

Early Design Guidance



811 STEWART

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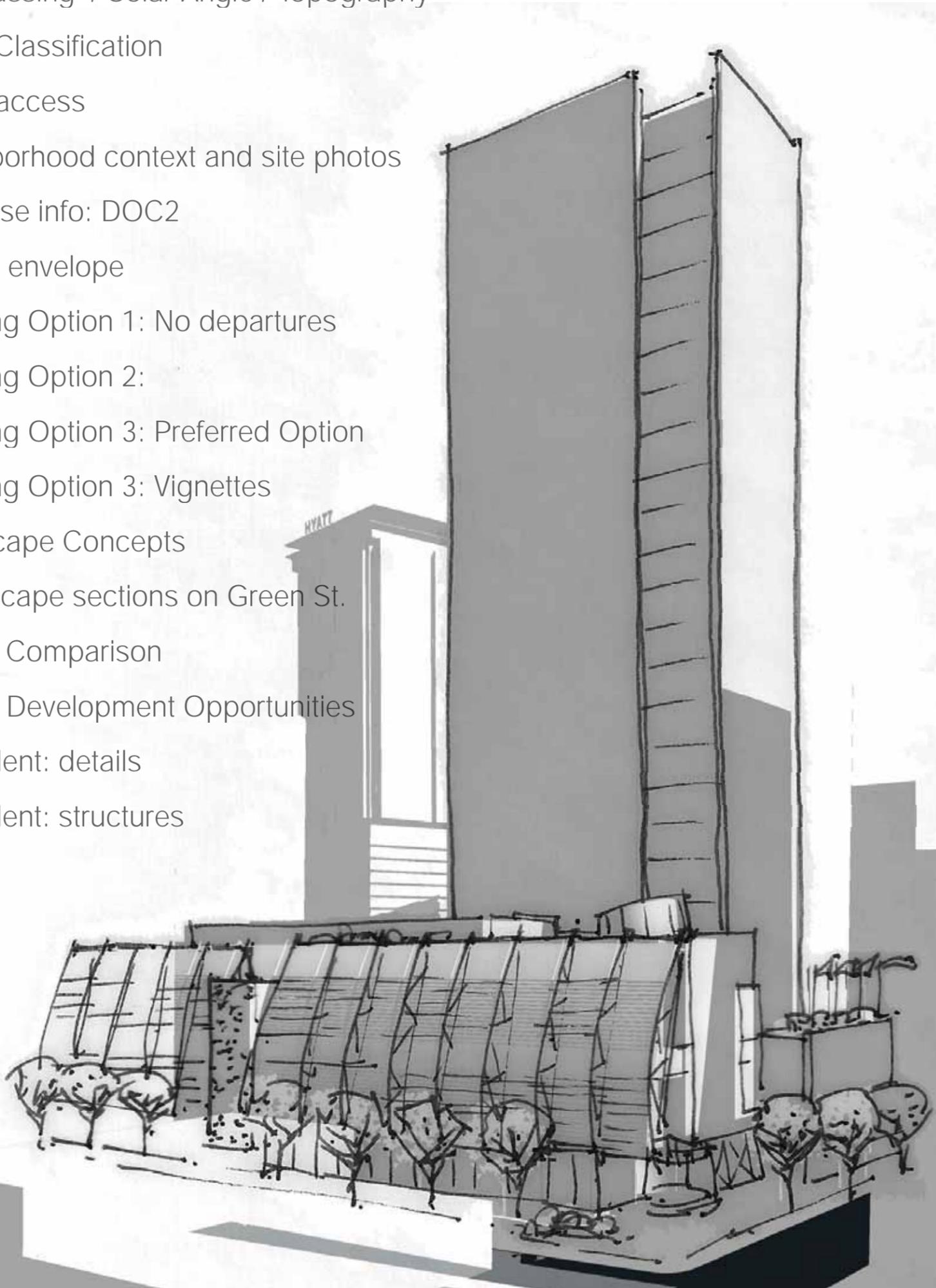
Date:
March 11, 2008

To:
Design Review Board



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1. We strive to further define and represent Seattle, marking this place in time, as a hub of technical leadership, deeply rooted, engaged in, and identified with our natural environment.

Met•ro•nat•u•ral \,mə-trō-'na-chə-rəl\ – *adj.*

1 : having the characteristics of a world-class metropolis within wild, beautiful natural surroundings **2** : A blending of clear skies and expansive water with a fast paced city life – *n.* **3** : one who respects the environment and lives a balanced lifestyle of urban and natural experiences **4** : Seattle

*Quote by Washington State Convention and Visitors Board



2. We, in the city of Seattle, are concerned about our natural environment and the impact that our built environment has on it. Seattle is leading the nation on the issue of sustainability, educating the public on how to build smarter and live greener. These values will be reflected in this building as we pursue LEED Platinum certification.

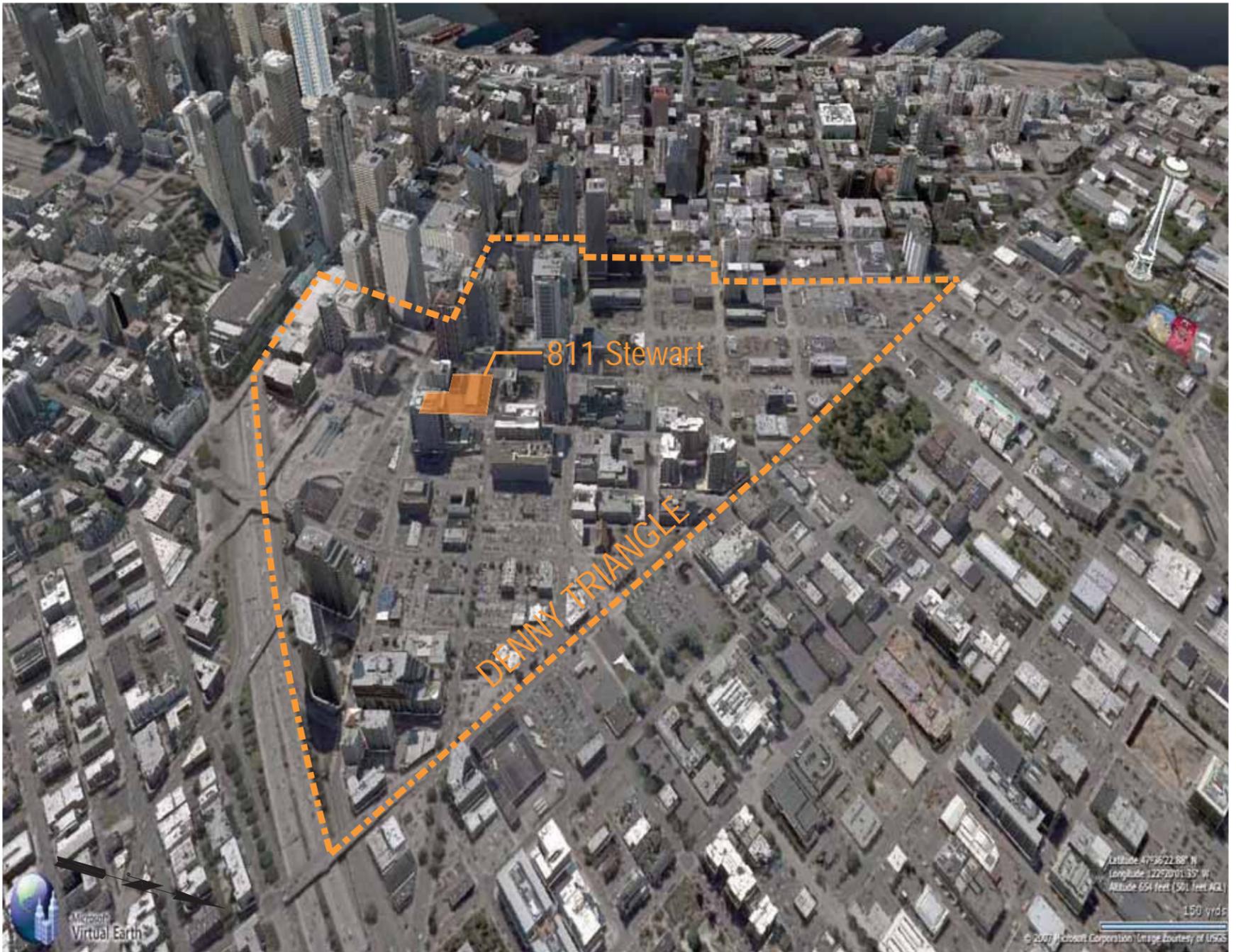


3. Tourism Matters 'Tourism and Convention business generated more than \$4 billion in the local economy in 2005. More than 9 million travelers from around the world visited Seattle and King County. The tourism industry supports more than 60,000 jobs throughout the region.' (Seattle Convention and Visitor Bureau) We endeavor to augment the capacity for convention business in Seattle, the smallest in the west coast, by adding roughly 100,000 SF of exhibition and meeting room space, supported by underground parking, retail, restaurants, and approximately 1200 guest rooms.



4. We are committed to the goal of enhancing the quality of the public realm through the pursuit of design excellence. The project should be a thoughtful expression of scale, context, and the character of the neighborhood, creating pockets of green relief in the urban fabric and providing opportunity for people to gather along an activated street environment.





DENNY TRIANGLE NEIGHBORHOOD VISION:

- Establish an urban “neighborhood” by providing vital streets, the connective tissue of the community.
- The buildings and architectural features, character, height and scale should serve to establish a sense of place.
- Street level facades should be activated by retail and service uses, enhanced with landscaping and weather protection.
- Providing open space serves a dynamic vital role in the quality of day and night life in the neighborhood.
- 9th Avenue, developed as a Green street, will strengthen the neighborhood, providing a pedestrian enhanced environment that links the residential enclave with the Convention Center and Sound Transit, establishing a sense of place and providing green visual relief.



ZONE

DOC 2 – 500’ height limit.

DOC 2 is a zone dedicated primarily to office use with housing & a mix of other activities encouraged to add diversity, particularly beyond working hours.



SITE DESCRIPTION

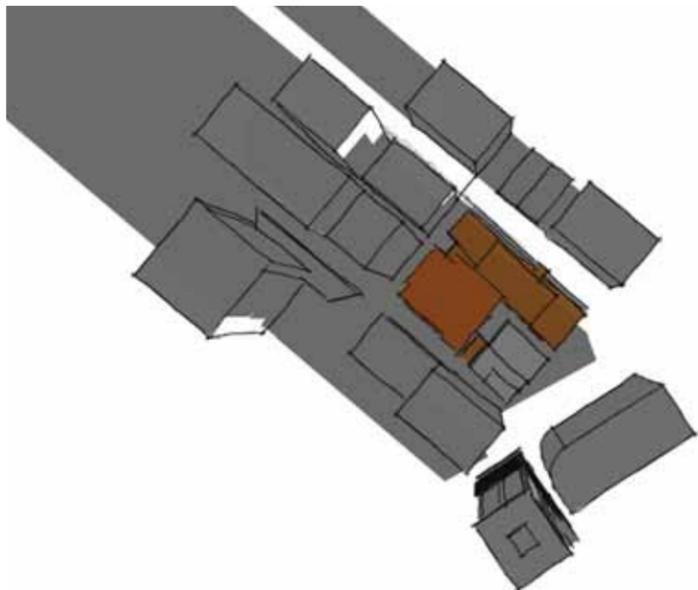
The project site consists of the 3 parcels on the northwest, northeast, and southeast corners of the block bound by 8th and 9th Avenues, Howell and Stewart Street. There is an existing alley from mid block on Howell that connects to 9th Avenue. The parcel areas total 75,599 SF. There are 5 mature American Sweetgum (*Liquidambar styraciflua*) trees on Stewart St..

