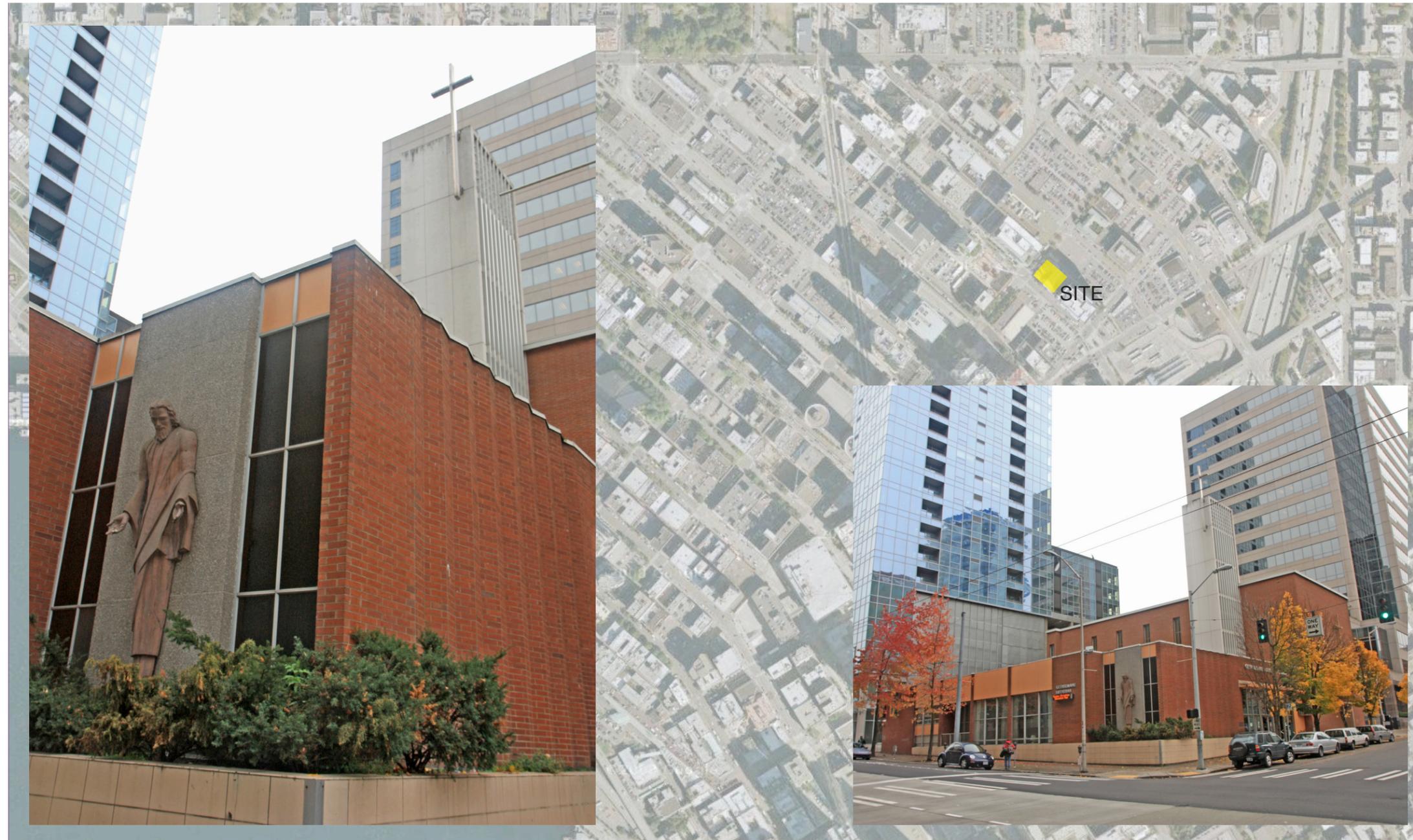


GETHSEMANE

REDEVELOPMENT PROJECT



GROW! Gethsemane Redevelopment, Outreach and Workforce Housing

A project to reinvigorate a downtown community of believers; create fifty units of affordable workforce housing; and improve a homeless shelter.

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- 29: Land Use Information

EARLY DESIGN GUIDANCE

DECEMBER 15, 2009

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SMR
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OSKA ARCHITECTS
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Bob Jakubik, AIA
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901 Stewart Street
Seattle, WA 98101-1477

Project Number:
3009393

**Attachment A
City of Seattle
Application for Early Design Guidance**

PART I: CONTACT INFORMATION

1. Property Address: 901 Stewart Street

2. Project number: 3009393

3. Additional related project number(s): 6187514

4. Owner: Gethsemane Lutheran Church

5. Contact Person* Name: Kimberly McKittrick, AIA
SMR Architects
911 Western Ave, Suite 200
(206) 623-1104
kmckittrick@smrarchitects.com

6. Applicant's Name: Kimberly McKittrick, AIA
Relationship to Project: Architect

7. Design Professional's Name: Kimberly McKittrick, AIA
SMR Architects
911 Western Avenue, Suite 200
(206) 623-1104
kmckittrick@smrarchitects.com

8. Applicant's Signature: _____
Date: 18 November 2009

PART II: SITE AND DEVELOPMENT INFORMATION

1. Please describe the existing site, including location, existing uses and/or structures, topographical or other physical features, etc.

Located at the corner of 9th Avenue and Stewart Street, the site has been home to Gethsemane Lutheran Church since 1885. As the street grid is not aligned cardinally in this area, the site's street exposures are to the northwest and southwest, as well as an alley exposure to the northeast. The site slopes gently downhill from east to west, with the corner of 9th and Stewart at the lowest point.

The existing church complex comprises the nave and associated spaces for the congregation, church offices, an overnight shelter for the homeless and related social services. The current nave, built on the southern portion of the site in 1954, will remain, and may be renovated in this project. On the northern portion of the site is a wing added in 1960, comprising church office and social spaces, which will be demolished. Both phases of the church buildings are in a period modern architectural style, and are clad in brick.

2. Please indicate the site's zoning and any other overlay designations, including applicable Neighborhood-Specific Guidelines.

The site is zoned Downtown Mixed Commercial, and is in the Downtown Fire District (DMC 340/290-400-DF). The site is in the Denny Triangle Urban Center Village overlay.

3. Please describe neighboring development and uses, including adjacent zoning, physical features, existing architectural and siting patterns, views, community landmarks, etc.

The surrounding area is zoned for high-rise development. The immediate neighborhood is characterized by a significant amount of recent large-scale redevelopment, typically office or mixed-use residential, that has replaced surface parking lots and very small-scale development. Except for the Greyhound station to the west, all structures adjacent to the site have been constructed in the past 20 years, and are much taller than the church structure. A 50-story hotel is proposed for the Greyhound site, but its development schedule is unknown. The retail core and Westlake Center are just a few blocks to the southwest, and the Metro transit tunnel station is located to the south. The new Federal Courthouse is located several blocks west along Stewart Street.

Because the area is gently sloping to nearly level, there are no significant views down street corridors, nor are there remarkable physical features. Newer construction tends to cover entire parcels and be built out to the property lines, often taking the form of a several-story plinth and high-rise tower.

The primary community landmark of note is the statue of Jesus on the north wall of the existing church building, overlooking the corner of Stewart and 9th. The statue will be preserved, and placed on the Stewart façade of the new structure not far from its current location, thereby maintaining its familiar, welcoming presence on the street.

4. Please describe the applicant's development objectives, indicating types of desired uses, structure height (approx), number of residential units (approx), amount of commercial square footage (approx), and number of parking stalls (approx). Please also include potential requests for departure from development standards.

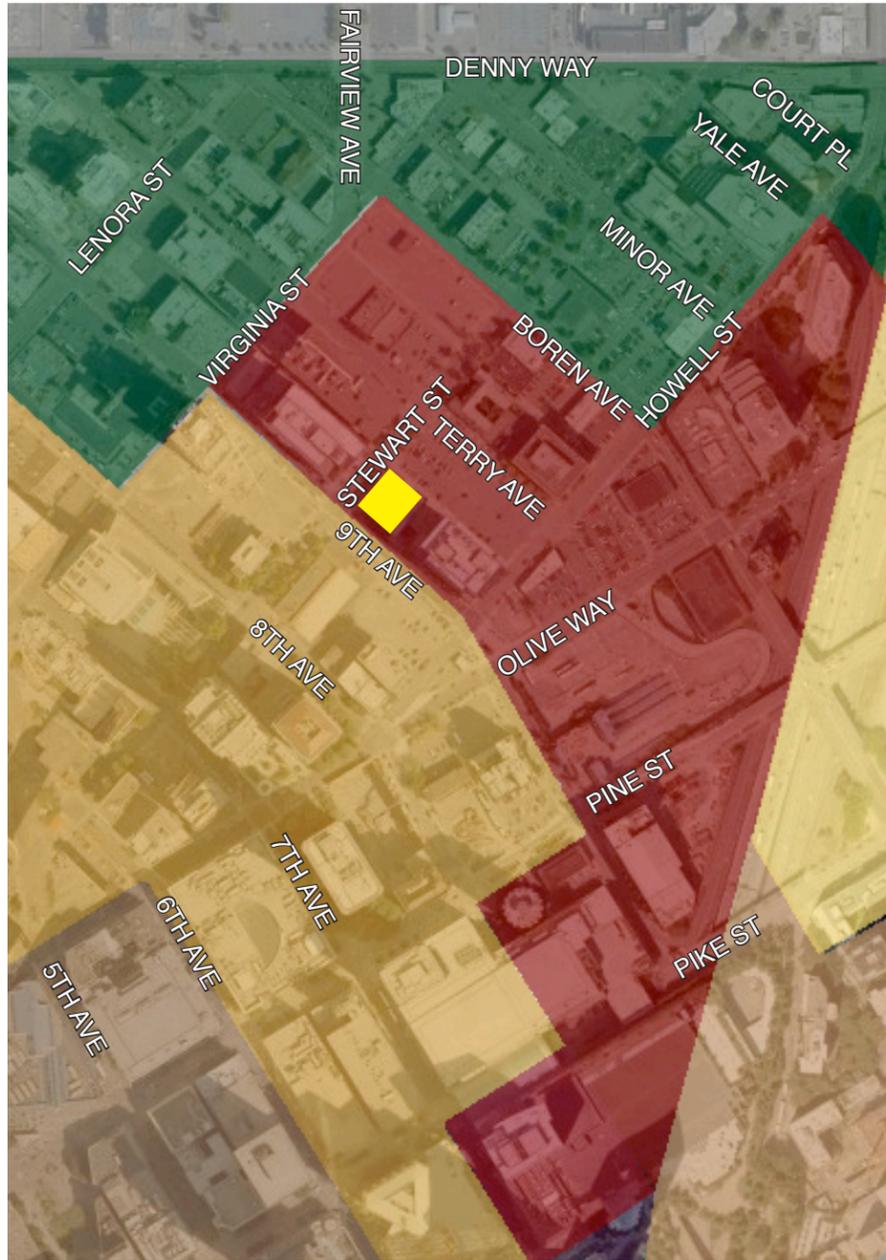
The church's existing nave will remain; the rest of the church complex will be demolished. In its place, a new, 70-foot-tall structure will comprise five floors of affordable workforce housing over two floors of church offices and congregation spaces, and a basement for an overnight shelter. The residential component will consist of 50 studio to two-bedroom units with associated common spaces. The new church facilities will consist of two lower floors totaling approximately 12,000SF, with 6,700SF of basement. The existing nave is approximately 6,900 SF and has a full basement. No parking is required or proposed.

We anticipate requesting departures for: the green street setback on 9th Avenue, continuity of overhead weather protection, and size/configuration of structural building overhangs.

GETHSEMANE

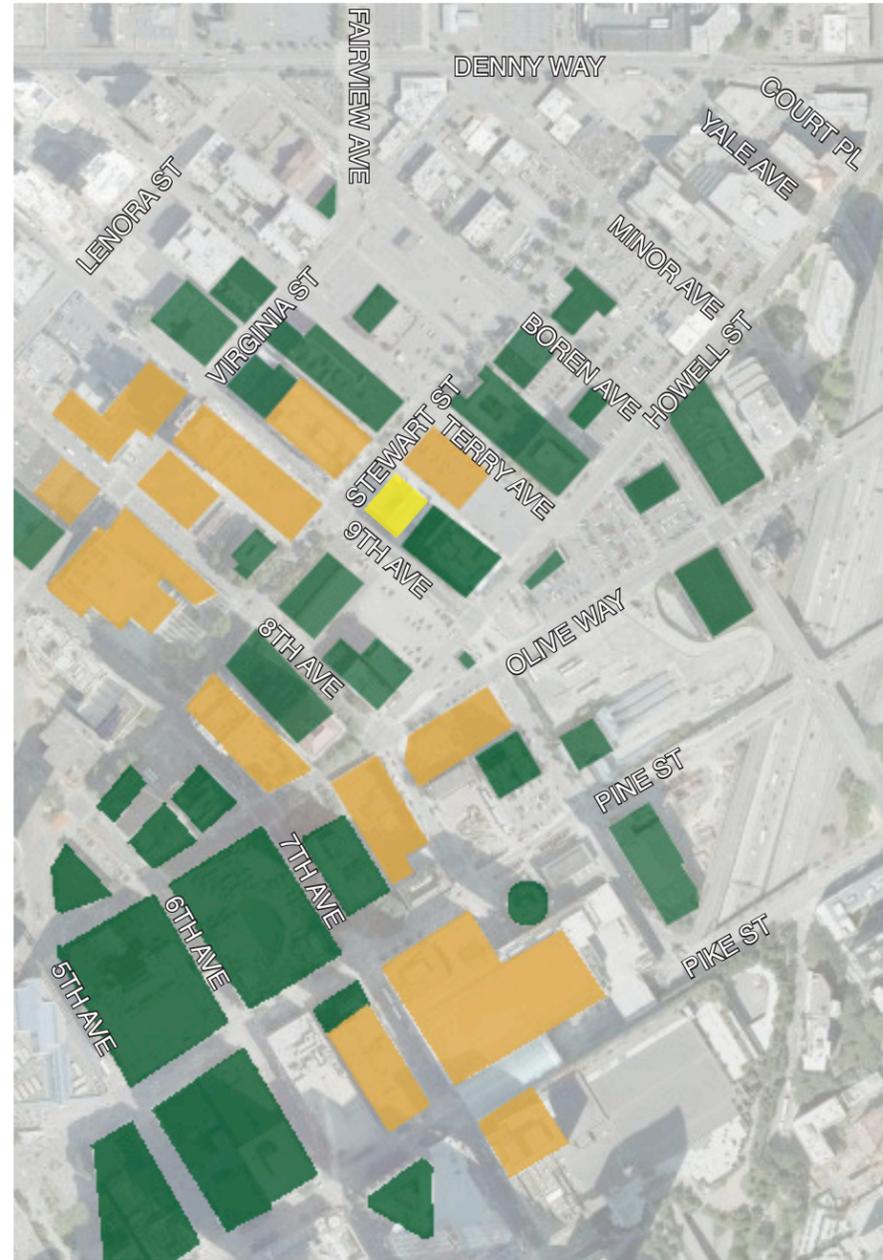
REDEVELOPMENT PROJECT

ZONING MAP



- DMC 340/290-400
- DMC 240/290-400
- DOC2 500/300-500
- NC 3-85
- HR
- DRC 85-150
- SM-125

DEVELOPMENT HISTORY MAP



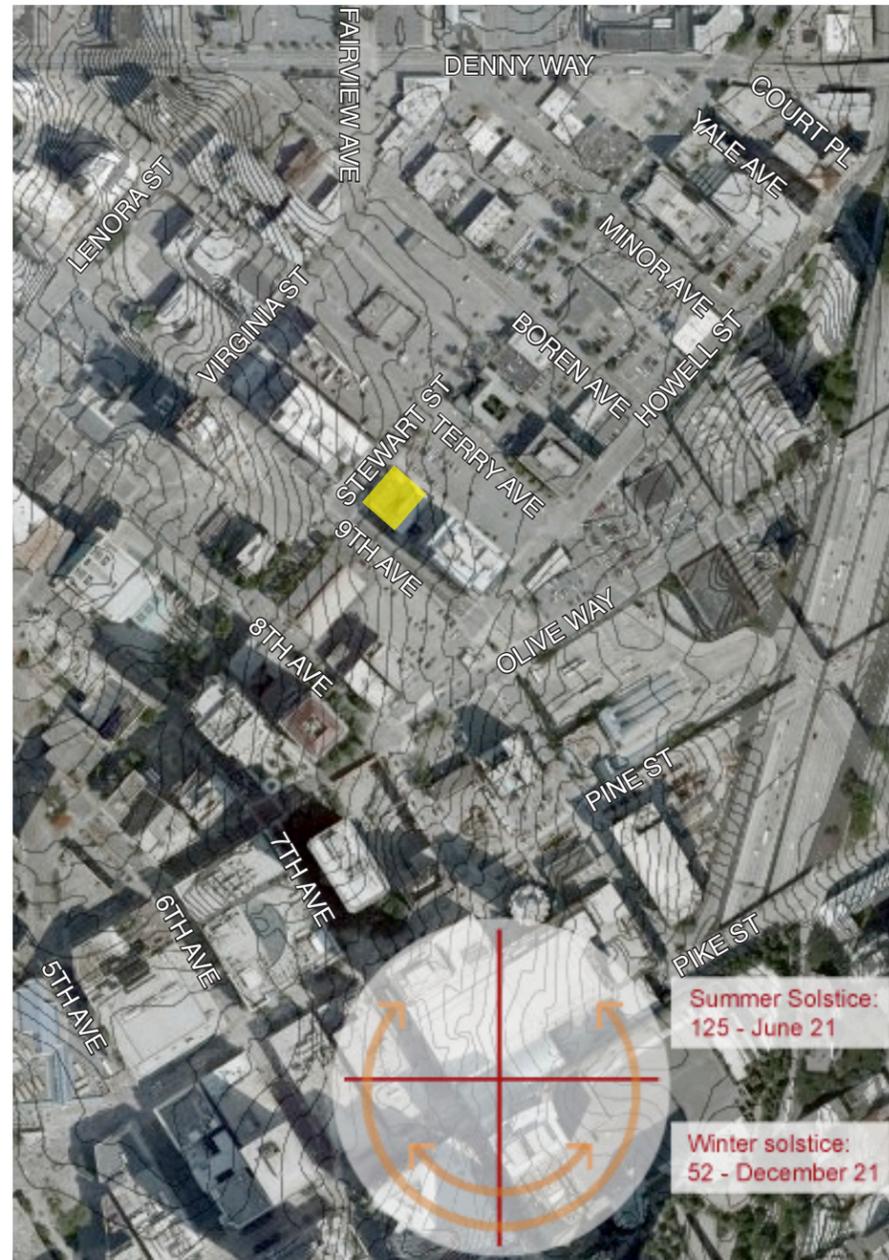
- CONSTRUCTED AFTER 2000
- CONSTRUCTED PRIOR TO 2000

