



901 DEXTER AVE. N



OWNER:
Capstone Partners, LLC
1001 Fourth Avenue PLaza, Suite 4400
Seattle, WA 98154

CONTACT:
Mike Hubbard
206.389.1636



ARCHITECT:
Runberg Architecture Group, PLLC
One Yesler Way, Suite 200
Seattle, WA 98104

CONTACT:
Brian Runberg
206.956.1970

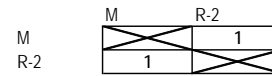
DEVELOPMENT OBJECTIVES

PROJECT DATA

1.1 Location: Rectangular parcel at corner of Aloha St. and Dexter Ave N
 1.2 Site Area: 52,321 sf 1.2 acres
 1.3 Zone: SM 65/ South Lake Union Urban Center
 Archaeological Buffer Area
 ECA- 40% Steep Slope
 ECA- Potential Slide Area
 Airport Height Overlay: Outer Transitional Surface
 Dexter Avenue North is a Class 2 Pedestrian Street

1.4 Building Code: 2006 Seattle Amendments to the IBC
 1.5 Proposed Use: Mixed-Use

1.7 Occupancy Classification / Separations
 Commercial
 Residential



1.8 Gross Floor Area:

Fir. Lev.	PARKING	VERT CIRC	LOBBY/ CORE	RETAIL (gsf)	RESID. (gsf)	SUPPORT	TENANT STORAGE	TOTAL (gsf)	Open Space
concrete Level P1	21,423	1,476				1699		24,598	
concrete Level 1	22,628	1,476	4,028	11,124	4,828	1,837	364	46,285	1884
concrete Level 1 Mezz.	21,490	1,476	939	1,756	5,504	3,045	706	34,916	
wood Level 2		1,496	4,191		31,879	132	750	38,448	
wood Level 3		1,496	4,114		32,695	132	419	38,856	7,770
wood Level 4		1,496	4,114		32,695	132	419	38,856	
wood Level 5		1,496	3,812		32,877	132	275	38,592	204
wood Level 6		1,496	3,634		32,107	132	275	37,644	949
wood Level 7		1,340	2,453		12,478	2,275		18,546	13,404
wood Level 7 Mezz.		675			1,916			2,591	
Bay windows					2,250			2,250	
Subtotal	65,541	13,923	27,285	12,880	189,229	9,516	3,208	321,582	24,211
w/o TH units ave res sf	178,897	/	272	units	=			658	gsf / unit
with TH units ave res sf	189,229	/	278	units	=			681	gsf / unit

Total Rentable Area (Retail + Residential) 202,109

UNIT MIX

	STUDIO	OPEN 1-BED	1-BED	1-BED +	2-BED	TH UNITS	Total with TH UNITS	Total w/out TH UNITS
Level 1						6	6	0
L1 Mezz.				1			1	1
Level 2	17	10	16	2	4		49	49
Level 3	18	10	15	2	5		50	50
Level 4	18	10	15	2	5		50	50
Level 5	20	8	19		5		52	52
Level 6	18	8	19		5		50	50
Level 7	6		9		5		20	20
	97	46	93	7	29	6	278	272

5% Type A Accessible units 14 ADA units reqd

PROVIDED PARKING

Level P1	77
Level 1	78
L1 Mezz.	76
TOTAL	231

UNIT SIZE - DOES NOT INCLUDE L7 & MEZZ. UNITS

	TARGET SIZE	AVG. SIZE
STUDIO	36% 500	487
STUDIO +	17% 600	701
1-BED	34% 725	687
1-BED +	3% 850	972
2-BED	11% 950	929
	100%	

ZONING

- 2.1 Use:** SMC 23.48.004
- | | |
|------------------------|-----------|
| Residential | Permitted |
| General Sales/Svc | Permitted |
| Eating & drinking est. | Permitted |
| Parks & Open space | Permitted |
- 2.2 Height:** SMC 23.48.010A
Max. Allowed 65' height limit
- SMC 23.48.010E
Projections allowed above height limit: clerestories, guardrails, elevator/stairs overruns
COMPLIANT See diagram on sheet DR-3
- 2.3 Upper Level Setbacks:** SMC 23.48.012 Map A
No upper level setbacks required
- 2.4 General Façade Requirements:** SMC 23.48.014A
A primary building entrance shall be required from the street +/- 3' from sidewalk grade.
- SMC 23.48.014B
Minimum Façade Height 25' on Class 2 pedestrian streets (Dexter Ave only)
DEPARTURE REQUESTED See request and diagram on sheet T0.1
- All other streets minimum façade height of 15'
SMC 23.48.014.D
- Street Level Setback:
1. May have setback 12' max. from property line
2. Additional setbacks for 30% of the setback area located 20' min. from street corner.
Allowed: 410'-11" total length x .30 = **123'-3"**
Provided: 3 setbacks totaling **121'-1"**
SMC 23.48.024.3.a
- Parking in structures must be separated from street by other uses along Class 2 Pedestrian streets
COMPLIANT See diagram on sheet T0.2
- 2.5 Transparency and blank façade:** SMC 23.48.018.A.1
- Transparency:
No transparency at residential use.
a. Class 2 pedestrian streets minimum 60% of façade width must be transparent
b. Other streets: minimum 30% of façade width must be transparent
c. When slope of street > 7.5%, transparency may be reduced to 45% on Class 2 streets.
SMC 23.48.018.B.3
- Blank Facades:
a. Class 2 pedestrian streets - maximum 15' wide (except garage doors), total <40%
b. Other streets - maximum 30' wide (except garage doors), total < 70%
COMPLIANT See diagram on sheet T0.2
- 2.6 Street Level Uses:** SMC 23.48.019
Only apply to Class 1 pedestrian streets. - None on this project.
- 2.7 Residential Amenity Area:** SMC 23.48.020
- | | | |
|-----------------------------------|------------------|-------------------------|
| Required: 5% gross resid. space = | 12,018 SF | Required |
| Provided: | 7,812 SF | roof deck amenity area |
| | 949 SF | street level open space |
| | 3,771 SF | interior amenity space |
| | 12,532 SF | provided |
- Up to 50% of residential amenity may be enclosed (atriums, greenhouses, solariums)
Allowed 12,532 x .50 = **6,266**
Provided **3,771**
- Minimum dimensions: 15 feet, not less than 225 square feet
The residential amenity area shall be available to all residents
COMPLIANT See diagram on sheet T0.2
- 2.8 Screening and Landscaping Standards:** SMC 23.48.024
Required: street trees per SDOT

Refer to Landscape Drawings

- 2.9 Solid Waste & Recyclables:** SMC 23.48.031
- Required: 100+units = 200 s.f. + 2 s.f. for each additional unit. **556**sf front-loading type
- Provided: 2 storage areas with a total area = **923** sf
- Access: 1. Front-loading dumpsters shall have direct access from alley or street
2. Gates and access way shall be a minimum of 10' wide.
3. 21' overhead clearance required.
- COMPLIANT** Refer to sheets A1.1, A3.0 & A4.3

- 2.10 Required Parking and Loading:** SMC 23.48.034
- Parking: 23.54.015.B applies:
"In commercial zones in urban centers, no parking is required, except for fleet vehicles."
It was confirmed that the project is in the South Lake Union Urban Center, and that the SM zone is a "commercial zone" according to the definitions section 23.84.048, "zone, commercial."
SMC 23.48.034.C4
- Access: Director shall determine whether location of parking/loading will expedite movement of vehicles
SMC 23.53.030F
Provided: Access to 150 parking spaces from Dexter Ave and access for 75 parking spaces from Aloha St.
- Curb cuts: Director to review & make recommendation
5 allowed on Dexter Ave and 2 allowed on Aloha St
Provided: 1 on Dexter Ave and 1 on Aloha St.

Provided Parking

	S	M	L	BF	S	M	L	Totals
P1				2	31	44	0	77
L1				2	28	48	0	78
L1-M				2	26	48	0	76
subtotal	0	0		6	85	140		231

COMPLIANT

61% Medium
37% Small
3% Barrier Free

SMC 23.54.030.D.2a(2)

Driveway Width

The minimum width of driveways for two way traffic shall be twenty-two (22) feet and the maximum width shall be twenty-five (25) feet.
DEPARTURE REQUESTED See request and diagram on sheet T0.1

SMC 23.54.030.G

Driveway sight triangle: 10' triangle required

DEPARTURE REQUESTED See request and diagram on sheet T0.1

Bicycle Parking

SMC 23.54.015 Chart E

		Ratio	Required
Sales & Service	12,880 sf	1/ 12000	1.07
Residential	278 units	1/ 4	69.50
			71 long-term bicycle parking stalls
Sales & Service	12,880 sf	1/ 4000	3.22 short-term bicycle parking stalls

SMC 23.54.015.K

After the first 50 spaces for bicycles, additional spaces are required at one half the ratio shown in Chart E.Ⓜ

69.50 total spaces required
50 spaces at 100% = 50
19.50 spaces at 50% = 9.75
Adjusted total required = 59.75 long and short-term bicycle parking stalls

Provided

Level 1 51 long-term bicycle parking stalls
Level 1 Mezzanine 21 long-term bicycle parking stalls
Level 1 4 short-term bicycle parking stalls
Total provided 76 long and short-term bicycle parking stalls

COMPLIANT

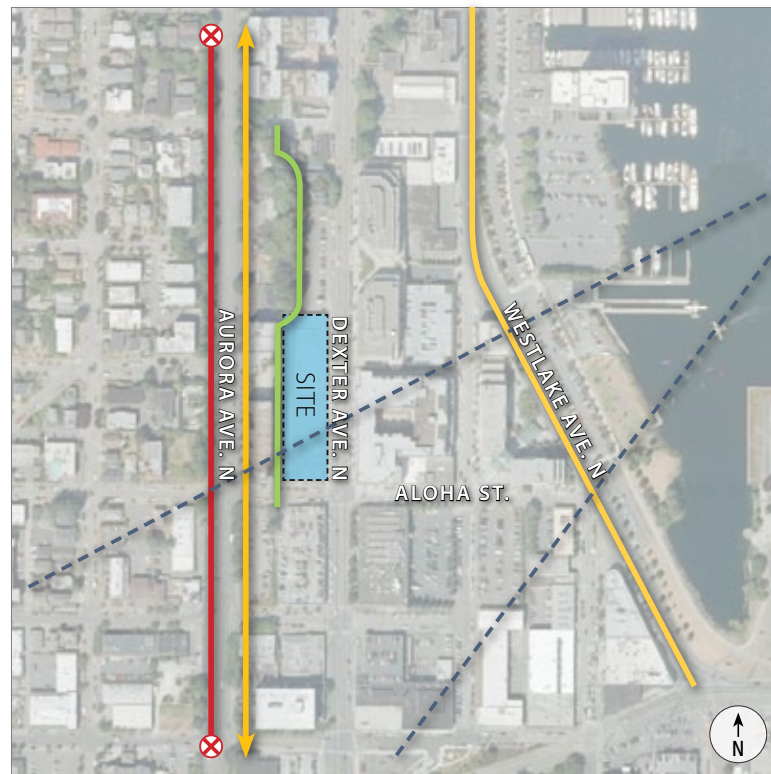
SMC 23.54.035 B.1

For uses with less than 16,000 gsf which provide a loading space on a street or alley, the loading berth requirements may be waived by the Director

COMPLIANT 40' commercial load zone located on Dexter Ave, to be coordinated through SDOT SIP process, Refer to sheet A0.1

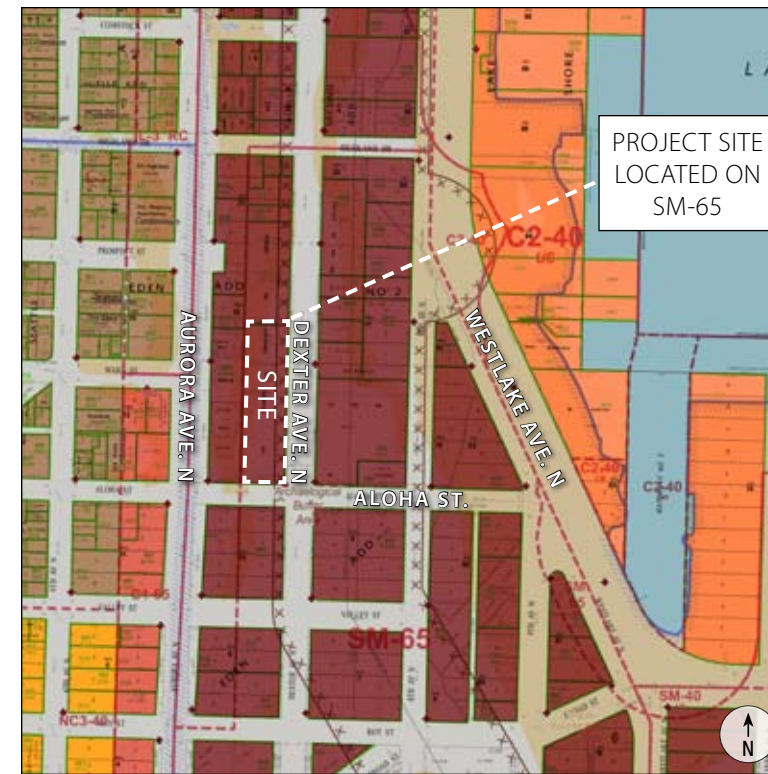
SITE CONTEXT: OPPORTUNITIES & CONSTRAINTS

CONSTRAINTS



- Steep slope
- Afternoon shadow
- Substantial shoring required
- Aurora cut-off pedestrian connection to west residential neighborhoods
- Fast moving traffic along Aurora Ave.
- Noise impact of traffic
- Seaplane approach
- Brisk traffic along Westlake Ave. N
- Close to industrial neighborhood

ZONING



- Seattle Mixed-Use
- Commercial 1
- Commercial 2
- Neighborhood Commercial
- Low-Rise
- SITE

OPPORTUNITIES



- Dexter is principal bike street:
 - Ease of access to bike downtown
 - Link to city's interconnected bike route
- Dexter is Class 2 pedestrian street:
 - Bus 26 & 28 stop right on the corner of Dexter & Aloha; Runs frequently
 - Walking distance to trolley
- East solar access
- Neighborhood Entertainment: (walking distance)
 - Jillians
 - Bucca De-Beppo
 - S. Lake Union Boats
 - S. Lake Union Restaurants
- Active Entertainment: (walking distance)
 - Lake Union Park
 - Center for Wooden Boats
 - Kenmore Airplanes
 - Kayaking, Boating

DEVELOPMENT HISTORY

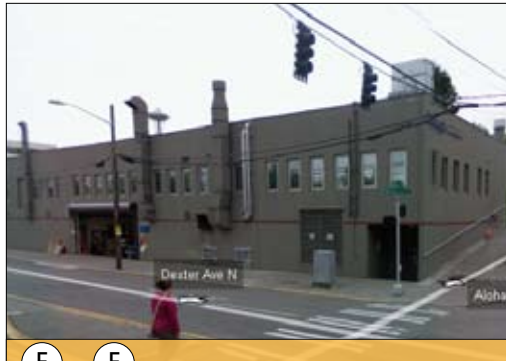


- Permit Before 2000
- Permit After 2000

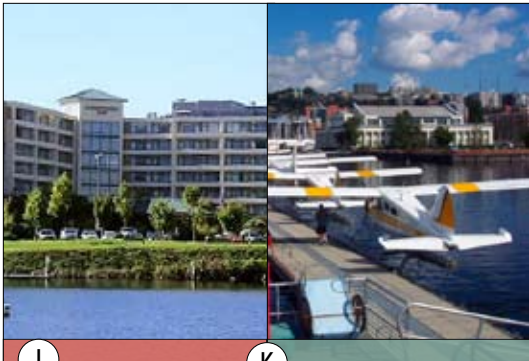
SITE CONTEXT: SURROUNDING USES



A 1200 Dexter Building - Apartments



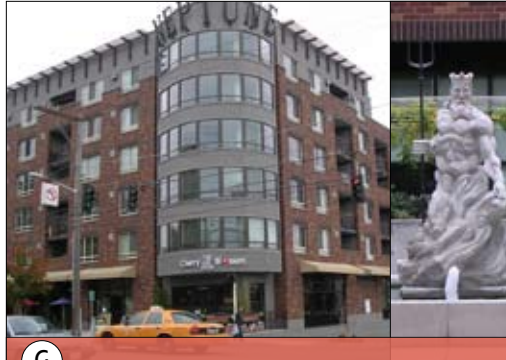
E **F** Esterline Korry Electronics Offices



