

Design Review

2301 West Newton Street

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Design Review

2301 West Newton Street

Project Information

Location
2301 West Newton Street Seattle WA 98119

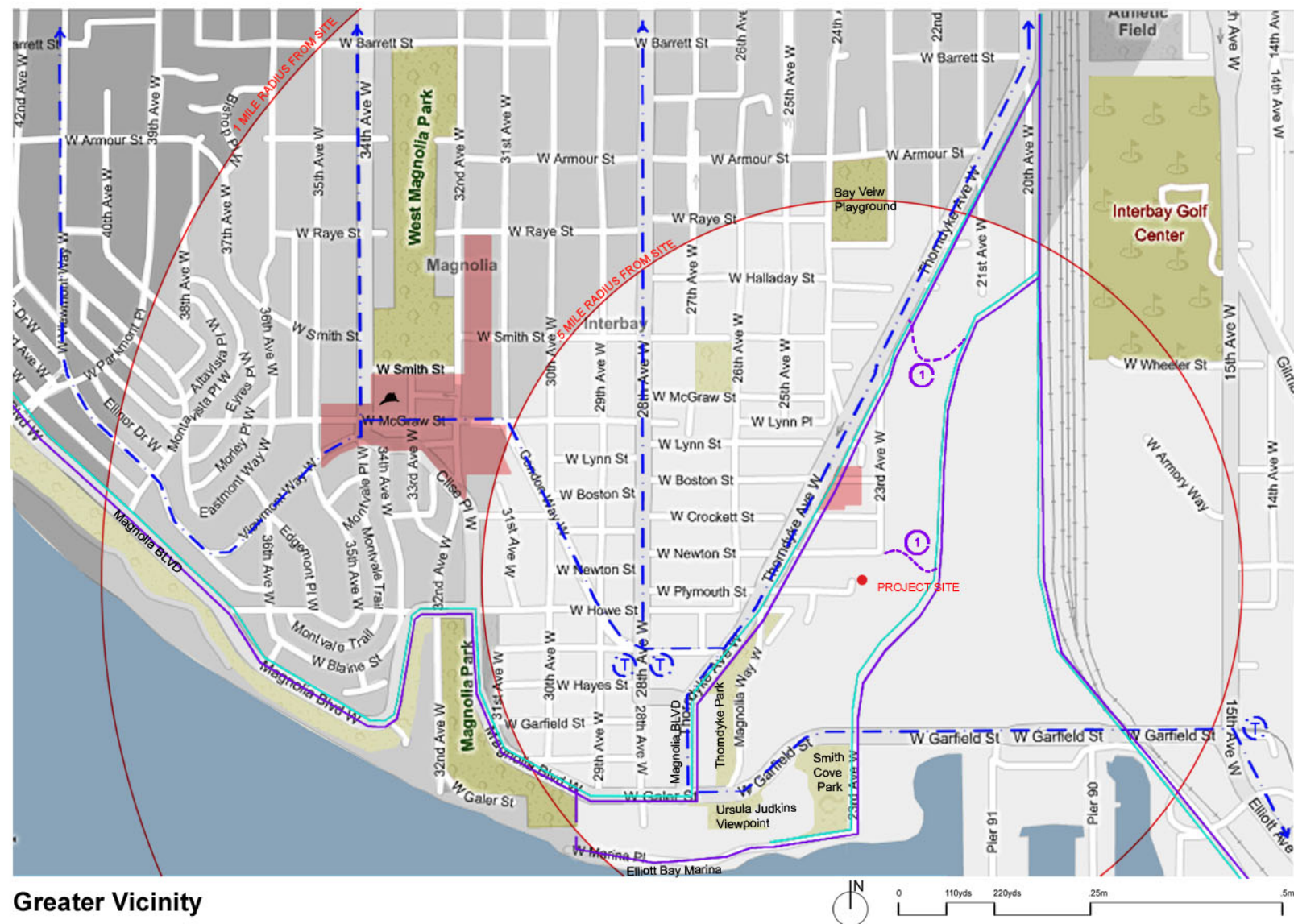
Owner
Christopher and Michele Toly

Architect
E. Cobb Architects Inc.
911 Western Avenue Suite 318 Seattle WA 98104

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Greater Vicinity

Site Analysis Summary

Location and Connection

Located on the edge of the Magnolia neighborhood and within 1 mile of the neighborhood commercial center. The site is visual connected to Magnolia, Ballard, Queen Anne, and Downtown neighborhoods and there are many available options adjacent to the project site for access to parts of Seattle by automobile, mass transit, bus, bicycle, and walking.

Transportation

The arterial street of Thorndyke Ave W. is 160 yards from the site; the buses running on this street enable connection to Downtown, the U. District, Ballard, Queen Anne, and greater Magnolia area.

Topography

The greater site topography slopes down from Thorndyke Ave W. and then drops away sharply at the project site. The site is approximately 70 feet above the Northbay Industrial Lands. The steep slope below the site provides a high degree of openness on the site.

Recreation Activities

Within a half mile of the site there are 5 public parks and recreational fields. The Elliott Bay Marina and Interbay Golf Center are also within a half mile of the site.

Views

Due to the topography, the site has exceptional views to Queen Anne, Downtown Seattle, and Elliott Bay.

Solar Access

The site is generally oriented along a north/south access but due to the openness of the site there is good eastern exposure and opportunities for southern and northern exposures.

Wind Access

Due to the elevation and location along the edge of Interbay valley the project site has the potential to capitalize on moderate wind flowing from the east-northeast during the winter and from the south-southwest during the summer.

Vegetation

Below the project site, the hillside extending down to the North Bay Industrial Lands is heavily vegetated with large deciduous trees and shrubs from W. Hallady Street to Elliott Bay Marina.

Graphic Legend



View to Queen Anne and Downtown Seattle



Local Site Conditions

Please describe neighboring development and uses, including adjacent zoning, physical features, existing architectural and siting patterns, views, community landmarks, etc.

The eastern part of the Magnolia neighborhood is characterized by a mix of industrial and residential development that has been created over the past 100 years. This mix of scale and use has created a rich and diverse context for the project site.

Adjacent Zoning

The neighborhood and greater vicinity has a mix of Land Use Zones that co-exist together. The project site is located in a multi-family zone that is situated between single family and industrial use zones. For more information on zoning boundaries see Zoning Code Summary and ECA Exemption sheet 5.

Neighborhood Context: Land Uses

Due to the adjacency of multiple zones the land surrounding the site has varied uses and unique character created by the contrasting developments. To the east of the project site is the large scale industrial character of Pier 91, North Bay, and the Magnolia Bridge and to the west is the small scale lots and structures of the single family neighborhoods. Directly adjacent to the project site, within the multi-family zone, there are both single family and multi-family structures which contribute to the varied scale of the the streetscape and pedestrian environment of the neighborhood.

Neighborhood Context: Architecture

The architectural context and character of the neighborhood is a result of the diversity of uses and the time periods in which the development has occurred. The industrial areas have been in use since the late 1800's and the existing architecture can be traced back to development by the Great Northern Railroad, U.S. Navy, and the Port of Seattle. The residential development has also occurred over time and within the adjacent neighborhood there have been examples of early homesteads, military housing, suburban housing, apartment, and condominium structures.

Neighborhood Context: Topography and Views

The context of the neighborhood has also been shaped by the physical attributes of the the area. The steep rise in topography above Interbay has provided a natural buffer between industrial and residential uses, allowing the two to co-existing in close proximity. The topographical condition has also afforded the neighborhood with panoramic views of the surrounding neighborhoods, the sound, port, and downtown.

Community Landmarks:

The dominant community landmarks, of the eastern part of the Magnolia neighborhood, are the port and pier operations, the railroad, and the Magnolia Bridge. These landmarks connect the community to the rich industrial history of Seattle and the region.

1. Predominantly Multi-Family Housing
2. Urban Center
3. Predominantly Single Family Housing
4. Interbay Athletic Complex
5. BNSF (Burlington Northern Santa Fe) Railroad
6. Port of Seattle North Bay
7. Magnolia Bridge (W. Garfield Street Viaduct)
* Proposed reconstruction 2009-2011
8. Pier 91
* Current renovation project for development of Cruise Ship Terminal completion date 2009
9. Elliott Bay Marina



Greater Vicinity Context



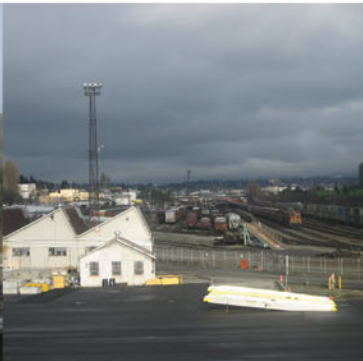
Pier 91



Elliot Bay Marina



Magnolia Bridge



BNSF Railroad

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Examples of Multi-Family Developments along West Newton Street



Neighborhood Aerial Photograph

● Multi-Family Development



Please describe the existing site, including location, existing uses and /or structures, topographical or other physical features, etc.

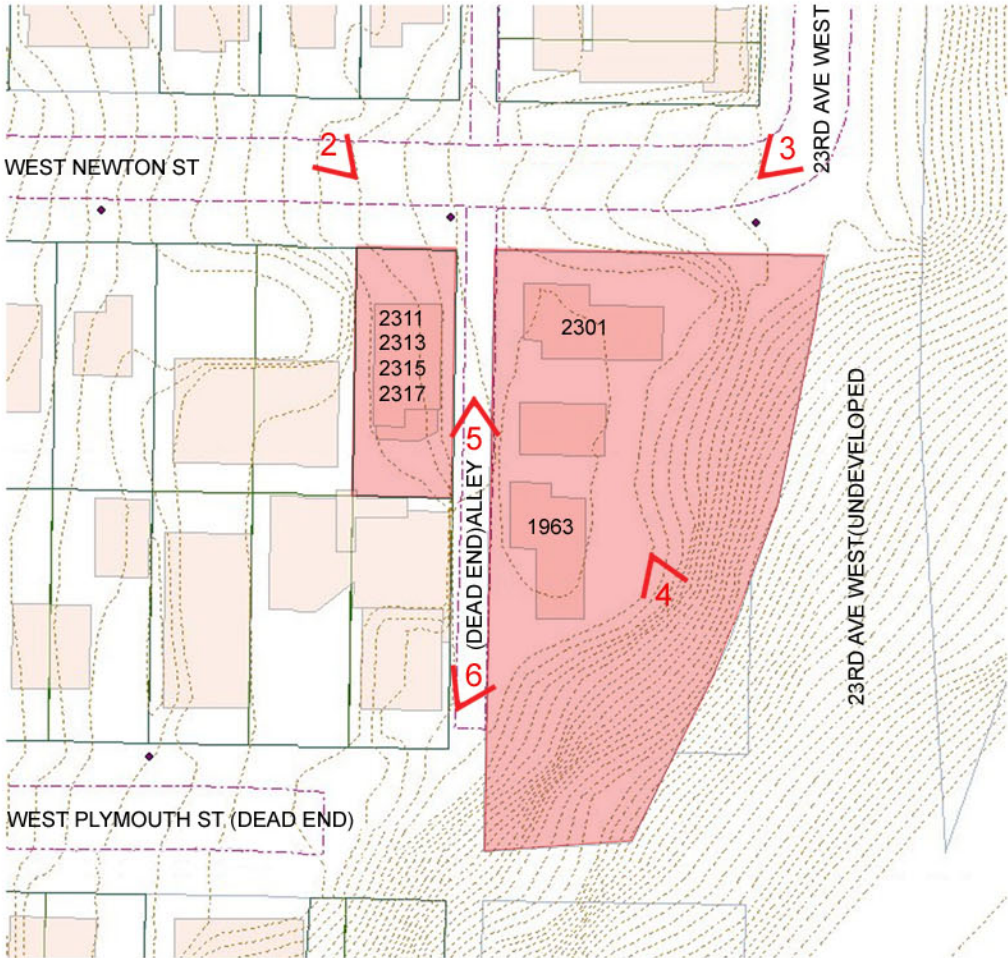
Existing Conditions

Location.
The site is located at the intersection of West Newton Street and 23rd Avenue West and includes two parcels that are separated by an alley. The western parcel is a total of 4,000 square feet and the eastern parcel is approximately 24,000 square feet. The irregular geometry of the eastern parcel includes vacated portions of 23rd Avenue West and West Plymouth Street right-of-ways.

Uses.
Both parcels of land included in the project site are currently being used for residential purposes in keeping with the zoning regulations. The western most parcel is occupied by a tri-plex apartment building and the eastern parcel has 2 detached residential structures and 1 accessory garage. The southern most portion of the eastern parcel is zoned Industrial Buffer and that area is currently undeveloped.

Topography.
The site is located at the eastern edge of the Magnolia bluff and overlooks the Port of Seattle North Bay Industrial Lands. The topography of the project site is characterized by a large flat area between a rocky edge on the western side of the alley and a steep hillside, to the east, with a crest that runs diagonally across the site from northeast to southwest.

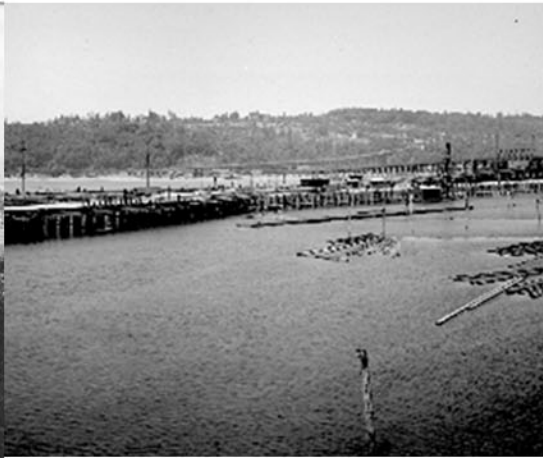
Access.
The residential structures on the site are currently accessed form West Newton Street and the alley connecting to West Newton Street. The alley currently dead ends before reaching and connecting to West Plymouth Street. The dead end area of the alley has been "privatized" by the residents of the adjacent apartment building for their personal parking and yard space. The privatization of the public alley has limited the ability for pedestrian connection between West Newton and West Plymouth and for connection to the neighborhood as a whole.



SITE PLAN



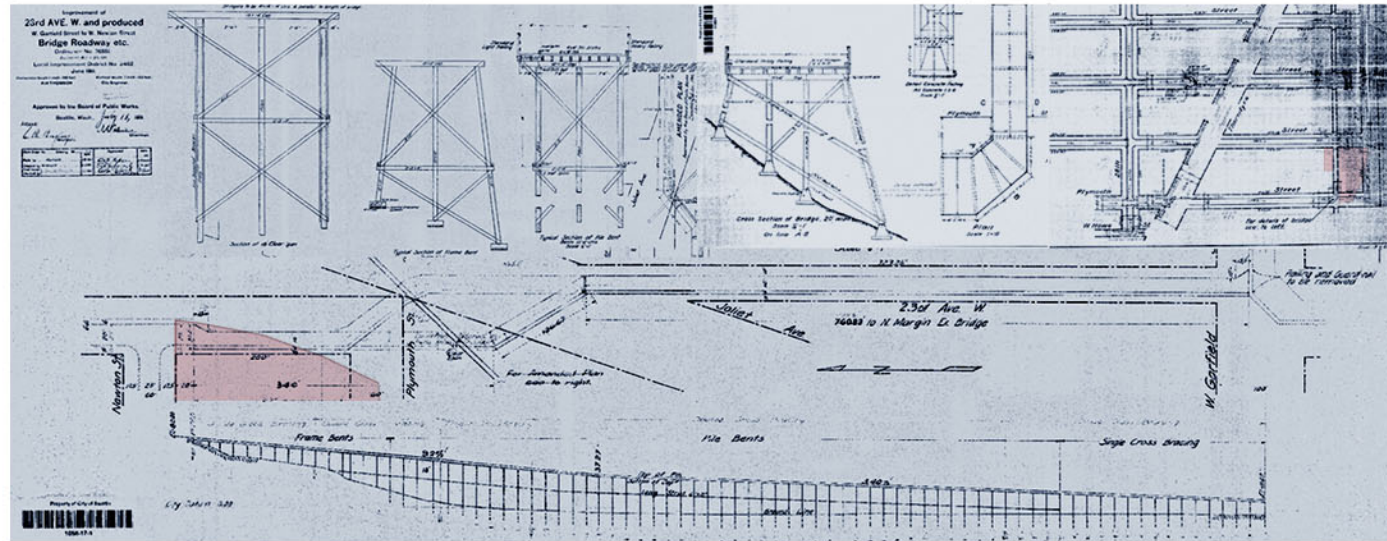
VIEW FROM SITE 1914



INTERBAY 1914



INTERBAY 1934



1911 23RD AVENUE AND WEST PLYMOUTH STREET TRESTLE ROADWAY STRUCTURES



US NAVAL DEPOT 1947



WATERMAIN WORK BELOW TRESTLE BRIDGE 1939



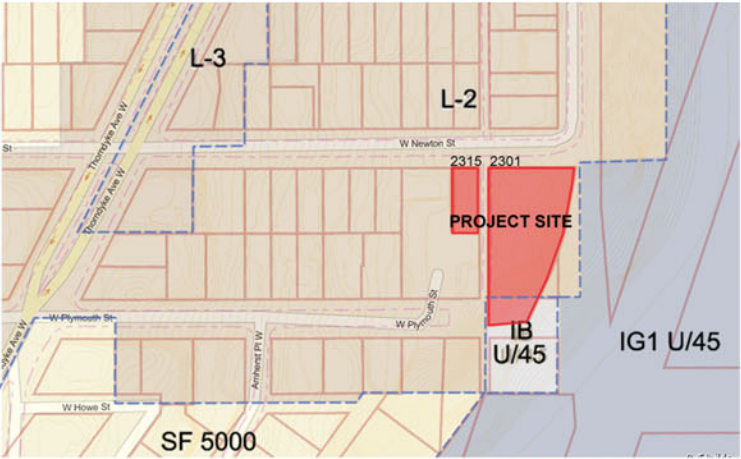
SITE WITH TRESTLE BRIDGE 1936



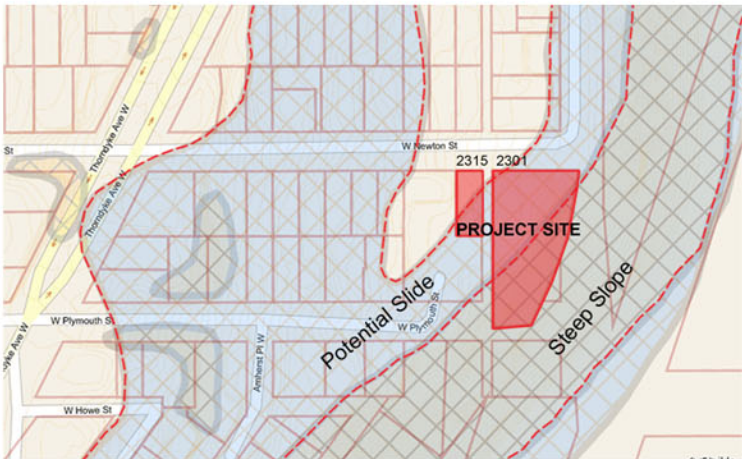
SITE WITH US NAVAL OFFICERS QUARTERS 1946

Please indicate the site's zoning and any other overlay designations, including applicable Neighborhood-Specific Guidelines.