TRANSPORTATION / STREET USE MAP

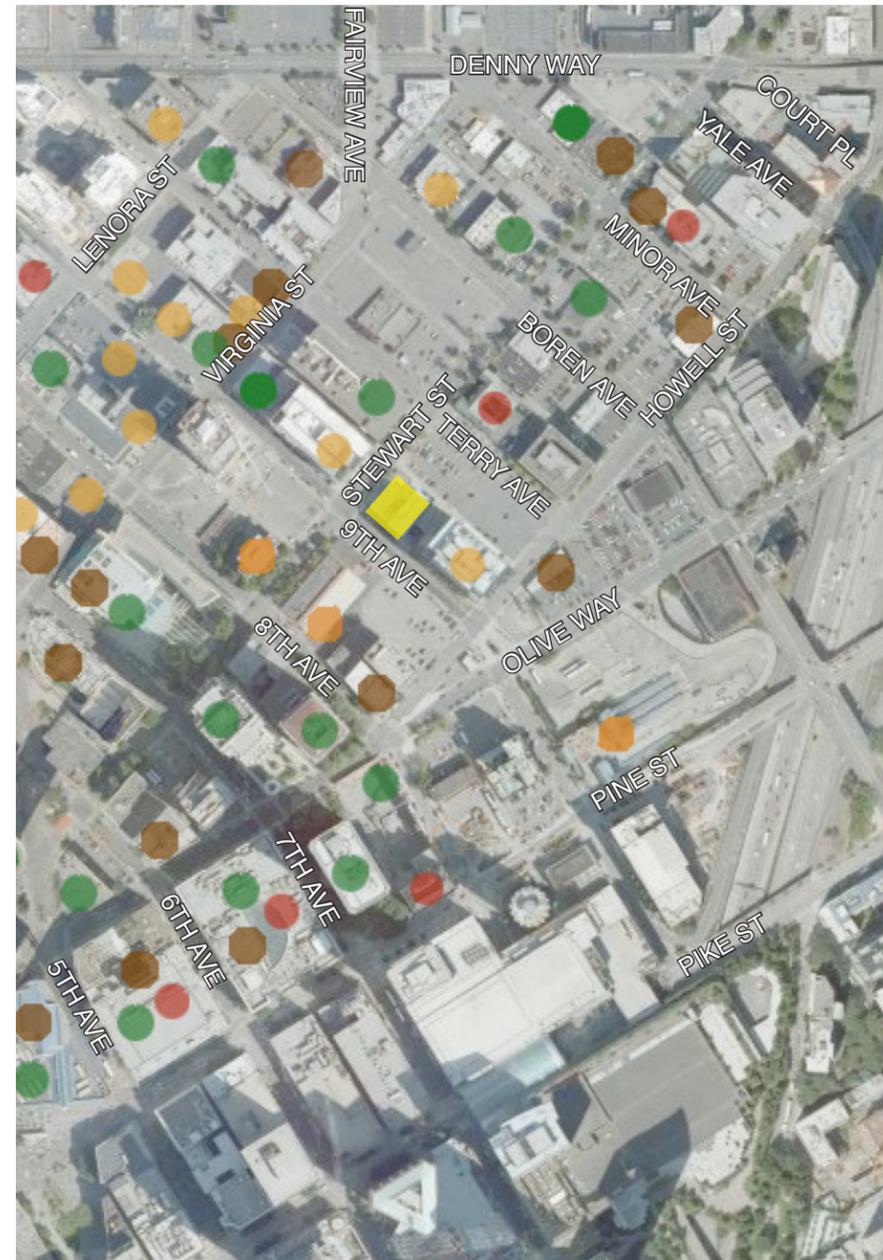


- PEDESTRIAN STREET
- TRANSIT STREET
- GREEN STREET
- TRANSIT STOP LOCATION
- BUS ROUTES

TOPOGRAPHY / SUN ANGLE MAP



AMENITIES / OPPORTUNITY MAP



- ENTERTAINMENT
- BUSINESS / RETAIL
- SOCIAL SERVICE / HEALTHCARE
- TRANSPORTATION
- FOOD / DINING

GETHSEMANE

REDEVELOPMENT PROJECT

AXONOMETRIC MAP OF SURROUNDING NINE-BLOCK AREA

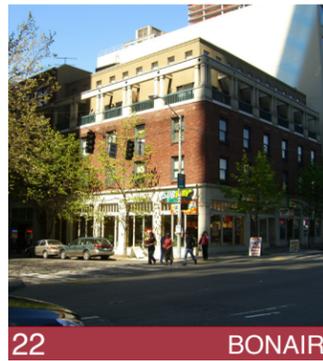


- SITE
- OFFICE
- RETAIL
- RESIDENTIAL
- TRANSPORTATION

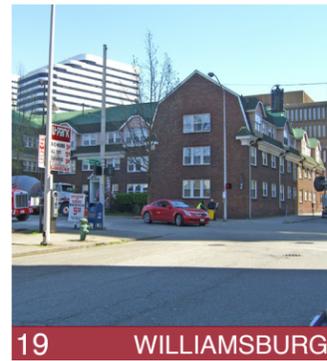
GETHSEMANE

REDEVELOPMENT PROJECT

SURROUNDING
NINE-BLOCK
AREA
BUILDING
IMAGES



22 BONAIR



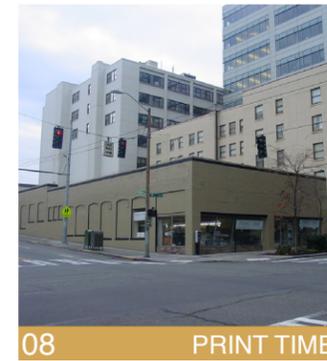
19 WILLIAMSBURG



16 OLIVE 8 SALES OFFICE



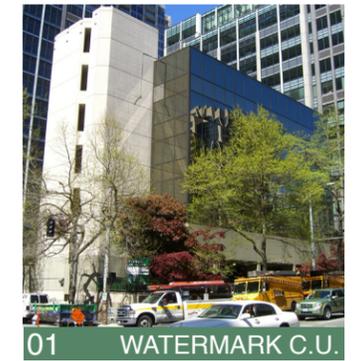
12 PARKING



08 PRINT TIME



04 CHILDRENS HOSP.



01 WATERMARK C.U.



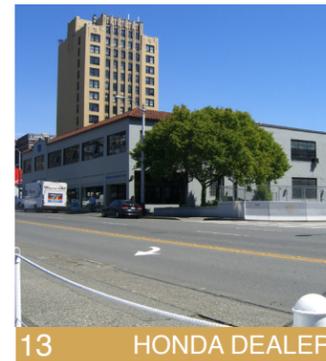
23 ASPIRA



20 COSMOPOLITAN



17 THE JULIE



13 HONDA DEALER



09 RETAIL



05 QWEST



02 818 STEWART



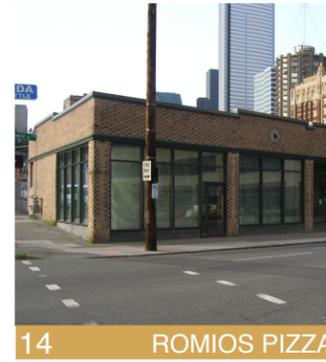
24 GREYHOUND



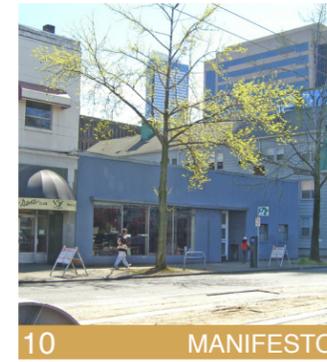
21 OLIVIAN



18 9TH & VIRGINIA



14 ROMIOS PIZZA



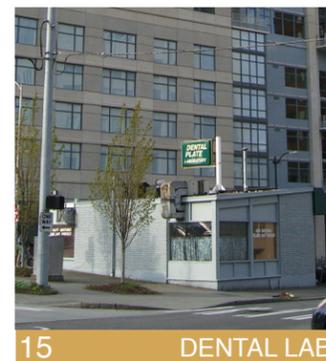
10 MANIFESTO



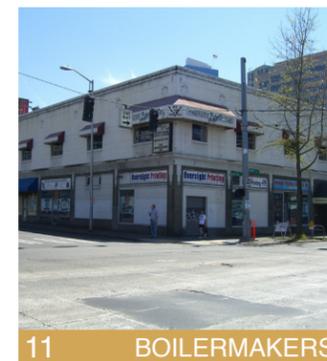
06 INSIGNIA



03 REGENCE



15 DENTAL LAB



11 BOILERMAKERS



07 ROFF BUILDING

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REDEVELOPMENT PROJECT

STREET ELEVATION - 9TH AVENUE LOOKING WEST



HOWELL STREET

50-STORY HOTEL WITH CONFERENCE CENTER PROPOSED

STEWART STREET

OVERHEAD PROTECTION
BREAKS AT BUILDING
ARTICULATION

STREET ELEVATION - 9TH AVENUE LOOKING EAST



VERTICAL FENESTRATION

HORIZONTAL FENESTRATION

OVERHEAD PROTECTION BREAKS AT
BUILDING ARTICULATION

STEWART STREET

BOREN STREET

FLUSH STREET WALL FACADE

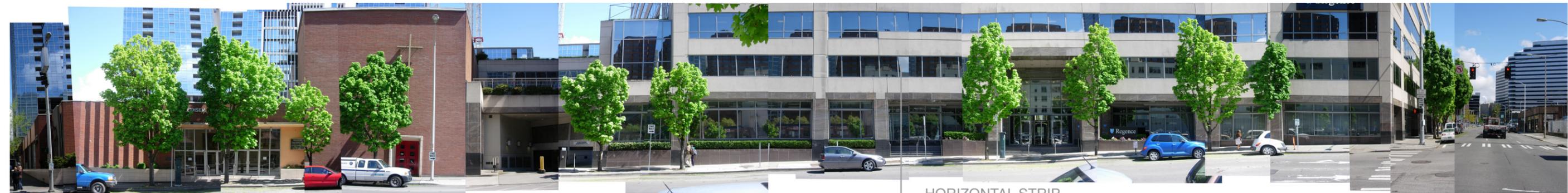
STREET ELEVATIONS



GARAGE

INTERMITTENT OVERHEAD PROTECTION

BOREN STREET



