



888 WESTERN AVENUE

COLMAN RESIDENTIAL

EARLY DESIGN GUIDANCE

AUGUST 24, 2010

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888 WESTERN AVENUE

The Design / Development / Construction Team

Developer:	Goodman Real Estate
Contractor:	Turner Construction
Structural Engineer:	Magnusson Klemencic Associates
Civil Engineer:	Magnusson Klemencic Associates
Architect:	Weber Thompson
Landscape Architect:	Weber Thompson
Interior Designer:	Weber Thompson

The Site

The full block, effectively level site is located directly to the West of the Colman Ferry dock, bounded by Marion on the south, Columbia on the north, and Western Avenue on the east. The Colman Ferry Pedestrian Bridge flanks the site on the north property line. There are currently no structures on the property, but the site was home to a pier, and later a hotel and The Societe candy factory early in the twentieth century. Historical buildings flank the proposed project: The Colman Building to the east (across Post Avenue); The US Post Office building to the north; The Paulson building to the east; and the DJC building to the south. The Norton Building, Millenium Tower and The Exchange Building provide a “backdrop.”

Site Challenges

The site has a high water table, liquefaction-prone soils, and a deep bore tunnel for Highway 99 running under a portion of the site. The structure will require deep piles and grade beams. All of these challenges combined make it infeasible for sub-grade structure.

Vision Statement

888 Western Avenue will be a modern, sixteen story, curtain-wall-clad residential tower that seeks to be a good neighbor to its historical context. Elements of scale, a curved glass façade, and modulation break the mass into smaller elements and relate to datum of nearby structures. The project will add 208 units of affordable, price-point rental housing to the Commerce District, with an average unit square footage of only 739 sf. In the mix are a handful of “micro-apartments” that help to activate the above-grade parking (which will contain 142 parking stalls).

At grade and at the bridge level, this project will add 11,859 sf of quality, high-ceilinged retail that will help to activate all pedestrian frontages. Some of this retail fronts Post Avenue in an effort to activate and improve the pedestrian quality of what has the prospect of helping to create a continuation of “Post Alley”.

Over 9,000 sf of green roof area will be provided in this LEED Basic Certified project. The project contains 8,300 sf of recreation space, including a generous 3,150 sf public plaza which will be constructed at the northwest corner of the site — a location which is a gateway to First Avenue from the waterfront. The main entry to the building will be located in this plaza, affording activation and “eyes on the street”, in addition to creating a welcoming sense of arrival to the lobby entrance.

History of the Project / Prior Guidance

The project has been approved in two prior design submittals for office use. Those design schemes were victims of the economy and an over-built condition in commercial office. There is a projected short-fall for affordable price point residential housing in the urban center, and this project seeks to help fill that anticipated lack of supply in the city center. Prior guidance has been incorporated into the currently proposed design, including these nine specific improvements:

1. Create scale elements that relate to the datum reference of adjacent historical buildings.
2. Post Avenue not to be relegated to ‘back of house’ or loading docks and instead, retail/commercial space is included at Post Avenue.
3. Recognize the gateway quality of the northwest corner of the site and to provide an appropriate response with some form of open space.
4. Engage the pedestrian bridge and provide a stair that would encourage pedestrian connection to Post Avenue.
5. Incorporate green roofs into the project, if possible.
6. Activate above grade parking with active uses.
7. Setting back the structure from the pedestrian bridge, in an effort to bring light and air to the on-grade structure.
8. Curve the western façade to minimize the glare reflecting from the bay and late afternoon sun.
9. Create an active, and landscaped, rooftop sky deck that will accommodate zoning requirements for recreation space and will also provide a “fifth elevation” for those looking down on the building.





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COLMAN RESIDENTIAL | AERIAL PHOTO

08.24.10

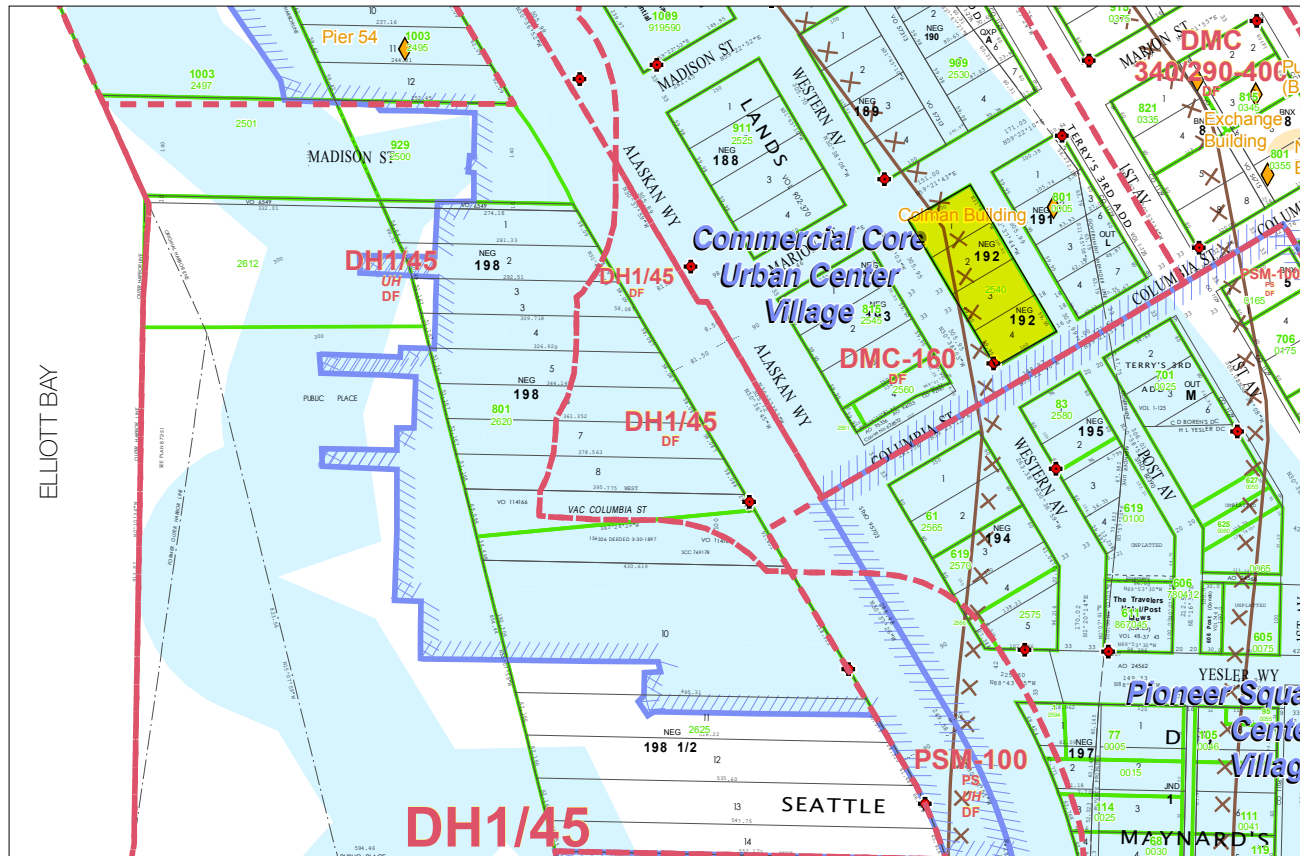
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Zoning & Overlays: Downtown Mixed Commercial DMC 160 Zone.

- The site is within the Commercial Core Urban Center Village.

Allowable Height Limit: 23.49.008.

- The base and maximum height limit is 160'.

Floor Area Ratio (F.A.R.): N/A in Residential Uses

- Base FAR = 5; Maximum FAR = 7 (with floor area bonuses)

Common Recreation Area: 23.49.10.B

- Common Recreation Area is required in an area equivalent to five (5) percent of the total gross floor area in residential use. In no instance shall the amount of required common recreation area exceed the area of the lot. The common recreation area shall be available to all residents and may be provided at or above ground level.
- A maximum of fifty (50) percent of the common recreation area may be enclosed.
- Common recreation area that is provided as open space at street level shall be counted as twice the actual area in determining the amount provided to meet the common recreation area requirement.
- For lots abutting designated green streets, up to fifty (50) percent of the common recreation area requirement may be met by contributing to the development of a green street.
- Continuous street-front overhead weather protection is required unless the building is: set back more than 5', abuts an open space, has a 2' landscaped area, or is at driveways. It must be <8' from the wall or 2' from the curb, and 10' to 15' above the sidewalk.

Parking Requirements: 23.49.019

- No parking is required on this site. Maximum parking = 1 stall per 1,000 sf of non-residential use.

STREET TYPES

Sidewalk & Alley Widths: 23.49.022 MAP 1B, 1F

- Western: Class II Pedestrian Street, Minor Arterial
Map 1C requires a 12' wide sidewalk, so required sidewalk widening into the property may result in a 2' dedication.
- Marion: Green Street
Map 1C requires a "variable" sidewalk width, so the required sidewalk width should be obtained.
- Post Avenue
Map 1C requires a 12' sidewalk width
SDOT will need to verify requirements
- Columbia Street: Class II Pedestrian Street, Principal Arterial
Map 1C requires a 12' sidewalk width

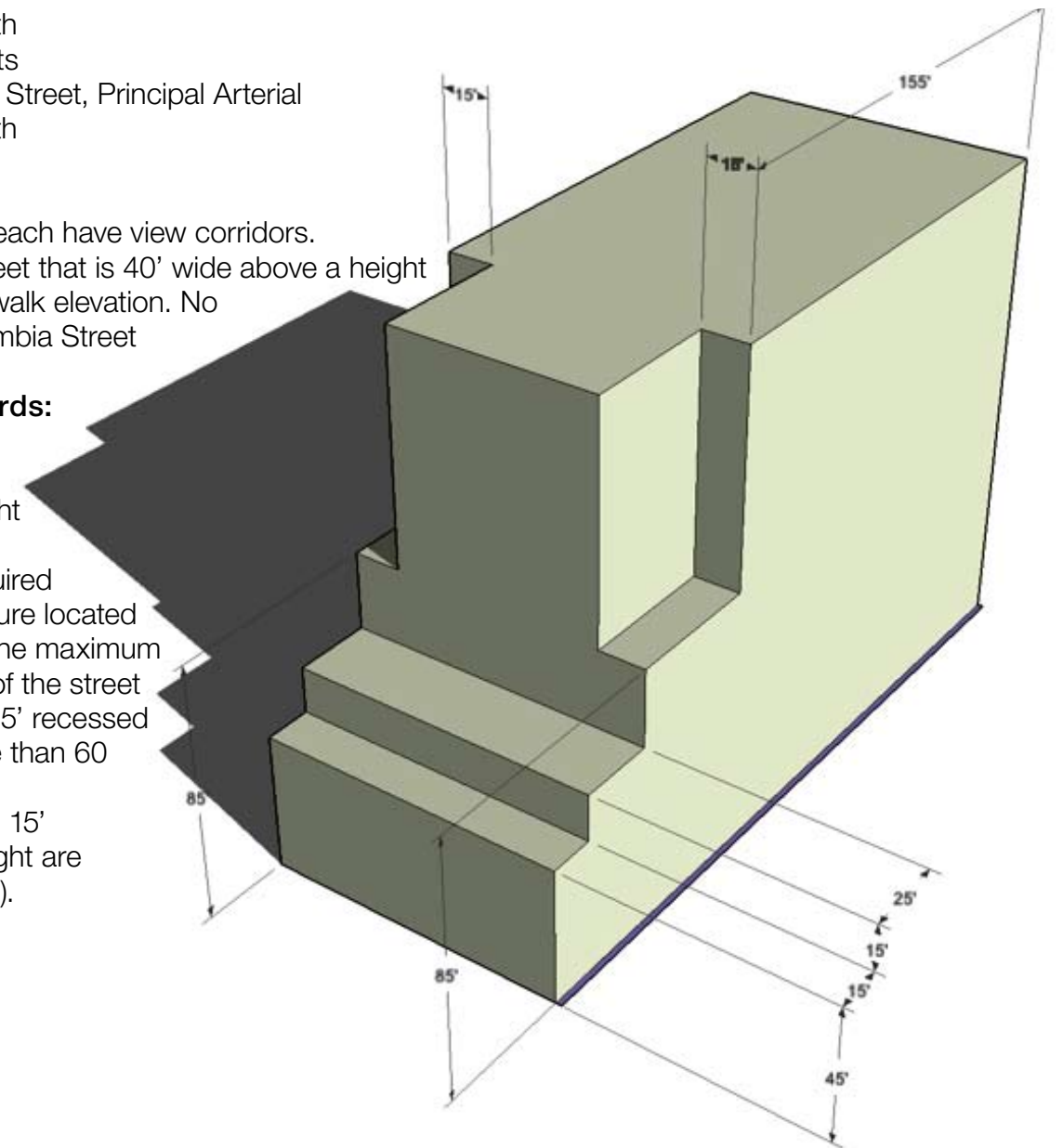
View Corridors: 23.49.024

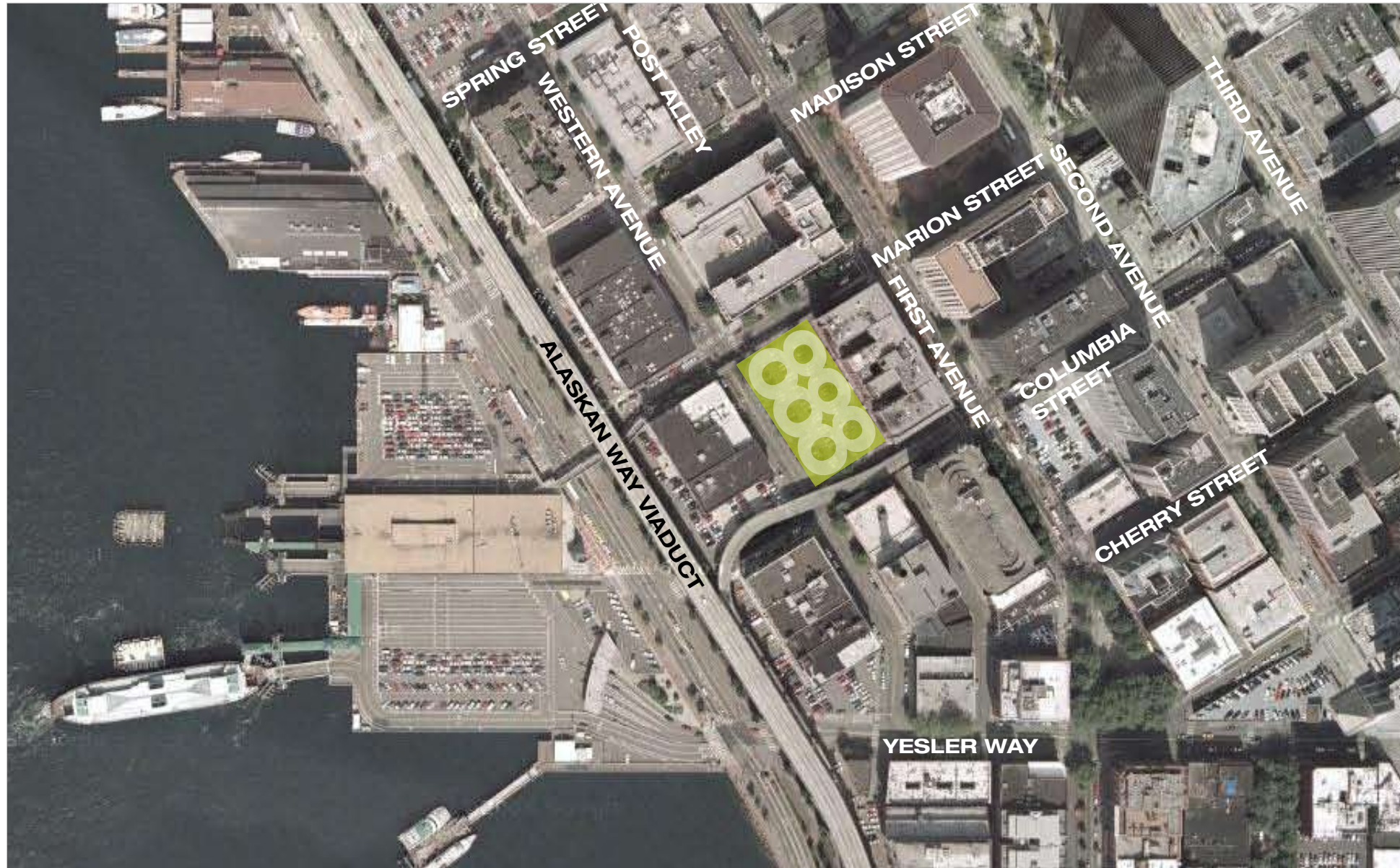
- Marion Street and Columbia Street each have view corridors.
- A setback is required on Marion Street that is 40' wide above a height of 60' above the Marion Street sidewalk elevation. No setback requirement exists on Columbia Street

Upper Level Development standards:

23.49.058

- 100% coverage is allowed on the developable lot area up to 160' height except for:
- Part B — Facade Modulation is required above 85' for any portion of a structure located within 15' of a street property line. The maximum length of a facade that is within 15' of the street property line is 155 linear feet. The 15' recessed facade length is required to be more than 60 linear feet.
- Part F — Upper Level Setbacks of a 15' continuous setback above a 45' height are required along green streets (Marion).