J Marriott Hotel **K** Kenmore Airplanes



B Proposed 1101 Dexter MUP # 3006945



G The Neptune Apartments



L Center for Wooden Boats



C Alterra Condo Building

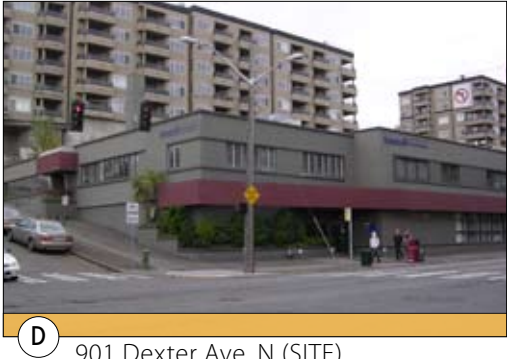


H 1000 Dexter - Office Building

M Jillians Restaurant & Bar

N Maaco Auto Repair

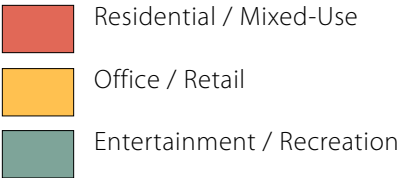
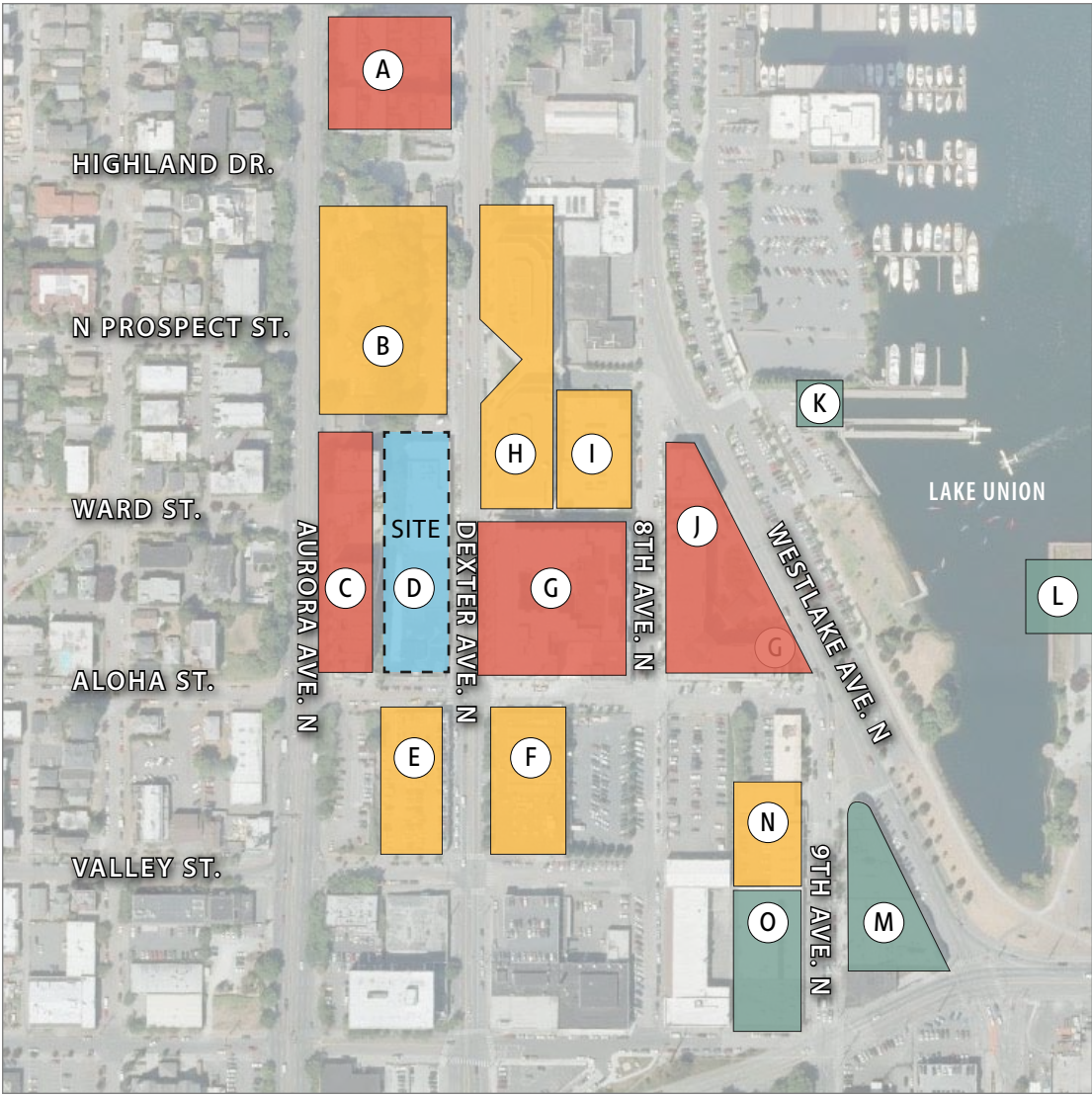
O Bucca De Beppo - Italian Restaurant



D 901 Dexter Ave. N (SITE)



I US Bank



SITE EXISTING CONDITIONS

PROJECT SITE



Corner of Dexter Ave. N & Aloha St. - Looking NW (toward site)



Existing Site Plan

SITE EXISTING CONDITIONS: PANORAMIC VIEWS



PROJECT SITE



Dexter Ave. N - Looking W (toward site)



Aloha St - Looking N (toward site)

PROJECT SITE



OPPOSITE PROJECT SITE



Aloha St - Looking S (away from site)

OPPOSITE PROJECT SITE



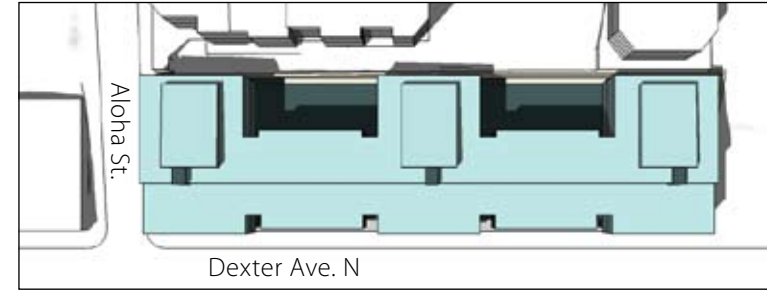
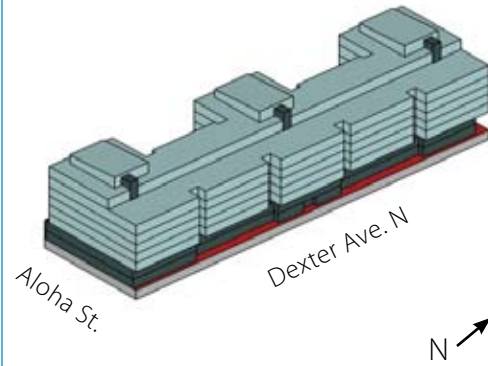
Dexter Ave. N - Looking E (away from site)

SUMMARY OF EARLY DESIGN GUIDANCE

MASSING PRESENTED TO EDG BOARD:

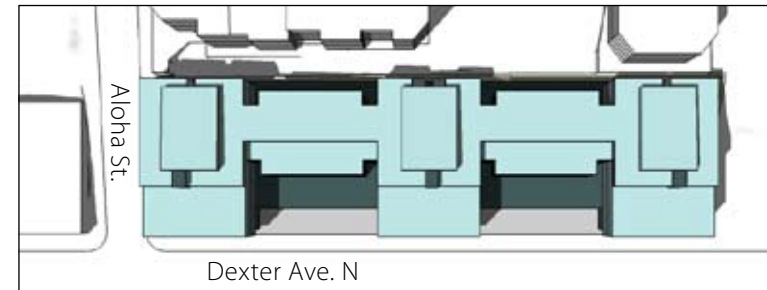
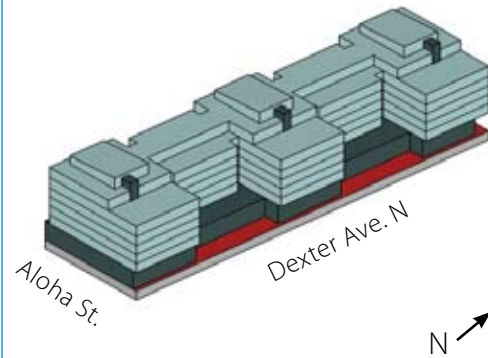
“E”-SCHEME

- The first scheme (“E”) included a continuous street wall along Dexter Avenue N with Level 2 courtyards on the west, facing the Alterra condominiums. The applicant noted that the west-facing units and courtyards will be severely compromised with low quality light due to the site’s steep slope and orientation to the west.



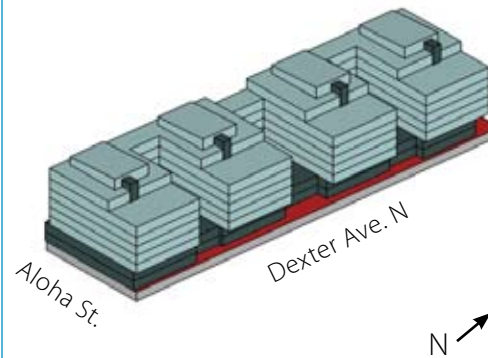
“3-POD” SCHEME

- The second scheme (“3-pod”) was a variation to the E-scheme, where the Level 2 courtyards are located on the east, breaking up the façade along Dexter Avenue N. The applicant also noted that the west-facing units would be severely compromised with low quality light but the east-facing courtyards will have better sun exposure than the E-scheme.



“4-POD” PREFERRED SCHEME

- The third, and applicant preferred, scheme (“4-pod”) proposed an expression of four solid blocks with voids for Level 2 courtyards fronting Dexter Avenue N, connected by single-loaded corridors along the west. The applicant noted that this scheme was a more appropriate response to the site by eliminating western-facing units on a steep slope, in an effort to take advantage of the eastern exposure and also to avoid privacy issues with the Alterra residents.



EDG BOARD RECOMMENDATIONS:

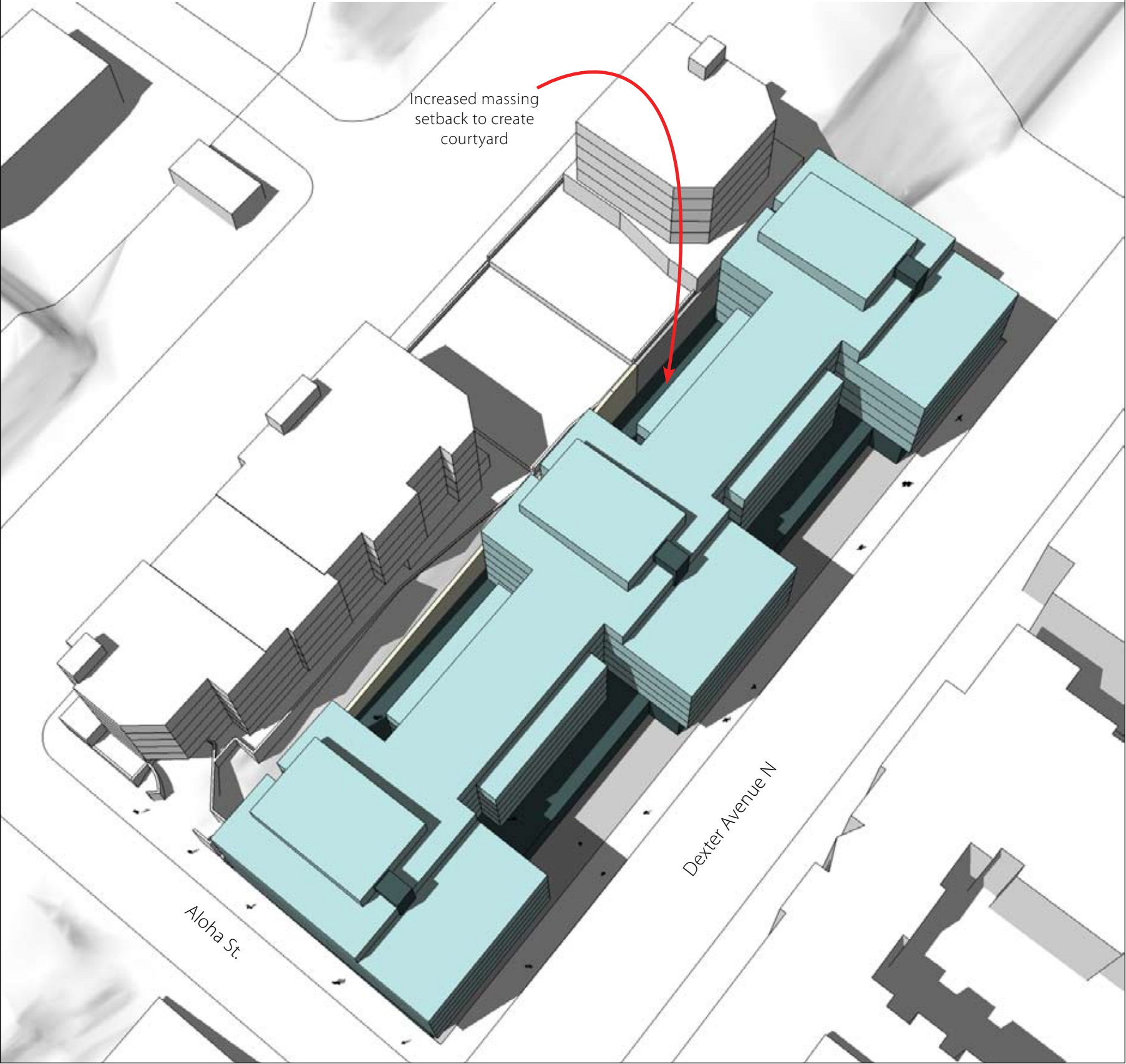
- 1 COMPROMISE FOR MASSING SUCH AS S-SCHEME OR MODIFICATION OF 3-POD SCHEME TO PROVIDE MORE RELIEF FROM ALTERRA BUILDING
- 2 ENCOURAGE LANDSCAPING ALONG WEST FAÇADE
- 3 BREAK UP LONG WALL AT WEST FAÇADE (FACING ALTERRA)
- 4 CONCERN WITH COURTS FEELING NARROW
- 5 ROOF DETAILING NEEDS SPECIAL CONSIDERATION FOR MECHANICAL EQUIPMENT
- 6 ENCOURAGED SETBACK ON UPPER FLOORS SPECIFICALLY ON WEST SIDE
- 7 CONSIDER NEIGHBORHOOD CONTEXT FOR MATERIAL PALETTE