HORIZONTAL STRIP FENESTRATION

NO OVERHEAD PROTECTION

HOWELL STREET

PROJECT SITE

GETHSEMANE

REDEVELOPMENT PROJECT

STREET ELEVATION - STEWART STREET LOOKING SOUTH



SMALL-SCALE AFFORDABLE HOUSING

TERRY AVENUE

OVERHEAD PROTECTION BREAKS AT BUILDING ARTICULATION

ALLEY

STREET ELEVATION - STEWART STREET LOOKING NORTH



9TH AVENUE

ALLEY

OVERHEAD PROTECTION BREAKS AT BUILDING ARTICULATION

9TH AVENUE

OPPOSITE PROJECT SITE

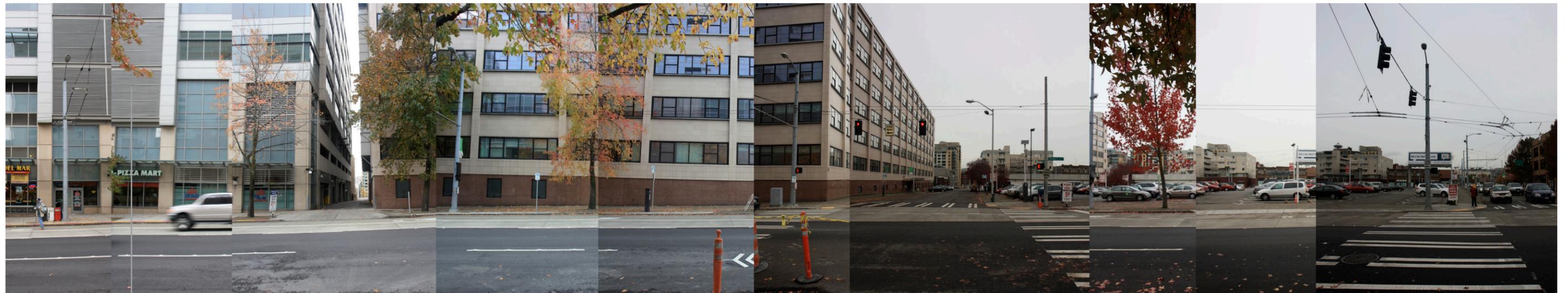


PROJECT SITE

9TH AVENUE

NO OVERHEAD PROTECTION

8TH AVENUE



ALLEY

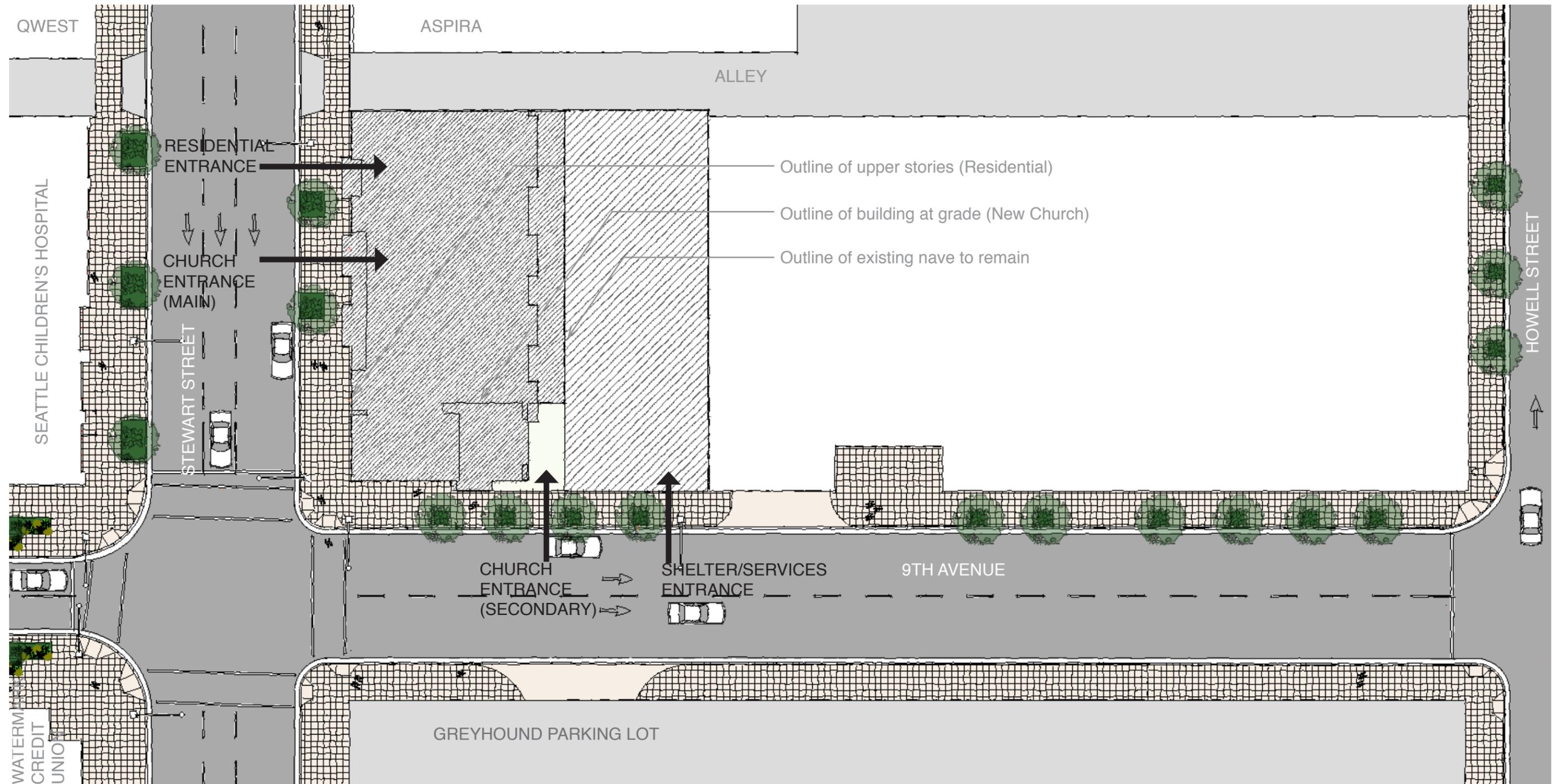
NO OVERHEAD PROTECTION

TERRY AVENUE

HORIZONTAL FENESTRATION

GETHSEMANE

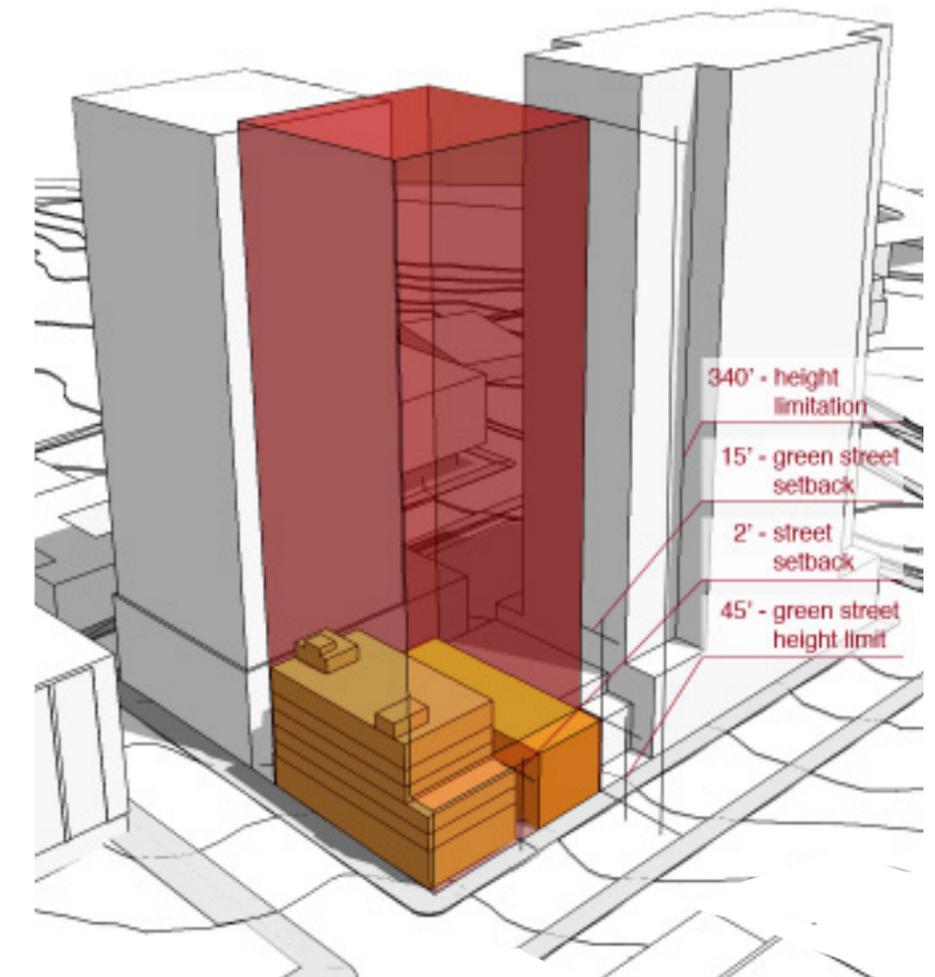
REDEVELOPMENT PROJECT



PROJECT GOALS

- Create a new and attractive presence downtown.
- Create a more open and inviting church building to support church outreach to the community.
- Enhance accessibility in the church to support aging members of the congregation.
- Develop 50 units of affordable workforce housing, which will:
 - support downtown employers by lowering housing and transportation costs for employees;
 - provide additional customers for local businesses;
 - provide 'eyes on the street' for security, and
 - add vitality to the neighborhood.
- Improve facilities for the 30-bed shelter and 'soup and movie' program for the homeless.

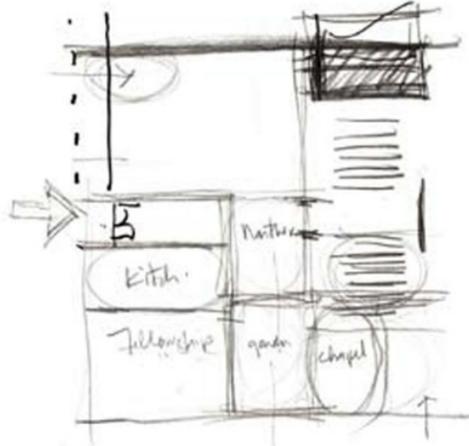
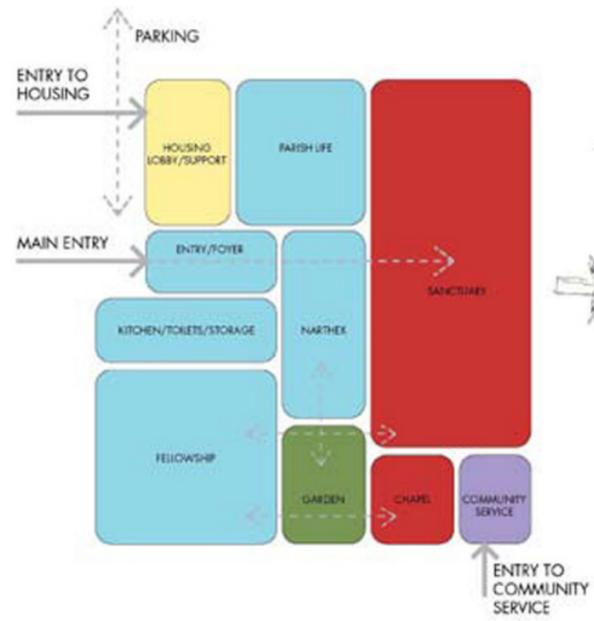
ZONING ENVELOPE



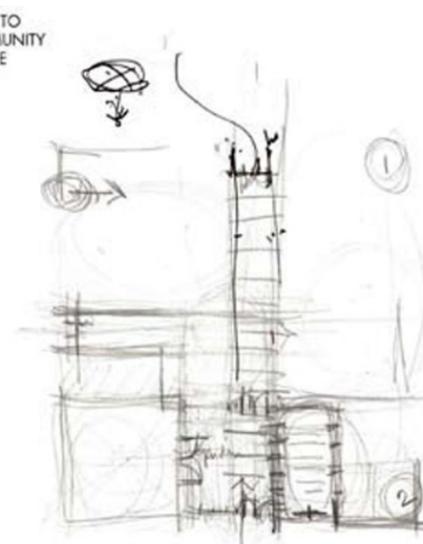
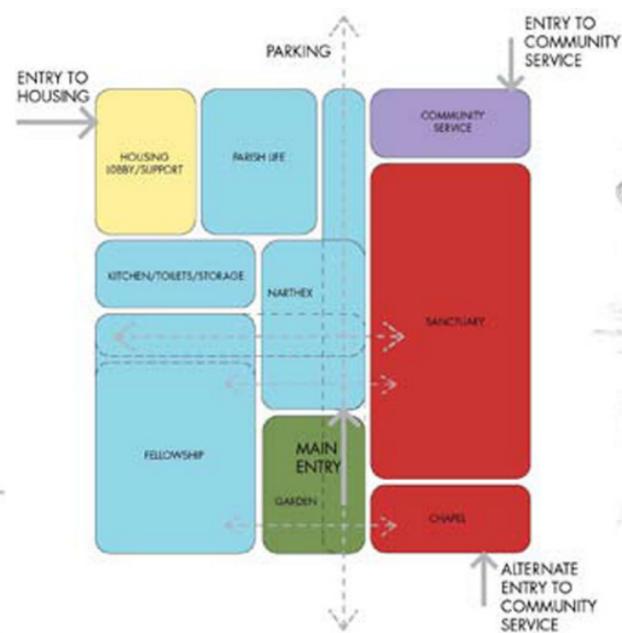
GETHSEMANE

REDEVELOPMENT PROJECT

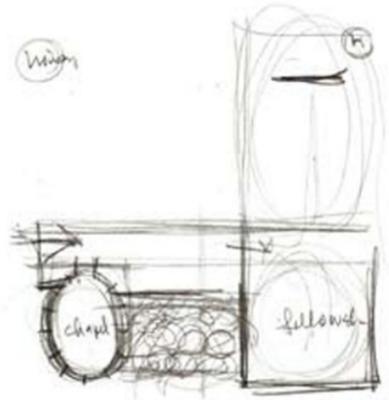
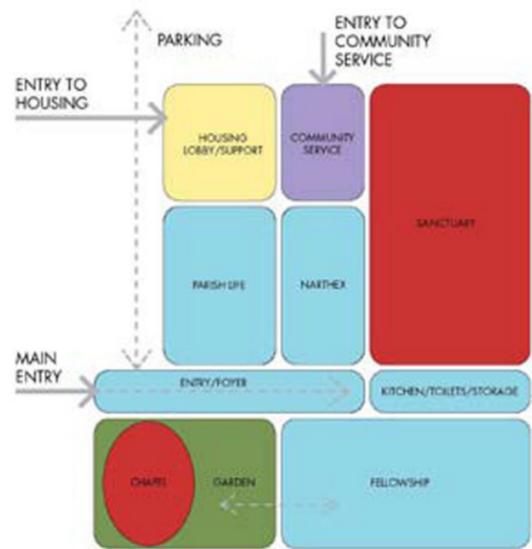
CONCEPTUAL DIAGRAMS OF FIRST FLOOR



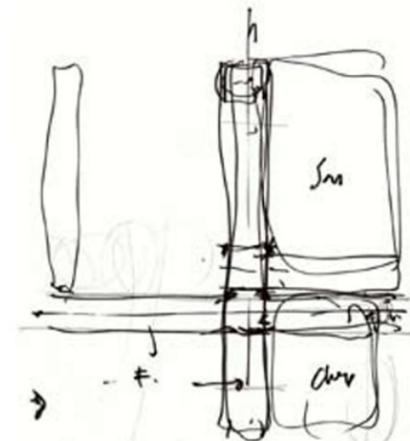
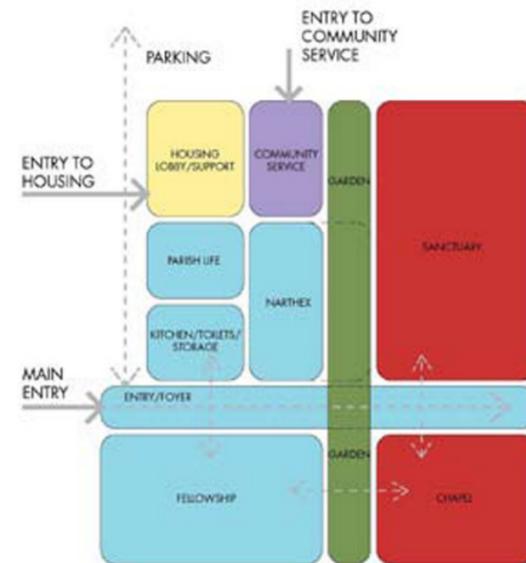
SCHEME A
Gethsemane Lutheran Church



SCHEME B
Gethsemane Lutheran Church

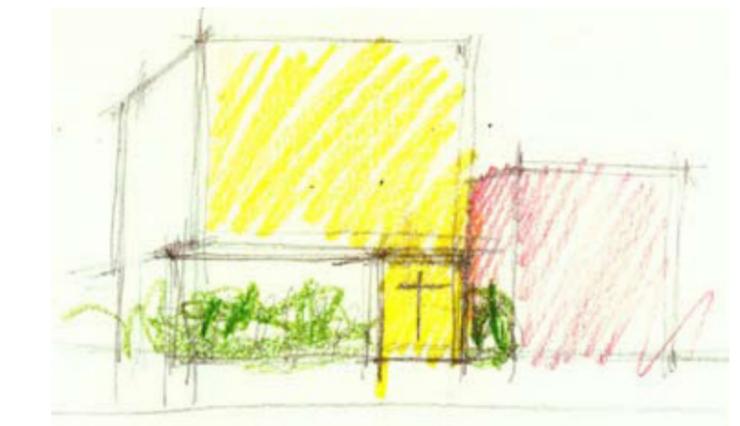
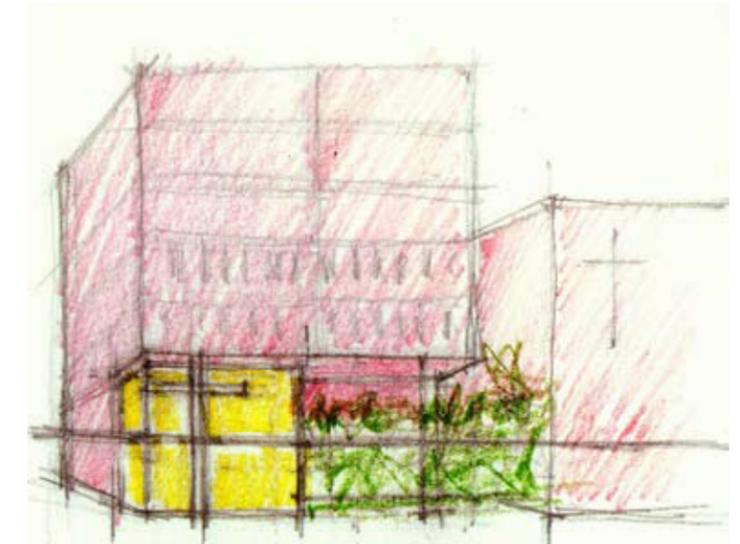


