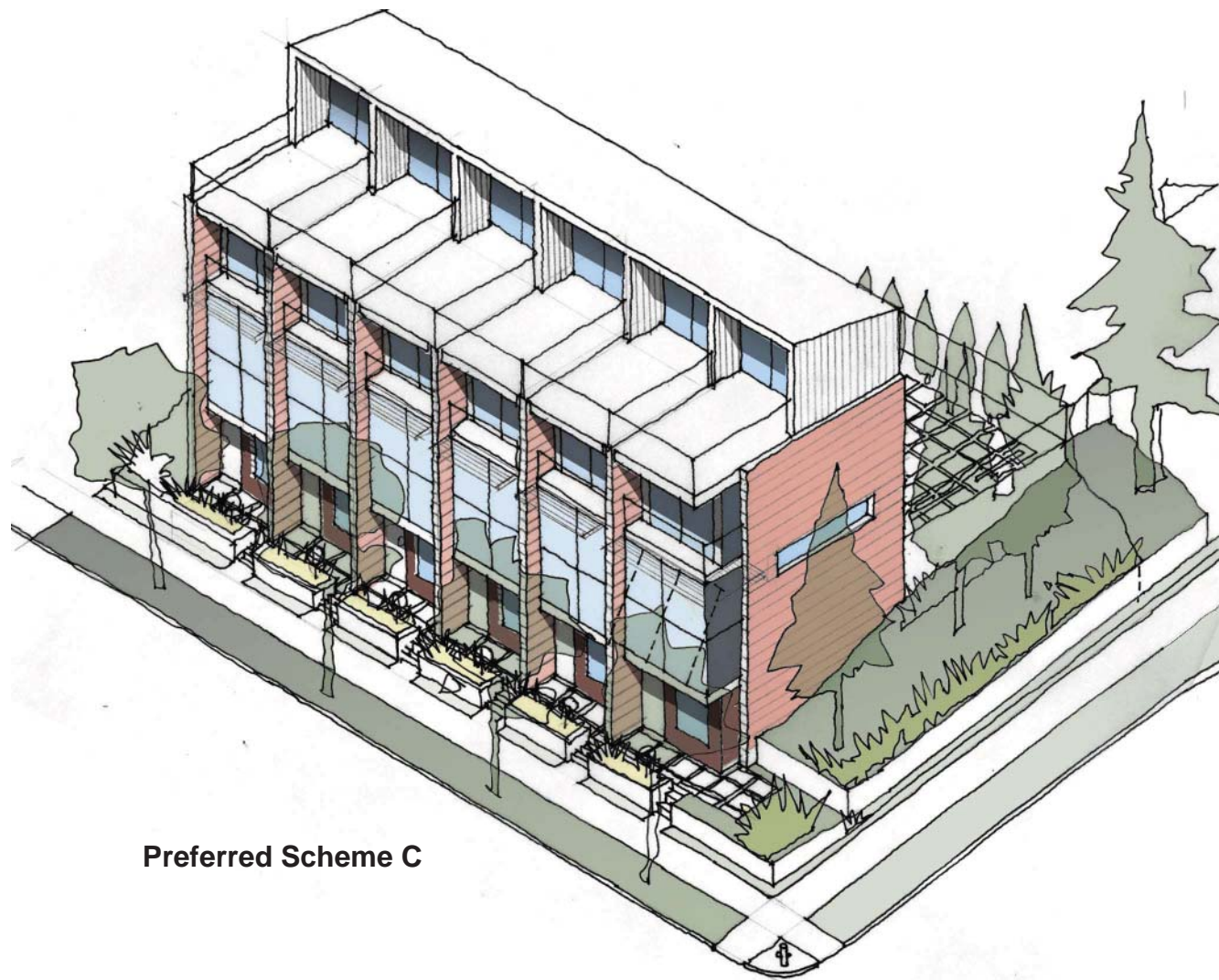




Project Site March 9, 2010



Preferred Scheme C

**3904 South Edmunds Street
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1 DEVELOPMENT OBJECTIVES

1. DEVELOPMENT OBJECTIVES

The project goal is to construct a high quality, “for sale” townhouse development that is a positive addition to the Columbia City neighborhood fabric. While not in the Historic Overlay District, the projects location on the “edge” of the district can build on the strong pedestrian character of the neighborhood, to enhance the street, neighborhood, and character of this specific project.

Putting priority on people spaces, a hierarchy of public to semi private outdoor spaces, and a strong “stoop” connection to enliven 39th Avenue South are prime small grain urban design goals for the developer. The fixed development resources will be used to create amenity that build on Columbia City’s position as the key walkable neighborhood in South Seattle. While not in the Station Overlay District, the site is easily walkable to the new Link Light Rail local station. Neighborhood walkable amenities are many, and it appears that many more will be added in the near future for shopping, dining, and support services.

Therefore, In developing the site, The proposal will plan for car parking, but not at the expense of quality design, outdoor spaces for people, or the urban fabric.

The potential purchaser will be savvy to all these opportunities, and will choose to live in Columbia City based on its unique character. The Developer’s goal is to further the development models that spring from a forward looking vision rather than past suburban real estate or car commuter models. The development team envisions a time when the the parking area may be used for other purposes, assuming the residents may choose to not own cars, or use a more compact form of transportation than the typical car such as scooters, bicycles, or compact vehicles. There are current examples of this type of “woonerf” development that are successful and appropriate for a changing, more dense, greener Seattle.

In an effort to keep a sales price that is consistent with the current relative affordability of the neighborhood, the site will be slightly underbuilt. Preferred scheme C minimizes excavation, shoring, and costs associated with vehicle storage.

The site is zoned for a maximum of 8 units in an L3 apartment configuration. Proposals are shown for Townhouse schemes that comprise, 4, 5 and 6 units.

The optimal scheme has been determined to be the 6 unit scheme. It best exemplifies the stated urban design goals, responds to site characteristics, and meets the program and financial feasibility goals of the developer.

Parking will be provided for 6 private vehicles.

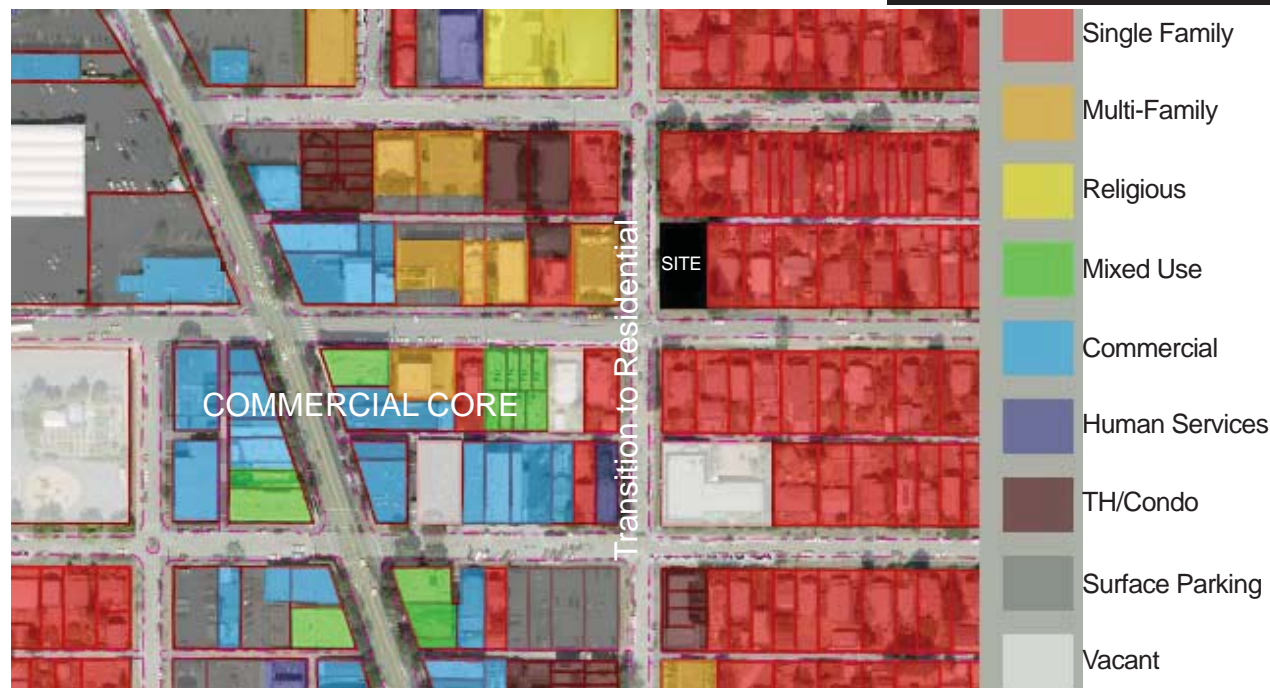
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4 d SITE ANALYSIS



2 a VICINITY MAP



2 URBAN DESIGN OPPORTUNITIES AND CONSTRAINTS

General

The corner lot of 6000 SF is at the northeast intersection of 39th Avenue South and South Edmunds Street, and bounded by a non through alley to the North. Man made grade change results in a bank of approximately 14' at the west property edge. there is currently a single family home and a small detached garage on the site, along with several trees.