Site Location	2301 and 2315 West Newton Street 24,179 sf and 4,000sf, 28,179sf total	Lot Coverage	50% for townhouses in L2 zone
Site Zoning	L2 and IB U/45	L2 23.45.010	
SEPA Review CAM 208	Required for construction of 6 or more units in Lowrise 2 zone	IB 23.50.028	FAR of 2.5 at IB zone. Exemption provided for area below grade, accessory parking, and mechanical equipment.
Design Review CAM 238	Required for new multi-family developments that exceed SEPA threshold in Lowrise 2 areas	Structural Width and Depth L2 23.45.011	Table 23.45.011A. 90' structural width allowed with modulation. Structural Depth, 65% of lot depth allowed.
Environmentally Critical Areas	44% of eastern parcel is in the 40% Steep Slope zone- L2 portion of eastern parcel has been exempted from Steep Slope Development Standard Requirements. 100% of eastern parcel is in the Potential Slide Zone 50% of western parcel is in the Potential Slide Zone	Modulation L2 23.45.012	Front facades over 30 feet wide shall be modulated 4 feet deep and modulation shall be 5 feet wide minimally and 30 feet wide maximum.
Permitted Uses L2 23.45.004	Multi-family Structures permitted outright in Lowrise 2 Zone	Setbacks L2 23.45.014	Front , determined by the average of adjacent setbacks or 5 feet minimum, 15 foot maximum required. Rear , 20% of lot depth or 15 foot minimum, 25 foot maximum required. Side , table 23.45.014A structures 25 feet high and 75 feet deep- side to be a minimum of 5 feet with an average fo 6 feet. For structures 25 to 30 feet high and 189 feet deep- side is to be a minimum of 8 feet and with an average of 19 to 21 feet.
IB 23.50.012	23.50.012 Chart A. All permitted uses allowed as either a principal or as a accessory use, unless otherwise indicated.	IB 23.50.030	A 5 foot setback shall be required from all street lot lines which are across a street right-of-way 80 feet or less in width from a lot in a SF, L1, L2 or L3 zone.
Density L2 23.45.008	1,200sf per unit permitted in L2 zone	Landscaping L2 23.45.015	A1. Minimum area equivalent to the square footage of 3 times the total length of all property lines. A4. Street trees required in the planting strip in accordance with SDOT Tree Planting Standards.
Height L2 23.45.009	25 feet allowed in L2 zone	Open Space L2 23.45.016	Average of 300sf per unit required 200sf minimum per unit required.
IB 23.50.024	45 foot height limit in IB U/45	Parking and Access L2 23.45.018	Alley access required. Access to parking shall be from the alley when the site abuts a platted alley.
Height: Rooftop Features L2 23.45.009	Open railings, planters, skylights, clere-stories, greenhouses, parapets and fire-walls may extend 4 feet above the max height limit. Stair and elevator penthouse may extend 10 feet above max height limit.		
IB 23.50.020	Structures with no story of at least fifteen foot in height shall be limited to a maximum height of 40 ft.		Chart B 23.54.015 Parking for Residential Uses 11-30 units, exceeding 500sf, 6 3bd units and 7 2bd



ZONING MAP



ECA OVERLAY

City of Seattle
Gregory J. Nickels, Mayor
Department of Planning and Development
Diane M. Sugimura, Director
April 24, 2008

Mr. Brian E. Malady, Architect
E. Cobb Architects Inc.
911 Western Avenue, #318
Seattle, WA 98104

Subject: Addendum to ECA Exemption Request
2301 West Newton Street
DPD Project No. 6163271

Subsequent to our April 16 meeting and additional documentations provided by your office on April 17, 2008, the Department of Planning and Development's (DPD) Geotechnical Section had performed an addendum review of your Environmentally Critical Areas (ECA) Exemption Request dated March 21, 2008. (Attachment A) The following summarizes DPD's review comments for this exemption request and should supersede the previous comments, Attachment A, for this project site.

- Additional documentations provided on April 17, 2008 included a 1935 topographic survey of the property and adjacent properties; and, an updated soil report by Geotechnical Consultants, Inc. dated April 17, 2008.
- Based on the submitted 1935 topography, the existing property (Lots 20 thru 27, inclusively) does not appear to contain any ECA 'steep slope' areas as defined in 25.09.020. In this respect, the 'steep slope areas' at these lots are inferred to have been created by previous legal grading and/or construction activities at the aforementioned lots. Further, the submitted updated report by Geotech Consultants, Inc. had implied that granting this exemption will not result in adverse impacts at this site and adjacent properties. In this respect, the ECA Steep Slope Development Standards (i.e. requirements for a Steep Slope Area Variance) are waived for the development associated with DPD Application No. 6163271 at Lots 20 thru 27, inclusively. All other ECA Submittal, General, and Landslide-Hazard, and development standards will apply for this project. **The subject site is also designated as a Potential Landslide Area due to Geologic Conditions and all future permit applications will require ECA Reviews.**

Further, based on the submitted Site Plan, the proposed development appeared to be located in close proximity to and/or within critical root zone of existing trees located at the subject site. The project arborists shall evaluate and provide mitigations to ensure minimal adverse impacts of the proposed development on the existing trees.

City of Seattle, Department of Planning and Development
700 Fifth Avenue, Suite 2000
P.O. Box 34019
Seattle, WA 98124-4019

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- Based on the submitted 1911 Street Profile for West Plymouth Street, 'steep slope' areas appears to be in existence south of Lot 27 prior to any development in and adjacent to this public right of way. Thus, the request under ECA 25.09.180.B.2.b, 'development is located on steep slope areas that have been created through previous legal grading activities, including rockeries or retaining walls resulting from rights of way improvements, if no adverse impact on the steep slope will result' may not be granted south of Lot 27 within the West Plymouth Street right of way. Please see Attachment B to note the 'steep slope areas' that are not exempted.

Please provide this document with the plans during Plan Intake Appointment. The aforementioned comments are only applicable for this site and the proposed development associated with DPD Project No. 6163271 as outlined in the submitted information.

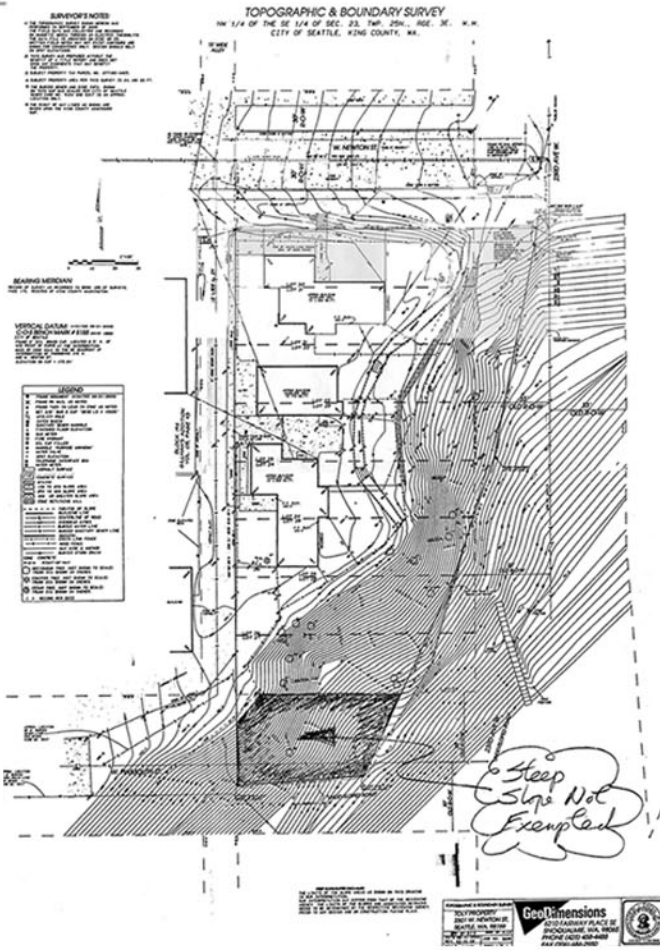
If you have any question, I may be reached at (206) 684-5979.

Sincerely,

Shao-Ho (William) Bou, P.E.
Geotechnical Engineer

- Attachments:
- Exemption Requests & Modifications to Submittal Requirement decision dated April 11, 2008.
 - Sketch of Steep Slope Areas not exempted.

cc: Ms. Susan Chang, Geotechnical Supervisor (DPD)

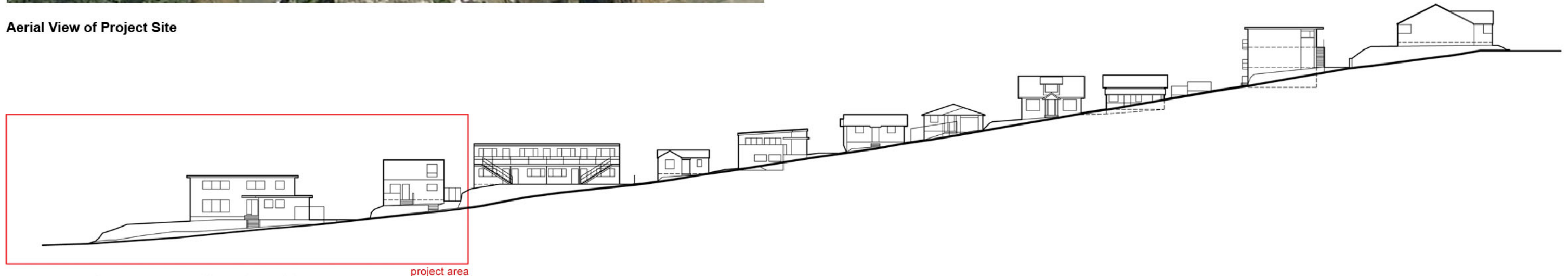


ECA EXEMPTION

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Aerial View of Project Site



West Newton Street Elevation - South Side of Street

Please describe the proponent's objectives, indicating types of desired use, structural height (approx), number of residential units (approx), amount of commercial square footage (approx), and number of parking stalls (approx). Please also include potential requests for departure from development standards.

Objectives

The primary objective of the project is to use the unique aspects of the site to create a strong housing design that will incorporate an interactive urban experience, inspired quality of life, material integrity, and sustainability in construction.

Urban Experience

- promote pedestrian movement, re-establish pedestrian connections, and create an active streetscape
- provide a communal space to promote activity, interaction, and safety
- minimize vehicular impact and create individual entrances and living spaces at street level for each unit

Quality of Life

- opportunity and access to unique views, light, and air for each unit
- provide a wide range of housing types to encourage diversity

Material Integrity

- primary structural materials selected for lasting quality
- finish materials selected for low maintenance and enduring quality

Sustainability

- built green
- alternative energy sources
- minimizing intrusion into the steep slope area

Approximate Structural Size

The proposed project is developing 2 parcels of land with a combined area of 28,179sf with 12 townhouses. The total lot coverage is to be 11,620sf at the larger parcel and 2,000sf at the smaller parcel. It is anticipated that there will be 6 three story units and 6 two story units. The overall lot coverage meets the 50% allowed but, due to the location of the the slope and irregularity of the site, it is anticipated that departures will be necessary for structural width and depth and for yard setbacks. These departures will allow for the creation of the courtyard and development of the streetscape along West Newton Street.

Residential Use

The proposed project is currently anticipating 9 residential units to be developed on the larger parcel and 3 residential units to be developed on the smaller parcel. The anticipated total of 12 residential units is far below the 21 units which would be allowed by the Land Use Code. A pedestrian walk connecting the courtyard and West Plymouth Street with bench seating is currently purposed for the West Plymouth Street right-of-way.

Access and Parking

In order to encourage pedestrian movement and to develop the streetscape along West Newton Street in a positive and active manner the proposed development consolidates the parking requirements for the east units in a below grade garage that would be accessed from West Newton Street. The single curb cut would be located below and away from the entry stoops and courtyard access along the street. This strategy works with the existing topography of the site, allowing the project to minimize the intrusion into the steep slope area. There are currently 17 parking spaces planned in the below grade parking garage. Recycling, garbage, yard waste collection and bicycle storage is combined and located off of the courtyard. In addition to the work on West Newton Street the proposed development plans to re-establish the public pedestrian connection between West Newton Street and West Plymouth Street by continuing the public sidewalk and landscaping along West Plymouth to the alley.

Design Guidelines and Preferred Scheme Response

- A. Site Planning**
 - A-1. Responding to Site Characteristics**

The siting of buildings should respond to specific site conditions and opportunities such as non-rectangular lots, location on prominent intersections, unusual topography, significant vegetation and views or other natural features.

 - **Topography.** Massing is articulated to follow the contours and elevation of the site topography.
 - **Environmental Constraints.** The massing has been configured to lessen the impact on the steep slope.
 - **Solar Orientation.** The massing has been oriented to the south east to enhance solar exposure. The open space increases the opportunity for solar exposure to all units by allowing access to light from multiple sides.
 - **Views.** The units have been oriented to enhance the opportunity for views to the south east. The overall structural massing has been articulated to provide the opportunity for public views from the courtyard.
 - **Edges.** The scheme looks to take advantage of the different conditions along the edges and corners of the site to create unique experiential opportunities in both public and private space.
 - **Articulation.** The massing is articulated into four separate structures. The resulting massing creates an overall scale that is in keeping with the adjacent neighborhood buildings.
 - A-4. Human Activity**

New Development should be sited and designed to encourage human activity on the street.

 - **Entrances and Living Space.** The scheme creates individual unit entrances and living spaces on the street and courtyard level.
 - **Pedestrian Connection.** The scheme re-establishes a public pedestrian connection between West Plymouth Street and West Newton Street.
 - **Courtyard.** The scheme creates a pedestrian oriented open space to encourage interaction, community, security, as well as a communal recycling and refuse management system (possibly run by CleanScapes).
 - **Streetscape.** The scheme develops West Newton Street with four entry stoops that will bring landscaping, activity, and human scaled architectural elements to enhance the street.
 - A-8. Parking and Vehicle Access**

Siting should minimize the impact of automobile parking and driveways on the pedestrian environment, adjacent properties, and pedestrian safety.