Site Development Area:

- This full-block is platted at 240'x100'

Residential Units for the Young, Urban Professional:

- 208 units of apartments
- 10 floors of residential
- Average unit size is 739 sf
- 183,329 sf of total residential

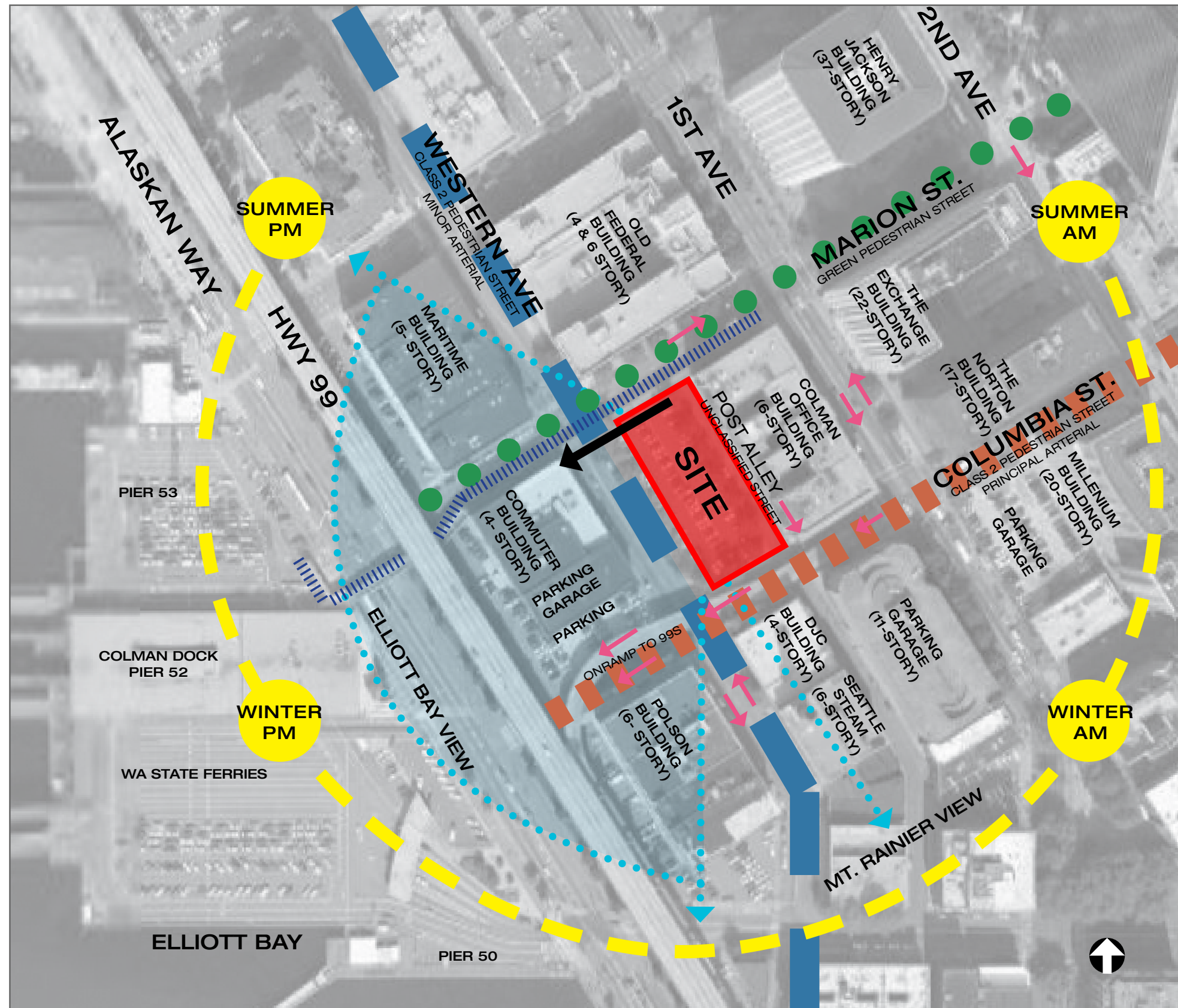
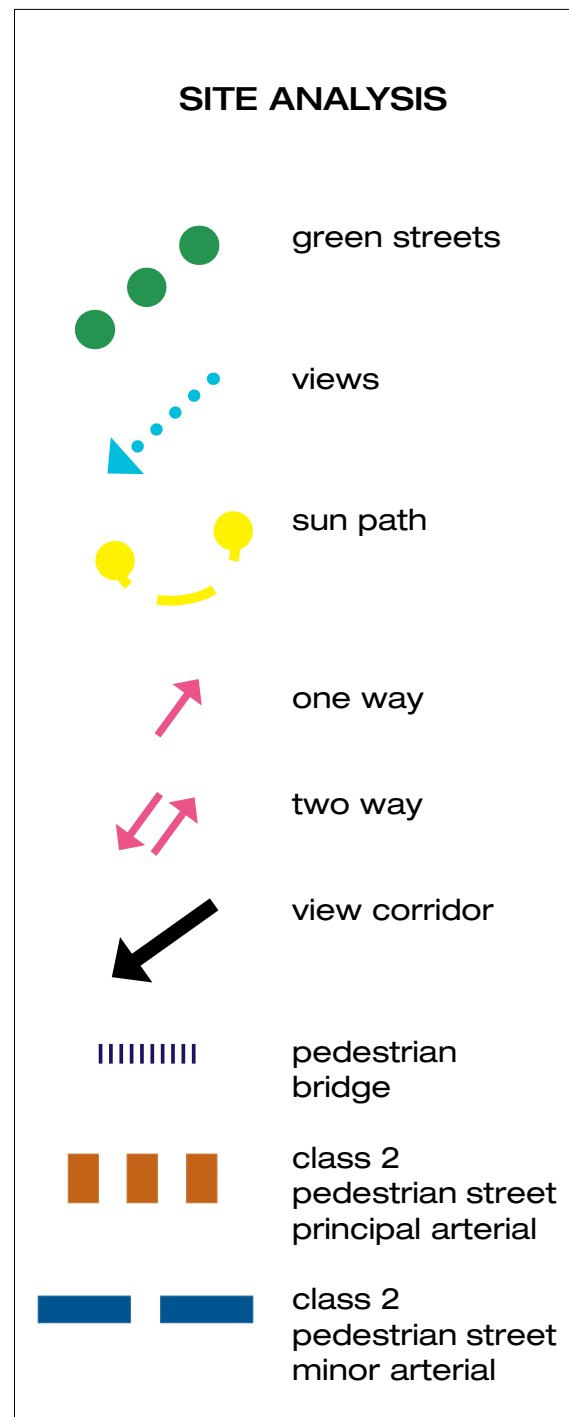
Retail Uses:

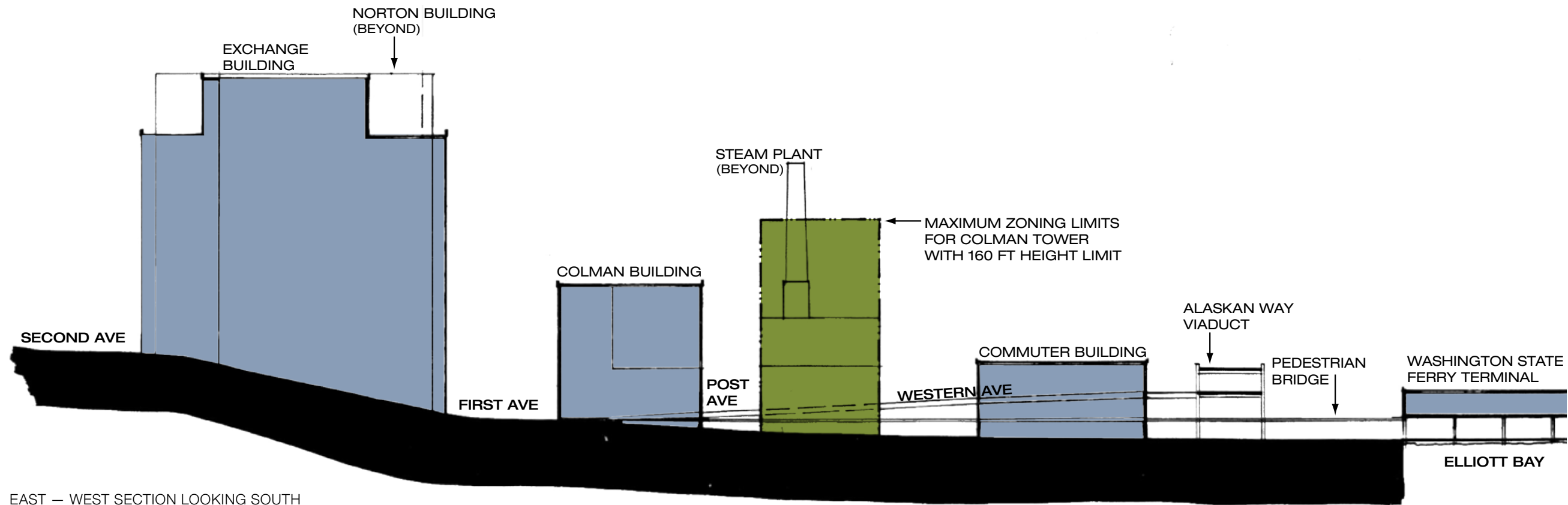
- 10,750 sf on the first floor
- 1,109 sf on the second floor

Parking:

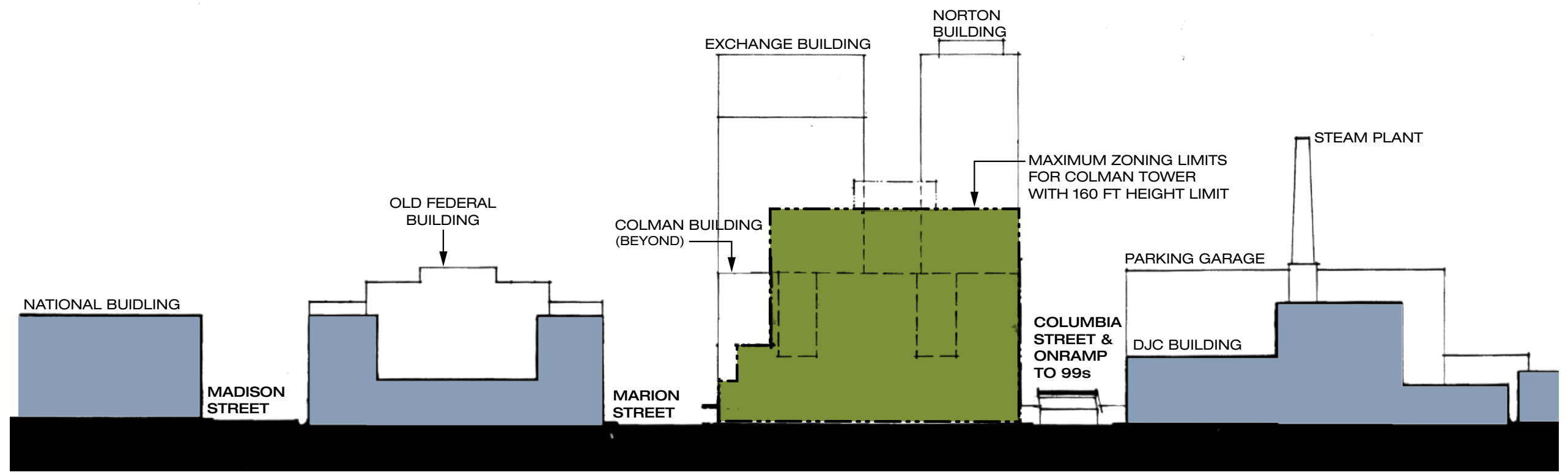
- Approx. 142 parking stalls
- 3-1/2 floors of above grade parking