USES

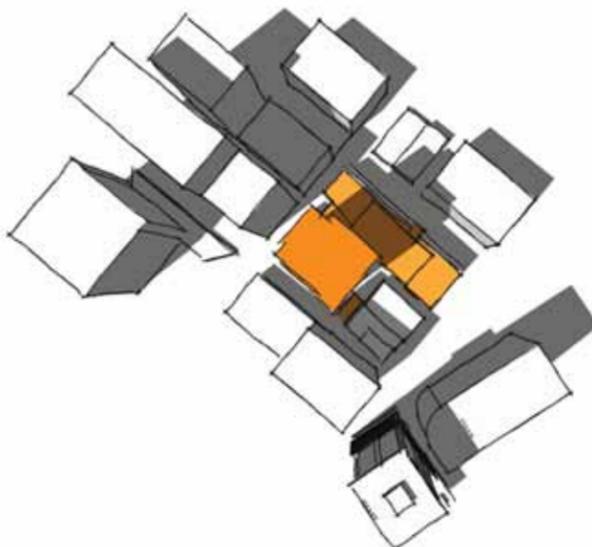
Structures on the site include the Greyhound Bus depot and a two story building which is currently serving as a presentation center for the O8 project and a temporary job shack for their contractors. There is a surface parking lot on the south east corner of the block. The Roffe Building, comprised of six stories of office, and the Bonaire apartment building, a 5 story masonry structure, fill out the remaining southwest corner of the block, and are not part of this project.

TOPOGRAPHY

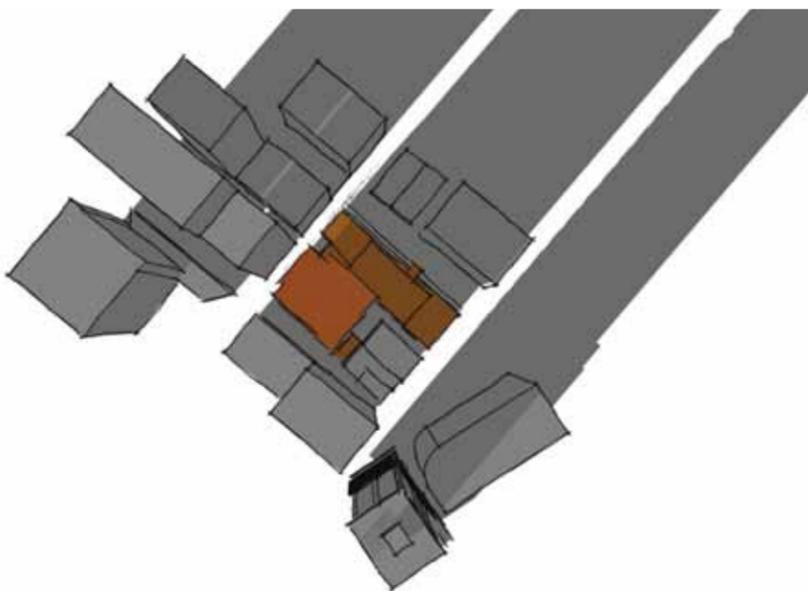
The site has a very gentle slope from the high point at the corner of 9th and Howell to a low point at the corner of 8th and Stewart. The 22' grade differential over roughly 475' equates to a maximum slope of less than 3 degrees.



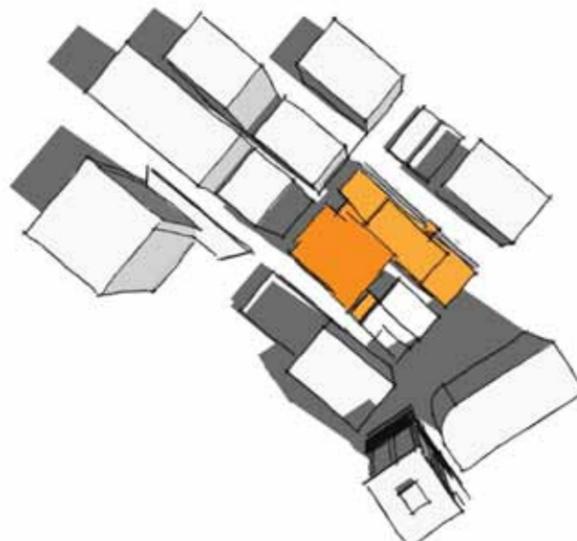
December 21st @ 8:30 am



June 21st @ 10:00 am



December 21st @ 3:00 pm



June 21st @ 2:00 pm



SOLAR ACCESS

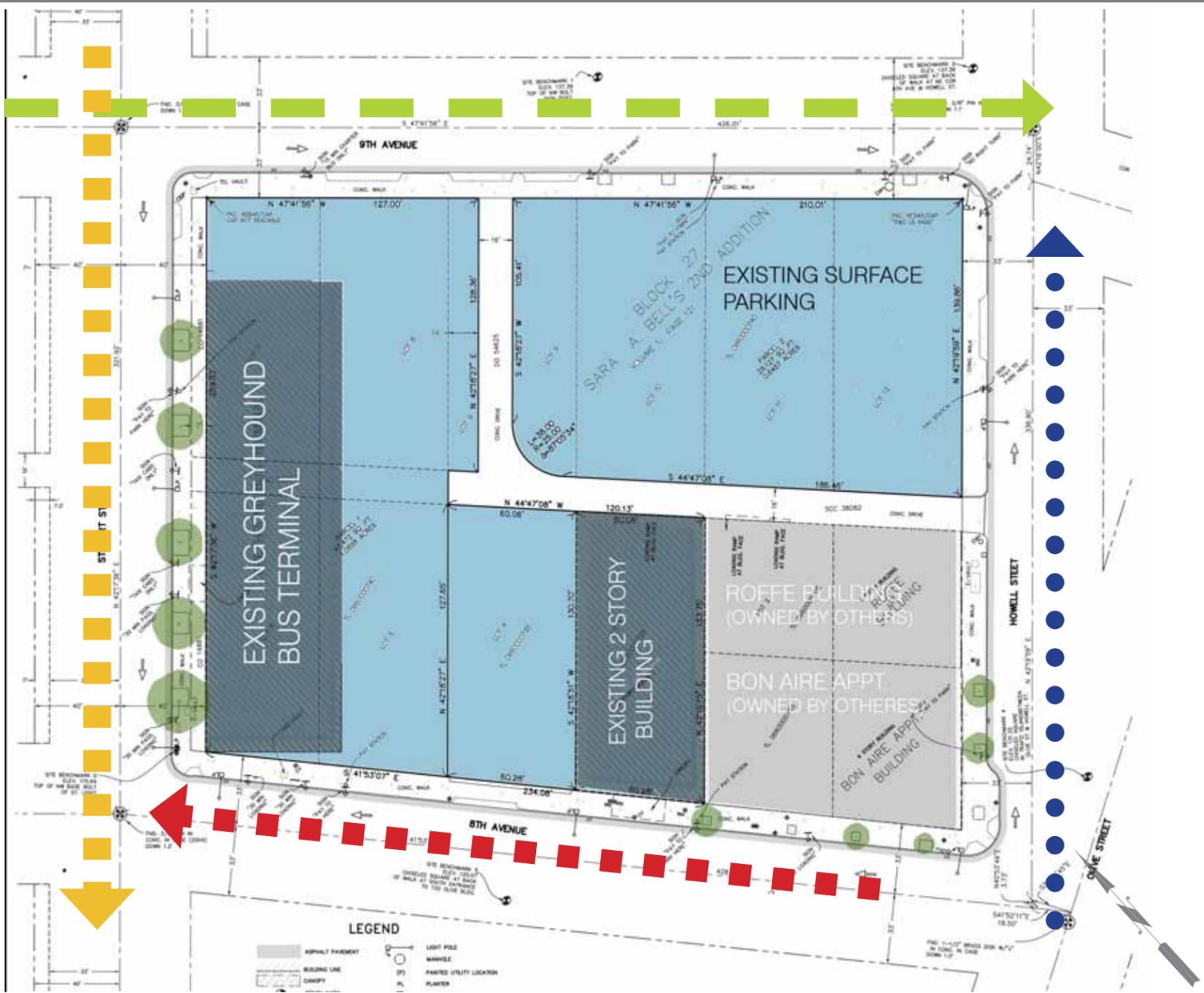
Currently there is access to morning light on 9th Ave and Howell. Midday sun continues on Howell and afternoon sun is available on Stewart St. for a short period during most of the year. Current and future development will affect solar access to the site.

SOLAR ACCESS

June 21st:

- 8th Ave: 9:45 am - 4:00 pm
6:45 pm - 8:00 pm
- 9th Ave: 9:00 am - 12:15 pm
- Stewart St.: 8:00 am - 2:30 pm
- Howell Ave.: 6:00 am - 2:15 pm