 - **Consolidated Parking.** The parking requirement for all units has been consolidated to one below grade parking structure.
 - **Driveway and Curb Cuts.** The consolidated parking limits the number of curb cuts. The proposed location for the new curb cut is in the location of an abandoned and dilapidated curb cut.
 - **Shared Driveway.** The proposed scheme combines the parking requirements of the adjacent parcel across the alley, using one curb cut and one driveway.
 - **Parking Location.** The parking has been located below grade to be less visible and by doing so, allows for the creation of a pedestrian oriented courtyard.
 - **Driveway Location.** The driveway is located at the eastern edge of the property, below and away from unit entrances and the entrance to the pedestrian courtyard. The location and configuration of the driveway works with the slope of the existing topography.
 - C. Architectural Elements and Materials**
 - C-2. Architectural Concept and Consistency**

Building design elements, details, and massing should create a well-proportioned and unified building form and exhibit and overall architectural concept. Buildings should exhibit form and features identifying the functions within the building.

 - **Architectural Concept of Wall and Infill.**
 - The clear expression of party walls, individual unit entries, consistent intervals, articulated massing, and frontality of units at the street work together to enhance the recognition of each unit as a single dwelling.
 - The consistent expression of party walls and infill allows for variation in site geometry, unit size, and streetscape while maintaining a unifying concept in material expression and detailing for the entire project.
 - The modulation and articulation allowed by the wall and infill concept creates opportunities for units to respond to unique aspects of the site while still maintaining a consistency in material expression and detailing.
 - By creating a clear scheme that is rooted in the structural and spatial reality of the project, the materials of the project are allowed to clearly express their true nature and function.
 - Inside/outside. The use of materials that can be durable and beautiful on the inside and outside allows for the extension of interior space to the outside; creating a rich layering of space at the edges of the units.
 - **Architectural Features.**
 - Party Walls.** The clear delineation of individual units is created by interior concrete party walls that are left exposed and can be seen selectively on the exterior of the units at the courtyard and in the aperture spaces.
 - Infill Walls.** The lighter weight infill construction between the party walls allows for variation in the location and size of openings and for an opportunity to create a material pallet that is complementary to the concrete party walls and that is in keeping with the residential context.
 - Unit Entries.** The unit entries provide the opportunity for a high level of detail to be development at canopies, steps, railings, doors and hardware to create a comfortable human scale and character of high quality.

- C-4. Exterior Finish Materials**

Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, and lend themselves to a high quality of detailing are encouraged.

The project's material selections and detailing are designed to exceed industry standards for multi-family development and have been selected and expressed because of their inherent functional traits and beauty.

 - **Lasting.** Interior concrete party walls provide separation and structural strength while expressing a sense of quality and permanence. By selecting materials which have an integral finish and that can be left exposed without additional layers of finish materials the initial beauty of the project will remain overtime without the requirement of constant maintenance.
 - **Durability.** The materials selected for the project are chosen not only for their strength and efficiency but also for their ability to withstand daily use, weathering, and to remain beautiful as they patina over time.
 - **Quality.** All materials chosen for the project will be selected and detailed in a manner that will express a character of high quality.
 - **Green.** By selecting materials that are lasting, durable, and require little maintenance the project will contribute to the creation of a sustainable future in which the life cycle costs of the built environment are greatly reduced.
- D. Pedestrian Environment**
 - D-1. Pedestrian Open Spaces and Entrances**
 - Convenient and attractive access to the building's entry should be provided. To ensure comfort and security, paths and entry areas should be sufficiently lighted and entry areas should be protected from the weather. Opportunities for creating lively, pedestrian oriented open space should be considered.**
 - **Courtyard.** Pedestrian oriented courtyard that incorporates unit entrances, communal space and is part of the re-established pedestrian route between West Plymouth Street and West Newton Street.
 - **Unit Entrances.** Entrances for individual units are located along the street edges and courtyard edges providing opportunities for activity, security, interaction and shelter.
 - **Paving.** The preferred scheme proposes pavers as the walking surface for the alley and courtyard.
 - **Human Scale.** Unit entrances provide opportunity for human scaled elements at the edges of the open space; steps, canopies, railings, etc..
 - **Landscaping.** Landscaping will be used at the edges of pedestrian routes, creating layers of space which will help define pedestrian routes while providing privacy for residential units.



View of Potential Streetscape at West Newton Street

Design Review Board Comments and Team Responses

- 1

West Newtons Streetscape Development
The Board is willing to consider the front setback departures depending on how well the units facing he right-of-way relate to the street. They look forward to seeing detailed drawings (including stoops, terraces, and landscaping) of how the units enhance the right-of-way and the public realm.

The Board is looking forward to a detailed landscape design that will soften the hard appearance of the concrete surfaces and present an active facade to West Newton.

- **Setback and Modulation.** Two thirds of the West Newton Street Elevation has been stepped back an additional three feet.
- **Stoops and Terraces.** The unit entrances along West Newton have been developed with generous stoops and terraces to create opportunities for personalization and structured buffering between public and private realms.
- **Materials.** The stoops, unit entrances and unit facades have been developed with expressed precast concrete, natural finish wood siding, clear anodized aluminum window systems, canopies, and screening.
- **Landscaping.** The landscaping along the West Newton Street has been developed with layers of native/ drought resistant paint-ings which include street trees, grasses and shrubs.
- 2

Development of the Courtyard
The Board questioned the location of the elevator tower shown in the center of the proposed courtyard. They feel that it creates a monolithic structure, breaks up the courtyard and reduces the usability of the space. They would like to see the elevator structure relocated to the side of the courtyard.

A landscape plan with plantings called out should be developed that will soften the effects of the hard concrete edges.

- **Elevator Location and Courtyard Configuration.** The elevator has been reposition to align with the edge of the courtyard and the sides of the elevator have been developed as an extension of the series of walls that create the townhouse units and defined the edges of the courtyard.
- **Landscaping.** Landscaping has been developed and includes a series of large trees that are positioned at the edges of the the courtyard and alley. These large trees provide a canopy for shading and the upper branches provide screening for windows on the upper levels of the units around the courtyard. The landscaping of the courtyard and alley has also been developed with concrete pavers, concrete scoring patterns, bar grating screens with vine planting, and under story grasses.
- 3

Revisions to Southwest Corner of Proposed Development
The Board was particularly concerned with optimizing views of the adjacent neighbors and asked that the acute angle on the south-ernmost unit be chamfered back approximately 8 feet, such that the southwest wall is parallel to its opposite wall.

- **Revisions to Southwest Corner.** The Massing of the southernmost unit has been revised to minimize the impact on he adjacent neighbor’s view to the southeast. The southern most end has been chambered back 9 feet from it’s previous location.
- 4

Pedestrian Connection to West Plymouth Street
The Board likes the proposed design for the extended pedestrian path continuing the alley with West Plymouth and this should include a bench for respite and viewing.

- **Pedestrian Connections and Bench.** The pedestrian connection from the alley to West Plymouth street has been developed with side walk, landscaping and a bench.

- 5

Sustainability
DPD would like the applicant to consider rain gardens and other sustainable features incorporated in to the courtyard landscape design.

- **Sustainability.** The architectural design has been lead by large scale, effective sustainable ideals that have informed the selection of the structural systems, climate control systems, and finish materials. For the most basic structural elements of the project, unit party walls, the proposed development utilizes insulated precast concrete panels. These walls create an efficient, modular structural system that minimizes production waste, minimizes construction time, and creates a durable, integral, thermally efficient building envelope. For climate control the proposed development will use radiant floor heating, thermal mass in the walls and floors to mod-erate temperature swings, and passive cooling and ventilation. All of the materials selected on the project have been chosen for their durability and quality and local materials have been utilized when ever possible.

The incorporation of holistic sustainable features extends to the landscape design which has been developed with innovative plant selection of native, draught resistant plants to create a sustainable landscape that works with the climate and surrounding vegeta-tion.

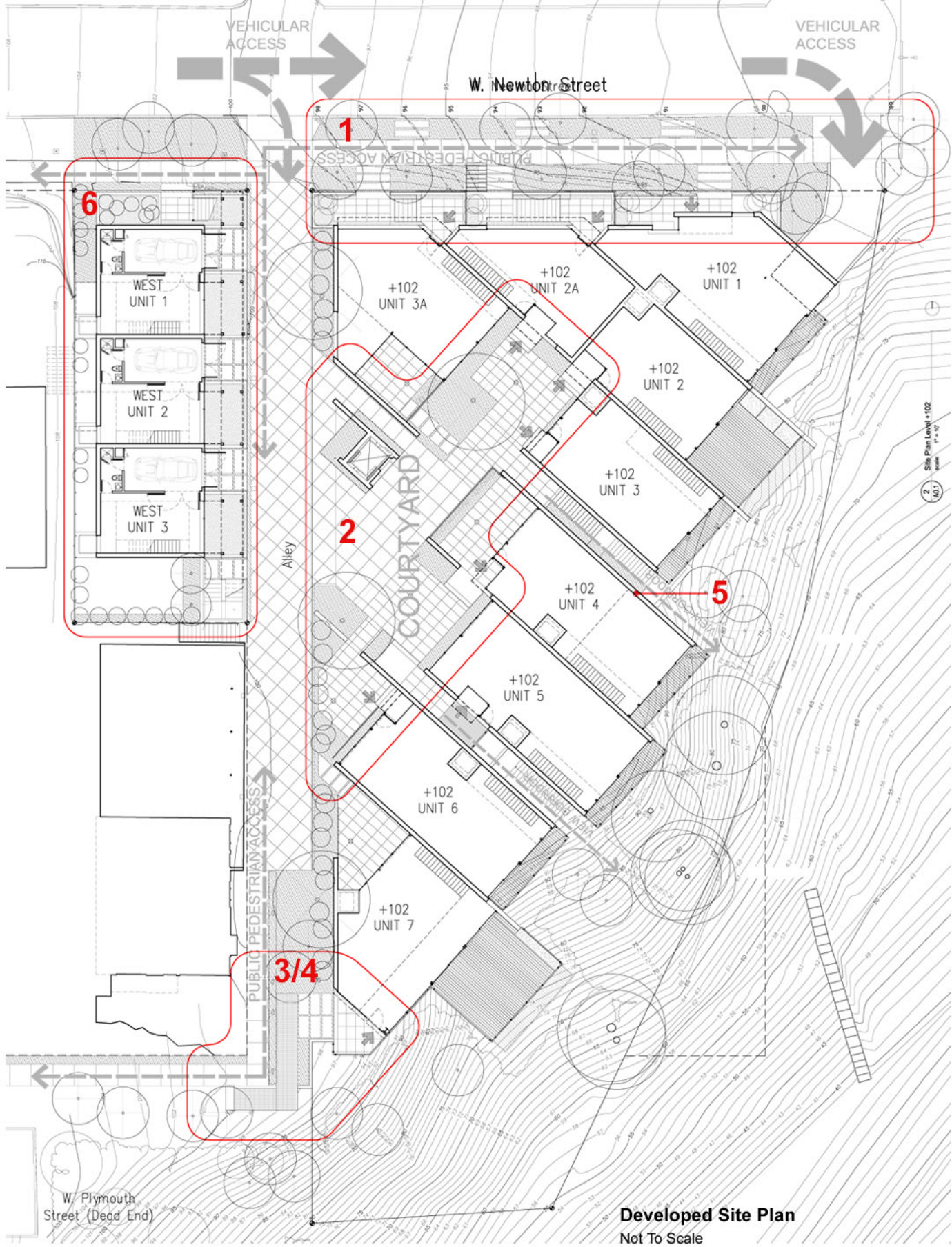
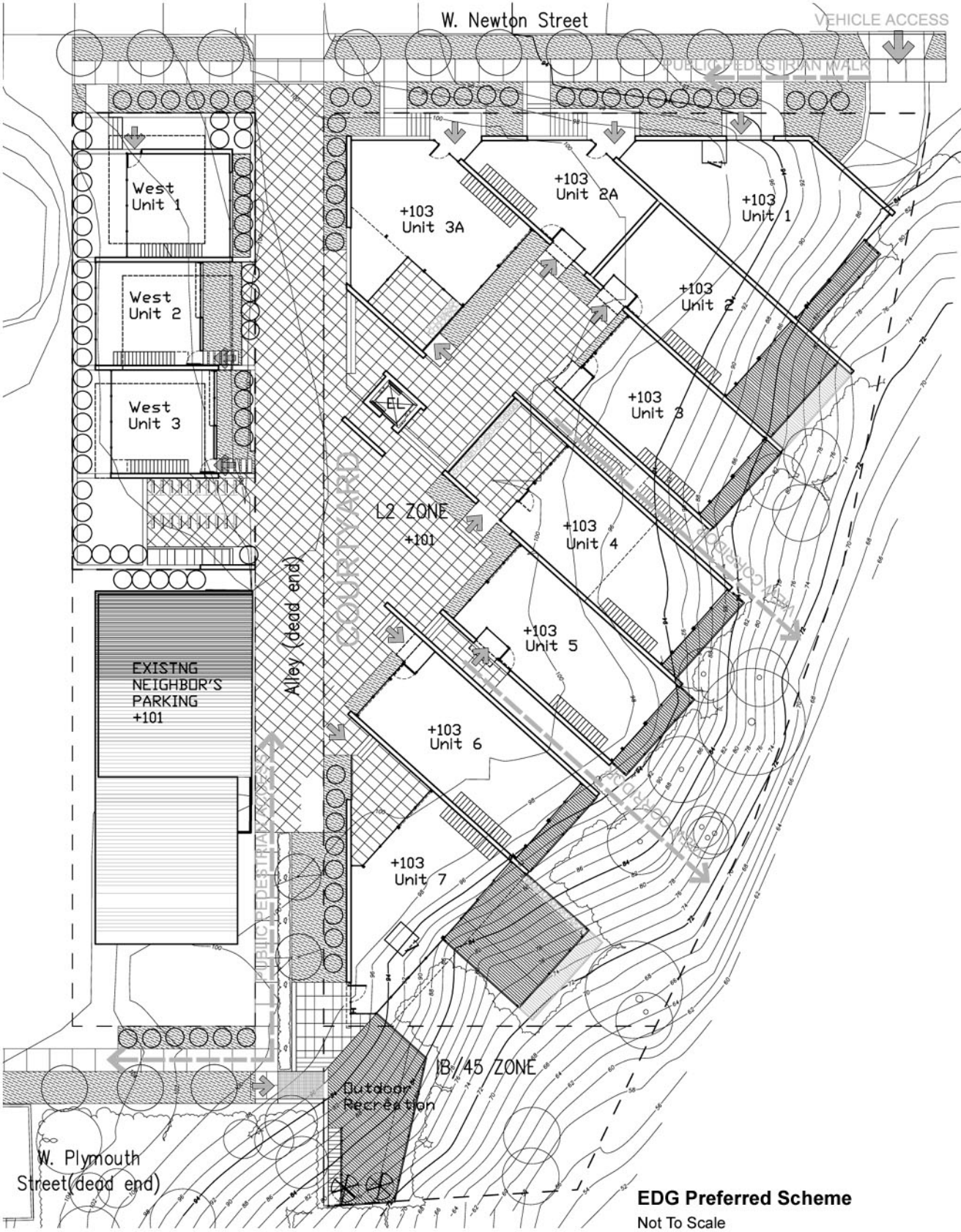
It is also planned that the proposed development will participate in the King County Built Green program and will incorporate addi-tional features and construction practices that promote energy efficiency, indoor air quality, conservation of materials, and water quality protection.
- 6

Development of Western Parcel
The Board agreed that the below grade parking enhances the public spaces at grade. Though they acknowledged that they have no authority over land use decisions, they wished to state that they supported a variance that would allow parking for the three units at the west of the alley in the below-grade structure.

- Although it was discussed and presented at the meeting and strongly supported by the Design Review Board, further discussions with the Department of Planning and Development has lead the design team away from seeking an off-site parking variance for the units on the western parcel. It was first hoped that the parking requirements for these three units could be consolidated with the parking of the nine units on the eastern parcel in the combined below grade garage. This strategy now appears unfeasible from a land use perspective and has lead the team to reconfigure the proposed units on the western parcel to include individual garages for each unit which would be accessed from the alley.

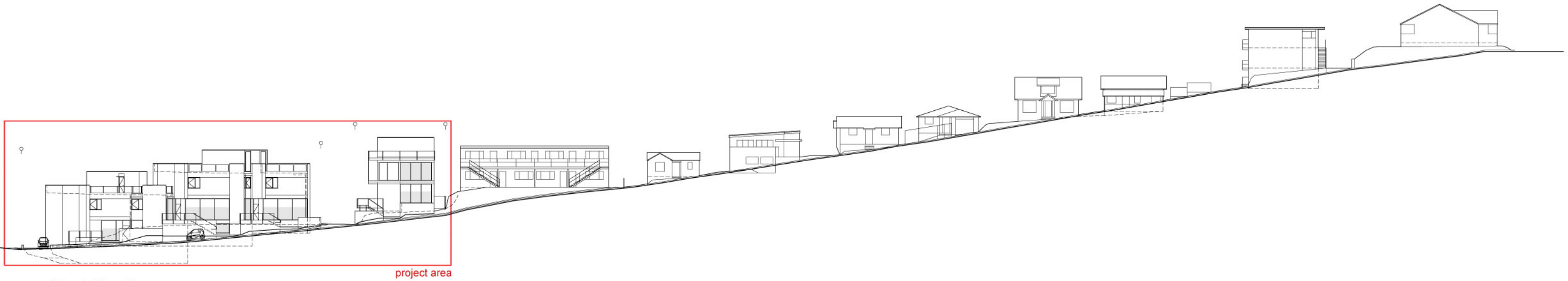
To maintain and work within the allowable height limits of the L2 zone the current proposal for the western units cuts three driveways into the existing high bank along the alley and establishes a pedestrian access bridge above the driveways to connect the unit entrances to the grade of the side walk along West Newton Street. The pedestrian access bridge establishes another level of pedes-trian activity along the alley and courtyard (where the current grade is already elevated) and also serves to minimize the impact of the vehicular access by helping to create a recessed access for the garages.