The site sits at the Eastern edge of the the Columbia City business district, on the the transitional line between the flat commercial neighborhood and the single family neighborhood up the hill that supports the district. At the Northeast corner of the S Edmunds and 39th Avenue South, the project provides the terminus of the pedestrian street that starts with the new link light rail station, moves through Columbia City, and stops at this intersection where the zoning, flat topography, and historical overlay district end.

Design Cues

The alley to the North of the site is paved to a minimal level, and does not go through to the west. It serves primarily single family homes, and the end of the alley is used as an informal local dump. SDOT has indicated that it has no plans to improve the alley to through access due the prohibitive grade change. The block of Edmunds on which the property sits is an unusually long block in the East-West direction. It spans from 39th to 42nd. South Edmunds has a gentle grade up to the east, and the homes along it are consistent in scale and siting. Most of the homes date from the prewar period, but a few newer infill houses can be found. In the North-South direction, this particular block of 39th Avenue South is one of the least pedestrian friendly in the area, with the high bank of the proposal site, and a large number of curb cuts and side walls on the west.

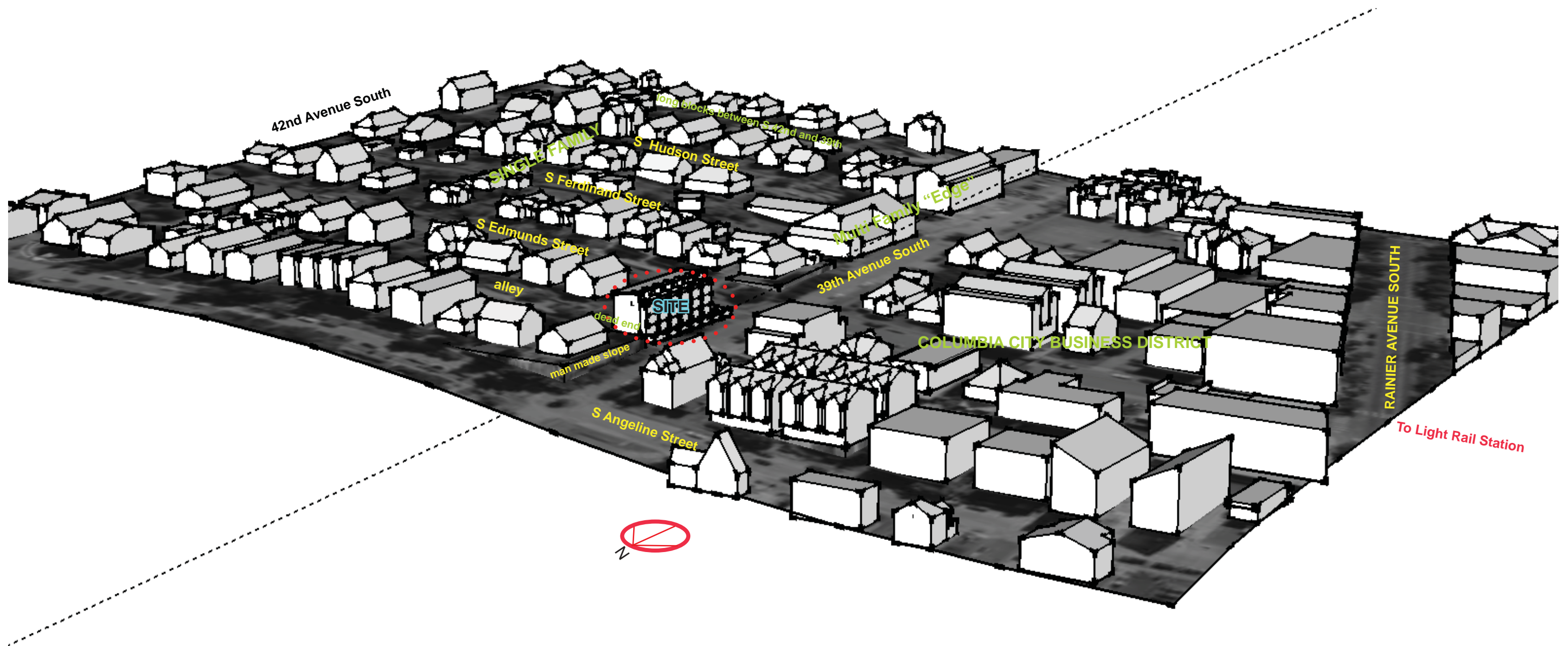
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2 c AXONOMETRIC DRAWING

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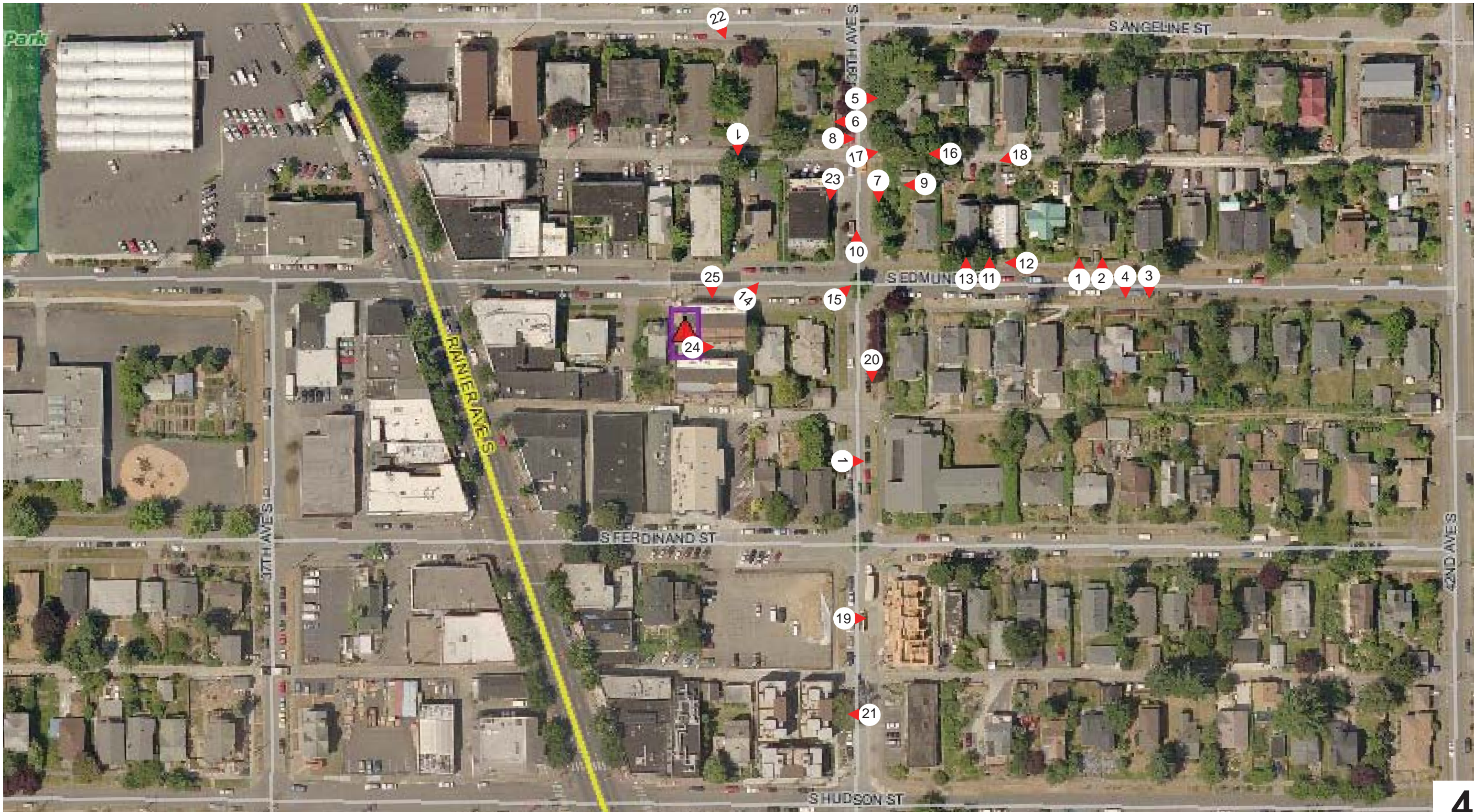
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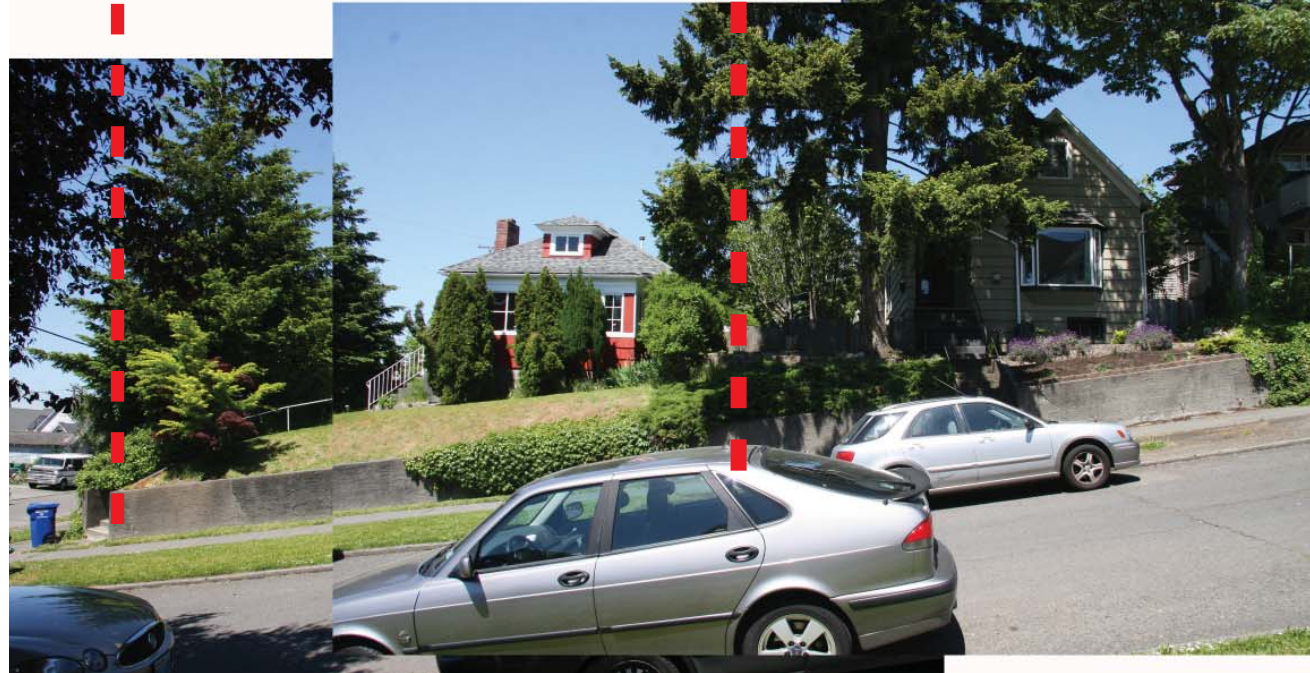
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PROJECT SITE