1

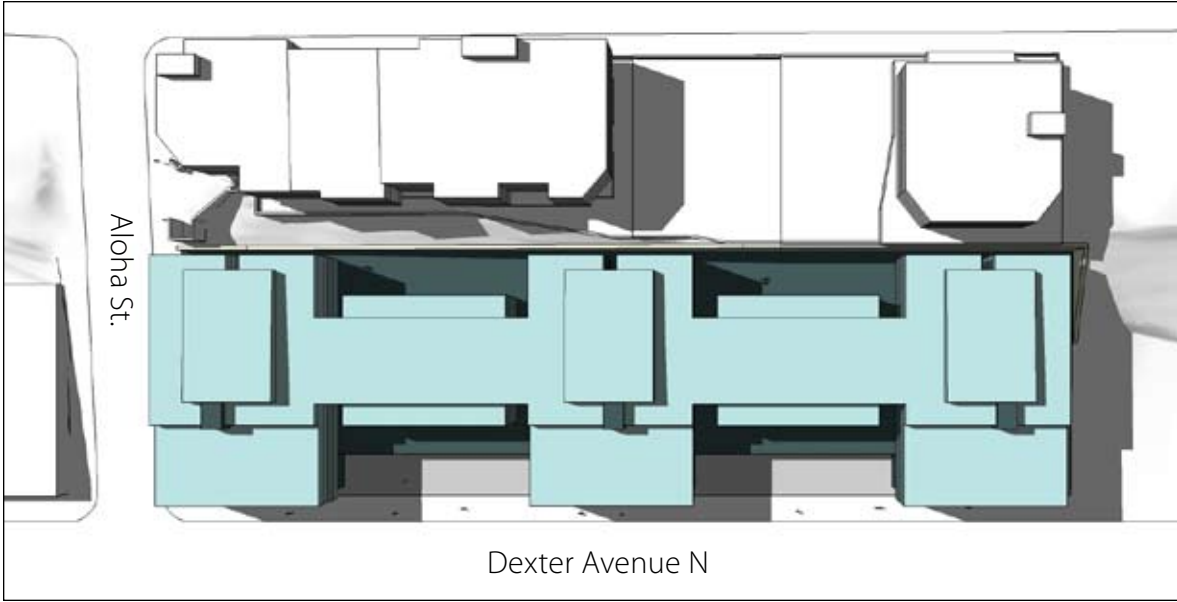
ALTERNATE SCHEME POST-EDG

At the EDG meeting the Board recommended a compromise for the massing such as an "S" scheme or a modification to the "3-pod" scheme by pushing the central corridor to the east to provide more relief from the Alterra buildings.

The result of the compromise, a modified "S" scheme (shown at right) pushes the massing to the east and creates a courtyard at the west. However, the courtyard is buried 20'-30' below grade, resulting in undesirable residential units facing a courtyard that is constantly in the shade.



Birds Eye View



Plan View

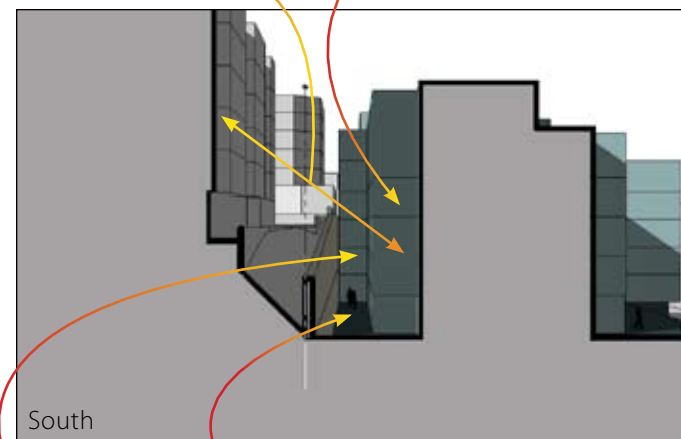
DESIGN PROGRESSION

A series of sections through the site at the south, middle and north were taken after the EDG meeting to analyze the quality of spaces for each of the schemes:

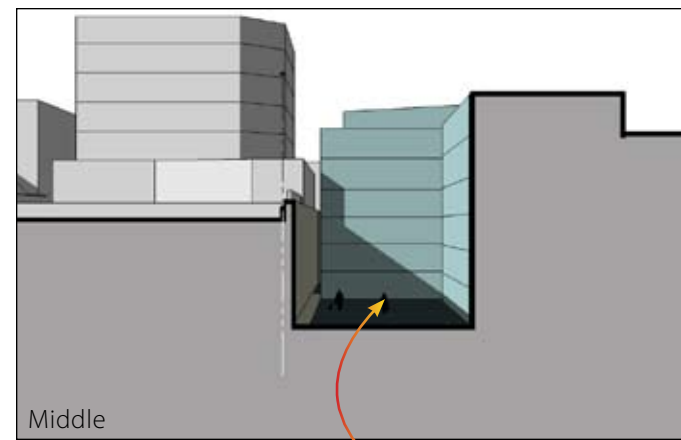
E SCHEME:

Majority of units would look down courtyard buried 20'-30'. Courtyard is constantly in the shade resulting in undesirable units. Alterra residents would look down five story building.

Privacy constraints on both buildings
50% Building facade facing Alterra & hole



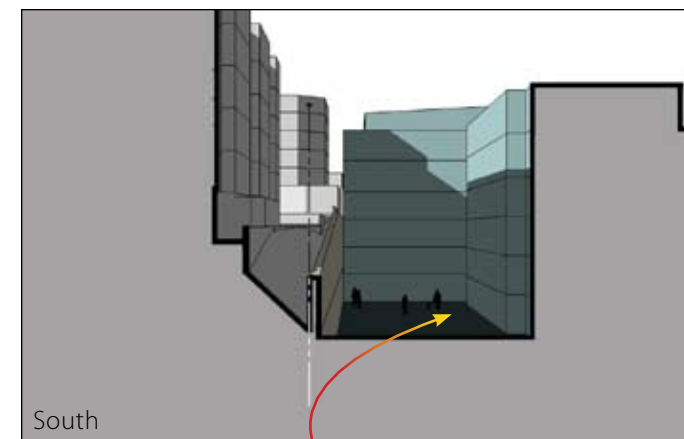
4 stories of subterranean units
Courtyard buried ≈ 30 ft. below adjacent grade, creating a hole & unlivable spaces



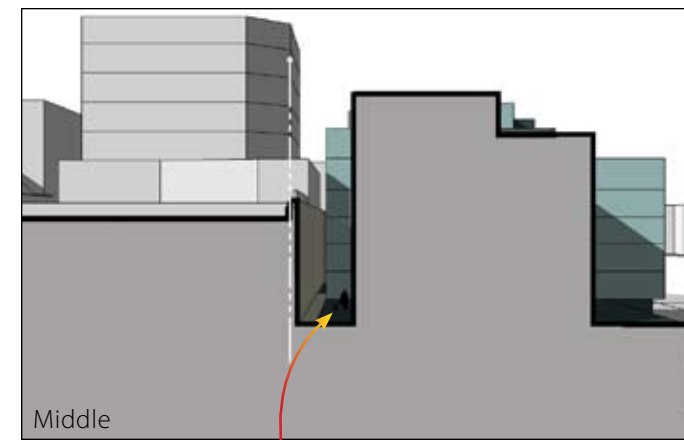
Dark courtyard

3 POD SCHEME:

Western courtyard buried 20'-30' is constantly in the shade and shallower depth results in undesirable units. Alterra residents would look down five story building.



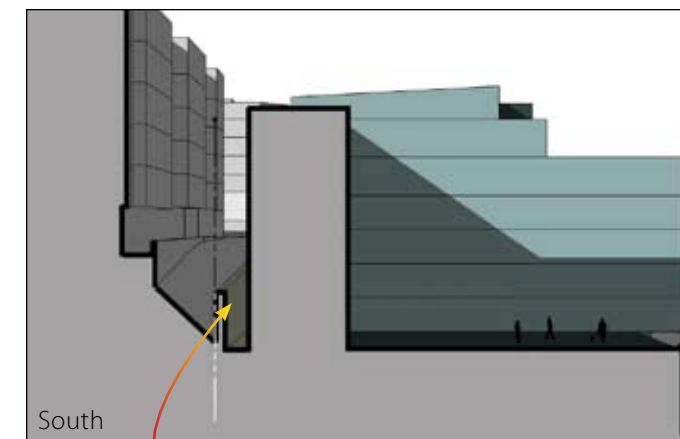
Courtyard buried ≈ 30 ft. below adjacent grade, creating a hole & unlivable spaces



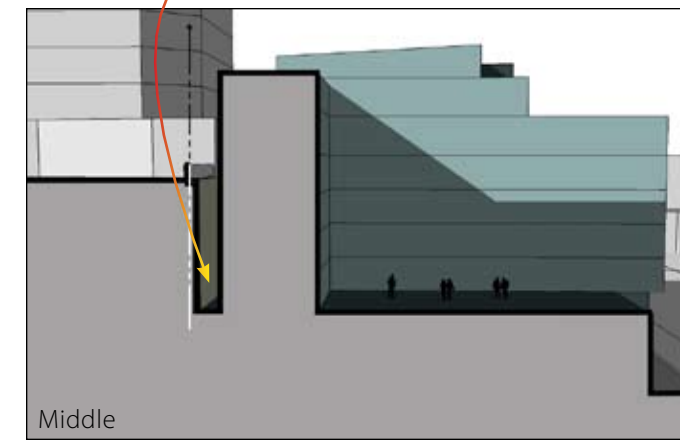
Dark shallow courtyard

4 POD SCHEME:

Majority of units face courtyard oriented to east with morning sun, resulting in the most desirable units and usable, desirable courtyards. Gap at west means Alterra residents would look down five story building but more privacy for Alterra units..



could be improved by filling in

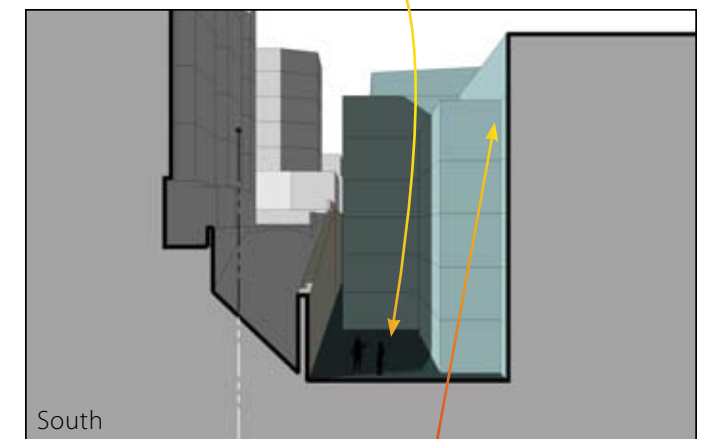


Dark slot

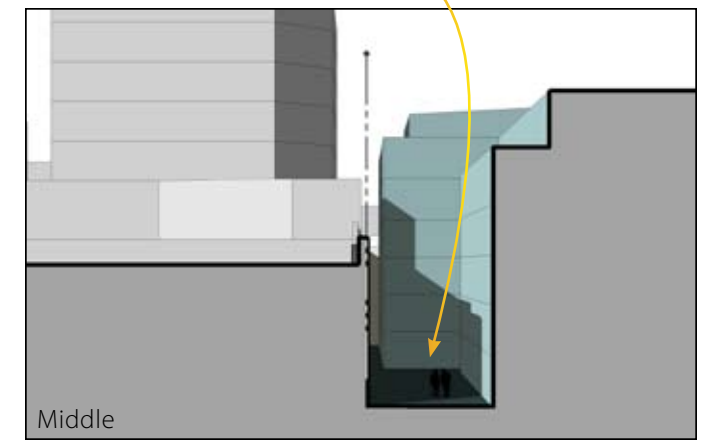
S SCHEME

Majority of units would look down courtyard buried 20'-30'. Courtyard is constantly in the shade resulting in undesirable units. Alterra residents would look down five story building

Dark, deep courtyard



Read scale of 5 story building

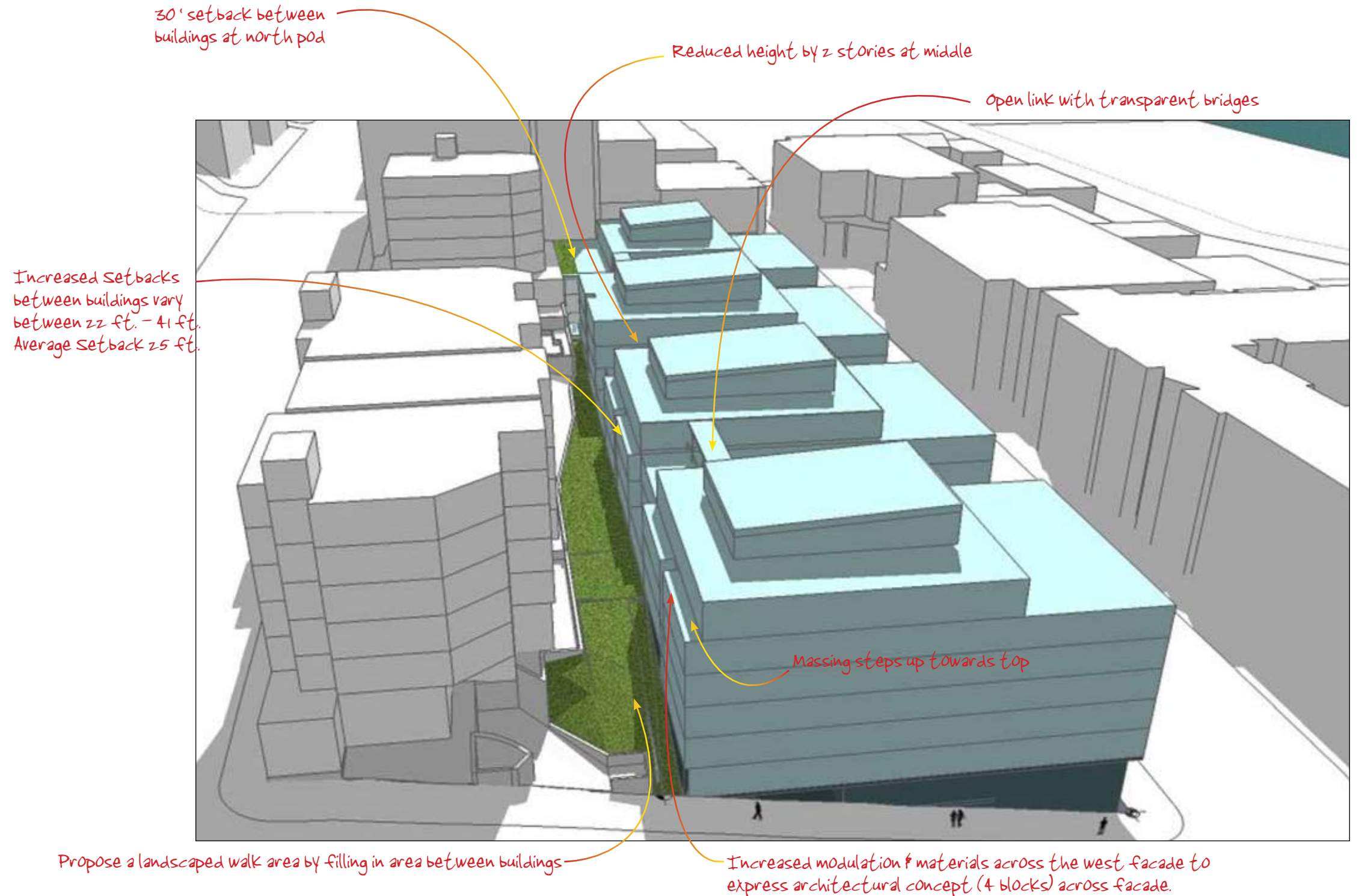
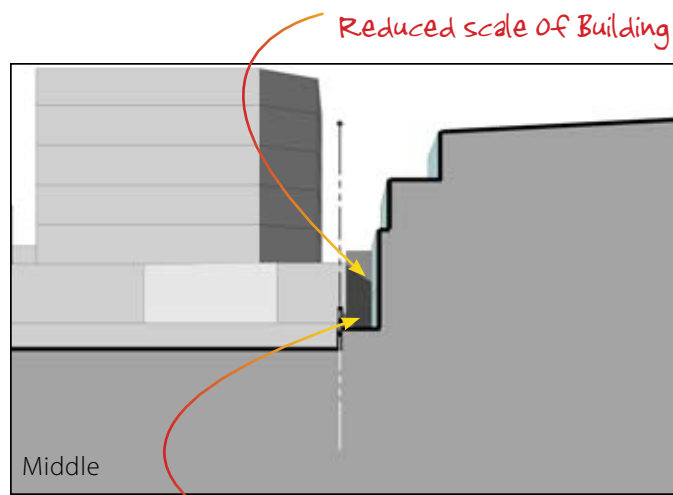
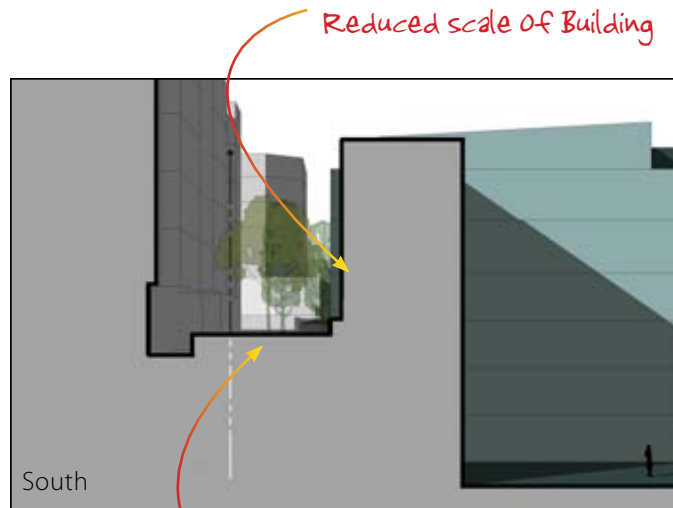


