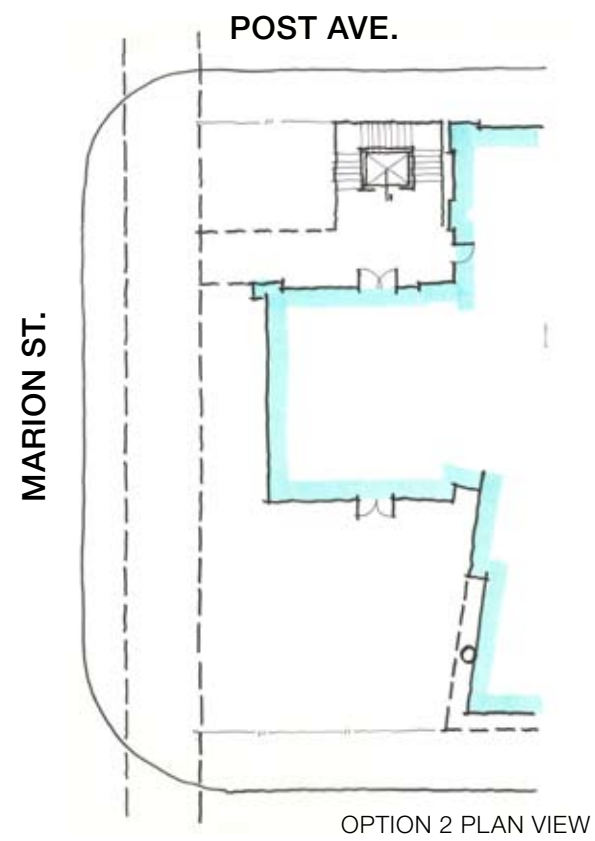
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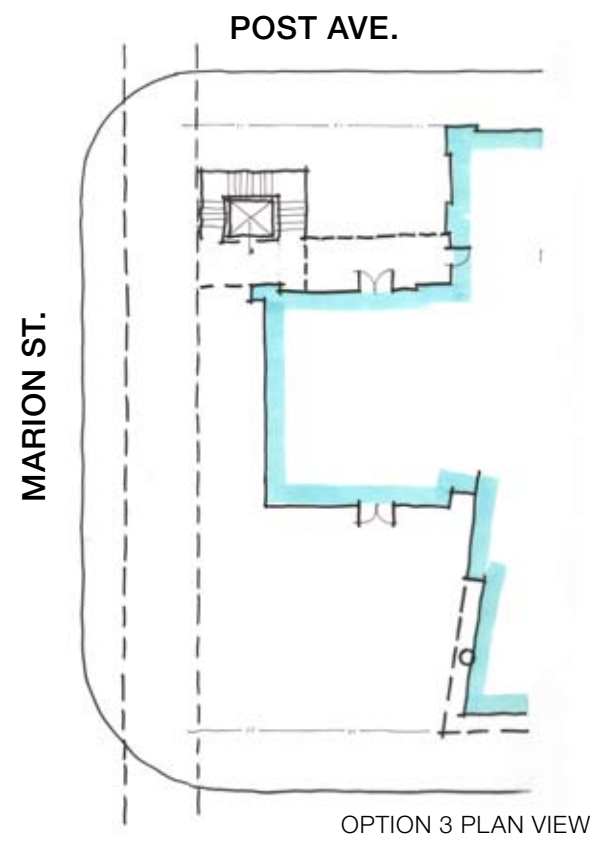
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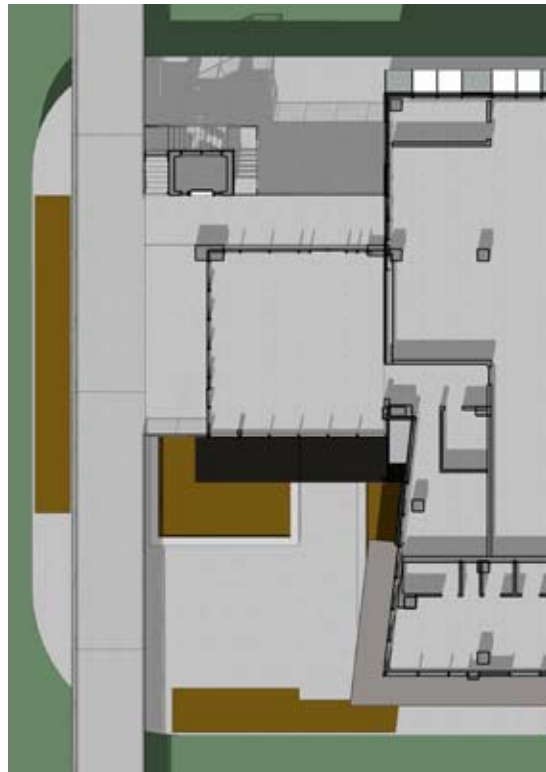
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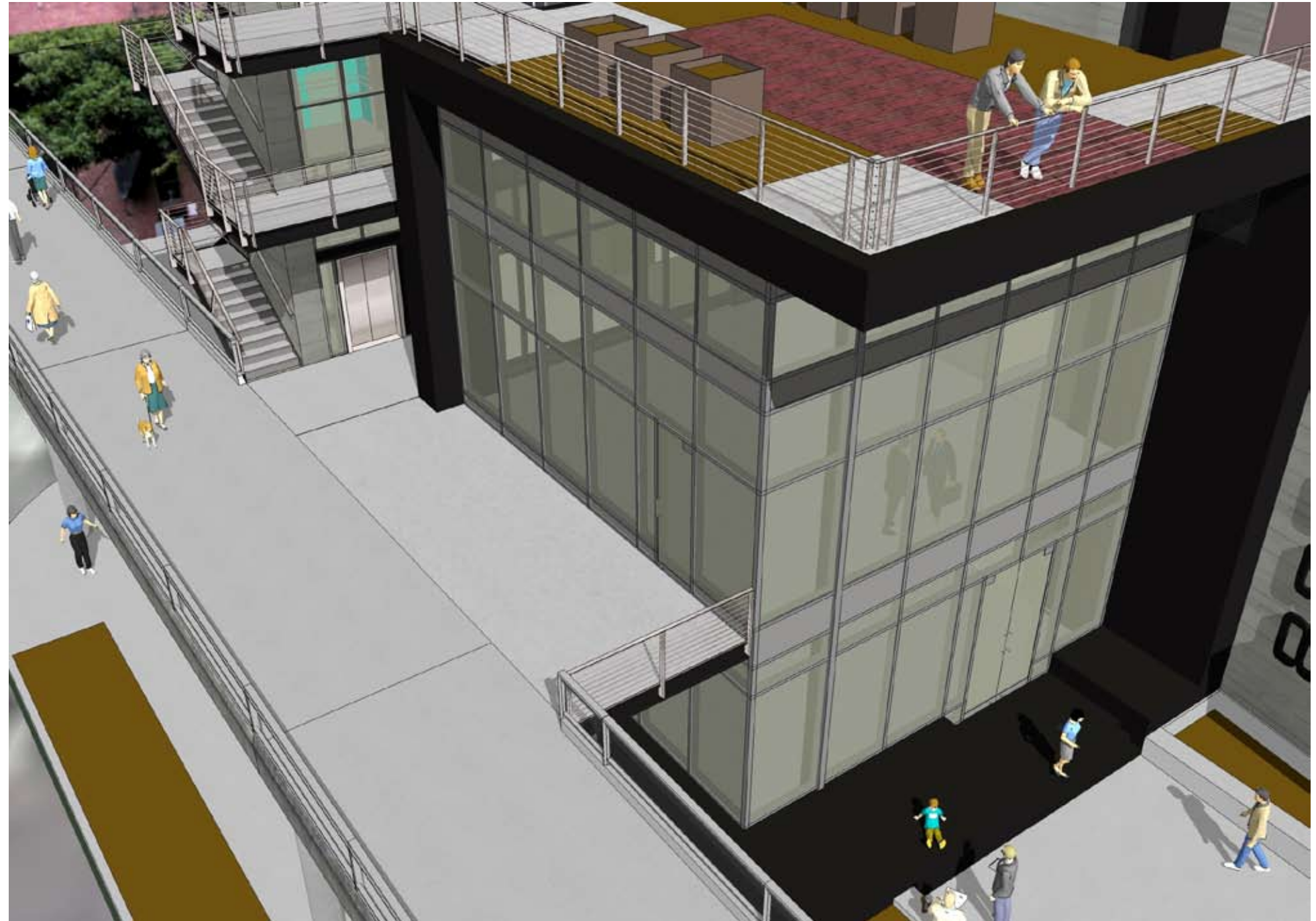




OPTION 1 PLAN



OPTION 1 STREET VIEW



OPTION 1 OVERVIEW



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COLMAN TOWER | CUBE ACCESS / FRONTAGE – OPTION 1