1 SOUTH EDMUNDS STREET
NORTH side of street between 39th AVE S and 40th AVE S
West half of block



2 SOUTH EDMUNDS STREET
NORTH side of street between 39th AVE S and 40th AVE S
East half of block

2 e PHOTOMONTAGE OF STREETScape

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3 SOUTH EDMUNDS STREET
SOUTH side of street between 39th AVE S and 40th AVE S
East end of block

4 SOUTH EDMUNDS STREET
SOUTH side of street between 39th AVE S and 40th AVE S
West end of block



2 e PHOTOMONTAGE OF STREETScape

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ALLEY (not through)

Steep. SDOT does not want to open it up.

5 39th AVENUE SOUTH
EAST side of street between S Angeline and S Edmunds streets
Full Block North to South



6 39th AVENUE SOUTH
West side of street between S Angeline and S Edmunds streets
Full Block South to North

2 e PHOTOMONTAGE OF STREETScape

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important guidelines

Context

A-1 Site Characteristics

The most significant characteristic is the man made change in elevation from the sidewalk at 39th Avenue South to the plateau at the top of the site. How the project is planned directly affects the way the proposal integrates into the neighborhood. A non through Alley borders the site to the North. SDOT does not plan to either extend or improve the alley.

Scheme A involves an all underground parking scheme, which would keep the appearance of the topography similar to its preconstruction state. However, the site would be almost 100% excavated, and a defensive relationship of the project to the pedestrian environment would result.

Scheme B is a stepped scheme, where some parking is proposed below grade off of 39th, and some off the Alley at the higher elevation. Grading would occur, and the slope would need to be stabilized and recreated with retaining walls.

Scheme C excavates out the west edge of the site, removing the slope and replacing it with inhabited structure at the street level. The retaining element is pushed to the center of the site, where it creates the least risk to the public and adjacent single family home. This proposal steps as well, moving from building to parking to relatively low retaining wall at the East property line.

A-2 Streetscape Compatibility

The site offers a unique opportunity to create an active edge along 39th Avenue South to the Columbia City business district to the West, and express the transition for more dense commercial and multi family uses to single family up the hill. The topography already creates this shift. Scheme A adds nothing to the pedestrian experience. Scheme B makes a nice gesture towards creating more activity on 39th. Scheme C realizes the full potential, creating an active "room" out of 39th Avenue South where there is currently a gap in the pedestrian experience.

A-3 Entrances visible from Street

Scheme A is the most defensive of the schemes, with a common pedestrian entry part way up S Edmunds. Scheme B offers a more welcoming, common pedestrian entry off of 39th Avenue S. The central stairs are clearly identifiable as the sites gateway. Preferred Scheme C develops a whole new relationship between the site and the street, with 6 stoops and pedestrian entries on 39th Avenue South.

A-4 Human Activity

Scheme A creates minimal human activity on the site. Scheme B creates a single entry point on 39th Avenue South that will generate pedestrian activity. The private open spaces are well above sidewalk grade. Preferred Scheme C creates human activity and inhabits the street edge on 39th Ave S, through stoops, small terraces, and building facades. This will occur to some degree on S Edmunds Street.

A-5 Respect for adjacent sites

The most directly affected adjacent site is the single family home to the east. Fortunately, it has an unusually



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3 CITY DESIGN GUIDELINES

wide side yard, and sits higher than the project site. Sun from the south will not be blocked, but sun from the west could be impacted. Preferred scheme C offers the greatest distance between structures, and the lowest roof height relative to the single family home. In the proposal, vehicle headlights would be screened by retaining walls and landscape features.

A-6 Transition between Residence and Street

Scheme A has a discreet common pedestrian entry on S Edmunds Street, creating little social interaction. The scheme is extremely private. Scheme B does a better job with the common entry and steps facing 39th towards the business district. Scheme C creates a classic combination of steps, stoops, and forecourts for 6 residences on 39th Avenue S.

A-7 Residential Open Space

All three schemes provide a mix of on grade and above grade open space.

A-8 Parking and vehicle access

The small corner lot, extreme grade change between 39th Avenue South and the top of the site, and the lack of a through alley make parking and vehicle access a challenge. Scheme A places all parking underground, through a single garage access point. It might be possible to negotiate with SDOT for access off the alley ROW in exchange for improvements and stabilization to avoid parallel curb cuts. Scheme B uses a mix of underground and on grade parking, off both 39th Avenue S and the Alley. Scheme C proposes a split level on grade parking scheme, off of both S Edmunds Street and the Alley. The intent is to plan for residents who may elect to not drive, and use the parking area for double duty.

A-10 Corner lots

Scheme B addresses the Uniqueness of a corner site well. The preferred Scheme, scheme C would be improved by making the 6th unit unique in its expression to address its prime location, and provide interest along South Edmunds Street that works with the single family facades along the block uphill.

Height Bulk and Scale

B-1 Height, Bulk and scale compatibility

The site is at a transition zone between multifamily and commercial and single family. Scheme A, while being built on the existing grade level, creates a wall to the west of the single family zone to the east that is full height. Scheme B creates an interesting mix of forms and open gaps in the massing that transitions to the single family to the East and North. Scheme C, by being placed down on 39th Avenue S, has the lowest impact on the single family due to its lower relative height and distance of the building to the east property line.

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Architectural Context and materials

C-1 Architectural Context

There are a variety of eras and building types represented in the immediate area. The historical context to the west gives strong cues, as does the single family context to the east. The preferred scheme "C" builds expresses individual unit identity as encouraged in the multifamily code. The strong rhythm of the storefronts of the neighboring historic district is also built upon for bay spacing. It is anticipated that some form of masonry or modular unit system will be used to create a substantial feel to the project. The top floor of the preferred scheme "C" steps back from the west to reduce the apparent height. Viewed from the east, the height of the building is 2 stories, consistent with the Single Family neighborhood.

