



BELROY COURT

EARLY DESIGN GUIDANCE PACKET



PROJECT VISION

Capitol Hill's BelRoy apartment building is widely regarded as one of Seattle's best examples of art deco architecture. It was designed in 1931 by renowned architect and University of Washington professor Lionel Pries. The building adjoins five single-family house lots that together make up the 55,870 sf development site.

The project proposes to preserve and restore the BelRoy apartment building and develop new housing on the adjacent lots consistent with the existing land use pattern and density of Capitol Hill's west slope neighborhood. The existing building and new multi-family structures will be integrated with gracious entry plantings, significant open space, a central courtyard and p-patch. Both old and new structures will be served by an underground parking garage that uses an existing entry on Roy Street. The project's extensive Bellevue Avenue street frontage will become safer and more pedestrian friendly through the removal of the existing driveway curbcuts, new street trees and sidewalk plantings that enhance entries to the new buildings.

The siting, scale and design of the new structures will respect the existing building through appropriate massing and setbacks. To meet the project's sustainability goals and to reduce energy use, the new units are designed to maximize daylighting and natural ventilation.

PROJECT GOALS

The proposed development presents an opportunity to accomplish a number of goals important to the City and the neighborhood. Over the past 9 months, the developer/architect team has met with homeowner associations, neighborhood representatives and historic preservation advocates to help shape our development proposal. These goals include:

HISTORIC PRESERVATION

Retain and respect the BelRoy Apartments.

Under the development proposal, the BelRoy apartment would be proposed for designation as an historic landmark to guide the protection of its most significant historic features. Although highly intact without significant alterations, the exterior is in need of significant repair as are building mechanical systems. Historic preservation strategies will include:

- Preserving the building’s historic exterior features, including cladding and cornice patterns, steel sash windows and entry ornaments
- Restoring the building’s entry court and exterior gardens to improve visibility to the street and the overall streetscape quality
- Upgrading systems to improve energy efficiency and reduce building wear and tear
- Preserving original unit size and layout (27 studios and 24 one-bedrooms, averaging 550 sf) to maintain affordability
- Appropriate siting and massing distribution of new structures to respect historic building



SUSTAINABILITY

Prioritize sustainability in the project design.

The proposed project would advance goals for sustainability through innovative design strategies that reduce energy demand, eliminate construction waste, promote use of shared transportation, and create or restore open space. The project has applied for participation in the City’s Priority Green program, established to advance green building practices and further climate protection. Design and construction strategies will include:

- Preserving the BelRoy to reduce construction waste and new construction costs
- Reducing energy demand through system upgrades and new building design that maximizes daylight and natural ventilation for all units
- Reducing and/or eliminating conditioned circulation space and vertical transportation
- Capturing rainwater and reducing stormwater through reduction of impervious surfaces and creation of generous landscaped areas
- Creating underground parking with allocation of “car-share” stalls and secure bicycle storage
- Eliminating four existing single-driveway curbcuts and plant street trees on Bellevue Avenue
- Partnering with WashDOT to restore and protect vegetated slope that adjoins the site to the west



A GREAT URBAN NEIGHBORHOOD

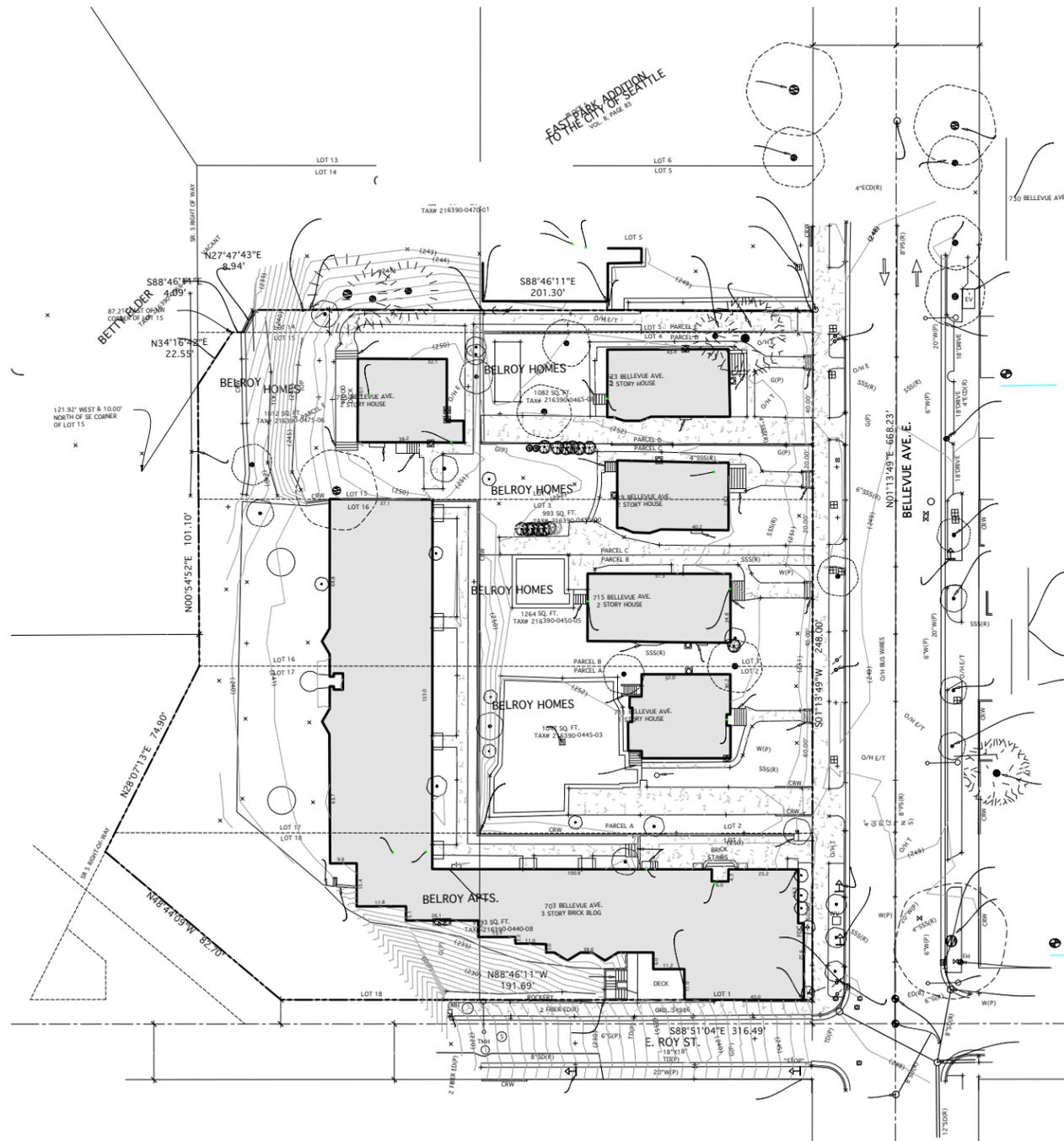
Create liveable urban density.

The project’s context is characterized by a diversity of housing types, reflecting the last 100 years of the neighborhood’s growth and development. Within close proximity to the project site are single-family homes (many now divided into rental units); three to four-story brick buildings from the early 1900’s; “modern” apartment buildings built as temporary housing for the 1962 World’s Fair; and a number of high-rise condominium towers.

The proposed project has been informed by the variety of existing building types, the midrise character of the West Slope neighborhood, feedback from the neighborhood representatives, and the need to achieve an economically viable project in the context of historic preservation, open space creation and an improved streetscape. The project accomplishes these objectives by:

- Massing and height arranged in a manner that is respectful of neighborhood context and site planning objectives
- Creating residential units at a density that is similar to the existing land use pattern of the neighborhood
- Achieving long-term residency through high-quality construction and on-site open space
- Locating parking below-grade, the elimination of four curb-cuts on Bellevue Avenue and street tree planting
- Preserving mature native tree species on the site’s north boundary
- Creating green view corridors into the site in the tradition of Capitol Hill’s earlier era of garden court-style housing





Existing Conditions

The site is located at the northwest corner of Bellevue Avenue East and East Roy Street. The west portion of the site is a wooded hillside. Interstate 5 lies to the west, and the site looks out over downtown, the Olympics and Seattle Center.

The project site is approximately 55,870 square feet and is made up of six platted lots.

Existing Buildings

The BelRoy Apartments has been identified as being architecturally significant, and its presence contributes to the history and character of the neighborhood. The five rental houses are in poor condition and have been altered and degraded over time. They are also out of alignment with the density designated by zoning and the Comprehensive Plan.

Topography

The site is generally flat across the occupied portions along Bellevue Avenue E. but slopes downward dramatically to the west – roughly a 20' drop in elevation - where the cut was made for the construction of Interstate 5.

Parking

A small parking garage serving the apartments daylights along the Roy Street slope, with 12 usable parking spaces below grade. The existing house lots are served by four curb cuts and driveways along Bellevue Avenue East.

Streets and Transit

Bellevue Ave E. is an arterial that runs north-south along the west edge of Capitol Hill. To the north, it provides a connection to both South Lake Union (via Belmont Ave) and north Capitol Hill, while to the south it provides a direct link to downtown as well as Capitol Hill's South Anchor District and the East Olive Way commercial corridor. To the east, Roy street connects directly to Broadway E. To the west, the Melrose Trail provides bicycle and pedestrian access to South Lake Union and the Eastlake neighborhood via Lakeview.

The site is well-served by public transit, with metro bus route #14 running southbound along Bellevue Ave. E. and northbound along Summit Ave. E. In addition, Metro bus routes 9, 49 & 60 run along Broadway E., which is within walking distance to the east.

Neighborhood Amenities

The site is near the full range of urban infrastructure and amenities. In addition to the restaurants, bars, cafes, retail and commercial establishments that are within walking distance of the site, there are 2 urban "pocket parks" located nearby: Tashkent Park at Mercer and Boylston; and the Thomas Street Park on the corner of Thomas and Bellevue, 4 blocks south of the site.

Neighborhood Plan

The site is within the Capitol Hill Urban Center Village. It is the only area that is not designated as a subarea of the Urban Center Village, but is just north of the West Slope District.



BelRoy Apartments



Rental Houses



Driveways at Bellevue Avenue E

NEIGHBORHOOD CONTEXT

Capitol Hill Urban Center Village has all the infrastructure needed for excellent residential living. The site is well served by transit, bike routes, and commercial uses.

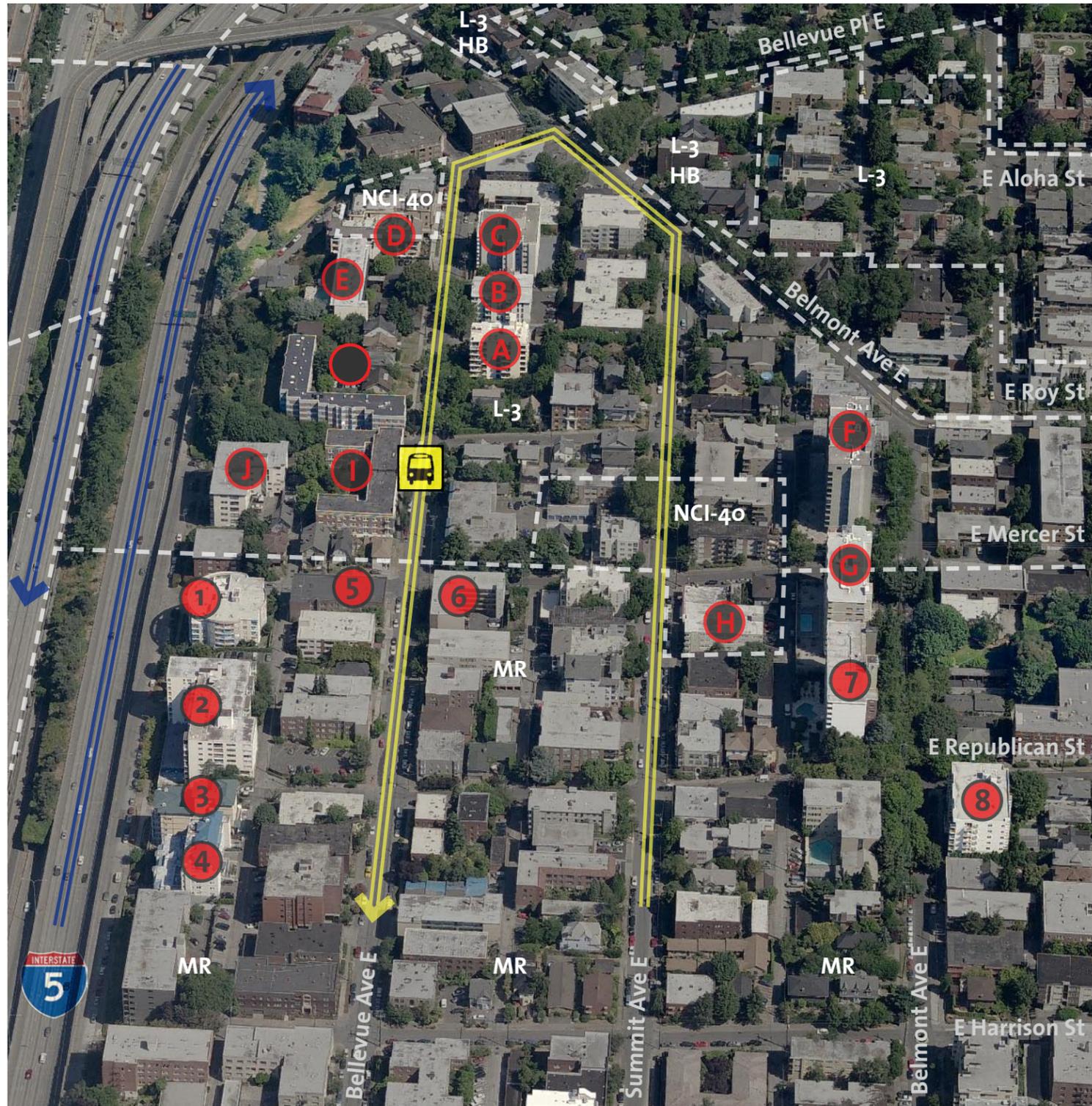


Commercial Use & Public Spaces
A full range of neighborhood amenities are within walking distance



Residential Building Types
Diverse building types add interest to the neighborhood





The site is zoned L3, and lies within the Capitol Hill Urban Center Village. There is an NC1-40 zone on the north end of the block, and another NC1-40 zone at the nearby intersection of Summit Avenue East and East Mercer Street. To the north of Belmont Avenue East is the Harvard Belmont Historic District. The entire area south of East Mercer Street is zoned Midrise (MR).

The existing urban form of the vicinity is highly varied in terms of height and density. We are seeking a contract rezone to Midrise, which will achieve the key project objectives and is consistent with the existing pattern of development surrounding the site. Only a small portion of the site would include the height allowable in MR zoning.

PROJECT SITE

- Belroy Apartments**
703 Bellevue Ave E
Stories: 3/4
Height: 34' +/-

BUILDINGS IN MR ZONE

- 1 Jackson Court Apts**
530 Melrose Ave E
Stories: 7
Height: 80' +/-
- 2 Melrose Terrace Apts**
308 E Republican St
Stories: 11
Height: 100' +/-
- 3 The Highmark Condos**
311 E Republican St
Stories: 9
Height: 90' +/-
- 4 420 Melrose Condos**
420 Melrose Ave E
Stories: 10
Height: 90' +/-
- 5 Marwood Apartments**
580 Bellevue Ave E
Stories: 3/4
Height: 30' +
- 6 Hillree Manor**
401 E Mercer St
Stories: 6
Height: 50' +/-
- 7 Lamplighter Condos**
505 Belmont Ave E
Stories: 11
Height: 100' +/-
- 8 Belmont Court Apts**
424 Belmont Ave E
Stories: 7
Height: 75' +/-

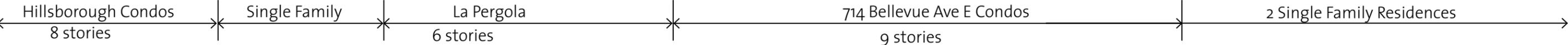
BUILDINGS IN L-3/NC ZONE

- A 714 Bellevue Ave E Condos**
714 Bellevue Ave E
Stories: 9
Height: 89.5' +/-
- B La Pergola**
730 Bellevue Ave E
Stories: 6
Height: 50' +
- C Hillsborough Condos**
740 Bellevue Ave E
Stories: 8
Height: 65' +/-
- D Bellagio On Capitol Hill Condos**
745 Bellevue Ave E
Stories: 4
Height: 50' +/-
- E Mark Spencer**
727 Belmont Ave E
Stories: 4-5
Height: 40' +
- F Shannon Condos**
601 Belmont Ave E
Stories: 12
Height: 102' +/-
FAR: approx 3.3
- G The Highlander Condos**
525 Belmont Ave E
Stories: 11
Height: 94.5' +/-
- H Arcadian Court Condos**
511 E Roy St
Stories: 5
Height: 50' +/-
- I Roy View Apartments**
615 Bellevue Ave E
Stories: 3/4
Height: 30' +
- J Mavis Condos**
301 E Roy St
Stories: 5
Height: 50' +/-

NEIGHBORHOOD CONTEXT: STREETScape



STREET LEVEL VIEW OF SITE (LOOKING WEST)



STREET LEVEL VIEW OPPOSITE OF SITE (LOOKING EAST)

NEIGHBORHOOD CONTEXT: PARKING

Garage entries and surface parking detract from the current streetscape.