SCHEME C
Gethsemane Lutheran Church



SCHEME D
Gethsemane Lutheran Church

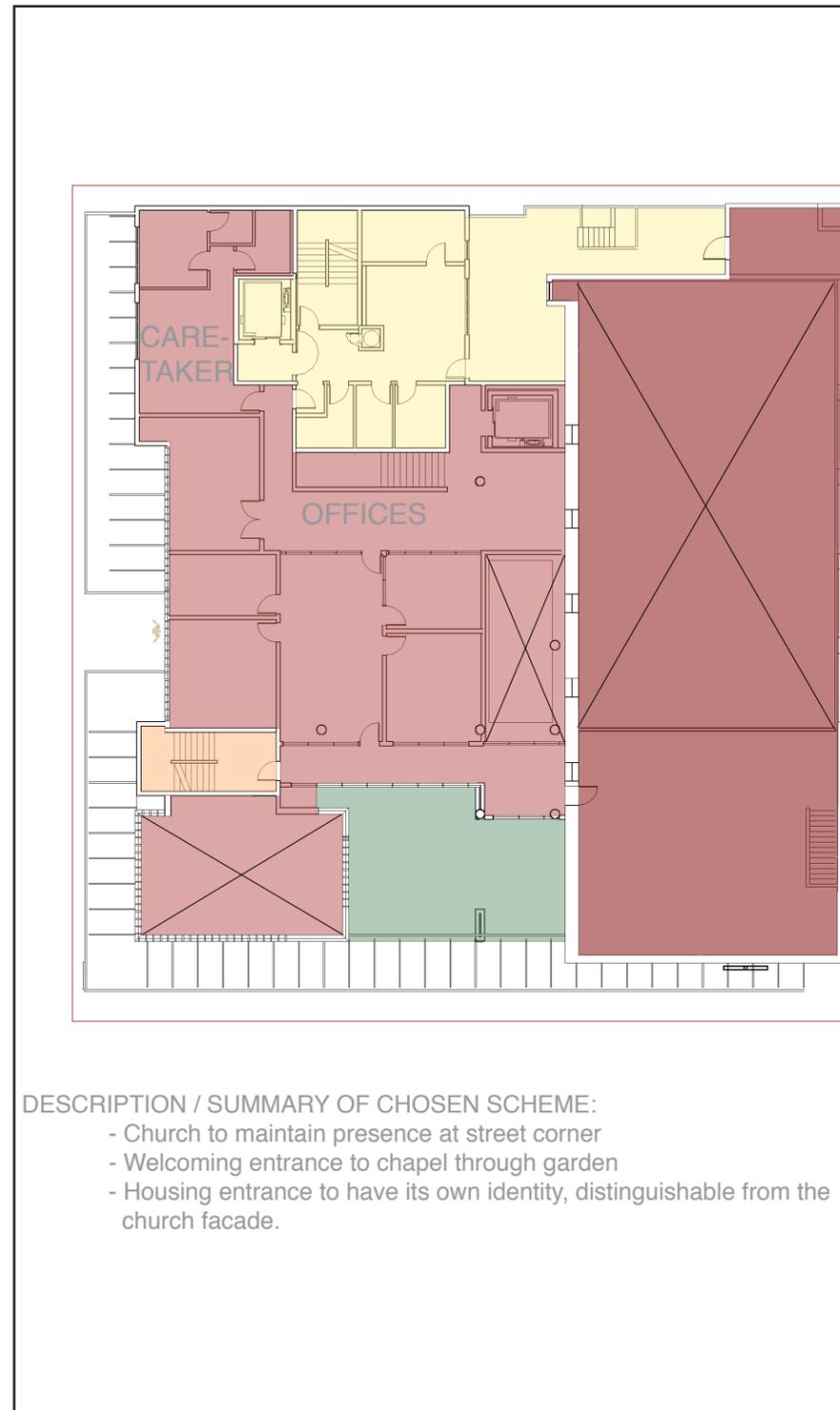
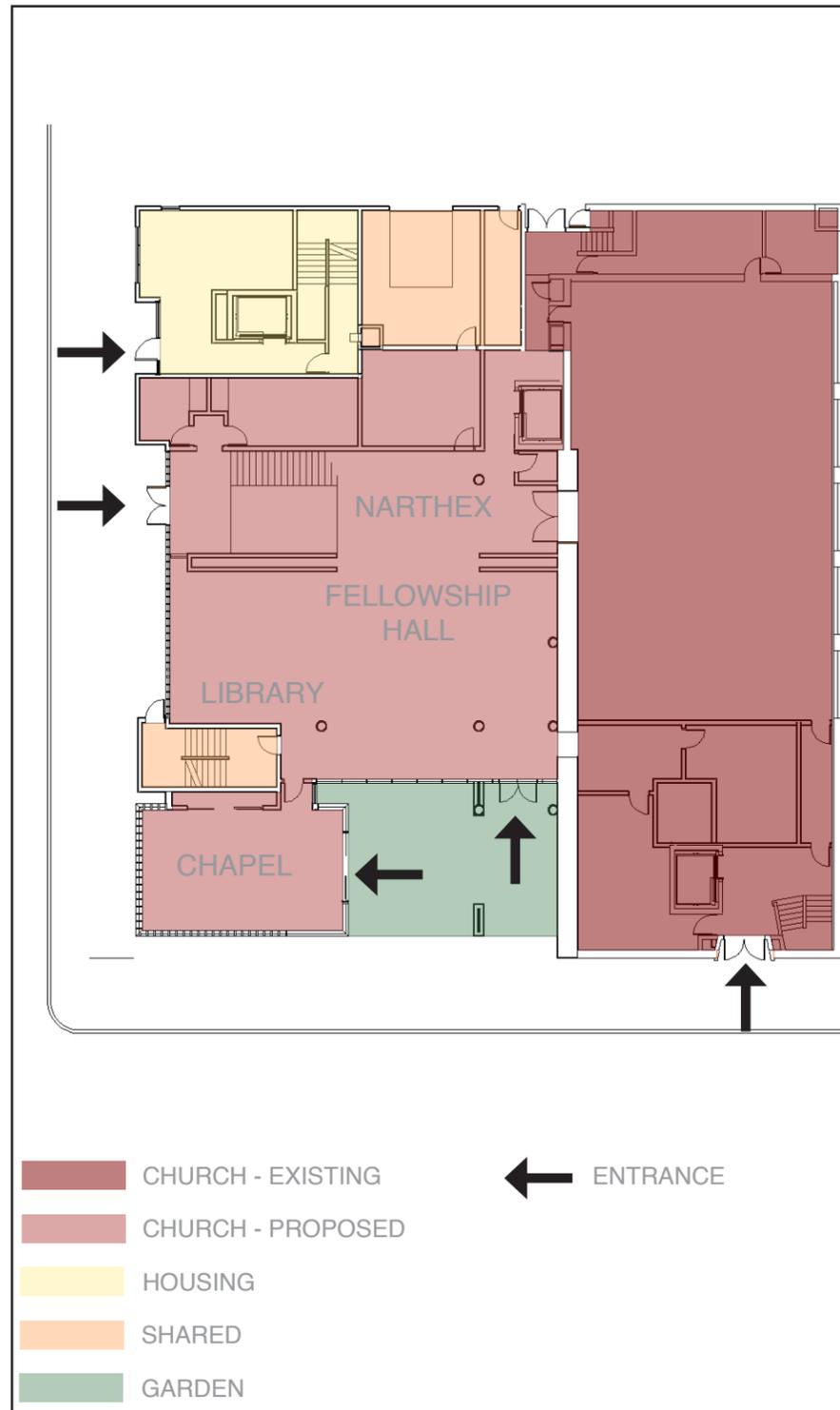
CONCEPTUAL ELEVATIONS



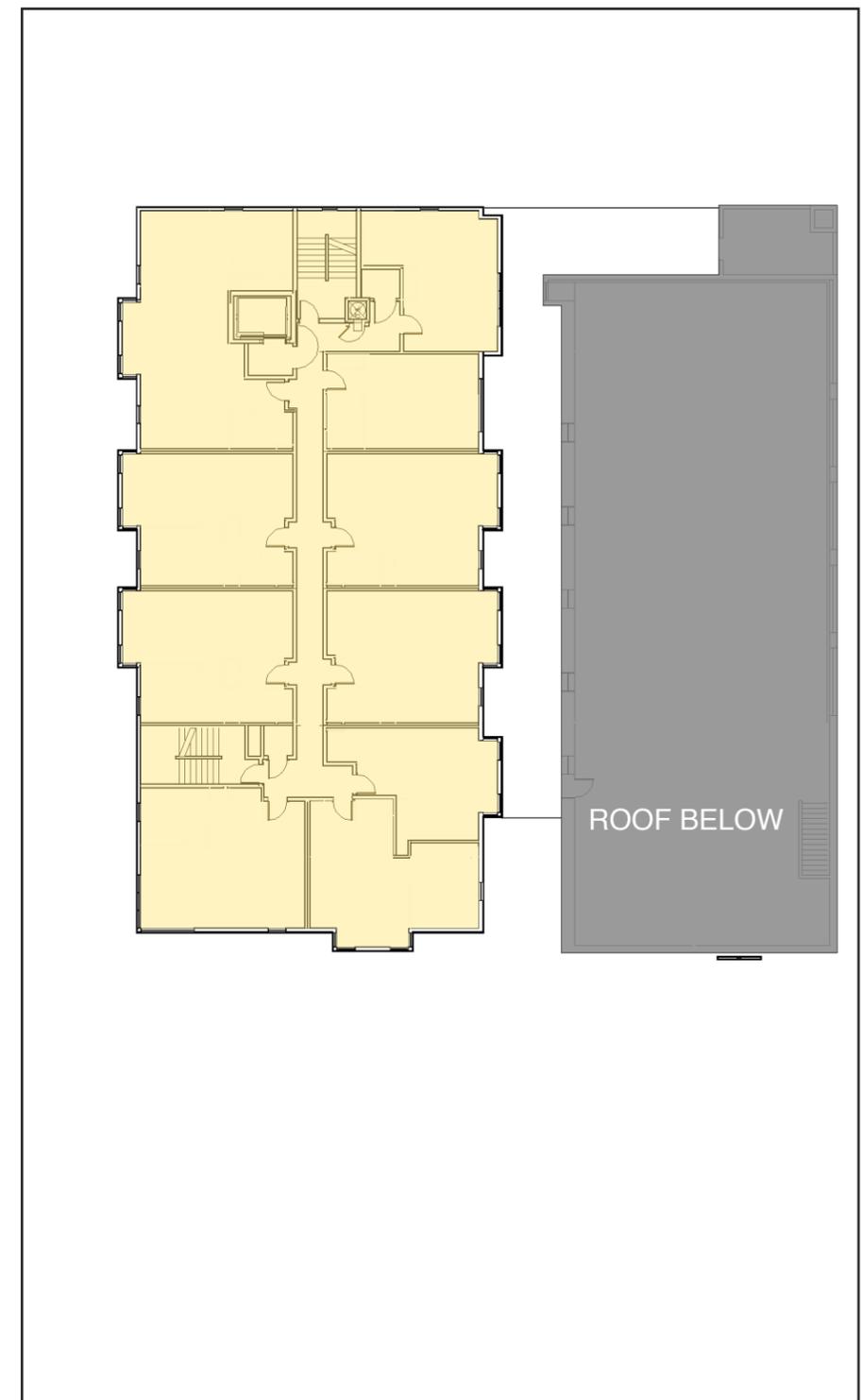
FIRST FLOOR PLAN

SECOND FLOOR PLAN

TYPICAL RESIDENTIAL PLAN

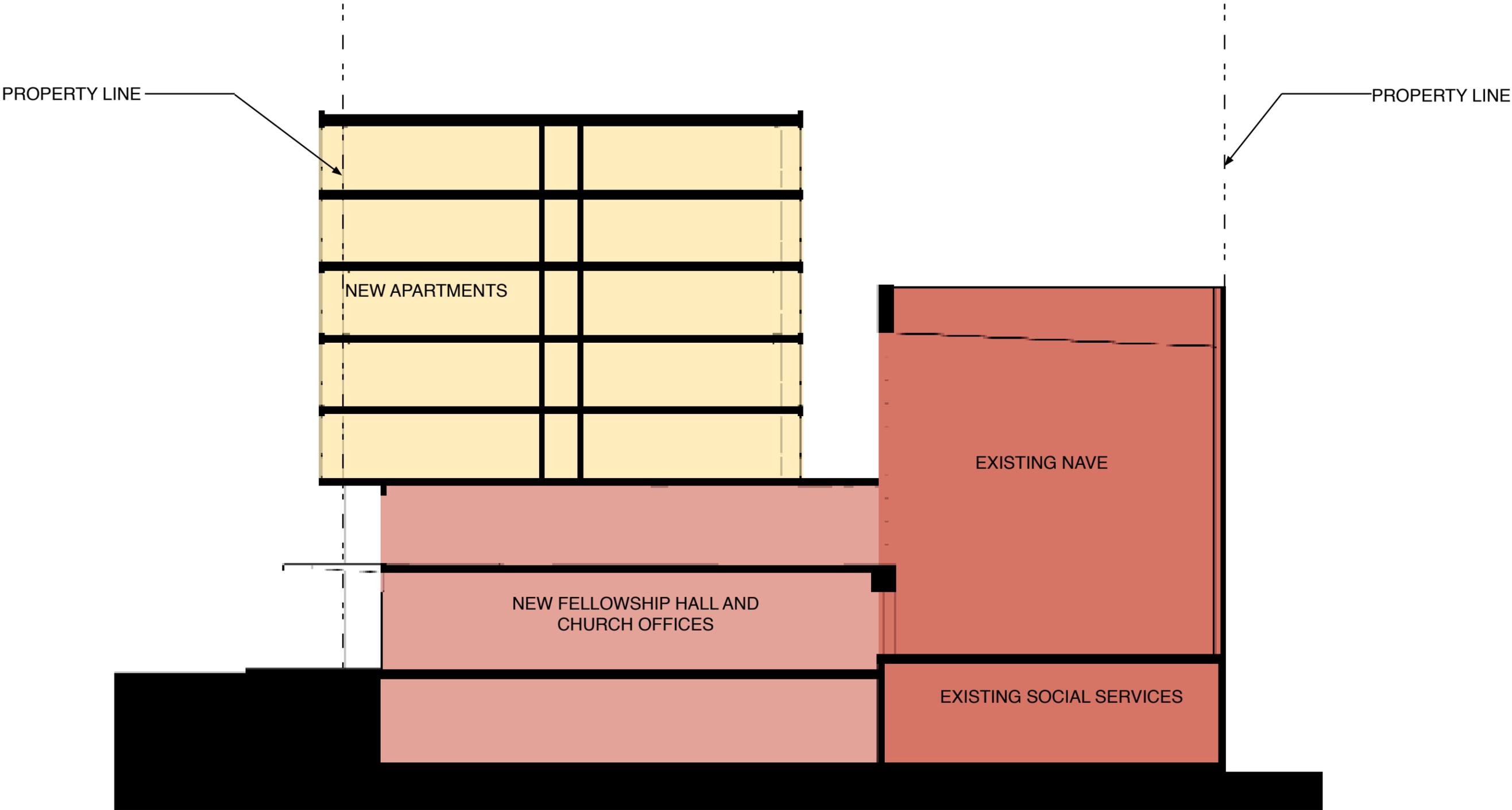


DESCRIPTION / SUMMARY OF CHOSEN SCHEME:
 - Church to maintain presence at street corner
 - Welcoming entrance to chapel through garden
 - Housing entrance to have its own identity, distinguishable from the church facade.



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REDEVELOPMENT PROJECT



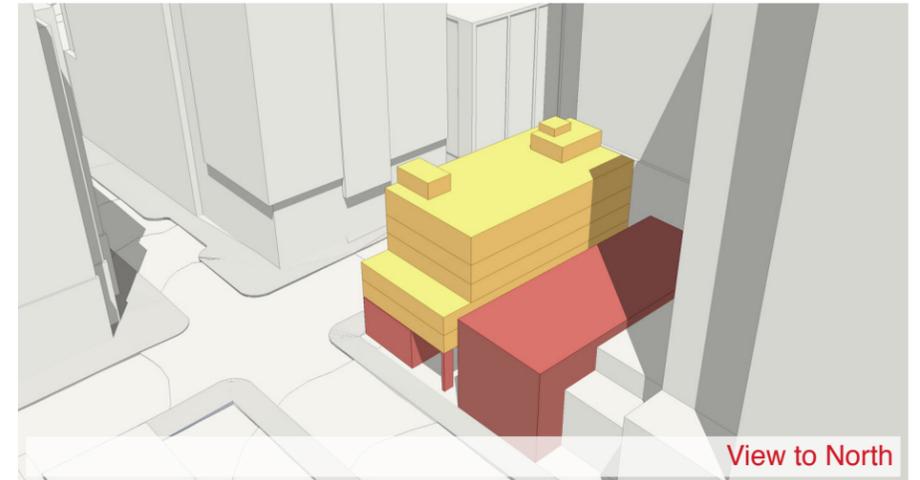
Pros:

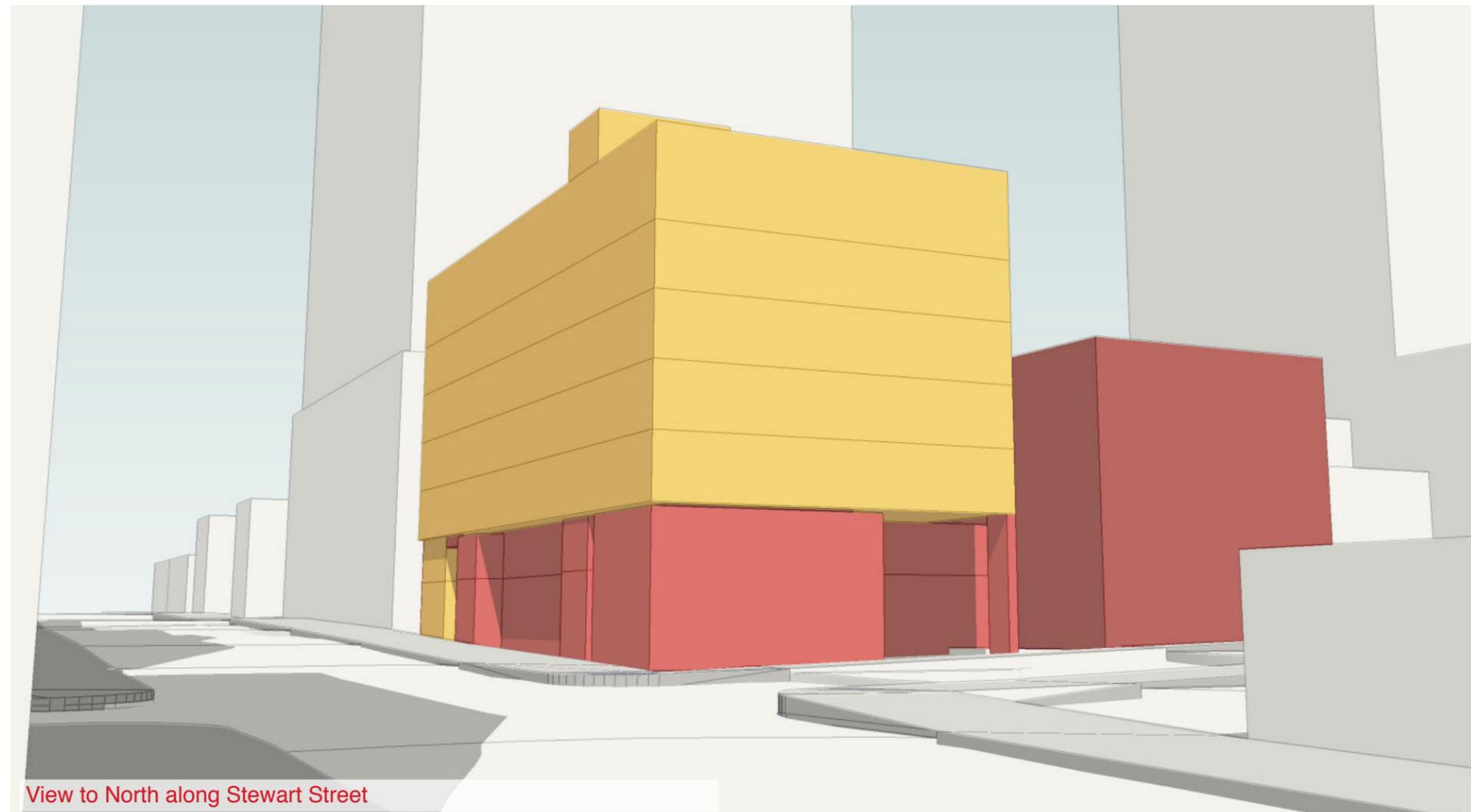
- Code-compliant (green street set-back)