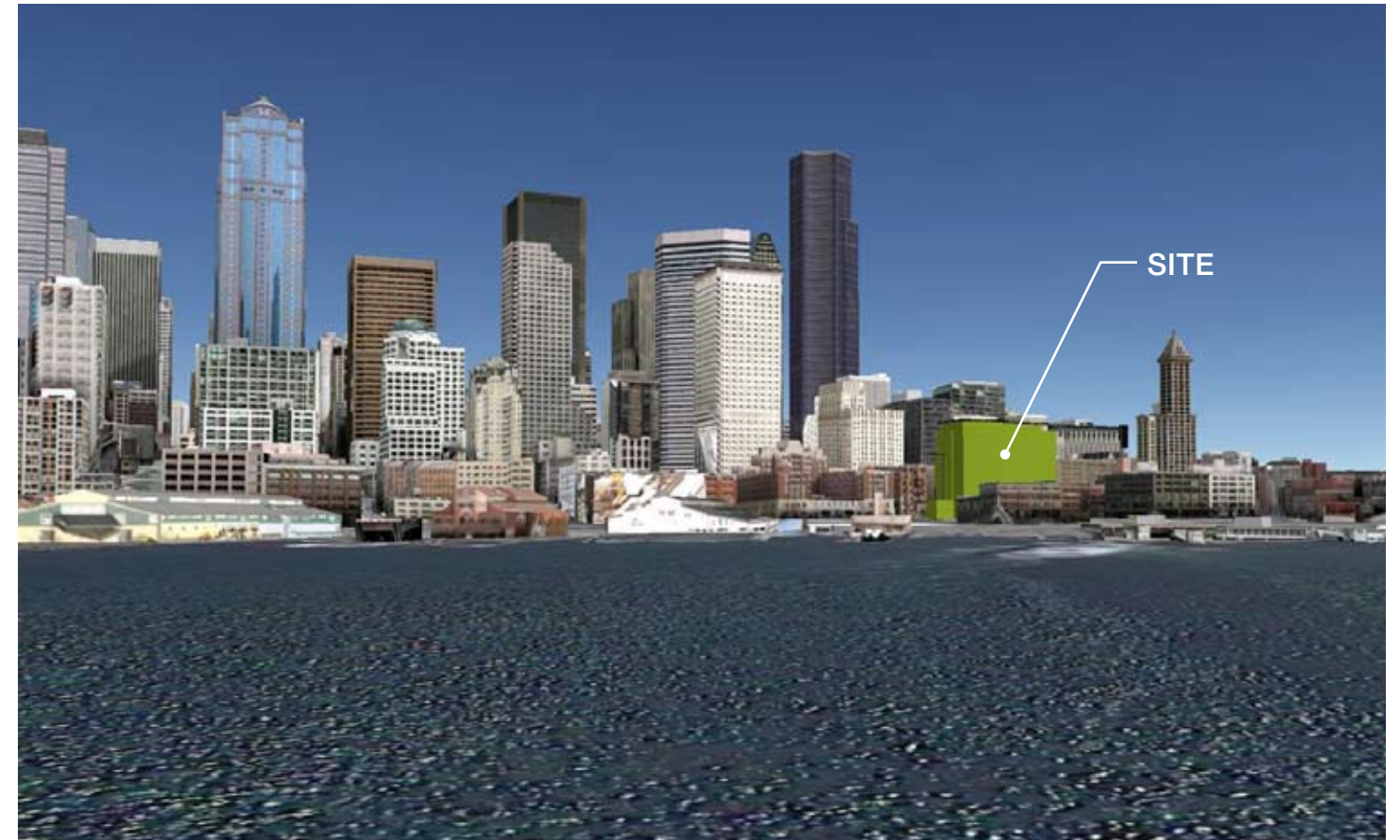
EAST – WEST SECTION LOOKING SOUTH



NORTH – SOUTH SECTION LOOKING EAST



VIEW LOOKING NORTH

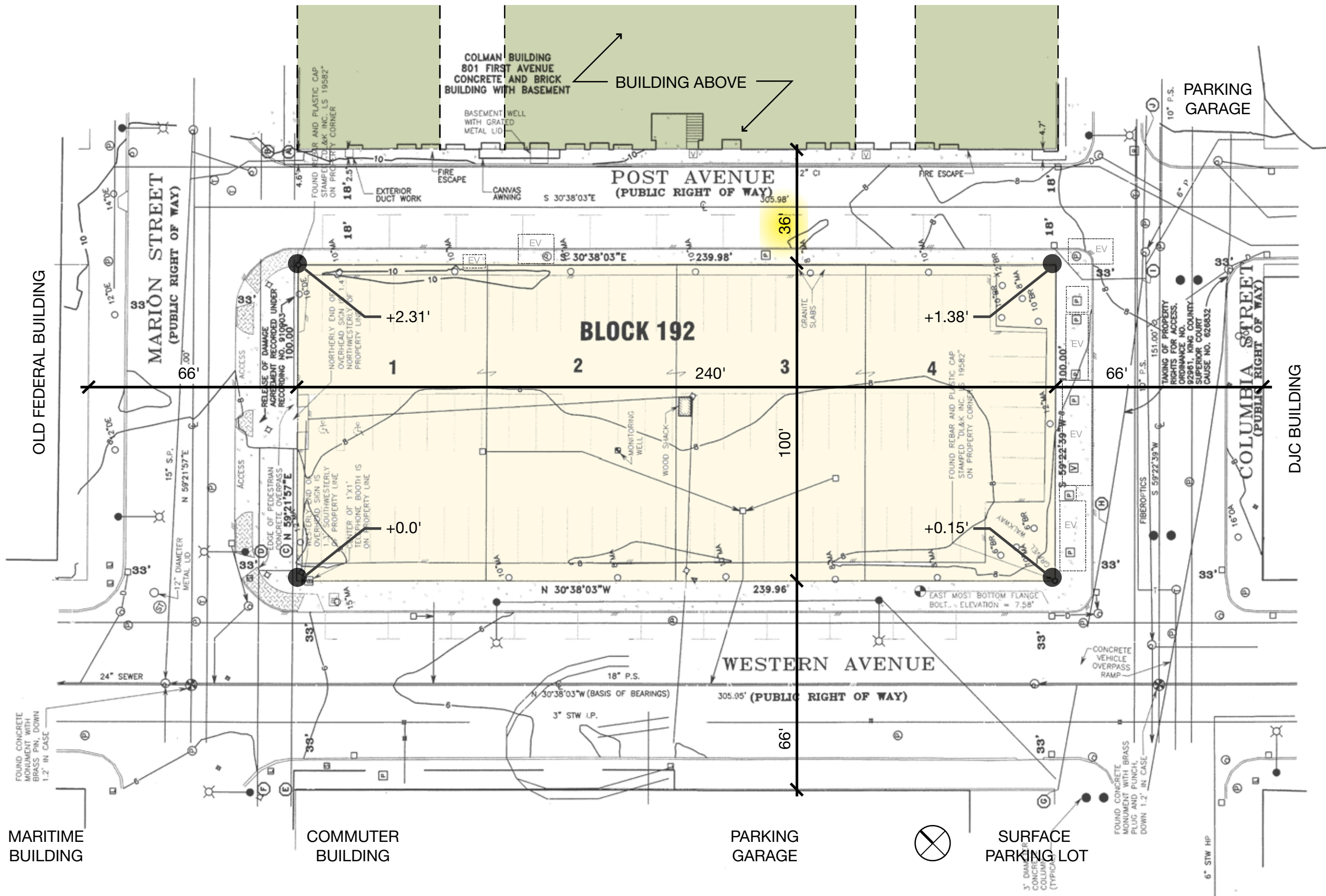


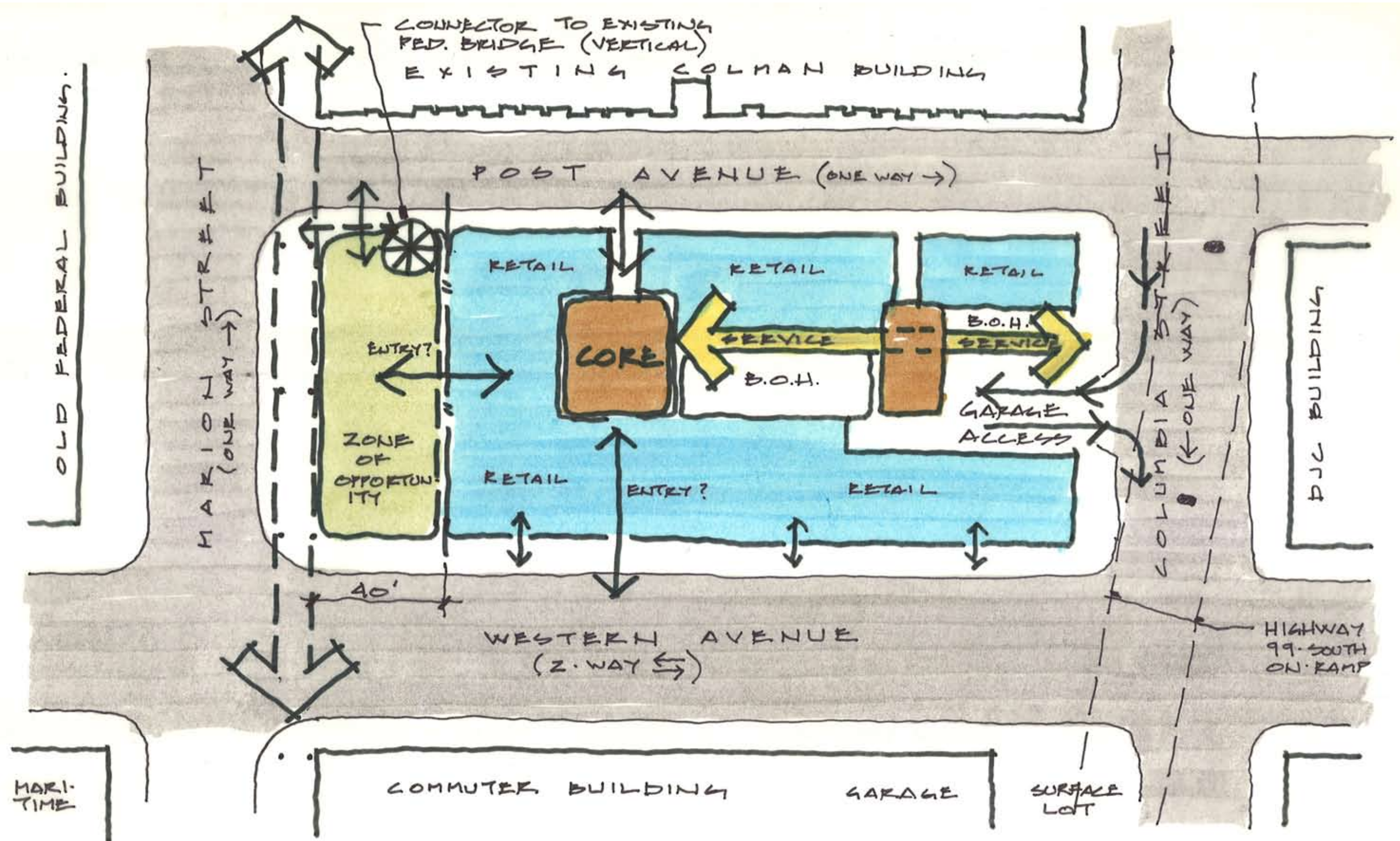
VIEW FROM FERRY



VIEW LOOKING SOUTH









VIEW FROM FERRY TERMINAL



MARION STREET FROM WEST



GARAGE ACCESS ON WESTERN



WESTERN AVENUE FROM NORTH



MARITIME BUILDING



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COLMAN RESIDENTIAL | SITE CONTEXT — CONTEXT PHOTOS

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COLUMBIA FROM EAST



MARION FROM EAST



MARION STREET FROM WEST



OLD FEDERAL BUILDING ON WESTERN



PEDESTRIAN BRIDGE ON MARION



COMMUTER BUILDING ACROSS WESTERN



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SITE FROM SW CORNER



SITE FROM SOUTH / ABOVE POST



POST AVE FROM NORTH



POST AVE FROM SOUTH

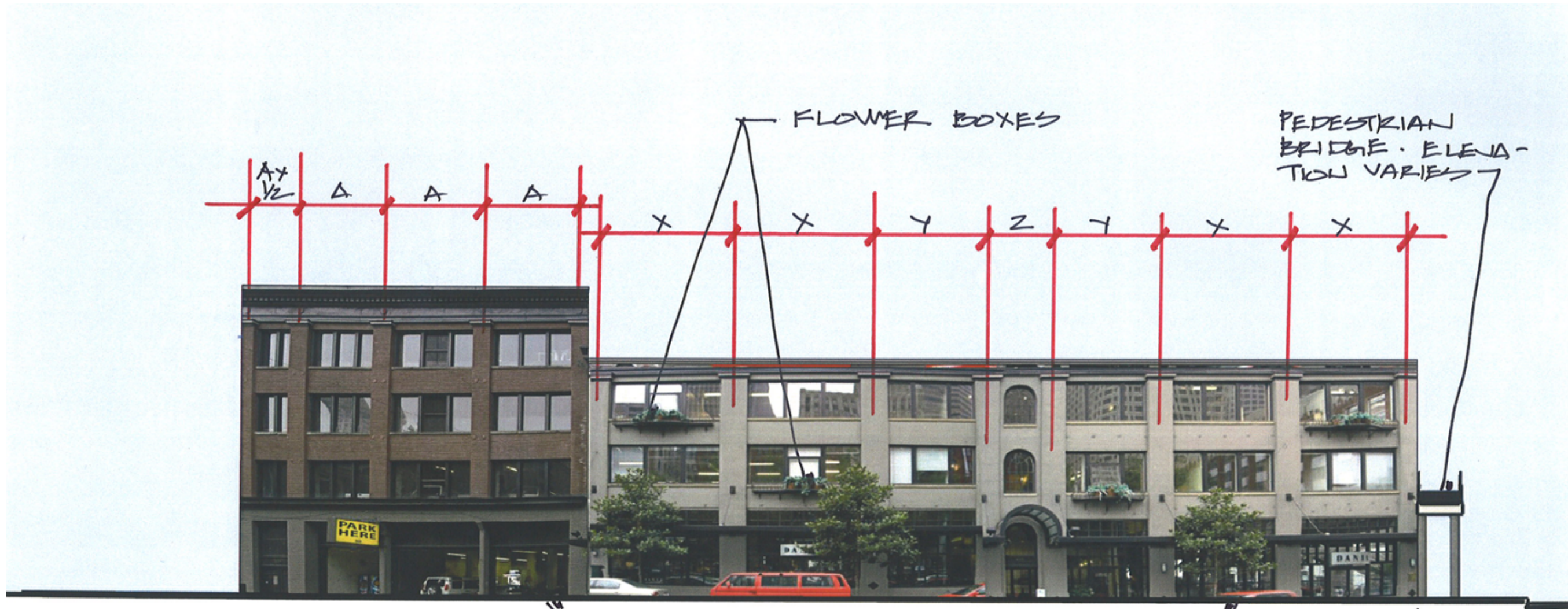


SITE FROM NORTHEAST / PEDESTRIAN BRIDGE



PANORAMA LOOKING WEST





SURFACE LOT

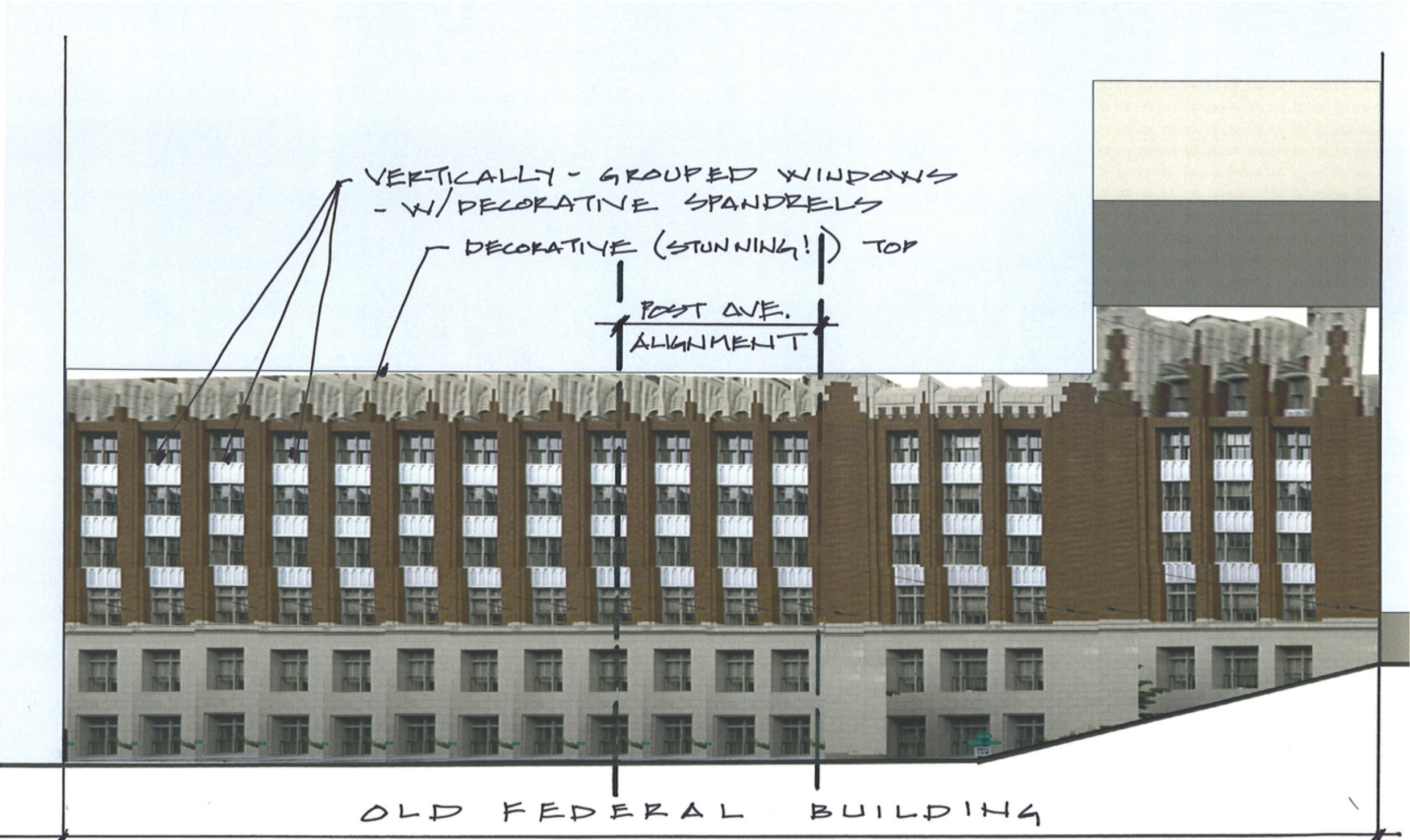
GARAGE

- BRICK FACADE:
 - PUNCHED WINDOWS
 - PRIMARY/EXPRESSED VERTICAL PILASTERS
 - SECONDARY BRICK BEAMS/SPANDRELS
- EXPRESSED CORNICE W/DENTALS
- APPROX. 50' TALL

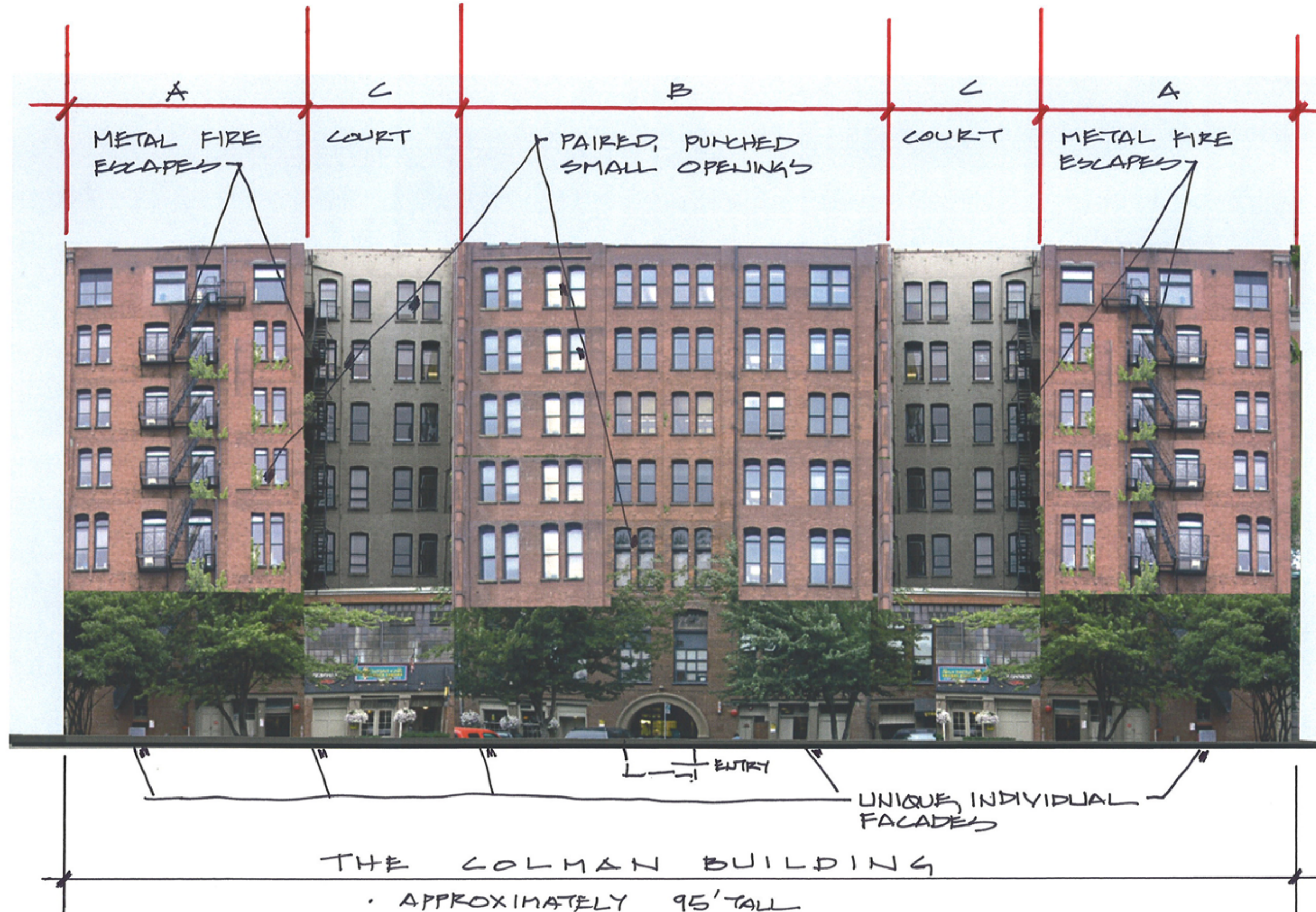
COMMUTER BUILDING

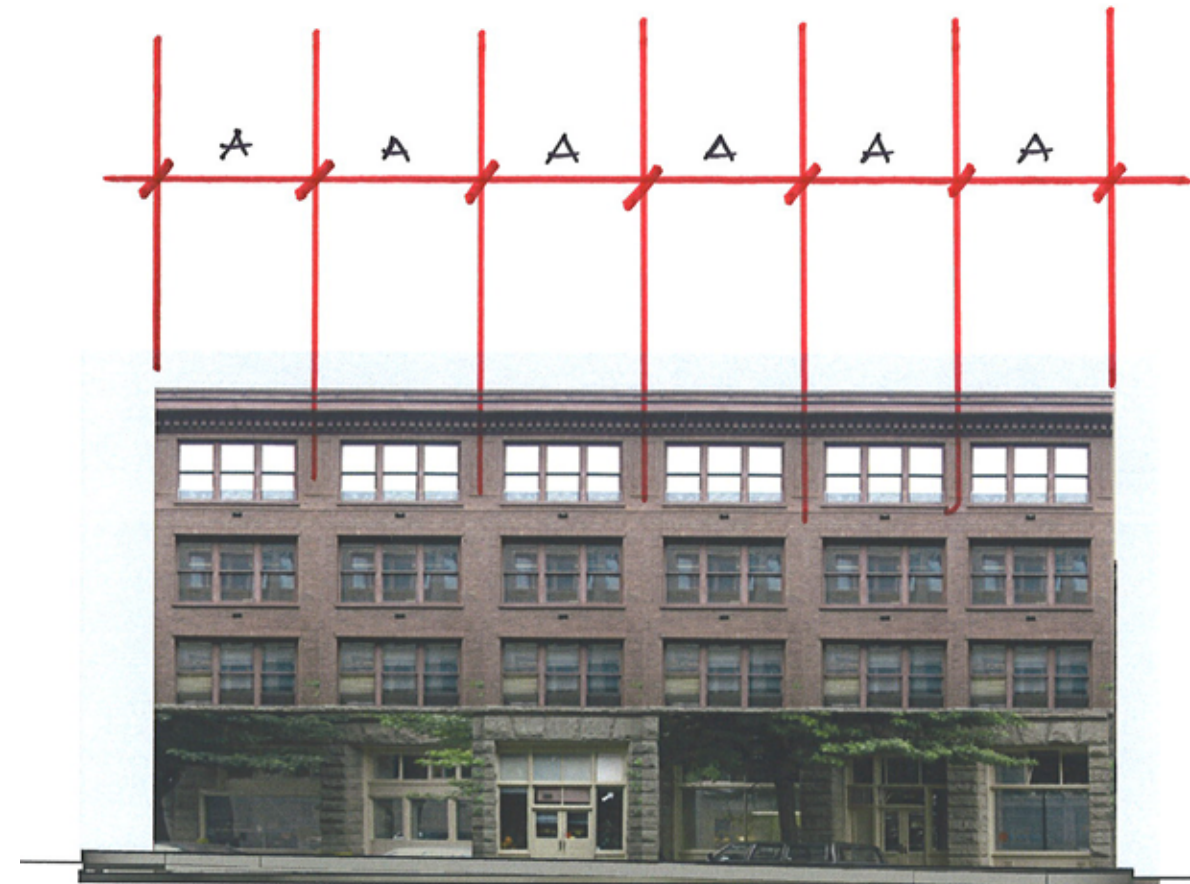
- CONCRETE FACADE, PAINTED:
 - HORIZONTAL EXPRESSION OF WINDOWS
 - PRIMARY/EXPRESSED VERTICAL PILASTERS
 - UNIQUE FACADE EXPRESSION FOR ENTRY TO UPPER FLOORS
 - MORE SUBTLE CORNICE (THAN NEIGHBORS)
- APPROX. 40' TALL

HIGH-BAY GROUND FLOOR OPENINGS



- HEAVY MASONRY BASE W/ PUNCHED, SQUARE OPENINGS
- BRICK PRIMARY FACADE MATERIAL W/ GROUPED WINDOWS
- VERY REGULAR, SHORT WINDOW BAY SPACING
- APPROX. 74'-TALL

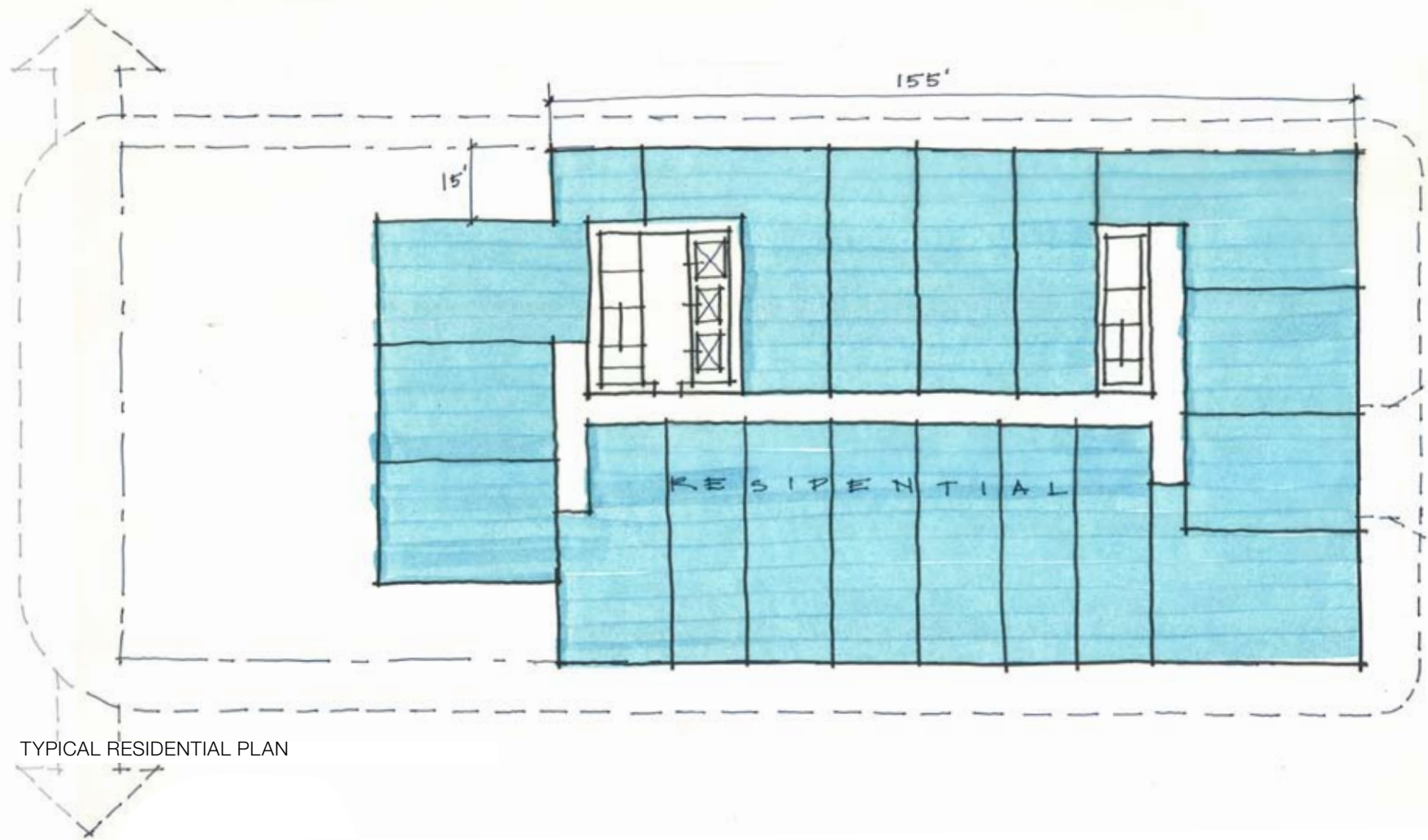




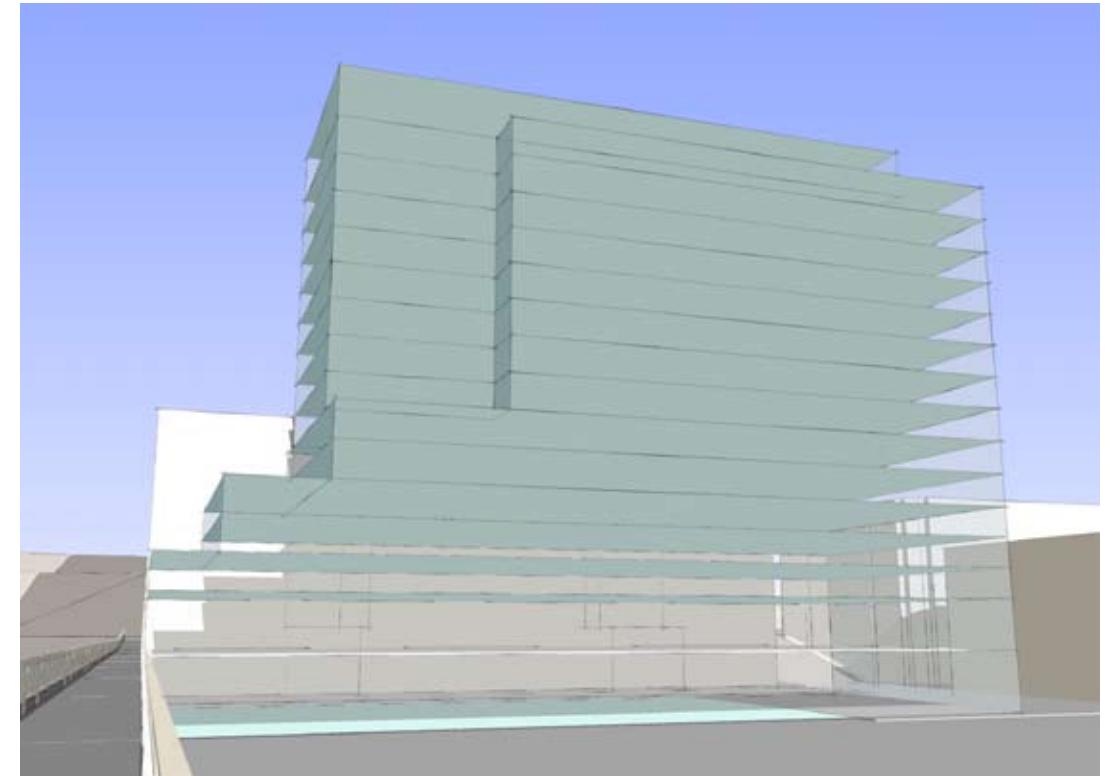
DJC BUILDING

- HORIZONTALLY EXPRESSED WINDOWS
- PRIMARILY A BRICK FACADE
- STONE / GLASS @ GROUND FLOOR
- FOUR STORIES
- EXPRESSED CORNICE
- APPROXIMATELY 55' TALL