C-3 Human Scale

Preferred scheme "C" proposes many elements that will give the project a human scale, including stoops, forecourts, individual site lighting and small scale fencing, entry doors, and architectural details.

Pedestrian Environment

D-1 Pedestrian Open Spaces & Entrances

Preferred scheme "C" proposes small stoops, usable forecourts, and details at sidewalk level along 39th Avenue South that give a friendly and interesting relationship to the street, while creating visual cues for the residents that protect privacy and territoriality. Landscaping, low scale fencing, signage, paving, lighting, and color will all be used to enhance the connection of public, semi private, and private spaces along 39th. Along South Edmunds Street, the facade is relatively short, and there will be opportunities for visual relief through landscaping and building elements that will ease the transition to the single family homes to the east.

D-3 Retaining Walls

Retaining walls are minimized in preferred scheme C. They are a significant challenge for the urban environment in Schemes A and B.

D-6 Screening of Dumpsters, utilities and Service Areas

It is anticipated that garbage and recycling will be located off the alley, or where the grade change occurs within the proposed parking court. Both locations would minimally impact the public and adjacent sites.

D-7 Personal Safety and Security

Creating more "eyes" on the street through the stoop scheme enhances personal safety for both residents and the sidewalks, in contrast to a more inwardly focused scheme.



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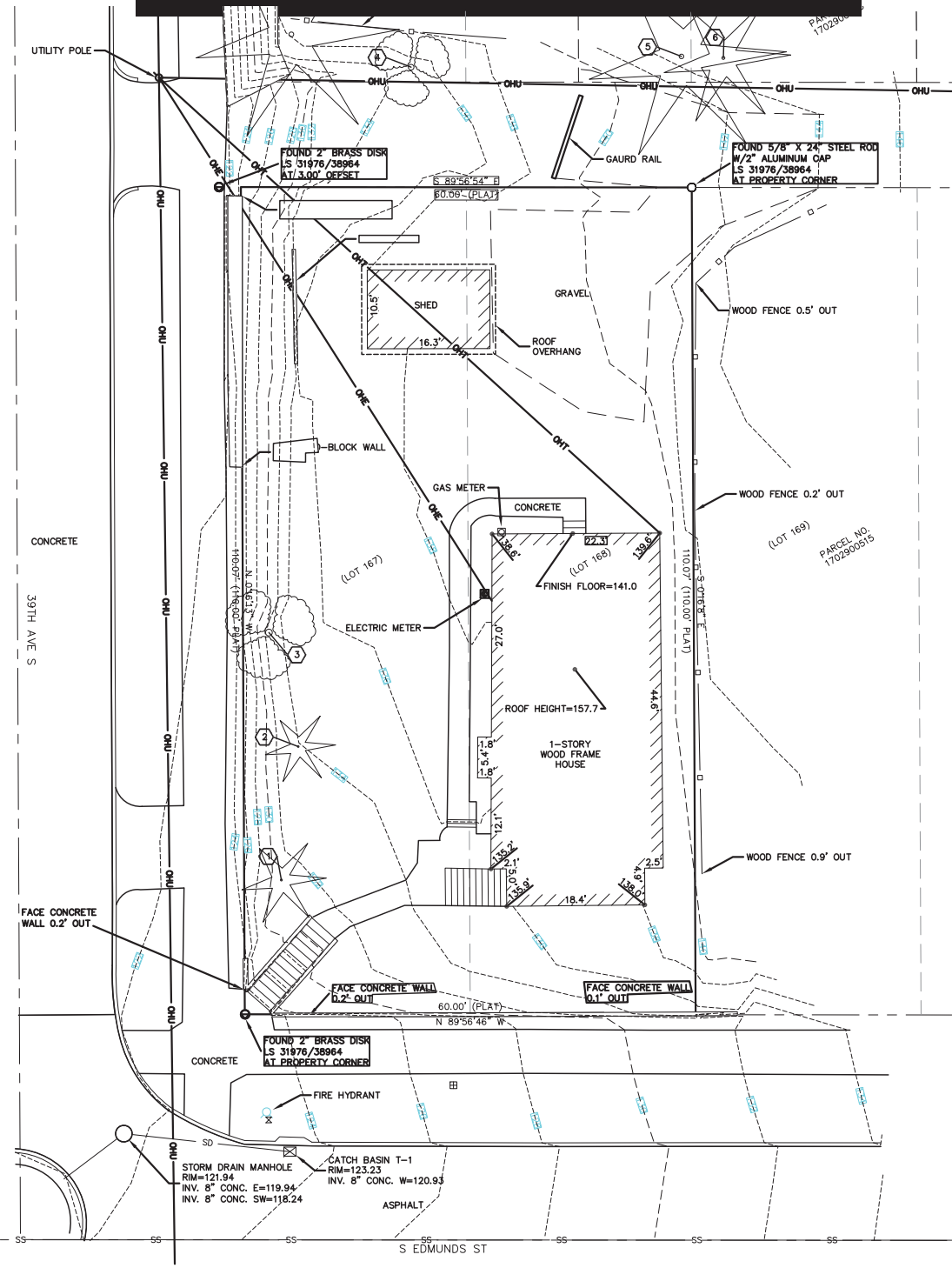
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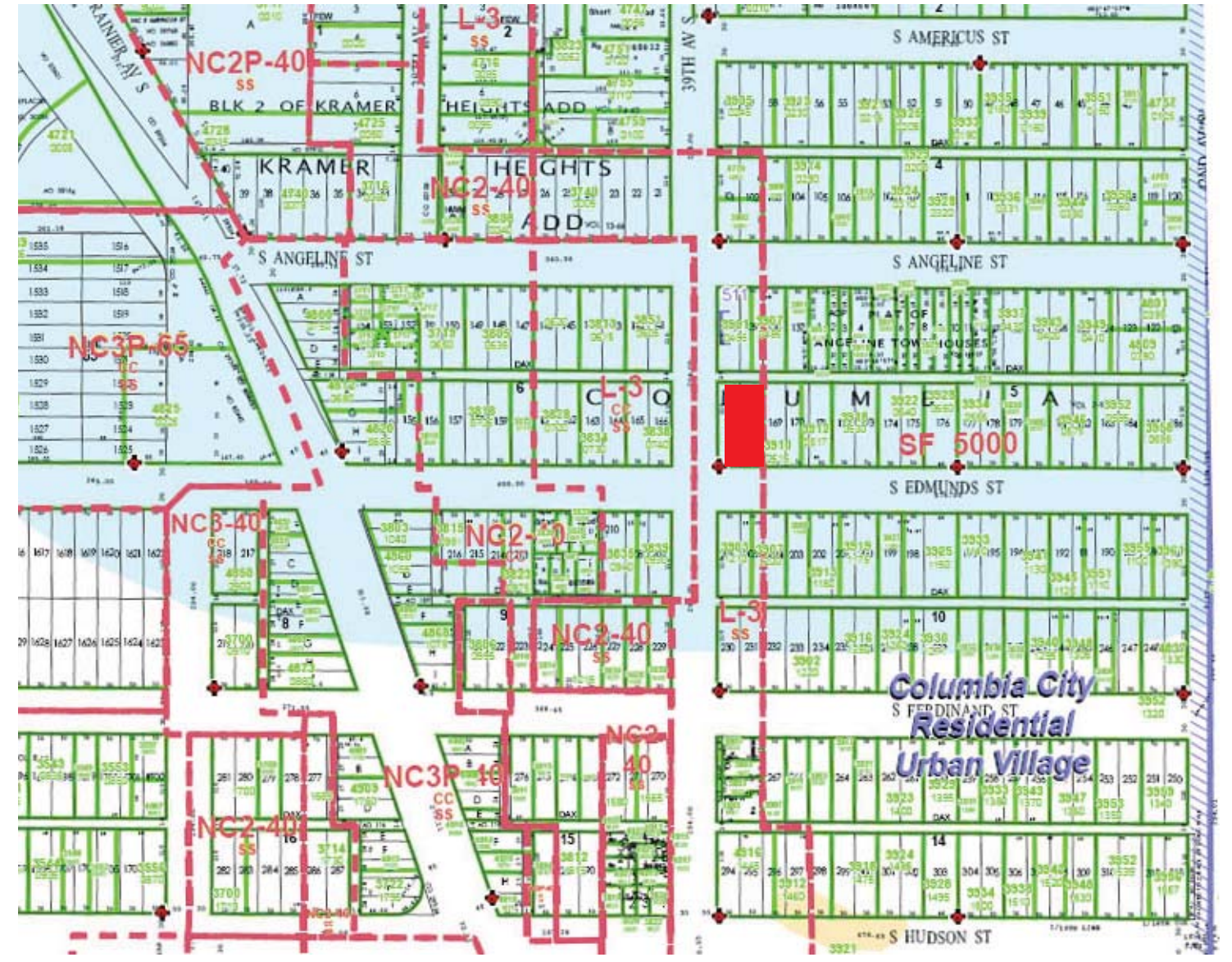
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4b Topography and Trees



TREE TABLE			
NO.	CALIPER(IN.)	SPECIES	DRIP LINE RADIUS(FT.)
01	12"	DOUGLAS FIR	12'
02	10"	DOUGLAS FIR	10'
03	12"	DECIDUOUS *	12'

4 SITE ANALYSIS



4a Zoning



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7 Looking South down the west property line. The man made steep slope is prominent, with about 16' rise.