Dark shallow courtyard

The revised 4-Pod Scheme is considered the best response to the site characteristics by the applicant. The revised 4-Pod scheme responds to the challenge of a sloped site by burying the building into the site and taking advantage of daylight on the downhill side. Additional revisions to the massing include: a proposed landscape walk along the west facade, modulation and setbacks at the upper levels along all facades.

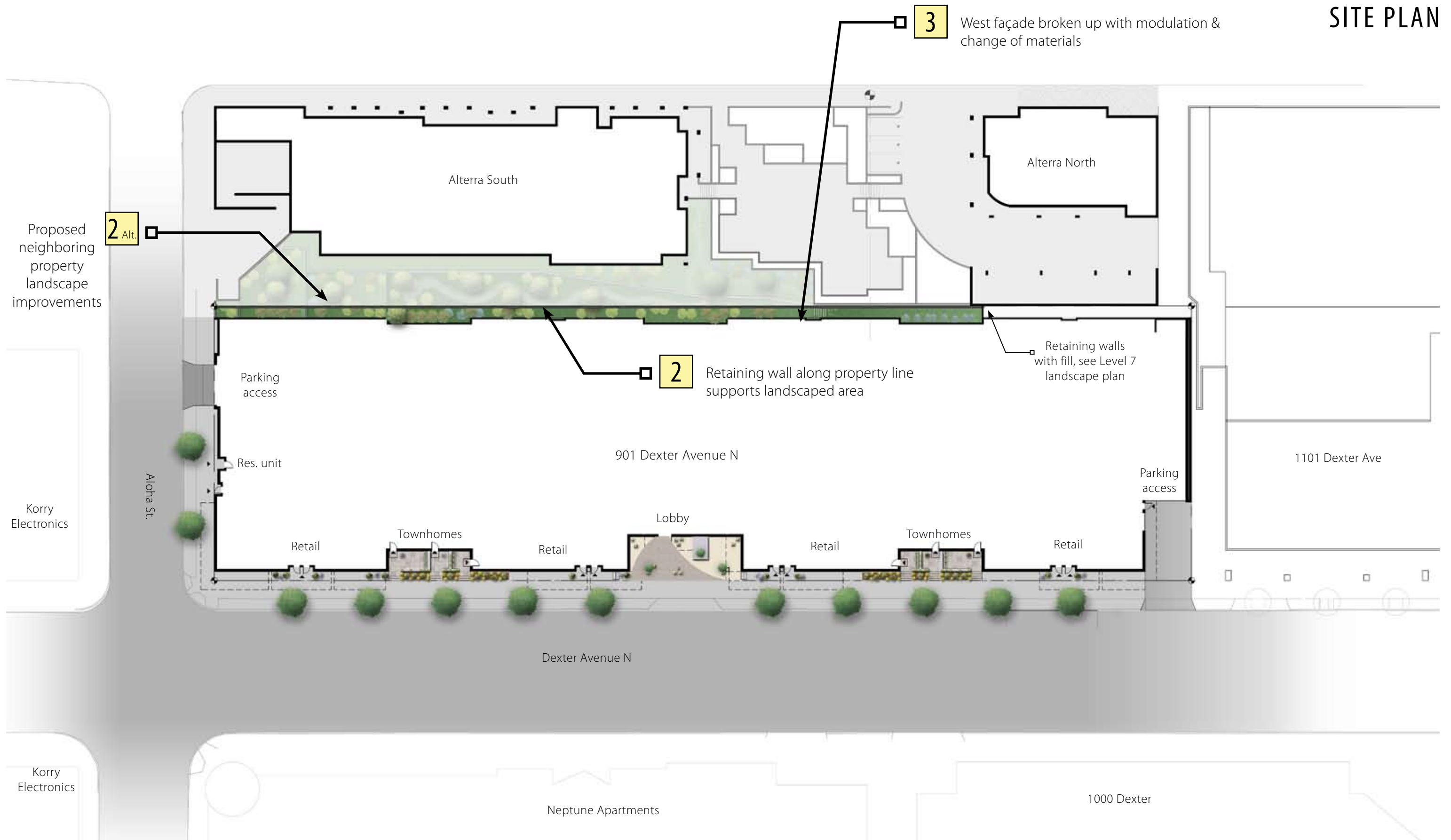
4 POD REVISED SCHEME

Narrow slot at west filled in as part of proposed neighboring property improvements, providing a usable green space for Alterra residents and reducing the scale of the proposed building along the west facade.



Propose a landscaped walk area by filling in area between buildings

Increased modulation & materials across the west facade to express architectural concept (4 blocks) across facade.



LANDSCAPE DETAIL ALONG WEST FAÇADE

PLANTING PLAN



PROPOSED PLANTING TYPES



Thuja Plicata Hogan



Cornus 'Midwinter Fire'