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Northeast Building Axonometric

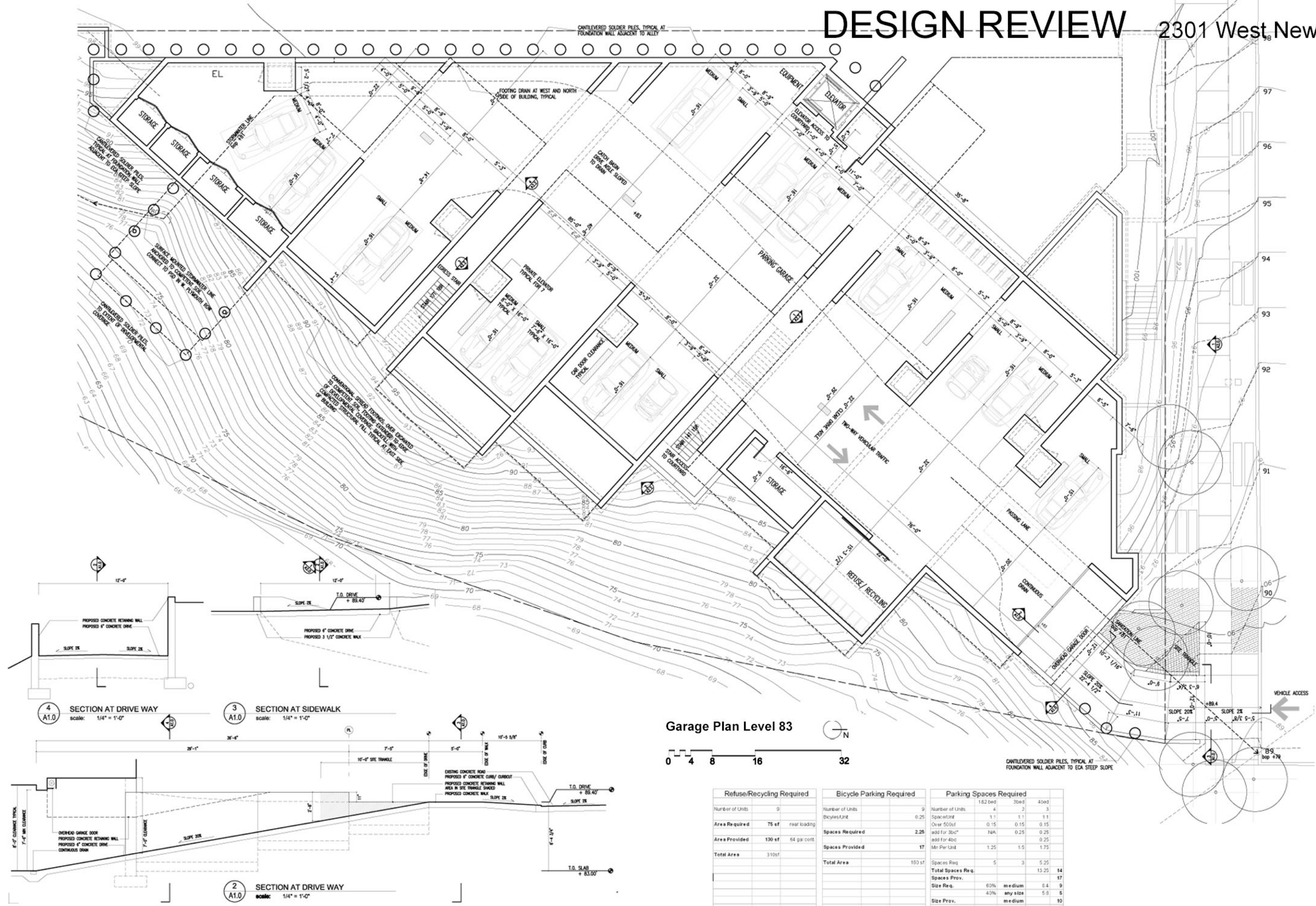


Street Elevation



Perspective View from the Southeast





Garage Plan Level 83

Refuse/Recycling Required		Bicycle Parking Required		Parking Spaces Required			
Number of Units	9	Number of Units	9	Number of Units	182 bed	30 bed	40 bed
Area Required	75 sf rear loading	Bicycles/Unit	0.25	Spaces/Unit	1.1	1.1	1.1
Area Provided	130 sf 64 gal cont	Spaces Required	2.25	Over 500sf	0.15	0.15	0.15
Total Area	310 sf	Spaces Provided	17	add for 300sf	N/A	0.25	0.25
		Total Area	193 sf	add for 400		0.25	0.25
				Min Per Unit	1.25	1.5	1.75
				Spaces Req	5	3	5.25
				Total Spaces Req			13.25
				Spaces Prov			17
				Size Req	60%	medium	8.4
				Size Prov	40%	any size	9
						medium	5.8
						small	10
							7
				Total Area			9315 sf

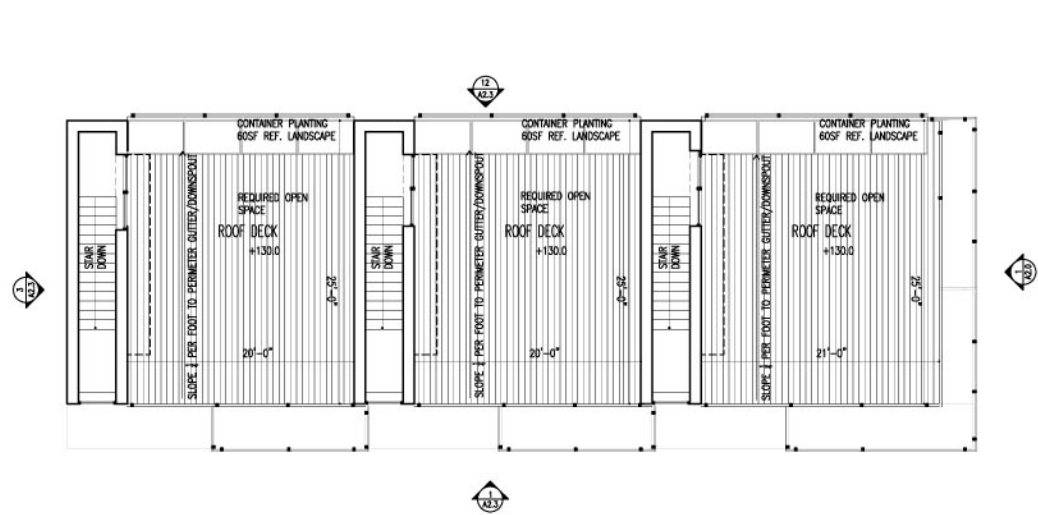




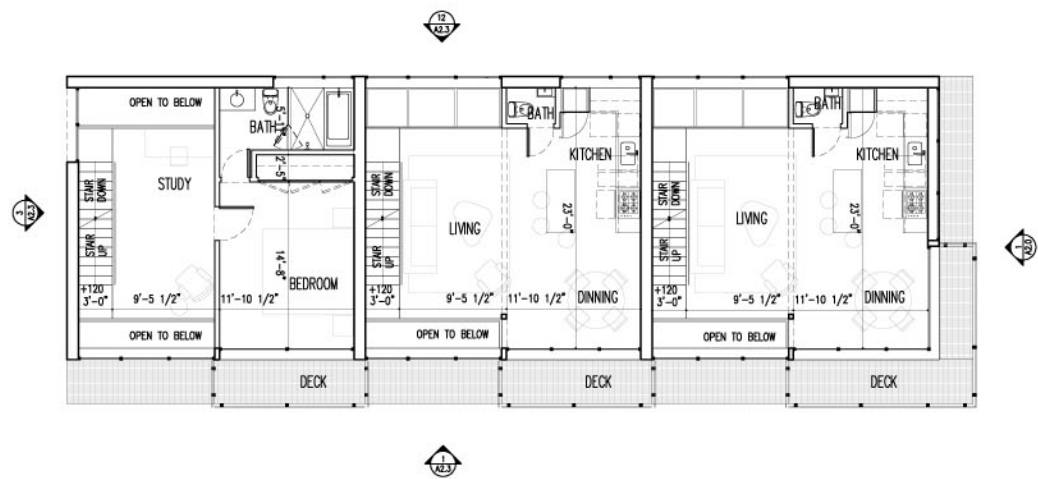
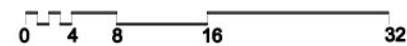
Building Plan Level 102

0 4 8 16 32

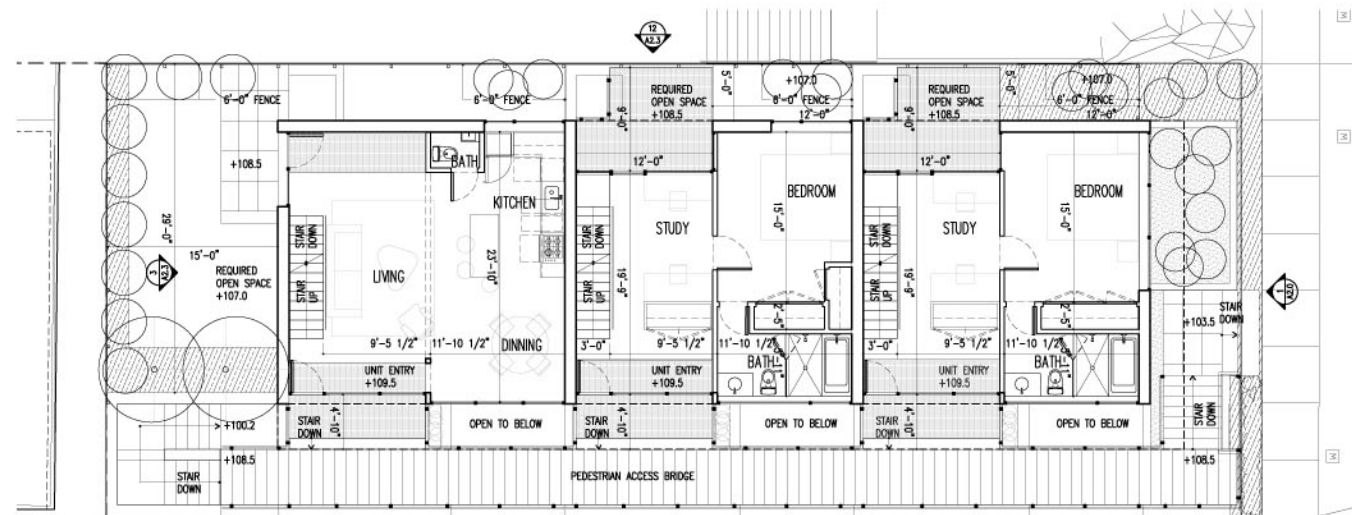
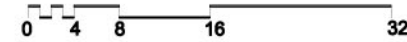




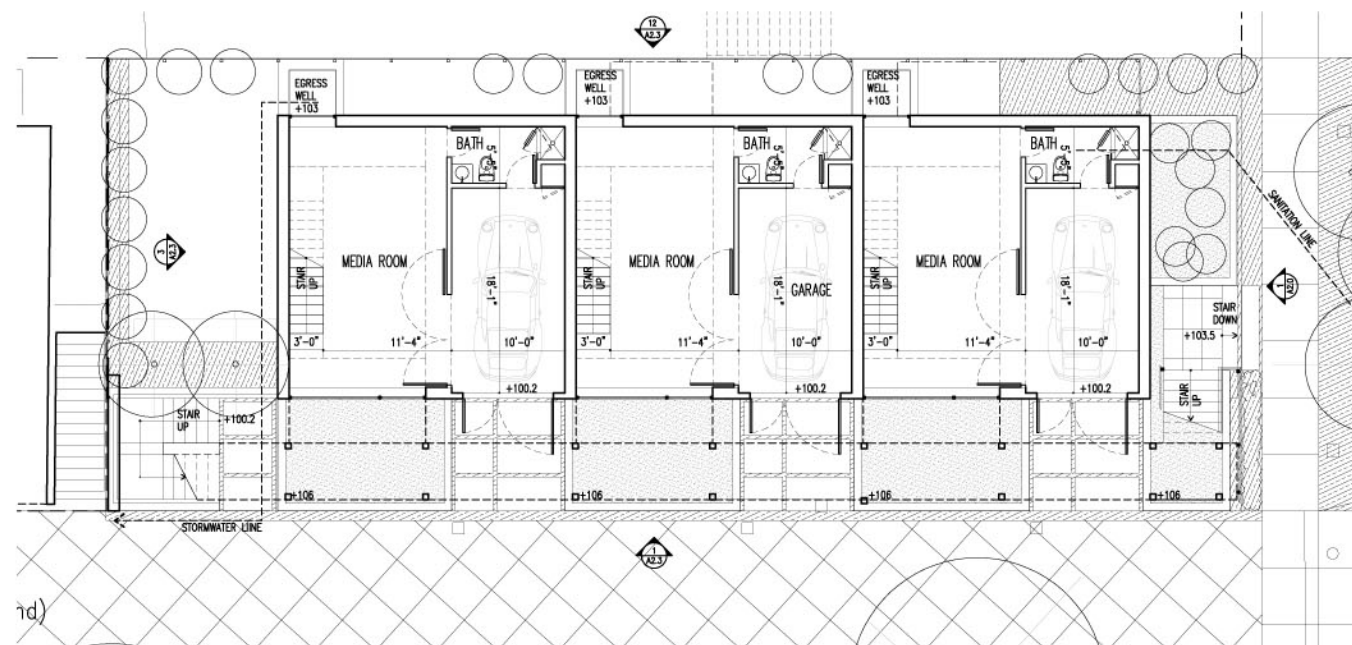
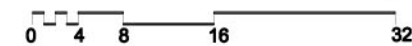
Building Plan Level 130