8 View of the Alley ROW from 39th Avenue South. It is an informal dumping ground.



9 Looking West from the top of the site. The top of the site is relatively flat.



10 Looking North up 39th Avenue South at the site. The site is a 16' high wall of vegetation.



11 View of the site from the intersection of S Edmunds and 39th Avenue South



12 Looking Down South Edmunds to the West. The historic district starts in the flat at the intersection of South Edmunds and 39th Avenue South.



13 Facing the site and adjacent house at 3910 South Edmunds to the left. The side yard and distance between the two houses is unusually large for Seattle.

4c Site Photos



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14 View from 3d floor of mixed use building 1/2 block west of site on S Edmunds Street. Notice the man made slope on the edge of the site, how high the single family residences march up the hill beyond the site, and the boxy apartment in the foreground across the street on 39th Avenue South.



15 This view shows the site on the corner, and the rise of the hill relative to the flat more dense commercial and historic district in the foreground.



16 View Southwest across the neighbors backyard at 3910 S Edmunds. Glare and noise from the proposed on grade parking would be shielded by the grade change. The proposed building in the preferred scheme sits further west than the existing house to be demolished.



17 Detail of the house across the alley to the North, on the same East side of 39th Avenue South. The house sits up very high, and very close to the street property line.



18 View West down the alley. The detached garage currently on the site is to the left. The entry to the proposed parking pad would be in approximately the same location as the current access.

4c Site Photos



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4c Context Photos & Comments



19 Non pedestrian oriented conversion of apartment to townhouses further south on 39th Avenue South. Part of the edge definition along the east side of 39th Ave S for the district. Similar in some ways to Scheme A.



20 the church in the foreground was recently purchased, and the Owner has plans to convert to housing and a B & B. The street wall on the East side of 39th at the edge of Columbia City is prominent. The proposal for scheme C plans to emphasize this edge. Setbacks on the East Side of 39th Avenue South are short.



21 Successful L3 Townhouse project at the corner of 39th Ave S and S Hudson Street. The structure is flush with the sidewalk property line.



22 This prescriptive style townhouse development just built on S Angeline replaced a nice lowrise apartment court.

The neighborhood would have been better served by a more urban approach. "Private" outdoor open areas in the foreground are in fact a no-mans land. Neither stoop nor true yard. Current code pushes the average developer towards this expedient solution. A missed opportunity.



23 Apartment building directly across the street from the site on 39th Avenue South. Excessive curbcuts make it a not very pedestrian oriented 1/2 block.



24 CCLA double duty courtyard



25 Columbia City Live Aboves - Glazing and rhythm add to street appeal



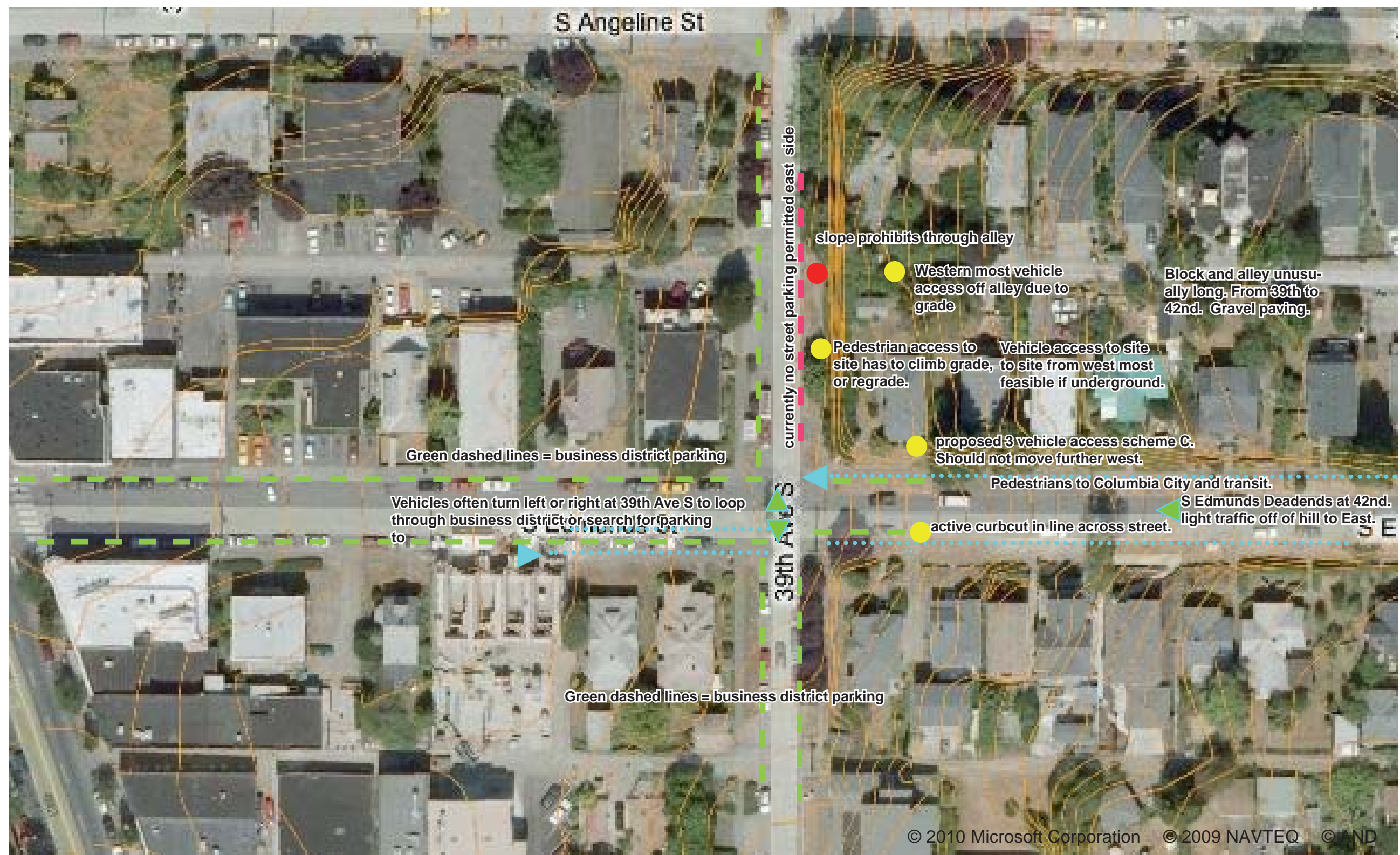
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4d Map of Access Opportunites and constraints