Oemleria Cerasiflora



Vaccinium Ovatum



Ribes Sanguineum



Philadelphus Lewisii



Acer Circinatum



Mahonia Aquifolium



Oxalis Oregana



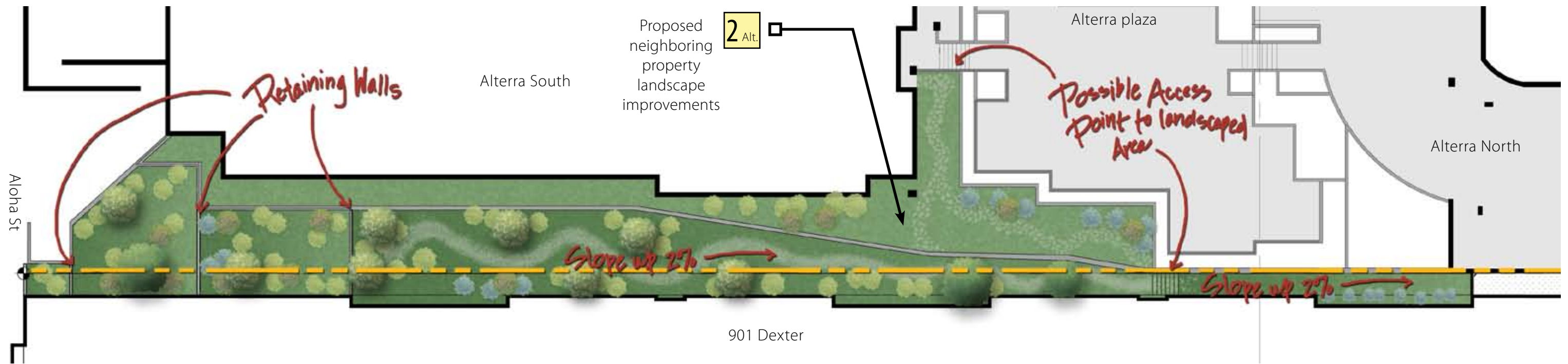
Blechnum Spicant



Vancouveria Hexandra

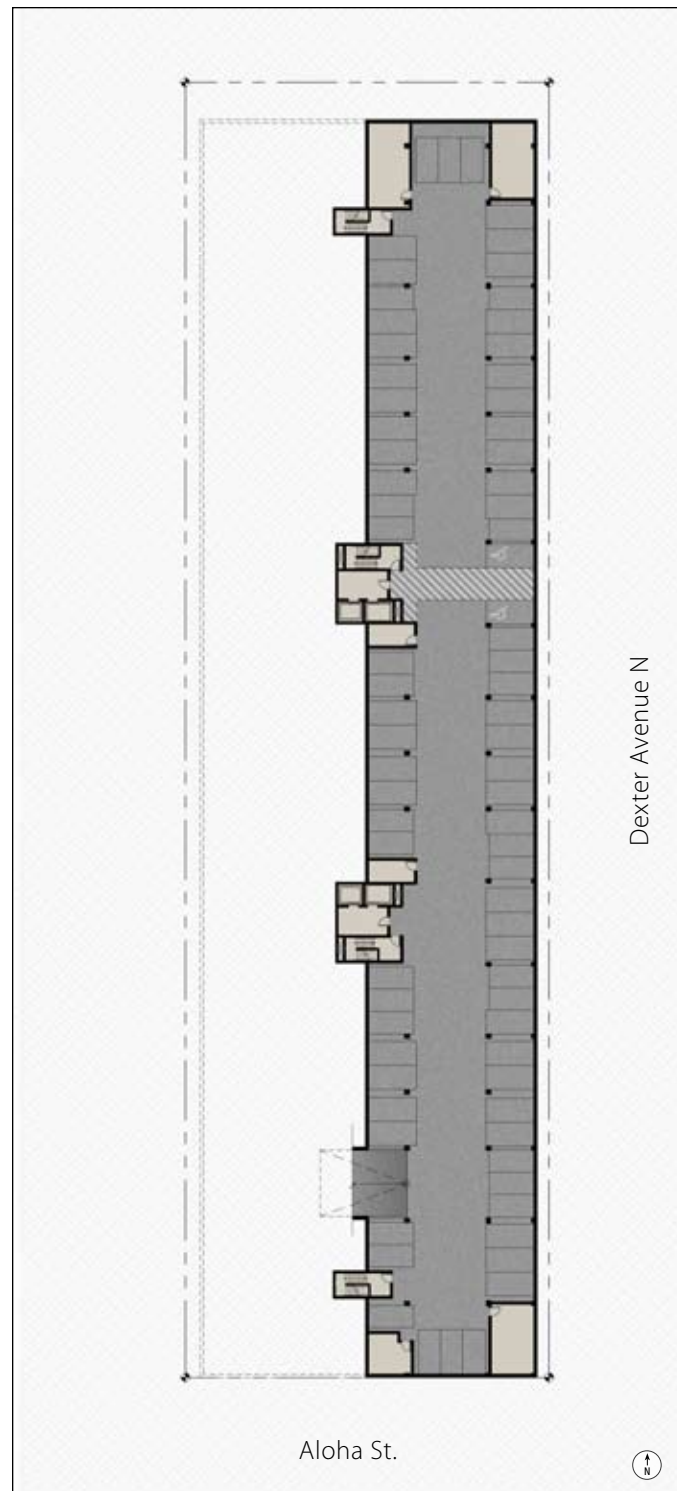
ALTERNATE STUDY: PROPOSED NEIGHBORING PROPERTY LANDSCAPE IMPROVEMENTS

PLANTING PLAN

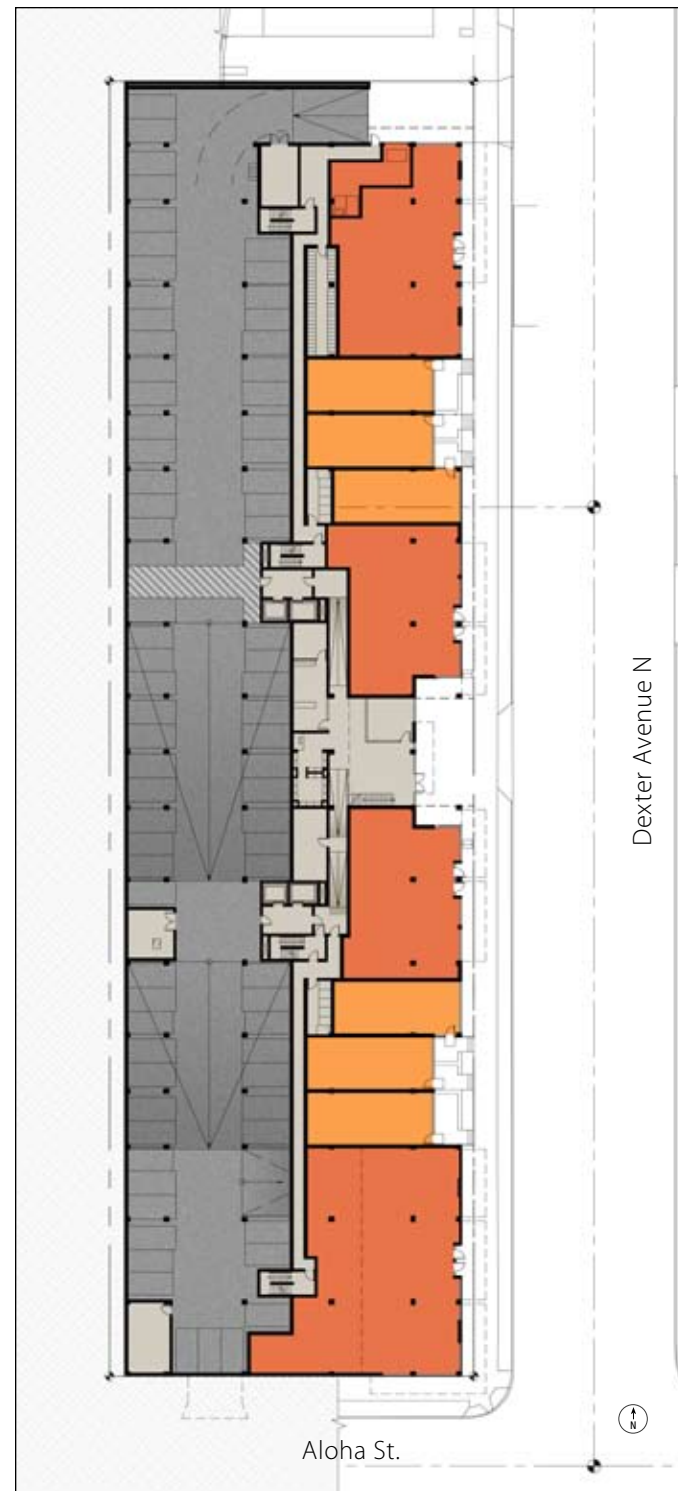


Landscape walk looking north from Alterra

ARCHITECTURAL PLANS



PARKING LEVEL 1



LEVEL 1



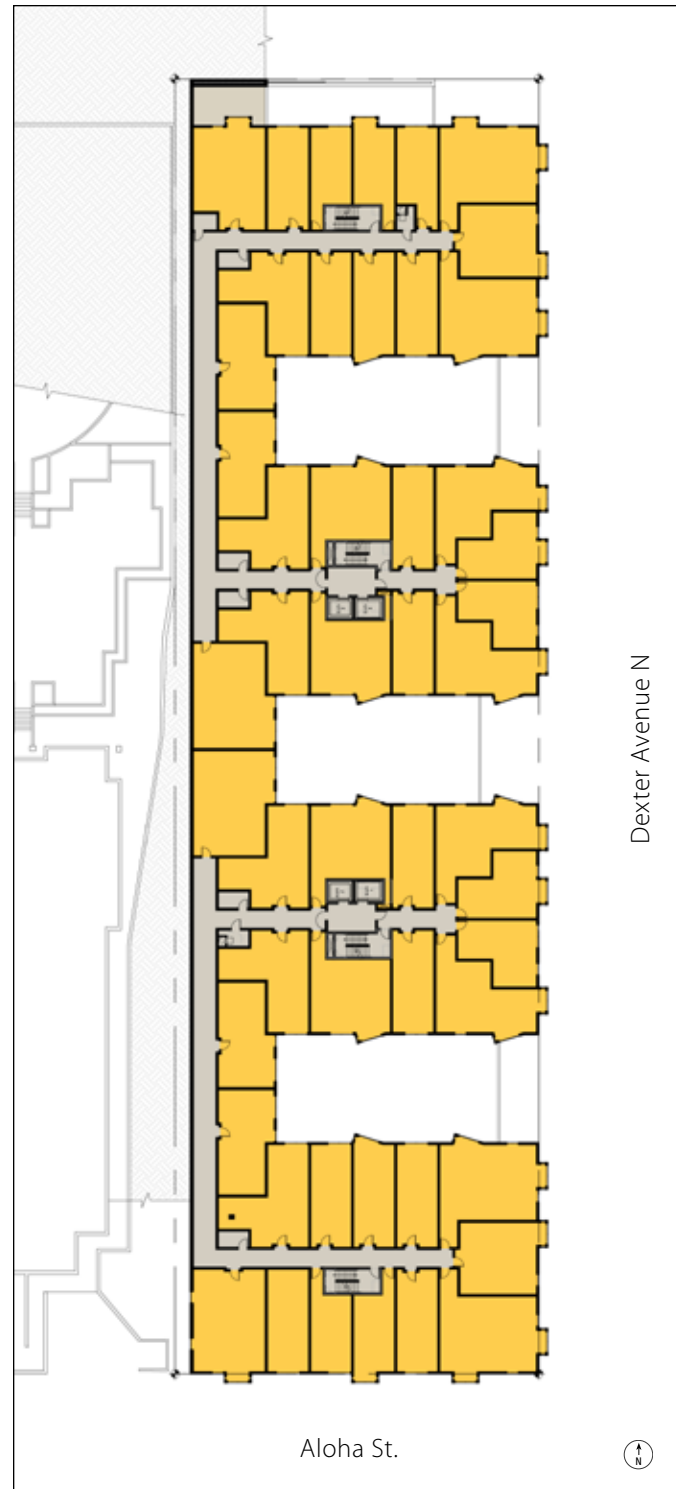
LEVEL 1- MEZZANINE



LEVEL 2

- Retail
- Townhome
- Residential
- Circulation
- Parking

ARCHITECTURAL PLANS



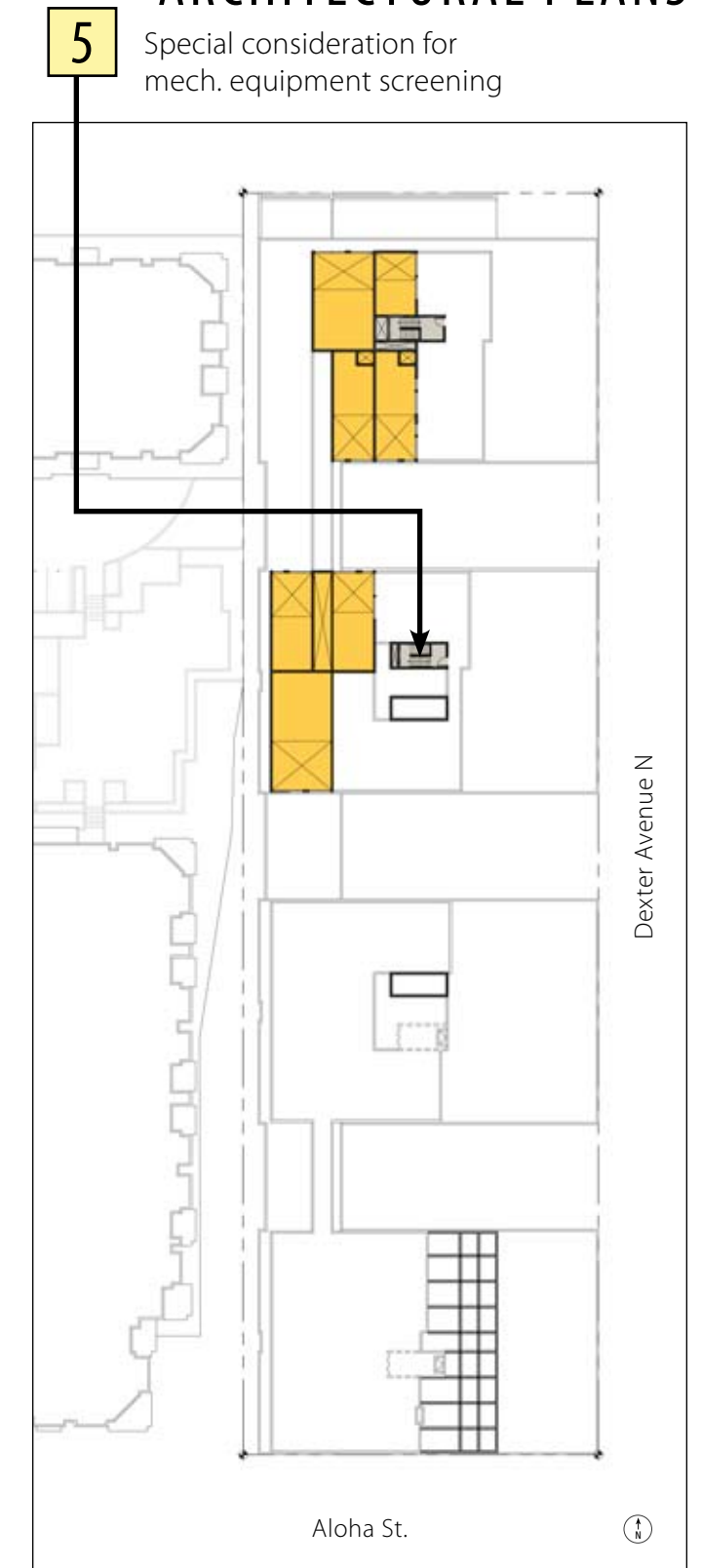
LEVEL 3-5



LEVEL 6



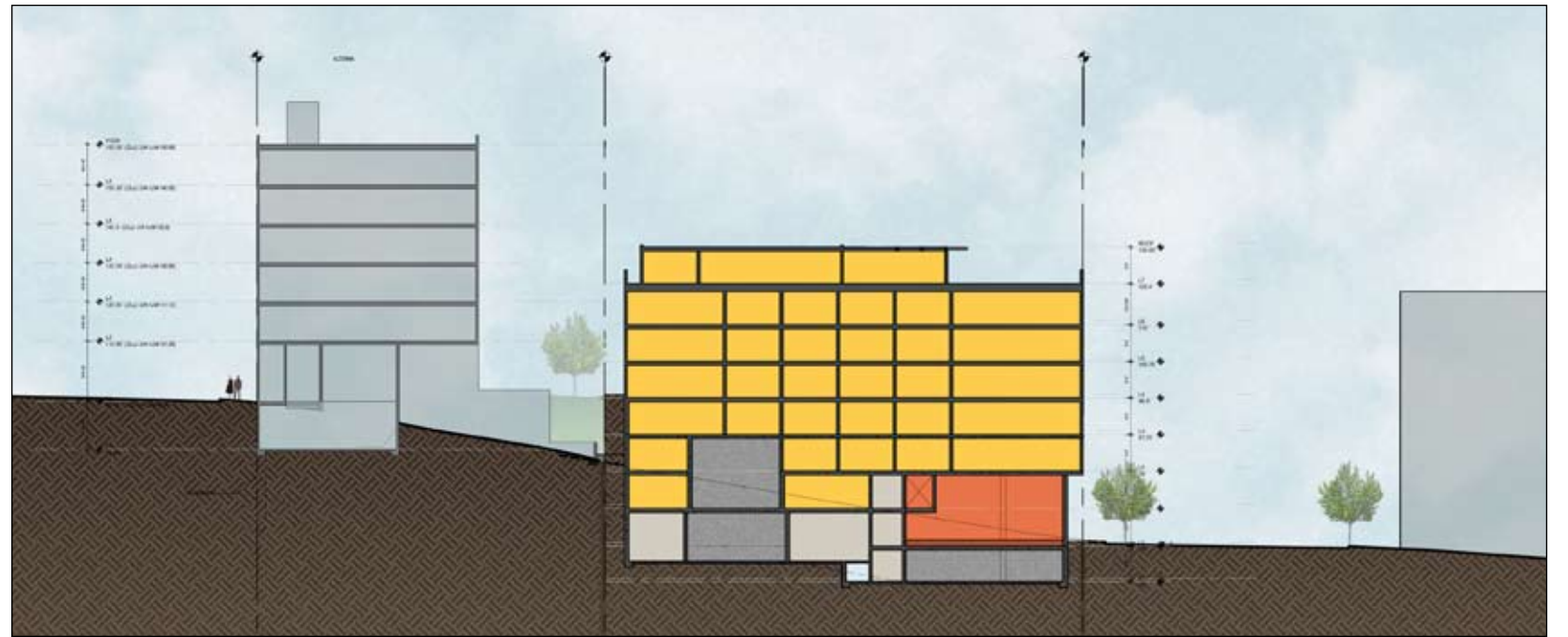
LEVEL 7



LEVEL 7-MEZZANINE

Townhome
 Residential
 Circulation

SECTIONS



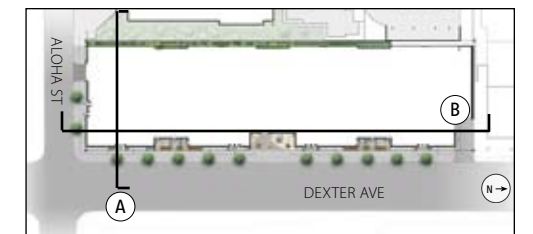
A-Transverse section

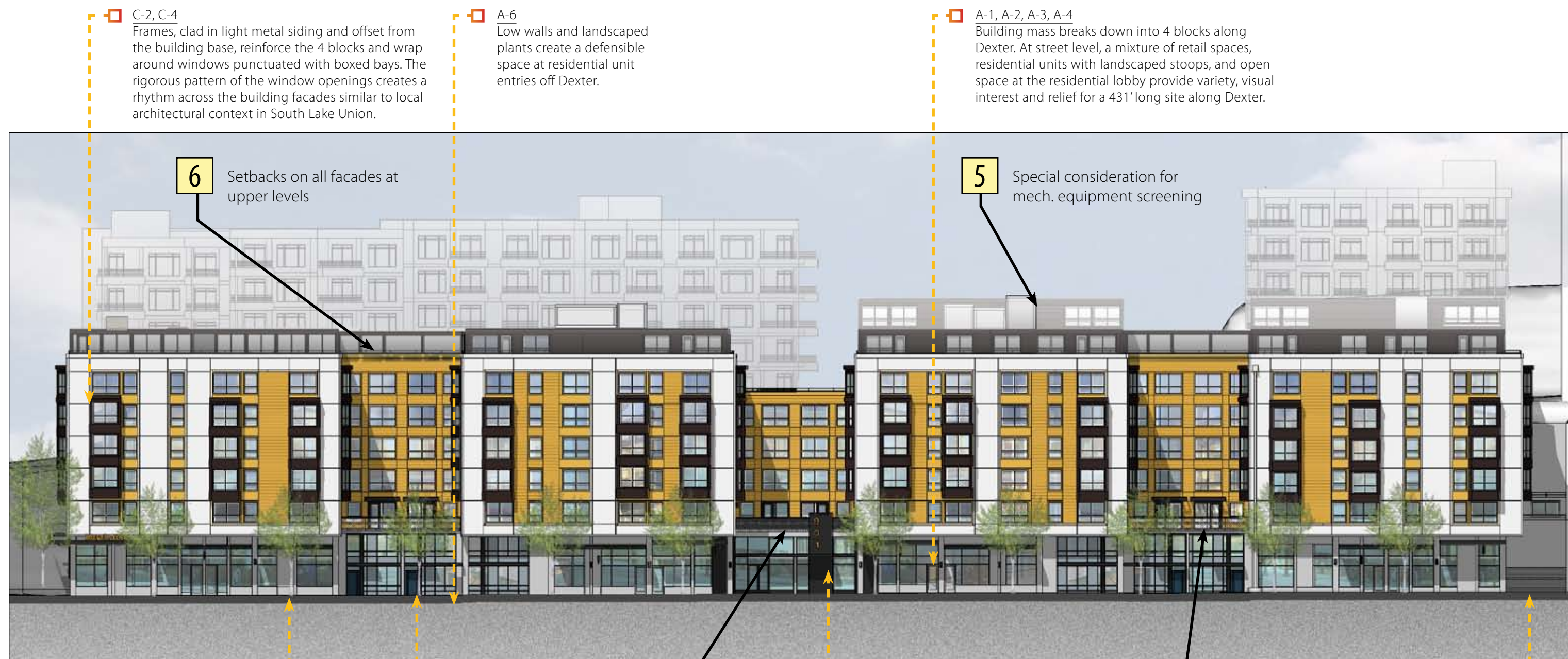


B- Longitudinal section

KEY

- Retail
- Townhome
- Residential
- Circulation
- Parking





East Elevation - Dexter Ave N

C-2, C-4
Frames, clad in light metal siding and offset from the building base, reinforce the 4 blocks and wrap around windows punctuated with boxed bays. The rigorous pattern of the window openings creates a rhythm across the building facades similar to local architectural context in South Lake Union.

A-6
Low walls and landscaped plants create a defensible space at residential unit entries off Dexter.

A-1, A-2, A-3, A-4
Building mass breaks down into 4 blocks along Dexter. At street level, a mixture of retail spaces, residential units with landscaped stoops, and open space at the residential lobby provide variety, visual interest and relief for a 431' long site along Dexter.

6 Setbacks on all facades at upper levels

5 Special consideration for mech. equipment screening

4 Increased quality of space in courts

7 Material palette consistent with neighborhood context

A-1, A-2, B-1
A voluntary setback of 4' along Dexter Ave. provides a more generous R.O.W. and allows for a variety of transitions and landscaping treatments from the sidewalk to the different functions along Dexter (retail, townhouse, lobby entry).

D-7
Lighting and landscaping at townhouse unit entries enhance personal and public safety and security.