PARKING SOLUTIONS IN THE EXISTING NEIGHBORHOOD

Parking and garage entries have a major impact on Bellevue Avenue East. Parking entries, often multiple entries, detract from the streetfront. This is particularly true for lower scale units built since mid-20th century.

Existing L3 zoning in the Capitol Hill Urban Center Village requires one parking space per unit. At 1 unit per 800 sf density, parking below grade is not affordable; an analysis of existing neighborhood parking strategies bears this out. Parking is either in a surface lot, or on the surface below the building. Such parking strategies in the neighborhood and throughout the city have led to unfortunate street-level treatment. In urban center village neighborhoods, using the ground for cars and parking access places too much emphasis on vehicles, does not foster a pedestrian-friendly street experience, and is a poor use of land in an urban neighborhood.

Finding a design solution that allows for below grade parking is critical to the project. Giving over the ground plane and the pedestrian realm to vehicle access and building services does not meet the City or the Neighborhood Guidelines.

Our preferred alternative:

- Removes vehicle access from Bellevue Avenue, and allows full use of the streetfront and site for landscaping and pedestrian uses.
- Reuses the existing curb cut on Roy Street for parking access
- Locates all parking below grade
- Establishes a safe and attractive pedestrian realm through new landscaping, building entries and views into and through the site



SITE: EXISTING BELROY APARTMENT BUILDING

The project builds on the historic character and unique design of the BelRoy.

The BelRoy Apartments, constructed in 1931, is well known in Seattle's architectural circles. Bain & Pries is the architect, with the design attributed to Lionel Pries, a highly regarded and influential designer and educator.

The BelRoy was recognized as one of the first modernist buildings in Seattle, with flat geometric lines and zizagging floor elevations that capture sweeping views to the south and the west. The BelRoy is noted by Jeffrey Ochsner, in his 2008 book, *Lionel H. Pries, Architect, Artist, Educator: From Arts and Crafts to Modern Architecture*. The BelRoy is also cited as "one of the best examples of Art Deco design in Seattle" by architect and journalist Peter Staten.

The BelRoy has 52 units, some as small as 350 sf. The plan of the BelRoy is unusual - instead of a single main lobby and double loaded corridor, the BelRoy has seven exterior entries with walk-up units clustered around stairs. This plan strategy allows all but a few of the units to be through-units, with natural ventilation and daylight from at least two sides.

The building is entered from a garden entry off Bellevue Avenue East, and a circulation path along the north and east edges of the building serves the entry stairs. The BelRoy building height measures 34.8 feet above the sidewalk at the southeast corner. There is a small parking area below grade, entered off of Roy Street.

To the west of the BelRoy is a steep wooded slope. The current topography was created by the construction of Interstate 5. The sloped areas are overgrown with both native and invasive species, and need to be cleaned up for both aesthetic and safety purposes.

The BelRoy is not currently landmarked and is in need of maintenance. The project aspires to:

- repoint the brick skin
- repaint exterior and interior
- rehabilitate the windows
- widen the garage entry
- make structural improvements
- repair the roof
- update the building systems



DATE:
September 9, 2009

LOCATION:
711 Bellevue Ave E, Seattle WA

PROJECT:
BelRoy Court

ZONING: MR

PROPERTY INFORMATION

55,870 per survey.

EXISTING ZONING

Existing zoning is L3. The site is in the Capitol Hill Urban Center Village. The project proposes a Contract Rezone to MR.

PERMITTED USES

Table A: Multi-family structures are permitted outright
23.45.110A.1 Street-level commercial uses are permitted within a one-block radius of a commercial zone.
23.45.110C.2 Maximum floor area per business enterprise is 4,000 sf. Our potential use is below 2,000 sf. 23.45.110C.4 Parking required per 23.54

GENERAL DEVELOPMENT STANDARDS

23.45.006 K1. Solid waste and recyclable materials. For multi-family structures of 51-100 units, 200 sf of storage space required. Front-load containers.
K2. Minimum dimension of storage space is 6 feet, width and depth.

MIDRISE STRUCTURE HEIGHT

A. 60 feet
D.3. Rooftop features including mechanical equipment, sun and wind screens, penthouse pavilions, greenhouses may extend 10 ft above the maximum limit so long as the area does not exceed 15% of the roof area or 20% if the total includes screened mechanical equipment.
Solar collectors may exceed the height limit per Section 23.45.146.C2. The combined total coverage of all rooftop features shall not exceed 15% if the total includes screened mechanical equipment.

MIDRISE STRUCTURE WIDTH AND DEPTH

A.1. The maximum width of a structure on a lot when the front façade is not modulated according to the standards of Section 23.45.054 C shall be 40 feet
A.2 When the front façade is modulated according to the standards of Section 23.45.054 C, the maximum width of each structure on a lot shall be one hundred fifty feet.
B. 1. c. Apartments: 65% of the lot depth
2. Exceptions to Maximum Depth Requirements - lot depth can exceed 65%, subject to the following conditions:
a. Total lot coverage shall not be greater than that which would have been possible with standard development requirements for maximum width, depth and setbacks.
b. When lot area is over 7,000 sf, required amount of usable open space shall be increased to 30% of lot area. Not more than 1/3 may be provided above ground as decks or balconies. $55,870 \times .3 = 16,761$ sf
c. Structure depth shall in no case exceed 150 ft.
d. Structures with depth great than 65% of lot depth shall be modulated along the side setbacks according to 23.45.054 Midrise Modulation Requirements
D.1. Required setbacks for Cluster Developments
Where 2 or more principal structures are located on a lot, the required setback between those portions of interior facades which face each other shall be as follows:
Length of facing portion of facades Setbacks
40' or less 15' ave 15' min
41 to 60' 20' ave 15' min
61 to 80' 25' ave 15' min
81 to 100' 30' ave 15' min
101 to 150' 40' ave 15' min
150' or more 50' ave 15' min

23.45.054 MIDRISE MODULATION REQUIREMENTS

A.1. Front facades - required if over 40 feet in length
B. Side facades - required if over 65% of lot depth
D. Modulation standards. Minimum depth = 8 ft. When balconies are part of modulation, they must have a min. depth of 6 ft and a min. area of 60sf. Min. width of modulation is 10 ft. Max. width of modulation is 40 ft (with exceptions)

MIDRISE - SETBACK REQUIREMENTS

A. Front setback = average of the setbacks of the first principal structure on either side. Front yard setback not required to be more than 5 ft than the setback of the first principal setback on either side which is closer to the front lot line, and in no case be required to exceed 15 ft.
B. Rear setback shall be either 10 feet, with modulation per 23.45.054 or an average of 15 feet, provided that no part of the setback shall be less than 10 ft.
C. Side setbacks per Table 23.45.056A For a building over 51 ft in height, 156-165 feet long, average setback = 23.5 ft, minimum setback = 10 feet.
D.2f. Underground structures are permitted in all setbacks.
3. Front and rear setbacks on lots containing certain environmentally critical areas or buffers may be reduced pursuant to Sections 25.09.280 and 25.09.300. Note: These sections do not appear applicable.

MIDRISE - SCREENING & LANDSCAPING

A. Quantity - Minimum landscaped area that is equivalent in square footage to 3 times the total length of all property lines
Landscape Area = Total Perimeter x 3' = $807\text{'-}6'' \times 3 = 2,422\text{'-}6''$ sf
A4. Street trees required in planting strip per SDOT standards
B. Landscaping at least 3 ft deep shall be along street property lines

MIDRISE - OPEN SPACE REQUIREMENTS

A. Quantity. 2. Apartments. A minimum of 25% of the lot area shall be provided as usable open space at ground level, except that up to one third of that area may be above grade in the form of balconies or decks if the required open space is increased to 30% of lot area.
B. Development Standards - Apartments. No horizontal dimension for required ground-level open space shall be less than 10 feet. Permitted in front, side or rear of the structure.
B.3. Development Standards, Apartments. d. Balconies and decks must have minimum horizontal dimension of 6 ft, and min area of 60 sf. E. For cluster development, at least 20% of the required open space shall be in one contiguous area.

MIDRISE - LIGHT & GLARE STANDARDS

A. Exterior lighting to be shielded and directed away from adjacent properties

MIDRISE - PARKING AND ACCESS

A. Quantity per 23.54 (below)
B. Access - access to parking shall be from the street when the lot does not abut a platted alley
C. 1 Parking shall be located on the same site as the principal use.
C.2 Parking may be located in or below any structures but must be screened from direct street view. C3 parking may be located outside a structure per provisions. Note: Preferred option does not have parking outside structure.

GROUND FLOOR USES IN MR

A. Location - In midrise zones, ground floor business and commercial may be located only within a one block radius of a commercial zone
B. Permitted uses include sales and services, medical, restaurants, business support services, offices, food processing/crafts, and retail.
C.2. Maximum gross floor area for any 1 business enterprise shall be no greater than 4,000 sf.
C4. Parking per chapter 23.54

REQUIRED PARKING

A. Minimum parking requirements as set forth in Chart B for residential uses
From Chart B:
Q. Multifamily structures within multifamily zones in the Capitol Hill Urban Center Village - one space for each unit

D. Parking waivers for non-residential use.3. In all other zones (other than commercial and pedestrian designated zones) no parking is required for the first 2500 sf of nonresidential uses in a structure (with non-applicable exceptions)
J. Existing legal parking deficits of legally established uses are allowed to continue
K2. If covered automobile parking provided, all long-term bicycle parking shall be covered.
From Chart E:
- (D2) Multi-family: 1 long-term space per 4 DU, no short-term space required

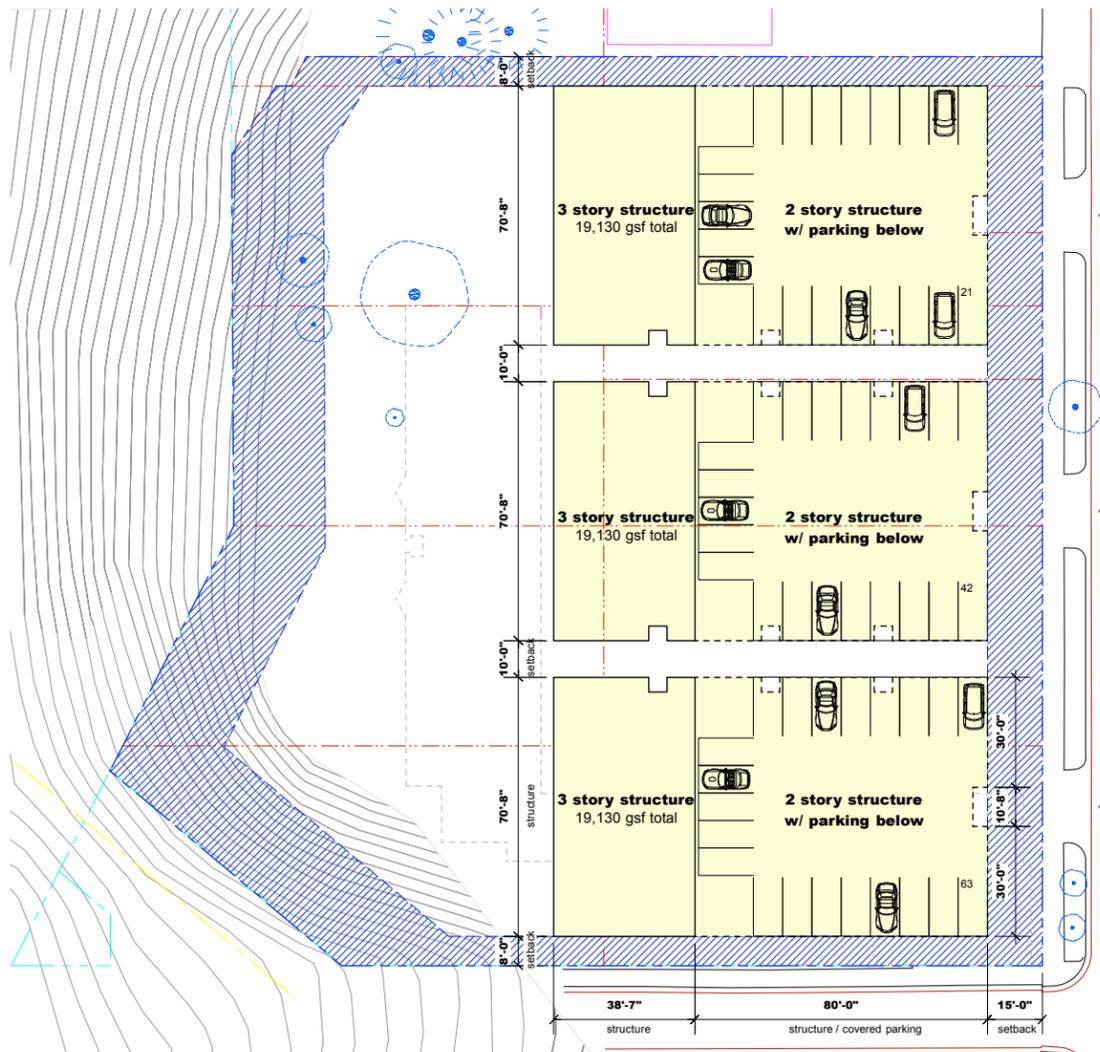
PARKING SPACE STANDARDS

A. Parking space dimensions:
- When angle is 90°: large space: 8'-6" x 19'; medium space: 8' x 16'; small space: 7'-6" x 15'
- When angle is 0°: medium space: 8' x 20'
- Barrier-free space: 8' minimum width with 5' side aisle; 8' side aisle if van-accessible space
A4b. Minimum length of 19' for barrier-free parking for at least one space, with lengths of other spaces apportioned by parking space size
A6. Columns or other structural elements may encroach max 6" except in car door opening area (see Exhibit A)
B1b. For residential use, when >5 spaces, minimum 60% to be medium; minimum parking space dimension shall also be the maximum
D1e. Minimum driveway width for residential uses with >30 spaces and two-way traffic to be 20'
E2. Minimum aisle widths per largest vehicles served by that aisle: 24' for van
G1. For two-way driveways up to 22' wide, provide a sight triangle on either side of drive clear of obstructions for a distance of at least 10'
G4c. When withing 10 feet of property line, site triangle may be reduced to 5' as depicted in 23.54.030 Exhibit F

DEFINITIONS

“Cluster development” means a development containing 2 or more principal structures on one lot. Cottage housing not considered a cluster development.
“Open space, usable” means an open space that is of appropriate size, shape, location and topographic siting so that it provides landscaping, pedestrian access or opportunity for outdoor recreational activity.

OPTION 1: CODE COMPLIANT L-3 WITHOUT BELROY



Program:

69 residential units, double loaded
69 parking spaces on grade

Description

BelRoy removed. Three individual buildings massed along Bellevue Avenue E with at-grade parking occupying approximately 2/3 of each building's ground level. Three curb cuts on Bellevue Avenue E. Open space to the west of the buildings.