TYPICAL RESIDENTIAL PLAN



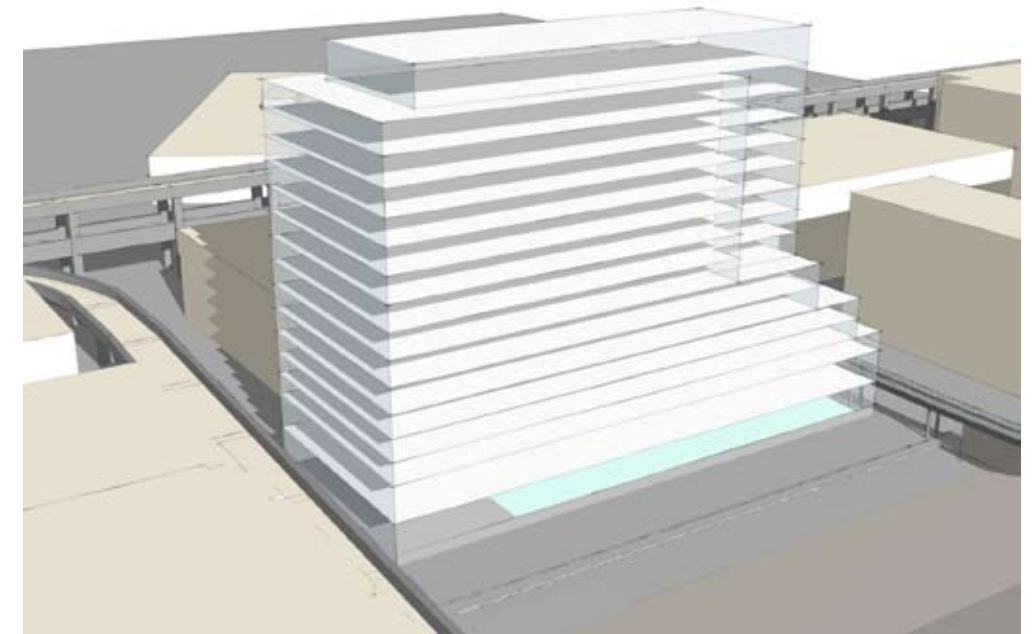
VIEW EAST FROM PEDESTRIAN BRIDGE



VIEW FROM NORTHWEST



VIEW FROM SOUTHWEST



VIEW FROM SOUTHEAST



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COLMAN RESIDENTIAL | MASSING OPTION ONE (ZONING ENVELOPE)

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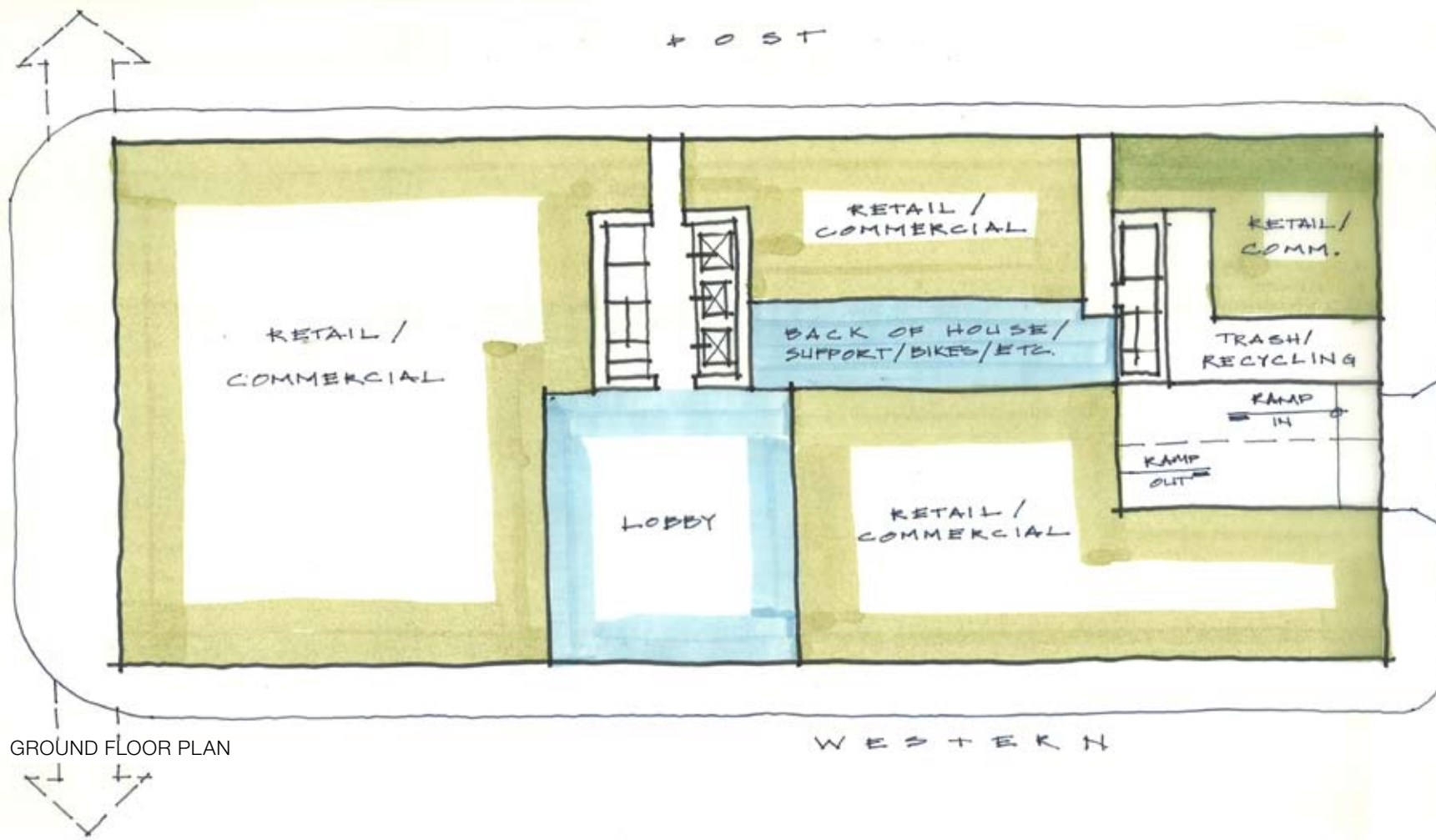
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GROUND FLOOR PLAN

Pros:

- Allows for more square footage because the design uses the maximum volume of view corridor area.

Cons:

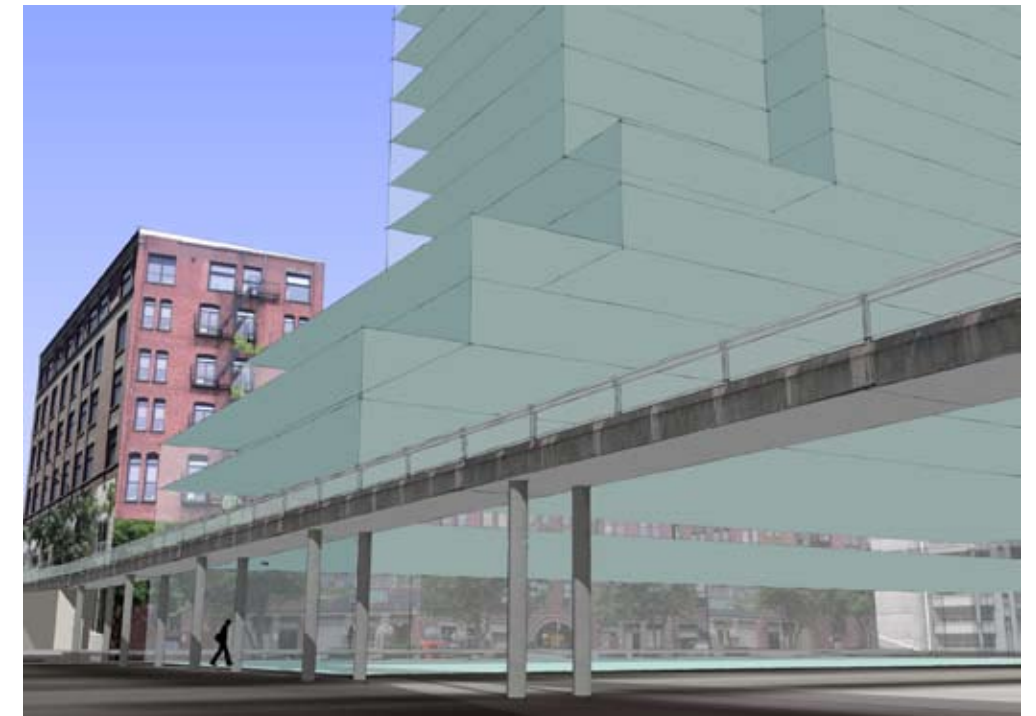
- Truncates the upper level development of residential floor plates.
- Does not necessarily slenderize the building.
- Does not provide relief for the historical Colman Building across Post Avenue which is only 36' wide.



VIEW ON WESTERN FROM NORTH



VIEW ON POST FROM SOUTH



VIEW ON MARION FROM WEST



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COLMAN RESIDENTIAL | MASSING OPTION ONE (ZONING ENVELOPE)

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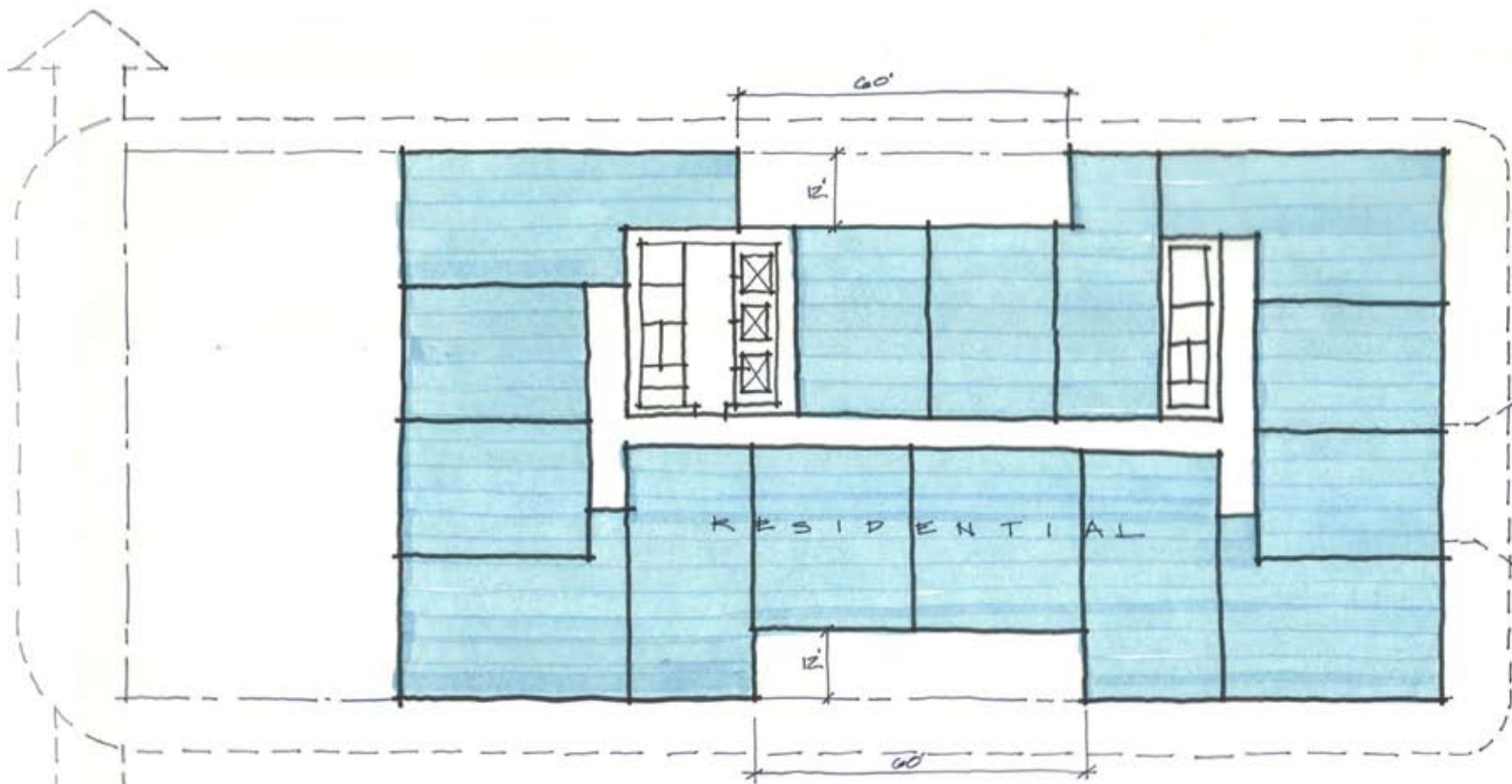
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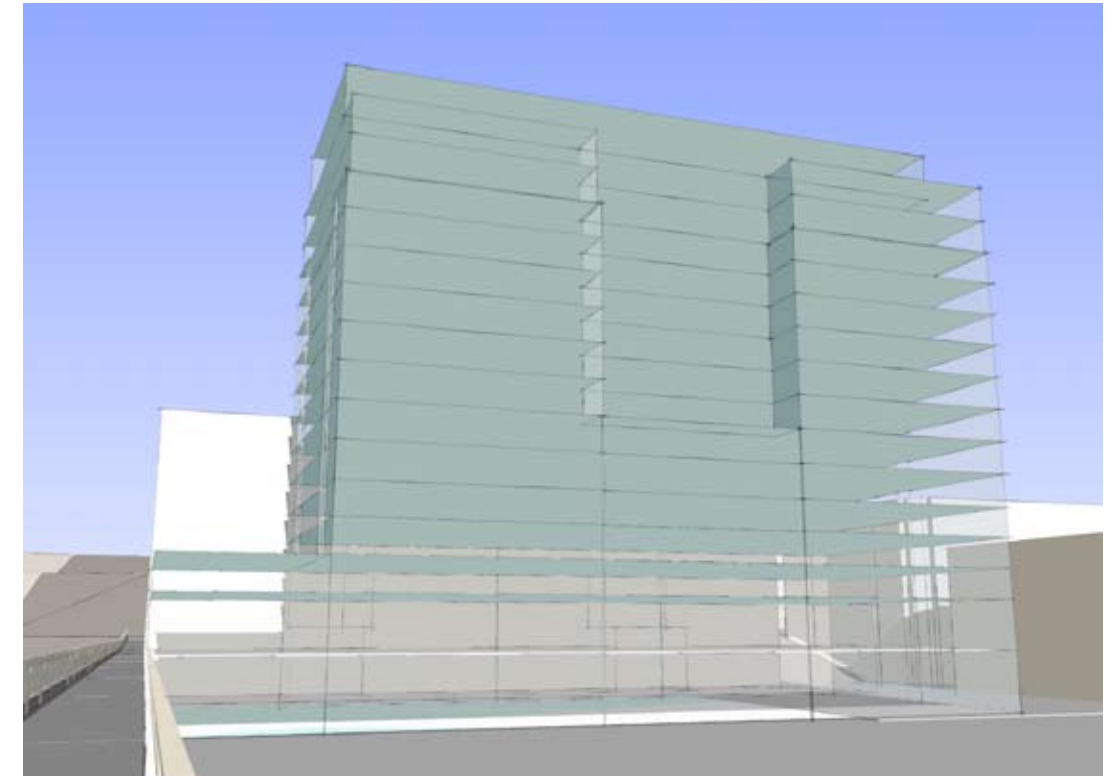
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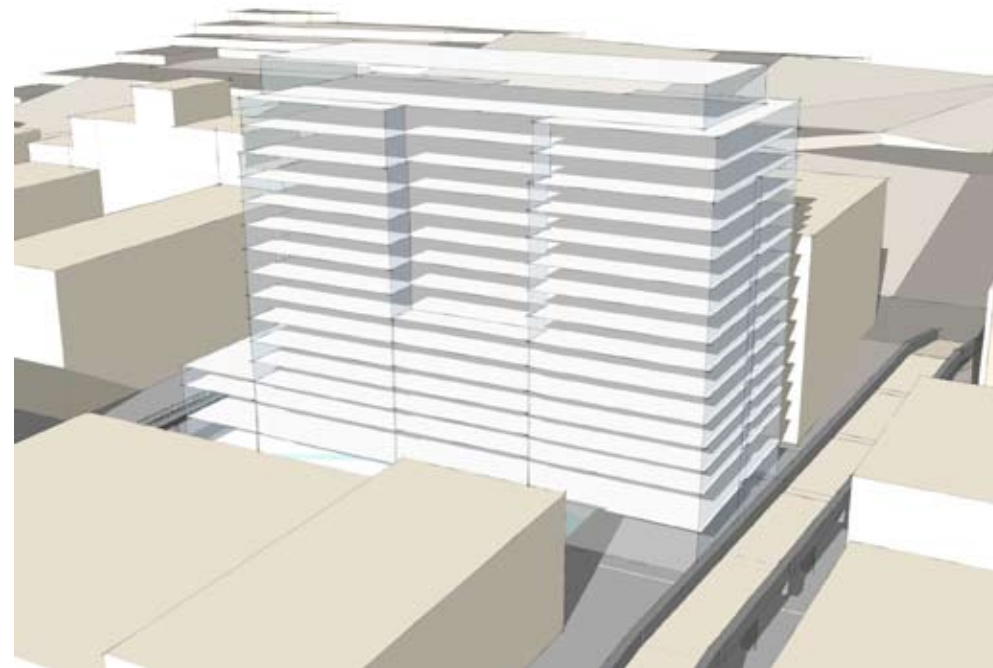
TYPICAL RESIDENTIAL PLAN