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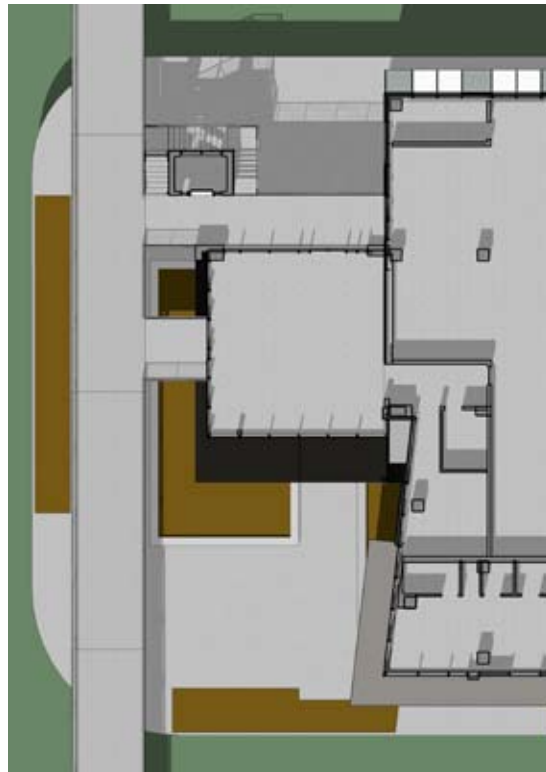
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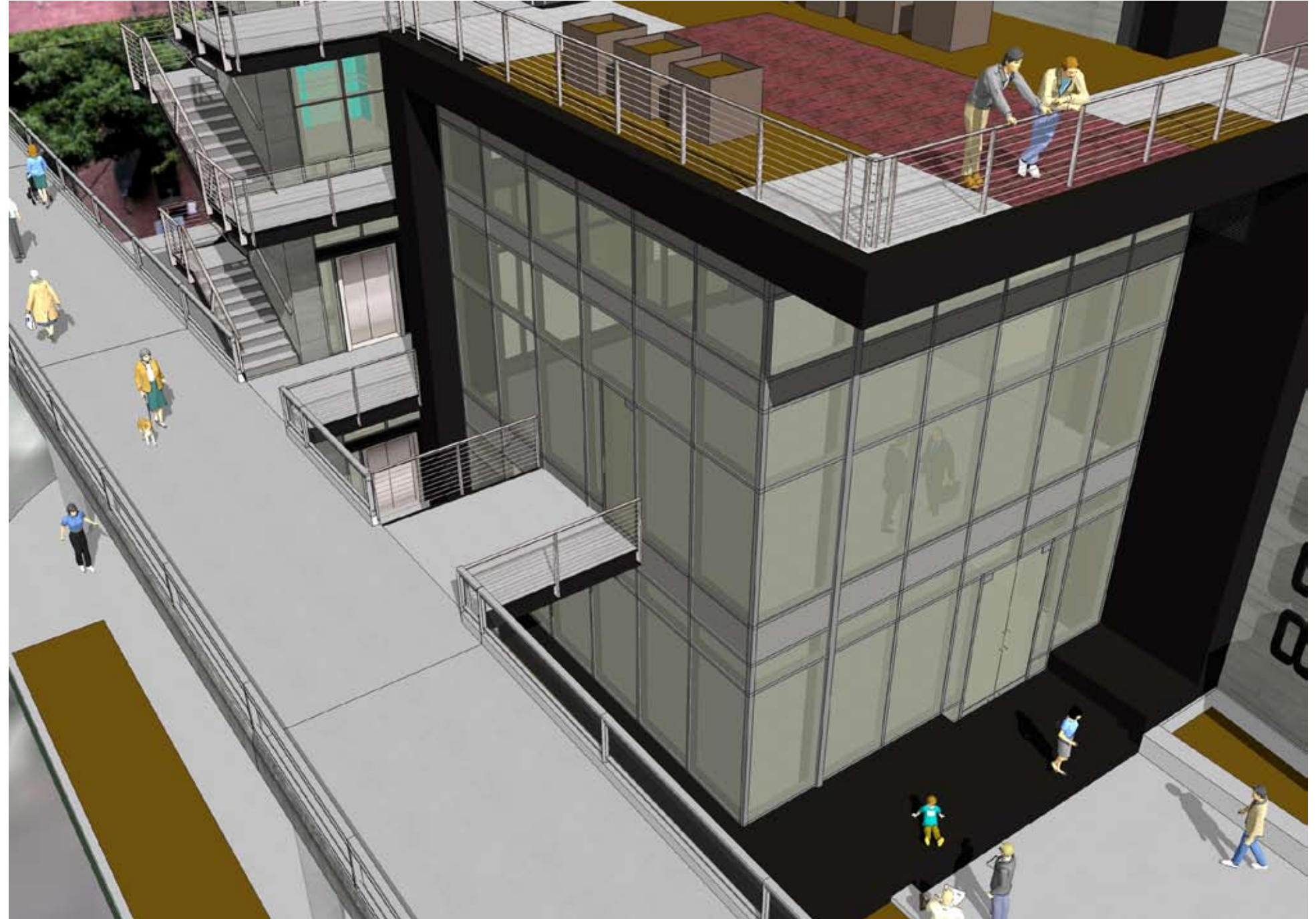




OPTION 2 PLAN



OPTION 2 STREET VIEW



OPTION 2 OVERVIEW



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COLMAN TOWER | CUBE ACCESS / FRONTAGE – OPTION 2

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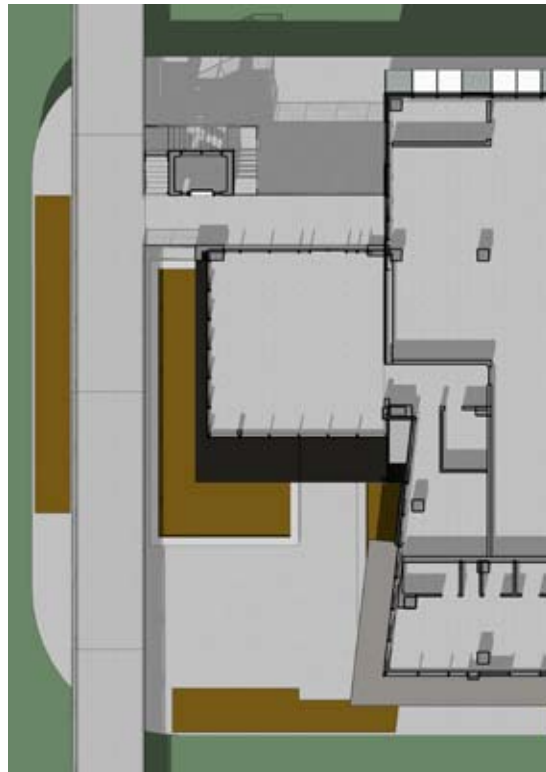
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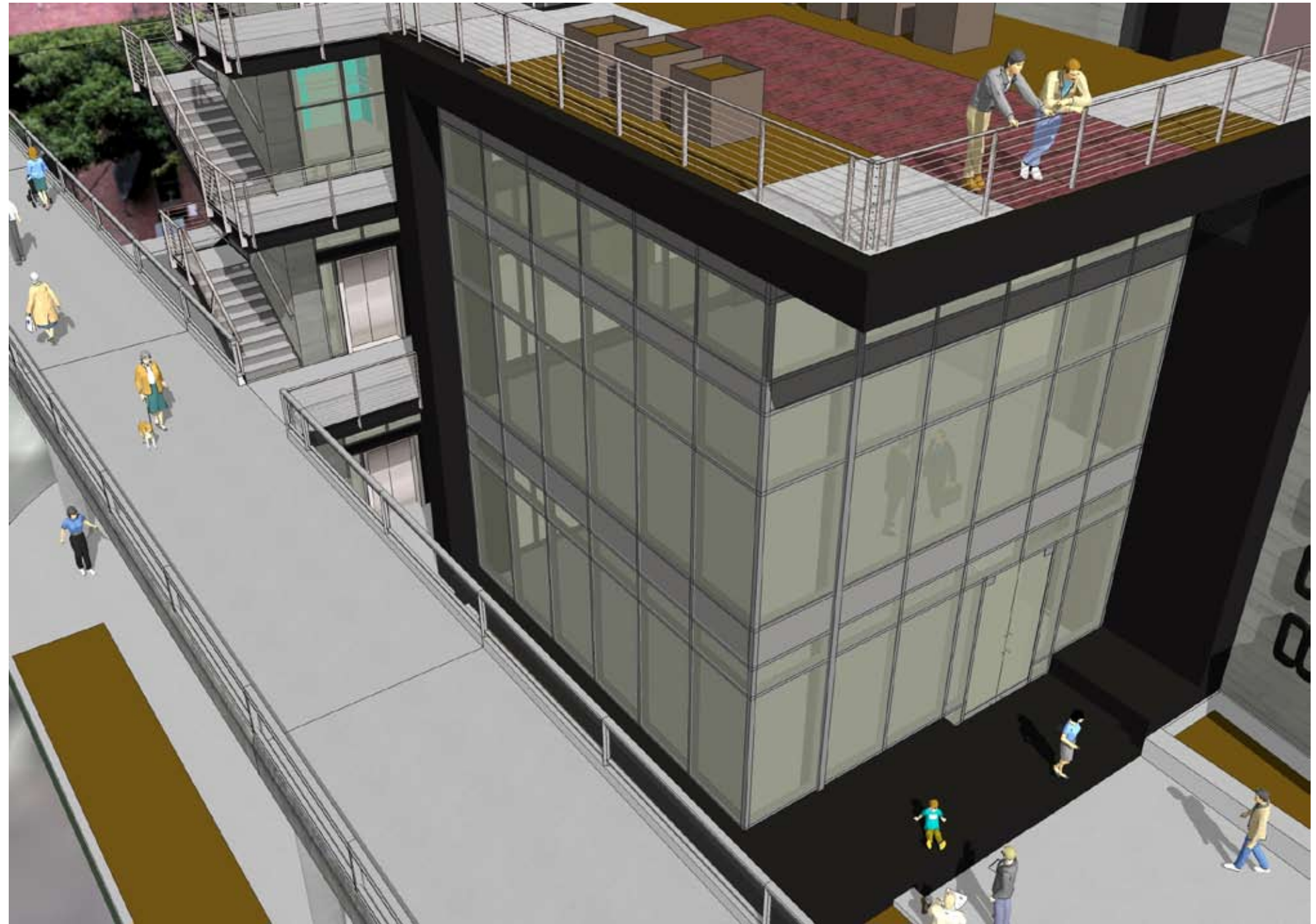