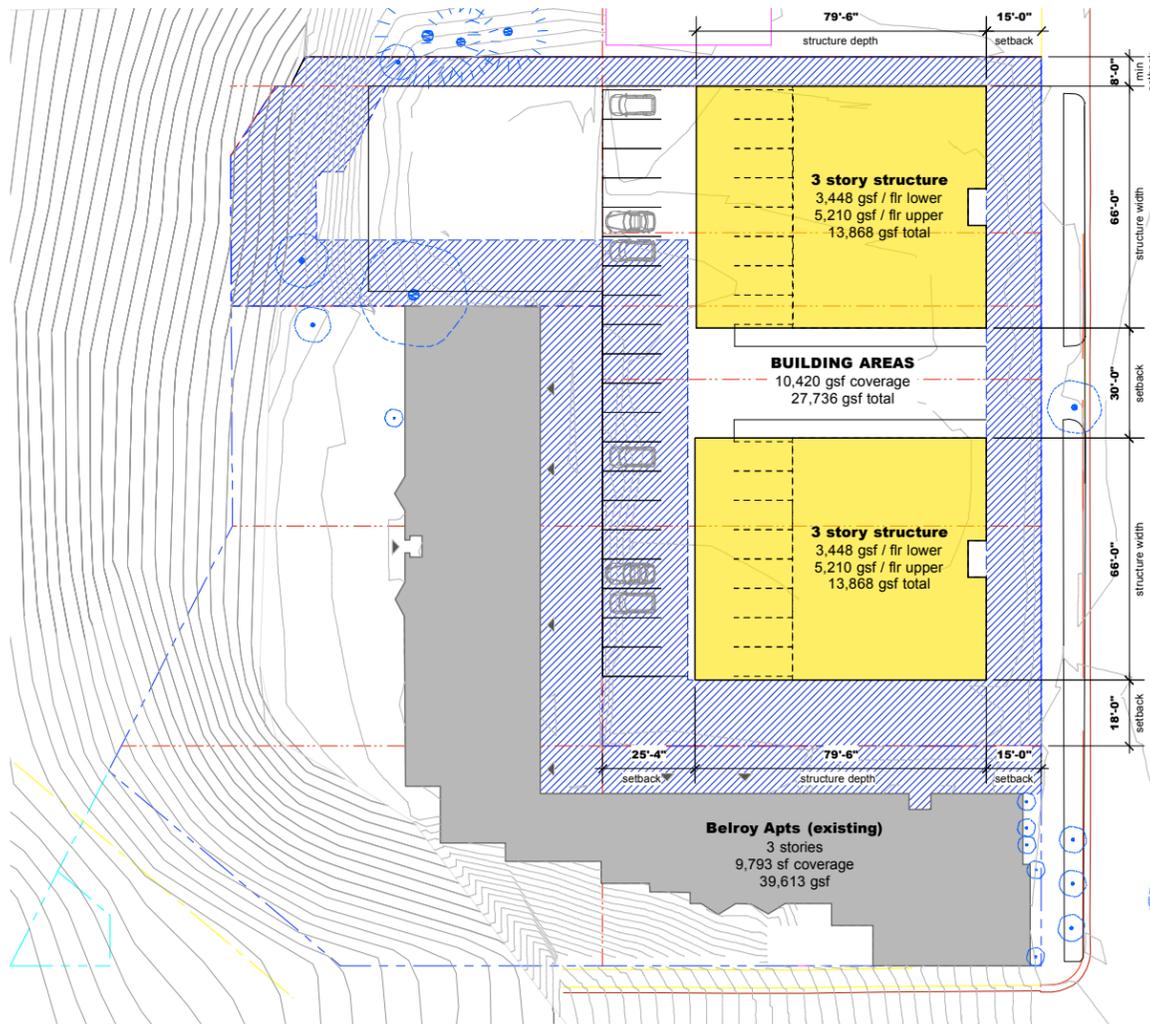
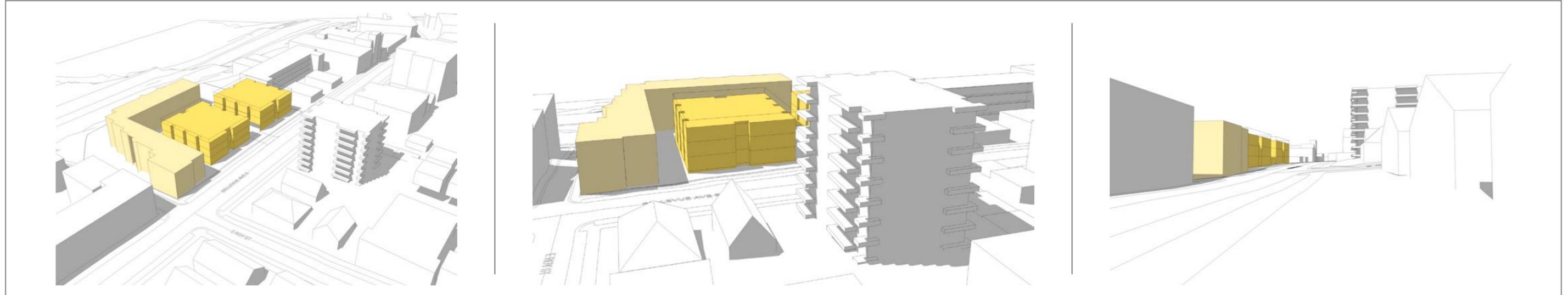
Advantages

- Meets current L3 zoning and land use code
- Holds street edge along Bellevue
- 69 larger units allowed

Challenges

- Demolishes the BelRoy
- Requires three curb cuts; garage doors on street
- Little opportunity for green space
- Double-loaded units with little light or view
- Poor solar orientation
- No sense of open space visible from street

- DOES NOT MEET HISTORIC PRESERVATION GOALS
- DOES NOT MEET SUSTAINABILITY GOALS
- DOES NOT MEET URBAN DENSITY & QUALITY GOALS



Program:

52 units in BelRoy retained; 36 new units
36 new parking spaces provided

Description

Two buildings along Bellevue Avenue E. Surface parking, partially below buildings, accessed from one curb cut off Bellevue Ave E. Open space on west portion of site

Advantages

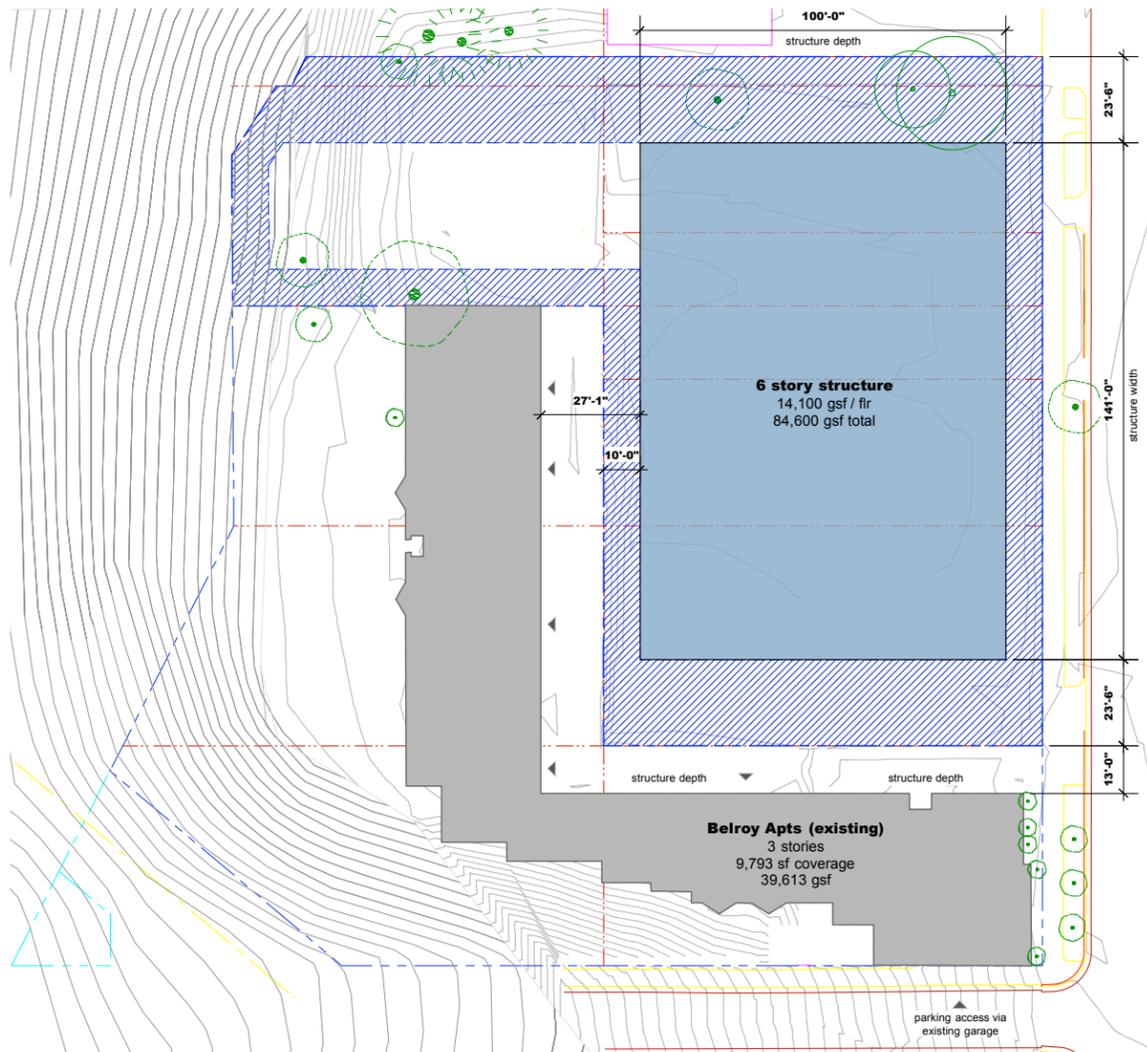
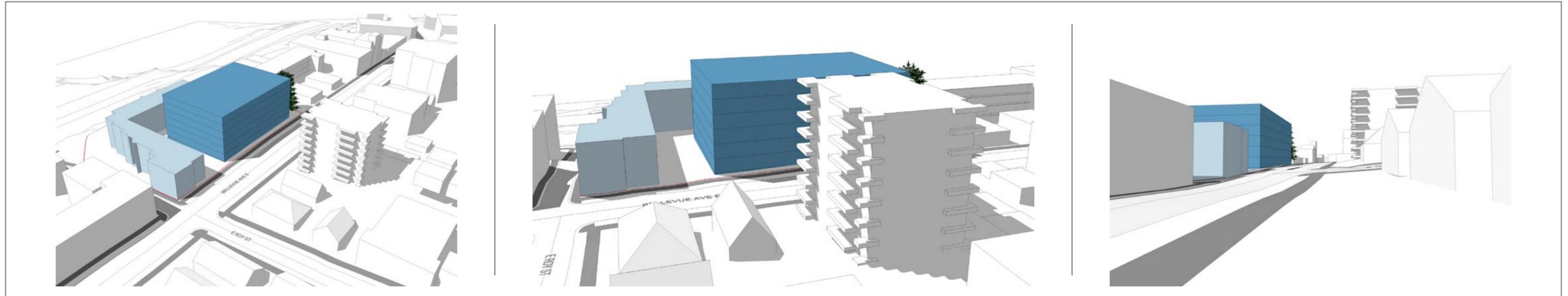
- Does not demolish BelRoy
- Meets curen L3 zoning and land use code

Challenges

- Lower density than existing neighborhood (36 units allowed)
- Site plan not advantageous for daylighting or ventilation
- At grade parking poor use of land
- BelRoy remains a separate entity, without shared systems and opportunity for upgrades
- Curb cut on Bellevue Avenue
- Double-loaded units with little light or view
- Poor solar orientation
- Project not economically viable

- **PARTIALLY MEETS HISTORIC PRESERVATION GOALS**
- **DOES NOT MEET MOST SUSTAINABILITY GOALS**
- **DOES NOT MEET URBAN DENSITY & QUALITY GOALS**

OPTION 3: CODE COMPLIANT MR WITH BELROY



Program:

Lots split between BelRoy and new construction
 52 units in BelRoy retained; 70 new units
 70 below grade parking spaces

Description

Structure that meet MR zoning for the site, with a six story building meeting setback and open space requirements.

Advantages

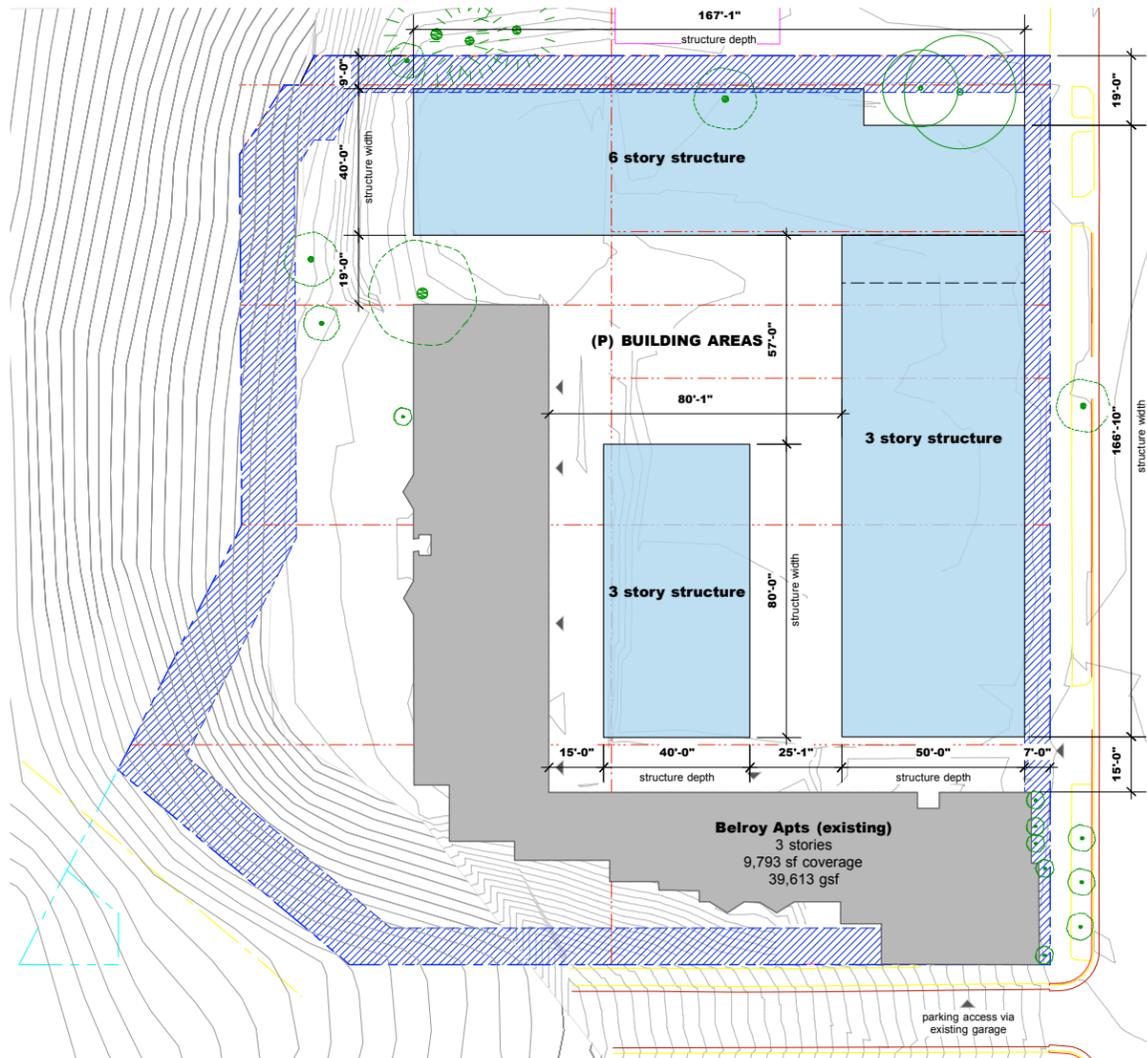
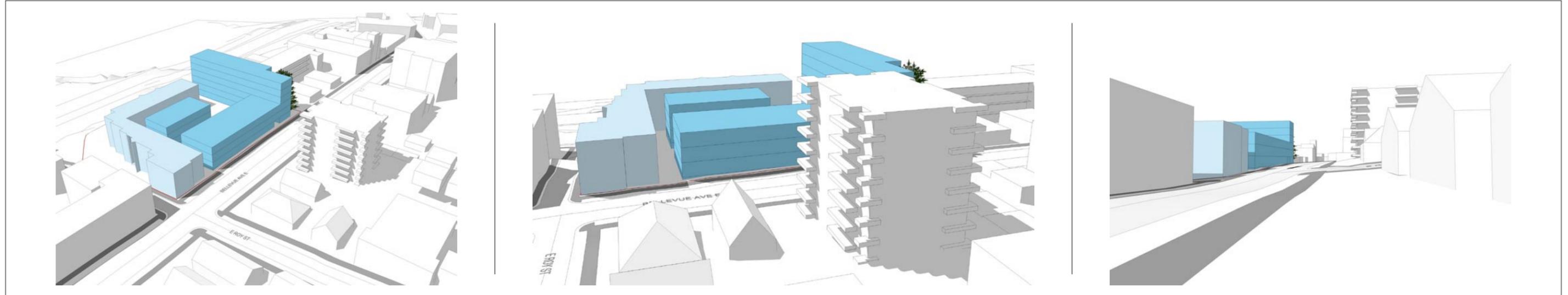
- Achieves urban density
- Below-grade parking

Challenges

- Blocks light and air along Bellevue
- Requires a Contract Rezone
- Site plan not advantageous for daylighting or ventilation

- PARTIALLY MEETS HISTORIC PRESERVATION GOAL
- DOES NOT MEET SUSTAINABILITY GOALS
- MEETS URBAN DENSITY GOALS;
 URBAN QUALITY COMPROMISED

OPTION 4: COMBINED L₃/MR WITH BELROY (PREFERRED)



Program

60 units - through units
Approximately 70 parking spaces below grade

Description

Two buildings: an "L" shaped footprint, with 6 narrow story building along the north, and 3 story building along Bellevue Avenue East; a 12-unit, three story building located between the existing building and the Bellevue Avenue building. The density would be consistent with the historic pattern of development and neighborhood goals.