VIEW EAST FROM PEDESTRIAN BRIDGE



VIEW FROM NORTHWEST



VIEW FROM SOUTHWEST



VIEW FROM SOUTHEAST



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COLMAN RESIDENTIAL | MASSING OPTION TWO

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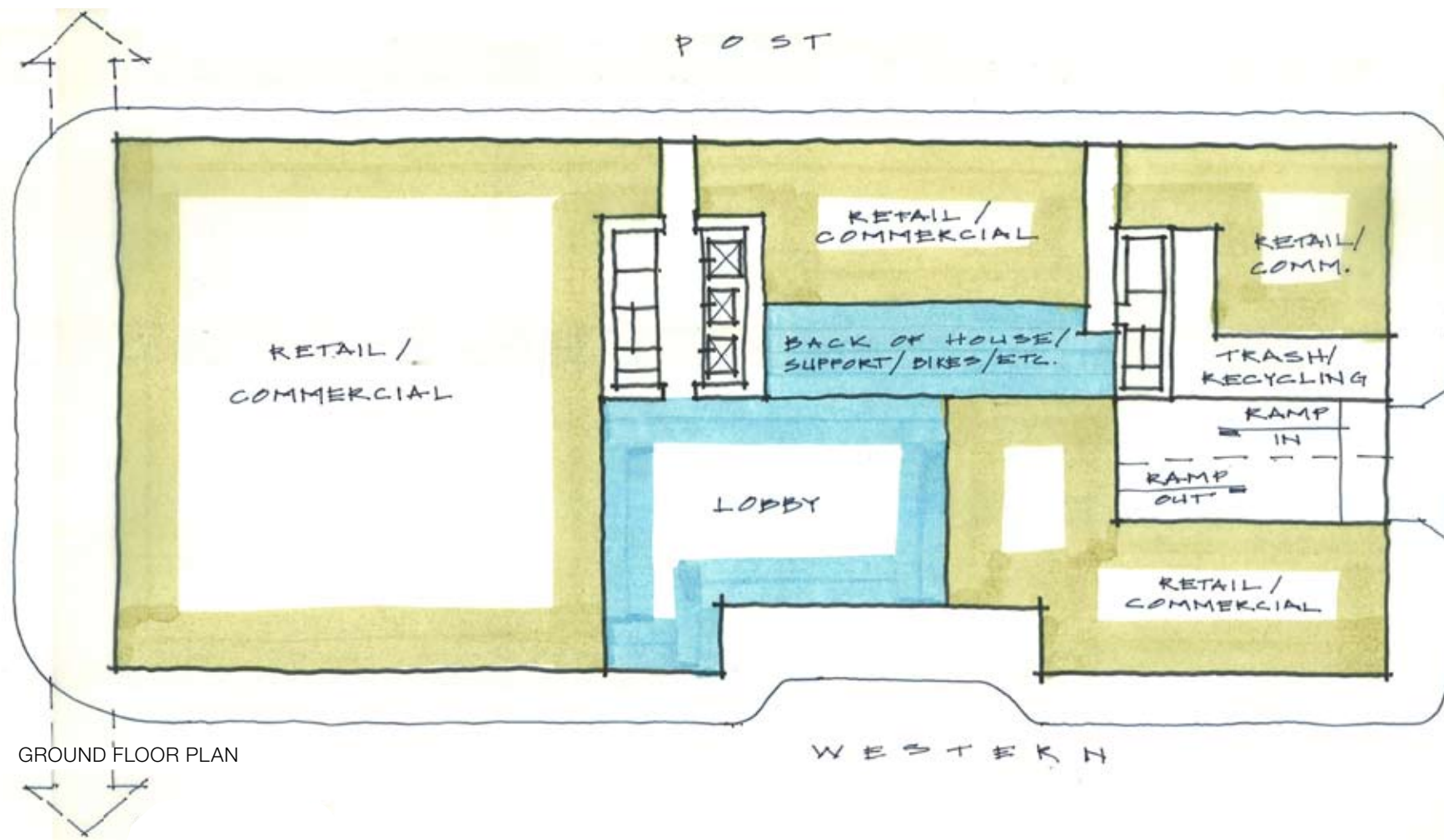
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GROUND FLOOR PLAN

Pros:

- Nearly code compliant, but center units don't work due to inadequate depth.

Cons:

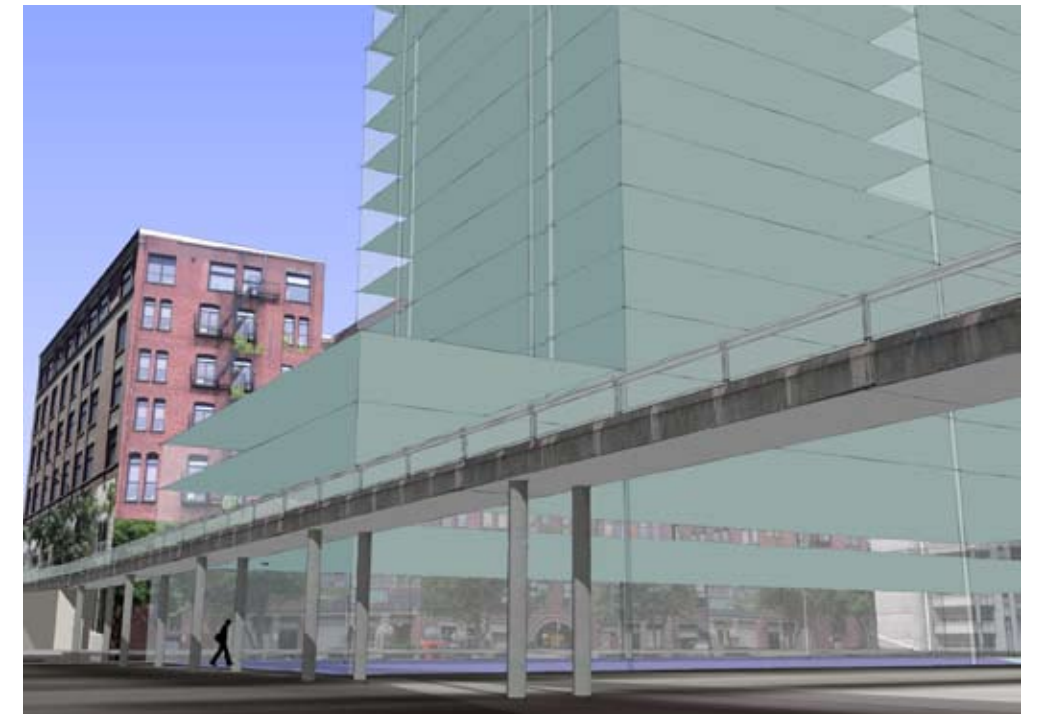
- Creates a dysfunctional floor plan.
- Inadequate relief for the historical Colman Building.
- Does not hold "street edge."



VIEW ON WESTERN FROM NORTH



VIEW ON POST FROM SOUTH



VIEW ON MARION FROM WEST



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COLMAN RESIDENTIAL | MASSING OPTION TWO

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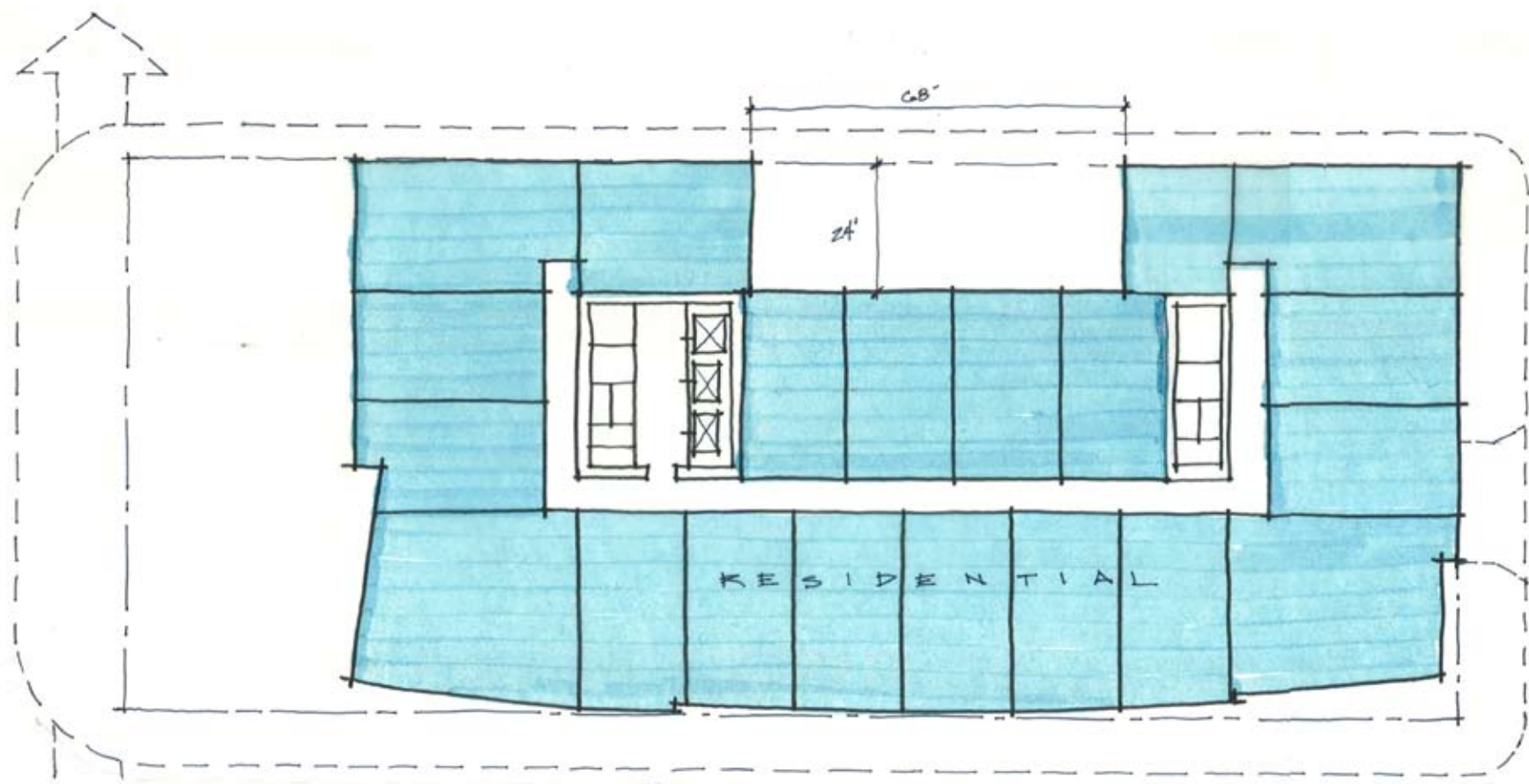
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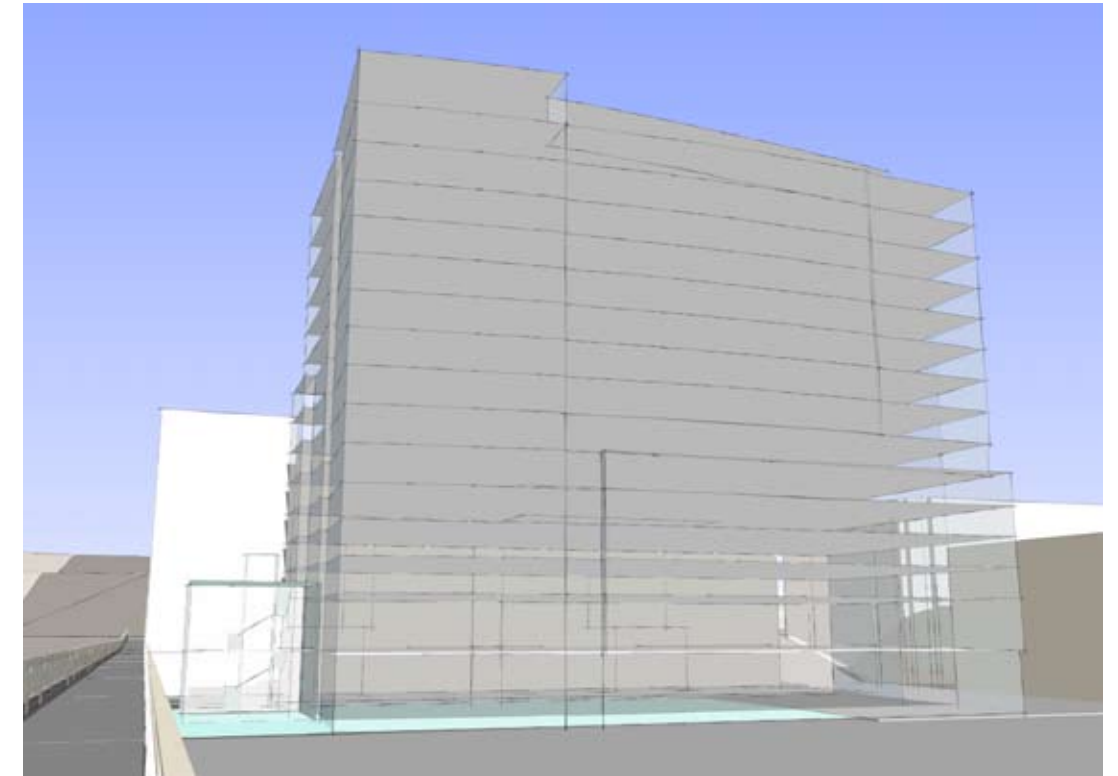
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TYPICAL RESIDENTIAL PLAN



VIEW EAST FROM PEDESTRIAN BRIDGE



VIEW FROM NORTHWEST

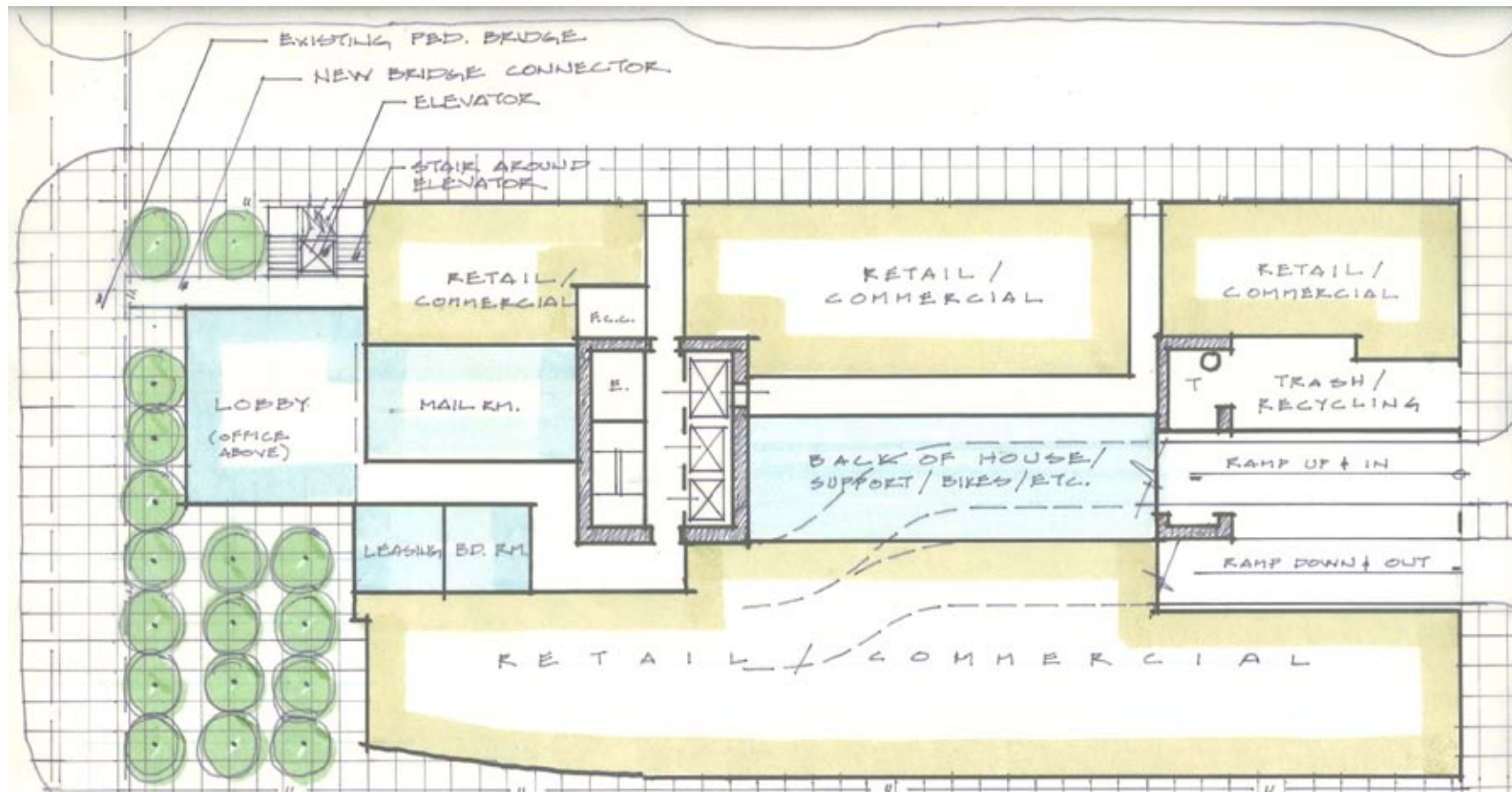


VIEW FROM SOUTHWEST



VIEW FROM SOUTHEAST





GROUND FLOOR PLAN

Pros:

- Provides adequate relief for the historical Colman Building and for Post Avenue.
- Curved façade reduces perceived bulk and mitigates glare.
- Scale elements relate to the datum of the historical district.
- Plaza at northwest corner provides public benefit and addresses gateway condition.

Cons:

- None perceived.