OPTION 3 PLAN



OPTION 3 STREET VIEW



OPTION 3 OVERVIEW



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COLMAN TOWER | STREET VIEW AT NORTHWEST PLAZA

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COLMAN TOWER | STREET VIEW OF NORTHWEST CORNER

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COLMAN TOWER | STREET VIEW ON COLUMBIA LOOKING NORTHEAST

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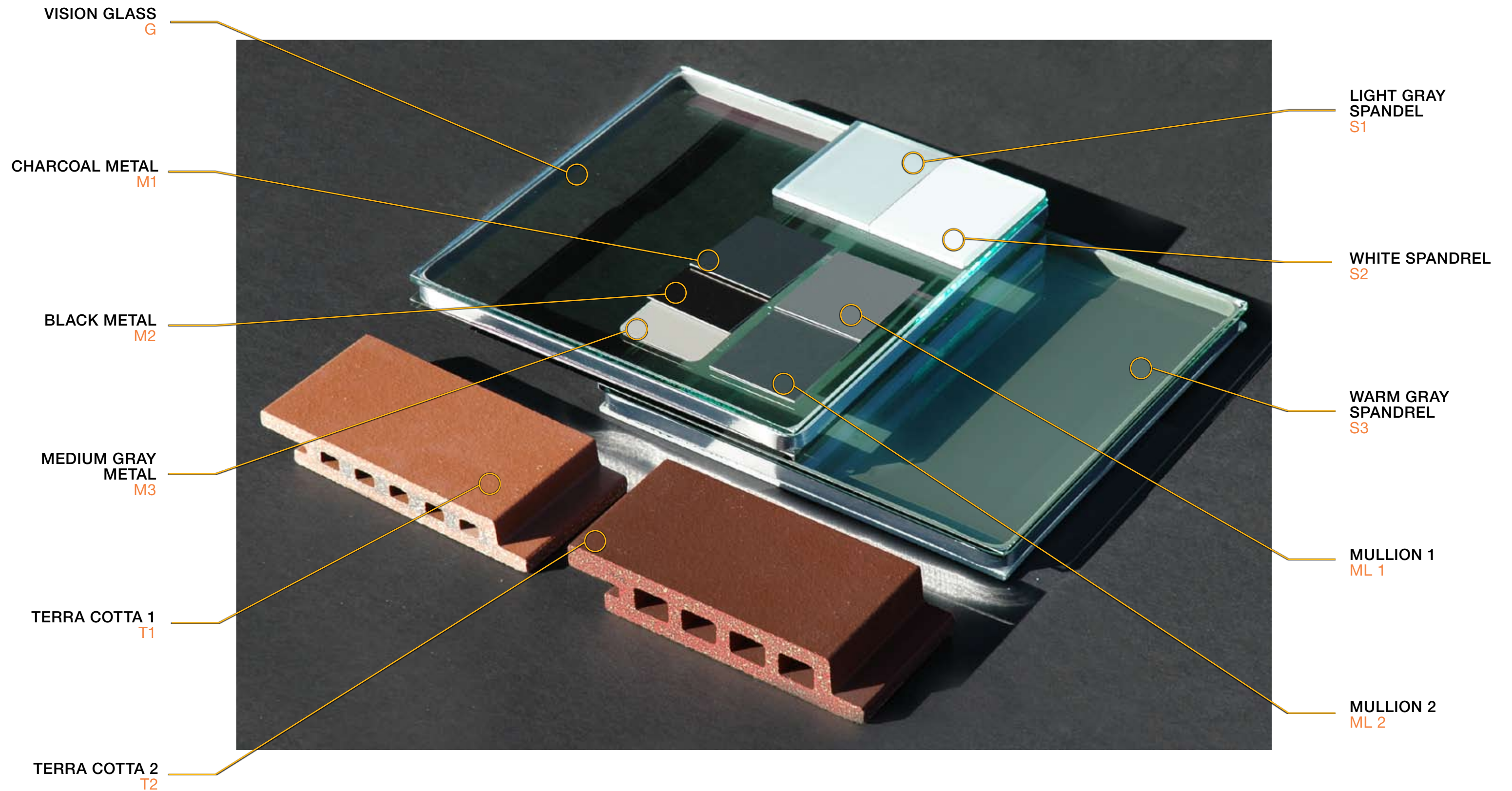
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COLMAN TOWER | ARCHITECTURAL MATERIALS

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SHADE MIX



JAPANESE FOREST GRASS



SUM & SUBSTANCE HOSTA



JAPANESE SKIMMIA



BIG GOLDE RAY



WESTERN SWORD FERN

HONEYLOCUST GROVE



HONEYLOCUST

BIORETENTION PLANTER

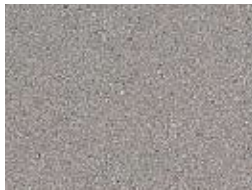


TWINBERRY

PAVING



BLACK CONCRETE



NATURAL CONCRETE



POROUS CONCRETE



NATIVE DOGWOOD TREE



MUSHASHINO ZELKOVA (COLUMNAR)