Advantages

- Preserves BelRoy
- Achieves urban density
- Consistent with neighborhood height and density
- Units have natural ventilation and light from at least 2 sides.
- Only 60' portion of building is along north portion of site, representing 15% of site's street frontage.

Challenges

- Requires a Contract Rezone
- Requires departures from land use code

- MEETS HISTORIC PRESERVATION GOALS
- MEETS SUSTAINABILITY GOALS
- MEETS URBAN DENSITY & QUALITY GOALS

OPTION COMPARISON



OPTION 1: CODE COMPLIANT L-3 WITHOUT BELROY

Program

69 residential units, double loaded
69 parking spaces on grade

Description

BelRoy removed. Three individual buildings massed along Bellevue Avenue E with at-grade parking occupying approximately 2/3 of each building's ground level. Three curb cuts on Bellevue Avenue E. Open space to the west of the buildings.

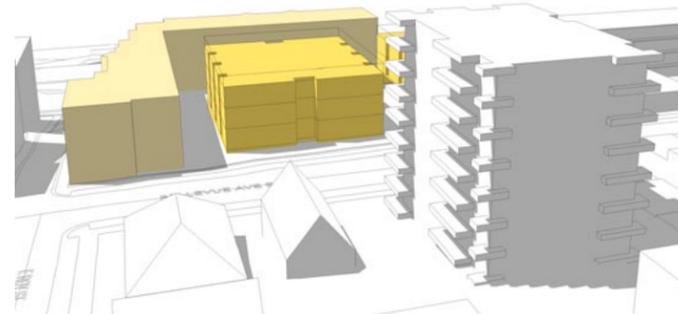
Advantages

- Meets current L3 zoning and land use code
- Holds street edge along Bellevue
- 69 larger units allowed

Challenges

- Demolishes the BelRoy
- Requires three curb cuts; garage doors on street
- Little opportunity for green space
- Double-loaded units with little light or view
- Poor solar orientation
- No sense of open space visible from street

- DOES NOT MEET HISTORIC PRESERVATION GOALS
- DOES NOT MEET SUSTAINABILITY GOALS
- DOES NOT MEET URBAN DENSITY & QUALITY GOALS



OPTION 2: CODE COMPLIANT L-3 WITH BELROY

Program

52 units in BelRoy retained; 36 new units
36 parking spaces provided

Description

Two buildings along Bellevue Avenue E. Surface parking, partially below buildings, accessed from one curb cut off Bellevue Ave E. Open space on west portion of site.

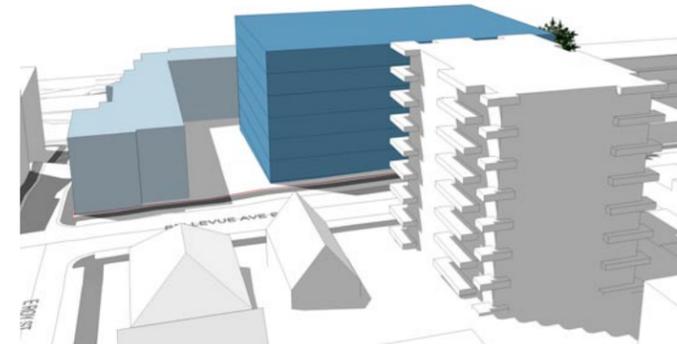
Advantages

- Does not demolish BelRoy
- Meets current L3 zoning and land use code

Challenges

- Lower density than existing neighborhood (36 units allowed)
- Site plan not advantageous for daylighting or ventilation
- At grade parking poor use of land
- BelRoy remains a separate entity, without shared systems and opportunity for upgrades
- Curb cut on Bellevue Avenue
- Double-loaded units with little light or view
- Poor solar orientation
- Project not economically viable

- PARTIALLY MEETS HISTORIC PRESERVATION GOALS
- DOES NOT MEET MOST SUSTAINABILITY GOALS
- DOES NOT MEET URBAN DENSITY & QUALITY GOALS



OPTION 3: CODE COMPLIANT MR WITH BELROY

Program

52 units in BelRoy retained; 70 new units
70 below grade parking spaces

Description

Structure that meets MR zoning for the site, with a six-story building meeting setback and open space requirements. Parking access off Roy.

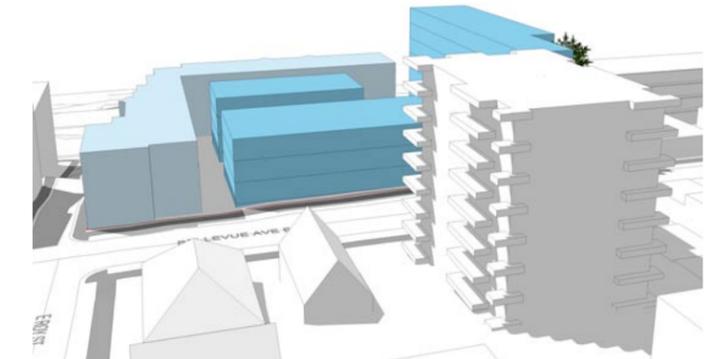
Advantages

- Achieves urban density
- Below-grade parking

Challenges

- Blocks light and air along Bellevue
- Requires a Contract Rezone
- Site plan not advantageous for daylighting or ventilation

- PARTIALLY MEETS HISTORIC PRESERVATION GOAL
- DOES NOT MEET SUSTAINABILITY GOALS
- MEETS URBAN DENSITY GOALS; URBAN QUALITY COMPROMISED



OPTION 4: COMBINED L3/MR WITH BELROY PREFERRED OPTION

Program

60 units - through units
Approximately 70 parking spaces below grade

Description

Two buildings: an "L" shaped footprint, with 6 narrow story building along the north, and 3 story building along Bellevue Avenue East; a 12-unit, three-story building located between the existing building and the Bellevue Avenue building. The density would be consistent with the historic pattern of development and neighborhood goals.

Advantages

- Preserves BelRoy
- Achieves urban density
- Consistent with neighborhood height and density
- Units have natural ventilation and light from at least 2 sides.
- Only 60' portion of building is along north portion of site, representing 15% of site's street frontage.

Challenges

- Requires a Contract Rezone
- Requires departures from land use code

- MEETS HISTORIC PRESERVATION GOALS
- MEETS SUSTAINABILITY GOALS
- MEETS URBAN DENSITY & QUALITY GOALS

SITE: ANALYSIS AND DESIGN DIRECTION

The attributes of the three buildable zones inform site planning and massing.

CHARACTERISTICS OF THE SITE

Given the project's historic preservation and sustainability goals, we are not pursuing a standard large footprint, double loaded corridor building. Instead, the design direction proposes narrower footprints that allow naturally ventilated and lit units. This design direction leads us to considering zones within the site with distinct characteristics that can inform the site planning and design:

North Zone

- Elongated east west
- Best solar orientation
- Large conifers along much of edge
- Potential small commercial use at corner

Bellevue Ave Zone

- Elongated along street
- View corridors at street level to site interior
- Height respects BelRoy
- Narrow building profile maximizes daylighting and natural ventilation
- Opportunity to activate and improve streetscape

Interior Zone

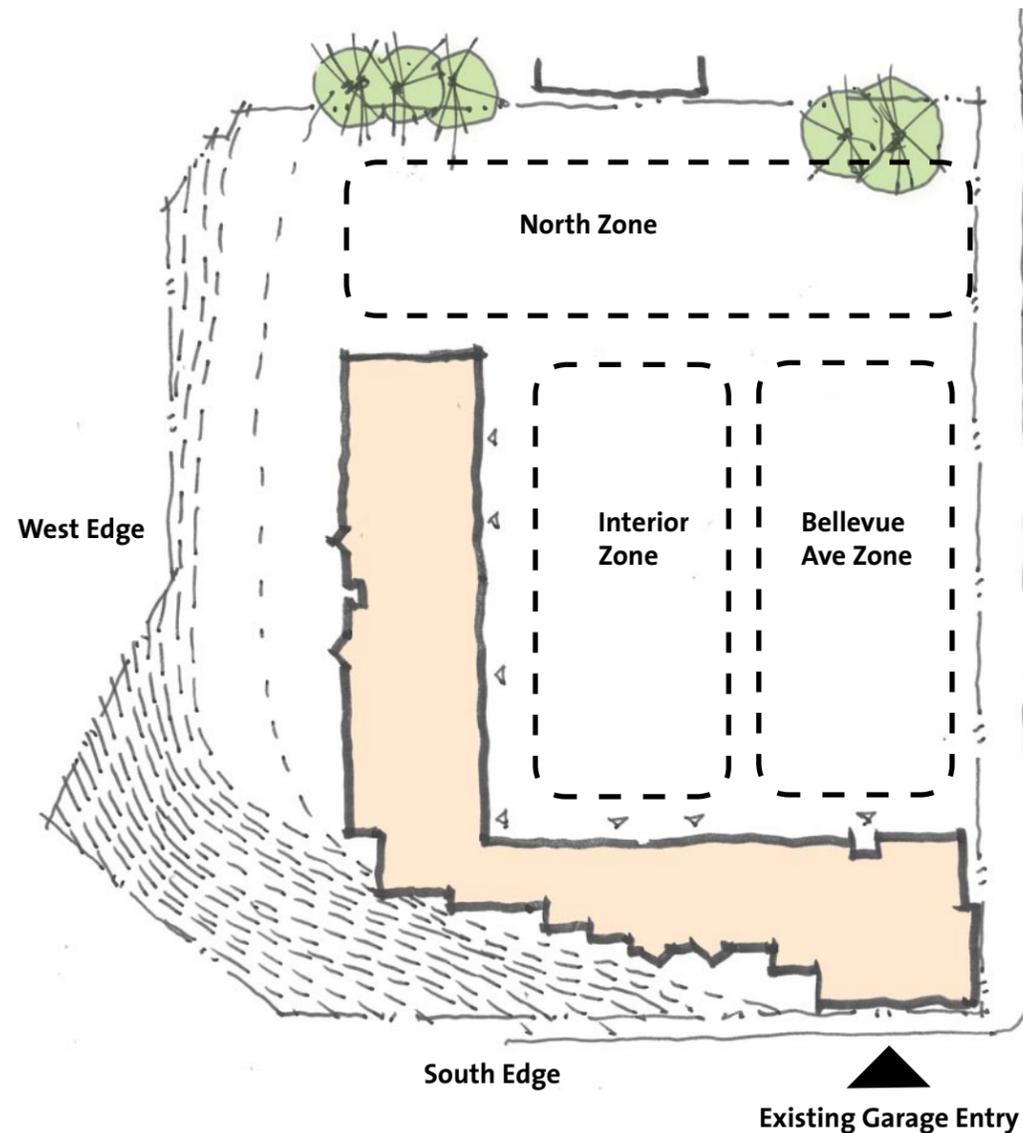
- Opportunity to co-locate community open spaces and residences
- Zone informed by existing interior circulation routes
- Narrow building profile maximizes daylighting and natural ventilation

The BelRoy Apartments

- Retain building and circulation zone along the interior of the "L"
- Utilize existing garage entry

West and South Edges

- No new construction
- Keep and enhance usable open space on flat areas
- Restore woodland with native vegetation
- Create visual connection between private and public property



PROPOSED DESIGN DIRECTION

The North Zone is a rectangle elongated east to west. This geometry is advantageous for solar orientation, with ample southern exposure. The width of the zone is set by the north property line and the wing of the BelRoy. Locating a narrow building on this portion of the site allows single-loaded units facing south, and creates two view corridors from the street through the entire site - one along the north property line and one through the courtyard space interior to the site. A narrow building would occupy approximately 16% of the frontage along Bellevue Avenue. Existing 60 foot tall trees near the street would help buffer a new building in the North Zone from the building to the north, and buildings across the street to the east.

The Bellevue Avenue zone responds to the streetscape and to the Bellevue Avenue end of the BelRoy. Our priority here is to maintain the height along Bellevue consistent with the BelRoy, to make an attractive courtyard at the entry between the two buildings, and to improve the streetscape along Bellevue. Again, a narrow building profile will be pursued in order to create naturally ventilated units with light from two sides.

The Interior zone has less effect on the street or adjacent properties. We propose a mix of open space along with a small cluster of units with patio spaces at grade, surrounded by lush landscaping and pathways bringing residents and visitors to building entries. Stairs will provide access from the garage to grade into the landscaped circulation system. A large open space at the north end of the zone will be a landscaped focal point of the residential community. The design in this zone will pay careful attention to window location and landscaping.

The south and west edges will include active open space where topography allows, with p-patches and common space for residents. The fencing would be removed to better integrate the wooded area into the site. The wooded area would be cleaned up so that the site edges are both safe and attractive.

DESIGN GUIDELINES

The following discussion responds to both the overall citywide guidelines and the neighborhood specific guidelines for Capitol Hill.

A. SITE PLANNING

A-1 Responding to Site Characteristics

The siting of buildings should respond to specific site conditions and opportunities such as non-rectangular lots, location on prominent intersections, unusual topography, significant vegetation and views or other natural features.

RESPONSE:
The primary driver for the site is the existing Bel Roy Apartment building. Maintaining this building is a fundamental project goal, and limits potential schemes for the site. In addition, it is appropriate to retain a similar height to the BelRoy in the Bellevue Avenue zone of the site.

The site also has significant topography on the western portion of the lot. No development would take place in the area identified as critical slope. We would take advantage of views to the extent possible for new residents, being mindful of the existing views of buildings on the east side of Bellevue Avenue East.

We propose keeping the two evergreens on the northeast corner of the lot that are both over 60 feet tall.



Two major site characteristics are the presence of the Belroy Apartments and the topography.



The building form can be modified to retain a cedar and a hemlock that are over 60 feet tall.

A-2 Streetscape Compatibility

The siting of buildings should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way.

RESPONSE:
The most desirable characteristic of the streetscape is a variety of building scales in the vicinity, the landscaped entry of the existing building, and the mature trees on the east side of the street. The presence of numerous garages and curb cuts detracts from the pedestrian character of Bellevue Avenue East. East Roy Street is steep and much of its length is wooded.

The project proposes to improve the street edge, add residential entries, offer views into and through the site, and to create an attractive landscaped edge. Four curb cuts would be removed. Existing mature trees at the northeast corner of the site would be maintained, and new street trees planted. Along Roy Street, the wooded area would be restored with good urban forestry practices and removal of invasive plants.