VIEW ON WESTERN FROM NORTH



VIEW ON POST FROM SOUTH



VIEW ON MARION FROM WEST



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COLMAN RESIDENTIAL | MASSING OPTION THREE (PREFERRED)

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VIEW FROM NORTHWEST (PLAZA AT MARION / WESTERN INTERSECTION)



VIEW FROM NORTHEAST (MARION / POST INTERSECTION)



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COLMAN RESIDENTIAL | CUBE DESIGN OPTION A

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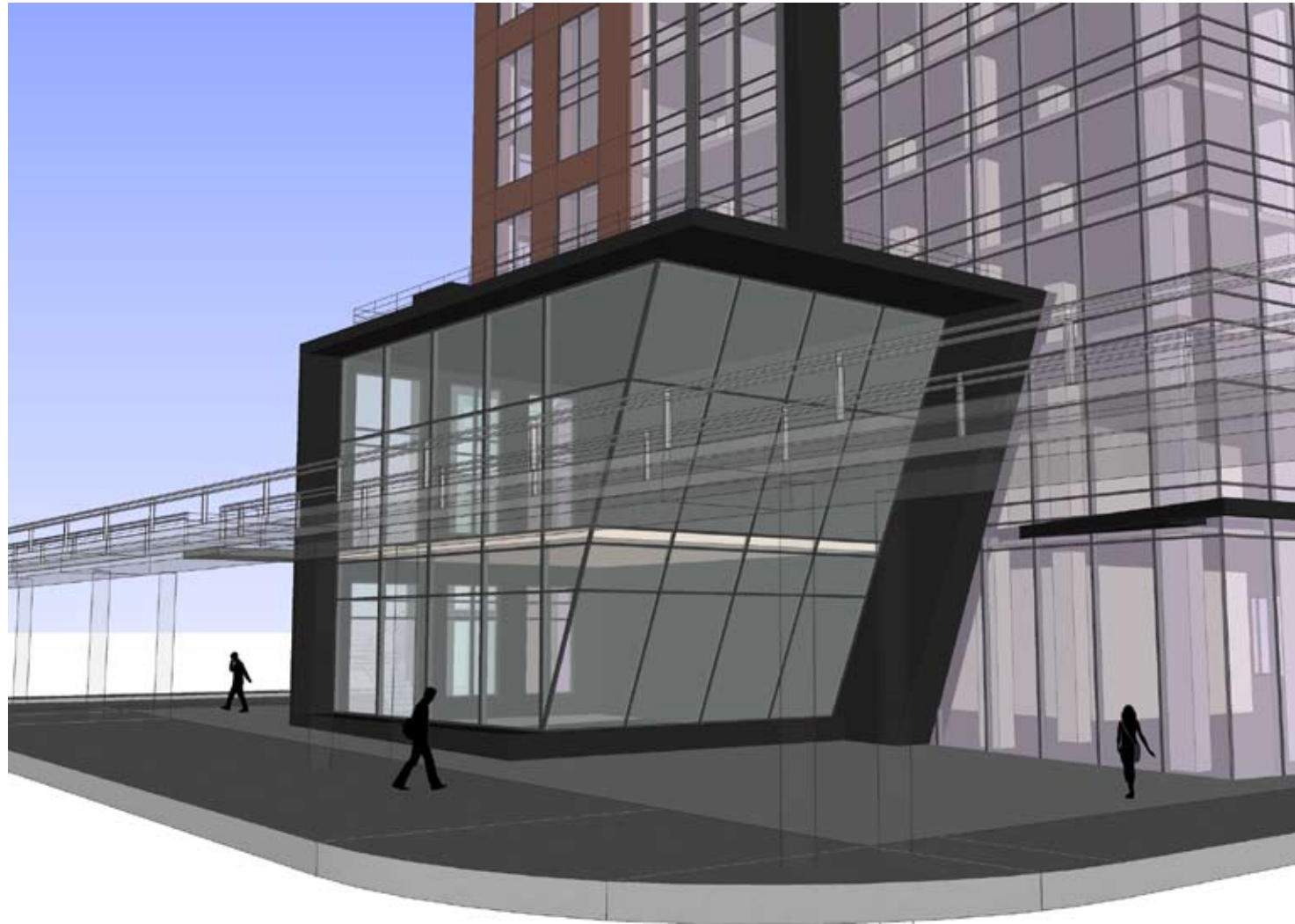
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VIEW FROM NORTHWEST (PLAZA AT MARION / WESTERN INTERSECTION)



VIEW FROM NORTHEAST (MARION / POST INTERSECTION)



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VIEW FROM NORTHWEST



VIEW FROM NORTHEAST

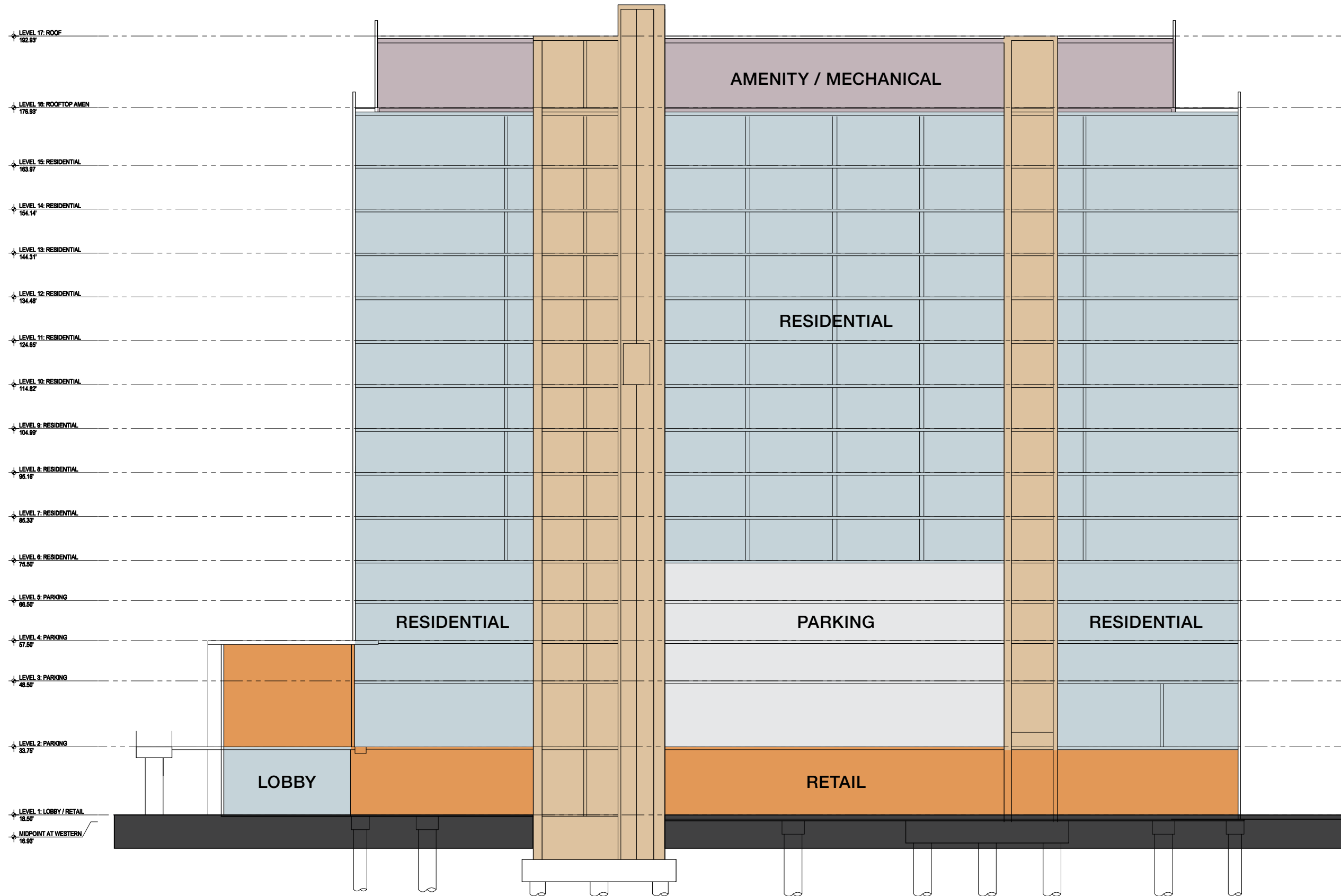


VIEW FROM SOUTHWEST



VIEW FROM SOUTHEAST





KEY

- Residential
- Retail
- Amenity
- Circulation
- Storage
- BOH
- Parking
- Mechanical

EXISTING COLMAN BUILDING

POST AVENUE

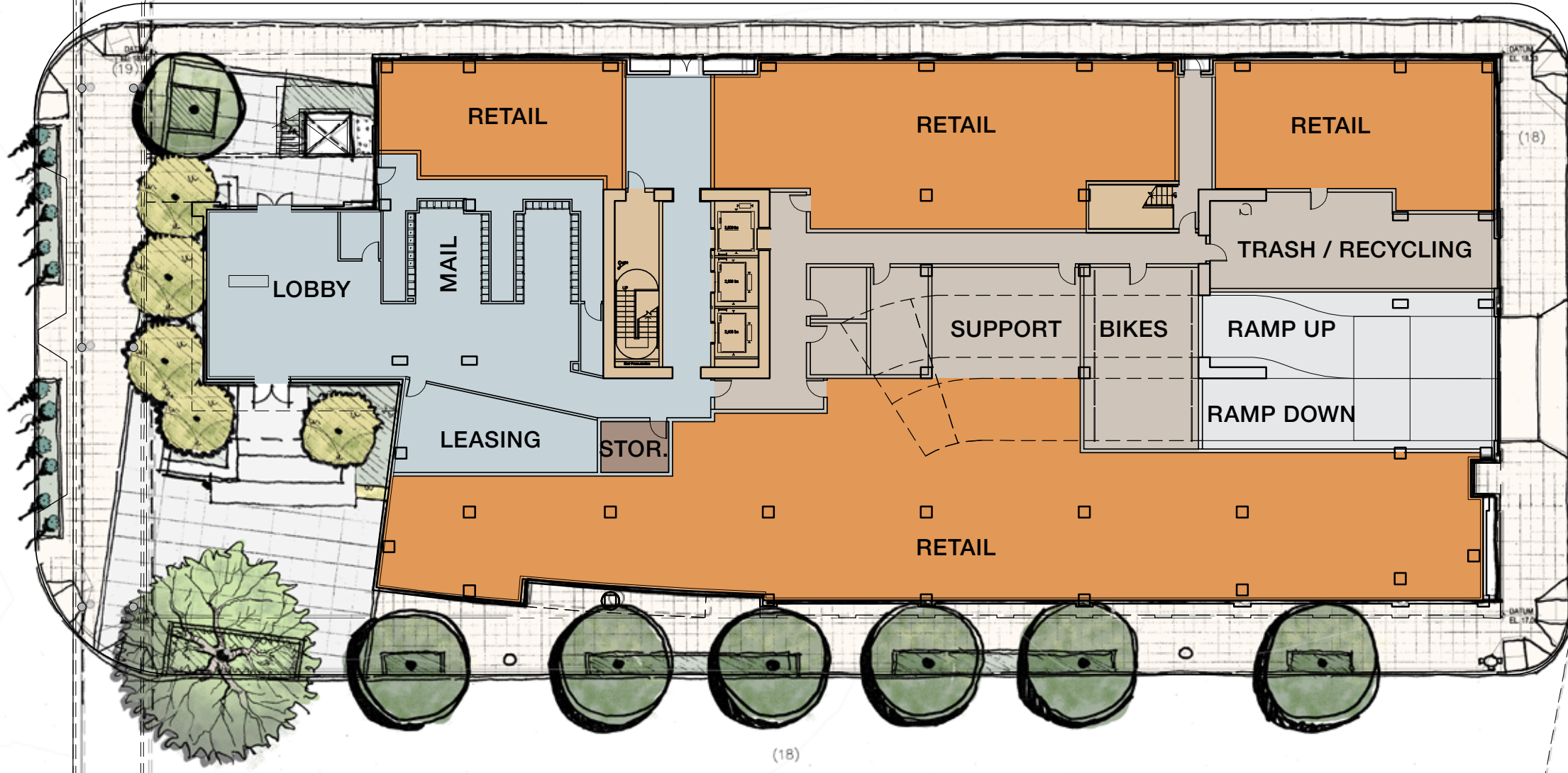
MARION STREET

COLUMBIA STREET

WESTERN AVENUE

KEY

- Residential
- Retail
- Amenity
- Circulation
- Storage
- BOH
- Parking
- Mechanical



GOODMAN REAL ESTATE
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COLMAN RESIDENTIAL | GROUND FLOOR PLAN

08.24.10

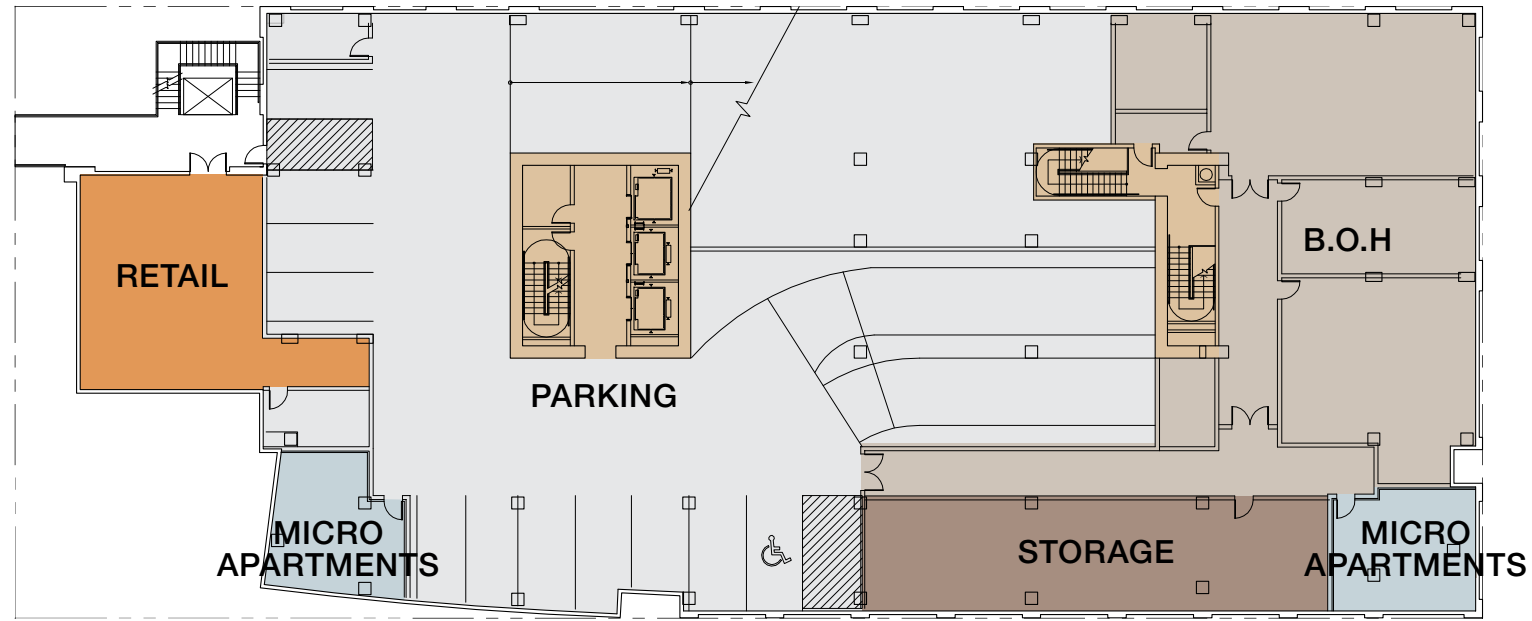
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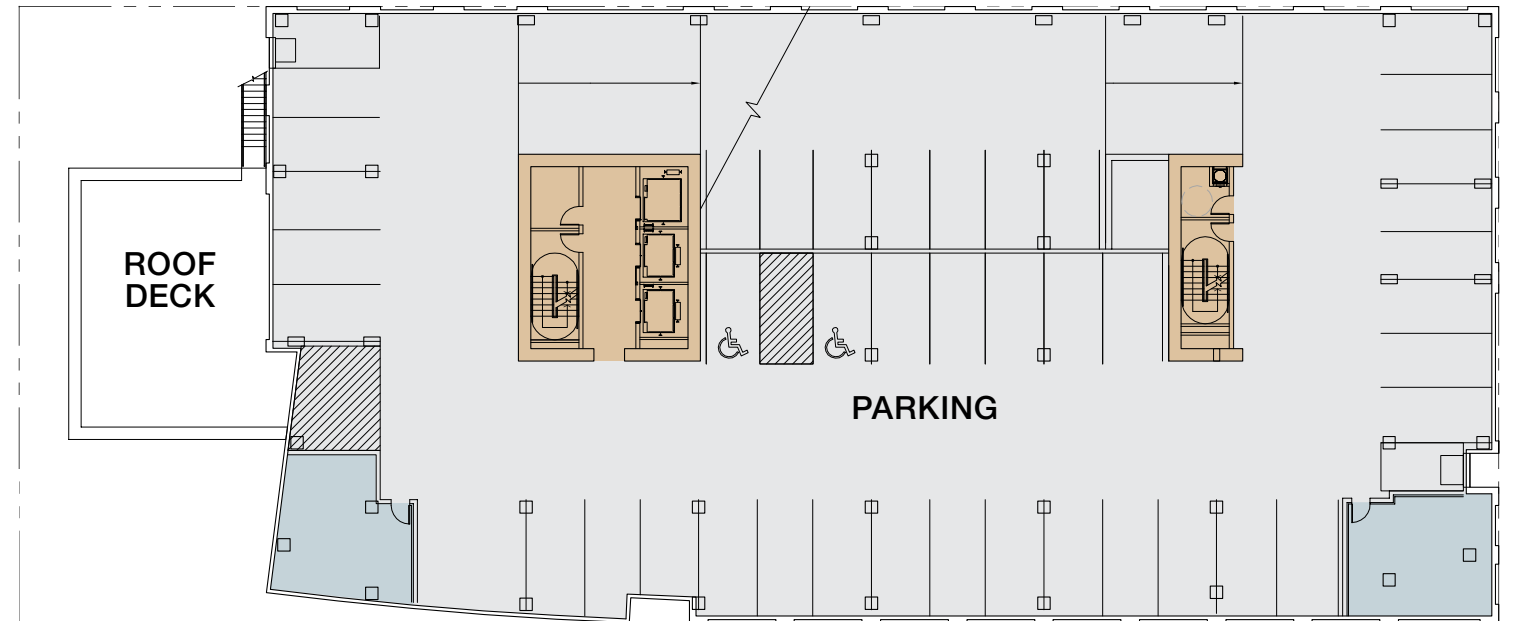
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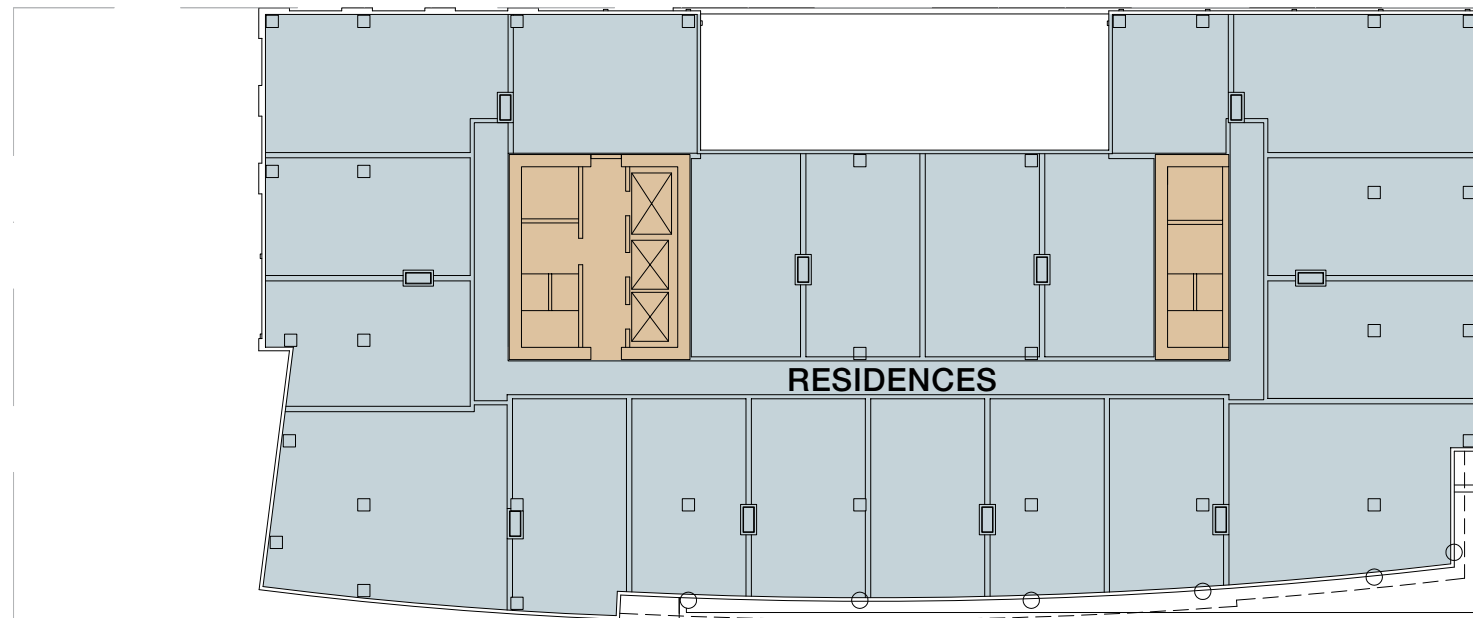