A-4, D-1, D-7
At the residential lobby entry, generous open space with clear sight lines is provided to encourage pedestrian use. Retail flanks the lobby entry plaza to further activate the space.

A-8
One curb cut along Dexter minimizes the impact of driveways and enhances pedestrian and cycling safety.

ELEVATIONS: NORTH & SOUTH

A-5
Building mass sets back incrementally at west façade to respond to the Alterra building.

D-7
A residential unit entry along Aloha activates the streetscape and provides eyes on the street. Lighting and access design will enhance personal and public safety and security.



South Elevation - Aloha St

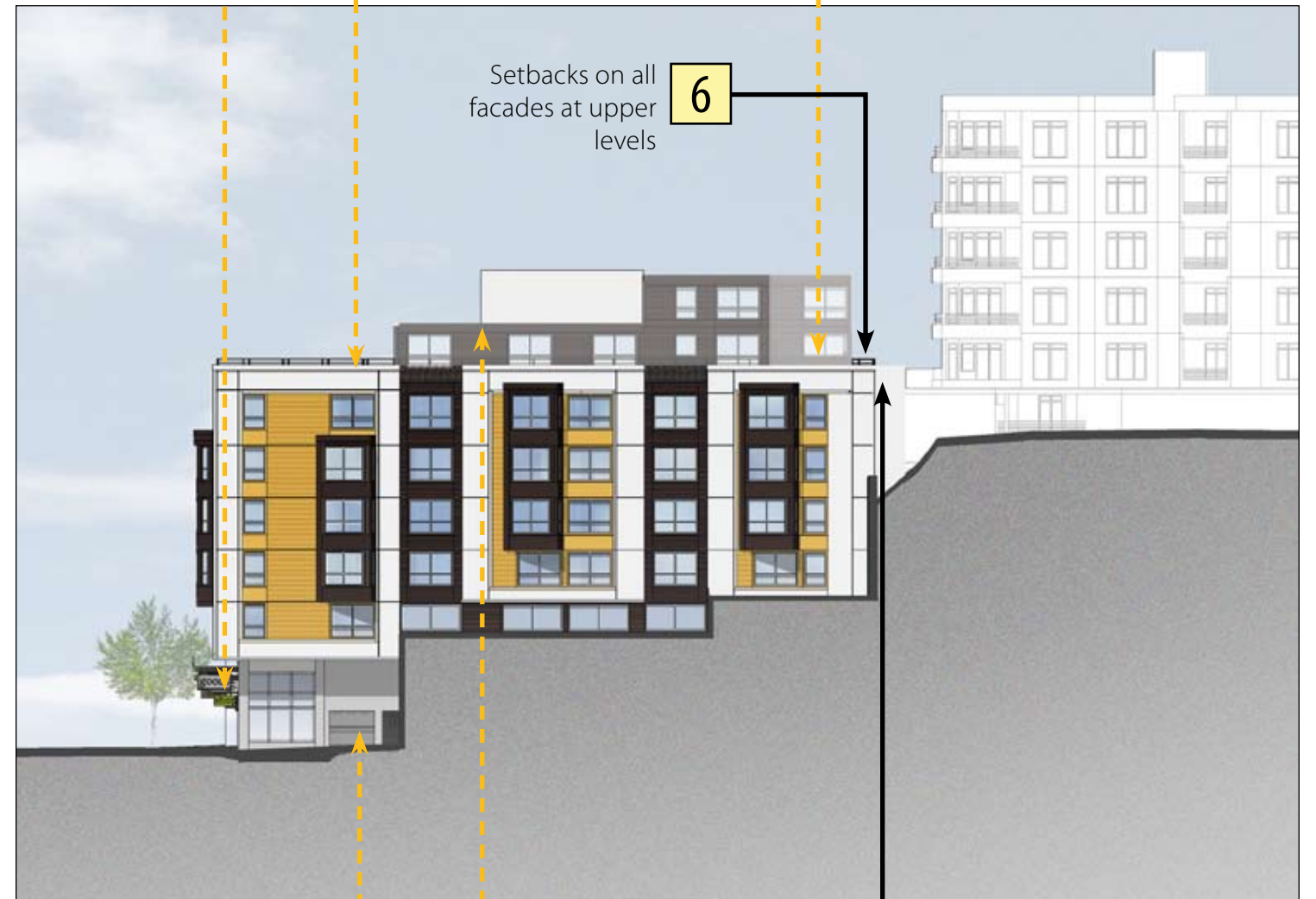
6 Setbacks on all facades at upper levels

A-1, A-2
Building mass responds to steep slope of site by burying parking and mechanical spaces and allowing residential units maximum exposure to eastern light and air.

A-10, C-2, C-4
Retail spaces are clad in concrete and expansive storefront to increase exposure and provide maximum transparency into the building.

A-10, C-2, C-4
Large, landscaped common roof decks located at east.

A-5
Building mass sets back at west façade to provide 30' separation between building and Alterra North. A landscaped common roof deck is located between building and west property line.



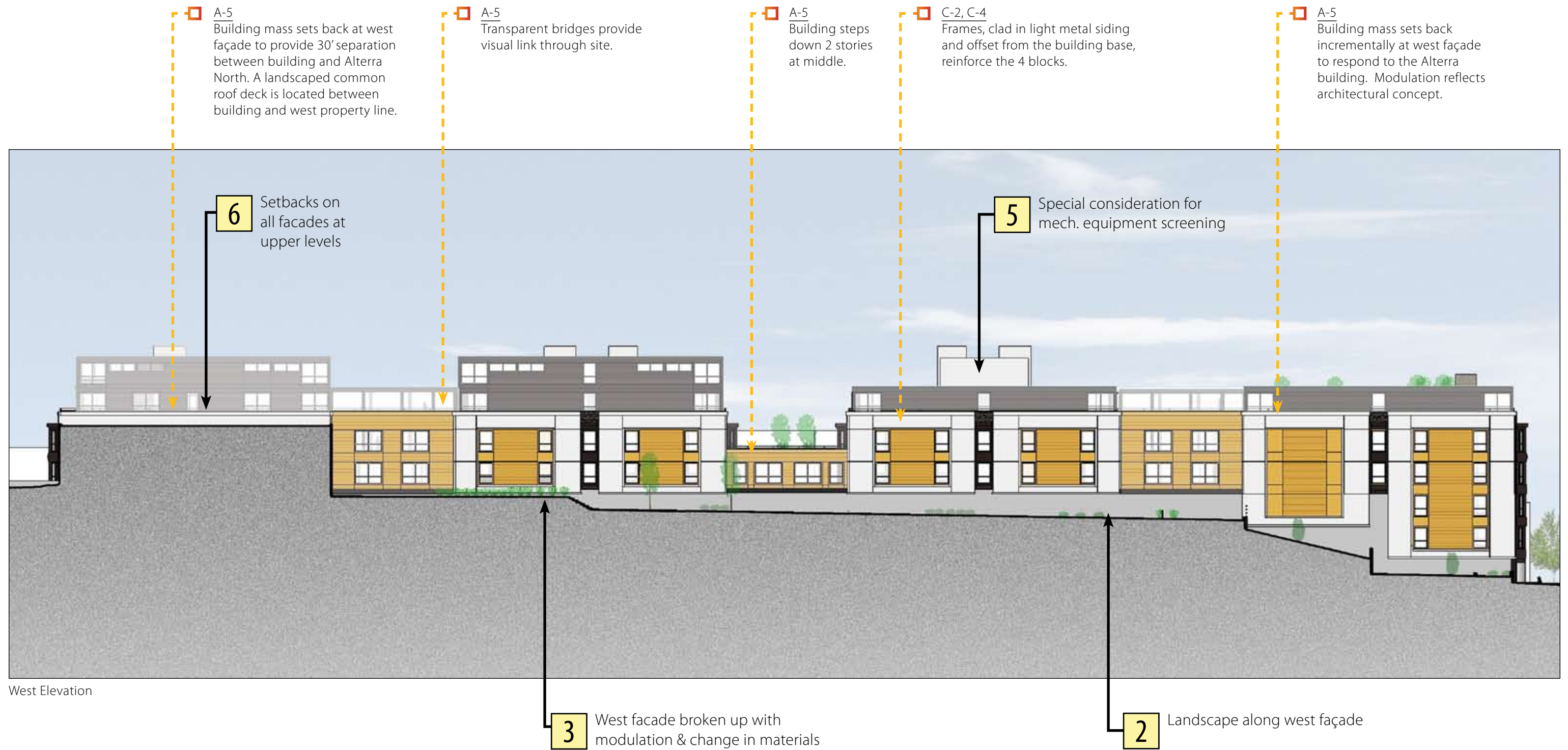
North Elevation

Setbacks on all facades at upper levels **6**

A-4, D-1, D-7
Trash is enclosed and access is located off Dexter to minimize visual impact.

A-1, A-2
Building mass responds to steep slope of site by burying parking and mechanical spaces into slope and allowing residential units maximum exposure to eastern light and air.

2 Landscape west façade



West Elevation

ARCHITECTURAL CONTEXT

With a mixture of building types and uses, from residential, industrial, maritime, artisan, bio-tech, social service to much more, the architectural context of the South Lake Union neighborhood is very eclectic. Unlike other Seattle neighborhoods that have a well-defined style and preferred materials palette, the South Lake Union neighborhood supports the variety of building styles and does not have neighborhood-specific supplemental guidance for exterior finish materials.

Materials within the immediate vicinity include, but are not limited to, architectural concrete, fiber-cement siding, metal siding, stucco, and brick. To illustrate the eclectic neighborhood style, the included images represent development within the last twenty years.



1101 Dexter Avenue - Proposed Building - MUP# 3006945



1200 Dexter Avenue



Alterra Condominiums



The Neptune Apartments - Dexter Avenue



The Neptune Apartments - 8th Avenue

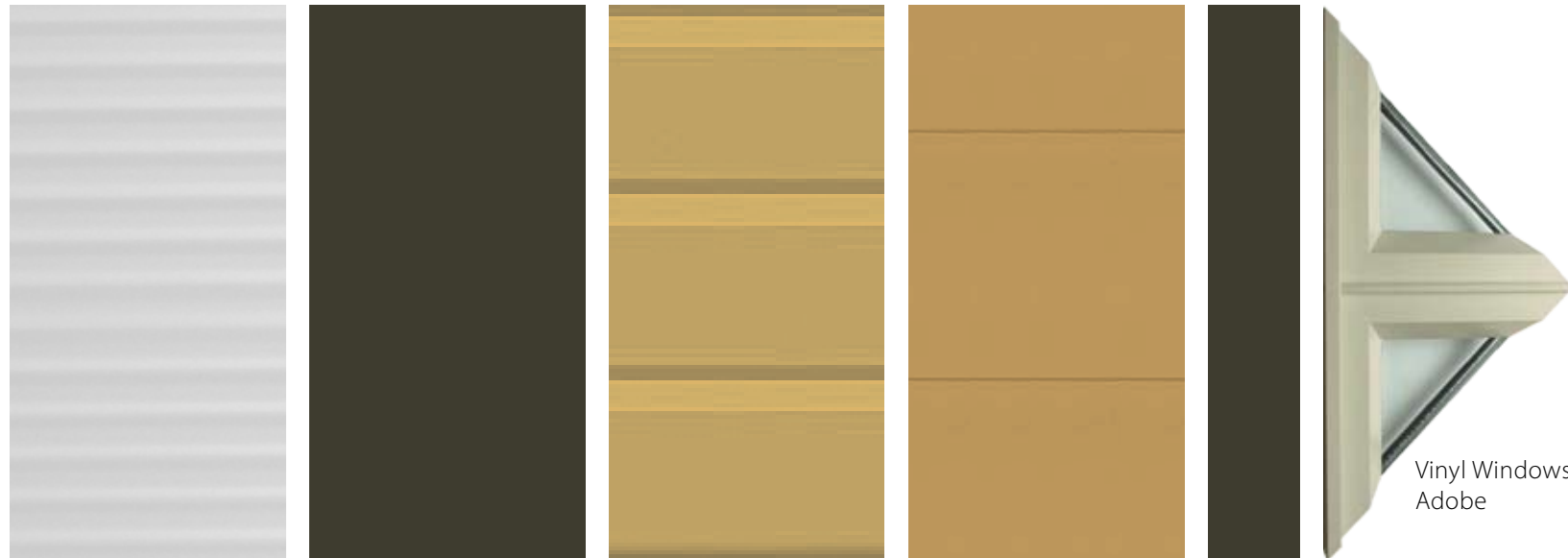


Marriott Hotel



Westlake Village Apartments

MATERIAL CALL-OUT: PALETTE A



Metal 1
AEP Span: Prestige Series
Full 12" Panel - Weave
Cool Metallic Silver

Metal 2
AEP Span: Prestige Series
Full 12" Panel - Flat Pan
Cool Dark Bronze

Cladding 1
Ceraclad - Cast Stripe
Desert Sand

Cladding 2
Ceraclad - Smooth
Desert Sand

Azek Trim
Dark Bronze

Vinyl Windows
Adobe



Architectural Concrete

Storefront 1
Anodized
Aluminum

Storefront
Metal Infill
Panel 1
#9955 Blue

Storefront
Metal Infill
Panel 2
#9921 Charcoal
Gray

Storefront 2 &
Awnings
Dark Bronze



Metal 2
AEP Span: Prestige Series
Full 12" Panel - Flat Pan
Cool Dark Bronze

Cladding 2
Ceraclad - Smooth
Desert Sand

Cladding 1
Ceraclad - Cast Stripe
Desert Sand

Metal 1
AEP Span: Prestige
Series
Full 12" Panel - Weave
Cool Metallic Silver

Metal 1
AEP Span: Prestige
Series
Full 12" Panel - Weave
Cool Metallic Silver

Storefront Infill
Panel 2
#9921 Charcoal
Gray

Vinyl Windows
Adobe

Metal 2
Dark Bronze

Storefront 1
Anodized Aluminum

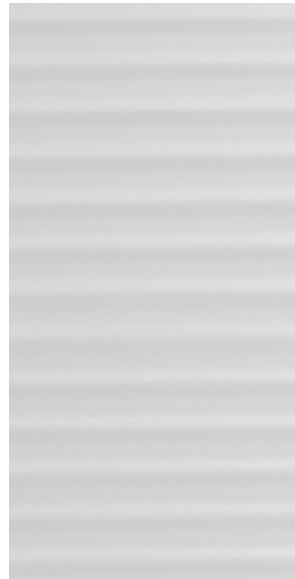
Architectural Concrete

Awnings
Dark Bronze

Storefront 2
Dark Bronze

Storefront Infill
Panel 1
#9955 Blue

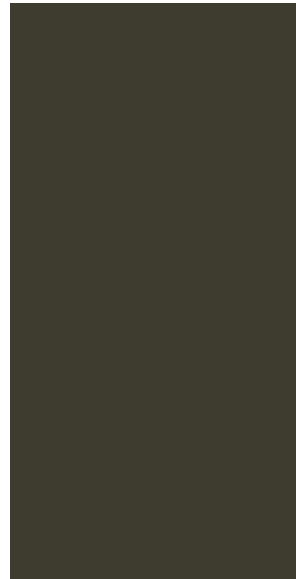
MATERIAL CALL-OUT: PALETTE B



Metal 1
AEP Span: Prestige Series
Full 12" Panel - Weave
Cool Metallic Silver



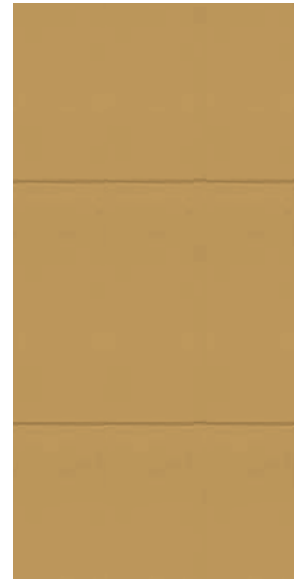
Metal 2
AEP Span: Prestige Series
Full 12" Panel - Weave
Cool Metallic Champagne