The Capitol Hill Design Guidelines highly value the pedestrian experience, provide neighborhood-specific guidelines for Streetscape Compatibility, adding that:

- Vehicle entries to buildings should not dominate the streetscape.

The proposed design responds to the desire to reduce vehicle entries.

- Orient townhouse structures to provide pedestrian entrances to the street

While the units are not strictly townhouses, they are designed to have point circulation rather than corridors in the same fashion as the BelRoy. This will provide pedestrian entrances at the street that are valued by the Capitol Hill Design Guidelines.



On the east side of Bellevue, mature trees stand between curb cuts.



The streetscape along the edge of the site has one street tree in poor condition, and four driveways.



Several stretches of streetscape given over to carports and garages



SITE PLANNING

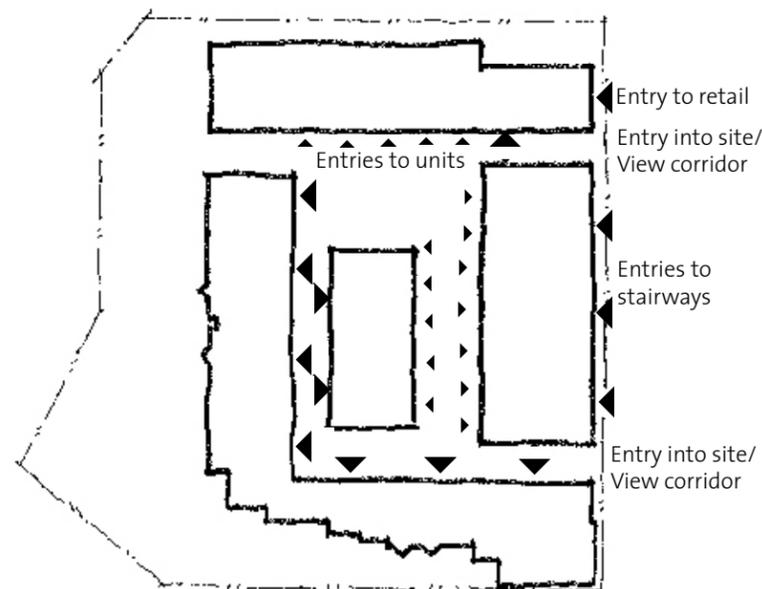
A-3 Entrances Visible from Street

Entries should be clearly identifiable and visible from the street.

RESPONSE: Two types of entries will be at the street. Two primary entries bring residents and visitors into the site and offer views into the site for people passing by. The southern primary entry is the BelRoy's court, shown below. The other would be a similar entry into the courtyard at the site's north end. Along Bellevue Avenue East, three entries provide access to stairwells that serve clusters of units, again drawing on the spirit of the BelRoy's circulation strategy, and adding a human presence along Bellevue Avenue East.



The entry court at the BelRoy sets a precedent for visibility into the site.



This diagram shows the preferred option with multiple entries along the street, carrying forward the circulation strategies

A-5 Respect for Adjacent Sites

Buildings should respect adjacent properties by being located on their sites to minimize disruption of the privacy and outdoor activities of residents in adjacent buildings.

RESPONSE: Because the project is based on a proposed rezone from L3 to MR, the north property line of the project would technically become a zone edge. For this reason, we have carefully considered the conditions at the north end of the project site, and the south portion of the neighboring site.

The existing north edge condition is a driveway accessing the house on the west side of the property. The two large conifers are adjacent to this driveway on the project property. The south end of the adjacent property, the Mark Spencer Apartments, includes surface

parking, building services (dumpsters), a blank south-facing facade, and three atlas cedars on the west side of the property.

The Mark Spencer Apartments is a 40-unit, 5 story residential building built in the late 1950's. Its narrow footprint is elongated north and south, orienting the units to the west-facing views. A swimming pool is on the west side of the building.

Stair towers on the north and south ends present windowless concrete masonry unit walls to the neighboring properties. The south wall is approximately 3 feet from the south property line.

Approximately 34 surface parking spaces are along Bellevue Avenue; some spaces are protected by free-standing canopies. A fence separates the project site from the Mark Spencer property,

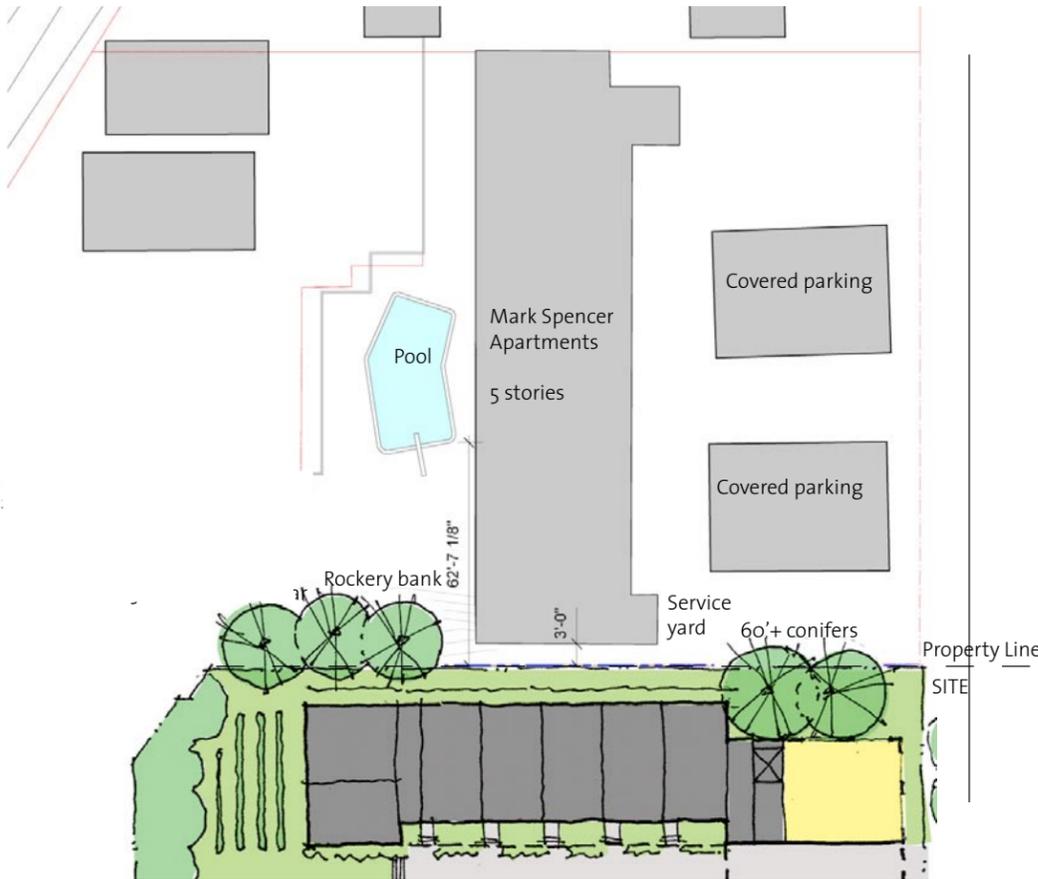


Bellevue Avenue E frontage of the Mark Spencer Apartments, immediately adjacent to the north of the project site. The fifth story of the building is entered through a partially below grade exterior corridor, and is at grade on the west side.

A. SITE PLANNING

A-5 Respect for Adjacent Sites (cont)

The guideline specifically refers to impact on adjacent open spaces. The Mark Spencer Aparatments has an open space with a swimming pool on the west side of the building that is screened by large evergreen trees that are approximately the same height as the apartment building. Windows for living spaces in the proposed building would be oriented toward the views to the south and west, away from the neighboring property. As shown in the photos, the south facade of the Mark Spencer Apartments is a windowless wall.



The north property line of the project site, with the Mark Spencer Apartments to the north.



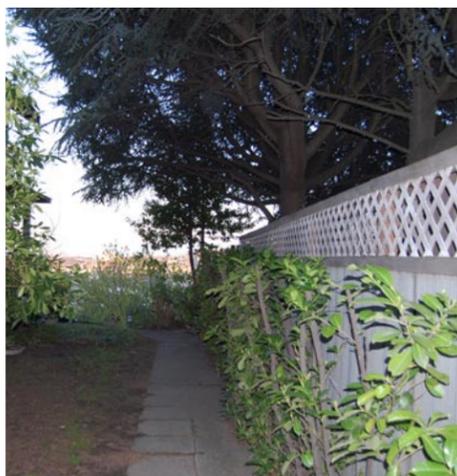
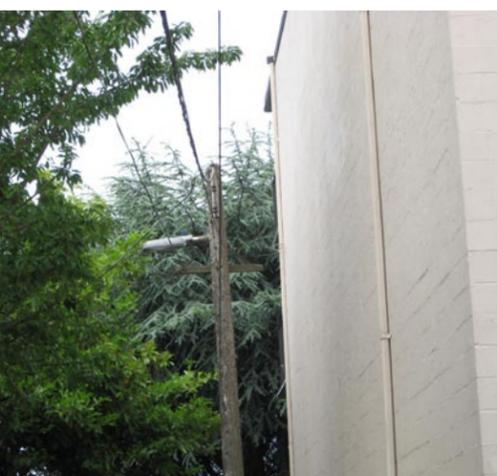
The east 74 feet of the apartments, fronting Bellevue Avenue E, is dedicated to parking, with some of the spaces under canopies.



Looking west on the driveway at the north end of the project site



The south wall of the Mark Spencer Apartments is solid, with dense trees beyond.



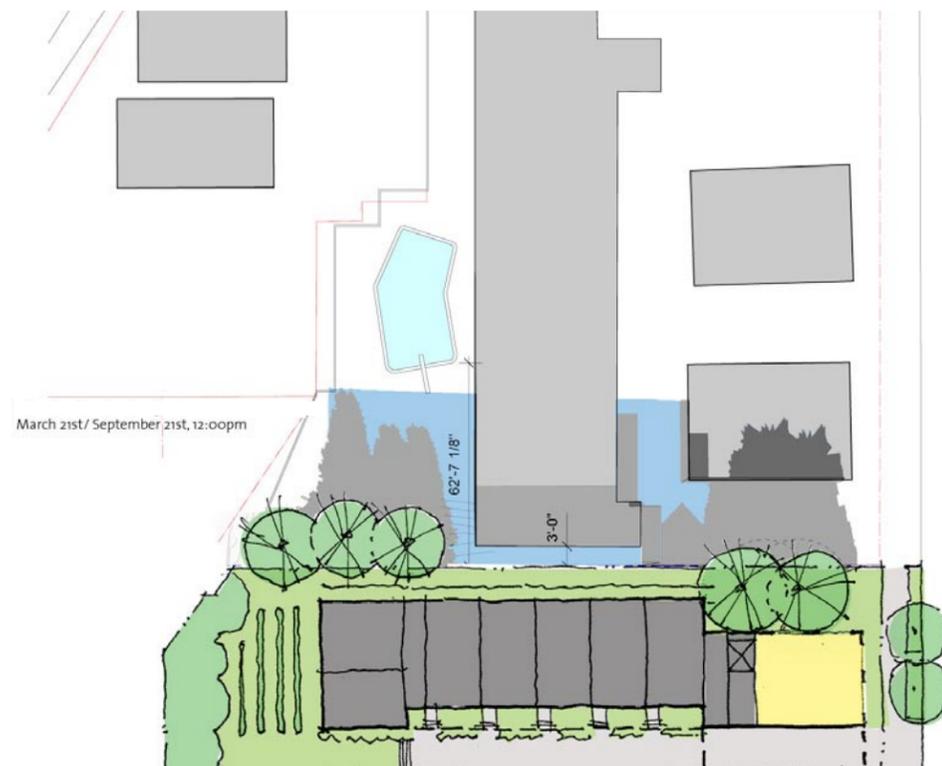
Along the north property line, the views to the neighboring property are heavily screened by mature conifers.

SITE PLANNING

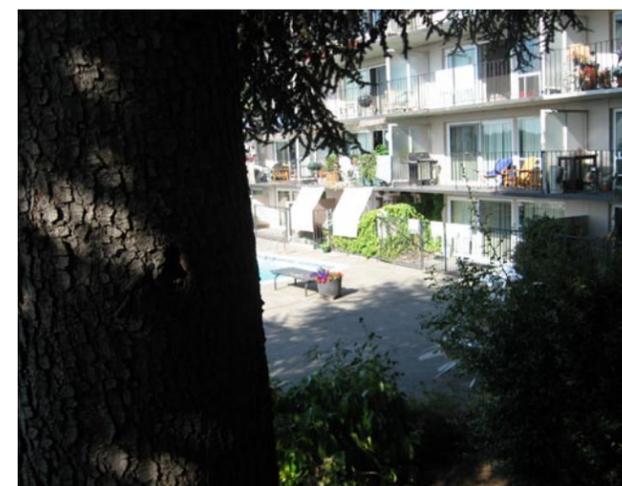
A-5 Respect for Adjacent Sites (cont)

The illustration below shows existing shadows at noon at the equinox condition (March 21st/September 21). The gray shadow approximates the existing shadowing from the trees. The blue indicates the additional shadowing from the proposed six-story preferred option.

The equinox condition is shown here. Between the March and September equinoxes, shadowing has less impact because the sun moves further to the north. The shadows from the proposed building typically infill the existing shadowing from the trees, but do not reach the pool or significantly increase shadows. See Appendix for fuller shadow studies.



The blue shadows infill the existing shadows from the trees in a worst-case situation at the equinox.



These photos, taken on a late September afternoon, show that the cedars shadow the south portion of the adjacent open space.

A-6 Transition Between Residence and Street

For residential projects, the space between the building and the sidewalk should provide security and privacy for residents and encourage social interaction among residents and neighbors.

RESPONSE: The existing BelRoy is set back three feet from the property line at the south end, expanding to five feet near the entry court. Our preference for the front yard setback is to match the existing five foot setback at the BelRoy. We will layer the landscaping in that five feet to give a feeling of depth, and to protect windows near the street. Because the floor level is set higher than the sidewalk level, the windows along the sidewalk will not feel overly exposed.



The existing setback width at the BelRoy, if properly landscaped, would provide an attractive street edge and privacy for residential windows.

A. SITE PLANNING

A-7

Residential Open Space

Residential projects should be sited to maximize opportunities for creating usable, attractive, well-integrated open space.

Residential Open Space: Capitol Hill

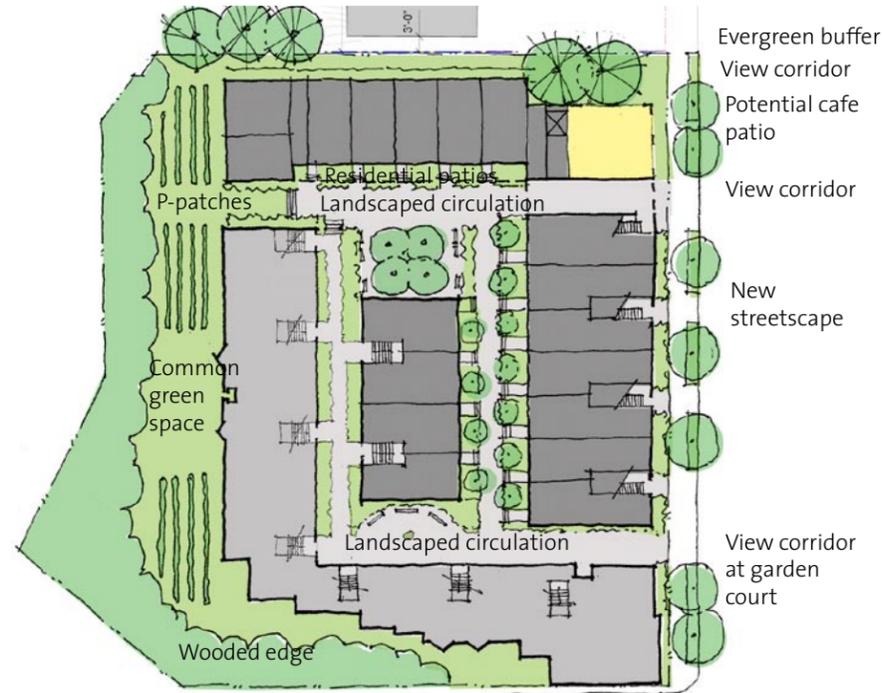
-Incorporate quasi-public open space with new residential redevelopment, with special focus on corner landscape treatments and courtyard entries.