Cons:

- Fewer residential units (therefore financially infeasible)
- Financially infeasible (as result of fewer units)
- Weak corner presence
- Lower roof area does not work well as either common deck or private unit decks
- In context of surrounding tall buildings, setback admits negligibly more sunlight to adjacent street

RESIDENTIAL CHURCH (NEW) CHURCH (EXISTING)





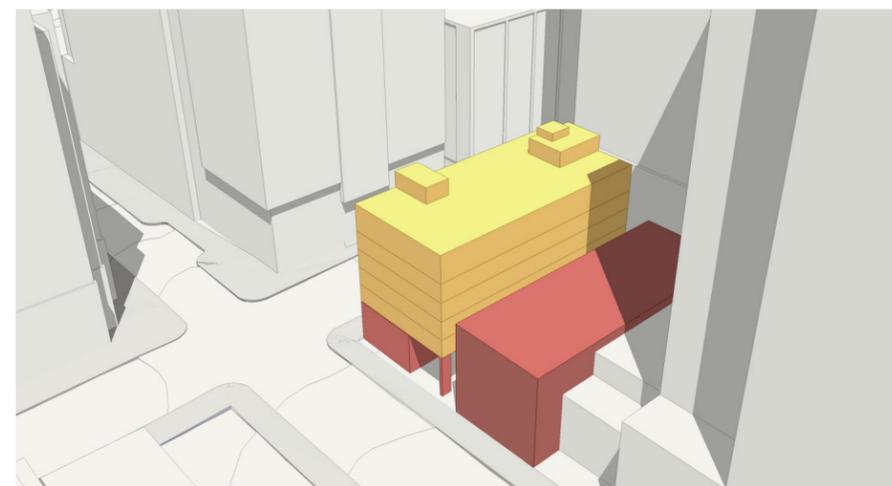
View to North along Stewart Street



View to East



View South along 9th Avenue



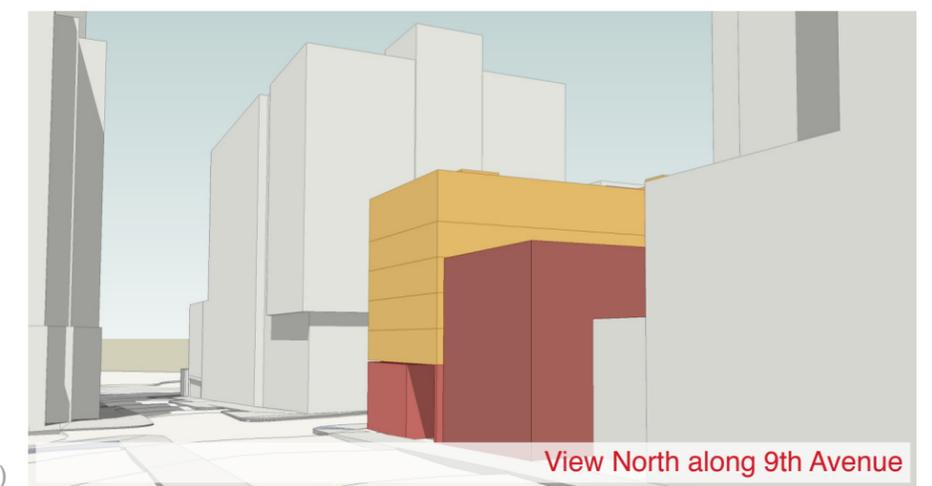
Pros:

- Simple massing
- More residential units
- Cost-effective
- Street wall more consistent with adjacent buildings
- Stronger corner presence

Cons:

- May not have residential feel
- Small units
- Success of simple massing dependent on quality of materials and detailing

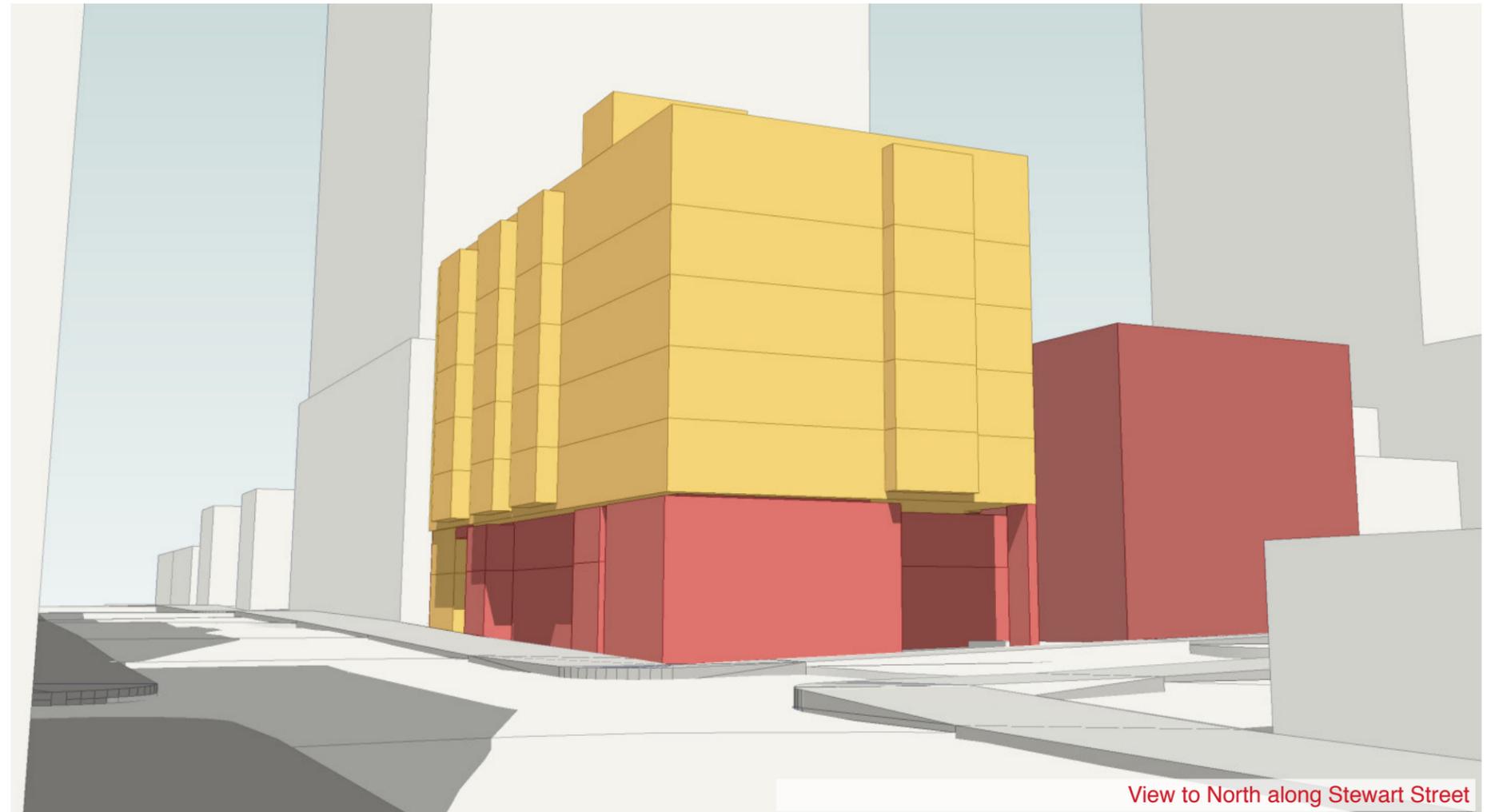
RESIDENTIAL
 CHURCH (NEW)
 CHURCH (EXISTING)



View North along 9th Avenue

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REDEVELOPMENT PROJECT



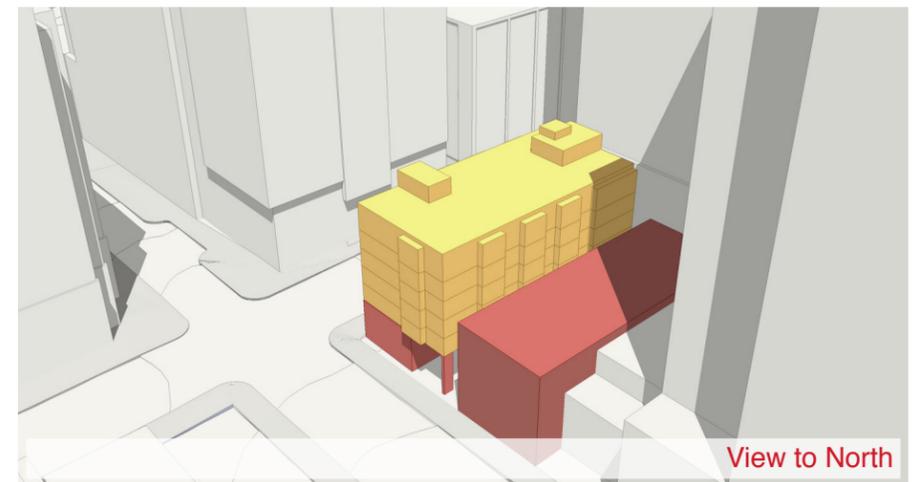
Pros:

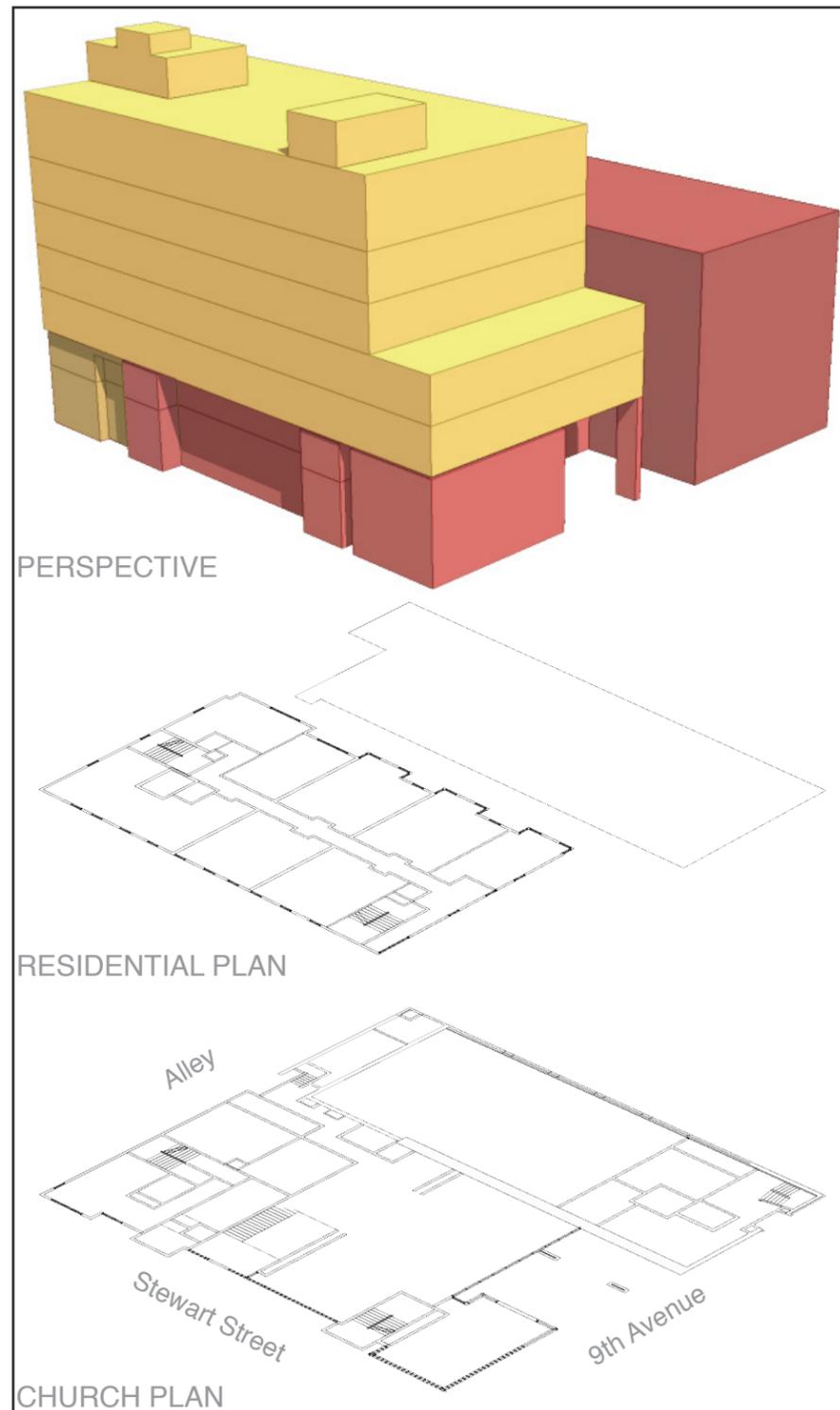
- Bays create more residential feel.
- More “eyes on street” with corner windows in most units.
- Bays provide additional living space in units.
- Modulation of bays reflects/reinforces modulation at the lower floors.

Cons:

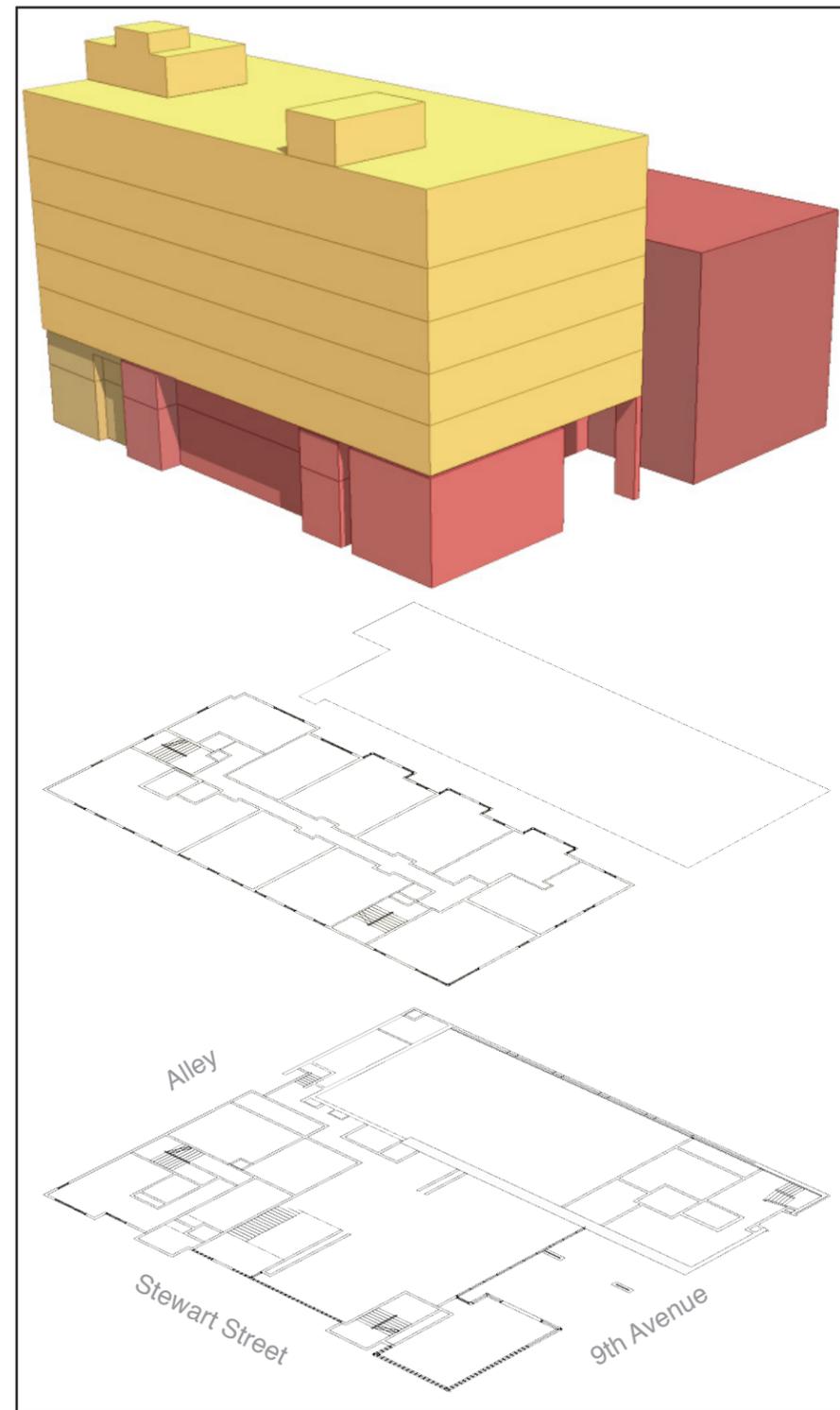
- Structural building overhang permit fees.
- Higher square foot construction costs.