DWARF CALIFORNIA MOCKORANGE



SENECIO



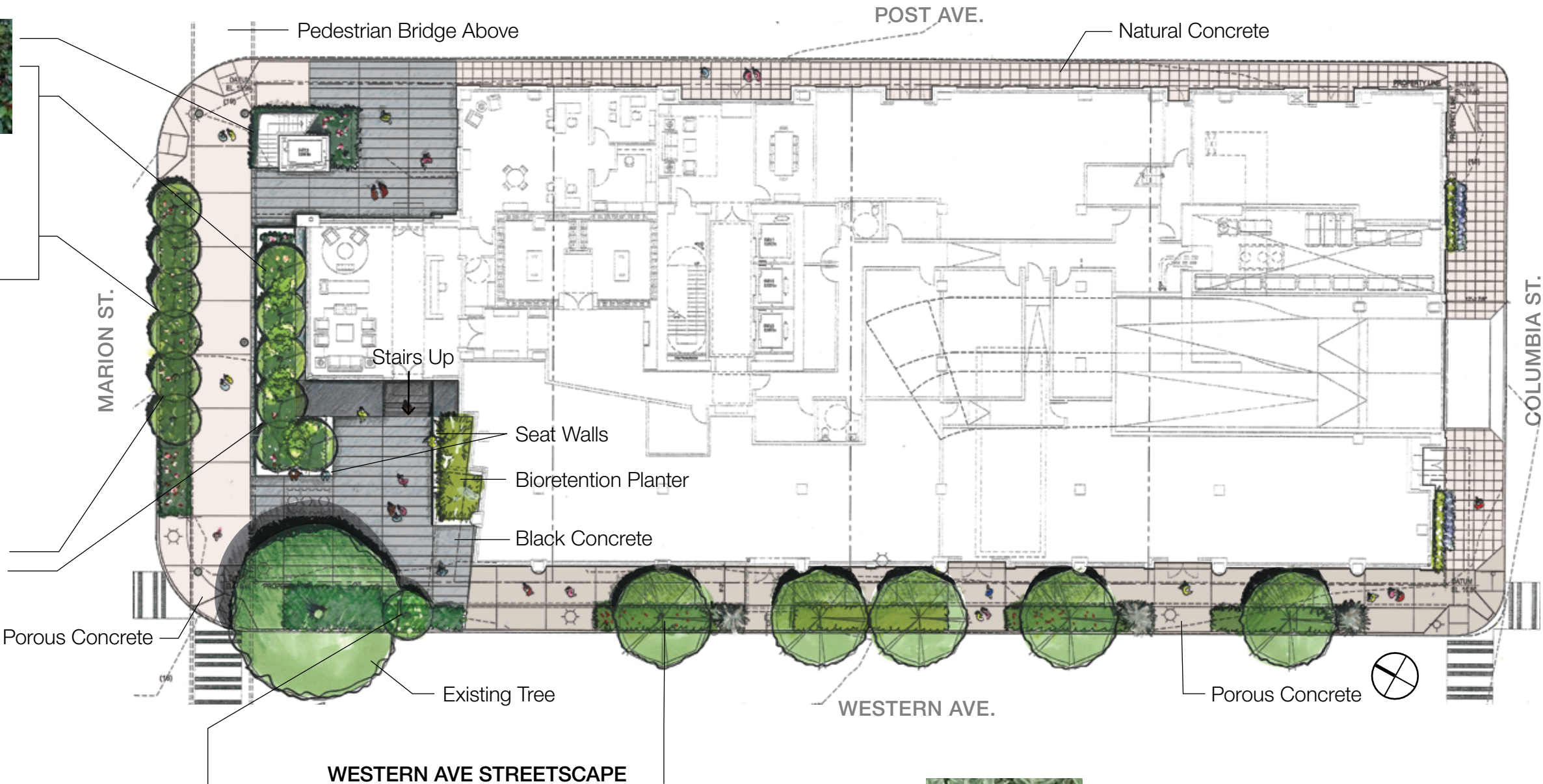
PURPLE EUPHORBIA



VARIEGATED ABELIA



DWARF ESCALLONIA



WESTERN AVE STREETSCAPE

DOG RUN



K9 TURF GRASS

BIORETENTION PLANTER L6 ROOF BELOW



COMMON RUSH



PACIFIC WAX MYRTLE



DWARF ARCTIC WILLOW



ISANTI RED TWIG DOGWOOD



SLOUGH SEDGE



SMALL FRUITED BULLRUSH

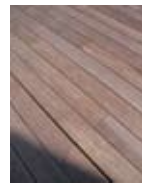
L4 ROOF BELOW



GREEN ROOF



NATCHEZ CRAPE MYRTLE



IPE DECK



SWEET BOX



POOL DECK



POOL



PEDESTAL PAVERS



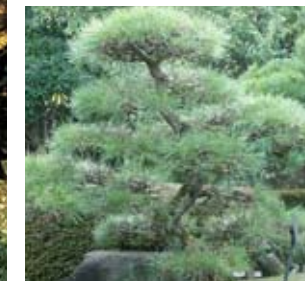
NEW ZEALAND FLAX



TWISTA CUBE PLANTERS



OSAKAZUKI JAPANESE MAPLE



JAPANESE BLACK PINE



JAPANESE BLOOD GRASS



CROSOMIA LUCIFER



OUTSIDE DINING



KARL FOESTER GRASS



MISCANTHUS MAIDEN GRASS



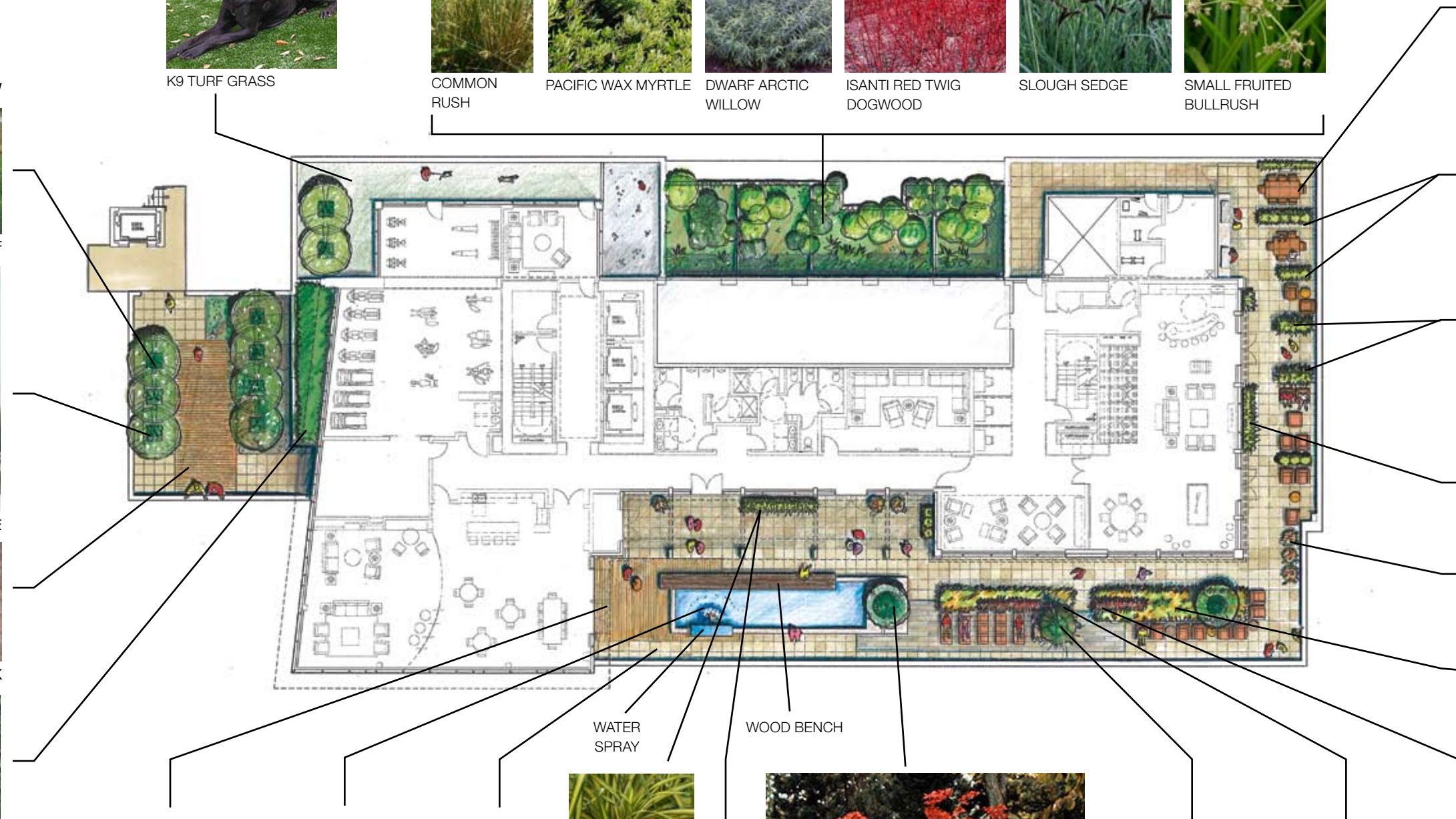
PLANTERS



SEASONAL PLANTERS



MEXICAN FEATHER GRASS



WATER SPRAY

WOOD BENCH



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COLMAN TOWER | ROOF LANDSCAPE

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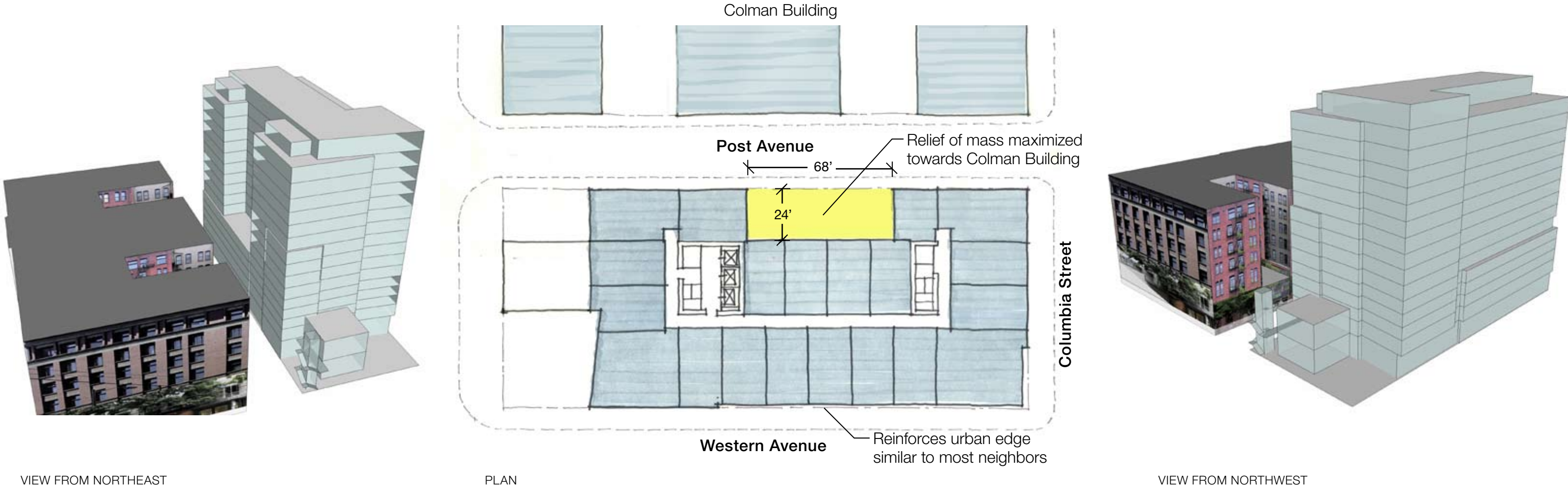
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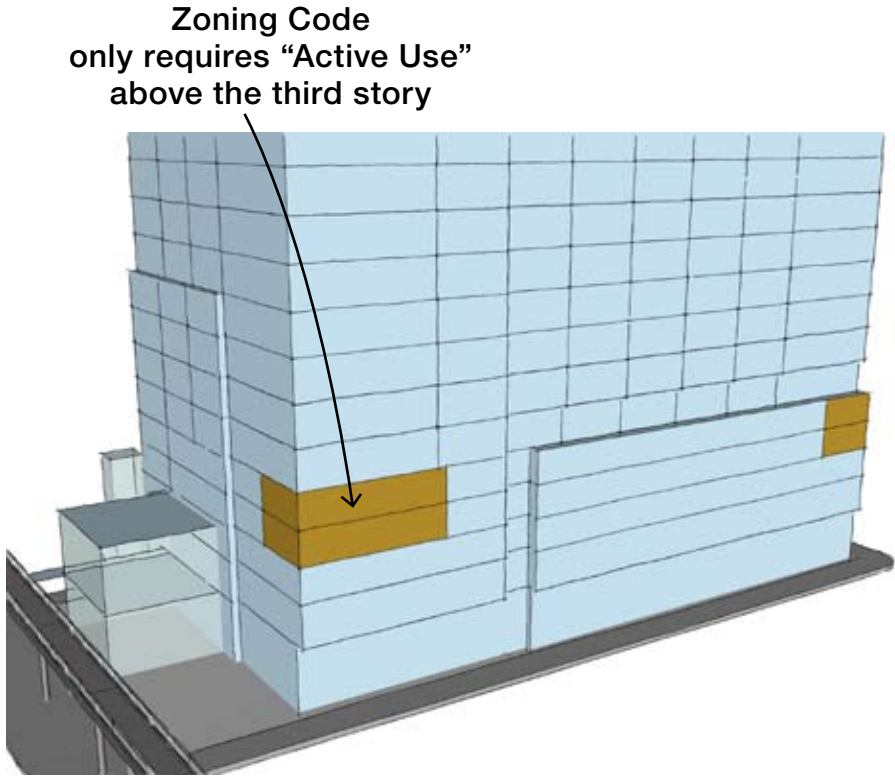
WEBER THOMPSON



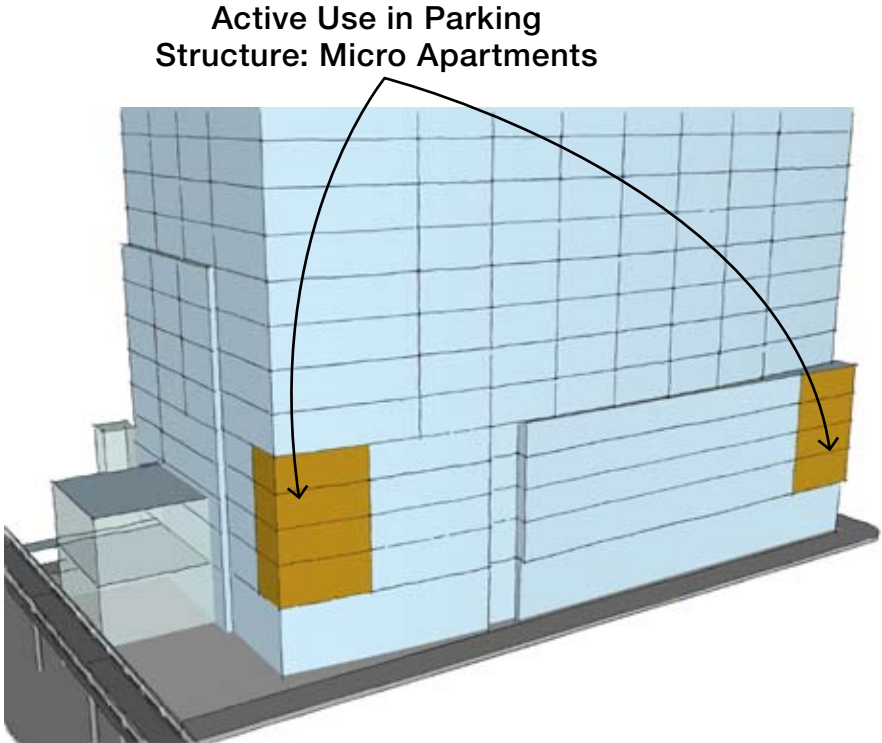
CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUEST	DIFFERENCE	RATIONALE FOR REQUEST
<p>Façade modulation</p> <p>SMC 23.49.058 B</p>	<p>Façade modulation is required above a height of eighty-five (85) feet above the sidewalk for any portion of a structure located within fifteen (15) feet of a street property line. No modulation is required for portions of a façade set back fifteen (15) feet or more from a street property line. Any portion of a façade exceeding 155' shall be set back a minimum of fifteen (15) feet from the street property line for a minimum distance of sixty (60) feet before any other portion may be within fifteen (15) feet of the street property line.</p>	<p>Eliminate the requirement to setback 15'-0", above 85', along the western façade in deference to maximizing relief to the Colman Building across Post.</p>	<p>15'-0"</p>	<p>The applicant proposes to focus all modulation and relief along Post Avenue where the right of way is extremely narrow (36'), providing a 24'-0" x 68'-0" setback along Post Avenue. The setback architecturally recalls the setbacks along the existing Colman Building along Post while the proposed frontage along Western Avenue is a more direct contextual massing response to the older buildings, which are typically "proud" to the property line, holding the street edge.</p> <p>This was generally supported by the DRB at the Early Design Guidance meeting.</p>