-Create substantial courtyard-style open space that is visually accessible to the public view.

-Use landscape materials that are sustainable, requiring minimal irrigation or fertilizer.

RESPONSE: This project is an opportunity to provide urban residences with a variety of quality open spaces. The existing building has an attractive green space on the west side of the site. The project aspires to create a circulation network to connect landscaped entries, new courtyard and open spaces, a residential p-patch and access to private outdoor space for ground-related units. Views into the open spaces will add interest to the streetscape.

The open space diagram illustrates the potential of quasi-public open space at the northeast corner with associated small retail or cafe space, and courtyard space open to public view. Sustainability is a primary goal of the project and will be reflected in the selection of plant materials appropriate to the Northwest climate.



OPEN SPACE DIAGRAM



Open space at the Belroy can be better connected to the wooded area, with new open space added to the north.

A-8

Parking and Vehicle Access

Siting should minimize the impact of automobile parking and driveways on the pedestrian environment, adjacent properties and pedestrian safety.

Parking and Vehicle Access: Capitol Hill

-Preserve and enhance the pedestrian environment in residential areas by providing for continuous sidewalks that are unencumbered by parked vehicles and are minimally broken within a block by vehicular access.

RESPONSE: Below grade parking is critical to creating good residential units and open space. The grade on East Roy Street and the existing garage entry offer the possibility to remove the four curb cuts along Bellevue Avenue East and dedicate the street frontage to pedestrian use. Our strong preference is to pursue a project that will allow underground parking, utilizing and making safety modifications to the existing parking entry on Roy Street.



The existing garage entry on East Roy Street could be modified for a larger below grade parking area.

B. HEIGHT, BULK AND SCALE

B-1

Height, Bulk and Scale Compatibility

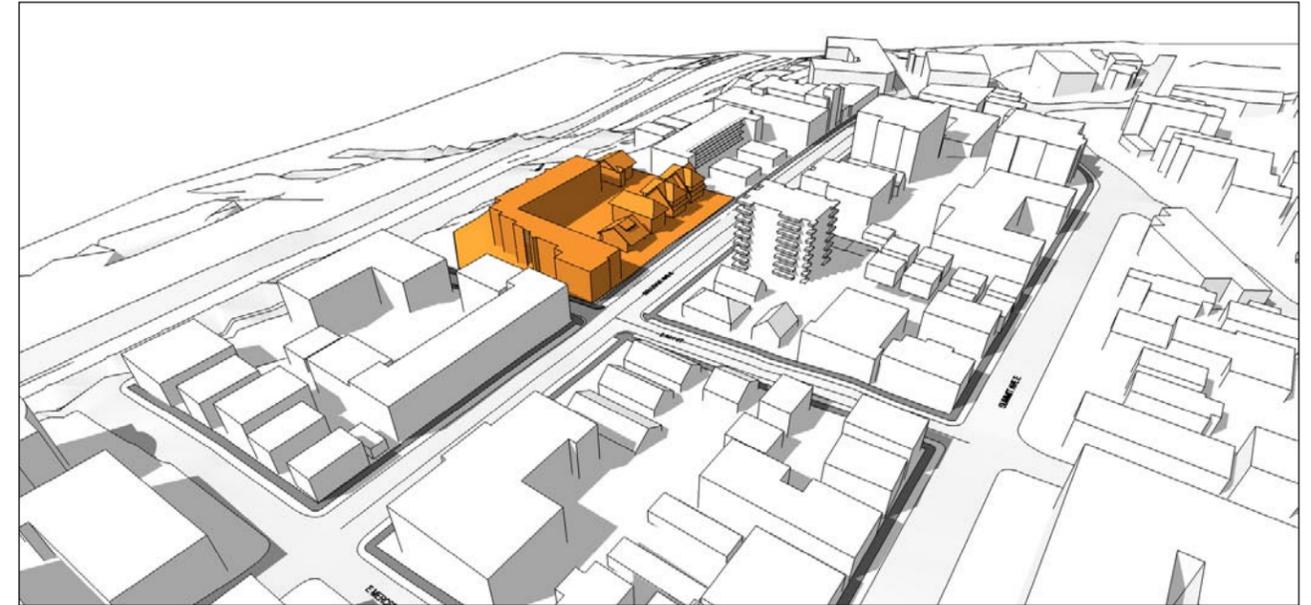
Projects should be compatible with the scale of development anticipated by the applicable Land Use Policies for the surrounding area and should be sited and designed to provide a sensitive transition to near-by, less intensive zones. Projects on zone edges should be developed in a manner that creates a step in perceived height, bulk, and scale between anticipated development potential of the adjacent zones.

Height, Bulk and Scale Compatibility: Capitol Hill

Neighborhood Priority: Preserve and augment the neighborhood's architectural qualities, historic character and pedestrian scale.

RESPONSE: The existing neighborhood is extremely varied in terms of its height and massing. The urban form reflects a number of historic shifts in allowable height, rather than the L3 zoning of the immediate area. The variation gives a sense of diversity to the neighborhood, instead of a single, uniform height and scale.

The preferred option provides a sensitive transition to less intensive zones by moderating the building scale within the site to reflect the diverse heights in the vicinity. With respect to the zone edge to the north, the preferred option includes sensitive site planning and design. The adjoining development on the north has a driveway and entire south facade adjacent to the site. (See p. 11 for a description and photographs of the property to the north.) The existing trees form an important buffer to this adjacent property. The new building would align with the height of these trees. In order to retain the trees, the new building would be pulled to the south by 10 feet, along a 50-foot length. In addition, the north facade would be designed with open air walkways to access the units and break down the massing of the facade.



The neighborhood's existing massing is highly diverse in terms of height, scale and massing.



The Hillsborough at 740 Bellevue Avenue East is seven stories over parking



Looking up Roy Street, with single family houses coexisting with the 12-story Shannon Condominiums beyond



The Bellaggio, a recently constructed building in a split zone, is a mix of higher and lower massing



The 714 Bellevue Avenue East building, across the street to the east, is 8 stories over parking.



There are a number of older three and four story buildings along Bellevue.

C. ARCHITECTURAL ELEMENTS and MATERIALS

C-1

Architectural Elements and Materials: Capitol Hill

Neighborhood Priority: Preserve and augment the neighborhood's architectural qualities, historic character and pedestrian scale.

Architectural Context

New buildings proposed for existing neighborhoods with a well-defined and desirable character should be compatible with or complement the architectural character and siting pattern of neighboring buildings.

RESPONSE: The design will take cues from the BelRoy building in terms of character and spirit, including the courtyard entry approach, and the through-unit design that offers natural ventilation and light on multiple sides.

C-2

Architectural Concept and Consistency

Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept. Buildings should exhibit form and features identifying the functions within the building. In general, the roofline or top of the structure should be clearly distinguished from its facade walls.

RESPONSE: Again, cues will be taken from the BelRoy, drawing on the massing and the courtyard approach. The new buildings will not imitate the existing building, but will draw on the architectural language and underlying strategies of the building. We believe that a lower building along Bellevue Avenue East and a taller building along the north will create the varied massing that fits well into the development patterns of the neighborhood.

C-3

Human Scale

The design of new buildings should incorporate architectural features, elements and details to achieve a good human scale.

RESPONSE: Entries at the street, views into the courtyards, textured landscaping and the mix of old and new architecture will create a varied, human scale streetscape.

C-4

Exterior Finish Materials

Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.

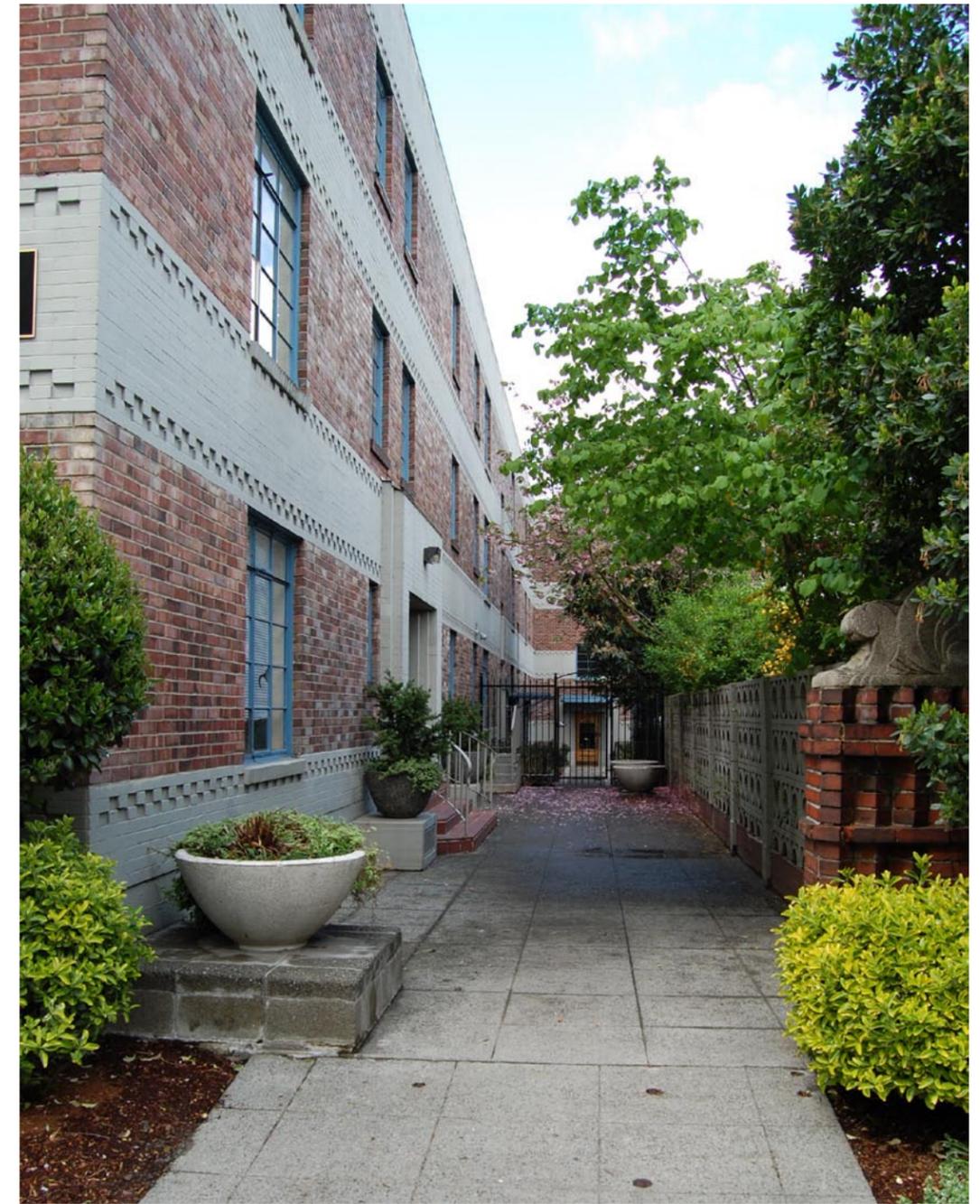
RESPONSE: Materials will be compatible with and complimentary to the existing building. Restoration of the BelRoy's brick facade will highlight its distinctive texture and patterns.

C-5

Structured Parking Entrances

The presence and appearance of garage entrances should be minimized so that they do not dominate the street frontage of a building.

RESPONSE: Reusing the garage entry eliminates four curb cuts and removes all parking from the ground plane.



BelRoy's entry is via a courtyard, with entries via stairways off a landscaped path. This circulation strategy results in naturally ventilated through-units.

D. PEDESTRIAN ENVIRONMENT

D-1

Pedestrian Open Spaces and Entrances

Convenient and attractive access to the building's entry should be provided. To ensure comfort and security, paths and entry areas should be sufficiently lighted and entry areas should be protected from the weather. Opportunities for creating lively, pedestrian-oriented open space should be considered.

Pedestrian Open Spaces and Entrances: Capitol Hill

- Provide entryways that link the building to the surrounding landscape.
- Create open spaces at street level that link to the open space of the sidewalk.
- Building entrances should emphasize pedestrian ingress and egress as opposed to accommodating vehicles.

RESPONSE: The proposed design creates a set of varied circulation routes, including landscaped paths, courtyards and green spaces that are intended to build community among the residents and provide views into a green oasis for passers-by.

Visible and generous entries along Bellevue Avenue will connect a newly planted streetscape with interior gardens, and replace what is now a compromised pedestrian experience. Landscape along the street is an important aspect of the project, and will be designed to create a green residential-scale edge and contribute to the tree-lined character of the street's east side.



While much of the block has large street trees, but the length of the site along Bellevue is an exception.

E. LANDSCAPING

E-1

Reinforce Existing Landscape Character

Where possible, and where there is not another overriding concern, landscaping should reinforce the character of neighboring properties and abutting streetscape.

Landscaping : Capitol Hill

Neighborhood Priority: Maintain and enhance existing landscape patterns in residential areas.

RESPONSE: The project will add new street trees to balance the two sides of the street, and a residential-scale green edge along the sidewalk. Planting materials will have a consistency along the street and on the interior, with plants selected for year-round interest, color, texture and low water usage.

E-3

Landscape Design to Address Special Site Conditions

The landscape design should take advantage of special on-site conditions such as high-bank front yards, steep slopes, view corridors, or existing significant trees and off-site conditions such as greenbelts, ravines, natural areas, and boulevards.

RESPONSE: The wooded area along the south and west edge will become more attractive with restoration and maintenance. The project will include replanting areas now devoid of vegetation and restoration of hillside with native species in partnership with the Washington State Department of Transportation.

The large existing trees on the northeast portion of the site will be saved.



The wooded area along the west and south slopes would benefit from maintenance.

PREFERRED OPTION: SITE PLAN



Site planning strategies draw on the legacy of Anhalt courtyard apartments on Capitol Hill, with lush landscaped courtyards

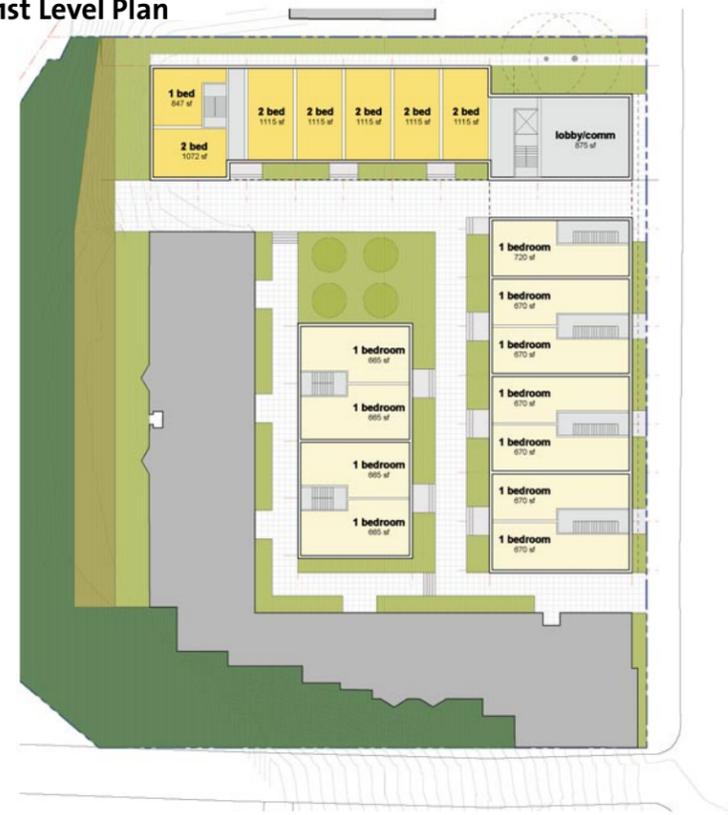


Landscaped pathways can be improved and expanded with the new project.