Street Classification

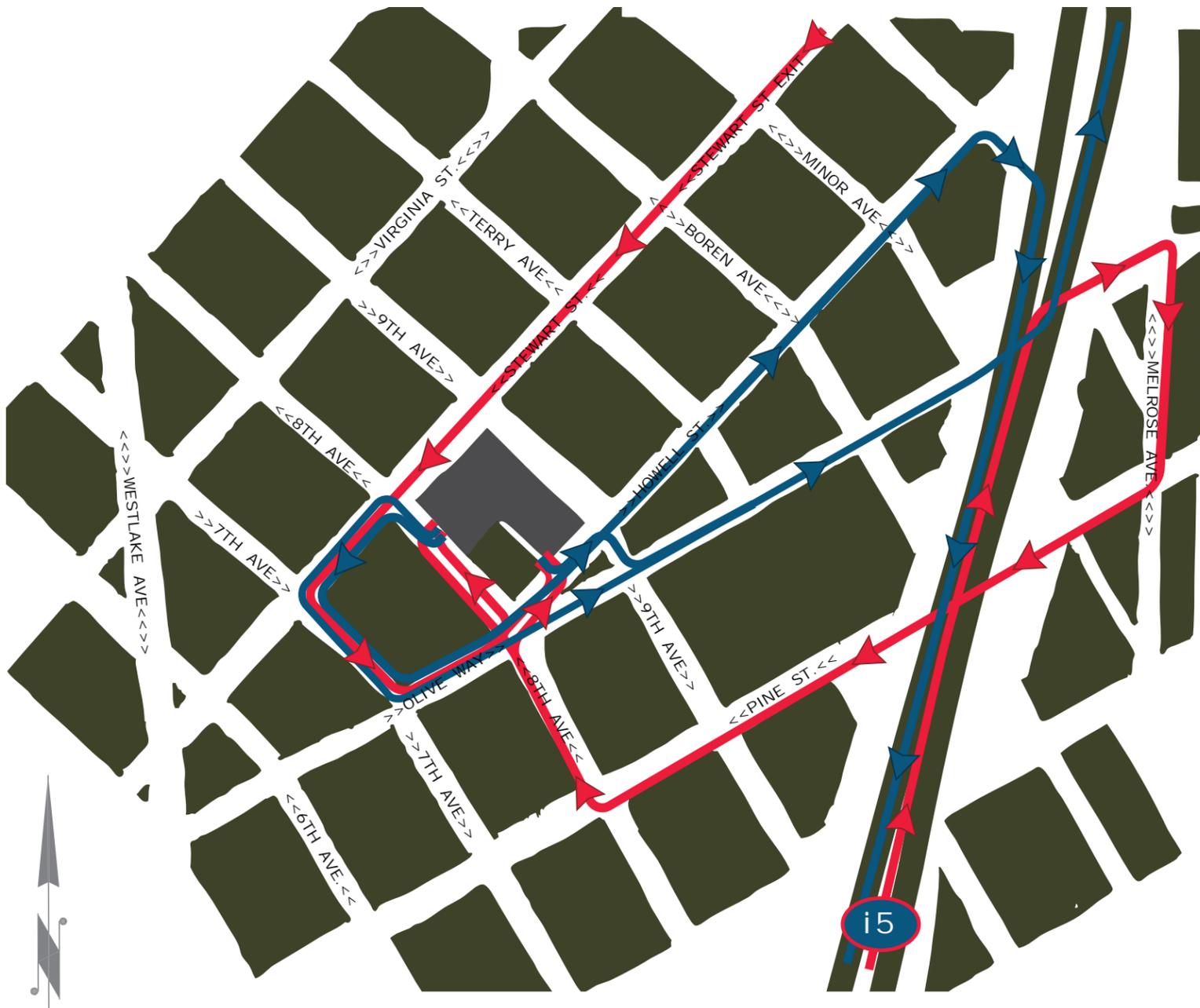


- 
9th Ave.
 Green Street
 Existing sidewalk width - 12'
 Required width - 12'

- 
Howell Ave.
 Class 2 Pedestrian
 Principal transit
 Existing sidewalk width - 12'
 Required width - 12'

- 
8th Ave.
 Class 1 Pedestrian
 Existing sidewalk width - 12'
 Required width - 15'

- 
Stewart St.
 Class 1 Pedestrian
 Principal transit
 Existing sidewalk width - 16'
 Required width - 18'



DEPARTURE TO I-5

ARRIVAL FROM I-5

| Project Component | Size | Daily Trips | AM Peak Hour Trips | | | PM Peak Hour Trips | | |
|-------------------|-------------|-------------|--------------------|-----|-------|--------------------|-----|-------|
| | | | In | Out | Total | In | Out | Total |
| Hotel | 1,220 rooms | 3,490 | 165 | 110 | 275 | 155 | 135 | 290 |

*Estimated trip generation provided by Heffron Transportation

Neighborhood context and site photos

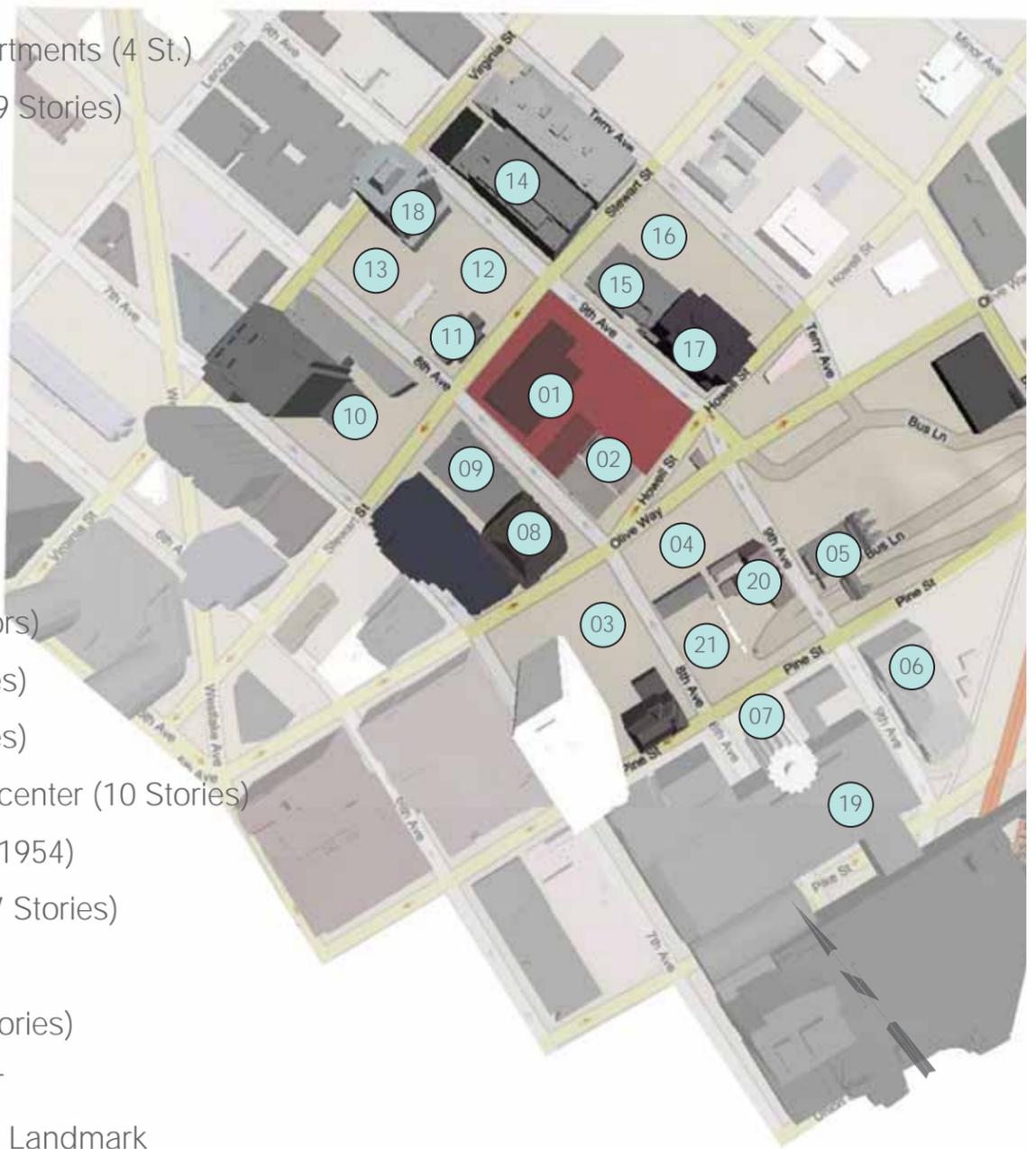


Stewart Street looking North



Stewart Street looking South

- 01 Current Greyhound station
-site of future development
- 02 Roffe building (7 St.) and Bonair apartments (4 St.)
- 03 Site of future Olive 8 Hotel/Condo (39 Stories)
- 04 809 Olivian tower
- 05 Downtown Seattle transit tunnel
- convention center station
- 06 Paramount theatre (Built 1928)
- 07 Tower @ 1801 (25 Stories)
- 08 720 Olive (19 Stories)
- 09 Stewart Court apartments (6 Stories)
- 10 Federal courthouse (30 stories)
- 11 Future Office Development (8-10 floors)
- 12 Future Office Development (14 stories)
- 13 Future Office Development (34 stories)
- 14 Seattle Children’s Hospital research center (10 Stories)
- 15 Gethsemane Lutheran Church (built 1954)
- 16 1823 Terry St. Apartment building (37 Stories)
- 17 Regence Blue Shield (15 Stories)
- 18 Cosmopolitan Condominiums (30 Stories)
- 19 Washington State Convention Center
- 20 Camlin Hotel (built 1926) designated Landmark
- 21 Future Mixed use building (AVA 39 floors)





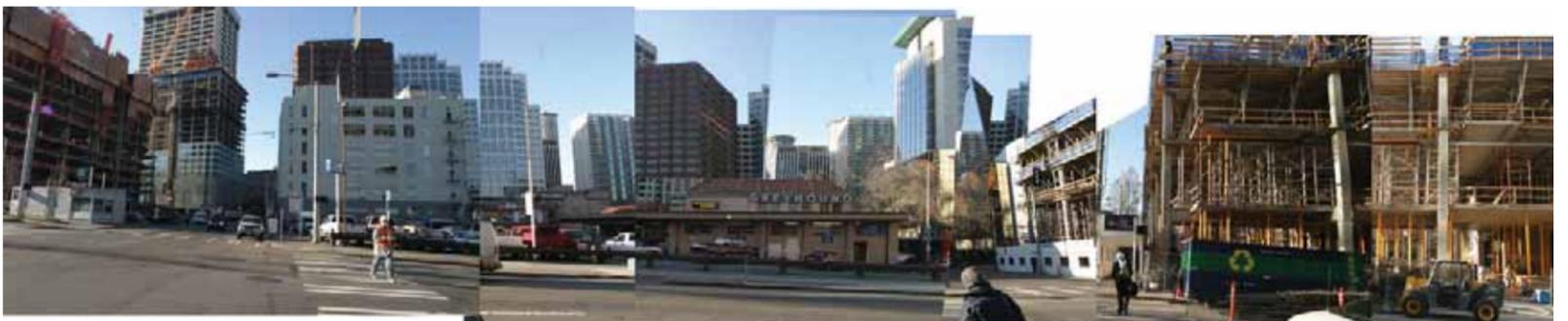
Howell St. looking North



Howell St. looking South



9th Ave. looking East



9th Ave. looking West



8th Ave. looking East



8th Ave. looking West

Land use info: DOC2

EXISTING SITE AREA: 75,599 SF
TAX PARCEL NUMBERS
0660000740
0660000735
0660000725

PROPOSED SITE AREA: 76,299 SF
GENERAL PROVISIONS FROM SEATTLE LAND USE CODE

ZONING
DOC2 500/300-500

FLOOR AREA RATIO: 23.49.011
BASE FAR : 5
MAX FAR: 14

TOTAL ALLOWABLE FAR
EXISTING 1,058,386 SF
PROPOSED 1,068,186 SF

ALLOWABLE HEIGHT: 500 FEET 23.49.008
EXCEPTIONS INCLUDE
4' ABOVE LIMIT FOR RAILINGS, PLANTERS, SKYLIGHTS, PARAPETS...
7' ABOVE LIMIT FOR SOLAR COLLECTORS
55% ROOF AREA MAY EXCEED HEIGHT LIMIT AS ALLOWED BELOW

15' FOR STAIR PENTHOUSE
23'-25' FOR ELEVATOR OVERRIDE (<8' CAB VS. >8' CAB)
50' FOR FLAGPOLE

STREET LEVEL USE:23.49.009
MAP 1GM
REQUIRED ON 8TH AND STEWART

OPEN SPACE: 23.49.016B
NOT REQUIRED FOR ACCESSORY OFFICE USE

OVERHEAD WEATHER PROTECTION AND LIGHTING: 23.49.018
CONTINUOUS OVERHEAD WEATHER PROTECTION REQUIRED ALONG ENTIRE FRONTAGE (WITH EXCEPTIONS) 5' FROM PROPERTY LINE OR WIDENED SIDEWALK.