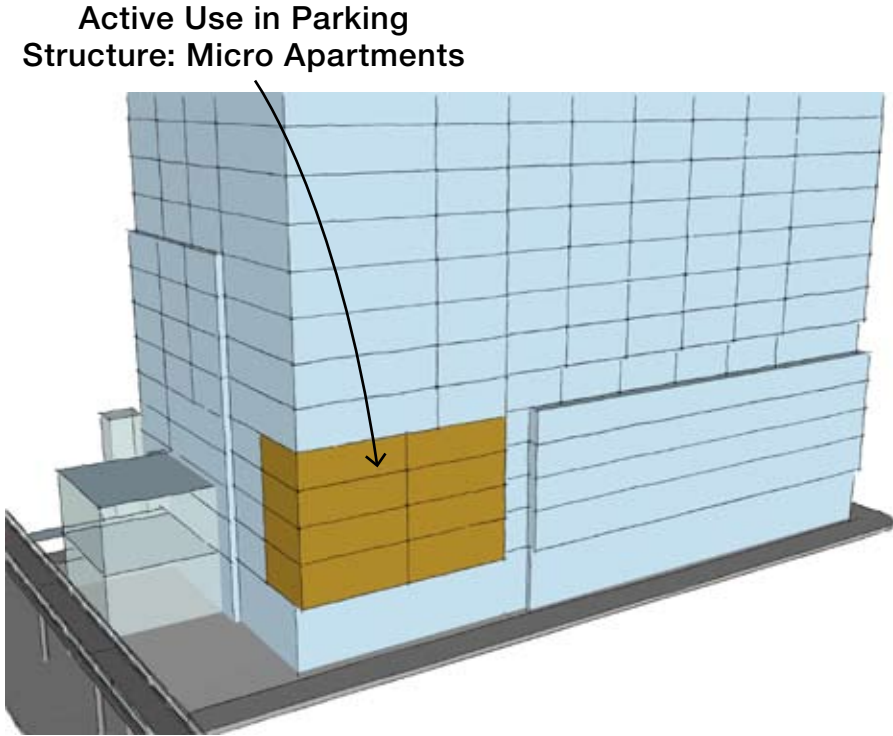
CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUEST	DIFFERENCE	RATIONALE FOR REQUEST
Above grade parking treatment. SMC 23.49.019-B2.A.2	Parking above the third story of a structure shall be separated from the street by another use for a minimum of thirty (30) percent of each street frontage of the structure. For structures on lots located at street intersections, the separation by another use shall be provided at the corner portion(s) of the structure.	A departure has been requested to decrease the required active street frontage at levels 4 and 5 in exchange for providing more activation, but on four levels, not just two (floors 2-5).	Reduce requirement to: 0 % Along Post 0 % Along Columbia 30 % Along Western 21 % Along Marion	<p>The design team proposes to provide active street facades in the form of studio “micro” apartments at the corner nearest Western and Marion. The configuration of the parking ramp (drive lane) precludes active uses along Post Avenue. To compensate for the departure, the design team proposes activating parking floors on all parking levels, 2-5, opposed to just the two required levels, 4 and 5, per the zoning requirement. This treatment provides a net gain of “activated area” on the building from the EDG. At the Early Design Guidance Meeting, the Board noted that on Western Avenue the residential units at the parking levels should be “shifted to the curtain wall area” so that the parking levels could be behind the punched masonry grid block area.</p> <p>This was generally supported by the DRB at the Early Design Guidance meeting.</p> <p><i>Note: The chart to the left is based on those percentages of activation facing Marion and Western to occur on all four levels of parking. Since DPD’s standard requirement of 30% is for two levels, one can double the 21% and 30% numbers in the chart for a more direct comparison.</i></p>



CODE COMPLIANT (ACTIVATION “ABOVE THE THIRD STORY”)