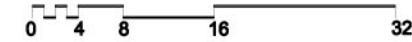
Building Plan Level 120



Building Plan Level 109.5



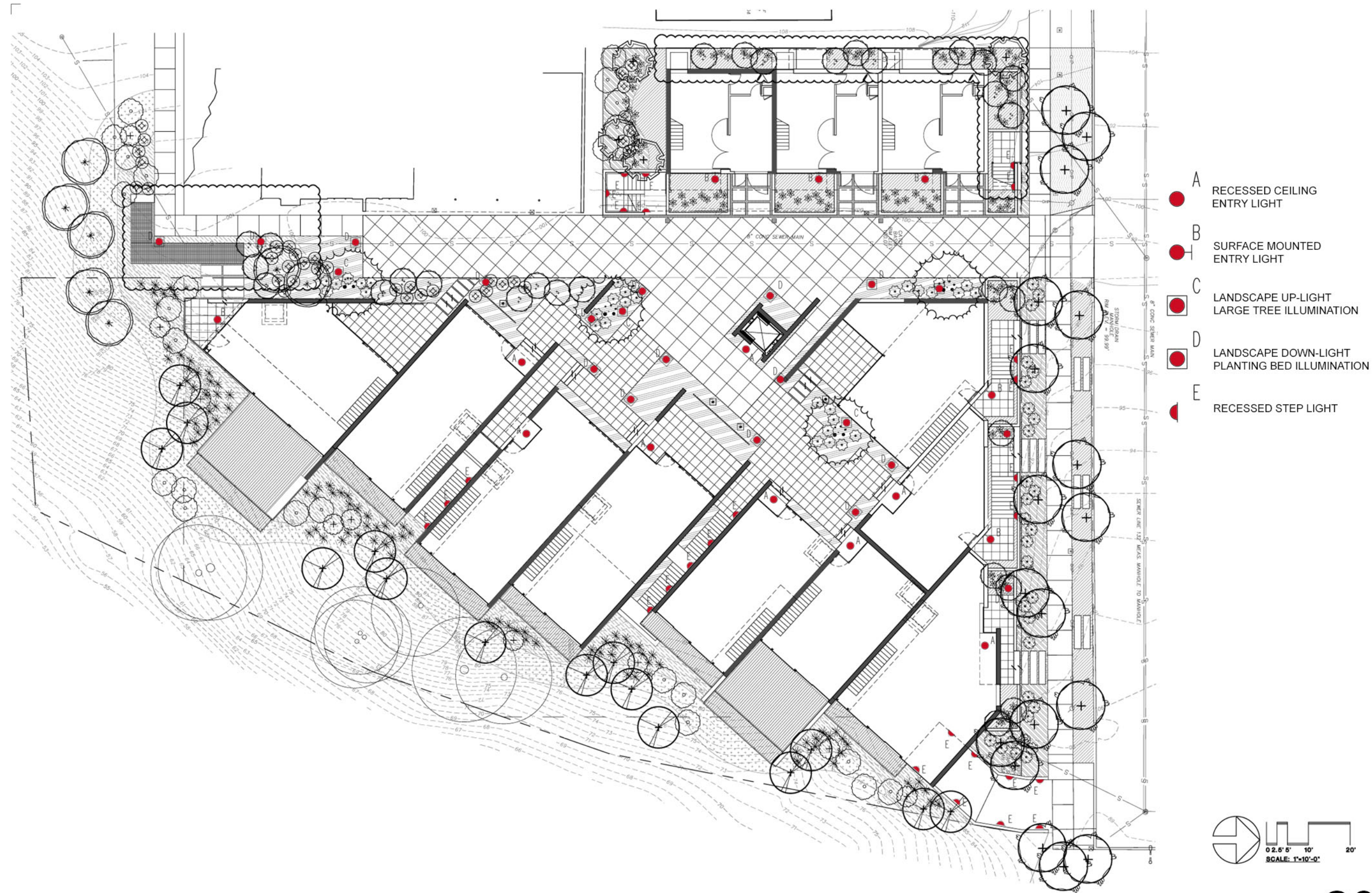
Building Plan Level 100



DESIGN REVIEW

2301 West Newton Street





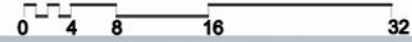


View from Northwest- West Newton Street

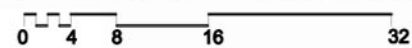
DESIGN REVIEW 2301 West Newton Street



East Elevation



North Elevation- West Newton Street

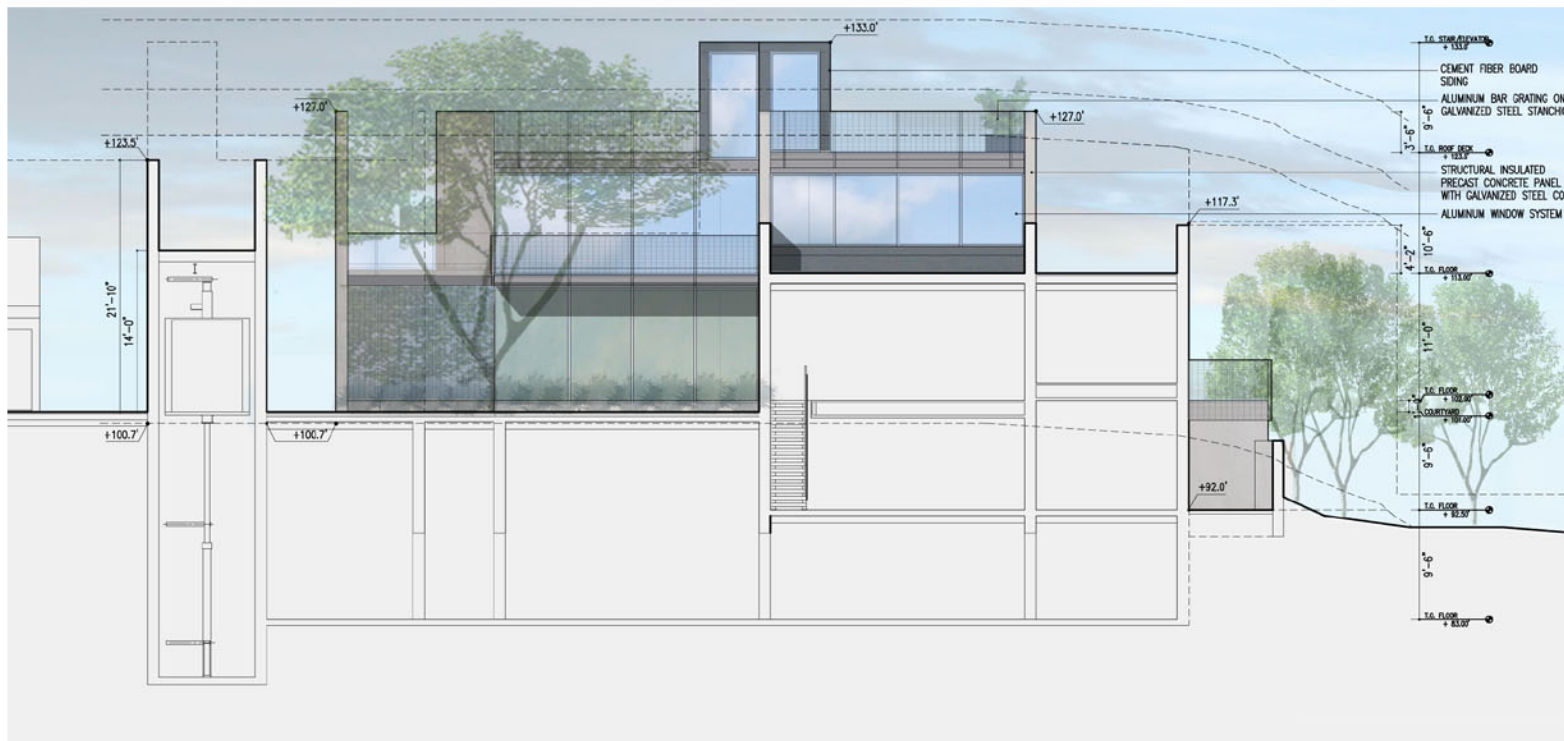
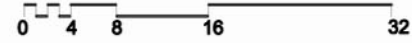




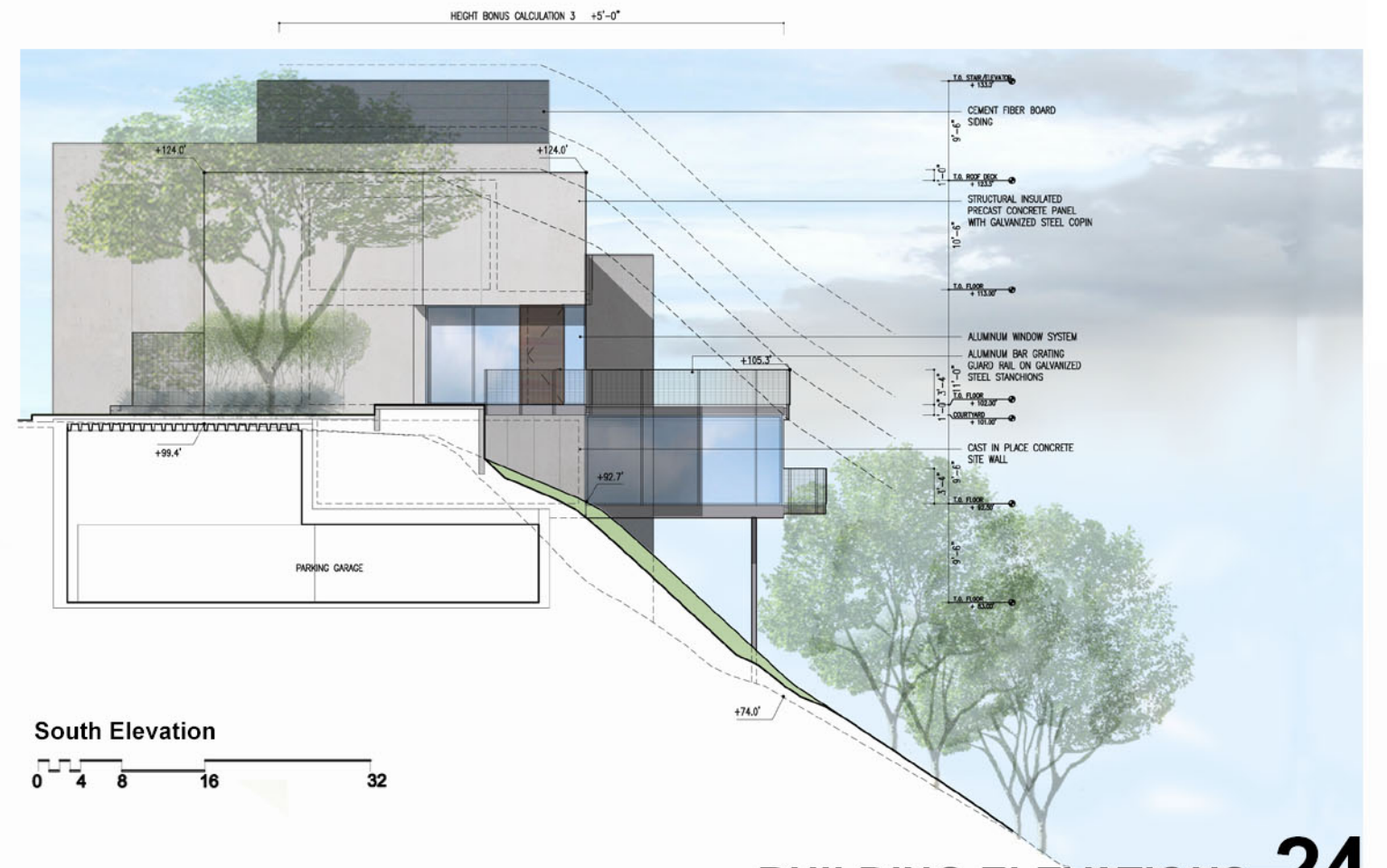
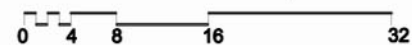
DESIGN REVIEW 2301 West Newton Street



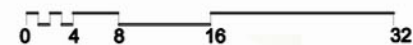
West Elevation- Courtyard

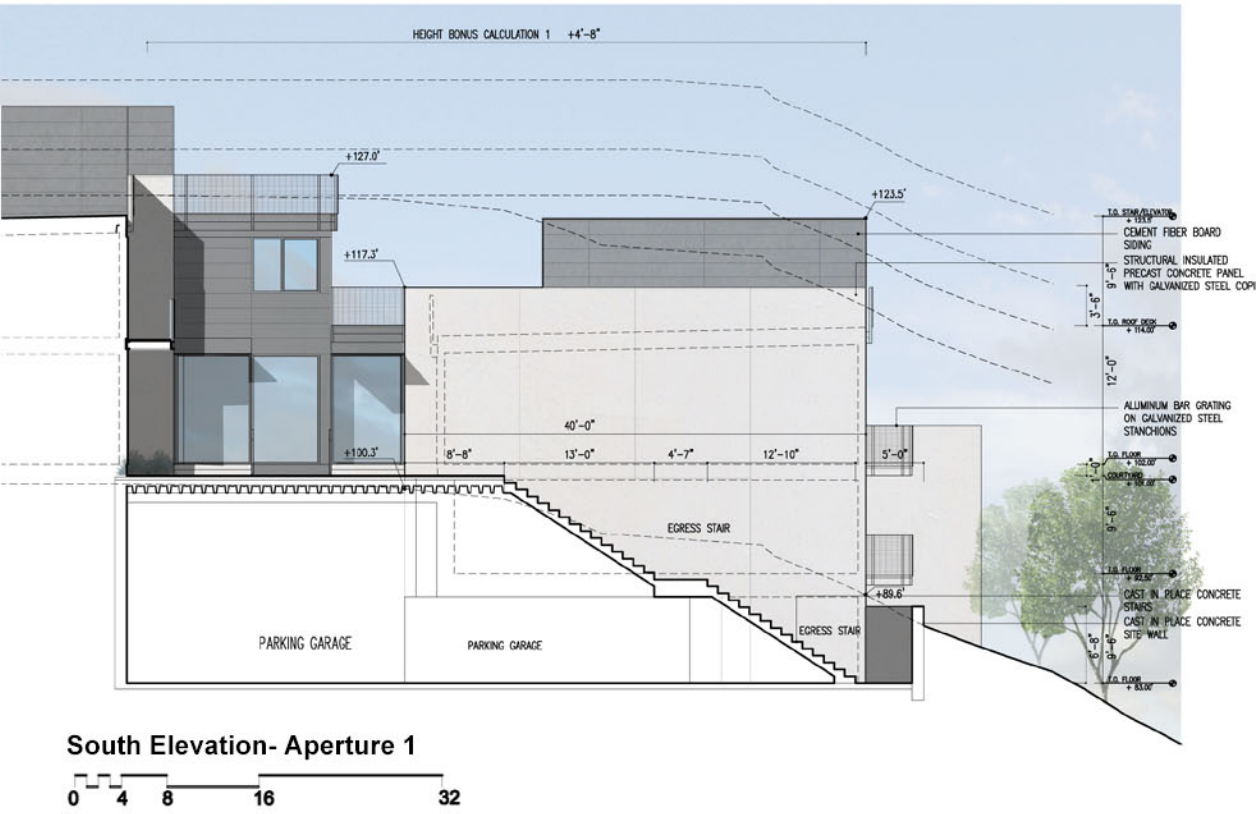
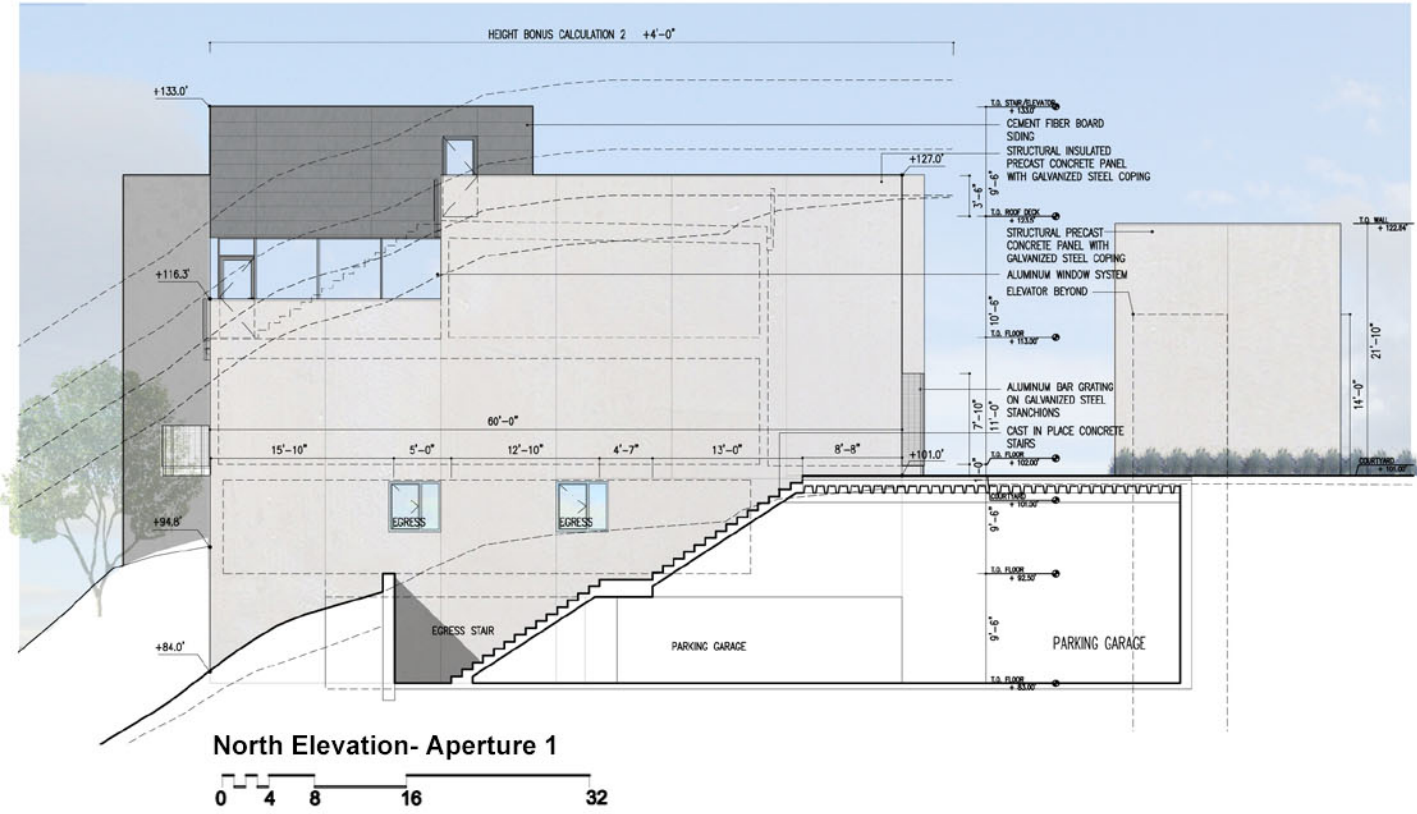
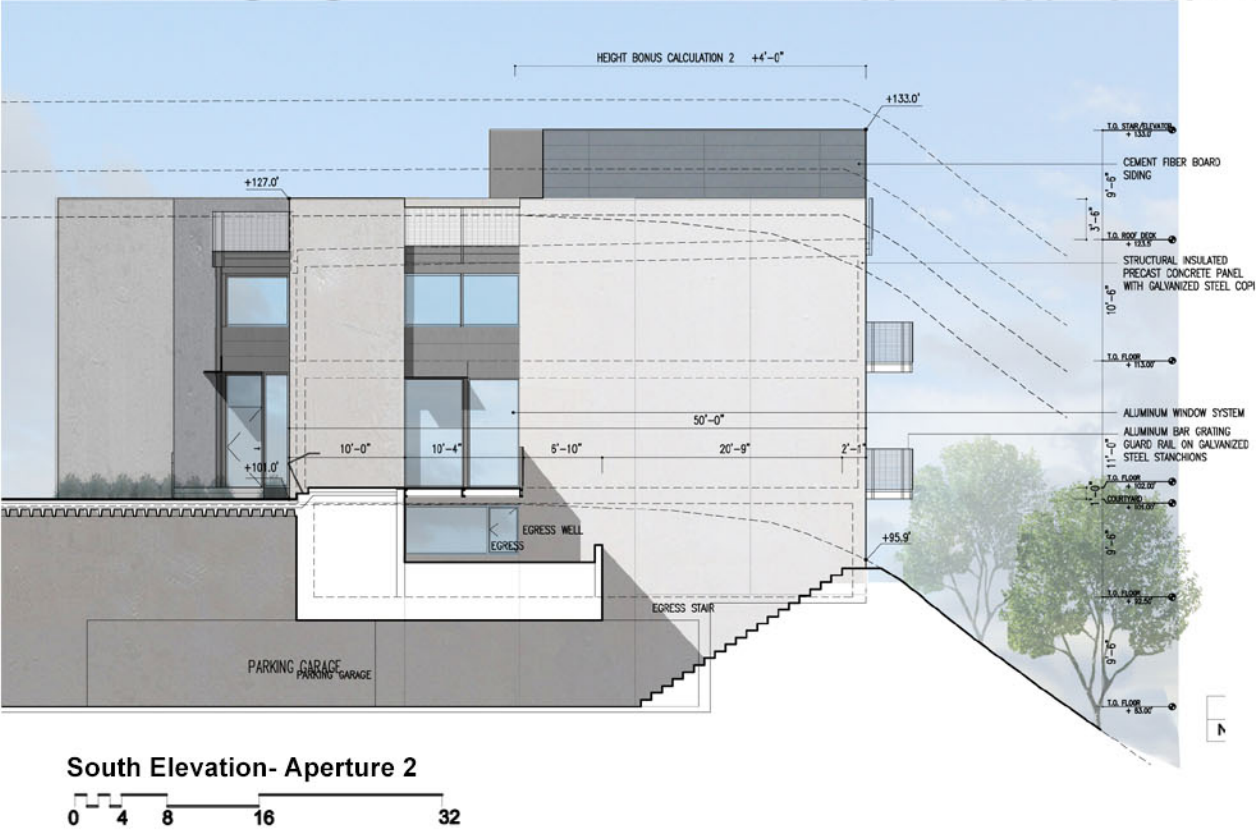
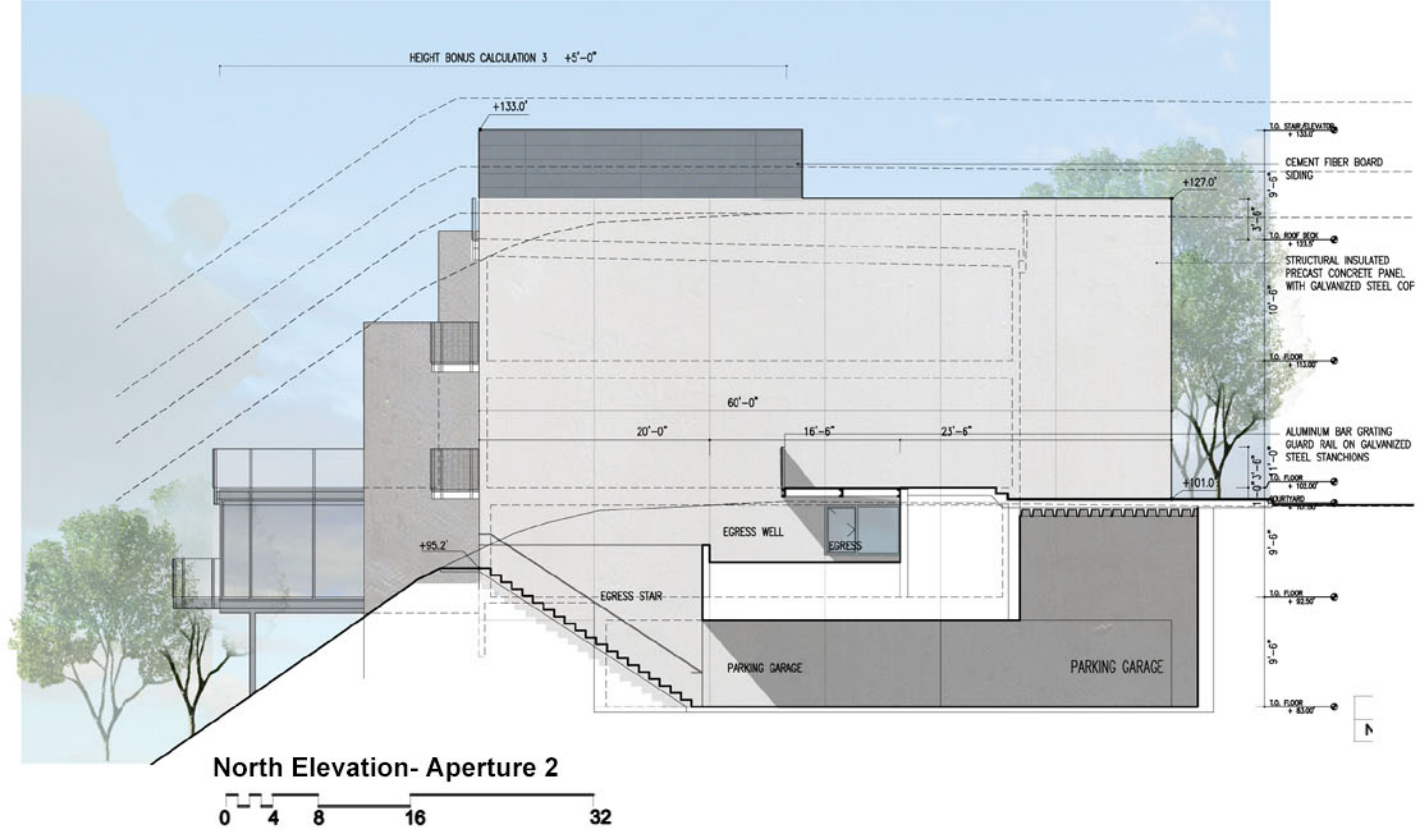