PARKING: 23.49.019
ACCESSORY USE PARKING ALLOWED AT 1/1000 SF
ADDITIONAL PARKING PERMITTED PURSUANT TO 23.76 SMC
BICYCLE PARKING: .05 SPACES PER HOTEL ROOM, 1 / 5000 GROSS SF OFFICE AND RETAIL

OFF STREET LOADING:23.54.030
9 SPACES REQUIRED BY CODE

CURB CUT LOCATION:
LOCATION LISTED IN ORDER OF PREFERENCE
ALLEY
CLASS I PEDESTRIAN STREET
PRINCIPAL TRANSIT STREET
GREEN ST. (GENERALLY NOT ALLOWED)

SIDEWALK WIDTHS MAP 1C 23.49.022
HOWELL STREET : 12 FEET
STEWART STREET: 18 FEET
8TH AVENUE: 15 FEET
9TH AVENUE: 12 FEET

VIEW CORRIDOR REQUIREMENTS: 23.49.024
N/A

PROPERTY LINE FAÇADES: NOT REQUIRED – MAP 1H

COMBINED LOT DEVELOPMENT: 23.49.041
THE BASE FLOOR AREA AND MAX. FLOOR AREA FOR ONE OR MORE LOTS MAY BE INCREASED UP TO THE TOTAL COMBINED MAXIMUM FLOOR AREA COMPUTE FOR ALL LOTS PARTICIPATING IN THE COMBINED LOT DEVELOPMENT.

STREET FAÇADE

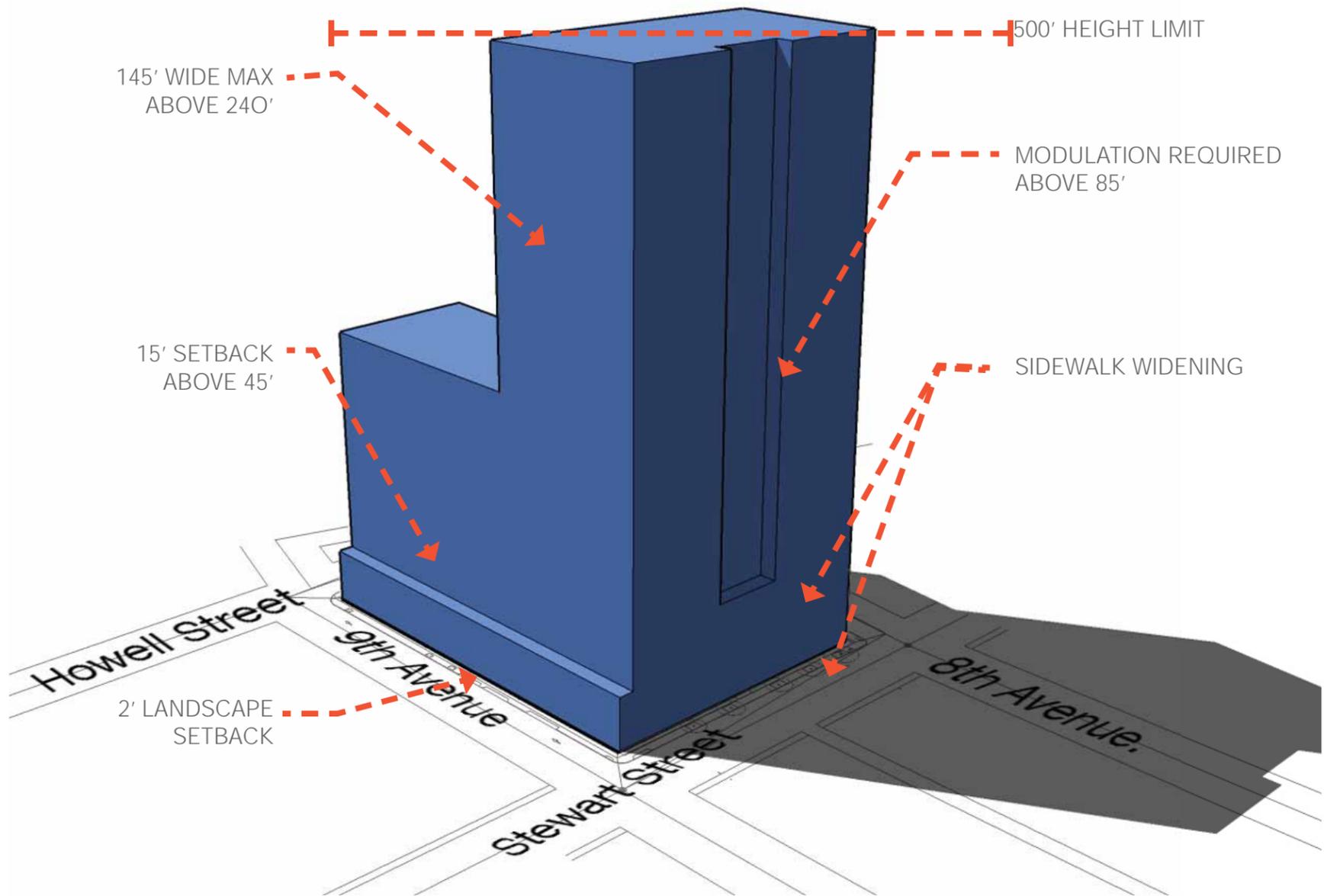
MINIMUM FAÇADE HEIGHT 23.49.056A
9TH AVENUE, HOWELL STREET – 25 FEET
8TH AVENUE – 35 FEET
STEWART STREET – 35 FEET

FAÇADE SETBACK LIMITS 23.49.056
APPLY TO FAÇADE BETWEEN ELEVATION OF 15' ABOVE GRADE AND MIN. FAÇADE HEIGHT REQ.
MAX. AREA OF SETBACKS BETWEEN LOT LINE AND FAÇADE AT STREET FRONT NOT TO EXCEED AREA OF WIDTH OF STREET X 5 ON CLASS I PED. AND 10 ON CLASS II PED AND GREEN STREETS.
HOWELL = 10X132' = 1320 SF
9TH = 10X 351 = 3510 SF
8TH = 5X 208=1040 SF
STEWART = 5X260=1300 SF
(EXTERIOR PUBLIC OPEN SPACE THAT MEETS AMENITY STANDARDS NOT APPLICABLE)

MAXIMUM WIDTH OF SETBACK GREATER THAN 15' FROM PROPERTY LINE NOT TO EXCEED 80' OR 30% OF STREET FRONTAGE AT PROPERTY LINE

MAXIMUM SETBACK OF FAÇADE AT STREET PROPERTY LINE AT INTERSECTION SHALL BE 10' FOR MIN. OF 20'

FAÇADE TRANSPARENCY REQUIREMENTS 23.49.056
AREA BETWEEN 2 AND 8 FEET ABOVE THE SIDEWALK
CLASS I PEDESTRIAN STREETS AND GREEN STREETS:
60% OF THE FAÇADE SHALL BE TRANSPARENT
CLASS II PEDESTRIAN STREETS: MIN. 30% OF THE STREET LEVEL FAÇADE SHALL BE TRANSPARENT



BLANK FAÇADE LIMITS: 23.49.056

CLASS I PEDESTRIAN & GREEN STREETS: 15' WIDE MAX EXCEPT FOR GARAGE DOORS. MAY BE INCREASED TO 30' IF ENHANCED W/ DETAILING, ARTWORK, LANDSCAPING ETC.

BLANK FACADES TO BE SEPARATED BY AREAS 2' WIDE MIN.

MAX OF ALL BLANK FACADES SHALL NOT EXCEED 40% OF THE STREET FAÇADE.

CLASS II PEDESTRIAN STREETS: MAX 30' WIDE EXCEPT FOR GARAGE DOORS. MAY BE INCREASED TO 60' IF ENHANCED W/ DETAILING, ARTWORK ETC. WIDTH OF GARAGE DOORS LIMITED TO WIDTH OF DRIVEWAY PLUS 5'.

BLANK FACADES TO BE SEPARATED BY AREAS OF 2' WIDE MIN.

MAX OF ALL BLANK FACADES SHALL NOT EXCEED 70% OF THE STREET FRONTAGE.

LANDSCAPING 23.49.56

20% OF AREA ABUTTING STREET PROPERTY LINE, NOT COVERED BY A STRUCTURE WITH A DEPTH OF 10' MIN. LARGER THAN 300 SF SHALL BE LANDSCAPED.

9TH AVE. SETBACK: IN ADDITION TO THE REQUIREMENT ABOVE, A 2' WIDE SETBACK FOR LANDSCAPING (AVERAGING ALLOWED BY DIRECTOR) 50% OF WHICH MUST BE LANDSCAPED.

UPPER LEVEL DEVELOPMENT

FAÇADE MODULATION: 23.49.058

REQUIRED ABOVE 85' WITHIN 15' OF A STREET PROPERTY LINE.

SEPARATION OF MODULATION IS 60 FEET MIN. AT LEAST 15' FROM PROPERTY LINE.