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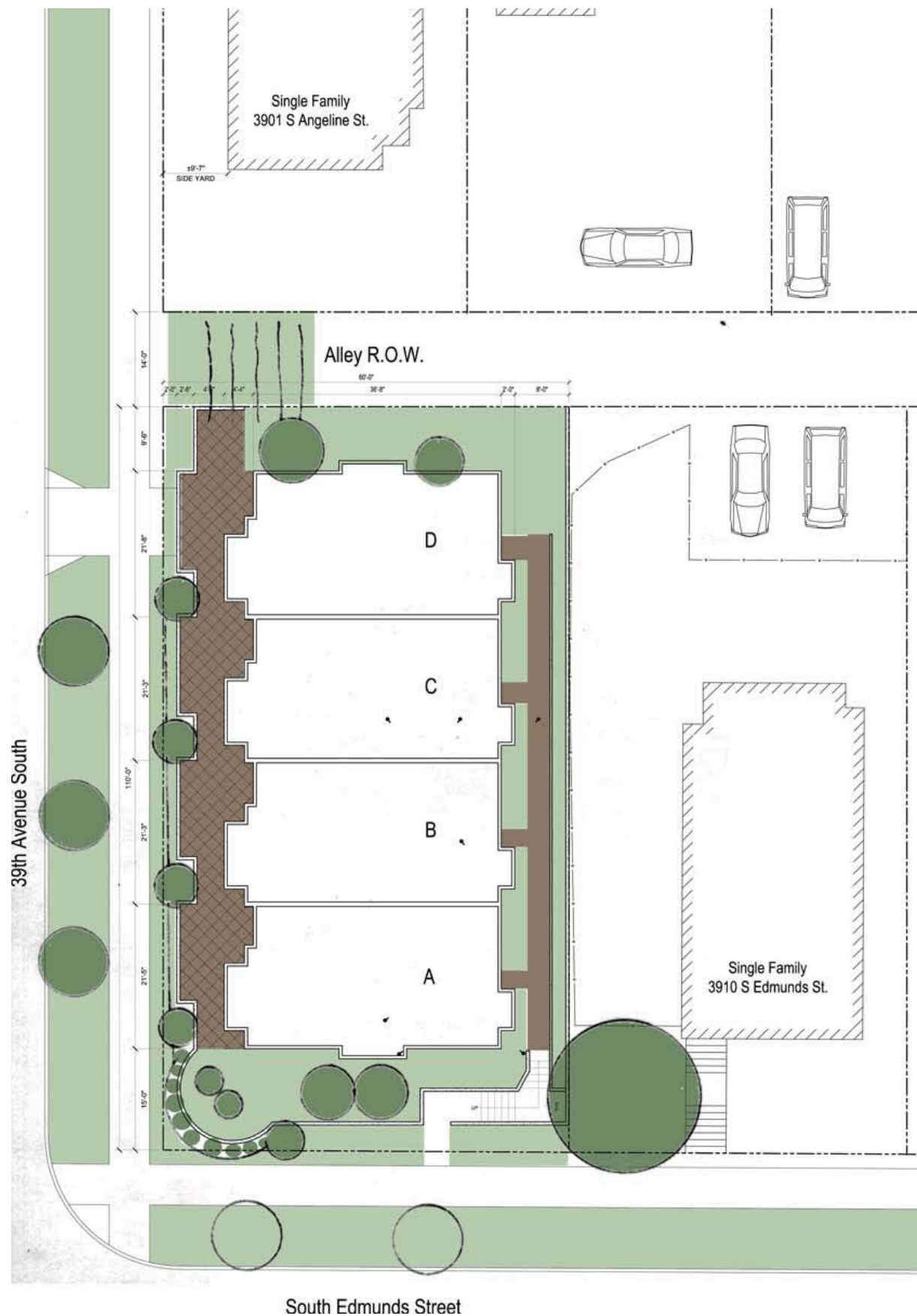
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Pros:

Nice Views for residents. Private and protected.
 Parking is hidden underground.

Cons:

Scheme sits high two stories above the street, and is very defensive relative to the neighborhood. Slope would need to be stabilized at street edge. Unlikely that rockery would provide adequate retainage in the slope required. A tall retaining wall would be needed. Pedestrian entry is very hidden. Typology would work better with stoops. Excavation and grade change make this difficult to achieve and get in parking. At 4, Units would be excessively expensive to both build and sell in this neighborhood. Does not pencil out for purchase price of lot. Excavation required for underground parking would result in disturbance of entire site and require shoring. This would be both disruptive, and would cause the project to not be financially feasible. Forms are mundane. Parking entry is close to existing Alley Right of Way. Not clear how the two would relate and what stabilization of alley slope would be needed.

CODE COMPLIANT **SCHEME A**

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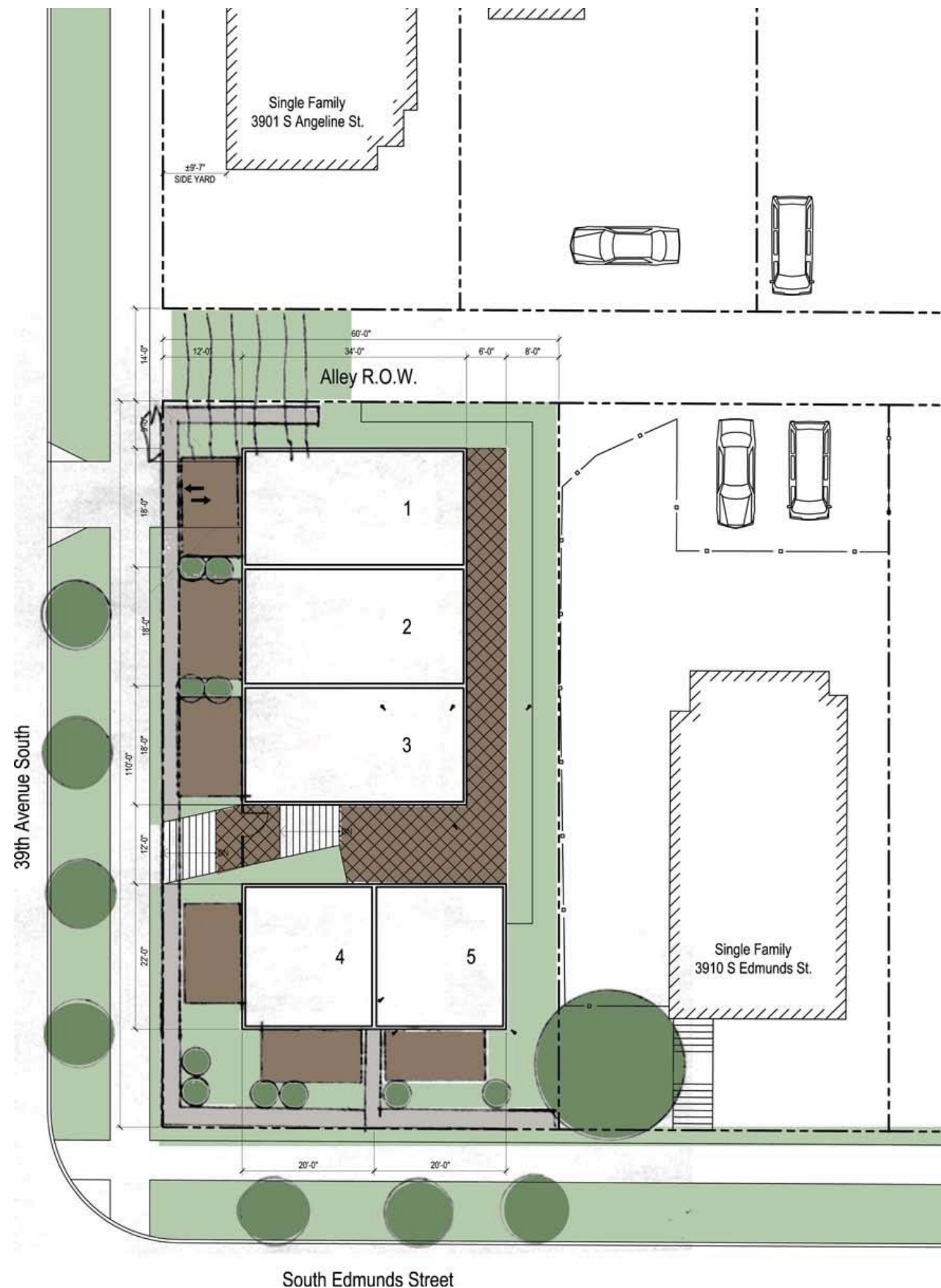
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Pros:

Makes an interesting interior court, and forms a step down with the site to the west.
 Minimizes presence of cars on the street.
 Single pedestrian entry through court is a well-tested site plan arrangement.
 Would not require ADR.

Cons:

Scheme sits high above the street, and is very defensive relative to the neighborhood.
 Slope would need to be stabilized at street edge. Unlikely that rockery would provide adequate retainage in the slope required.
 Does not pencil out for purchase price of lot.
 Excavation required for underground parking would result in disturbance of entire site and require shoring. This would be both disruptive, and would cause the project to not be financially feasible.
 Parking entry is close to existing Alley Right of Way. Not clear how the two would relate and what stabilization of alley slope would be needed.