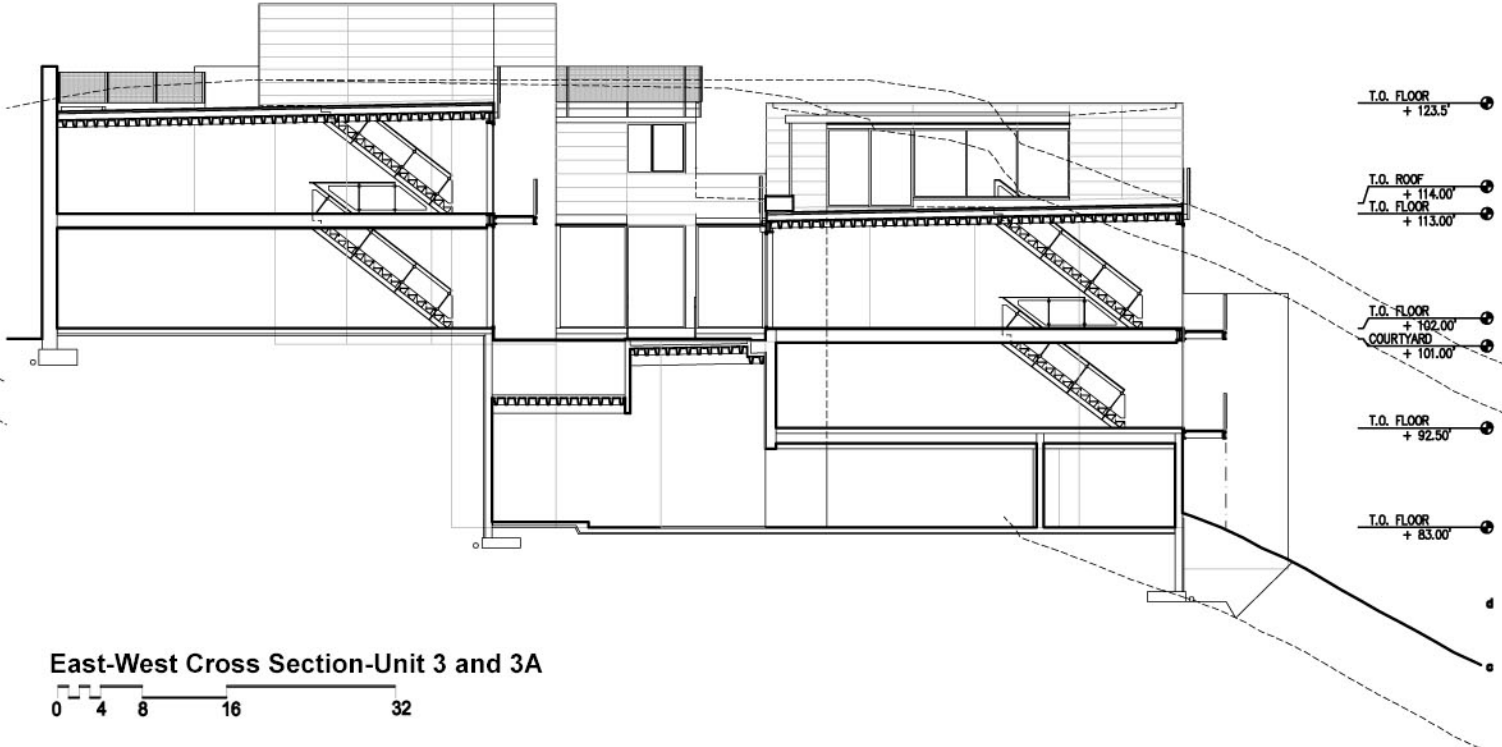
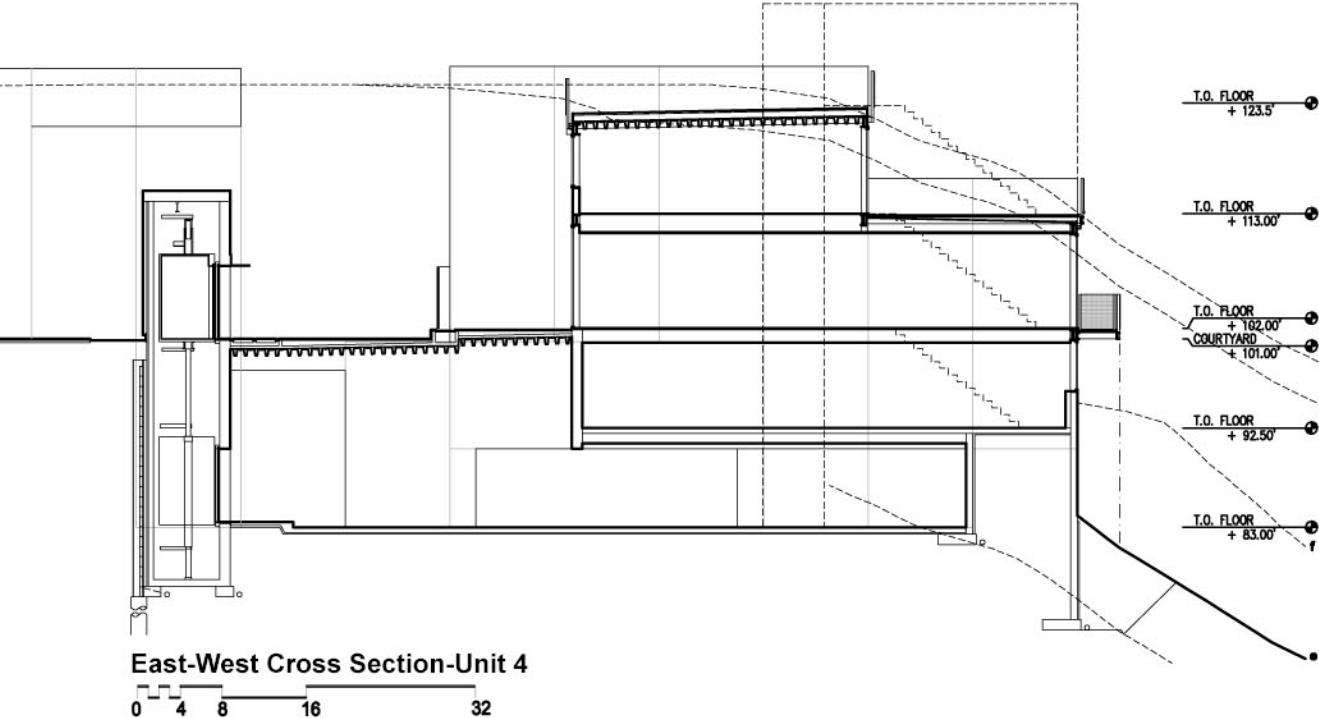
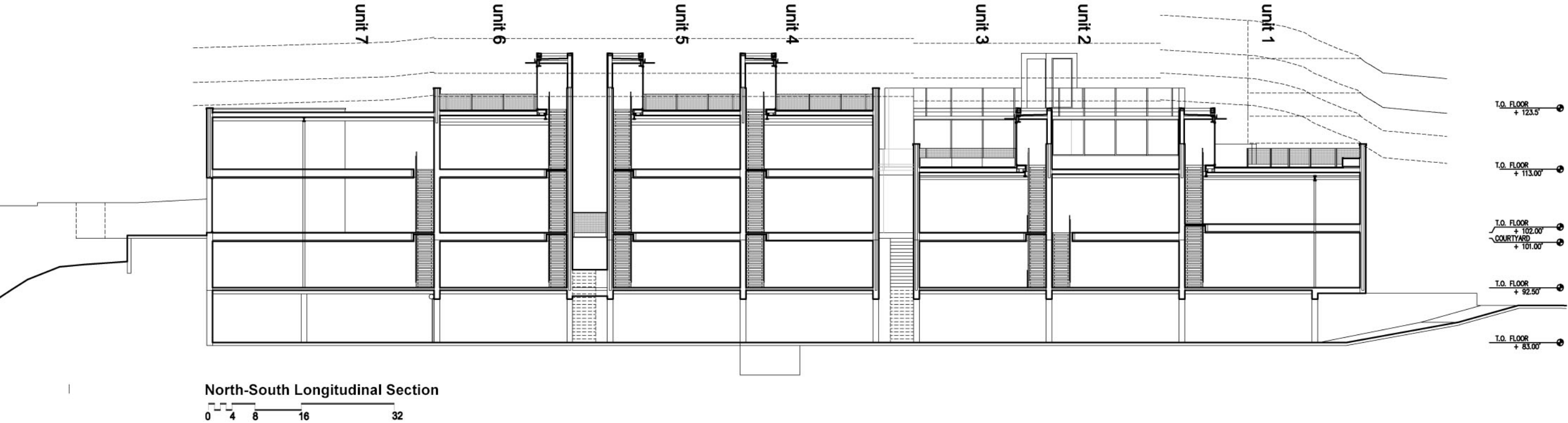
East Elevation- Courtyard