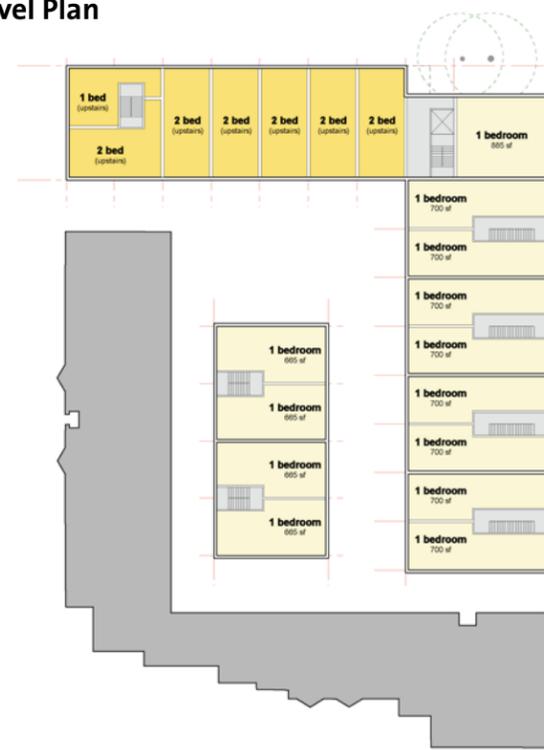
Entries to stairs are integral to the existing BelRoy, and will activate the streetscape and interior site.

PREFERRED OPTION: CONCEPT FLOOR PLANS

1st Level Plan



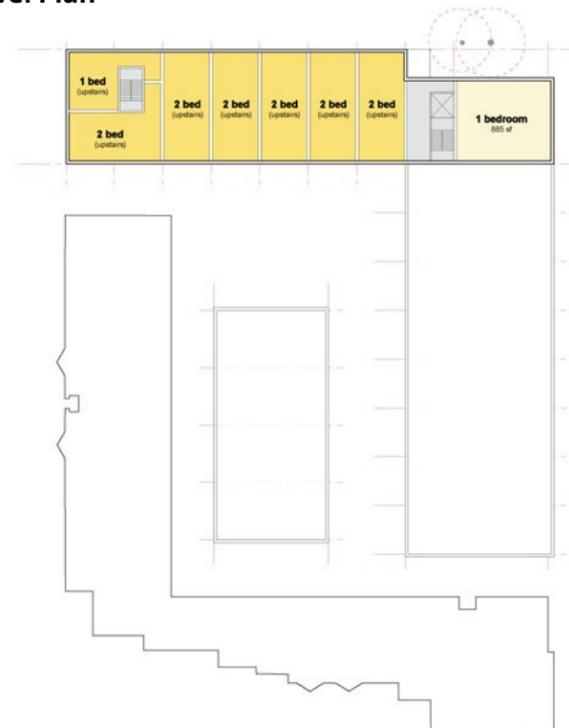
2nd Level Plan



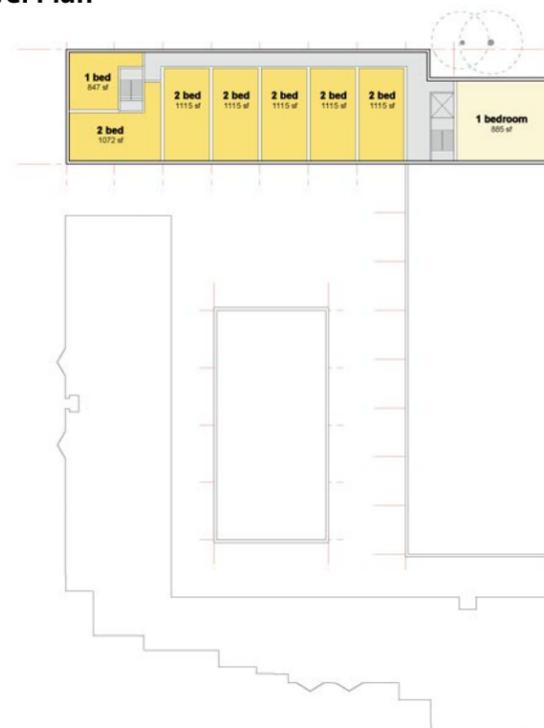
3rd Level Plan



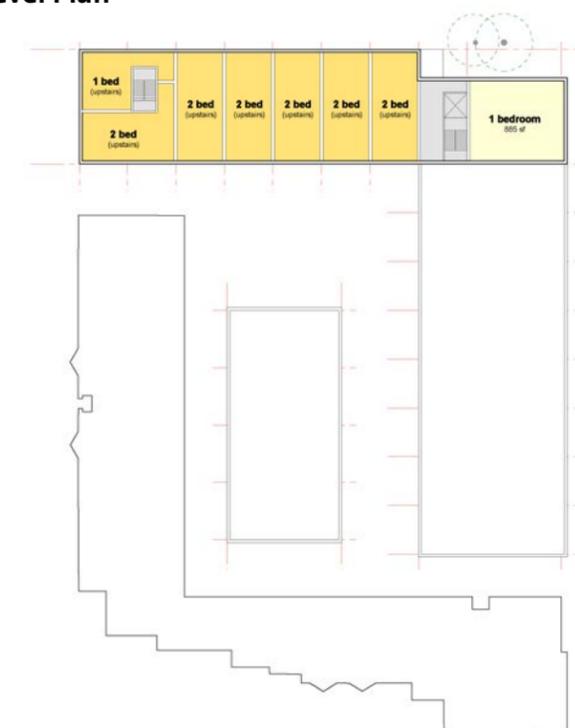
4th Level Plan



5th Level Plan



6th Level Plan



POTENTIAL DEPARTURES

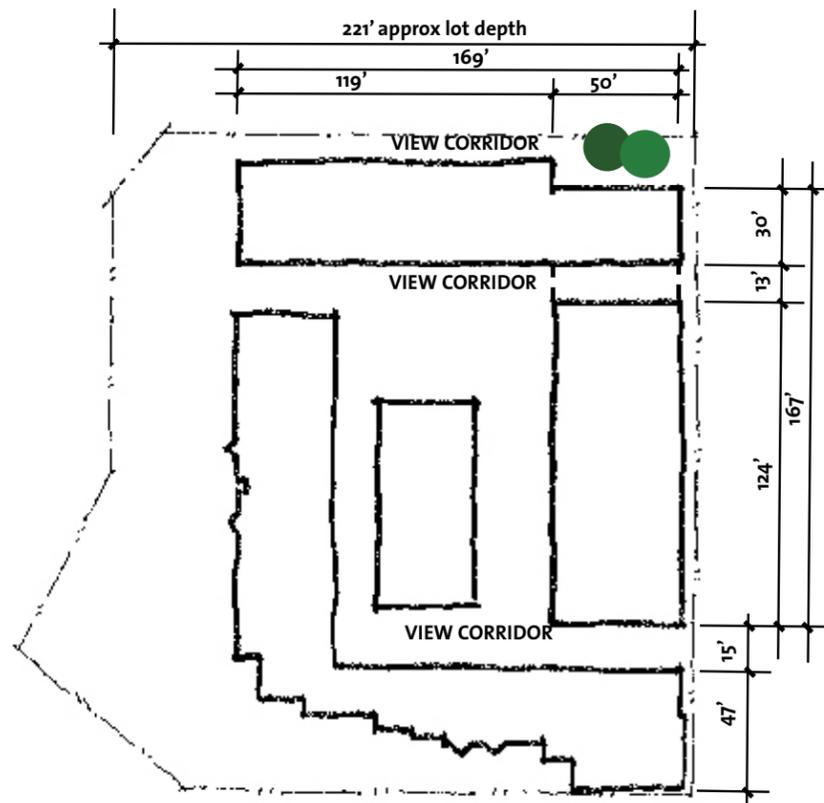
23.45.052 Midrise Structure Width and Depth

FRONT FACADE WIDTH

A.2 When the front facade is modulated according to the standards of Section 23.45.054 C, the maximum width of each structure on a lot shall be one hundred fifty feet

RESPONSE: The proposed design along the street would have the BelRoy frontage of approximately 47 feet, an opening between the old and new buildings that would be approximately 15 feet, and a new building that would be approximately 167 feet long. This longer width would have an additional opening into the site that would serve as an open air breezeway and view corridor.

An additional view corridor would be on the north end of the property, between the lobby/commercial space and the property line, and includes the two existing trees. This arrangement allows three view corridors into the site, which would be more effective than a 150' building length, and fewer view corridors.



The perception of structure width is broken down with view corridors into and through the site.

23.45.052 Midrise Structure Width and Depth

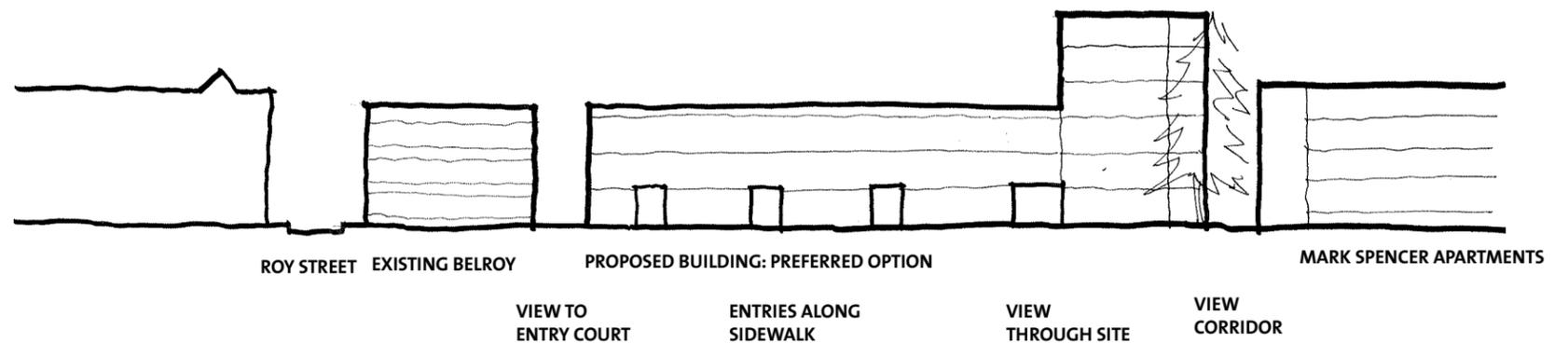
LOT DEPTH

B.1 c. Apartments: 65% of the lot depth. Exceptions to Maximum depth requirements: lot depth can exceed 65% subject to the following conditions:

- a. Total lot coverage shall not be greater than that which would have been possible with standard development requirements for maximum width, depth and setbacks.
- b. When lot area is over 7,000 sf, required amount of usable open space shall be increased to 30% of lot area. Not more than one third may be provided above ground or as decks or balconies. (For a 55,870 sf lot, 30% open space would be 16,761 sf).
- c. Structure depth shall in no case exceed 150 feet.

RESPONSE: The depth of the lot is angled, but is approximately 221'. Sixty-five percent of that depth would be 143'. The proposed north building edge has a fifty foot length of building along the area with the trees. The remaining length of the building is 119', aligning with the west edge of the BelRoy. The total length of the building is proposed to be 169', matching the depth of the BelRoy.

The geometry of the lot and sustainable building practices favor longer, narrower footprints for buildings.



The structure width is relieved with view corridors and entries at the street.

23.45.052 Midrise Structure Width and Depth

D.1. Required setbacks for cluster developments.

Where two or more principal structures are located on a lot, the required setback between those portions of interior facades which face each other shall be as follows:

- Length along facing portion of facades:
- 40 or less - 15' average setback; 15' minimum
 - 41 to 60' - 20' average setback; 15' minimum
 - 61 to 80' - 25' average setback; 15' minimum
 - 81 to 100' - 30' average setback; 15' minimum
 - 101 to 150' - 40' average setback; 15' minimum
 - 150 or more - 50' average setback; 15' minimum

RESPONSE:

The project has multiple buildings because of the presence of the existing building and because we are creating narrow buildings for through-units. We propose minimum setbacks of 15 feet in all cases, but prioritize solar access and natural ventilation and the series of open spaces and circulation routes.

23.45.054 Midrise Modulation Requirements

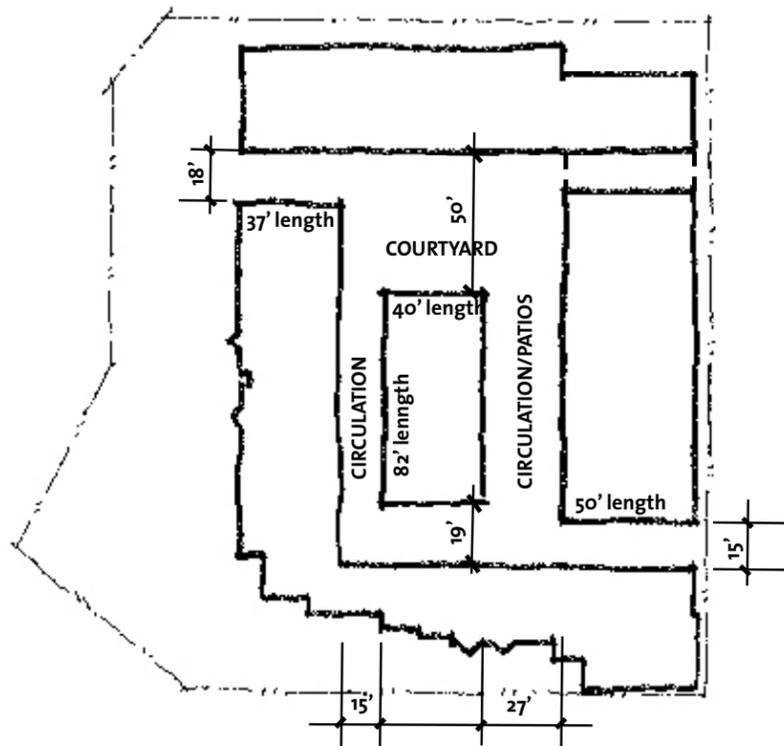
A. Front Facades modulation shall be required if the front facade width exceeds forty (40) feet. Ground-related structures may follow either the modulation standards for L3 (23.45.012) or the standards in this section.

B. 2. Side Facades - Apartments with a structure depth greater than sixty-five percent of the lot depth shall be modulated according to the standards of subsection D of this section.

C. Within a cluster development, all interior facades wider than fifty feet shall be modulated according to the standards of Section 23.45.054D, provided that maximum modulation width shall be fifty feet.

RESPONSE:

We may find that the modulation requirements diminish the design in relationship to the relationship of the new building and the design of the BelRoy. The interior facade of the BelRoy, with significant lengths, to not notch in and out, but are strong and simple, emphasizing horizontal layers, and residential stair entryways.



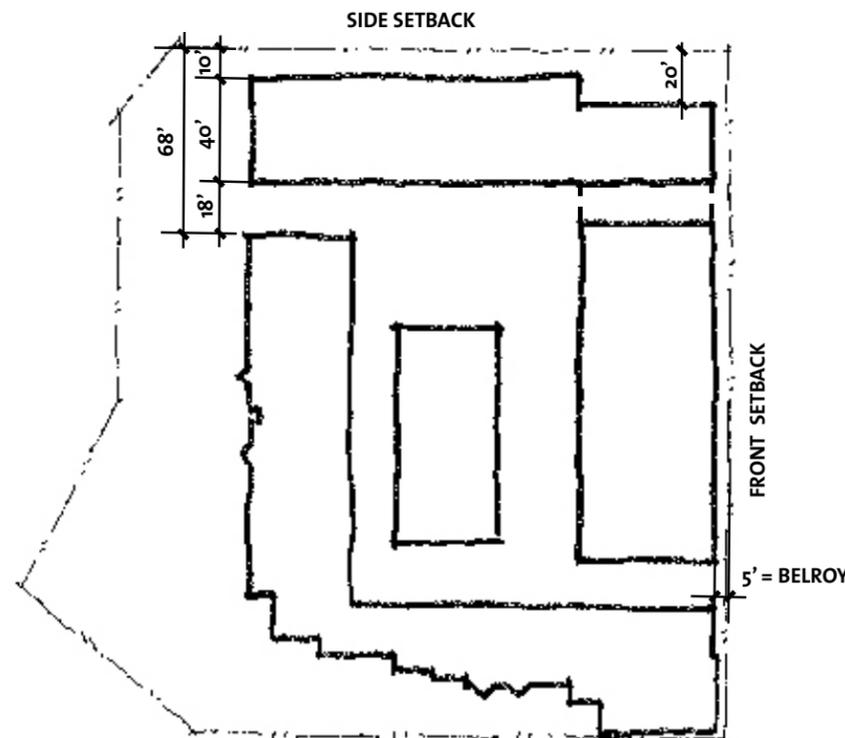
The site plan, with its building footprints and their separation, is based on light and air for units and for open space arrangements.