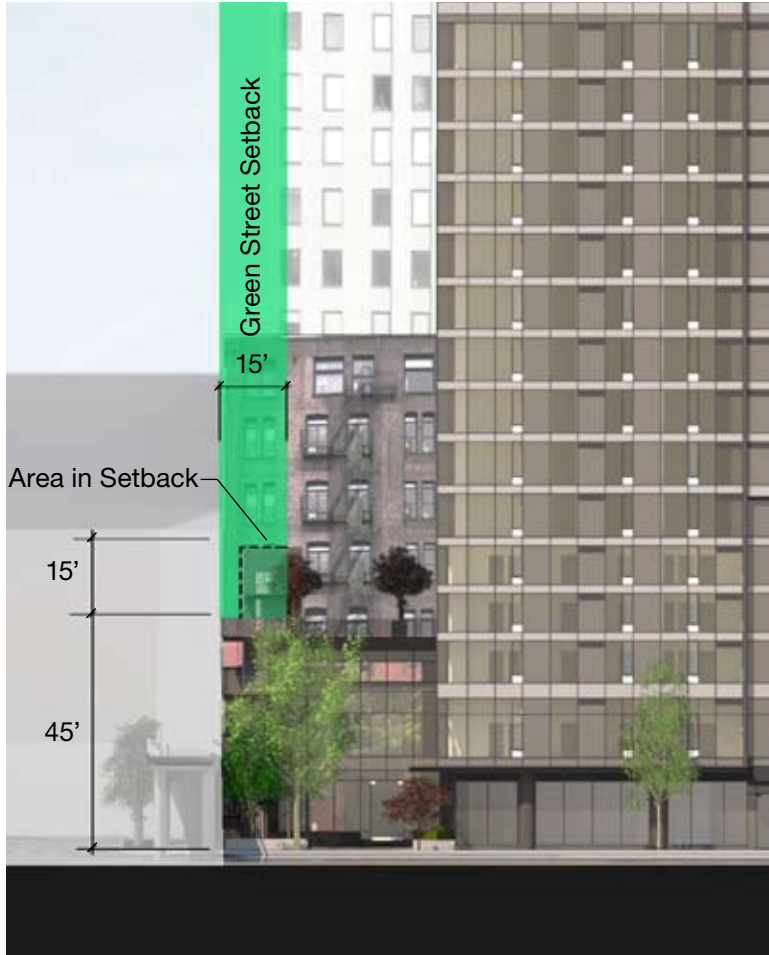


NOTE: FROM EDG

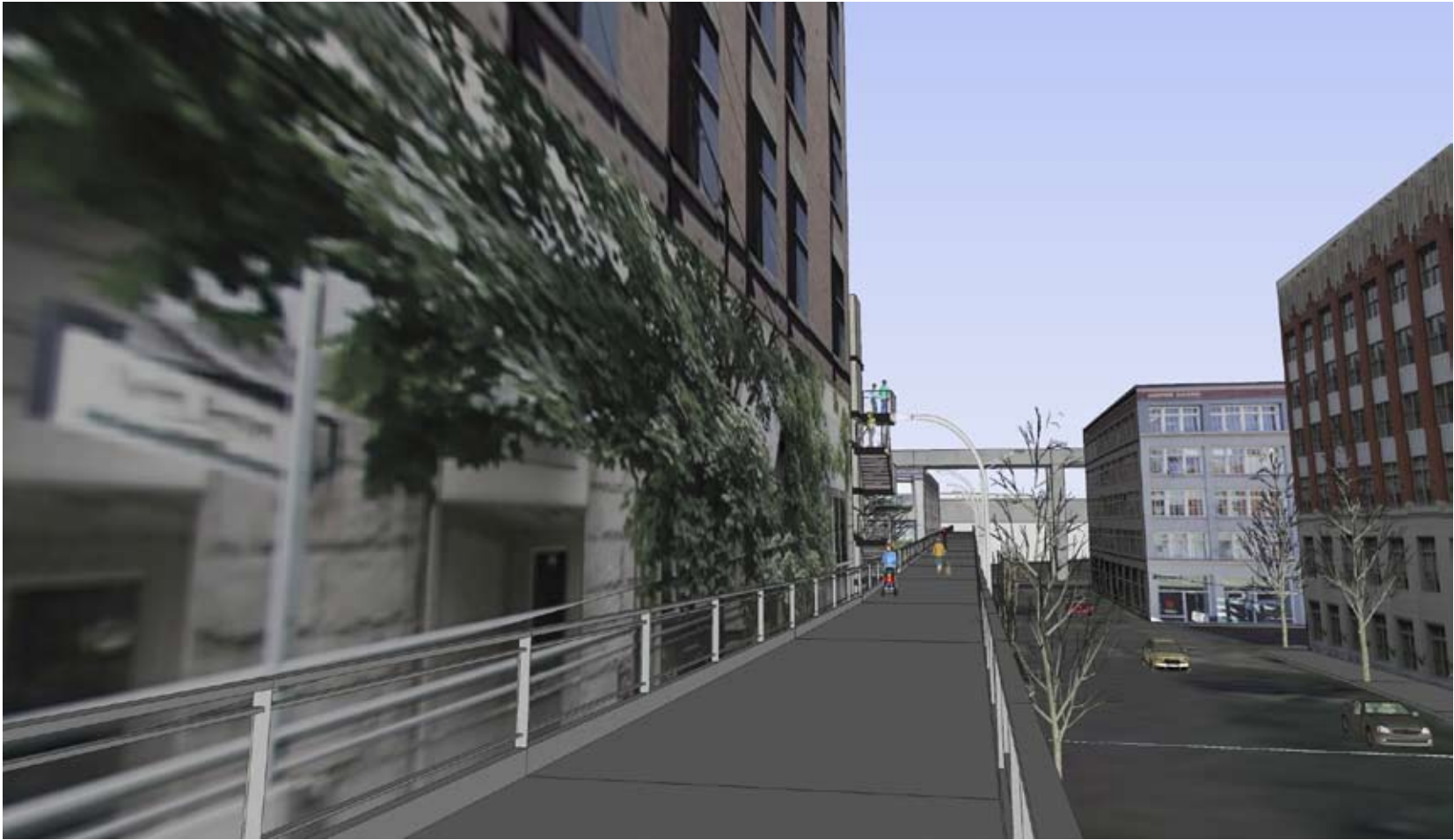


NOTE: DEPARTURE REQUESTED PER EDG DIRECTION

CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUEST	DIFFERENCE	RATIONALE FOR REQUEST
Setback requirements on green streets SMC 23.49.058 F.2	When a lot in a DMC zone is located on a designated green street, a continuous upper-level setback of 15' shall be provided on the street frontage abutting the green street at a height of 45'.	Allow the public elevator shaft enclosure to encroach into the green street setback, from a height of 45' to 60'.	15' (for one, incidental element)	<p>Since the EDG meeting, the applicant studied further the placement of the public elevator and stair to provide better, more convenient and “intuitive” use. This vertical circulation element connects Post Avenue, the existing pedestrian bridge, second floor retail, parking, the building’s lobby and also accesses the public open space/“sunset terrace” at the roof of “the cube”. Better proximity to the pedestrian bridge, than was presented at EDG, encourages and improves upon these connections.</p> <p>If the board supports the preferred option for the elevator/stair location (Option 3, page 28), this departure, if recommended, allows only the elevator (not the stairs, as they are below 45') to encroach into the green street setback, as a public amenity.</p> <p><i>Note: The applicant feels that in order to maximize this vertical circulator as a public amenity, it should go to the sunset terrace. If the elevator does not go to the roof terrace, this departure is not needed as the elevator structure serving only as high as the current pedestrian bridge would be less than 45' high.</i></p> <p><i>Note: The elevator enclosure is 5' south of the pedestrian bridge and the 91' tall Colman Building is right to its property lines, essentially masking the elevator from most views from the east, looking west.</i></p>

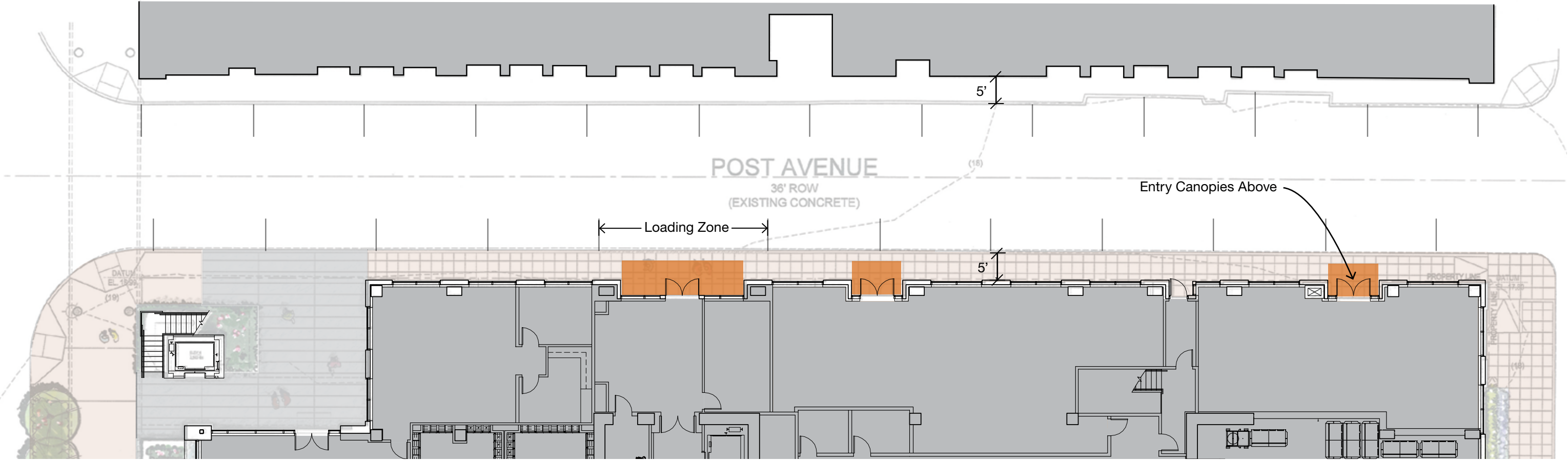


SECTION LOOKING EAST



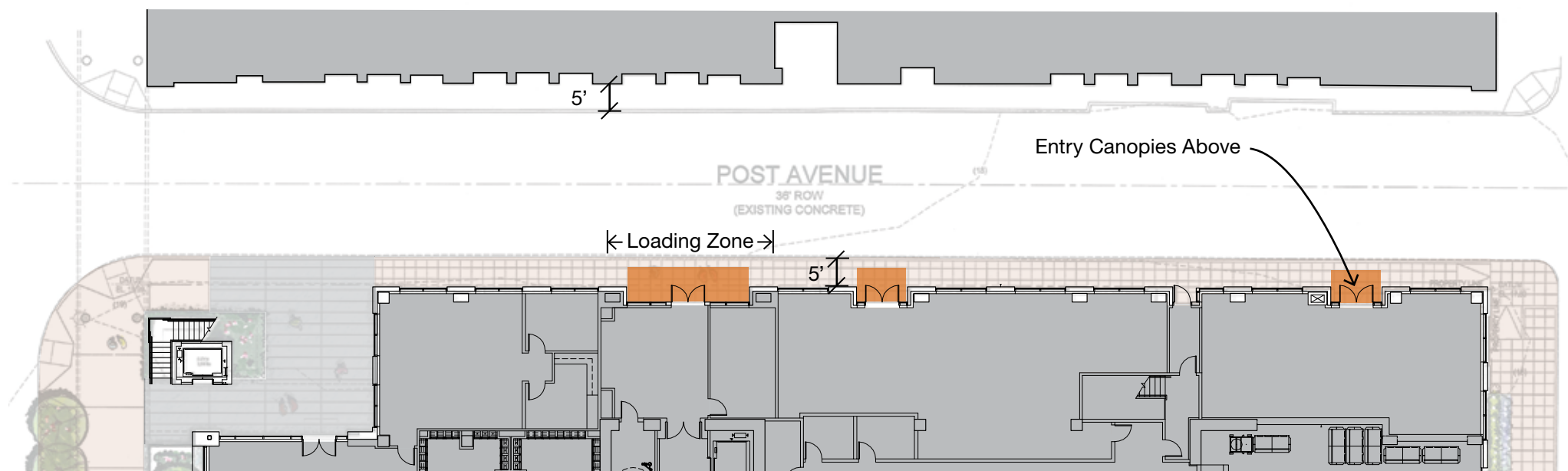
VIEW FROM PEDESTRIAN BRIDGE AT 1ST AVE

CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUEST	DIFFERENCE	RATIONALE FOR REQUEST
Sidewalk width Downtown zoning map 1c	12' sidewalk is required on Post Avenue.	Allow the existing 5' wide sidewalk (albeit, replaced by a new concrete sidewalk)	7'-0"	<p>Since this three-block section of Post Avenue does not show up in SDOT's sidewalk criteria map, 12' sidewalk width is a standard default dimension, typically applying to Right of Ways that are 66' or greater, throughout Seattle. Post's anomalous ROW is 36'. In order to provide a 12' sidewalk, the project would need to widen the sidewalk into the right of way (narrowing the street) or move the ground floor facade west, creating a 7'-0" overhang.</p> <p>The Seattle Dept. of Transportation does not support the sidewalk widening into the right of way. Moving the sidewalk under a deep overhang would hinder the viability of the commercial spaces along Post Avenue. Structural columns would likely need to also occur within such a 7' setback. The applicant has met with SDOT and SDOT supports a 5' sidewalk on the west side of Post, however, Streets and Bridges does not support any encroachment into the Row.</p>



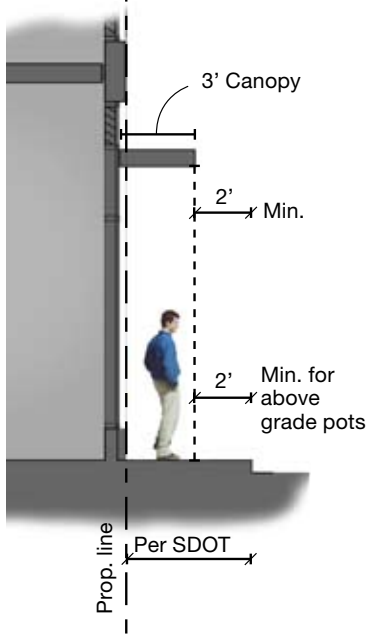
POST AVENUE PLAN

CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUEST	DIFFERENCE	RATIONALE FOR REQUEST
Overhead weather protection-along post avenue SMC 23.49.018	Continuous overhead weather protection shall have a minimum dimension of 8' measured horizontally from the building wall or must extend to a line of 2' from the curb line.	A departure is requested to allow the omission of 186 linear feet of overhead weather protection. (156' along post and 30' along Columbia)	186 linear feet	Given a 5' sidewalk and the requirement to have the overhead weather protection be no less than 2' from the curb, a 3' deep canopy is the result. The small canopy would need to be a minimum of 10' above the sidewalk. With those proportions, weather protection is greatly reduced and mostly ineffective. The portion of the canopy in question is associated with the architectural element which wraps around the corner onto Columbia. For design consistency purposes the applicant proposes omitting 30' of canopy along Columbia St. The applicant proposes to provide deep canopies at the three entry locations on Post, for architectural interest and as visual organization and invitation/identification of entries.
Overhead weather protection-along Western Ave. SMC 23.49.018	Continuous overhead weather protection shall have a minimum dimension of 8' measured horizontally from the building wall or must extend to a line of 2' from the curb line.	A departure is requested to allow the reduction of the canopy depth from 8' to 7'-2" for 137 linear feet along the southern portion of the building.	10" less canopy depth for 137 linear feet	The proposed deduction is due to a conflict with the canopy and an existing street lamp. One alternative would be to keep the bulk of the canopies at 8' in depth, and "push in" the one canopy that is in conflict with the light pole.