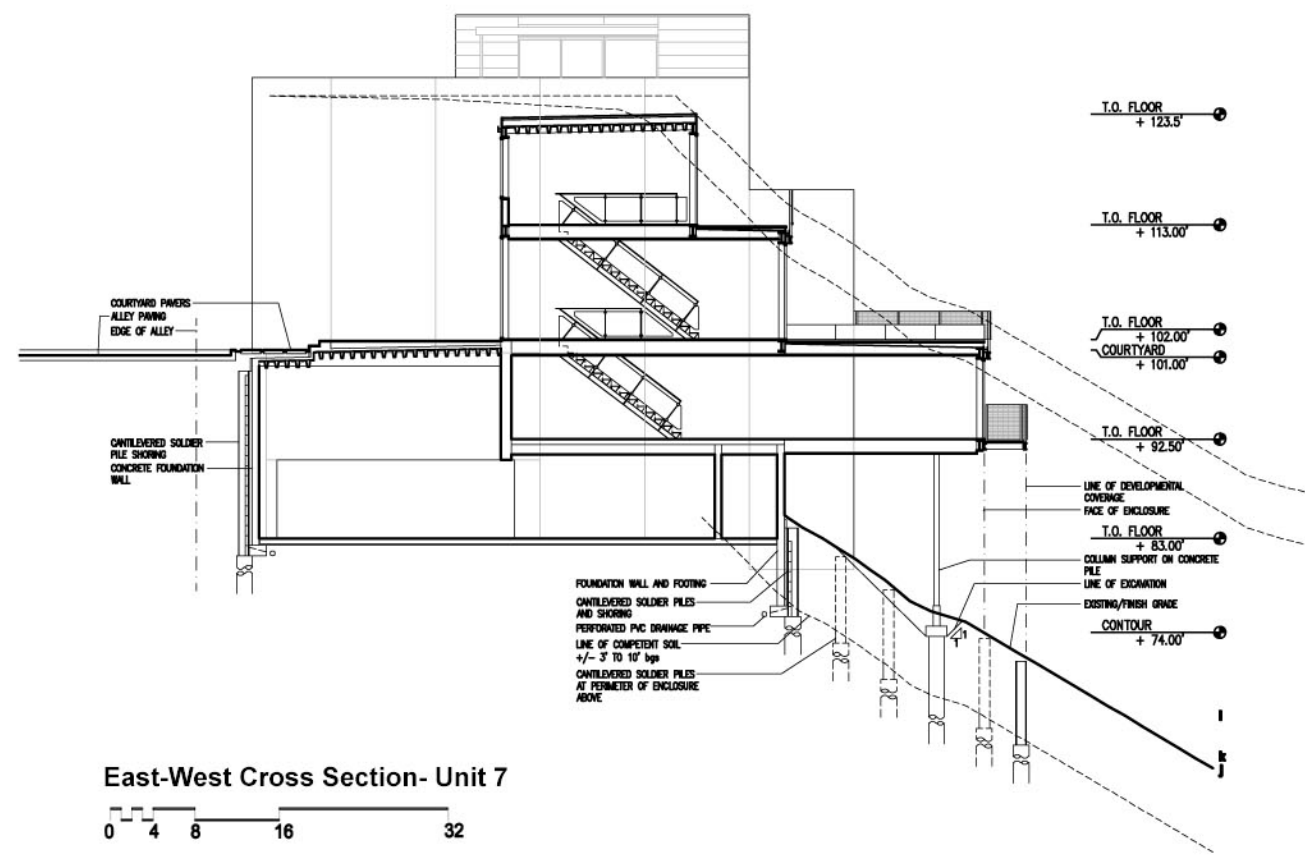


South Elevation

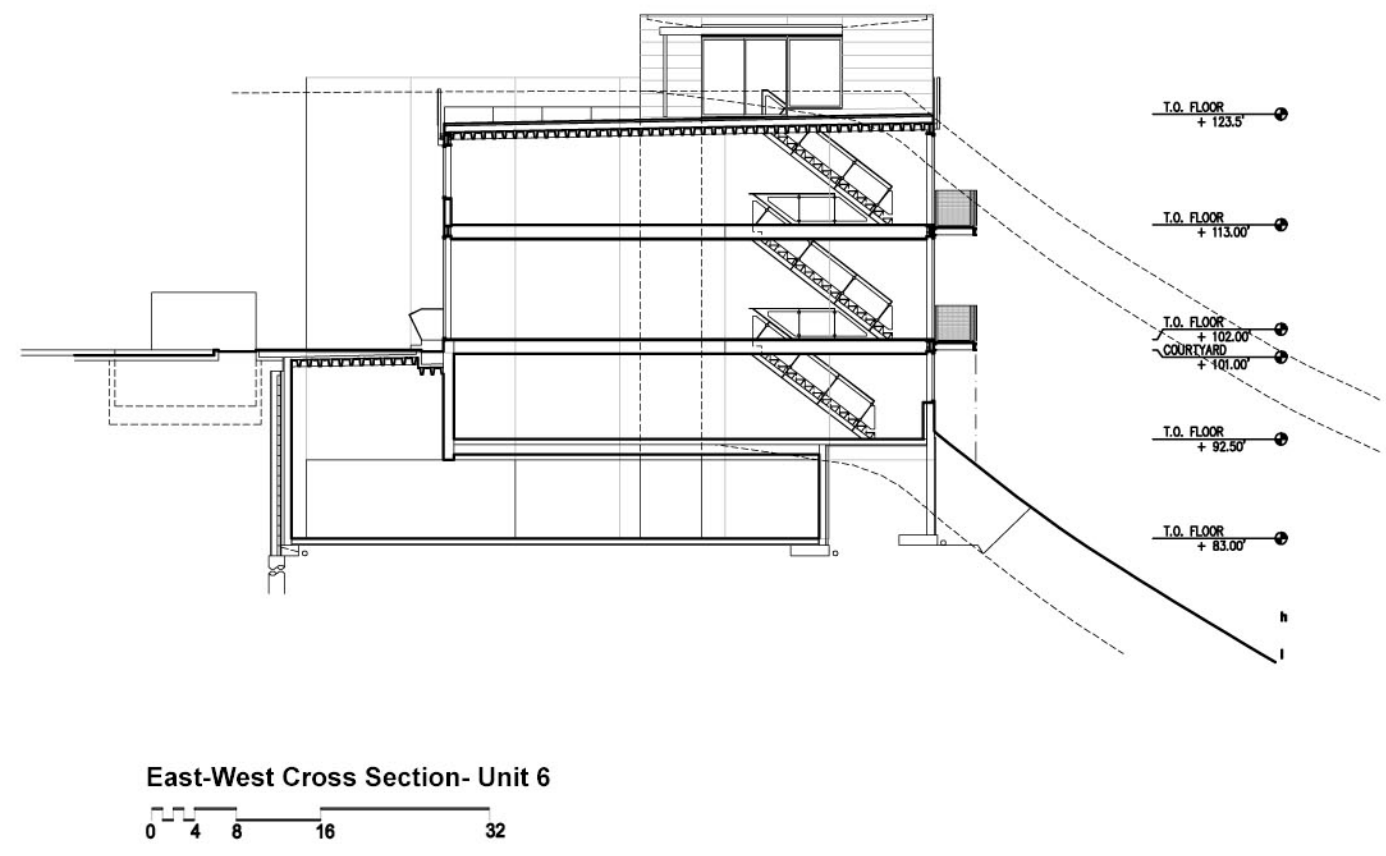




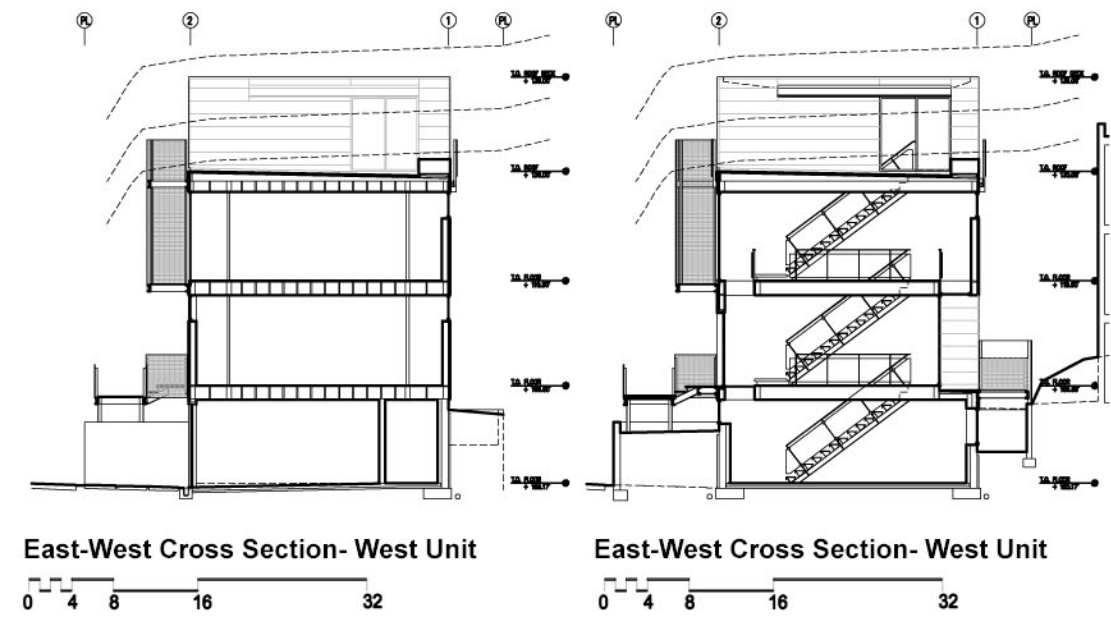




East-West Cross Section- Unit 7

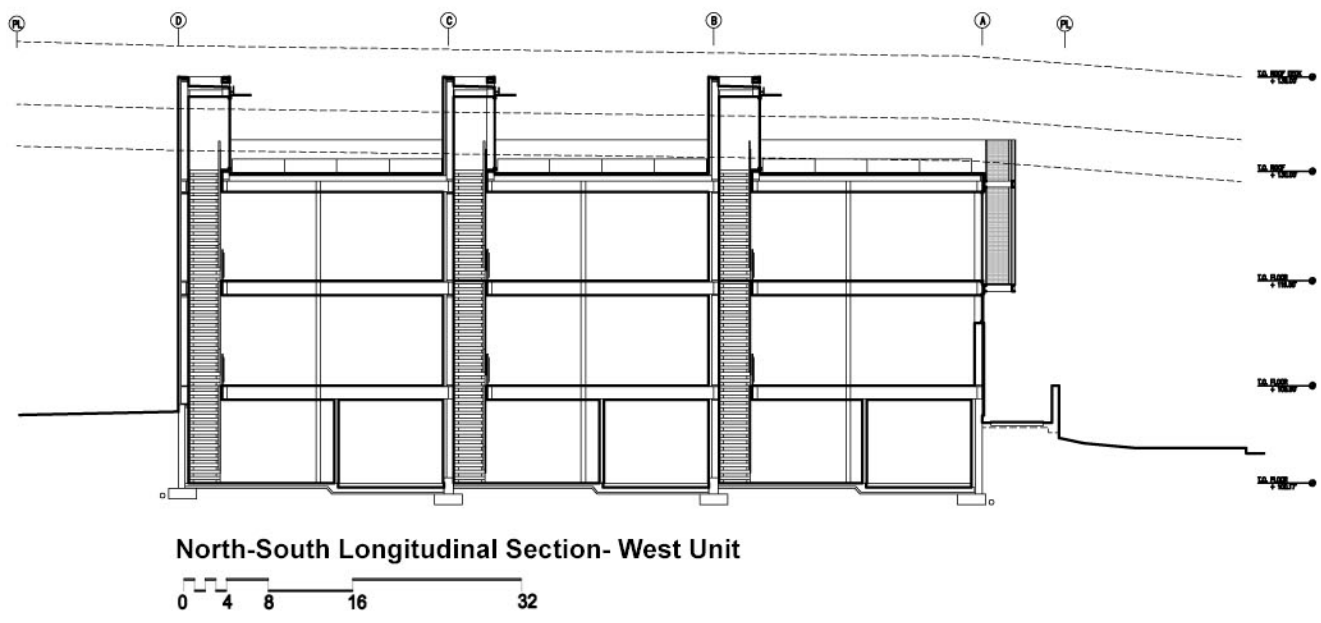


East-West Cross Section- Unit 6



East-West Cross Section- West Unit

East-West Cross Section- West Unit



North-South Longitudinal Section- West Unit

DESIGN REVIEW 2301 West Newton Street

Precast Concrete

Wood Siding

Clear Anodized Aluminum Canopy

Galvanized Steel Stanchions

PL



Translucent Glazing

Fiber Cement Board Siding with Aluminum Window System



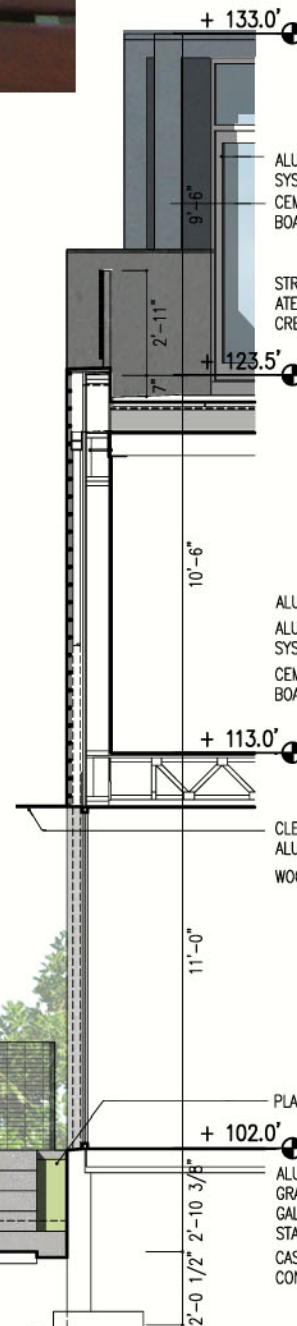
Aluminum Bar Grating



Wall Section at Entry-Unit 3A

3/16" = 1'-0"

E. Cobb Architects Inc. 911 Western Avenue, Suite 318 Seattle Washington 98104



North Elevation-Unit 3A

3/16" = 1'-0"

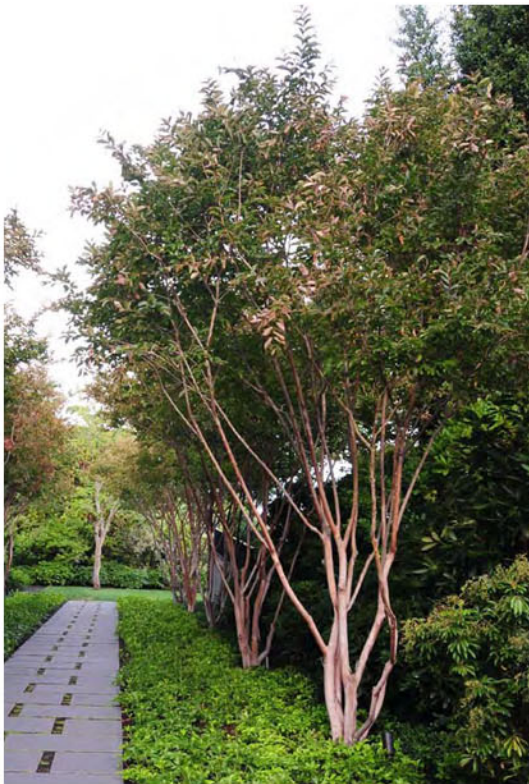
ALUMINUM BAR GRATING ON GALVANIZED STEEL STANCHIONS
GALVANIZED STEEL CHANNEL COPING