Metal 3
AEP Span: Prestige Series
Full 12" Panel - Flat Pan
Cool Dark Bronze



Cladding 1
Ceraclad - Cast Stripe
Desert Sand



Cladding 2
Ceraclad - Smooth
Desert Sand



Azek Trim
Dark Bronze



Vinyl Windows
Adobe



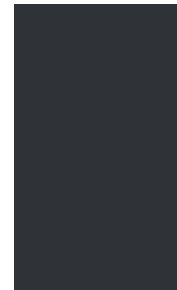
Architectural Concrete



Storefront 1
Anodized
Aluminum



Storefront
Metal Infill
Panel 1
#9955 Blue



Storefront
Metal Infill
Panel 2
#9921 Charcoal
Gray



Storefront 2 &
Awnings
Dark Bronze



Metal 1
AEP Span: Prestige
Series
Full 12" Panel - Weave
Cool Metallic
Champagne

Cladding 2
Ceraclad - Smooth
Desert Sand

Cladding 1
Ceraclad - Cast Stripe
Desert Sand

Metal 2
AEP Span: Prestige Series
Full 12" Panel - Flat Pan
Cool Dark Bronze

Metal 1
AEP Span: Prestige
Series
Full 12" Panel - Weave
Cool Metallic Silver

Storefront Infill
Panel 2
#9921 Charcoal
Gray

Vinyl Windows
Adobe

Metal 2
Dark Bronze

Storefront 1
Anodized Aluminum

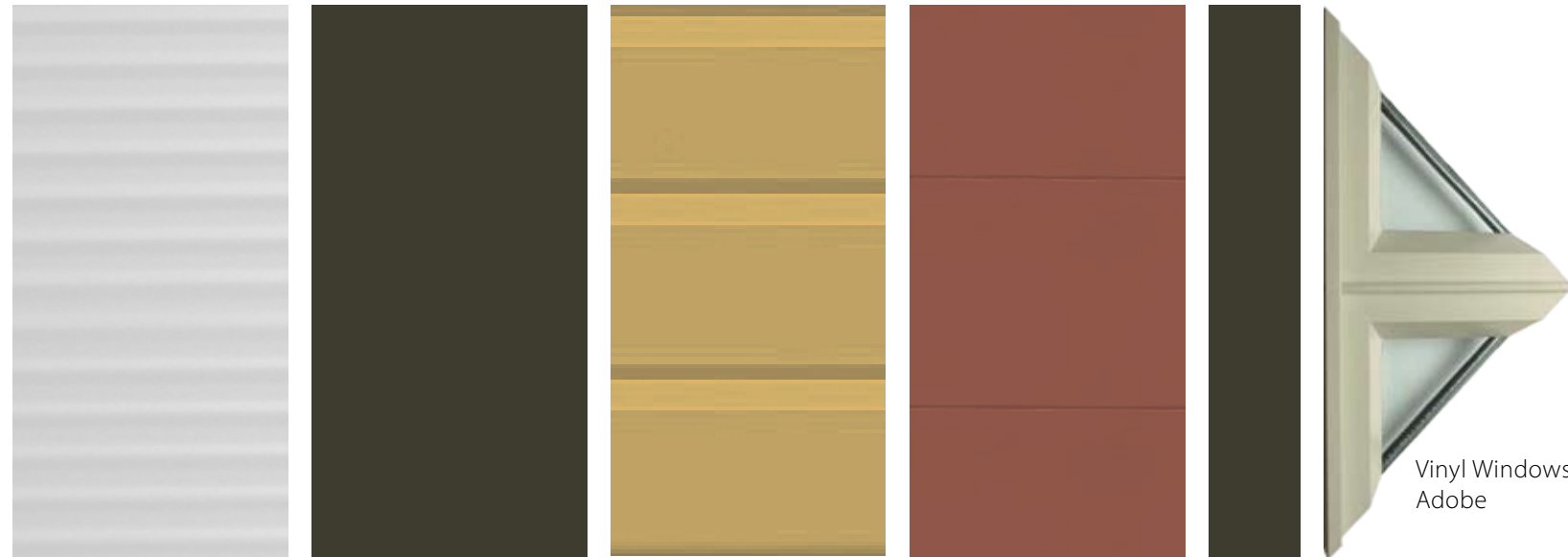
Architectural Concrete

Awnings
Dark Bronze

Storefront 2
Dark Bronze

Storefront Infill
Panel 1
#9955 Blue

MATERIAL CALL-OUT: PALETTE C



Metal 1
AEP Span: Prestige Series
Full 12" Panel - Weave
Cool Metallic Silver

Metal 2
AEP Span: Prestige Series
Full 12" Panel - Flat Pan
Cool Dark Bronze

Cladding 1
Ceraclad - Cast Stripe
Desert Sand

Cladding 2
Ceraclad - Smooth
Saffron

Azek Trim
Dark Bronze

Vinyl Windows
Adobe



Architectural Concrete

Storefront 1
Anodized
Aluminum

Storefront
Metal Infill
Panel 1
#9955 Blue

Storefront
Metal Infill
Panel 2
#9921 Charcoal
Gray

Storefront 2 &
Awnings
Dark Bronze



Metal 2
AEP Span: Prestige Series
Full 12" Panel - Flat Pan
Cool Dark Bronze

Cladding 2
Ceraclad - Smooth
Saffron

Cladding 1
Ceraclad - Cast Stripe
Desert Sand

Metal 1
AEP Span: Prestige
Series
Full 12" Panel - Weave
Cool Metallic Silver

Metal 1
AEP Span: Prestige
Series
Full 12" Panel - Weave
Cool Metallic Silver

Storefront Infill
Panel 2
#9921 Charcoal
Gray

Vinyl Windows
Adobe

Metal 2
Dark Bronze

Storefront 1
Anodized Aluminum

Architectural Concrete

Awnings
Dark Bronze

Storefront 2
Dark Bronze

Storefront Infill
Panel 1
#9955 Blue

CONCEPTUAL SIGNAGE PLAN



Address numbers



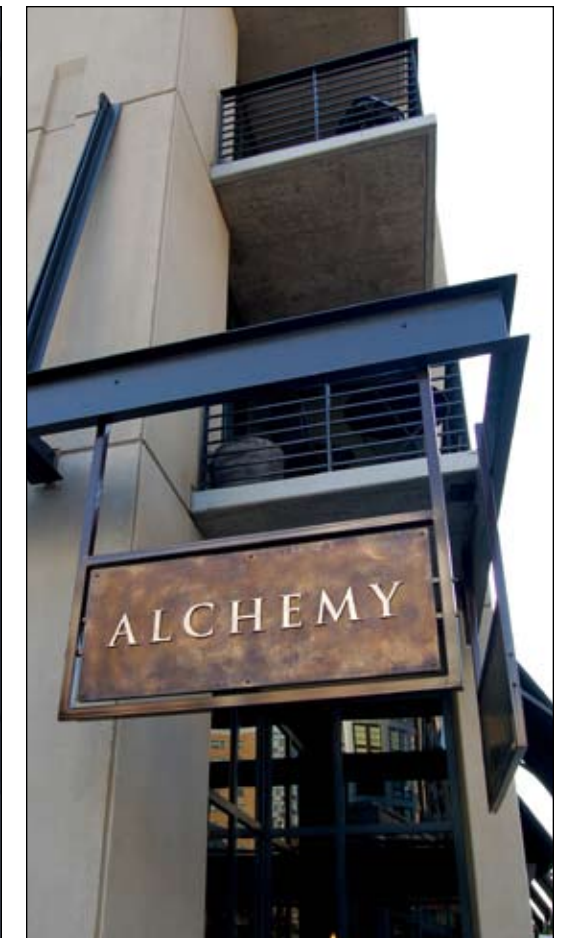
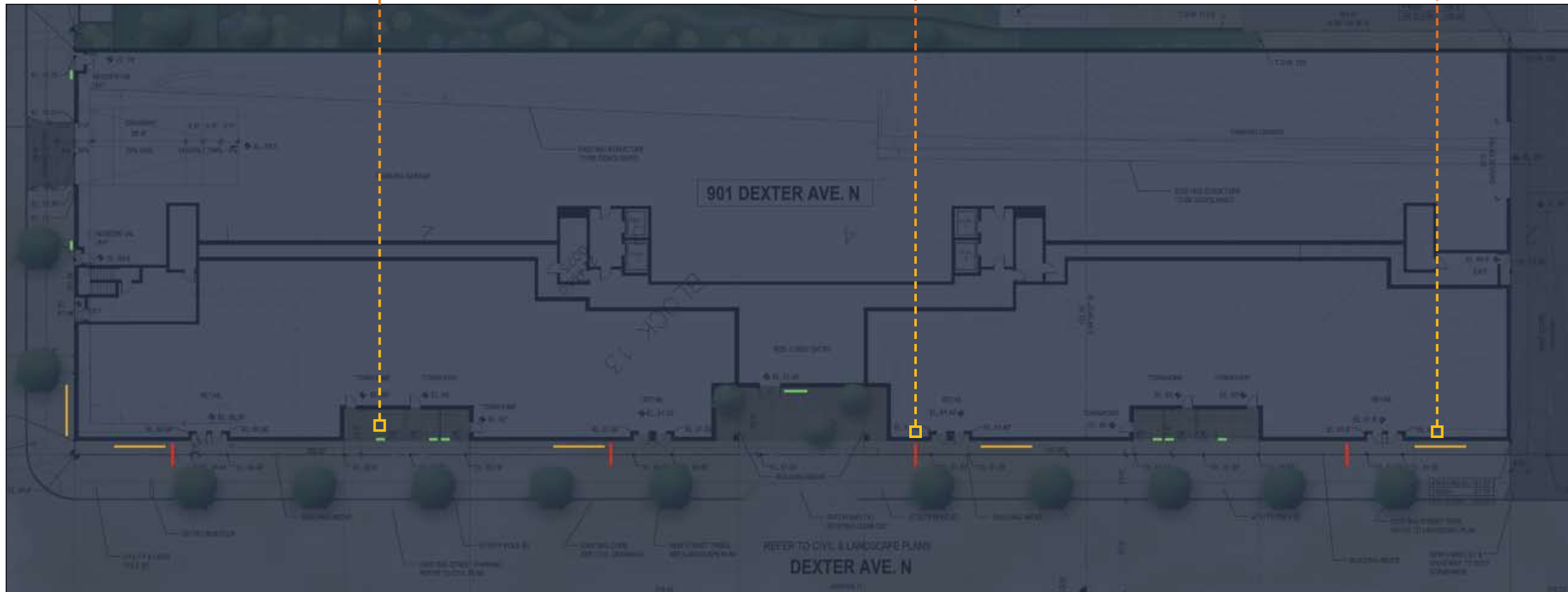
Retail Signage: Type 1



Retail Signage: Type 2



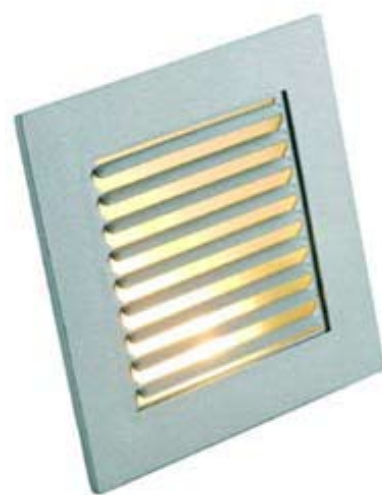
SIGNAGE PLAN



LIGHTING PLAN



Recessed Wall Light
at Parapet wall on roof decks



Wall Sconce Typical
at Street level



Recessed Down Light
at Residential patios

NIGHT RENDERING





Achillea Feuerland



Achillea Millefolium



Anaphalis Triplinervis



Asarum Caudatum



Blechnum Spicant



Cornus 'Midwinter Fire'