CODE COMPLIANT - 5 UNITS **SCHEME B**

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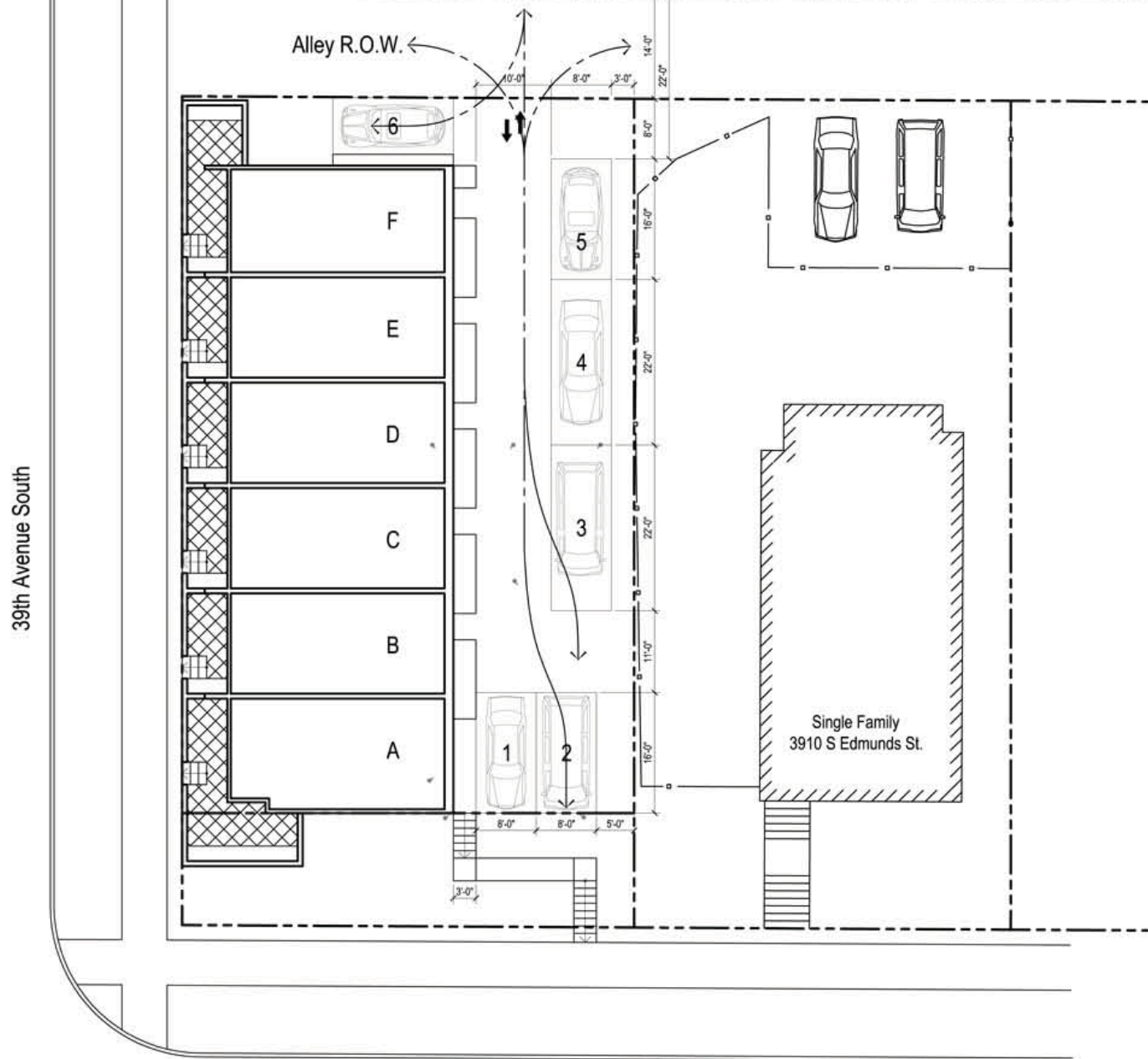


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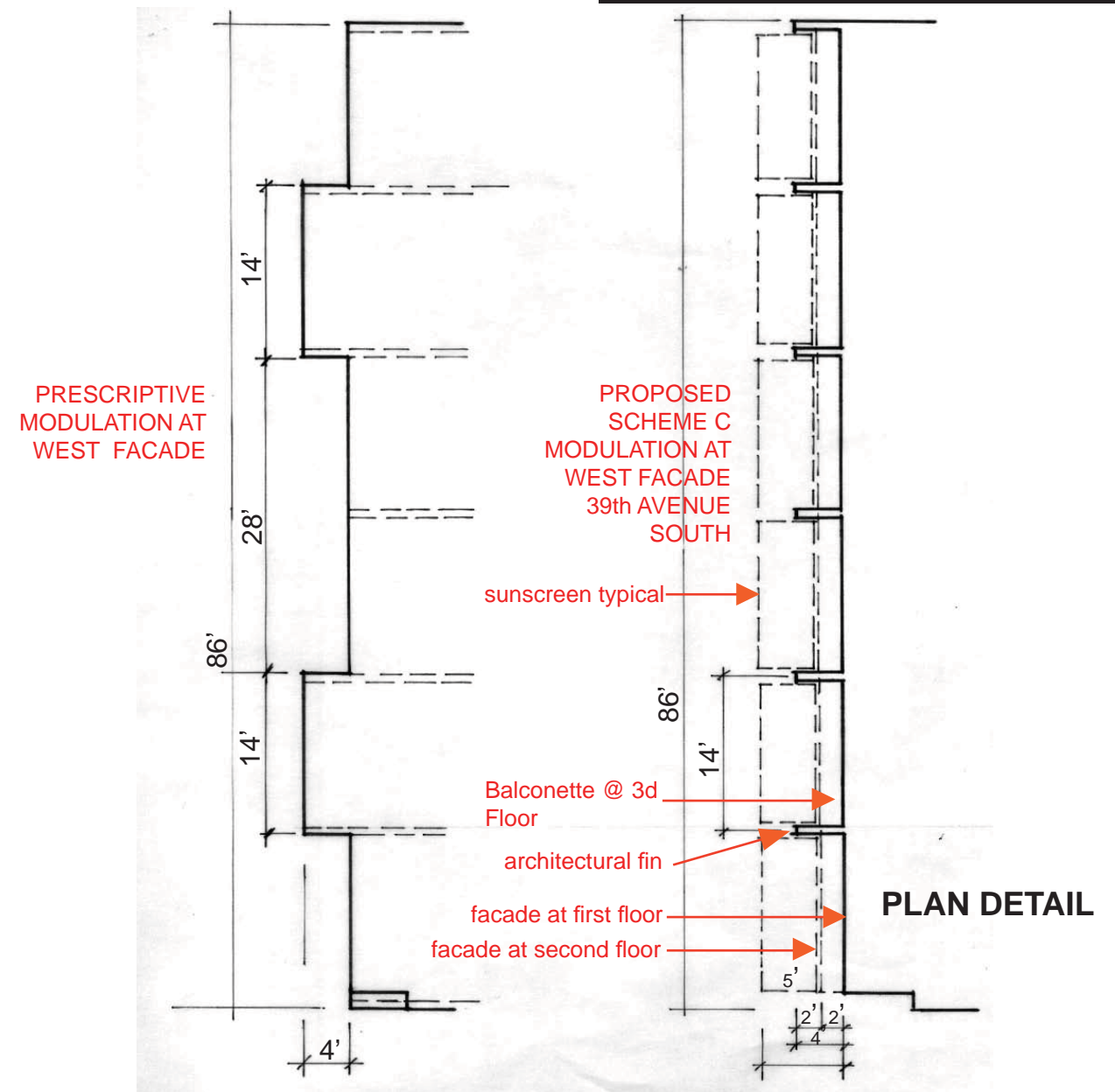


Scheme C proposed parking

Requested Departure:
23.54.C.2. Backing Distance.
 6 vehicles total exceeds 3 parking space limit on lots for backing greater than 50'. However, 3 of proposed 6 spaces back greater than 50', (Spaces 1, 2, and 3). Spaces 4, 5, and 6 do not back greater than 50'.
 This departure fosters multiple uses for parking court.



Modulation diagrams



PRESCRIPTIVE CODE COMPLIANT per 23.45.012.B.
 A minimal amount of interest is required. The code does not have a method for alternate, but equally expressive techniques.

PROPOSED SCHEME C
 The modulation expresses each unit, and provides interesting relief to the elevation through fins, recesses, balconettes, sunscreens, and material changes. See 3 D drawings.