STRUCTURAL INSULATED PRECAST CONCRETE PANEL
WOOD SIDING WITH CLEAR FINISH
ALUMINUM WINDOW SYSTEM

ALUMINUM GUTTER
ALUMINUM WINDOW SYSTEM WITH TRANSLUCENT GLAZING
CAST-IN-PLACE CONCRETE



ELEVATION VIGNETTE 29



Crape Myrtle



Burkwood Osmanthus



Cast-in-Place Concrete



Bishop's Hat



Dwarf Maiden Grass



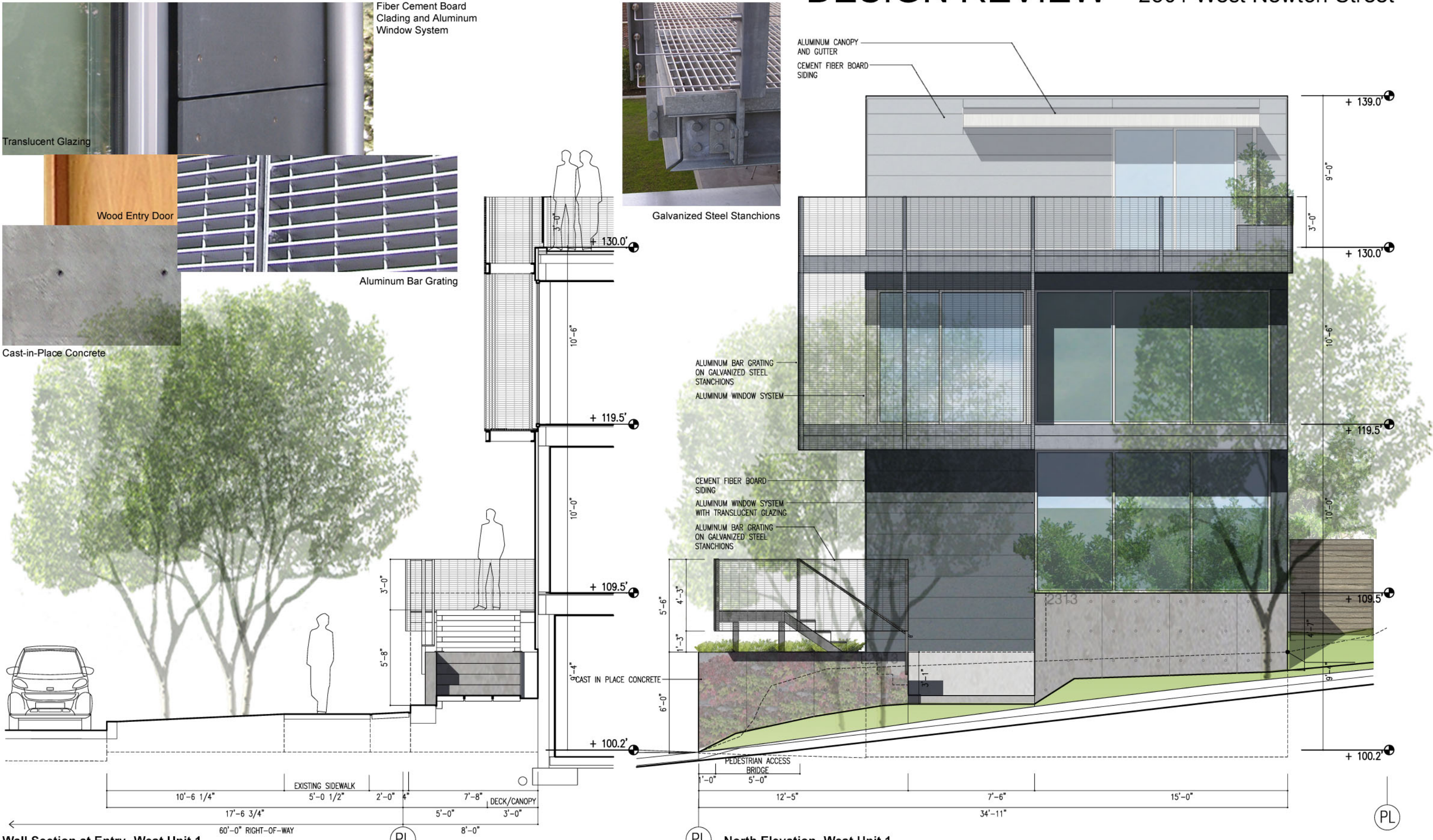
View from West Newton Street

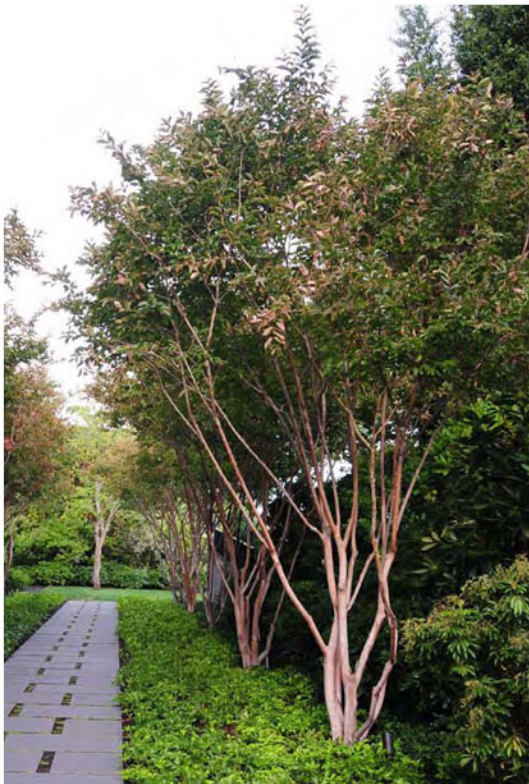


Pavers with planted joints



Lily Turf





Crape Myrtle



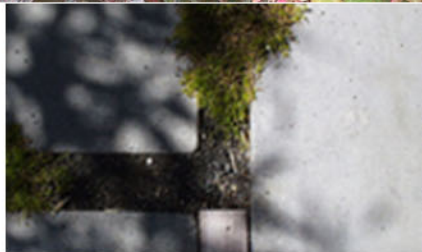
Japanese Stewartia

Boston Ivy



Japanese Tassel Fern

Bishop's Hat



Pavers with planted joints



View from West Newton Street

2301 West Newton Street 3008430
Potential Departures

REQUIREMENT	REQUEST	APPLICANT'S JUSTIFICATION	BOARD RECOMMENDATION from EDG Meeting
Access to Parking (SMC23.45.018B1) Access to parking shall be from the alley when the site abuts a platted alley improved to SDOT standards.	Request access to below-grade parking garage from West Newton Street where it intersects with 23rd Avenue West.	Access is proposed at the point where West Newton ends at the lowest grade. Access here would decrease traffic in the alley so it could more freely be used as a pedestrian thruway. A-1; A-7; A-8; C-3, C-5; D-1	The Board will continue to entertain these departures based on how well the design responds to the Design Guidelines noted.
Front Setbacks (SMC23.45.014) Average of adjacent structures or 15' maximum. 15' required.	Request 5' front setback for both sites. A reduction of 10'.	Reducing the front setback will encourage activity on the street. West Newton is a 60' ROW and 17' from curb to property line. A-1; A-2; A-4; A-6, C-3	The Board will continue to entertain these departures based on how well the design responds to the Design Guidelines noted.
Side Setbacks (SMC23.45.014A) Structures 25' high and 192' deep; side setback to a minimum of 11' and average of 21'.	Request a reduction in side setbacks to 5' minimum and 11' average on west side; 5' minimum and 19' average on east side.	Reduced side setbacks allows the building to respond to the site's topography; allows for the creation of a larger courtyard; allows the building to be oriented towards the SE to take advantage of views and solar exposure. A-1; A-7; B-; C-5; D-1	The Board will continue to entertain these departures based on how well the design responds to the Design Guidelines noted.
Structural Depth (SMC23.45.011) 65% of lot depth permitted or 156'.	Request increase in structure depth to 80% of lot depth or 192'.	Increased depth does not increase allowable lot coverage; allows building to be oriented to the SE to take advantage of solar exposure and views; allows the building to respond to site's topography; allows for a larger courtyard. A-1; A-5; A-7; B-1; C-1; D-1	The Board will continue to entertain these departures based on how well the design responds to the Design Guidelines noted.
Structural Width (SMC23.45.011) 90' permitted for townhouses with modulation.	Request structural width of 120'	Increased width does not increase allowable lot coverage; allows for three residential entries on the street level activating the street and re-enforcing existing street pattern; allows the building's massing to respond to the sit's topography; allows for a larger courtyard. A-1; A-2; A-3; A-4; A-7; B-1; C-1; C-3; C-5; D-7	The Board will continue to entertain these departures based on how well the design responds to the Design Guidelines noted.
Open Space (SMC23.45.016) Average of 300sf per unit, min. 200sf located off main living level.	Request that the private open space be relocated to rooftop decks.	Allows for large common courtyard at ground level. A-3; A-4; A-7; C-3; D-1; D-7	The Board will continue to entertain these departures based on how well the design responds to the Design Guidelines noted.

2315 West Newton Street 3008429
Potential Departures

REQUIREMENT	REQUEST	APPLICANT'S JUSTIFICATION	BOARD RECOMMENDATION from EDG Meeting
Front Setbacks (SMC23.45.014) Average of adjacent structures or 15' maximum. 15' required.	Request 5' front setback for both sites. A reduction of 10'.	Reducing the front setback will encourage activity on the street. West Newton is a 60' ROW and 17' from curb to property line. A-1; A-2; A-4; A-6, C-3	The Board will continue to entertain these departures based on how well the design responds to the Design Guidelines noted.
Rear Setback (SMC23.45.014A) 20' required.	Request a reduction in rear setbacks to 15'	Reducing the rear setback does not increase lot coverage; allows for an increase in private open space; A-1; A-7; C-5; D-1	The Board will continue to entertain these departures based on how well the design responds to the Design Guidelines noted.
Side Setbacks (SMC23.45.014A) Structures 25' high and 90' deep; side setback to a minimum of 6' and average of 8'.	Request a reduction in side setbacks to 5' min. and 5' average at west side; 6' min. and 8' average at east side.	Reducing the side setbacks will not increase lot coverage; allows for the creation of a pedestrian access bridge; allows for more open space; increases pedestrian interaction along the alley and minimizes the impact of vehicular access. A-1;A-7; C-5; D-1	
Structural Depth (SMC23.45.011) 65% of lot depth permitted or 65'.	Request an increase in structural depth to 90% of lot depth or 90'-0".	Increased depth does not increase lot coverage; Allows for the creation of a pedestrian access bridge along the alley increasing pedestrian interaction and minimizing the impact of vehicular access; allows for more openspace. A-1; A-4; A-7; A-8; B-1; C-5; D-1	
Open Space (SMC23.45.016) Average of 300sf per unit, min. 200sf located off main living level.	Request that the private open space be relocated to rooftop decks.	Allows for common green space at the ground level; allows for pedestrian access bridge at ground level. A-3; A-4; A-7; C-3; D-1; D-7	

Leschi Residence E. Cobb Architects



E. Cobb Architects Inc. 911 Western Avenue, Suite 318 Seattle Washington 98104



Shapiro Residence E. Cobb Architects

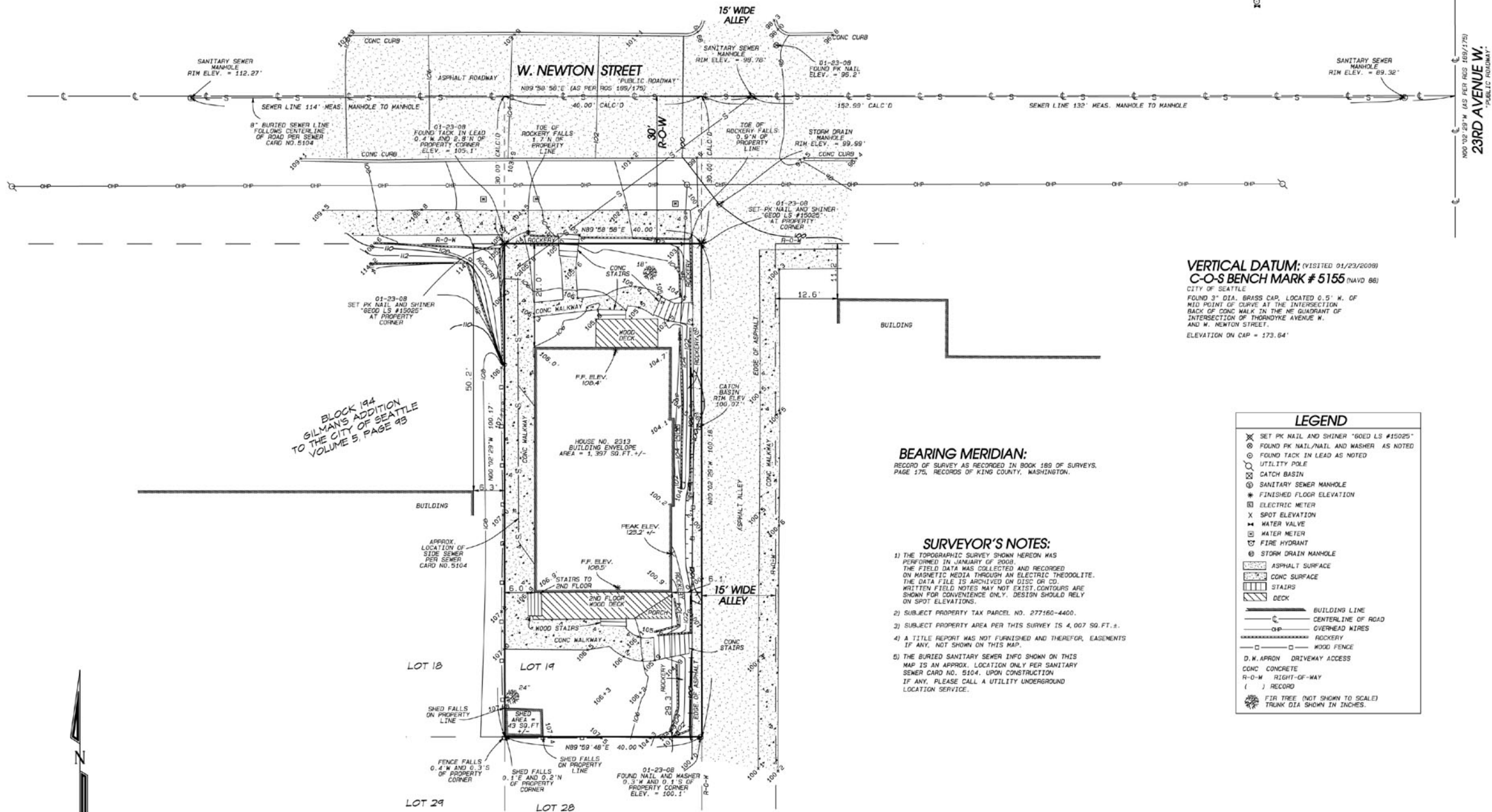
BonV Townhomes E. Cobb Architects



TOPOGRAPHIC & BOUNDARY SURVEY
NW 1/4 OF THE SE 1/4 OF SEC. 23, TWP. 25N., RGE. 3E., W.M.
CITY OF SEATTLE, KING COUNTY, WA.

LEGAL DESCRIPTION:

LOT 19, BLOCK 194, GILMAN'S ADDITION TO THE CITY OF SEATTLE,
AS PER PLAT RECORDED IN VOLUME 5 OF PLATS, PAGE 93, RECORDS
OF KING COUNTY, WASHINGTON.



VERTICAL DATUM: (VISITED 01/23/2009)
C-O-S BENCH MARK # 5155 (NAD 86)
CITY OF SEATTLE
FOUND 3" DIA. BRASS CAP, LOCATED 0.5' W. OF
MID POINT OF CURVE AT THE INTERSECTION
BACK OF CONC WALKWAY IN THE NE QUADRANT OF
INTERSECTION OF THORNDYKE AVENUE N.
AND W. NEWTON STREET.
ELEVATION ON CAP = 173.84'

BEARING MERIDIAN:

RECORD OF SURVEY AS RECORDED IN BOOK 180 OF SURVEYS,
PAGE 175, RECORDS OF KING COUNTY, WASHINGTON.

SURVEYOR'S NOTES:

- 1) THE TOPOGRAPHIC SURVEY SHOWN HEREON WAS PERFORMED IN JANUARY OF 2008. THE FIELD DATA WAS COLLECTED AND RECORDED ON MAGNETIC MEDIA THROUGH AN ELECTRIC THEODOLITE. THE DATA FILE IS ARCHIVED ON DISC OR CD. WRITTEN FIELD NOTES MAY NOT EXIST. CONTOURS ARE SHOWN FOR CONVENIENCE ONLY. DESIGN SHOULD RELY ON SPOT ELEVATIONS.
- 2) SUBJECT PROPERTY TAX PARCEL NO. 277160-4400.
- 3) SUBJECT PROPERTY AREA PER THIS SURVEY IS 4,007 SQ. FT. ±.
- 4) A TITLE REPORT WAS NOT FURNISHED AND THEREFORE, EASEMENTS IF ANY, NOT SHOWN ON THIS MAP.
- 5) THE BURIED SANITARY SEWER INFO SHOWN ON THIS MAP IS AN APPROX. LOCATION ONLY PER SANITARY SEWER CARD NO. 5104. UPON CONSTRUCTION IF ANY, PLEASE CALL A UTILITY UNDERGROUND LOCATION SERVICE.

LEGEND	
	SET PK NAIL AND SHINER "6000 LS #15025"
	FOUND PK NAIL/WASHER AS NOTED
	FOUND TACK IN LEAD AS NOTED
	UTILITY POLE
	CATCH BASIN
	SANITARY SEWER MANHOLE
	FINISHED FLOOR ELEVATION
	ELECTRIC METER
	SPOT ELEVATION
	WATER VALVE
	WATER METER
	FIRE HYDRANT
	STORM DRAIN MANHOLE
	ASPHALT SURFACE
	CONC SURFACE
	STAIRS
	DECK
	BUILDING LINE
	CENTERLINE OF ROAD
	OVERHEAD WIRES
	ROCKERY
	WOOD FENCE
	D.W. APRON DRIVEWAY ACCESS
	CONC CONCRETE
	R.O.W. RIGHT-OF-WAY
	() RECORD
	FIR TREE (NOT SHOWN TO SCALE)
	TRUNK DIA SHOWN IN INCHES.

METHOD OF SURVEY:

TRIMBLE TOTAL STATION FOR THIS SURVEY WAS A TOTAL STATION.

TOPOGRAPHIC & BOUNDARY SURVEY

TOLY RESIDENCE
2313 W. NEWTON STREET
SEATTLE, WA. 98199

DWN. BY T.J.B.	DATE 02/06/08	JOB NO. 8013
CHKD. BY E.J.G.	SCALE 1"=10'	SHEET 1 OF 1

GeoDimensions

6210 FAIRWAY PLACE SE
SNOQUALMIE, WA. 98065
PHONE (425) 458-4488
FAX (206) 686-2950



2301 West Newton Street