0-85 FEET – NO LIMIT

86-160 FEET – 155 FEET

161-240 FEET - 125 FEET

241 – 500 FEET – 100 FEET

MAXIMUM TOWER WIDTH: 23.49.058

ABOVE 240' SHALL BE 145 FEET PARALLEL TO THE AVENUES (NORTH SOUTH)

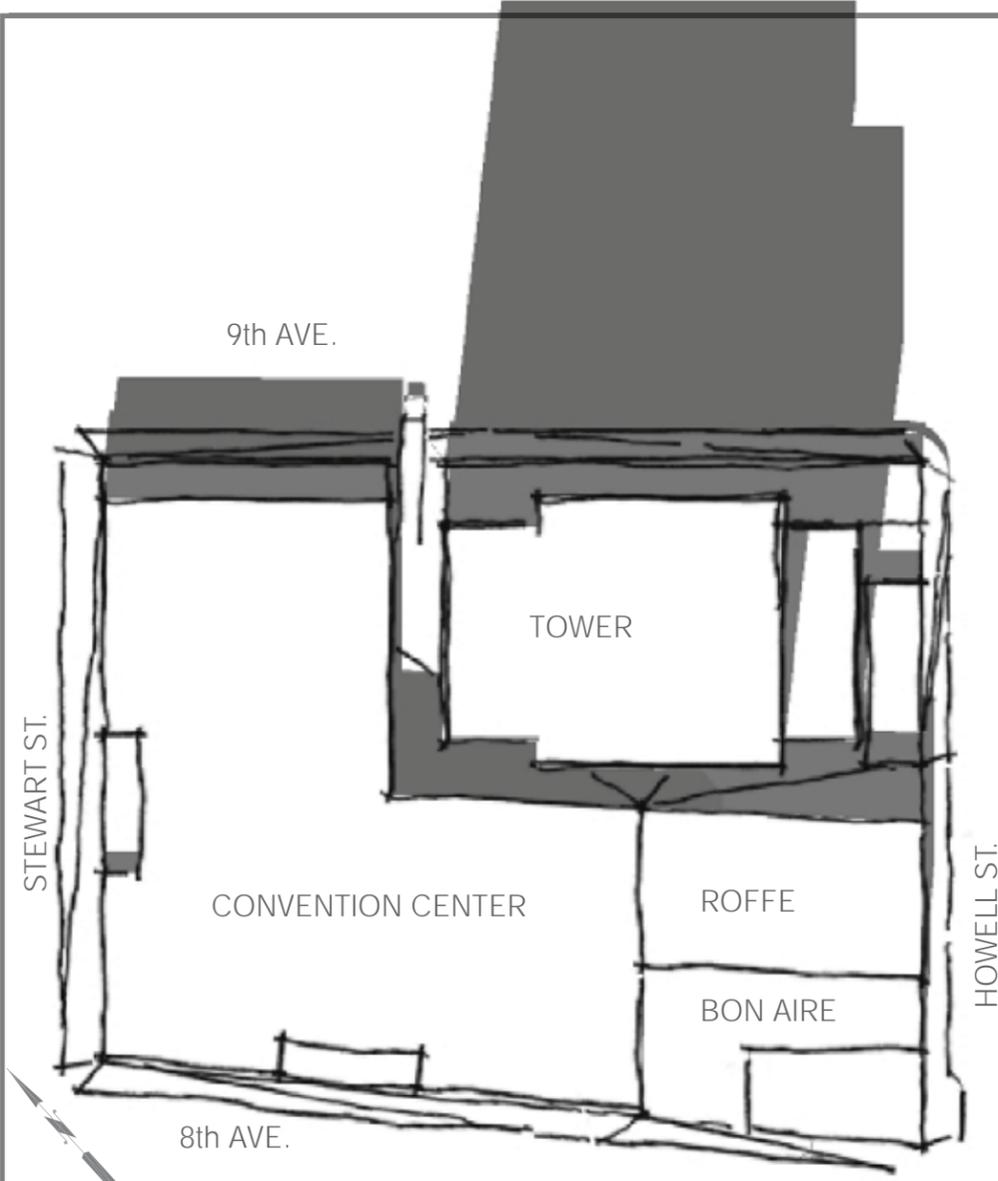
UPPER LEVEL SETBACKS: 23.49.058

SETBACK OF 15' ABOVE 45' SHALL BE PROVIDED ALONG 9TH AVENUE.

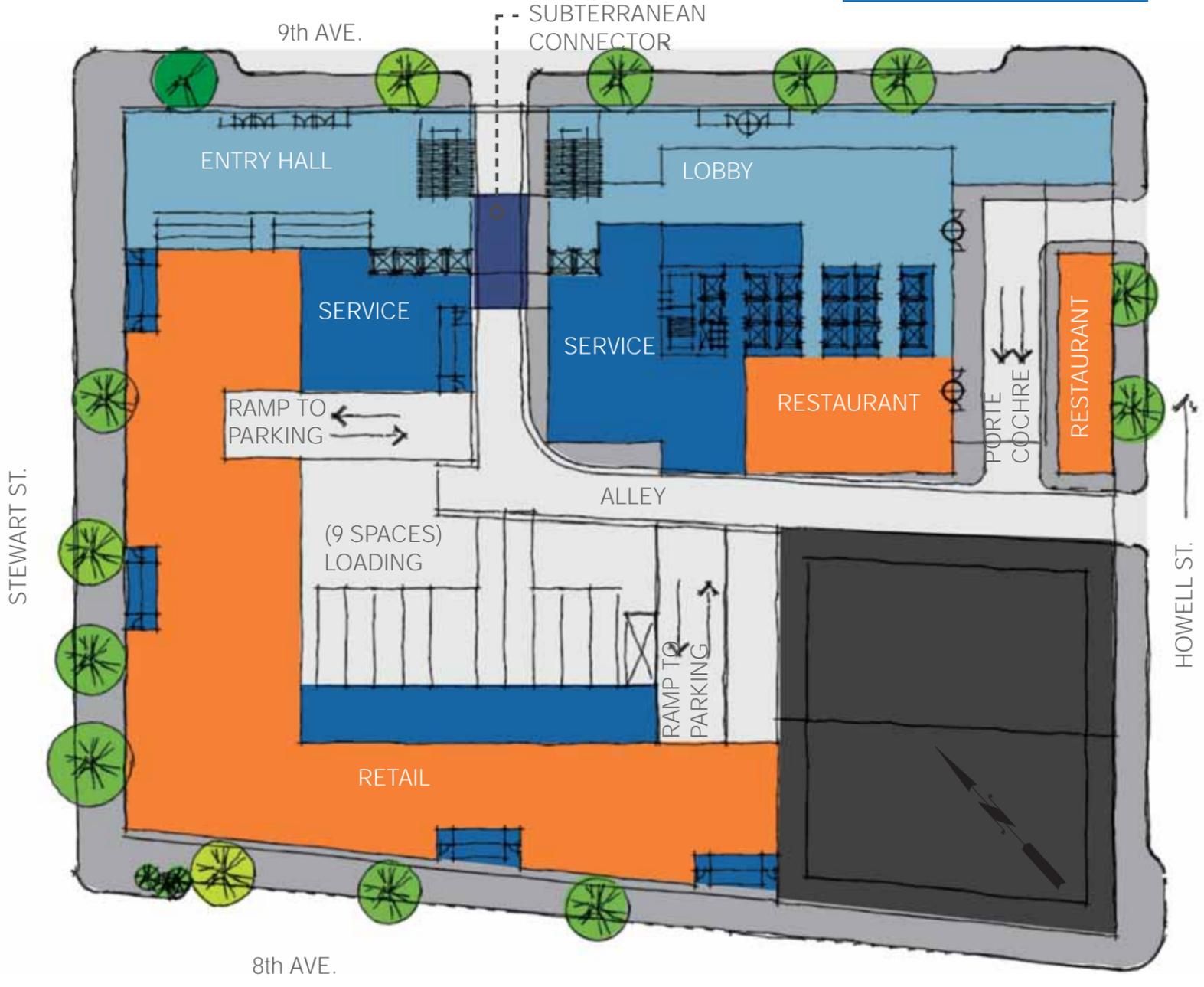
Massing Option 1: No departures

Description

The program requires large column free space for the convention / exhibition and ballroom functions. This structural requirement led in all three options to separating the hotel tower element from the exhibition space. Working with the site as it exist, the programmatic needs could be accommodated most efficiently by locating the tower on the corner of Stewart and Howell. The long span convention and meeting room space is stacked above ground level retail on the parcels of land bound by 9th Ave, Stewart St. and 8th Ave. The Porte Cochere function where hotel guests are dropped off and picked up, is located on Howell, and ties into the alley. Loading and below grade parking are accessed from the alley, which connects to 9th Avenue, a green street. The program for a convention hotel requires a direct interior connection for both the public and operational services between the hotel and the Exhibition space. Without seeking an alley vacation, this must be accomplished with a below grade tunnel.