23.45.056 Midrise Setback Requirements

A. Front setback = average of the setbacks of the first principal structure on either side. Front yard setback not required to be more than 5 feet than the setback of the first setback on either side which is closer to the front lot line, an in no case be required to exceed 15 feet.

RESPONSE:

The BelRoy is set back 3 feet from the Bellevue Avenue East property line at the south end, and jogs to a five foot setback. We would propose matching the setback of the BelRoy at 5 feet, with dense landscaping.



Proposed setbacks match the BelRoy along the street and allow view corridors into and through the site.

23.45.056 Midrise Setback Requirements

C. Side setback per Table 23.45.056A. For a building over 51 feet in height, 156 - 165 feet long, average setback would be 23.5 feet, minimum setback of 10 feet.

RESPONSE:

We propose a setback of approximately 20 feet along the north edge where the setback will retain the trees (about 50 feet in length), and narrowing to the ten foot minimum along the remainder of the building.

The zone between the north edge of the BelRoy and the north property line is fairly narrow. We can build a narrow building, with single loaded circulation and stay in the vicinity of 42 feet wide. The design proposes a 15 foot wide view corridor into the courtyard and through the site, which is an important feature for the street.

The property to the north has a narrow, windowless facade set back approximately 3 feet from their property line. The rest of the mutual property line is heavily screened with several blue atlas cedars, from 18" to 24" in diameter. These effectively buffer the properties along the shared property line, and the new building would not increase shading on the open space. The units of the building to the north all face views to the west.

APPENDIX: Existing Shading

March 21st

June 21st

September 21st

December 21st

9:00 a



12:00 p



3:00 p



APPENDIX: Preferred Option Shading

March 21st

June 21st

September 21st

December 21st



9:00 a



12:00 p



3:00 p

APPENDIX: Existing Shading – North Property Line

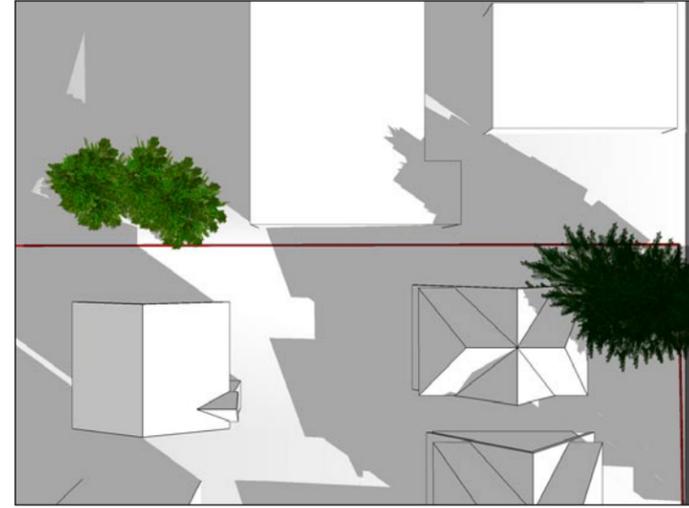
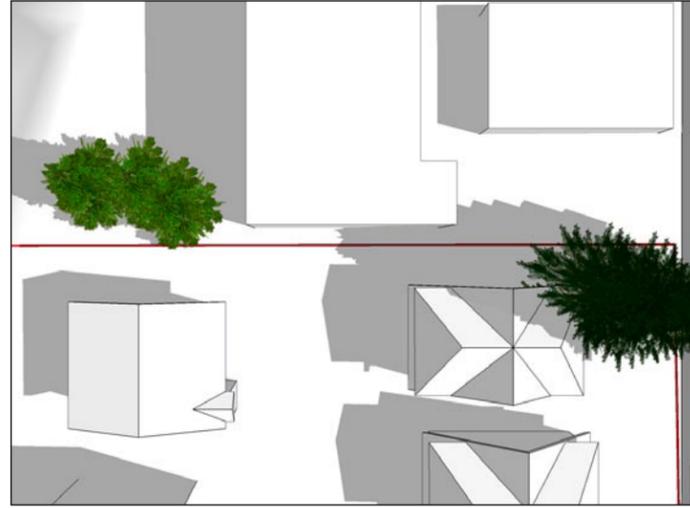
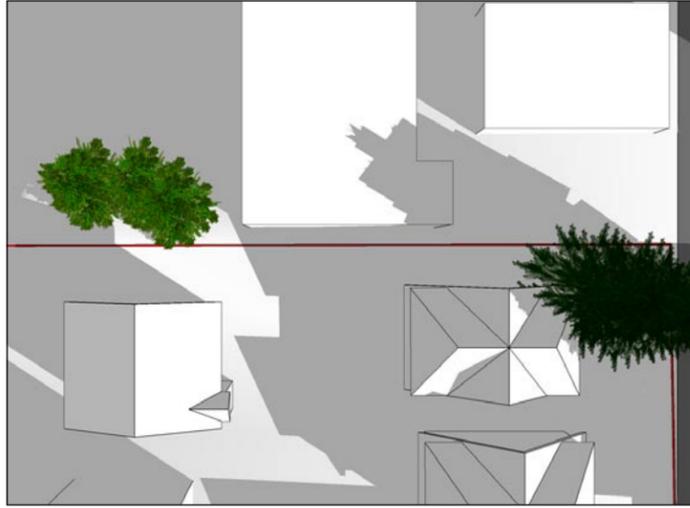
March 21st

June 21st

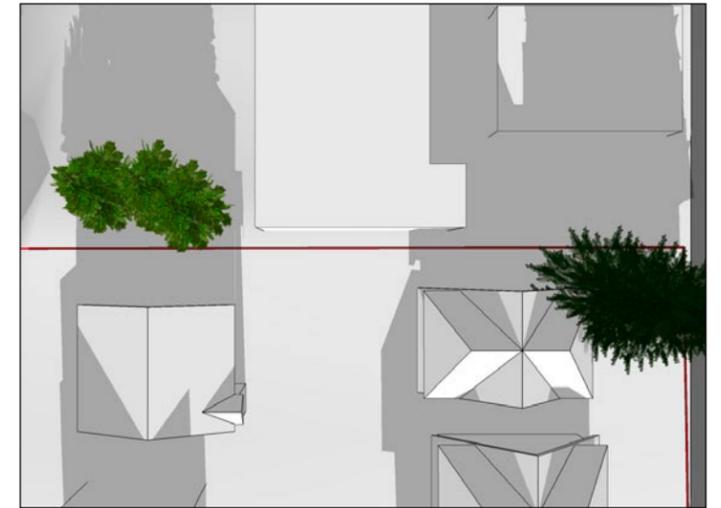
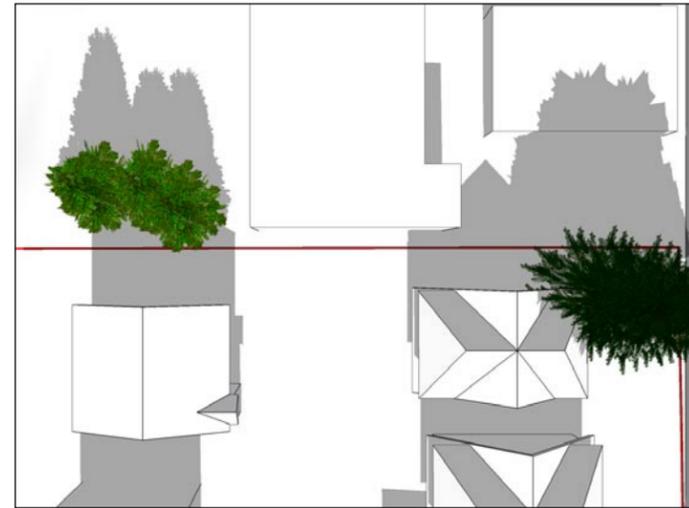
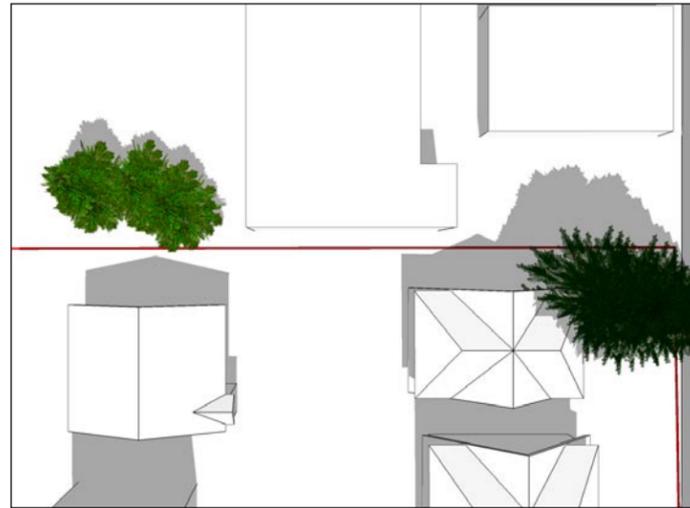
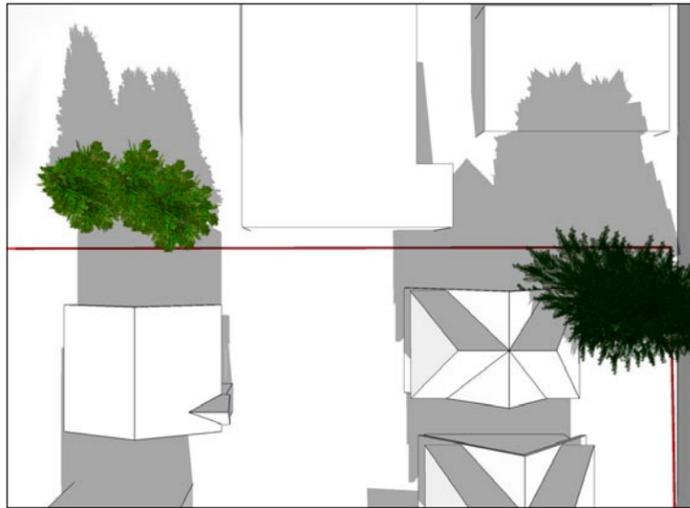
September 21st

December 21st

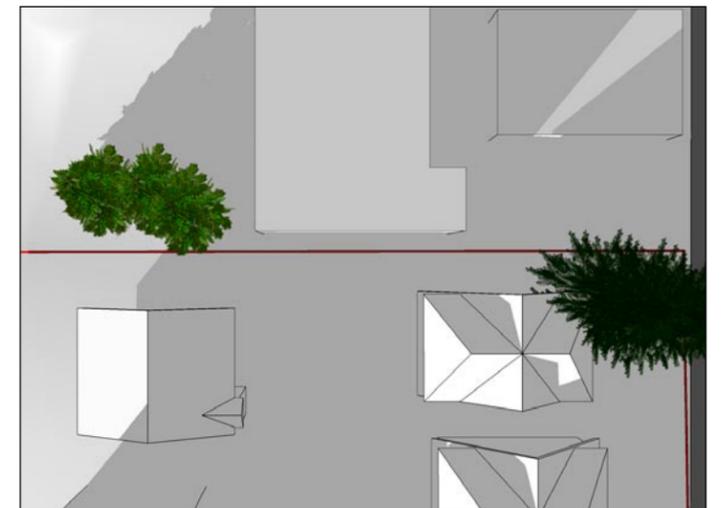
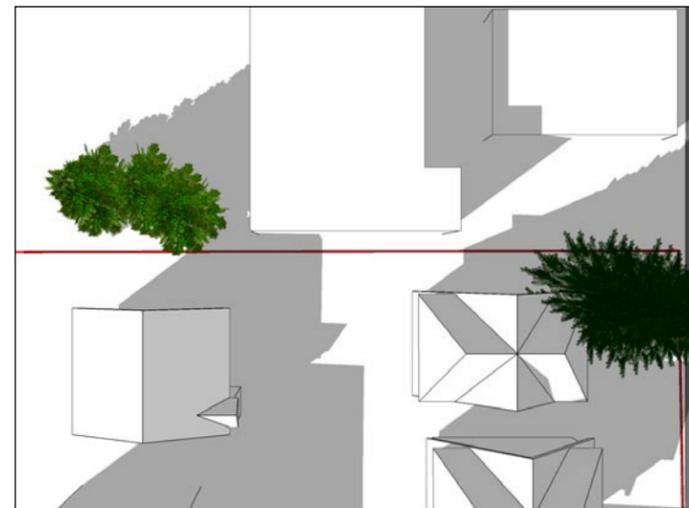
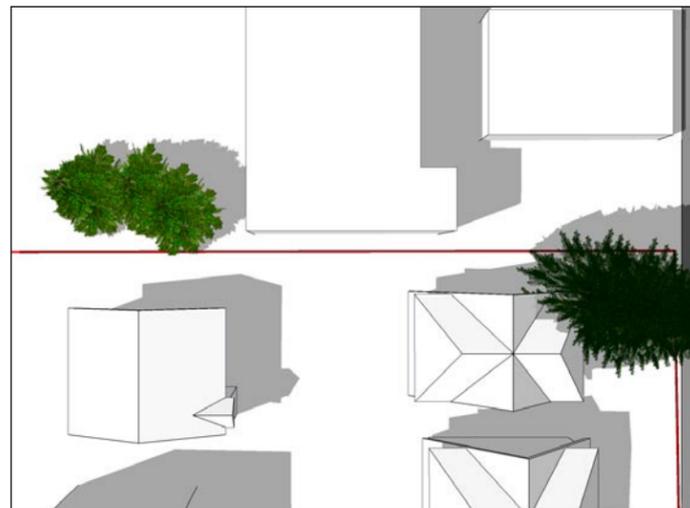
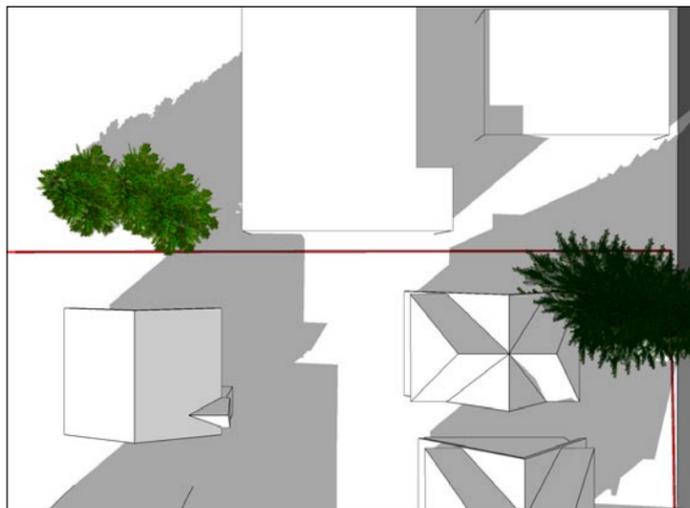
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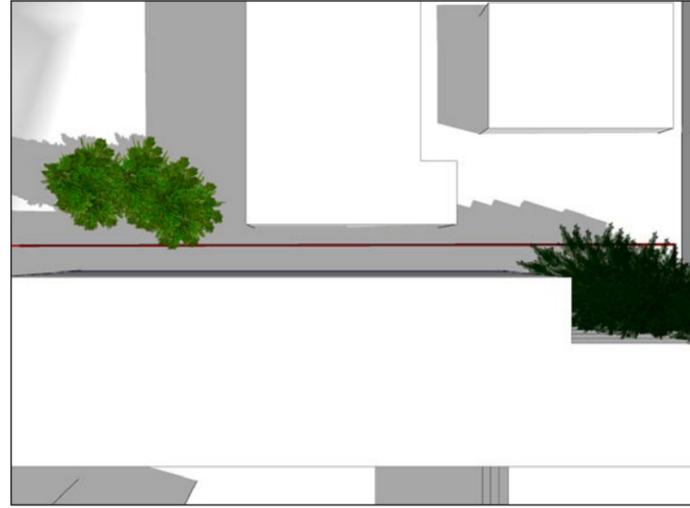
APPENDIX: Preferred Option Shading – North Property Line

March 21st

June 21st

September 21st

December 21st



9:00 a



12:00 p



3:00 p