RESIDENTIAL
 CHURCH (NEW)
 CHURCH (EXISTING)

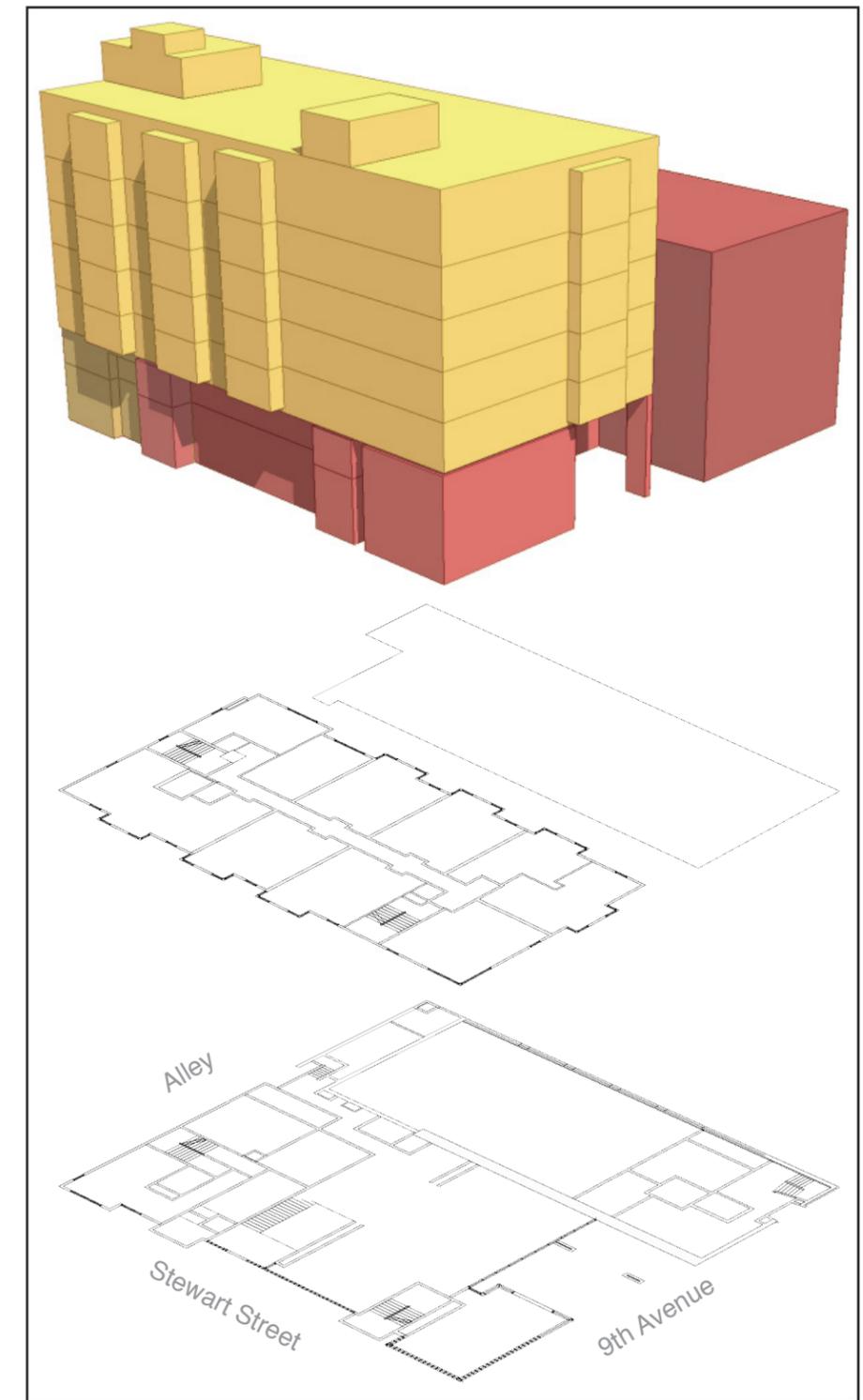




MASSING OPTION 1 - CODE COMPLIANT



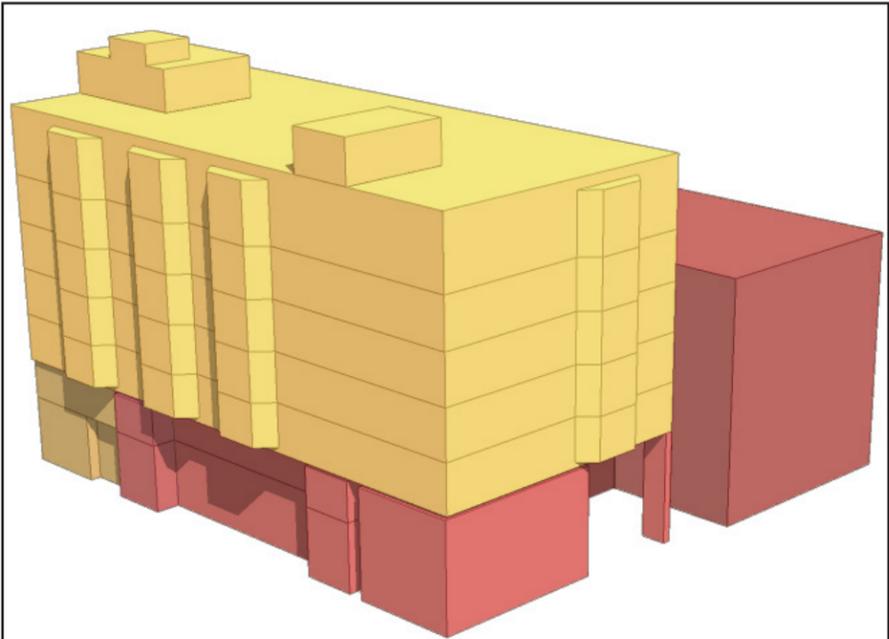
MASSING OPTION 2



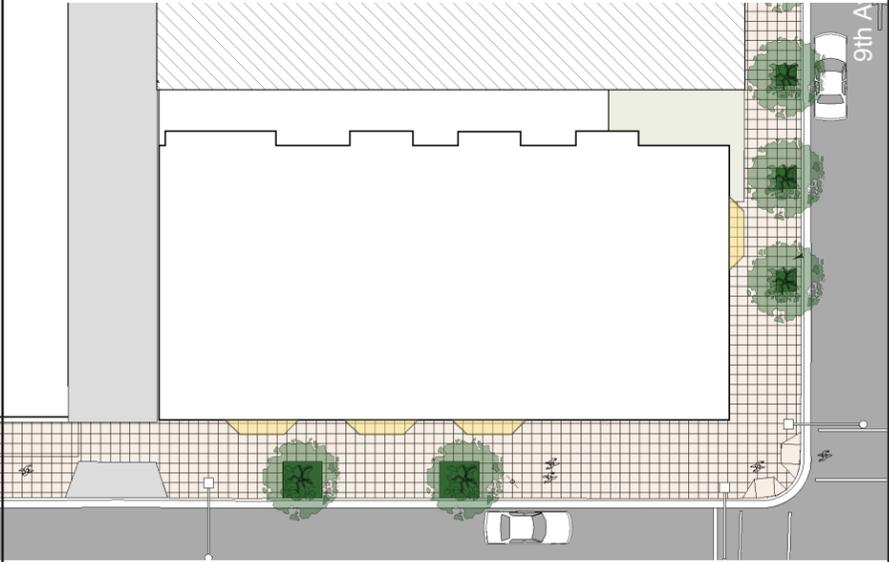
MASSING OPTION 3 - PREFERRED

GETHSEMANE

REDEVELOPMENT PROJECT

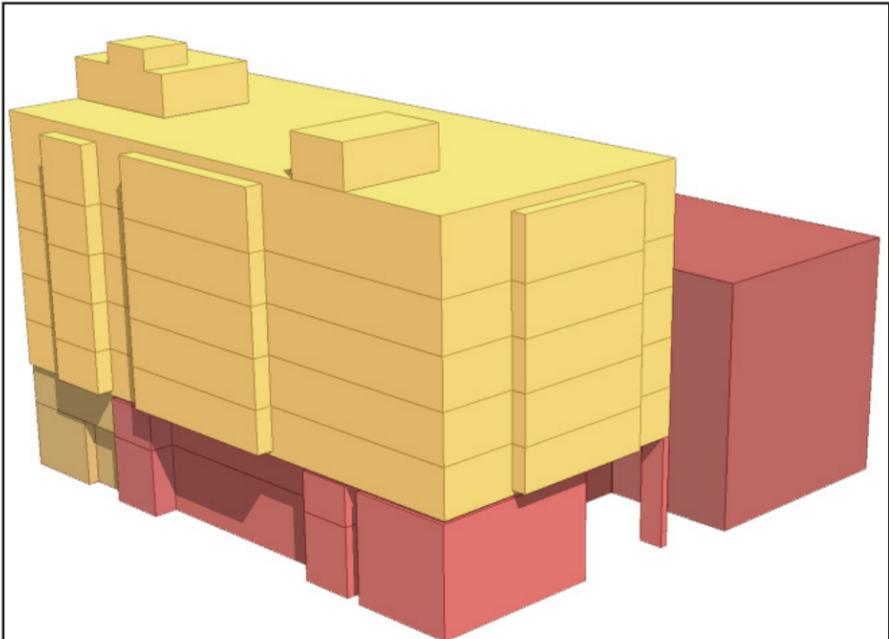


PERSPECTIVE

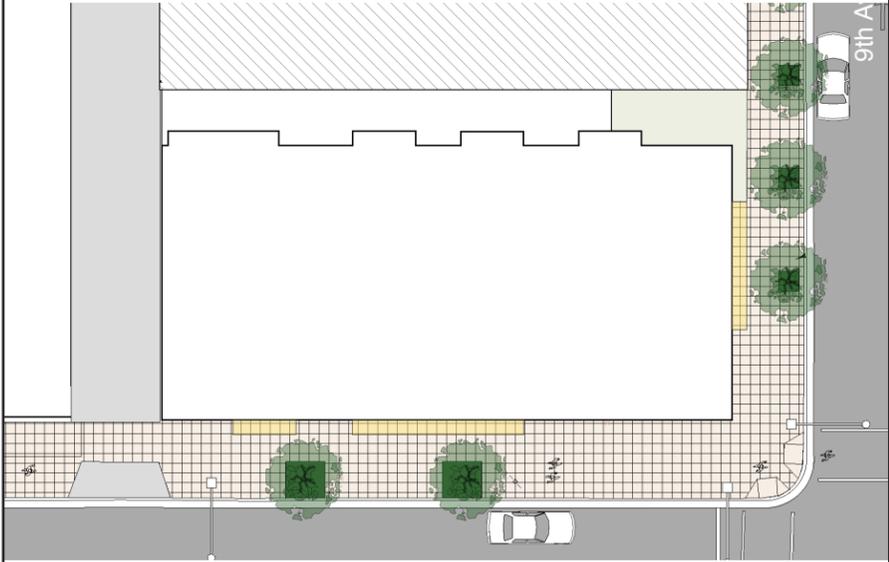


PLAN

BAY OPTION 1 - CODE COMPLIANT

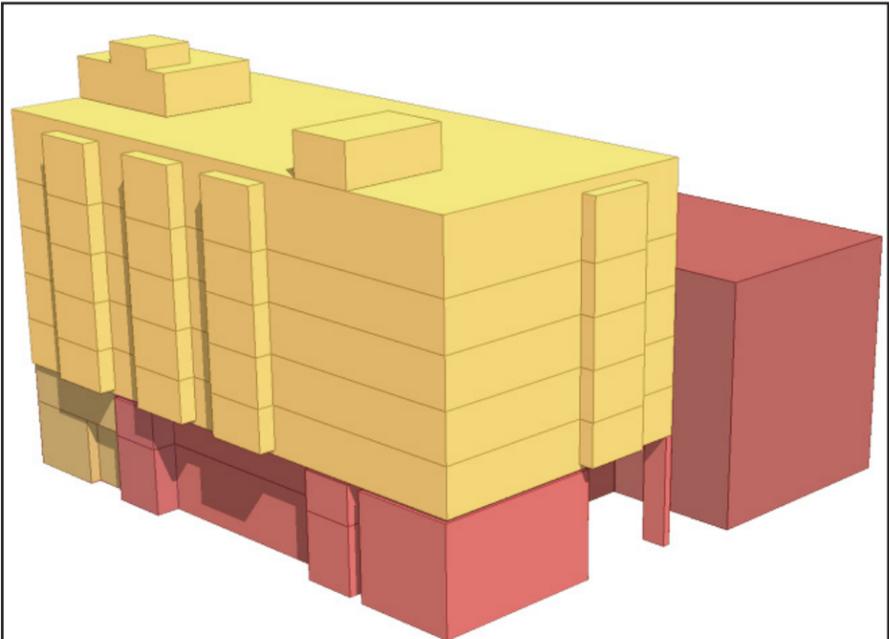


PERSPECTIVE

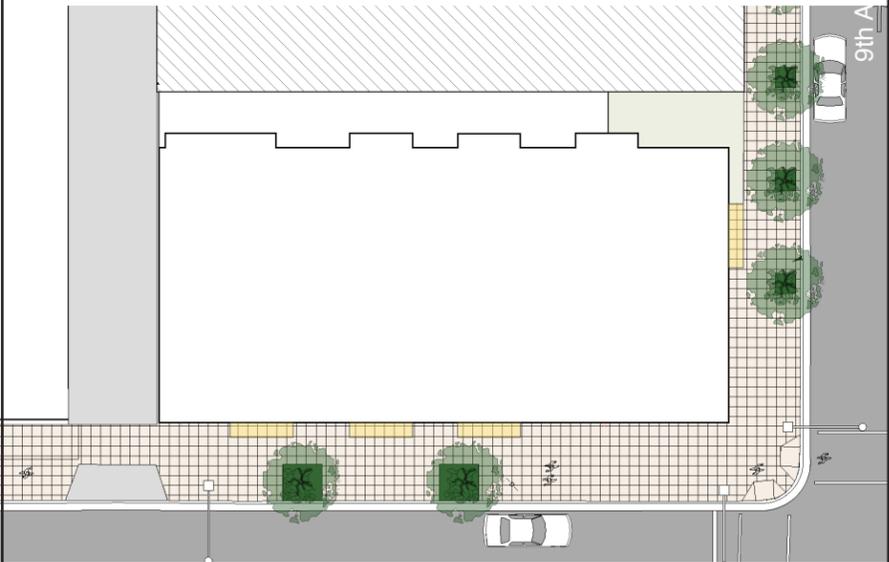


PLAN

BAY OPTION 2 - DOUBLE BAYS

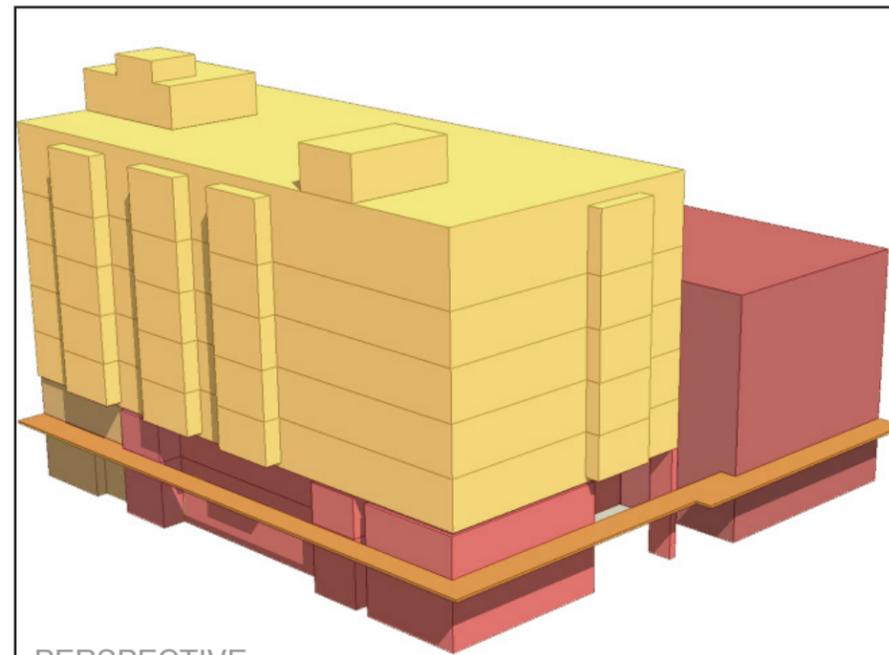


PERSPECTIVE

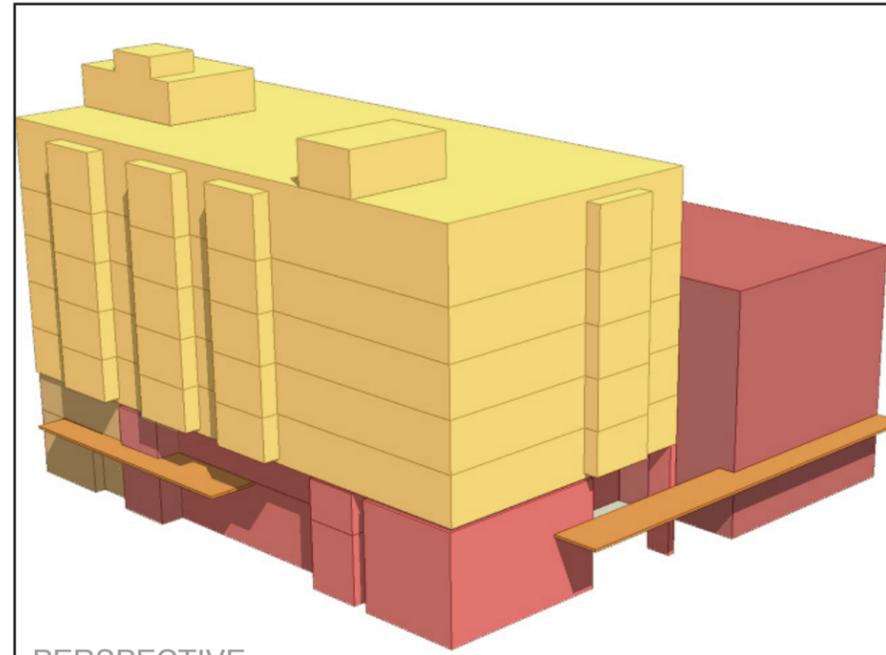


PLAN

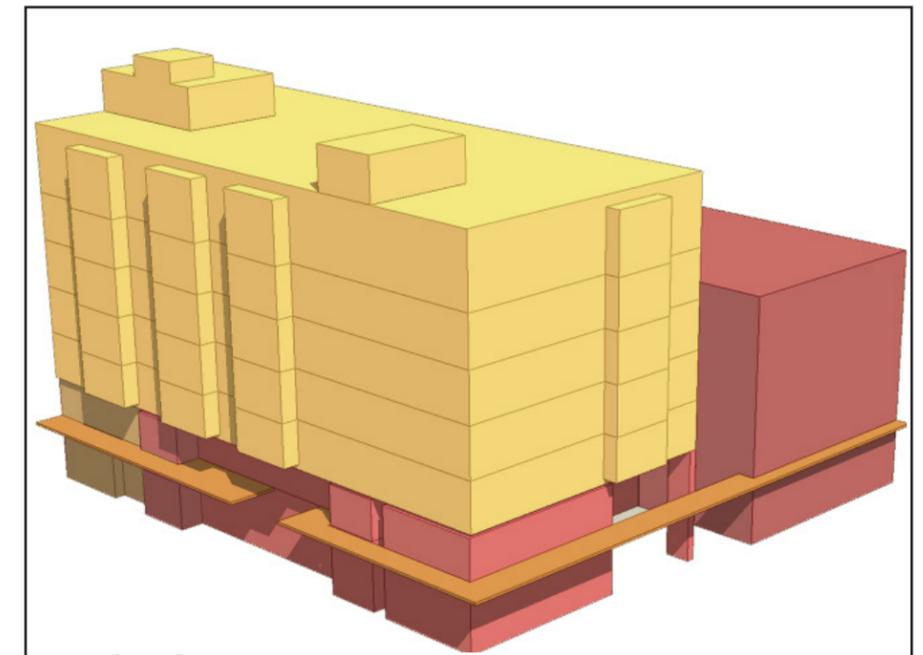
BAY OPTION 3 - PREFERRED



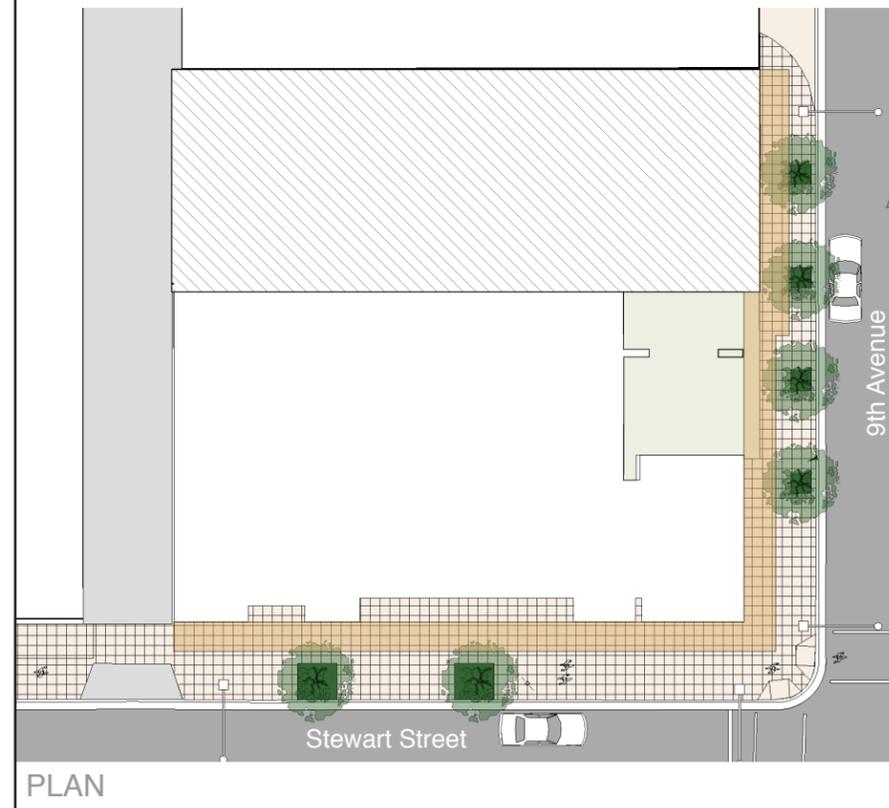
PERSPECTIVE



PERSPECTIVE

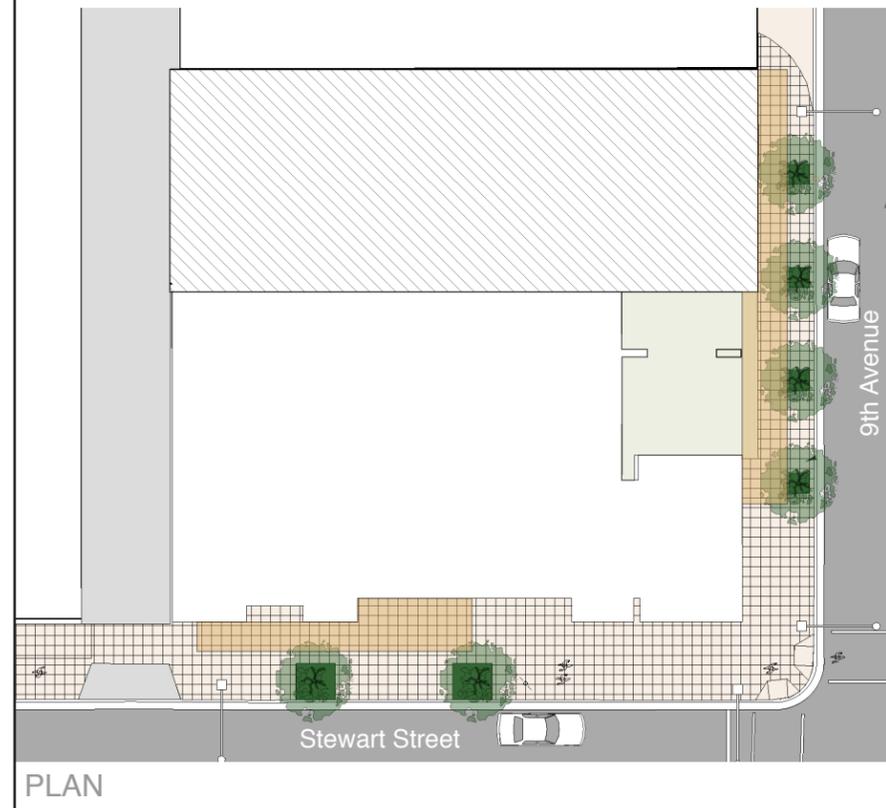


PERSPECTIVE



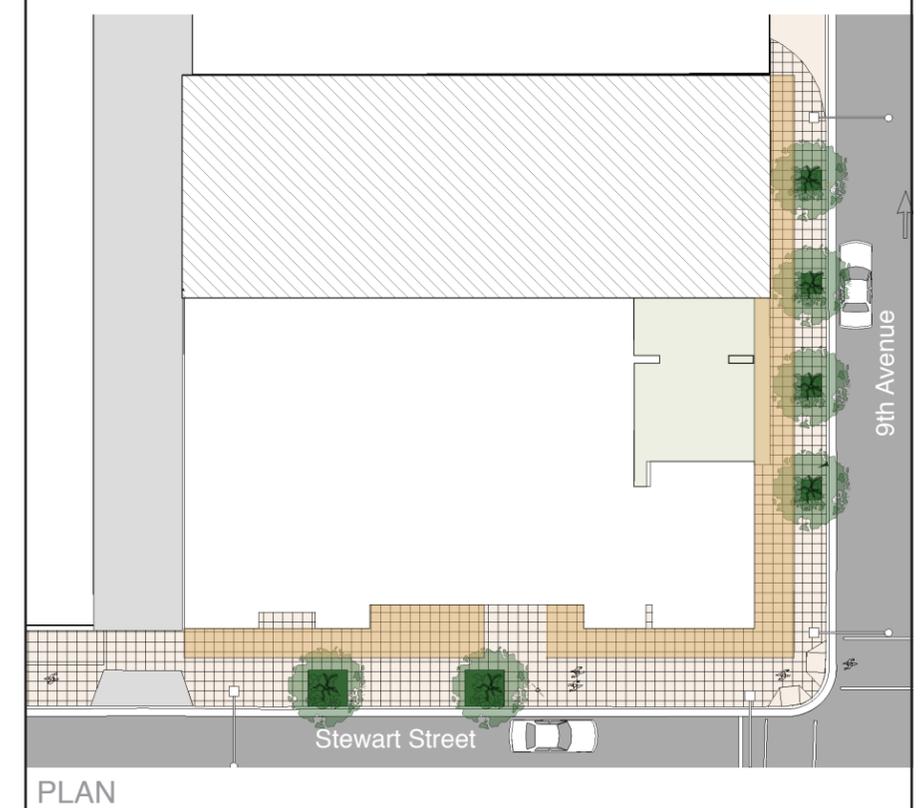
PLAN

CANOPY OPTION 1 - CODE COMPLIANT



PLAN

CANOPY OPTION 2



PLAN

CANOPY OPTION 3 - PREFERRED

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REDEVELOPMENT PROJECT



FLAT PANEL METAL SIDING

MATERIAL OPTION 1



METAL SIDING

MATERIAL OPTION 2



FIBER-CEMENT PANEL SIDING

MATERIAL OPTION 3

DESIGN GUIDELINES

These are the design guidelines that we feel are most pertinent to the project.

B-4: Design a well-proportioned & unified building.

Compose the massing and organize the interior and exterior spaces to create a well-proportioned building that exhibits a coherent architectural concept. Design the architectural elements and finish details to create a unified building, so that all components appear integral to the whole.

While this building will be smaller in scale than much of the surrounding development, it will be designed to have a strong presence on the street. Taking cues from the existing modernist church nave, the new structure will have strong, distinctive forms, with a more visually active base for the church functions and residential entry, and a simple, clean-lined top containing the residential units. A warm color palette, drawn from and expanding on the red brick of the nave, and organization of modulation and fenestration between base and top will create a unified composition that enhances the existing church.

C-2: Design façades of many scales.

Design architectural features, fenestration patterns, and material compositions that refer to the scale of human activities contained within. Building façades should be composed of elements scaled to promote pedestrian comfort, safety and orientation.

The street-level façades of the new structure will be visually interesting, and easily comprehensible to pedestrians approaching the site. Exterior finish materials will be used to differentiate church uses from the residential entry and lobby. Fenestration at the church entry and fellowship hall will provide visual connection between occupants and passersby. At the upper levels of the building, the façades will have fenestration scaled appropriately for the residential use.

C-3: Provide active – not blank – façades.

Buildings should not have large blank walls facing the street, especially near sidewalks.

The corner chapel will have tinted, translucent glazing, providing color and light on the street, and limited views into the space. Substantial glazing along Stewart will provide views into the church's entry and fellowship hall, as well as the residential lobby.

C-4: Reinforce building entries.

To promote pedestrian comfort, safety, and orientation, reinforce building entries.