DEPARTURE – POST AVE. OVERHEAD WEATHER PROTECTION

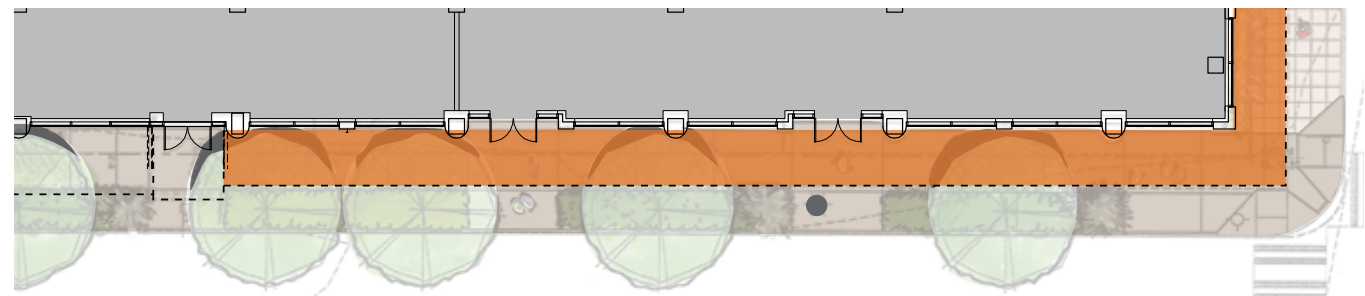
PLAN



SECTION

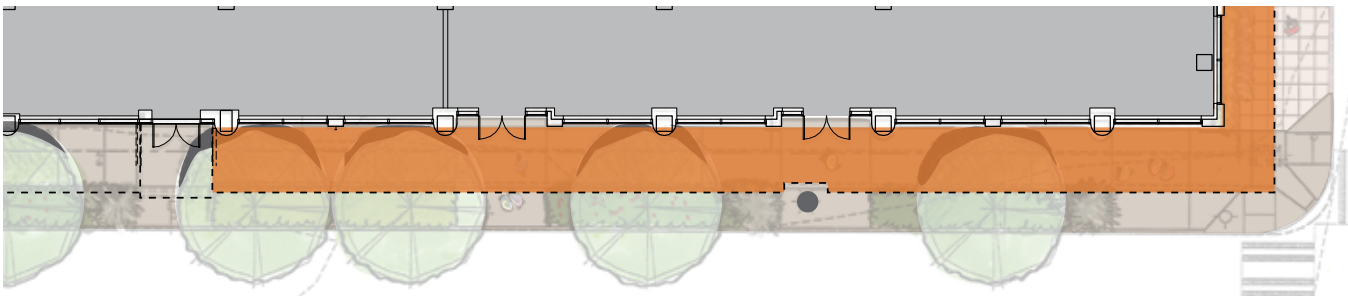


PERSPECTIVE



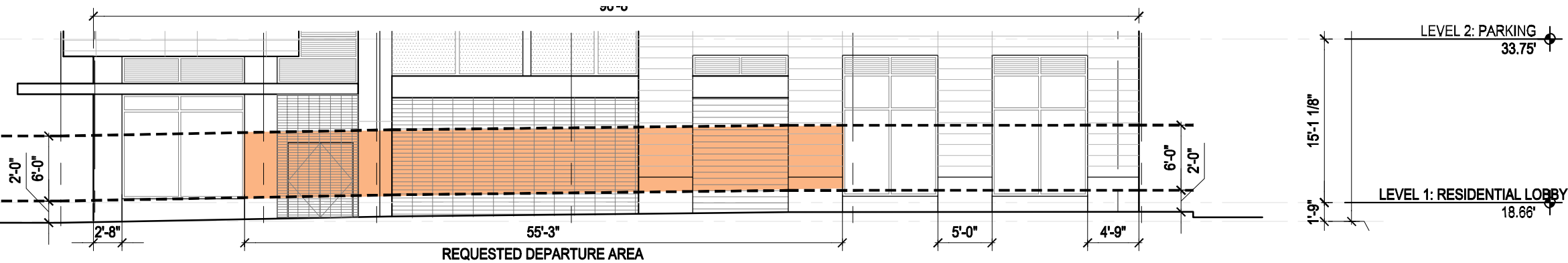
DEPARTURE – WESTERN AVE OVERHEAD WEATHER PROTECTION

PLAN WITH DEPARTURE



PLAN WITH NOTCHED CANOPY

CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUEST	DIFFERENCE	RATIONALE FOR REQUEST
SMC 23.49.056D Blank Façade	Columbia St.: Maximum blank façade length along class II pedestrian street is 30'	A departure is requested to allow a blank façade of 55'-3".	25'-3" more blank façade.	The proposed design intent is to create as much commercial use as possible along Post Ave., Marion st. and Western Ave. Due to the lack of an alley on this site, and following the design review board's direction, the building services have been co-located along Columbia St. In order to free up the areas on the other streets, creating more viable commercial areas.
SMC 23.49.056D Blank Façade	Marion St.: Maximum blank façade length along class I pedestrian street is 15'	A departure is requested to allow a blank façade of 18'-9"	3'-9" more blank façade.	The proposed design intent is to provide an architectural feature that accents the residential entry and helps disguise the parking levels. The blank façade is tucked into a corner with significant areas of landscaping in front of it. Although the façade is blank, it is architecturally dynamic and does not detract from retail frontage possibilities



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CLASS II PEDESTRIAN STREET

SCALE: 1/8"=1'-0"

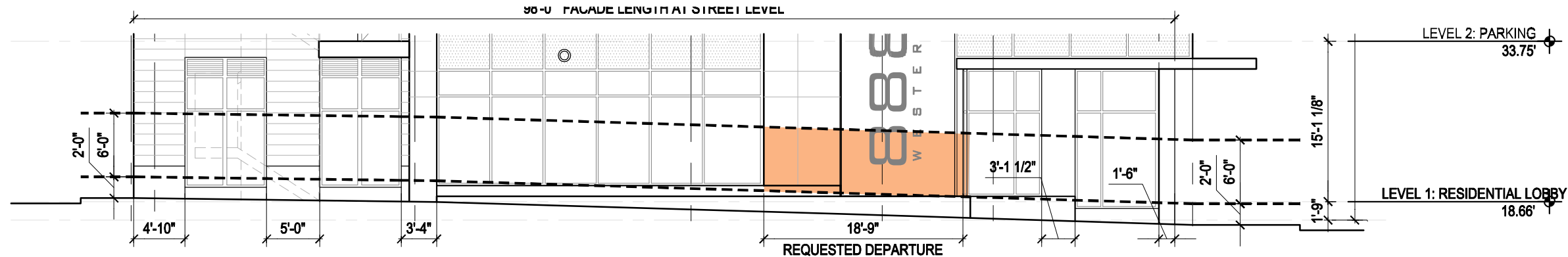
BLANK FACADE ALONG COLUMBIA

MAX BLANK FACADE LENGTH ALONG CLASS II PEDESTRIAN STREET IS 30'

MAXIMUM LENGTH PROVIDED 55' - 3" > 30'

MAX BLANK FACADE PERCENTAGE ALONG CLASS II PEDESTRIAN STREET IS 70%

70% OF 96' - 8" = 67' - 8" = 67' - 8" PROVIDED



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CLASS I PEDESTRIAN STREET

SCALE: 1/8"=1'-0"

BLANK FACADE ALONG MARION

MAX BLANK FACADE LENGTH ALONG CLASS I PEDESTRIAN STREET IS 15'

MAXIMUM LENGTH PROVIDED 18' - 9" > 15'