Cornus Sericea



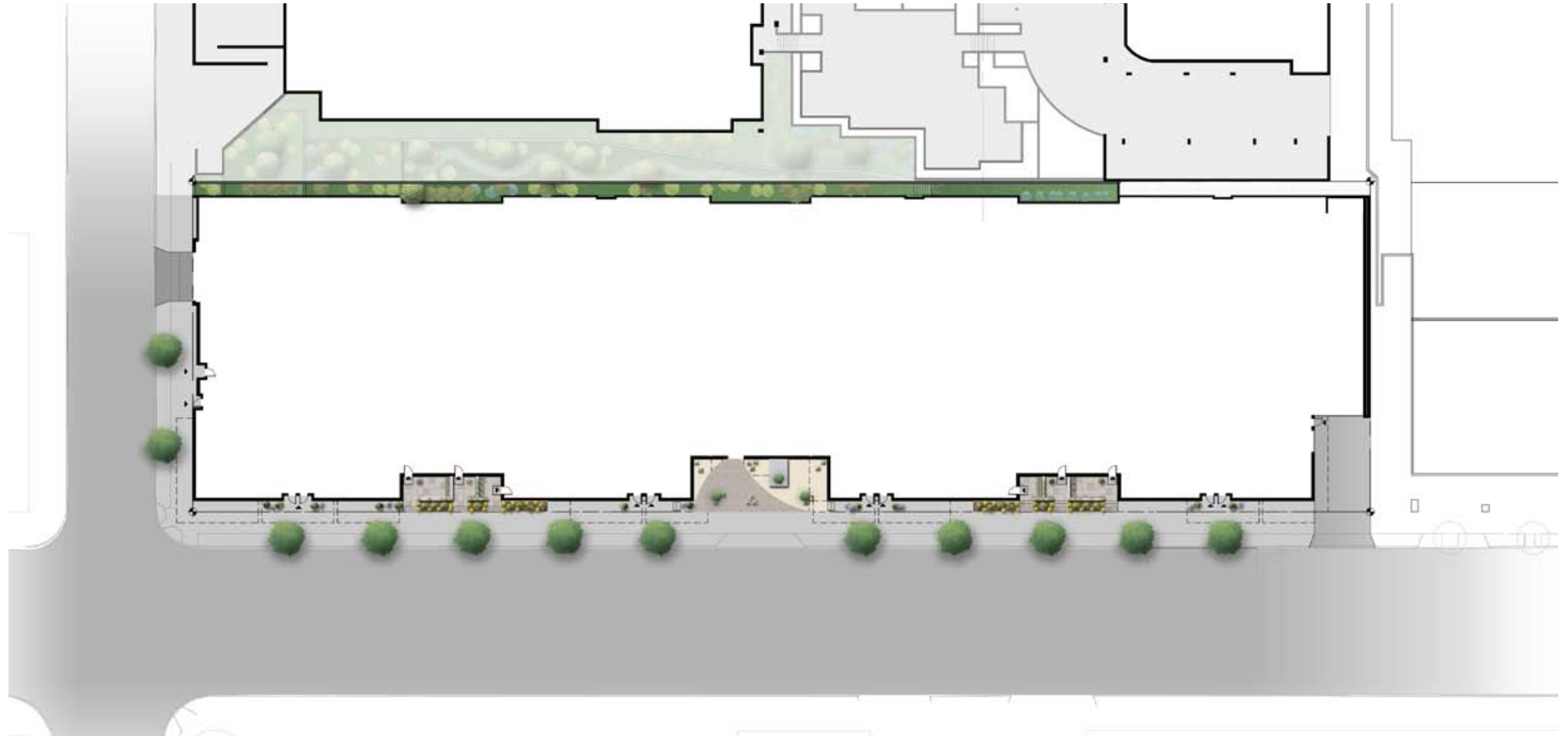
Dicentra Formosa



Festuca Idahoensis



Festuca Mairei



Ground Level Landscaping



Acer Circinatum



Acer Palmatum



Amelanchier Alnifolia



Pinus Contorta



Pyrus Calleryana

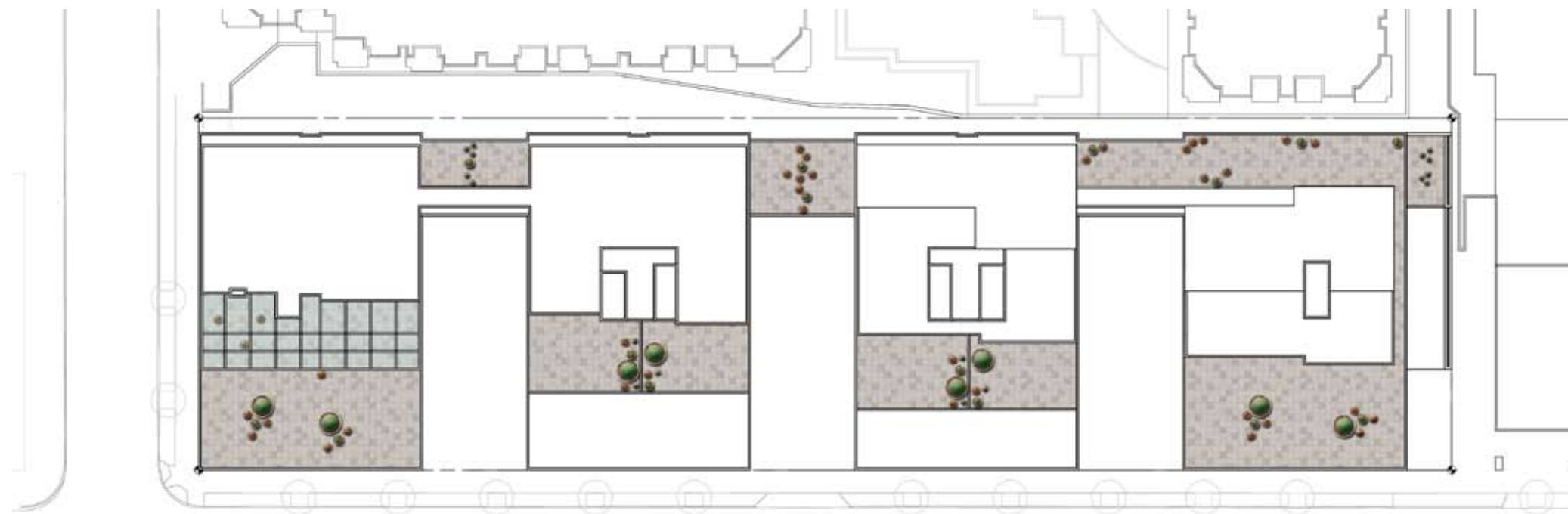


Thuja Plicata Hogan

LANDSCAPE



Lower Roof decks



Upper Roof decks



Gaultheria Shallon



Hebe Pinguifolia



Lavendula Angustifolia



Mahonia Aquifolium



Oemleria Cerasiflora



Oxalis Oregana



Philadelphus Lewisii



Phormium Tenax



Phyllostachys 'Alata'



Polystichum Munitum



Ribes Sanguineum



Rudbeckia Fuldgida



Sarcococca Hookeriana



Sedum Telephium



Selseria Caerulea



Skimmia Japonica



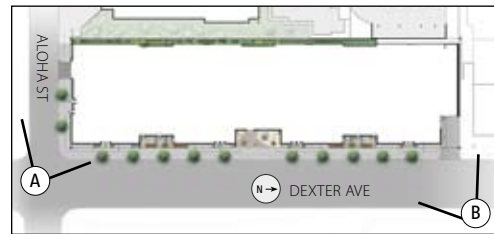
Symphoricarpos Albus



Vaccinium Ovatum



A- View up Aloha St

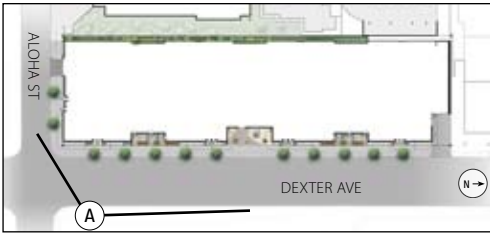


B- Bird's eye view from northeast along Dexter Ave

RENDERINGS



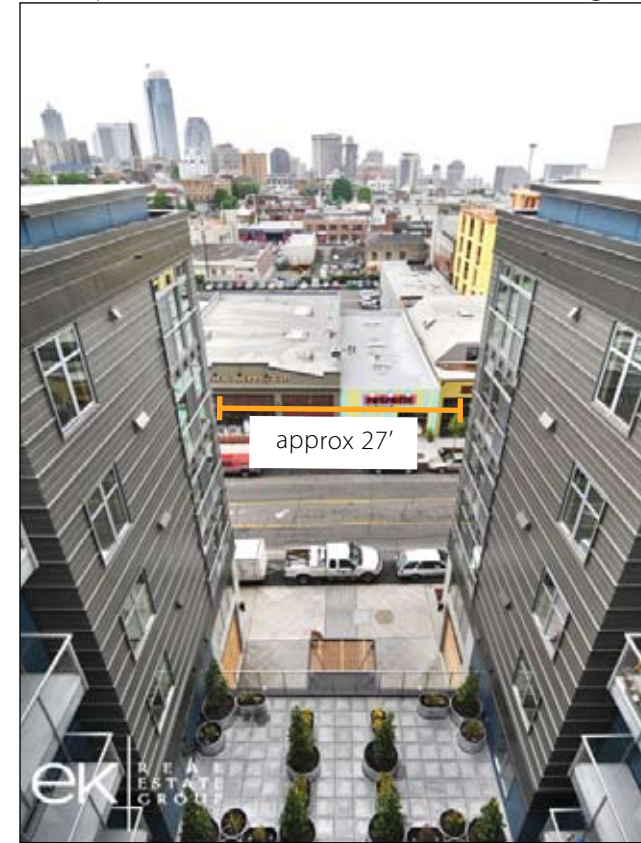
A- View from east along Dexter Ave



Courtyard at middle of proposed building



Comparable courts in width and height



Trace Lofts, Seattle



Westlake Village, Seattle

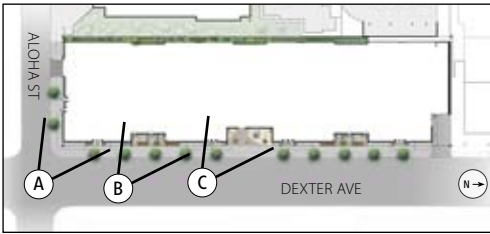
STREETSCAPE RENDERINGS



A- Retail entry at Dexter Ave



B- Townhome entries at Dexter Ave





C- Residential lobby entry at Dexter Ave

ROOFTOP ELEMENT: SOLARIUM



A - View from roof deck to solarium



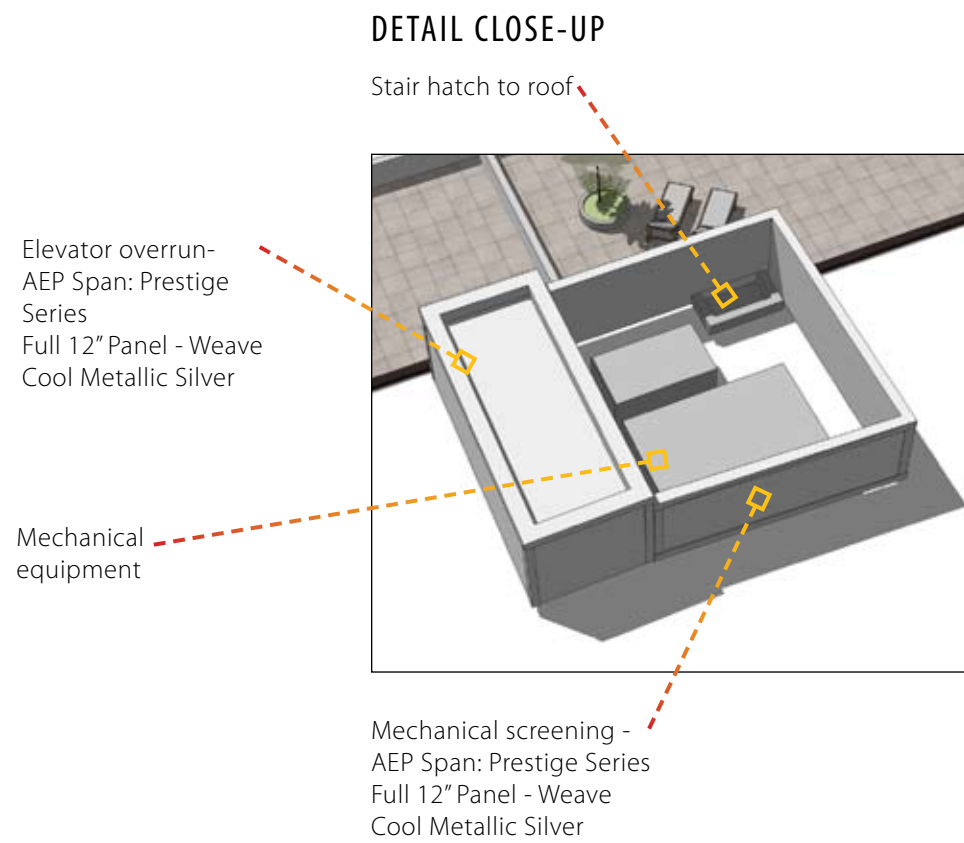
B - Looking North

ROOFTOP ELEMENT: MECHANICAL SCREENING



5 Special consideration for mech. equipment screening

Examples of rooftop mechanical screening



Aerial View

SUMMARY OF DEPARTURES: DEPARTURE 1 & 2 EXHIBITS

DEPARTURE 1: PARKING SPACE STANDARDS

SMC 23.54.030.D2.A2

The minimum width of driveways for two (2) way traffic shall be twenty-two (22) feet and the maximum width shall be twenty-five (25) feet.

REQUEST:

1. At Aloha Street, the driveway for two-way traffic has a width of 18'-0".
2. At Dexter Street, the driveway for two-way traffic has a width of 20'-6".

JUSTIFICATION:

1. The Aloha Street grade drops steeply across the width of the drive thus making a 22' wide driveway and curb cut unfeasible. The narrower driveway creates a more pedestrian friendly R.O.W. and is only serving 75 cars.
2. The project site is 431' along Dexter Avenue and the applicant has made an effort to provide only 1 curb cut off Dexter to minimize disruptions to bike and pedestrian traffic because Dexter Avenue is a class 2 pedestrian street.



Diagram 1: Parking Space Standards Departure 1 & 2 at Aloha St driveway access

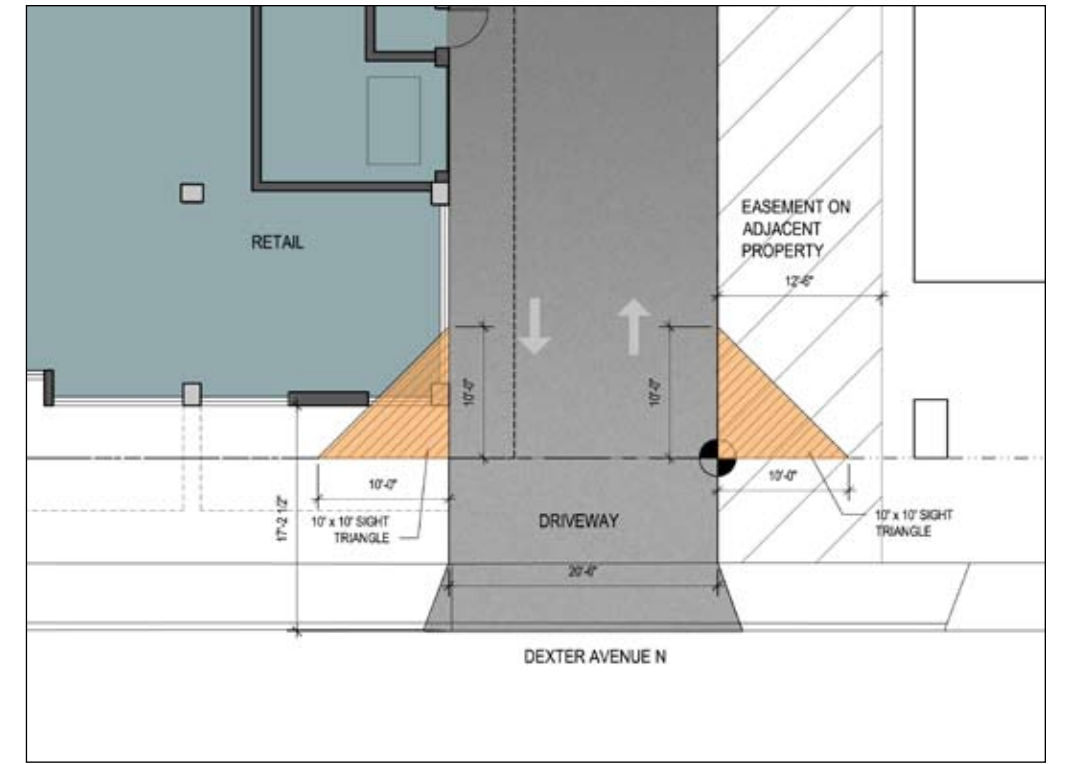


Diagram 2: Parking Space Standards Departure 1 & 2 at Dexter Ave N driveway access

DEPARTURE 2: PARKING SPACE STANDARDS

SMC 23.54.030.G1

For two way driveways or easements less than twenty-two feet wide, a sight triangle on both sides of the driveway used as an exit shall be provided, and shall be kept clear of any obstruction for a distance of ten feet from the intersection of the driveway or easement with a driveway, easement, sidewalk or curb intersection if there is no sidewalk.

REQUEST:

The use of traffic safety mirrors to mitigate the absence of the sight triangle at the Aloha Street driveway and at the Dexter Street driveway.

JUSTIFICATION:

1. Full compliance with the sight triangle requirements at the Aloha Street driveway would compromise the feasibility of 2 residences at the Aloha Street level and 2 residences above. These residences do more to enliven the facade than a larger entrance to the garage. These spaces might otherwise be filled with storage or mechanical uses.
2. The site triangle to the south on Dexter Avenue N would compromise the structure of the project: Currently there is a structural column. The sidewalk on Dexter Avenue is 17' wide, providing a high level of safety.

DEPARTURE 3: GENERAL FAÇADE REQUIREMENT

SMC 23.48.014.B2

On class 2 pedestrian streets, all façades shall have a minimum façade height of 25'.

REQUEST:

The applicant is requesting a minimum façade height of 15'-3" along Dexter Avenue N.

JUSTIFICATION:

1. The applicant has extended the sidewalk and R.O.W. landscaping by setting the retail areas back from the sidewalk by 4'-0". This provides additional space for pedestrian use, breaks up what might otherwise be a 65'-high façade, and responds to the EDG board direction to reduce height, bulk & scale. The total rentable area given over to this setback is 1,159 GSF.
2. The façade height is greater at the lobby and ground-level residences but still less than 25' in order to enhance light, air, & views from the level 2 courtyards to the street.



Close up detail of lobby entry



Lobby entry perspective view



Dexter Ave. Street Level Façade

Façade Height
20' 9"

Façade Height
15' 3"

Façade Height
20' 6"

Façade Height
14' 11"

Façade Height
20' 2"

Façade Height
14' 8"

Façade Height
14' 5"