The project has multiple entries: for the church, for the residential units and for the shelter. The entries will be reinforced via distinctive doorways, recessed entryways, signage and lighting.

C-5: Encourage overhead weather protection.

Project applicants are encouraged to provide continuous, well-lit, overhead weather protection to improve pedestrian comfort and safety along major pedestrian routes.

For pedestrian comfort, canopies will be provided along both 9th Avenue (including at the existing church nave) and Stewart Street. Weather protection will be continuous, except for a break in the canopy where the existing statue of Jesus is relocated on the new building wall.

D-1: Provide inviting & usable open space.

Design public open spaces to promote a visually pleasing, safe and active environment for workers, residents and visitors. Views and solar access from the principal area of the open space should be especially emphasized.

A small, sidewalk-level garden will be located between the existing church nave and the new chapel, providing an entrance into the church. The garden will be oriented toward the southwest, and will receive afternoon sunlight. The residential roof deck will receive some morning sunlight through the gap between the Aspira and Regence buildings, and will have afternoon sunlight.

D-2: Enhance the building with landscaping.

Enhance the building and site with generous landscaping – which includes special pavements, trellises, screen walls, planters, and site furniture, as well as living plant material.

On 9th Avenue, a small garden with water feature will be located between the new chapel and the existing church nave. The residential roof deck will feature decorative paving, site furniture and plantings, and will be designed to be an attractive part of the view from taller adjacent buildings.

D-3: Provide elements that define the place.

Provide special elements on the façades, within public open spaces, or on the sidewalk to create a distinct, attractive and memorable “sense of place” associated with the building.

The street-level façade will include several distinctive features: a two-story chapel, fully glazed with warm-toned glass to create a glowing presence at the corner; a small garden with water feature adjacent to the sidewalk on 9th; the existing statue of Jesus, already a landmark in the neighborhood, which will be relocated to a new wall on Stewart.

D-5: Provide adequate lighting.

To promote a sense of security for people downtown during nighttime hours, provide appropriate levels of lighting on the building façade, on the underside of overhead weather protection, on and around street furniture, in merchandising display windows, in landscaped areas and on signage.

The fully-glazed chapel at the corner of 9th & Stewart will be lit from the interior at night, providing a warm glow on the adjacent sidewalk. Accent lighting will be provided on the statue of Jesus, in the garden adjacent to the chapel, and will be used to highlight building entrances and signage.

GETHSEMANE

REDEVELOPMENT PROJECT



THE RED HOUSE

OSKA ARCHITECTS



OPPORTUNITY PLACE

SMR ARCHITECTS



TASHIRO KAPLAN

SMR ARCHITECTS



ARTSPACE HIAWATHA LOFTS

SMR ARCHITECTS



HILLCLIMB COURT

OSKA ARCHITECTS



WING LUKE ASIAN MUSEUM

OSKA ARCHITECTS



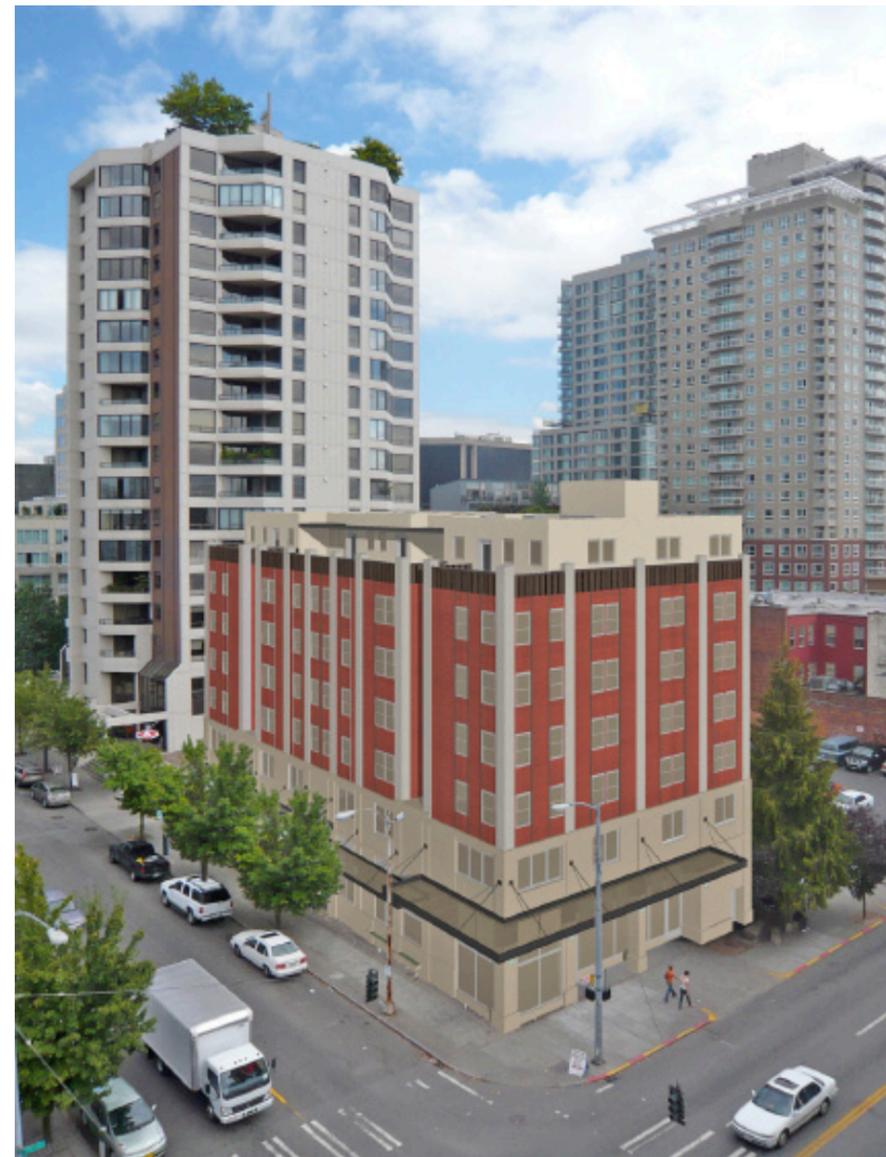
SAINT MARK'S CATHEDRAL

OSKA ARCHITECTS



ARTSPACE HIAWATHA LOFTS

SMR ARCHITECTS



A.L. HUMPHREY HOUSE

SMR ARCHITECTS



PLYMOUTH PLACE

SMR ARCHITECTS

Zoning: DMC 340/290-400-DF
(Downtown Mixed Commercial, Downtown Fire District)
Urban Village Overlay: Denny Triangle Urban Center Village

SMC 23.49.010 General requirements for residential uses.