LEVEL 2



LEVEL 4 PARKING

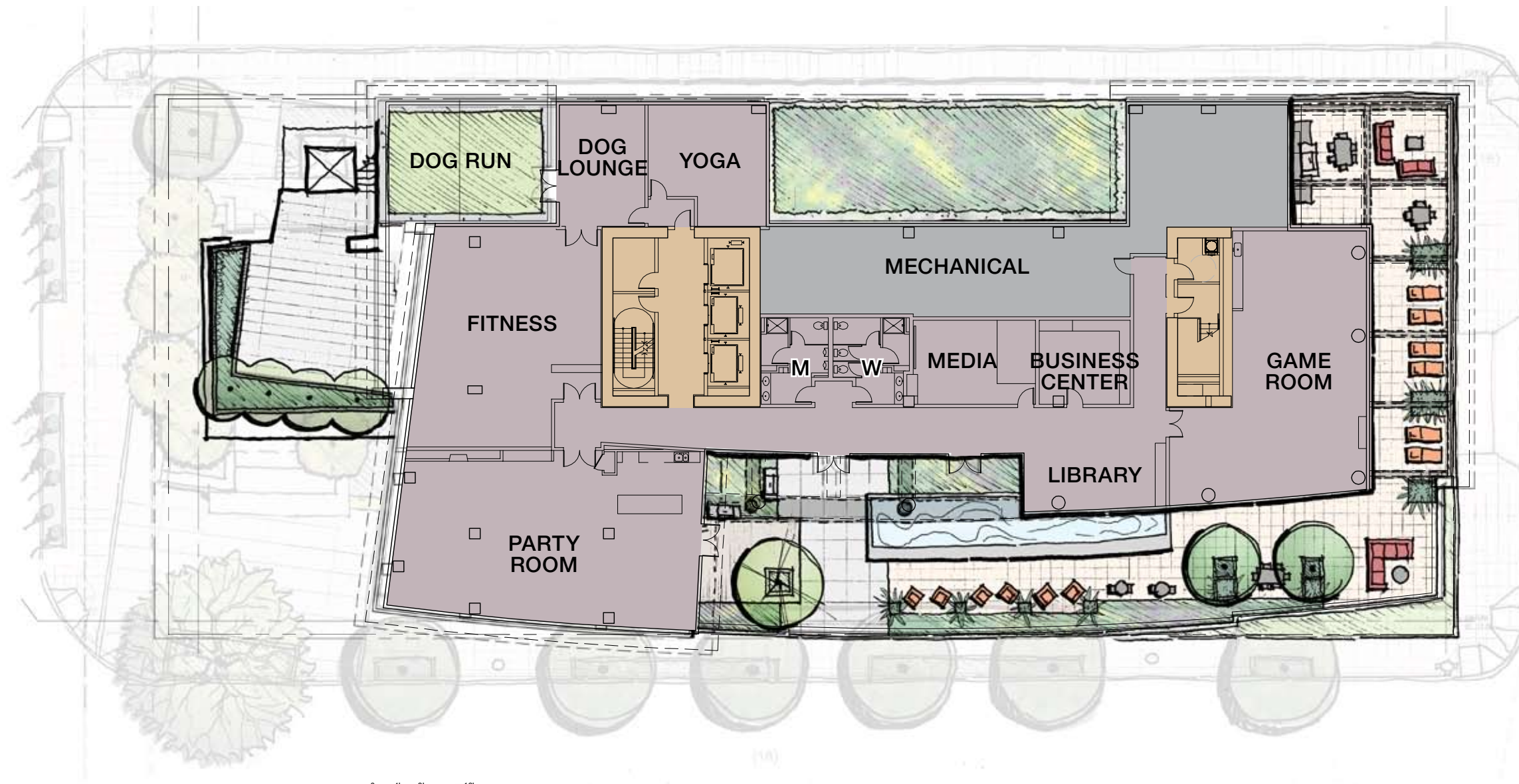


LEVEL 6 RESIDENTIAL



LEVELS 7 - 15 RESIDENTIAL





KEY

- Residential
- Retail
- Amenity
- Circulation
- Storage
- BOH
- Parking
- Mechanical





GOODMAN REAL ESTATE
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COLMAN RESIDENTIAL | PROPOSED SCHEME

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VIEW WEST FROM PEDESTRIAN BRIDGE



VIEW OF WESTERN FROM NORTH



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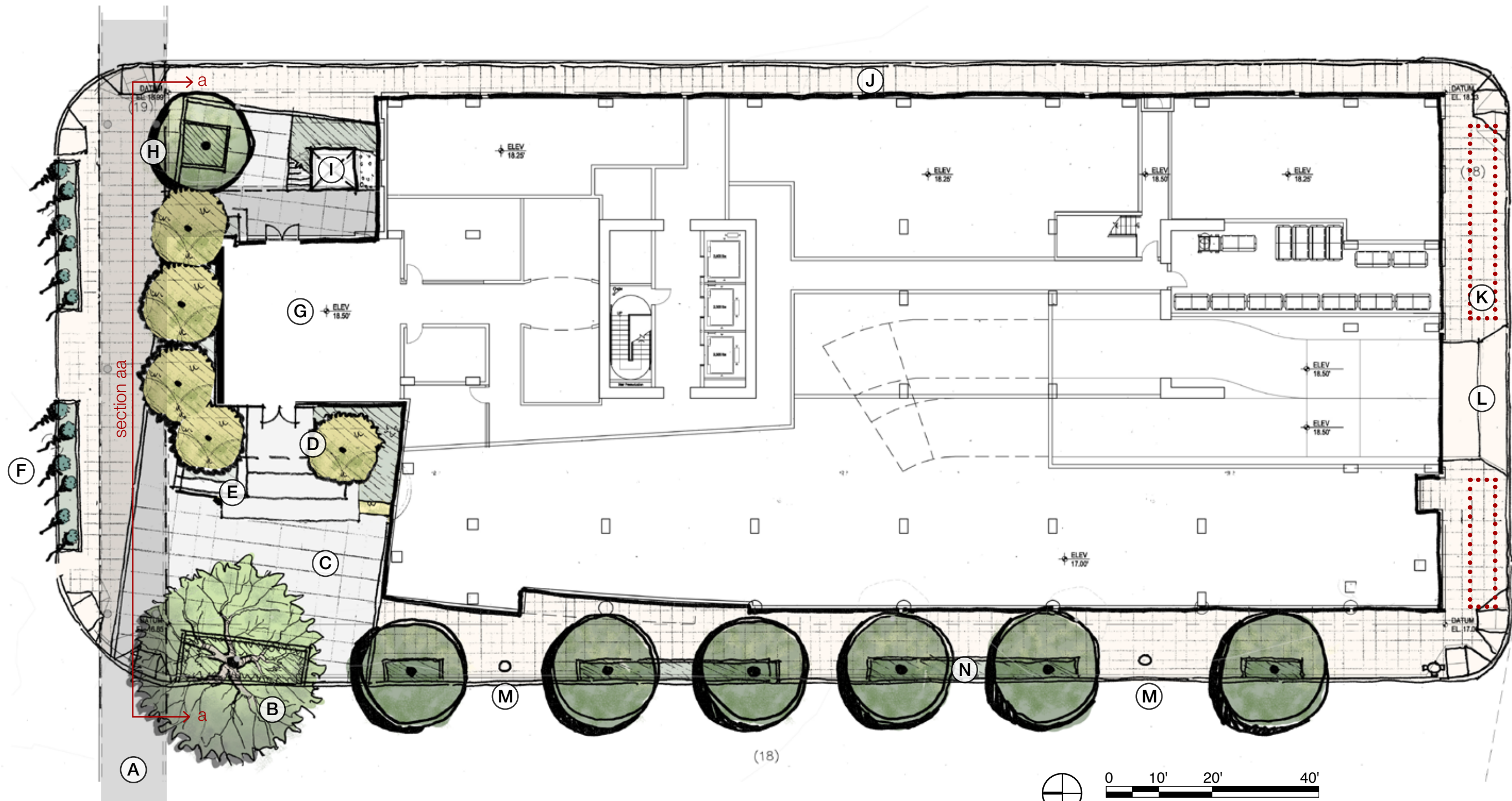


VIEW OF POST AVENUE FROM SOUTH



VIEW OF WESTERN FROM SOUTH





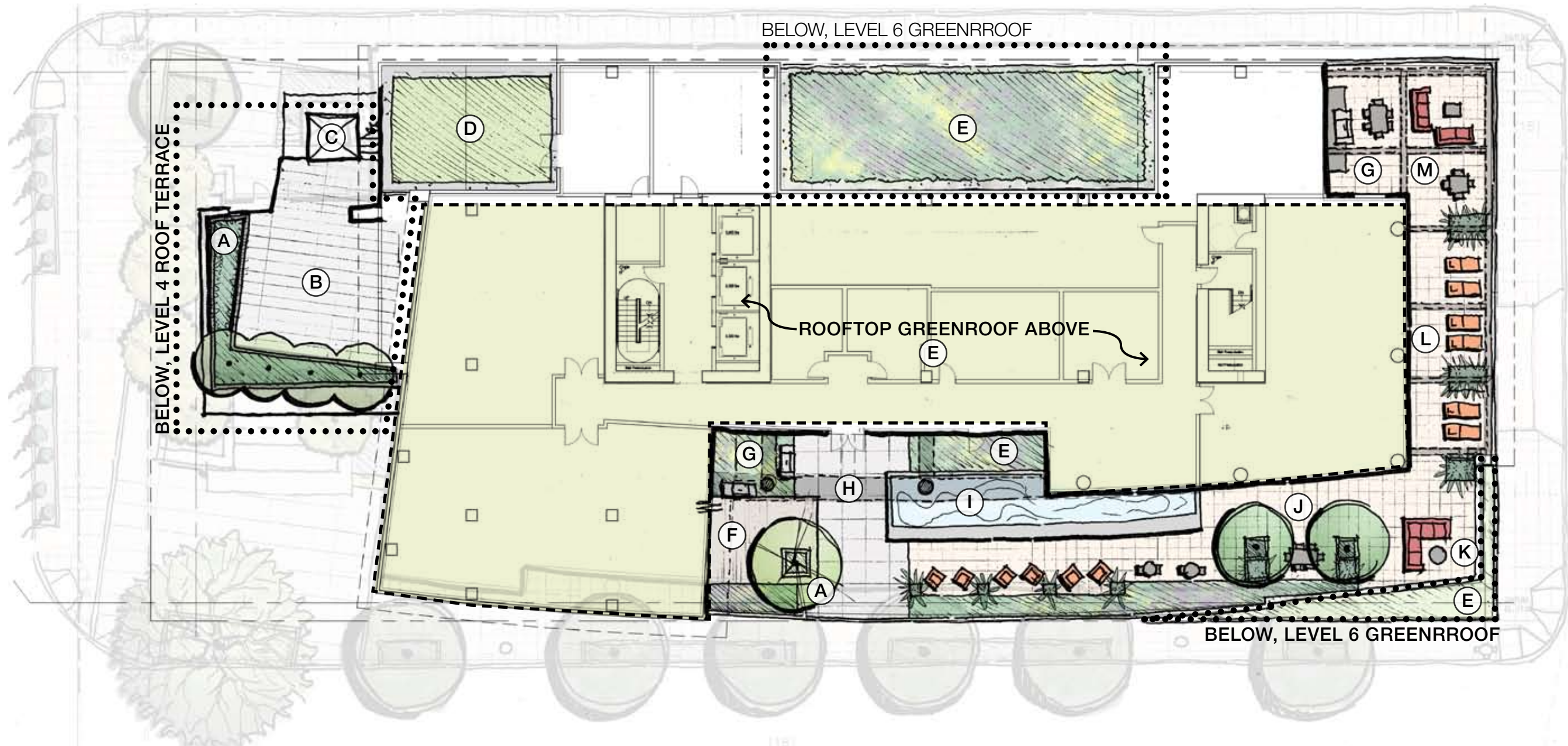
LEGEND

- A. Overhead walkway
- B. Existing Maple tree
- C. Plaza — accent paving
- D. Grove of Honeylocust with shade garden
- E. Bench — possibly recycled on-site stone
- F. Columnar evergreen with low groundcover

- G. Residential lobby
- H. Accent tree
- I. Exterior stair and elevator with shade plantings below
- J. Existing 6' sidewalk
- K. Utility vaults — no room for planting
- L. Parking entrance

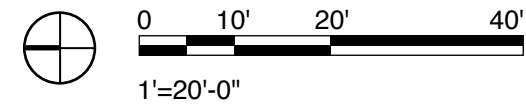
- M. Light pole
- N. Columnar street tree with ground cover/shrubs

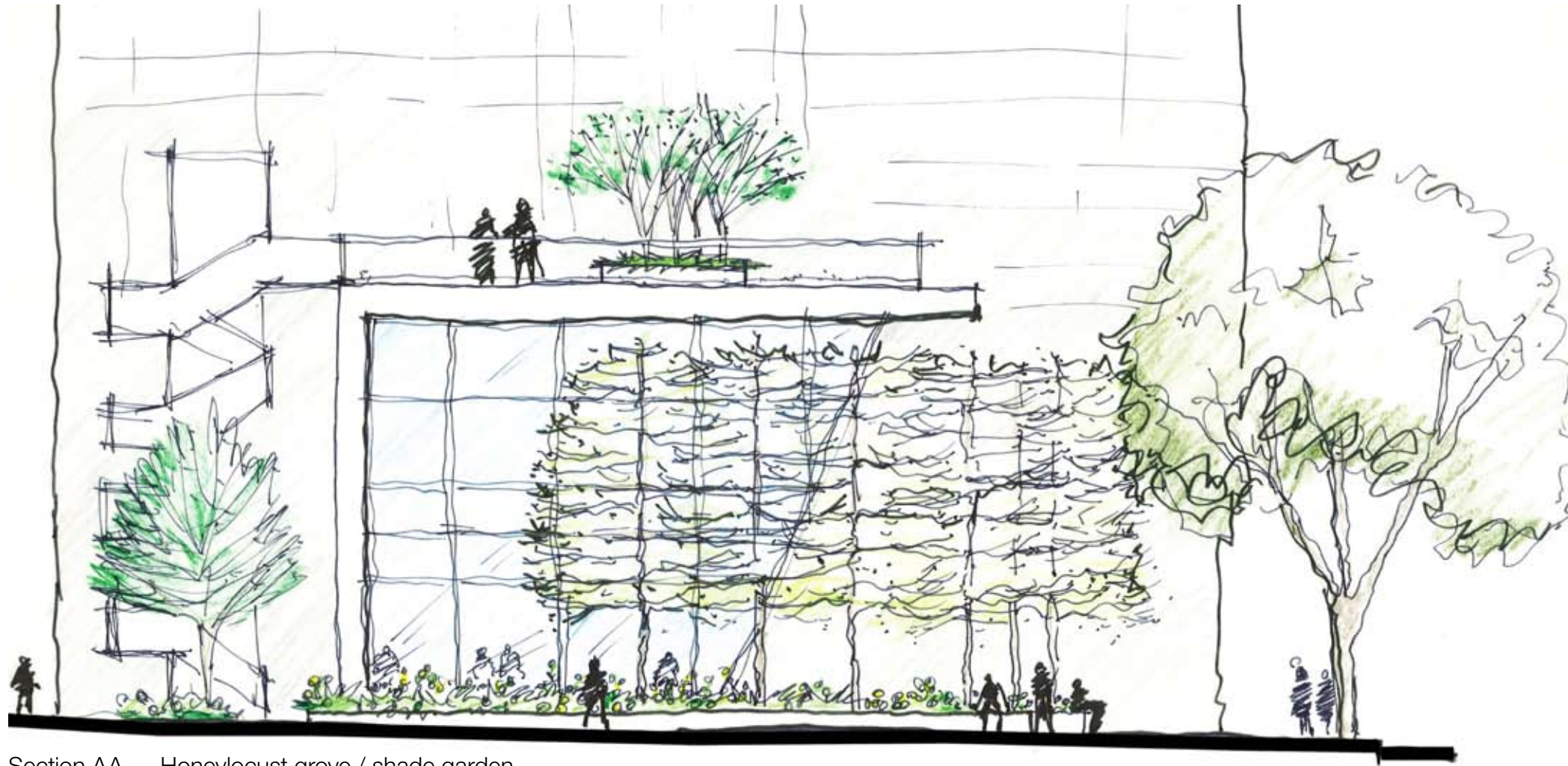




LEGEND

- | | | |
|--|---------------------------------------|--|
| A. Roof top planter with small trees | F. Wood deck connected to interior | K. Outdoor living space with fire pit |
| B. Accent paving to match plaza below | G. Grill area | L. Sun deck with overhead trellis |
| C. Exterior stair and elevator | H. Overhead trellis | M. Auxillary outdoor kitchen with adjacent dining area |
| D. Dog run area (artificial turf) — Roof terrace | I. Reflecting pool with seating ledge | |
| E. Sedum mix green roof | J. Dining tables and chairs | |





Section AA — Honeylocust grove / shade garden



DEPARTURE #1 — PARKING ACTIVATION

CODE REQUIREMENT

SMC 23.49.019-B2-a(2)

Parking quantity, location and access requirements, and screening and landscaping of surface parking. Parking above the third story of a structure shall be separated from the street by another use for a minimum of thirty (30) percent of each street frontage of the structure. For structures on lots located at street intersections, separation by other uses shall be provided at the corner portion(s) of the structure.