GROUND FLOOR PLAN

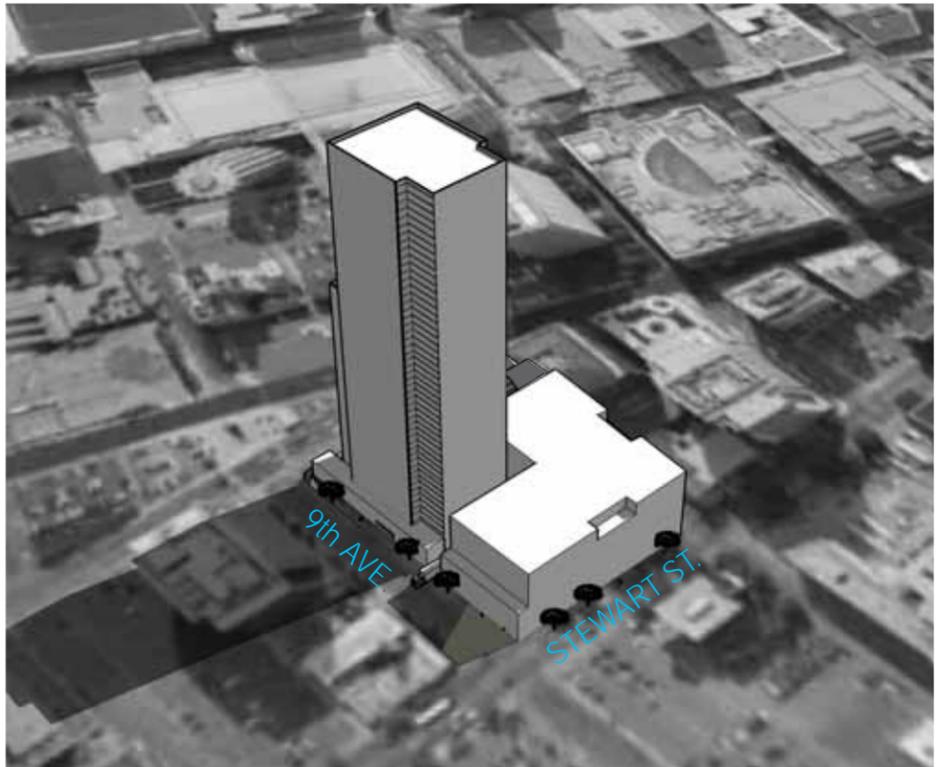


Analysis

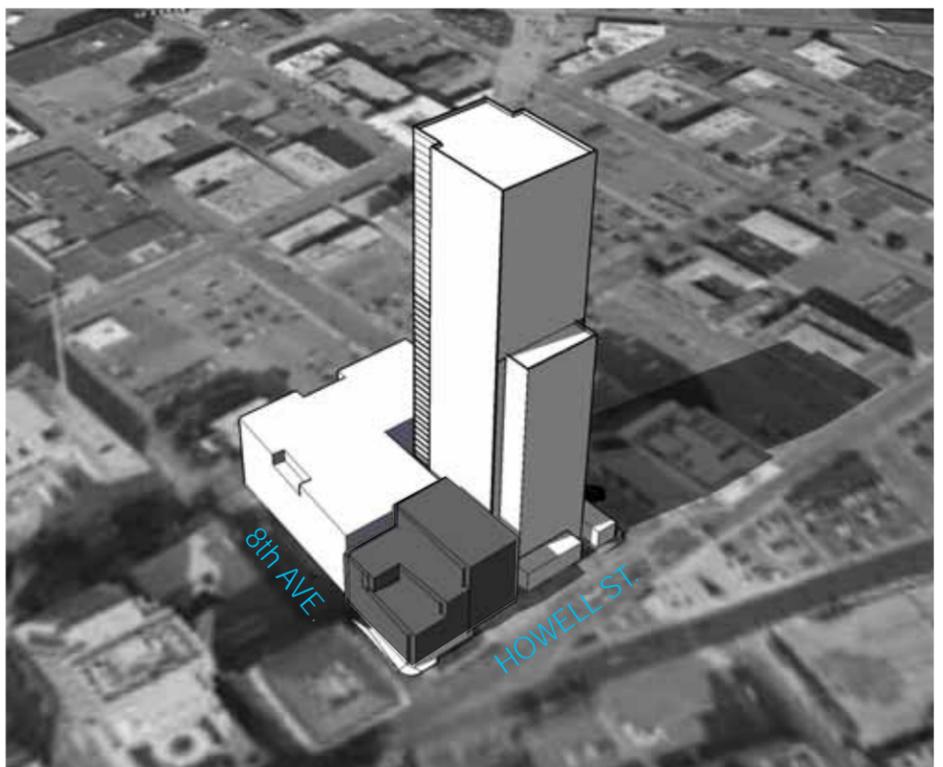
- The location of the existing alley and curb cut on 9th Avenue contradicts the vision of a green street being a space that focuses on the experience of the pedestrian over that of the automobile. Pedestrians on the Green Street are exposed in this scheme to a drastically increased number of moving vehicles entering and exiting the parking garage from the alley, as well as large Semi trucks up to 62' in length accessing the loading area. This provides a safety concern for pedestrians, limits opportunities for landscaping and on street parking along 9th Ave. and interferes with uses which promote pedestrian interaction.
- The subterranean tunnel connecting the convention space and the hotel lobby further deadens the street environment by limiting pedestrian interaction with the building. Activity is disconnected from the street environment along 9th Avenue, a green street, affecting the vitality of the streetscape. The tunnel connection also creates logistic complications in servicing the convention and exhibition space from the hotel operations, and complicates the first floor parking plate, losing a great deal of efficiency.
- The capacity for convention exhibition and meeting room space is greatly diminished due to the limitations of the alley in it's existing location, rendering this program largely unusable for the site.
- The Porte Cochere as shown does not provide adequate queuing for vehicles which may cause back ups on Howell, a Principal Transit Street. Another option of the Porte Cochere is to connect to 9th Avenue, thus lengthening the queuing area, but further aggravating the issue of vehicles dominating the streetscape along a designated green street.
- The location of the tower at the south east corner of the site puts the 9th Avenue street in shadow most of the day, limiting growth opportunity for landscaping on the Green Street.

Departures

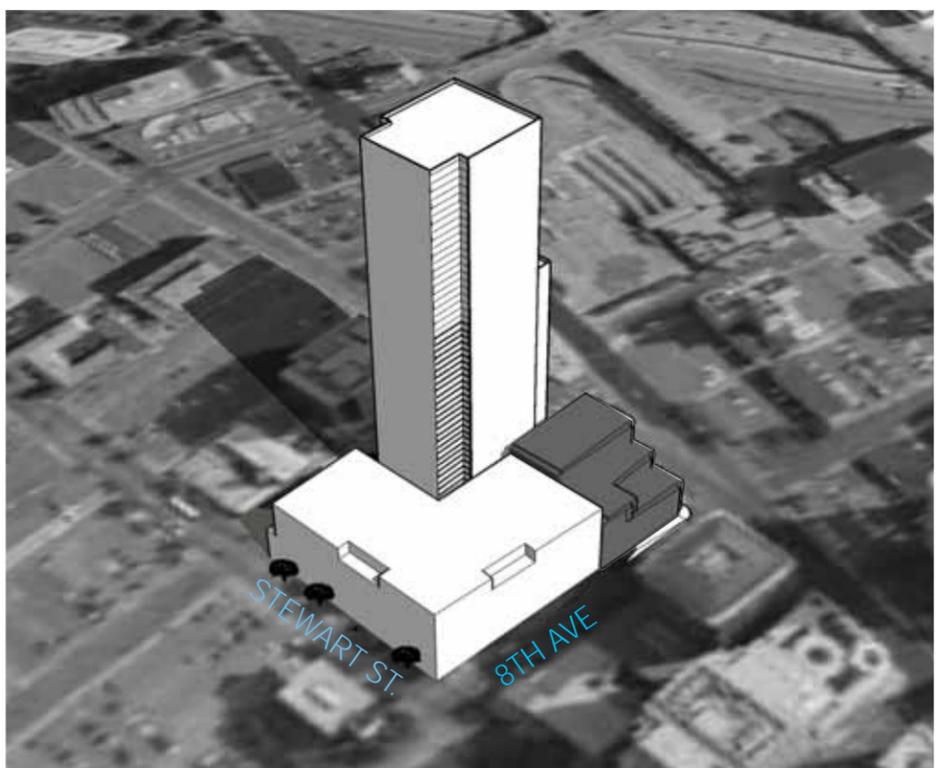
None



FROM NORTH



FROM SOUTH

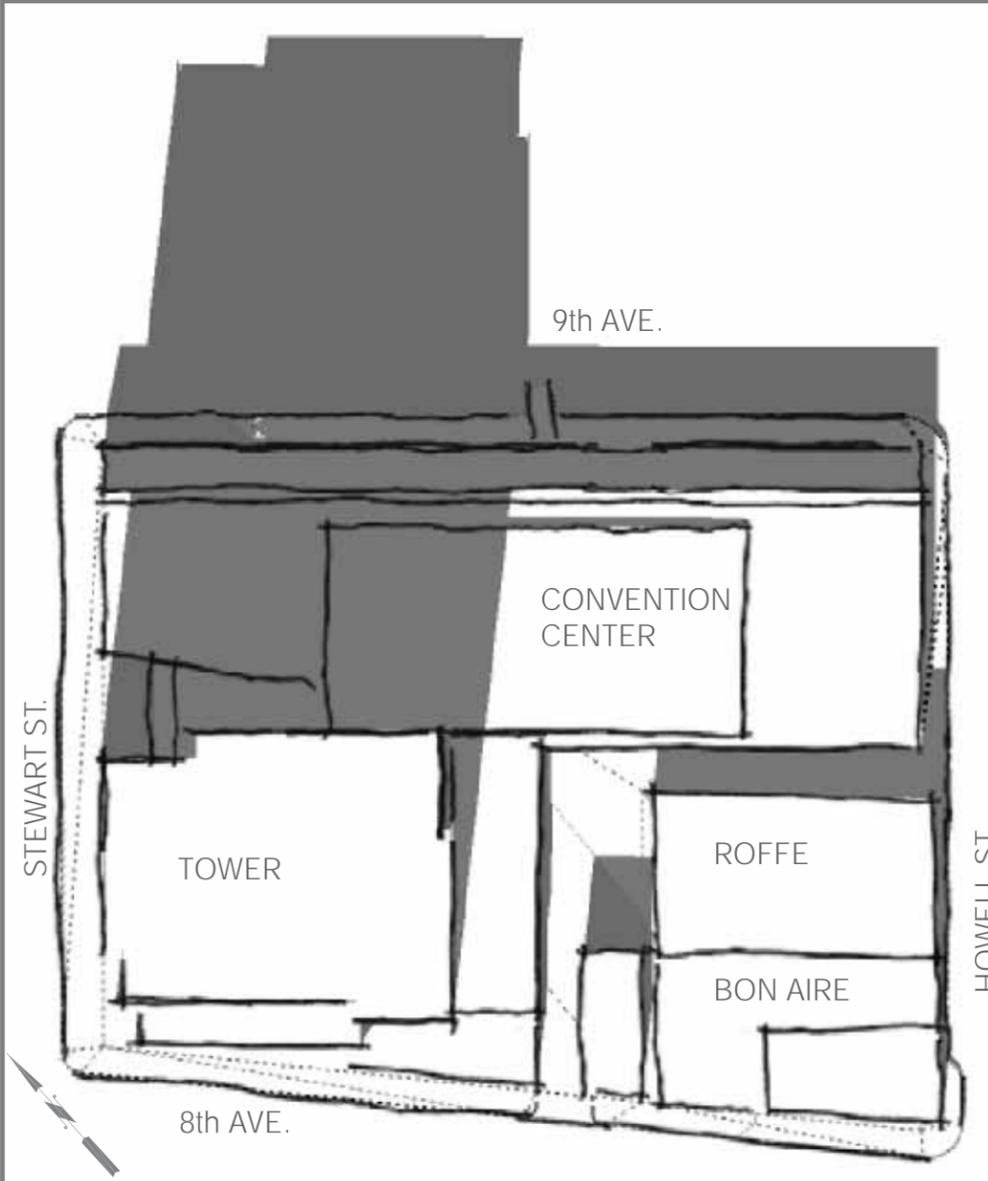


FROM WEST

Massing Option 2:

DESCRIPTION

Option 2 and 3 propose a relocation of the alley away from 9th Ave to 8th Ave. This reconfiguration of the alley eliminates the conflict of semi trucks and other service vehicles from accessing the alley functions from a Green street, and provides a contiguous parcel of land, allowing the program to function with much greater efficiency. The response to the program and the contextual site analysis in option 2 led to an architectural composition of two basic forms. The forms are defined with the convention space organized along a low linear building fronting 9th Avenue and a high rise tower housing the hotel functions, on Stewart St. and 8th Avenue. The tower is shouldered by a lower block of space to the south on 8th Avenue, housing hotel service areas and additional meeting rooms. The hotel lobby at grade on 9th Avenue and Stewart St., is fronted by the Porte Cochere. The Porte Cochere connects to the alley where the parking and loading is accessed. Additional retail and restaurant space are located at grade.



Analysis

- The location of the Porte Cochere on 9th Ave. separates the interior activity from the Green street, interrupting the continuity and vitality of the streetscape.
- Traffic is still able to access the parking garage entries through the Porte Cochere, further degrading the pedestrian experience along this street.