B. Common Recreation Area. Common recreation area is required for all new development with more than twenty (20) dwelling units. Required common recreation area shall meet the following standards:

1. An area equivalent to five (5) percent of the total gross floor area in residential use, excluding any floor area in residential use gained in a project through a voluntary agreement for housing under SMC Section 23.49.015, shall be provided as common recreation area. In no instance shall the amount of required common recreation area exceed the area of the lot. The common recreation area shall be available to all residents and may be provided at or above ground level.
2. A maximum of fifty (50) percent of the common recreation area may be enclosed.
3. The minimum horizontal dimension for required common recreation areas shall be fifteen (15) feet, except for open space provided as landscaped setback area at street level, which shall have a minimum horizontal dimension of ten (10) feet. No required common recreation area shall be less than two hundred twenty-five (225) square feet.

SMC 23.49.011 Floor area ratio.

A. General Standards.

1. The base and maximum floor area ratio (FAR) for each zone is provided in Chart 23.49.011 A1.
2. Chargeable floor area shall not exceed the applicable base FAR except as expressly authorized pursuant to the provisions of this chapter.

SMC 23.49.018 Overhead Weather Protection and Lighting

A. Continuous overhead weather protection shall be required for new development along the entire street frontage of a lot except along those portions of the structure facade that:

1. are located farther than five (5) feet from the street property line or widened sidewalk on private property; or
2. abut a bonused open space amenity feature; or
3. are separated from the street property line or widened sidewalk on private property by a landscaped area at least two (2) feet in width; or
4. are driveways into structures or loading docks.

B. Overhead weather protection shall have a minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.

C. The installation of overhead weather protection shall not result in any obstructions in the sidewalk area.

D. The lower edge of the overhead weather protection must be a minimum of ten (10) feet and a maximum of fifteen (15) feet above the sidewalk.

E. Adequate lighting for pedestrians shall be provided. The lighting may be located on the facade of the building or on the overhead weather protection.

SMC 23.49.056 Street facade and street setback requirements.

Standards for the street facades of structures are established in this section for DOC1, DOC2, and DMC zones, for the following elements:

Minimum facade heights;
Setback limits;
Facade transparency;
Blank facade limits;
Street trees; and
Setback and Landscaping Requirements in the Denny Triangle Urban Village.

These standards apply to each lot line that abuts a street designated on Map 1F as having a pedestrian classification, except lot lines of open space TDR sites. The standards for each street frontage shall vary according to the pedestrian classification of the street on Map 1F, and whether property line facades are required by Map 1H. Standards for street landscaping and setback requirements in subsection G of this section also apply along lot lines abutting streets in the Denny Triangle Urban Village, as shown on Exhibit 23.49.056 F

A. Minimum Facade Height.

1. Minimum facade height(s) are prescribed in the chart below, and Exhibit 23.49.056 A but minimum facade heights shall not apply when all portions of the structure are lower than the elevation of the required minimum facade height listed below.

B. Facade Setback Limits.

2. General Setback Limits. The following setback limits apply on streets not requiring property line facades, as shown on Map 1H:

a. The portion of a structure subject to setback limits shall vary according to the structure height and required minimum facade height, as follows:

(1) Except as provided in subsection C2a(3) of this section, when the structure is greater than fifteen (15) feet in height, the setback limits apply to the facade between an elevation of fifteen (15) feet above sidewalk grade and the minimum facade height established in subsection A of this section and Exhibit 23.49.056 C

(2) When the entire structure is fifteen (15) feet or less in height, the setback limits apply to the entire street facade.

(3) When the minimum facade height is fifteen (15) feet, the setback limits apply to the portion of the street facade that is fifteen (15) feet or less in height.

b. The maximum area of all setbacks between the lot line and facade along each street frontage of a lot shall not exceed the area derived by multiplying the averaging factor by the width of the street frontage of the structure along that street (see Exhibit 23.49.056 D). The averaging factor shall be five (5) on Class I pedestrian streets and ten (10) on Class II pedestrian streets and designated green streets.

c. The maximum width, measured along the street property line, of any setback area exceeding a depth of fifteen (15) feet from the street property line shall not exceed eighty (80) feet, or thirty (30) percent of the lot frontage on that street, whichever is less. (See Exhibit 23.49.056 D)

d. The maximum setback of the facade from the street property lines at intersections shall be ten (10) feet. The minimum distance the facade must conform to this limit shall be twenty (20) feet along each street. (See Exhibit 23.49.056 E)

e. Any exterior public open space that meets the Downtown Amenity

Standards, whether it receives a bonus or not, and any outdoor common recreation area required for residential uses, shall not be considered part of a setback. (See Exhibit 23.49.056 C)

f. When sidewalk widening is required by Section 23.49.022, setback standards shall be measured to the line established by the new sidewalk width rather than the street property line.

C. Facade Transparency Requirements.

1. Facade transparency requirements apply to the area of the facade between two (2) feet and eight (8) feet above the sidewalk, except that when the slope along the street frontage of the facade exceeds seven and one-half (7 1/2) percent, the transparency requirements apply to the area of the facade between four (4) feet and eight (8) feet above sidewalk grade. Only clear or lightly tinted glass in windows, doors, and display windows is considered to be transparent. Transparent areas shall allow views into the structure or into display windows from the outside.

2. Facade transparency requirements do not apply to portions of structures in residential use.

3. When the transparency requirements of this subsection are inconsistent with the glazing limits in the Energy Code, this subsection shall apply.

4. Transparency requirements are as follows:

a. Class I pedestrian streets and designated green streets: A minimum of sixty (60) percent of the street level facade shall be transparent.

D. Blank Facade Limits.

1. General Provisions.

a. Blank facade limits apply to the area of the facade between two (2) feet and eight (8) feet above the sidewalk, except that where the slope along the street frontage of the facade exceeds seven and one-half (7 1/2) percent, blank facade limits apply to the area of the facade between four (4) feet and eight (8) feet above sidewalk grade.

b. Any portion of a facade that is not transparent shall be considered to be a blank facade.

c. Blank facade limits do not apply to portions of structures in residential use.

2. Blank Facade Limits for Class I Pedestrian Streets and designated Green Streets.

a. Blank facades shall be no more than fifteen (15) feet wide except for garage doors which may exceed fifteen (15) feet. Blank facade width may be increased to thirty (30) feet if the Director determines that the facade is enhanced by architectural detailing, artwork, landscaping, or similar features that have visual interest. The width of garage doors shall be limited to the width of the driveway plus five (5) feet.

b. Any blank segments of the facade shall be separated by transparent areas at least two (2) feet wide.

c. The total of all blank facade segments, including garage doors, shall not exceed forty (40) percent of the street facade of the structure on each street frontage, or fifty (50) percent if the slope of the street frontage of the facade exceeds seven and one-half (7 1/2) percent.

F. Setback and Landscaping Requirements for Lots Located Within the Denny Triangle Urban Village.

1. Landscaping in the Street Right-of-Way for All Streets Other Than Those With Green Street Plans Approved by Director's Rule. All new development in DMC zones in the Denny Triangle Urban Village, as shown on Exhibit 23.49.056 F shall provide landscaping in the sidewalk area of the street right-of-way, except on streets with a Green Street plan approved by Director's Rule. The square footage of landscaped area provided shall be at least one and one-half (1 1/2) times the length of the street property line (in linear feet). The following standards apply to the required landscaped area:

a. The landscaped area shall be at least eighteen (18) inches wide and shall be located in the public right-of-way along the entire length of the street property line, except for building entrances, vehicular access or other connections between the sidewalk and the lot, provided that the exceptions may not exceed fifty (50) percent of the total length of the street property line(s).

b. As an alternative to locating the landscaping at the street property line, all or a portion of the required landscaped area may be provided in the sidewalk area within five (5) feet of the curblin.

c. Landscaping provided within five (5) feet of the curblin shall be located and designed in relation to the required street tree planting and be compatible with use of the curb lane for parking and loading.

d. All plant material shall be planted directly in the ground or in permanently installed planters where planting in the ground is not feasible. A minimum of fifty (50) percent of the plant material shall be perennial.

2. Landscaping on a Designated Green Street. Where required landscaping is on a designated Green Street, or on a street with urban design and/or landscaping guidelines promulgated by Seattle Department of Transportation, the planting shall conform to those provisions

3. Landscaping in Setbacks.

a. In the Denny Triangle Urban Village, as shown on Exhibit 23.49.056 F at least twenty (20) percent of the total square footage of all areas abutting the street property line that are not covered by a structure, have a depth of ten (10) feet or more from the street property line and are larger than three hundred (300) square feet, shall be landscaped. Any area under canopies or marquees is considered uncovered. Any setback provided to meet the minimum sidewalk widths established

by Section 23.49.022 is exempt from the calculation of the area to be landscaped.

b. All plant material shall be planted directly in the ground or in permanently installed planters where planting in the ground is not feasible. A minimum of fifty (50) percent of the plant material shall be perennial and shall include trees when a contiguous area, all or a portion of which is landscaped pursuant to subsection G1a above, exceeds six hundred (600) square feet.

4. Terry and 9th Avenues Green Street Setbacks.

a. In addition to the requirements of subsections G2 and G3 of this section, a two (2) foot wide setback from the street property line is required along the Terry and 9th Avenue Green Streets within the Denny Triangle Urban Village as shown on Exhibit 23.49.056 F The Director may allow averaging of the setback requirement of this subsection to provide greater conformity with an adopted Green Street plan.

b. Fifty (50) percent of the setback area must be landscaped.

SMC 23.49.019 Parking quantity, location and access requirements, and screening and landscaping of surface parking areas.

A. Parking Quantity Requirements.

1. No parking, either long-term or short-term, is required for uses on lots in Downtown zones, except as follows:

c. Bicycle parking is required as specified in E1 of this section.

E. Bicycle Parking.

1. The minimum number of off-street spaces for bicycle parking required for specific use categories is set forth in Chart 23.49.019 A. In the case of a use not shown on Chart 23.49.019 A, there is no minimum bicycle parking requirement. After the first fifty (50) spaces for bicycles are provided for a use, additional spaces are required at one half (1/2) the ratio shown in Chart 23.49.019 A. Spaces within dwelling units or on balconies do not count toward the bicycle parking requirement.

2. Required bicycle parking shall be provided in a safe, accessible and convenient location. Bicycle parking hardware shall be installed according to its manufacturer's instructions, and the Seattle Department of Transportation design criteria, allowing adequate clearance for bicycles and their riders. Directional signage shall be installed when bike parking facilities are not clearly visible from the street or sidewalk. When any covered automobile parking is provided, all required long-term bicycle parking shall be covered. When located off-street, bicycle and automobile parking areas shall be separated by a barrier or painted lines.

3. Bicycle parking facilities for nonresidential uses shall be located on the lot or in a shared bicycle parking facility within one hundred (100) feet of the lot, except as provided in subsection 6 below.

4. Bicycle parking for residential uses shall be located on-site.

5. Co-location of bicycle parking facilities by more than one (1) use is encouraged.

6. For nonresidential uses, the applicant may make a payment to the City to fund public bicycle parking in the public right-of-way in lieu of providing required bicycle parking on- or off-site, if the Director determines that:

a. Safe, accessible and convenient bicycle parking accessory to a non-residential use cannot be provided on-site or in a shared bicycle parking facility within one hundred (100) feet of the lot, without extraordinary physical or financial difficulty;

b. The payment is comparable to the cost of providing the equivalent bicycle parking on-site, and takes in consideration the cost of materials, equipment and labor for installation; and

c. The bicycle parking funded by the payment is located within sufficient proximity to serve the bicycle parking demand generated by the project.

d. Any such payment shall be placed in a dedicated fund or account and used within five (5) years of receipt to provide the bicycle parking.

G. Off-street Loading.

1. Off-street loading spaces shall be provided according to the standards of Section 23.54.030 Parking space standards.

SMC 23.49.058 Downtown Office Core 1, Downtown Office Core 2, and Downtown Mixed Commercial upper-level development standards. The provisions of this section apply in DOC 1, DOC 2, and DMC zones.

F. Upper Level Setbacks.

2. When a lot in a DMC or DOC2 zone is located on a designated green street, a continuous upper-level setback of fifteen (15) feet shall be provided on the street frontage abutting the green street at a height of forty-five (45) feet.

SMC.53.035 Structural building overhangs.

A. Structural building overhangs are encroachments into public property that include cornices, eaves, sills, belt courses, bay windows, balconies, facade treatment and other architectural features. They shall be designed in accordance with the standards set forth in this section and rules promulgated by the Director. Structural building overhangs, when approved, shall meet the following requirements:

1. Vertical clearance shall be a minimum of eight (8) feet from the sidewalk or twenty-six (26) feet from an alley, or greater when required by other regulations.

2. Overhead horizontal projections of a purely architectural or decorative character such as cornices, eaves, sills, and belt courses shall be limited to a maximum horizontal extension of one (1) foot and maximum vertical dimension of two (2) feet six (6) inches, and shall not increase the floor area or the volume of space enclosed by the building. At roof level, the projections may extend not more than three (3) feet horizontally. The vertical dimension of the overhead horizontal projection at the roof level may be increased if the roof level is one hundred (100) feet or higher above the street elevation. The total area of these projections shall not exceed thirty (30) percent of the area of any one (1) facade (see Exhibit 23.53.035-A).

3. Exception to the standards in this section may be authorized for historic or rehabilitated buildings, when they are consistent with the scope and intent of these standards.

4. Vertical bay (projecting) windows, balconies (other than balconies used for primary access), and similar features that increase either the floor area of the building or the volume of space enclosed by the building above grade, shall be limited as follows:

a. The maximum horizontal projection shall be three (3) feet and the projection shall be in no case be closer than eight (8) feet to the center-line of any alley (see Exhibit 23.53.035-B).

b. The glass areas of each bay window, and the open portions of each balcony, shall not be less than fifty (50) percent of the sum of the areas of the vertical surfaces of such bay window or balcony above the required open area. At least one-third of such required glass area of such bay window, and open portions of such balcony, shall be on one (1) or more vertical surfaces situated at an angle of not less than thirty (30) degrees to the line establishing the required open area. In addition, at least one-third of such required glass area or open portions shall be on the vertical surface parallel to, or most nearly parallel to, the line establishing each open area over which the bay window or balcony projects.

c. The maximum length of each bay window or balcony shall be fifteen (15) feet at the line establishing the required open area, and shall be reduced in proportion to the distance from such line by means of forty-five (45) degree angles drawn inward from the ends of such fifteen (15) foot dimension, reaching a maximum of nine (9) feet along a line parallel to and at a distance of three (3) feet from the line establishing the open area (see Exhibit 23.53.035-C).

d. Where a bay window and a balcony are located immediately adjacent to one another, and the floor of such balcony in its entirety has a minimum horizontal dimension of six (6) feet, the limit set in subsection A4c above, shall be increased to a maximum length of eighteen (18) feet at the line establishing the required open area, and a maximum of twelve (12) feet along a line parallel to and at a distance of three (3) feet from the line establishing the required open area (see Exhibit 23.53.035-D).

e. The minimum horizontal separation between bay windows, between balconies, and between bay windows and balconies (except where a bay window and a balcony are located immediately adjacent to one another, as provided for in subsection A4d above), shall be two (2) feet at the line establishing the required open area, and shall be increased in proportion to the distance from such line by means of one hundred thirty-five (135) degree angles drawn outward from the ends of such two (2) foot dimension, reaching a minimum of eight (8) feet along a line parallel to and at a distance of three (3) feet from the line establishing the required open area (see Exhibits 23.53.035-E).

f. Each bay window or balcony over a street or alley shall also be horizontally separated from interior lot lines (except where the wall or a building on the adjoining lot is flush to the interior lot line immediately adjacent to the projecting portions of such bay window or balcony) by not less than one (1) foot at the line establishing the required open area, with such separation increased in proportion to the distance from such line by means of a one hundred thirty-five (135) degree angle drawn outward from such one (1) foot dimension, reaching a minimum of four (4) feet along a line parallel to and at a distance of three (3) feet from the line establishing the required open area (see Exhibit 23.53.035-F).

B. Submittal Requirements.

1. An application for a structure containing features overhanging the public right-of-way must show the following:

a. Dimensions on the site plan for canopies that overhang no closer than six (6) feet to the curb;

b. Windows in any bays;

c. Where the SDOT landscape architect indicates that retention of the street trees would be unfeasible, indicate planting of new street trees of at least four (4) inch caliper.