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DEPARTURES REQUIRED - 6 UNITS **SCHEME C** PREFERRED

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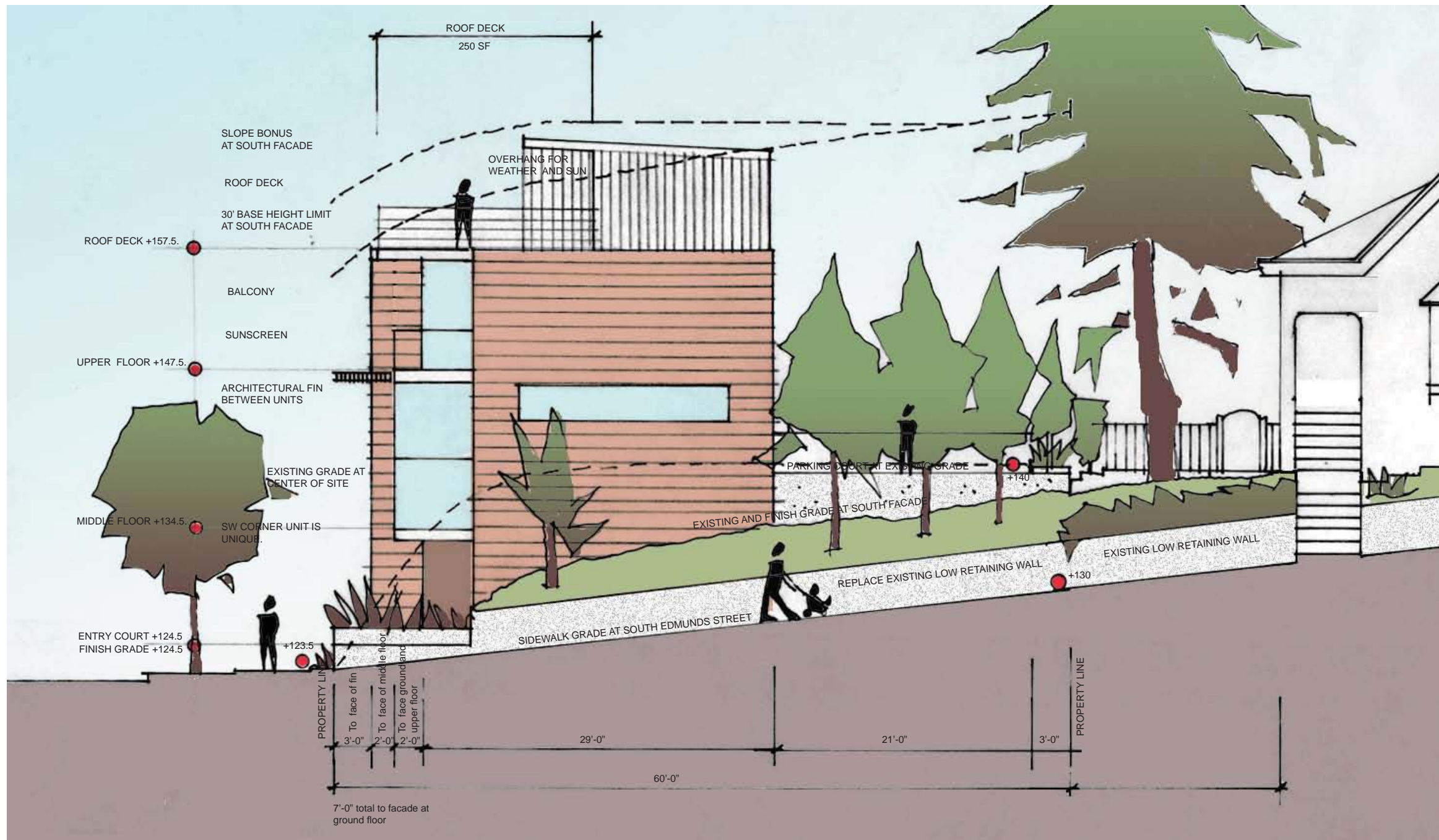
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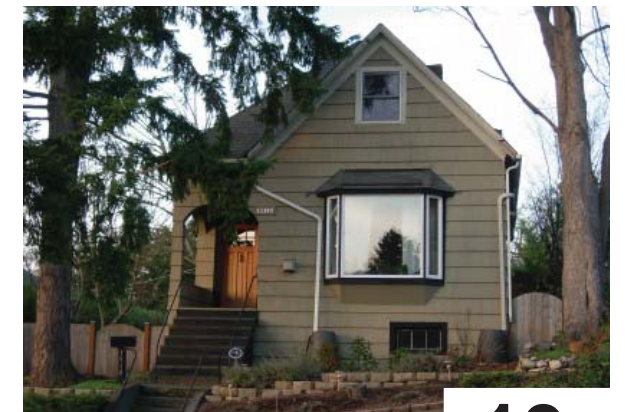
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South Elevation/Section Scheme C

ADJACENT HOUSE



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DEPARTURES MATRIX

CODE COMPLIANT Assumes Front Yard designation on 39th Avenue South							
Section	Location	Allowed/Required	Proposed	Departure	Departure	Comments	
						Assumes street access for underground garage permitted per 23.45.018 a. and b. Negotiations with SDOT in later phase might allow access from North in exchange for alley work.	
CODE COMPLIANT Assumes Front Yard designation on S Edmunds Street							
Section	Location	Allowed/Required	Proposed	Departure	Departure	Comments	
						Assumes street access for underground garage permitted per 23.45.018 a. and b. Negotiations with SDOT in later phase might allow access from North in exchange for alley work.	
PREFERRED Assumes Front Yard designation on 39th Avenue South							
Section	Location	Allowed/Required	Proposed	Departure	Departure	Comments	
23.45.011 A	West Façade @ 39th Avenue South	120' with modulation. 75 feet without.	75.0	85	10.0	13%	Allows urban expression that enlivens 39th Avenue S. Proposed modulation does not meet measurement requirements of prescriptive code, but full modulation is provided. No impact on single family zone to east and north. See Front yard setback below additional points.
23.45.012	West Façade @ 39th Avenue South			7' to façade at first floor. 5' to façade at 2nd floor. 5' to face of balcony at 3d' floor. 7' to façade at 3d floor. 3' to face of architectural "fins". Sunscreens create detail.			Each unit proposed will be fully articulated through glazing, recesses and overhangs, material changes, balconettes, sunscreens, and railings. The corner unit A will receive a different treatment to acknowledge its unique position at the street corner. While modulation is less than full prescriptive depth, there is more of it, and it is richer than required by code. See modulation diagrams, section/elevation, and 3 dimensional drawings for Scheme C.
23.45.014	West Property Line @ 39th Avenue South	5' minimum. 15' maximum	15.0	7' to façade at first floor. 5' to façade at 2nd floor. 5' to face of balcony at 3d' floor. 7' to façade at 3d floor. 3' to face of architectural "fins".	varies		Helps to create vibrant pedestrian oriented street wall along 39th Avenue South where none exists. Minimizes impact of project on adjacent single family properties. Allows greater than required setback between project and single family property to east. of 25'. Helps reduce bulk of project relative to Single Family property to the East. Eliminates impact of tall retaining walls on pedestrian environment. Connects to historic district on other side of 39th Avenue South. There is precedence for the creation of an "edge" to the commercial district in this manner on the two blocks to the south between S Edmunds and S Hudson Streets. There is precedence for a tighter setback along 39th Ave S both in the Single Family house to the north, and on other blocks along 39th Ave S.
23.45.016	West PL and roof	Required 300 SF @ grade for ground related units	300.0	8 x 9 = 72 minimum at street level units B,C,D, E	at grade	76%	Combination of intimate entry court at street level and private deck at roof satisfies urban living needs for privacy and safety while connecting to street through stoops and entries.
				14 x 18 at roof decks = 250 SF	at roof		Required area is not reduced, but articulated in an Urban infill manner that works in the context of the project being on the edge of a Neighborhood Commercial District and Historic Pedestrian oriented district.
		Total combined min.	310		No reduction in total area		Note: pedestrian path through entry court is not deducted from area.
							Larger size for Units A and F of forecourt is not entered into average.



**3904 South Edmunds Street
Seattle, WA. 98118**

harmonica projects LLC

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SUMMARY: HOW PROPOSED SCHEME C MEETS OR EXCEEDS CITY OF SEATTLE GUIDELINES

The goal of the zoning code is to foster design that fits into neighborhoods, creates a positive experience for pedestrians and residents, and stipulates a minimum benchmark for developers and designers to hit in terms of articulate and expressive design that adds to the building stock of Seattle.

Preferred Scheme C, while non-compliant relative to some of the basic design devices stipulated in the code, achieves the high level objectives of the code using alternative methods. The articulation of the west facade exceeds the amount of modulation required for interest and individual unit expression, even though some of the dimensions do not meet prescriptive requirements. Through study of the particulars of the topography and context, the proposal adds to the Columbia City building stock by reflecting the historic core, and giving breathing room to the single family structures up the hill.

The “woonerf” court, with interior spaces of the project connected to it, will be much more than a tight asphalted driveway could be. The departures required for backing distance are in keeping with the long range goals of a more dense, landscaped, and pedestrian oriented City. Pervious paving is planned to allow residents to extend their sense of space and openness to the space to the east. Precious dollars will not be spent on housing the car, but will be concentrated on creating a lively facade, living spaces for people and high quality architectural materials and detailing.

Allowing open space at the roof decks more accurately how people in a dense urban environment want and use private space, without sacrificing the “stoop” connection encouraged in the code. The proposal draws on urban prototypes in use around the world, and is particularly appropriate for a historic neighborhood like Columbia City.



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