Departures & Explanation

1. SMC 23.49.56 Façade Setback Limits

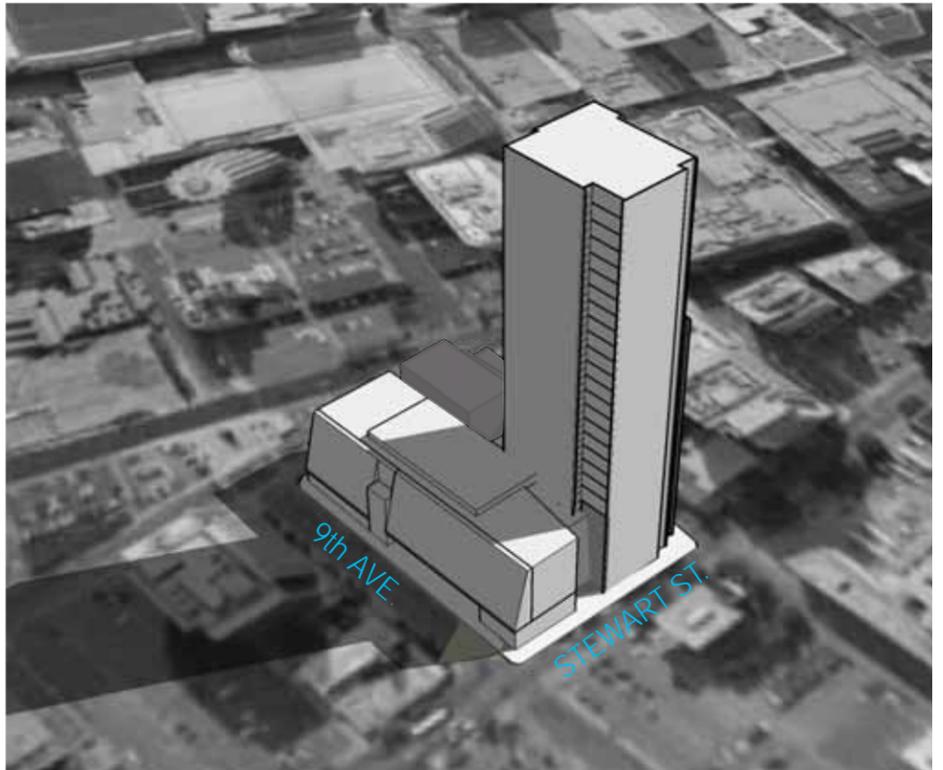
- The façade on 9th Ave. the Porte Cochere does not conform to façade setback limits for both width and area.
- The corner of 8th and Stewart does not conform to the code requirement of façade within 10' of the property line at the corner.

2. SMC 23.49.58 Upper Level Development Standards

- The tower exceeds the upper level development standard of a maximum width of 145' above 240' by 5'.
- A continuous setback requirement of 15' above 45' on 9th Ave. is exceeded.

3. SMC 23.54.030 Off Street Loading

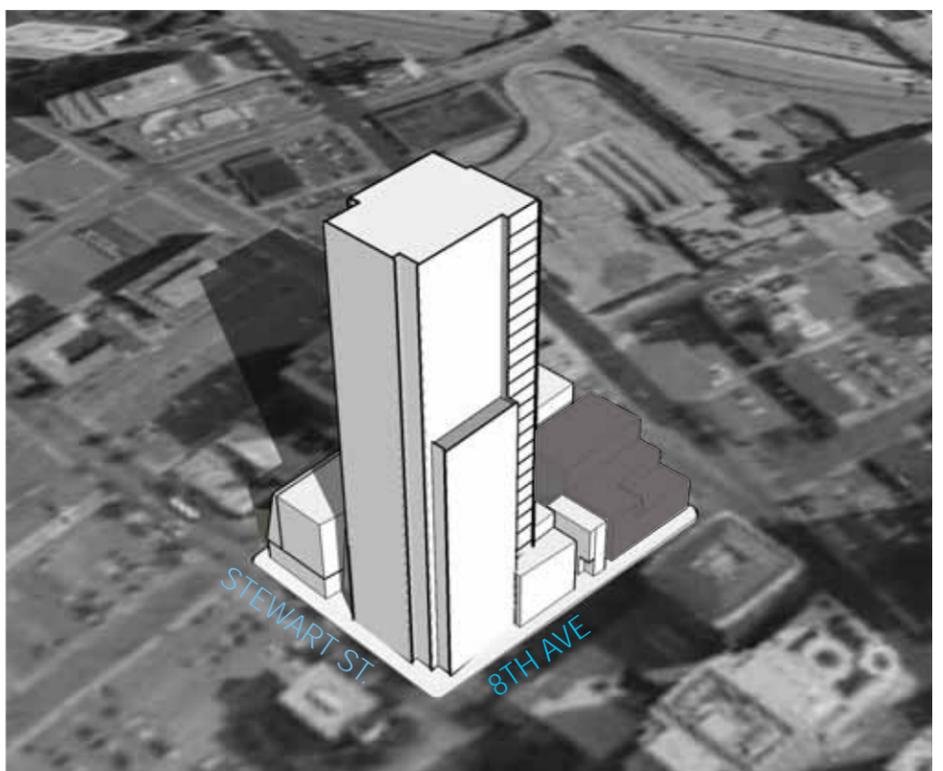
- This option does not meet the code requirement of 9 loading berths. 6 berths are provided at or above code size. 3 berths will be provided in the parking garage measuring 9'x20'.