DEPARTURE REQUEST

A departure is requested to reallocate the active street frontage to all levels of the parking structure (not just “above the third level”) and to provide the required activation at Western Avenue only. Allocation would be as follows:

ALLOCATION

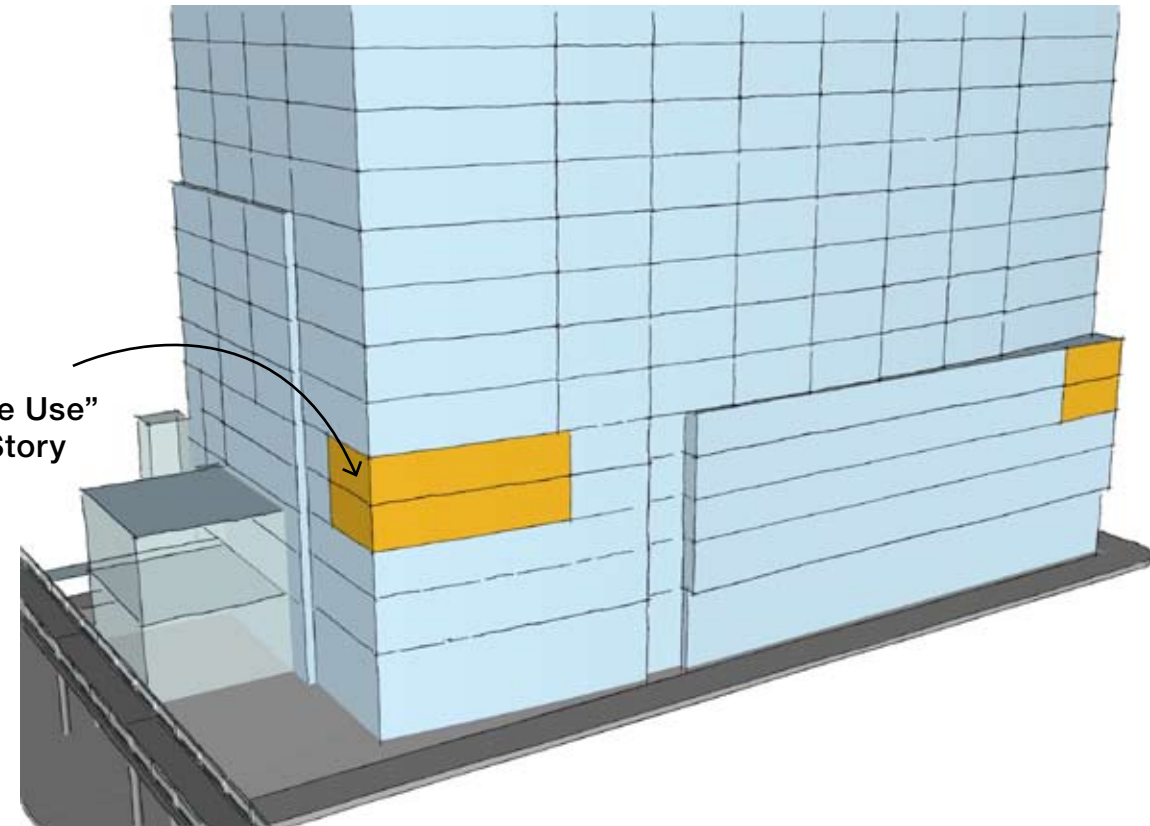
0% along Post Avenue
20% along Columbia Street
24% along Western Avenue
24% along Marion Street

RATIONALE FOR REQUEST

The design team proposes to provide active street façades in the form of studio apartments at the corner of Western/ Columbia and Western/Marion only. The configuration of the parking ramp precludes additional façade treatment along Columbia / Post Avenue. To compensate for the departure, the project proponent proposes activating parking floors on the levels 2–5, not only the code required floors 4, 5 (above the third story). This treatment provides a net gain of activated area on the building, concentrated on the corners listed above, which front the major street on the site as follows:

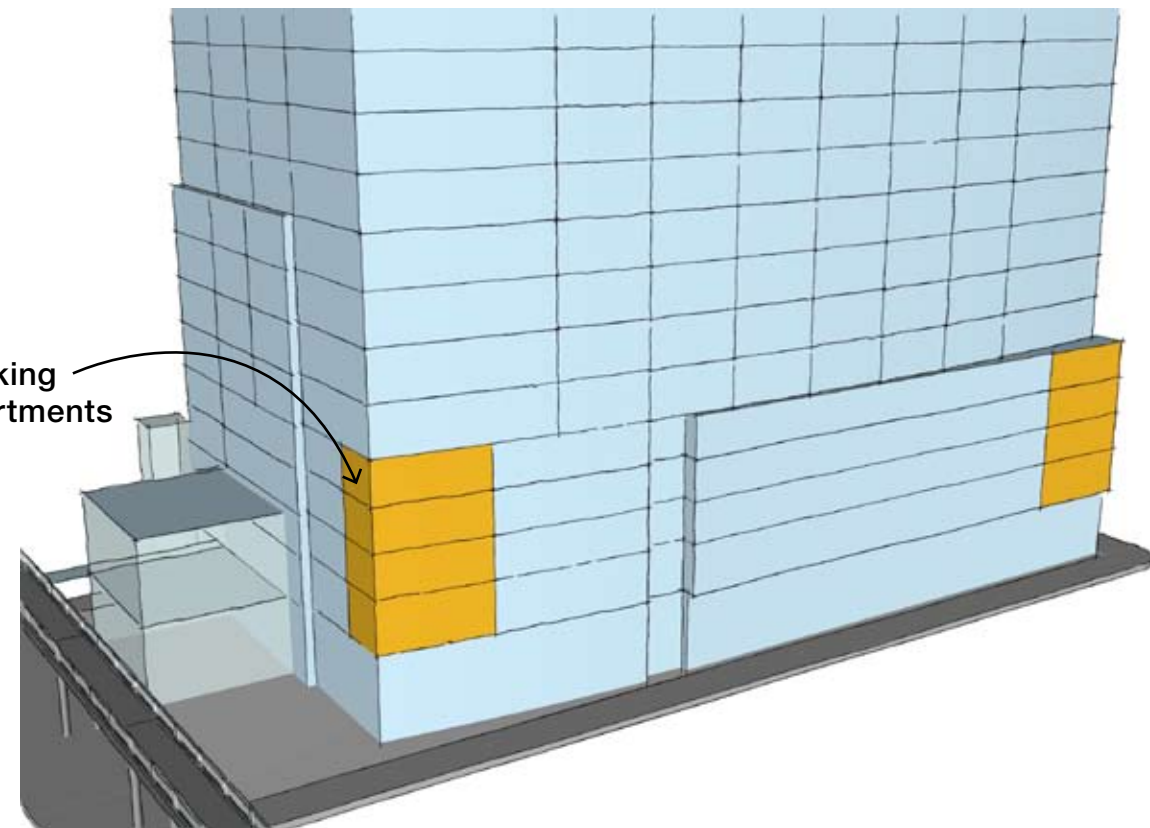
Code Required Face Frontage at Western Avenue = 120'-0" LF
Departure Provided Face Frontage at Western = 192'-8" LF

Zoning Code
only requires “Active Use”
Above the Third Story



CODE COMPLIANT (ACTIVATION “ABOVE THE THIRD STORY”)

Active Use in Parking
Structure: Micro Apartments



DEPARTURE REQUESTED ALTERNATE



DEPARTURE #2 — UPPER LEVEL DEVELOPMENT STANDARDS

CODE REQUIREMENTS

- SMC-23.49.058-DMC Upper-level development standards.

B. Façade Modulation

1. Façade modulation is required above a height of eighty-five (85) feet above the sidewalk for any portion of a structure located within fifteen (15) feet of a street property line. No modulation is required for portions of a facade set back fifteen (15) feet or more from a street property line.
3. Any portion of a façade exceeding the maximum length of façade prescribed on Table 23.49.058A shall be set back a minimum of fifteen (15) feet from the street property line for a minimum distance of sixty (60) feet before any other portion may be within fifteen (15) feet of the street property line.

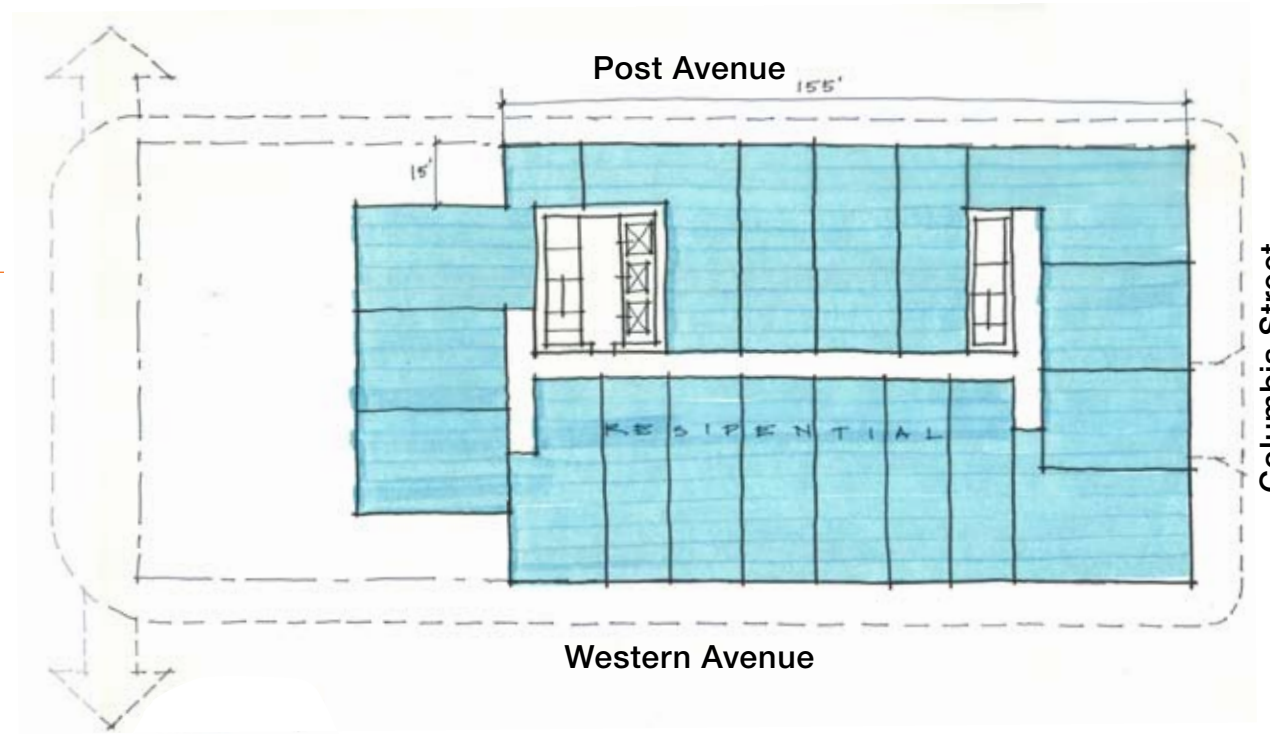
DEPARTURE REQUEST

Allow increased modulation on Post Avenue in lieu of the required modulation on Western Avenue in order to mitigate the impact on the historical Colman Building, and to enhance the Post Avenue pedestrian experience.

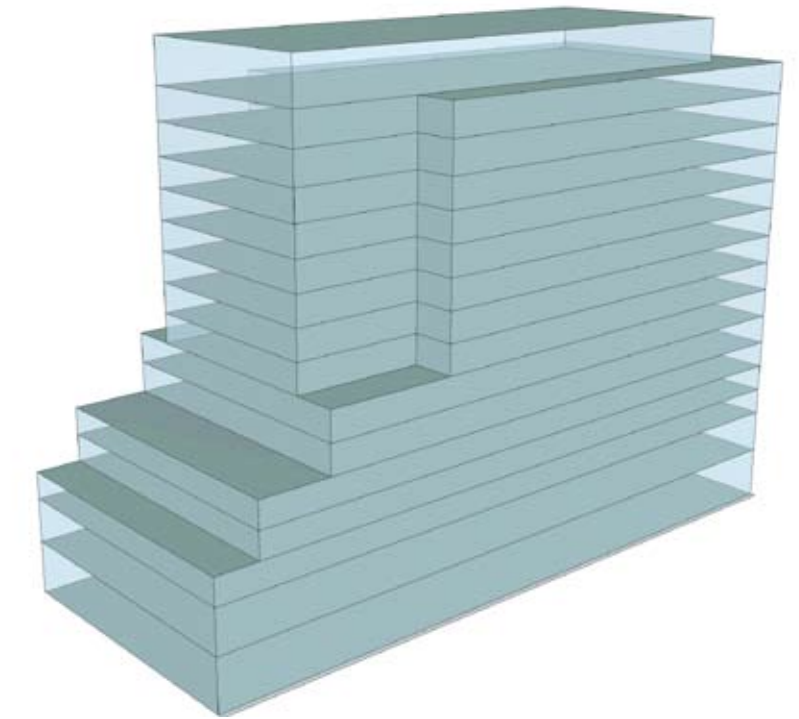
ALLOCATION: 15'-0"

RATIONALE FOR REQUEST

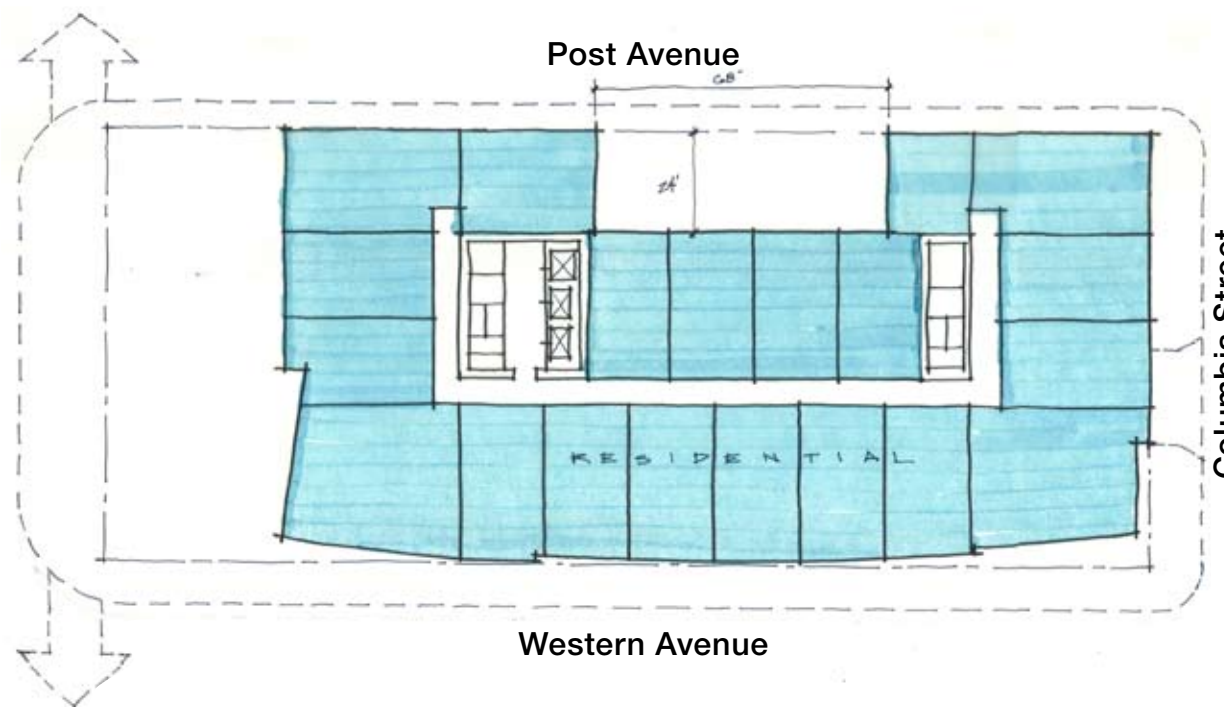
The project proponent has provided a 24'-0" x 68'-0" setback along Post Avenue. The code would require a 15'-0" setback pinch point in the middle of the site. This pinch point precludes an efficient double loaded corridor unit layout and would not mitigate impact on the historical Colman Building. For this reason the design team maximizes the setback along Post Avenue where the right of way is very tight. The setback architecturally recalls the setbacks along the existing Colman Building and it holds the street edge along Western which mirrors the existing context.



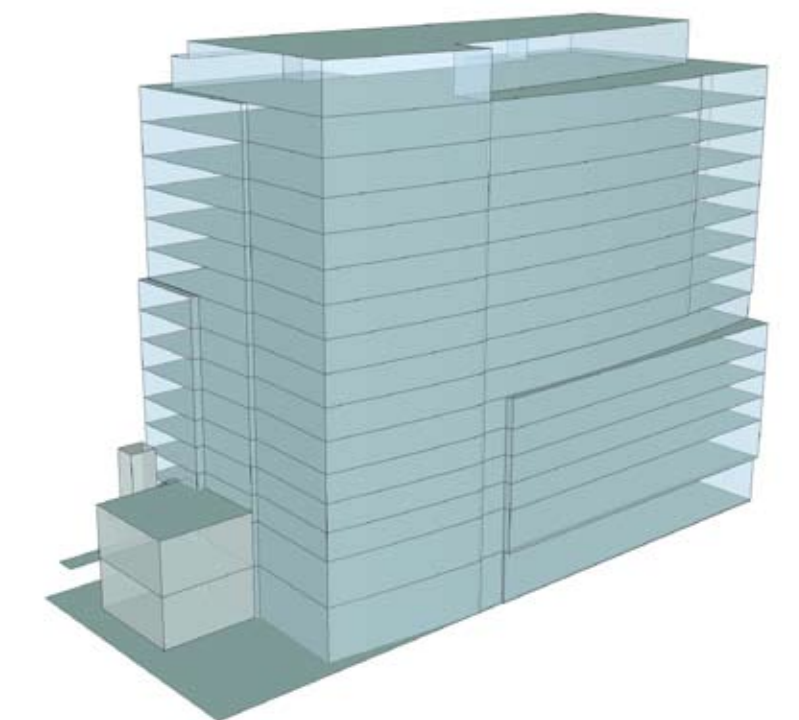
CODE PRESCRIPTIVE PLAN



CODE PRESCRIPTIVE MASSING



PREFERRED ALTERNATE DEPARTURE



PREFERRED MASSING



REQUESTING BOARD SUPPORT — POST AVENUE SIDEWALK

(NOT A DEPARTURE)

CODE REQUIREMENT

Downtown Zoning Map C

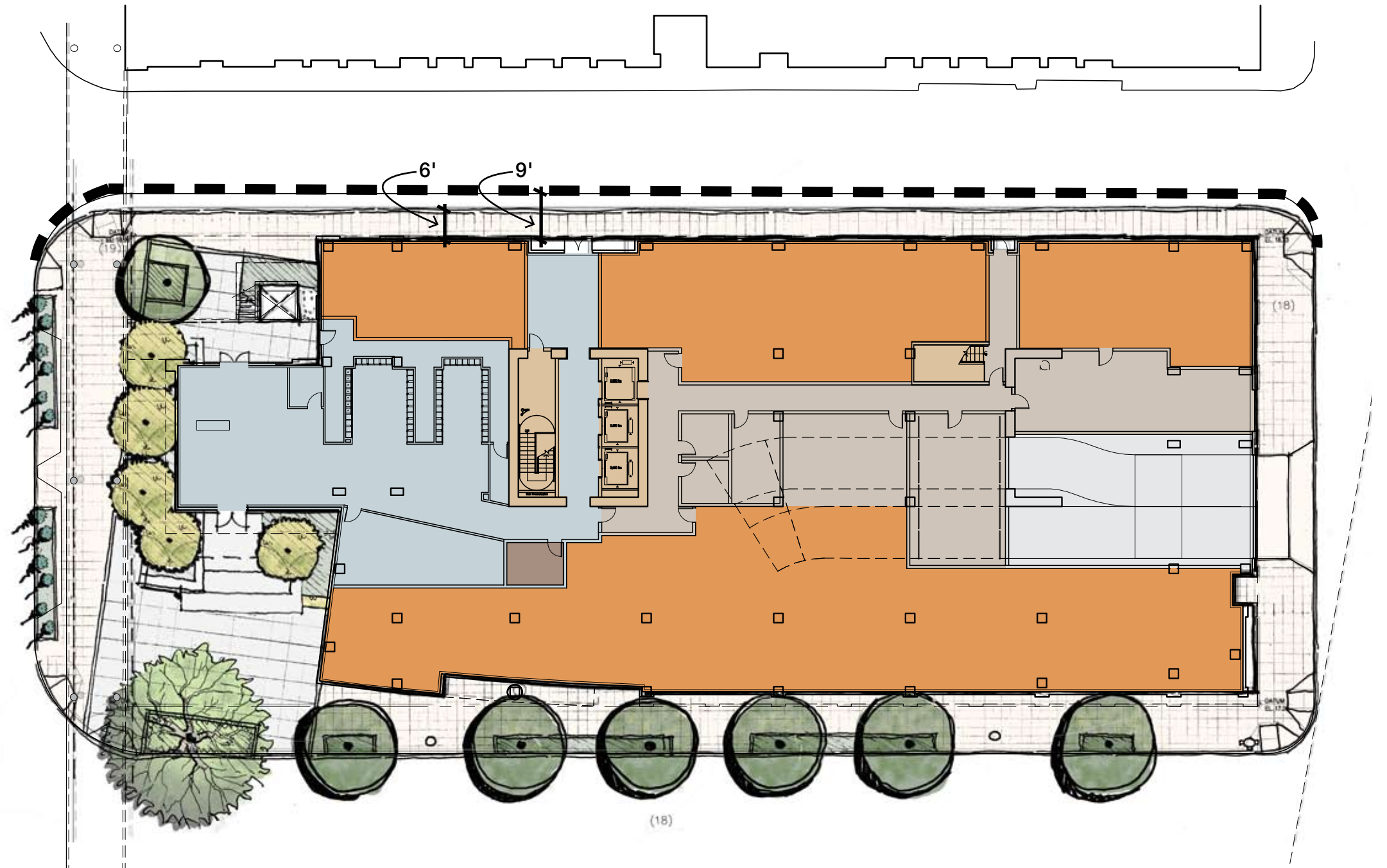
12' sidewalk us required on Post Avenue

DEPARTURE REQUEST

Allow for 9' sidewalk, with 3' extension into the street ROW.

RATIONALE FOR REQUEST

The Project proponent requests DRB support for providing a 9'-0 sidewalk instead of a 12' sidewalk which leaves approximately 22'-0" for one lane of traffic and one lane of parking/delivery. The project proponent requests board support for moving into the ROW so that retail on Post Avenue is not reduced, and would be more viable.



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COLMAN RESIDENTIAL | REQUESTING BOARD SUPPORT ITEMS #1 AND #2

08.24.10

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SUMMER SOLSTICE



9AM



12PM



6PM

WINTER SOLSTICE



9AM



12PM



3PM