MAX BLANK FACADE PERCENTAGE ALONG CLASS II PEDESTRIAN STREET IS 40%

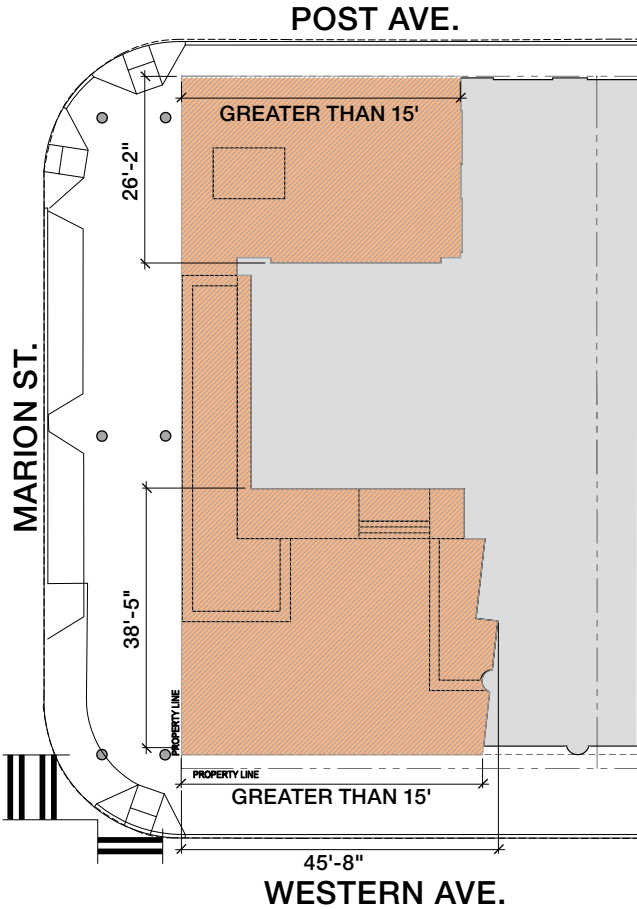
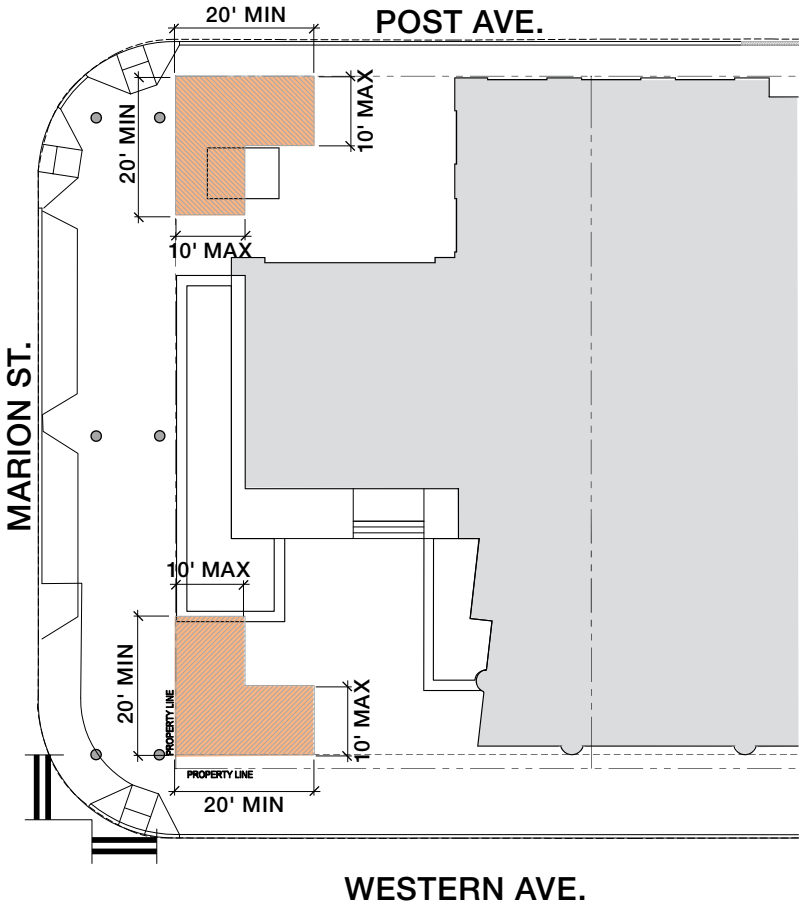
40% OF 98' - 1" = 39' - 1" > 36' - 7" PROVIDED

CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUEST	DIFFERENCE	RATIONALE FOR REQUEST
23.49.0562B Façade setback requirements	The maximum area of all setbacks between the lot line and facade along each street frontage of a lot shall not exceed the area derived by multiplying the averaging factor by the width of the street frontage of the structure along that street (see exhibit d 23.49.056. The averaging factor shall be five (5) on class i pedestrian streets and ten (10) on class ii pedestrian streets and designated green streets.	Increase the Marion Street limit from 500 sf of setback to 3100 sf.	An increase of 2600 sf.	Thr proposed design seeks to improve Marion as a designated green street. Due to the WSDOT elevated ferry pedestrian bridge, the marion street sidewalk is largely covered. Creating 2 plazas flanking the residential entry allows the green street to flow out from under the bridge. We are incorporating significant areas of planting and hardscape plaza space for public use.
23.49.0562C Façade setback requirements	The maximum width, measured along the street property line, of any setback area exceeding a depth of fifteen (15) feet from the street property line shall not exceed eighty (80) feet, or thirty (30) percent of the lot frontage on that street, whichever is less.	Increase the Marion Street limit from 30' to 65'-0"	An increase of 35'	In order to create the plazas for public use, the amount of setback for areas 15+ feet in depth has been increased.
23.49.0562D Façade setback requirements	The maximum setback of the façade from the street property lines at intersections shall be ten (10) feet. The minimum distance the facade must conform to this limit shall be twenty (20) feet along each street.	Increase the maximum setback at the corner of Marion / Western and Marion / Post to to 45'-8"	An increase of 35'-8"	In order to create the plazas for public use, the maximum setback at the corners of marion and post and western have been increased.

SETBACK DEPTH: *DEPARTURE REQUESTED*
MAX SETBACK DEPTH = 10'-0"
SETBACK DEPTH =
40'-6 3/4" ALONG MARION
26'-11 3/8" ALONG POST
SETBACK LENGTH:
MIN SETBACK LENGTH = 20'-0"
SETBACK LENGTH =
26'-2 3/8" ALONG MARION
40'-3 3/4" ALONG POST

SETBACK DEPTH: *DEPARTURE REQUESTED*
MAX SETBACK DEPTH = 10'-0"
SETBACK DEPTH =
43'-7 3/8" ALONG MARION
38'-5 1/8" ALONG WESTERN

SETBACK LENGTH:
MIN SETBACK LENGTH = 20'-0"
SETBACK LENGTH =
43'-7 3/8" ALONG WESTERN
38'-5 1/8" ALONG MARION



SETBACK LENGTH: *DEPARTURE REQUESTED*
MAXIMUM SETBACK OF 15'+ ALONG WESTERN = 30'
SETBACK LENGTH OF 15'+ ALONG WESTERN = 64'-7 1/2"
SETBACK AREA: *DEPARTURE REQUESTED*
MAXIMUM SETBACK AREA CALC =
LOT LENGTH (100') X CLASS 1 PED MODIFIER (5)
SETBACK AREA ALLOWED = 500 SF
SETBACK AREA PROVIDED = 3074 SF



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COLMAN TOWER | LIGHTING PLAN

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COLMAN TOWER | LIGHTING: VIEW FROM NORTHEAST

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COLMAN TOWER | LIGHTING: VIEW LOOKING SOUTH ON POST AVENUE

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APPENDIX



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COLUMBIA FROM EAST



MARION FROM EAST



MARION STREET FROM WEST



OLD FEDERAL BUILDING ON WESTERN



PEDESTRIAN BRIDGE ON MARION



COMMUTER BUILDING ACROSS WESTERN



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COLMAN TOWER | SITE CONTEXT — CONTEXT PHOTOS

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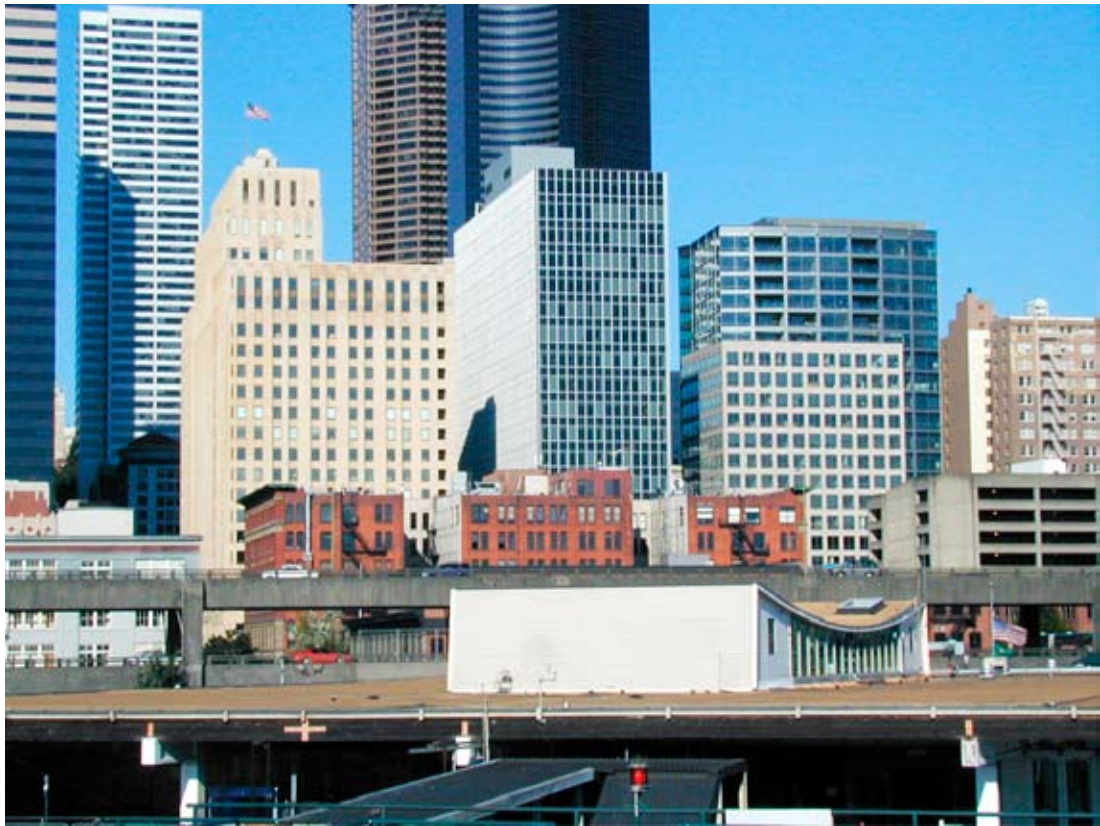
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VIEW FROM FERRY TERMINAL



MARION STREET FROM WEST



GARAGE ACCESS ON WESTERN



WESTERN AVENUE FROM NORTH



MARITIME BUILDING



SITE FROM SW CORNER



SITE FROM SOUTH / ABOVE POST



POST AVE FROM NORTH



POST AVE FROM SOUTH



SITE FROM NORTHEAST / PEDESTRIAN BRIDGE



PANORAMA LOOKING WEST



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COLMAN TOWER | SITE CONTEXT — SITE PHOTOS

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SITE FROM NE CORNER [POST ALLEY]



SITE FROM NORTH



SITE FROM NW CORNER



POST AVE FROM NORTH

SUMMER SOLSTICE



9AM



12PM



6PM

WINTER SOLSTICE



9AM



12PM



3PM



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COLMAN TOWER | SHADOW STUDIES

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PANORAMA AT 150'



PANORAMA AT 85'