FROM NORTH



FROM SOUTH

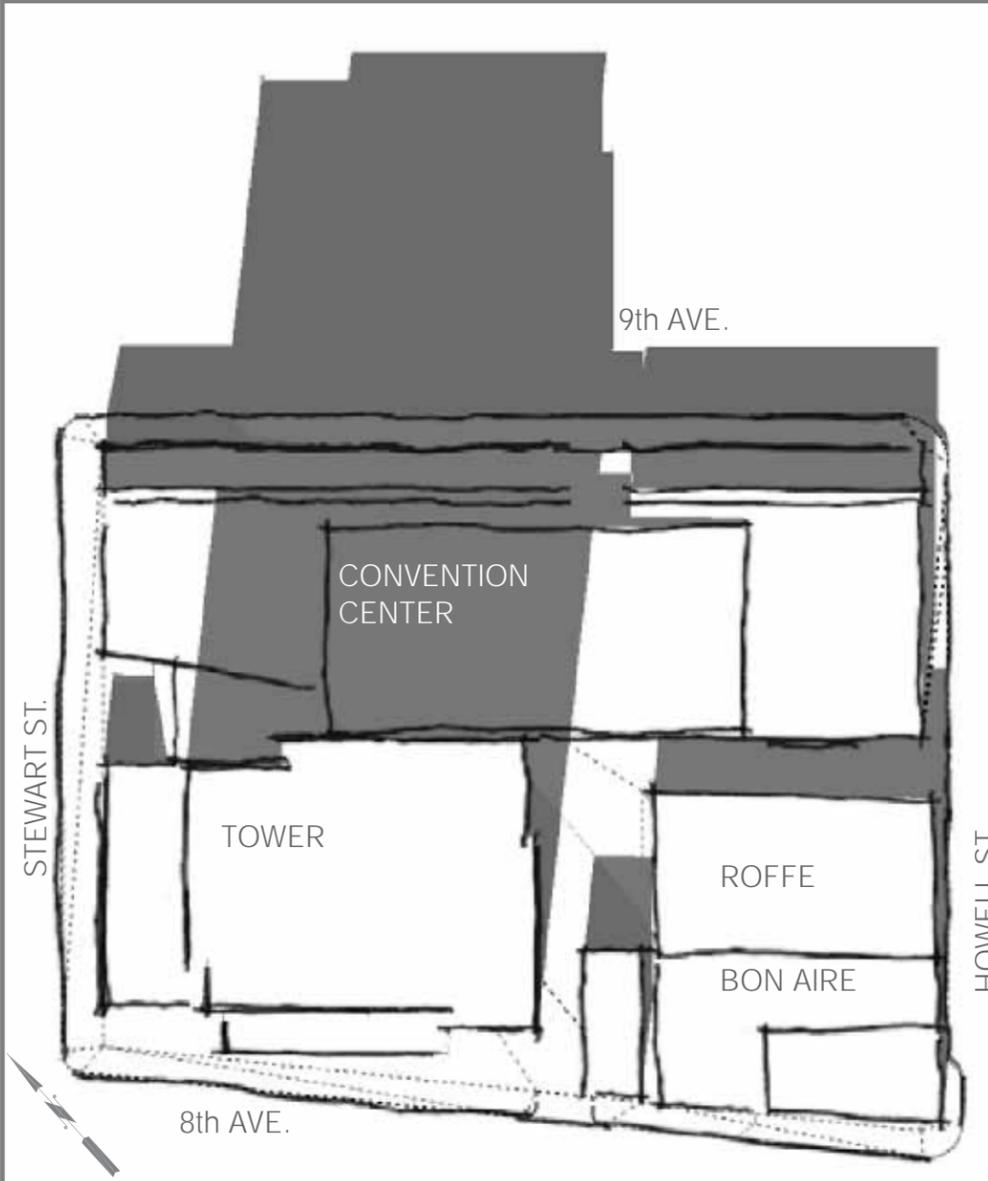


FROM WEST

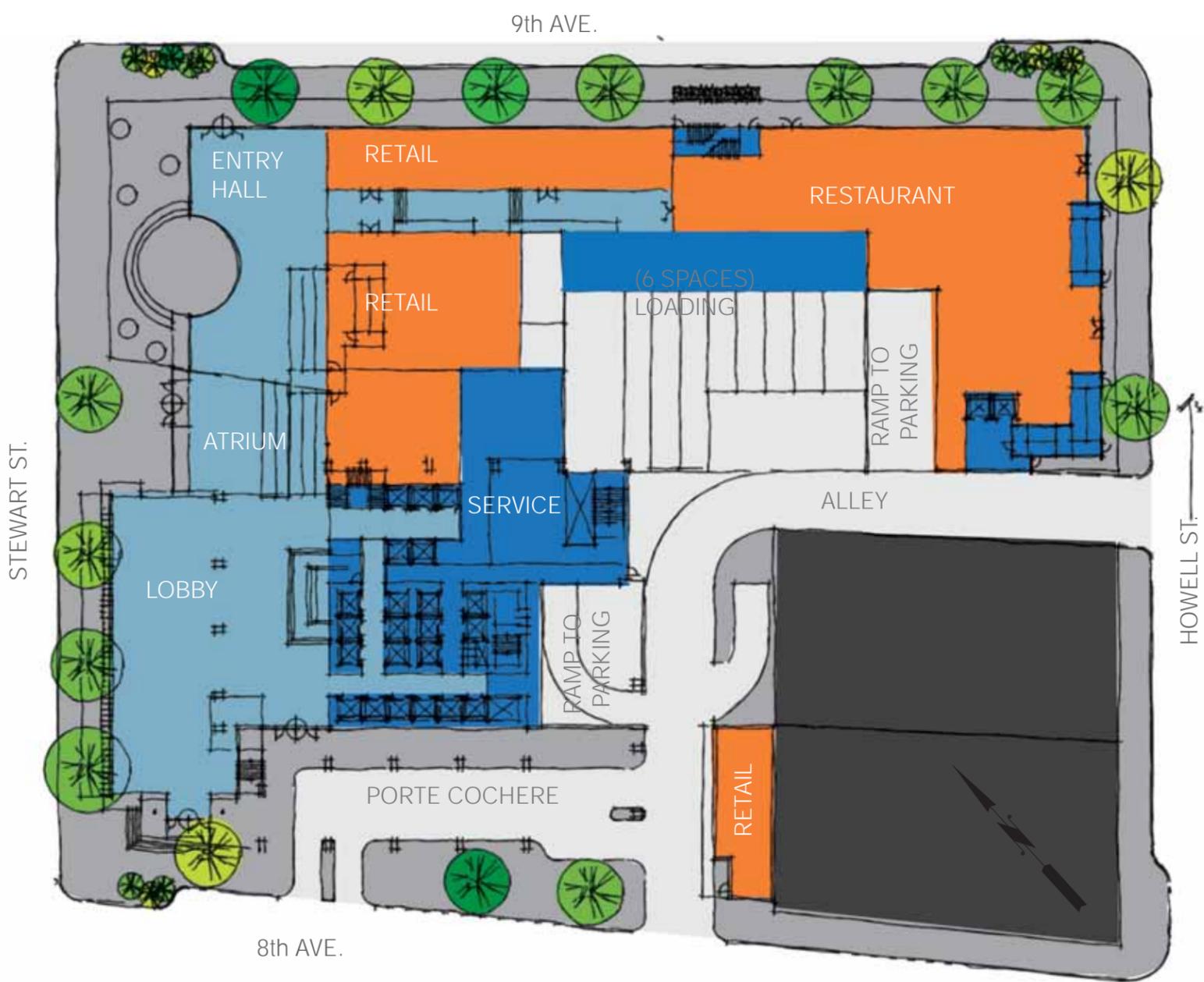
Massing Option 3: Preferred Option

DESCRIPTION

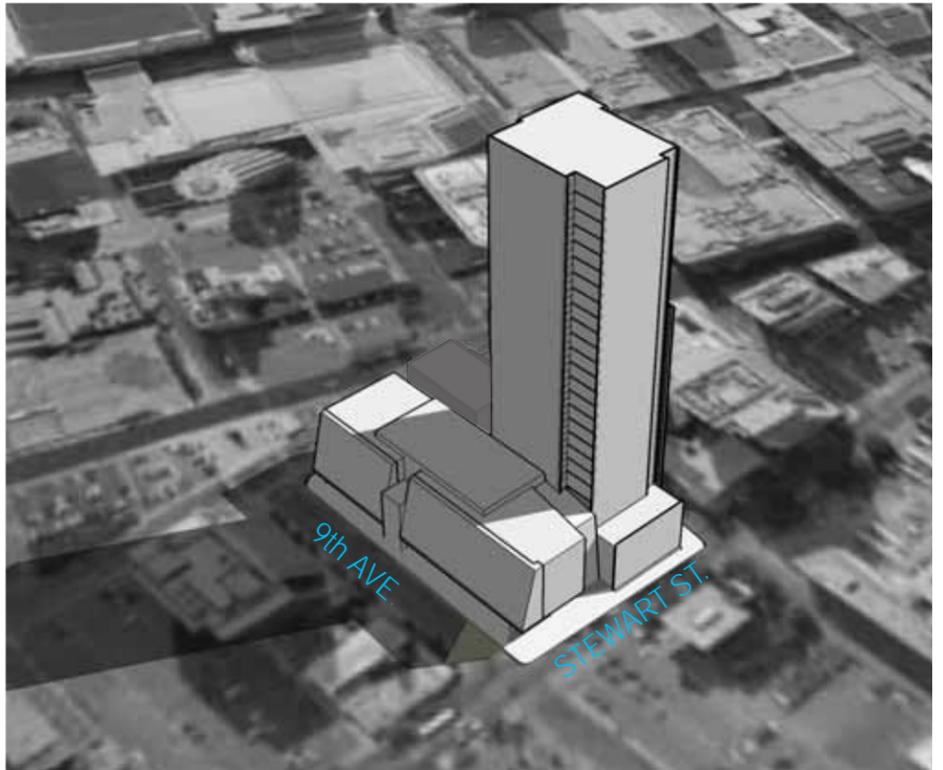
Option 3 differs from Option 2 by the location of the tower, the location of the Porte Cochere, and an increase of exterior area dedicated to enhancing the streetscape and the pedestrian experience. The hotel tower is set back from Stewart St. on 8th Ave. The tower is shouldered by a lower building mass on Stewart St., housing additional restaurant and meeting rooms with an accessible rooftop garden for outdoor banquets. The convention and exhibition space is organized in a low linear building fronting 9th Ave. The horizontal and vertical towers in this scheme are joined by a transparent atrium, a 'hall of light'. This interstitial space contains vertical circulation for the convention area, providing visual continuity and a sense of organizational logic to the meeting room floors above. The façade of the horizontal building situated on 9th Avenue is angled away from the street to increase access to day lighting for the exhibition and meeting spaces within, as well as provide additional light to the pedestrians and plantings at the street level. This façade is further articulated with a vertical green wall midblock, providing a relief in the form and massing. A spa and fitness center is located on the roof of this building, setback from the edge to allow for usable outdoor area, enhanced with additional landscaping, and further breaking down the massing along 9th Ave. The horizontal form of this building is lifted off the ground, creating a



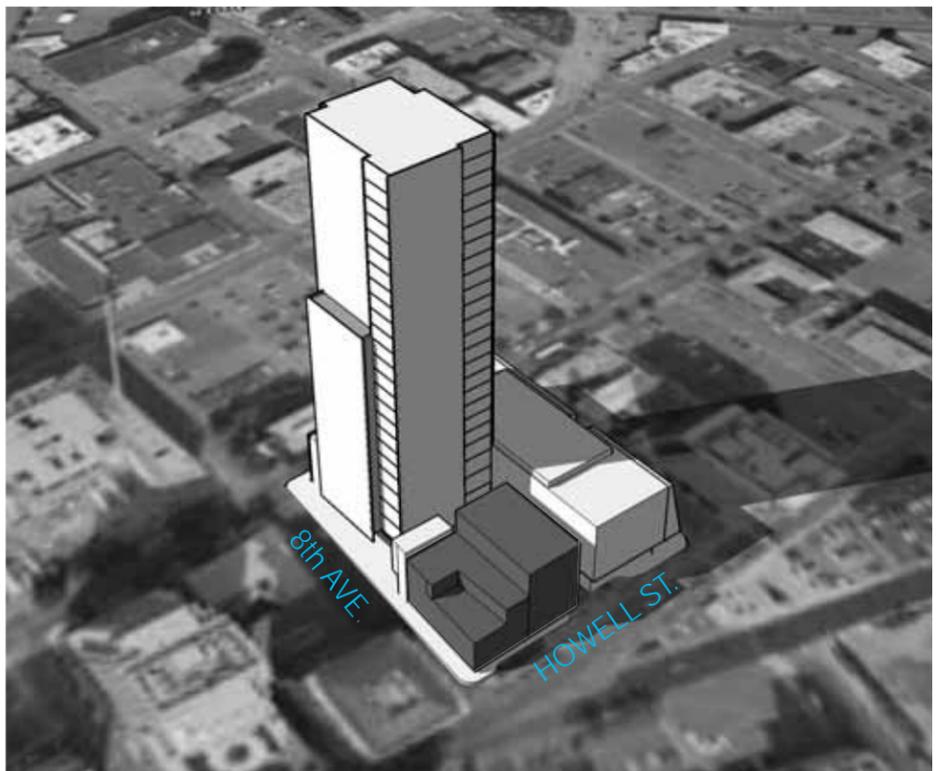
| | | |
|-------------------|--------|-----------------|
| GROUND FLOOR PLAN | RETAIL | HOTEL (PUBLIC) |
| | | HOTEL (SERVICE) |



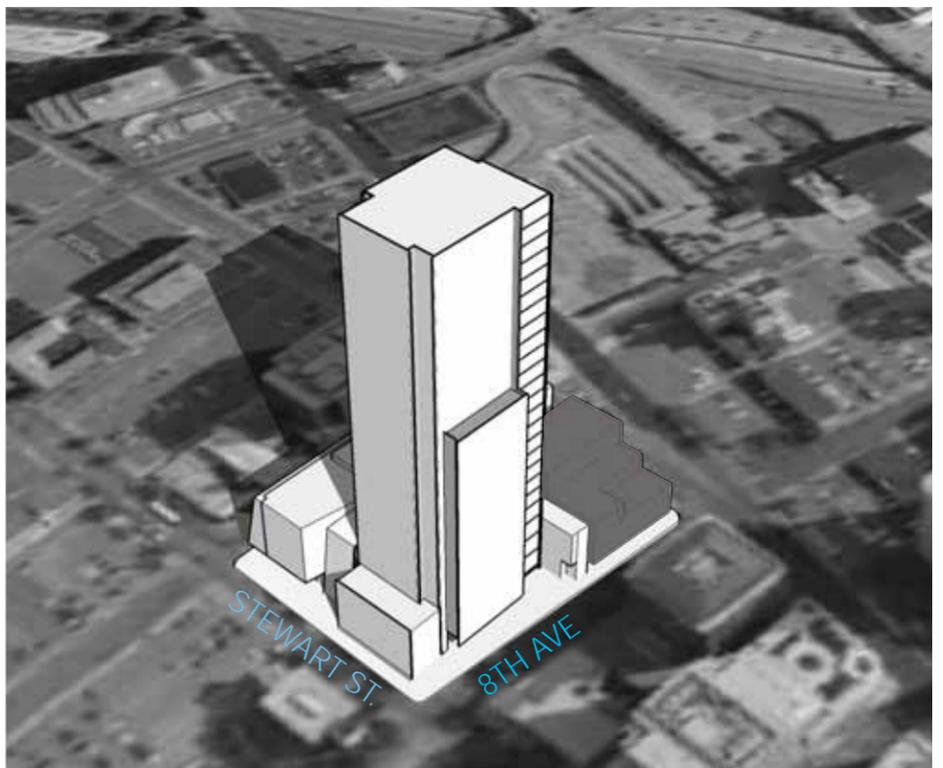
continuous linear open space for pedestrians at grade. Sitting on a transparent base allows passersby glimpses into the interior retail and restaurant spaces. Setback from the façade above, a gentle transition is created between the exterior and the interior by providing a covered walkway in which retail and dining activities may spill out onto the streetscape while allowing ample area for plantings along the green street. A public plaza on the corner of 9th and Stewart provides a respite for pedestrians in the urban fabric, allowing opportunity for gatherings to invigorate the street scene. The plaza is envisioned as a mix of covered area, and area open to the sky through an oculus cut through the convention floors above. The oculus allows additional daylight to the plaza level and provides a visual connection between people in the plaza with the activity in the convention spaces above. Rain-water runoff from the roof above would be filtered and carried through the oculus to a water feature at the plaza level, celebrating both the imagery of rain in Seattle, and providing an opportunity to educate the public about sustainable practices of recycling runoff water as it is re-circulated back through the building systems. Hotel lobby and entry spaces are also planned at grade with entrances provided at the corner of 9th and Stewart, 8th and Stewart, and midblock on Stewart, providing a high level of connectivity and interaction with the street activity. A Porte Cochere is accommodated with one curb cut on 8th Avenue, connecting to the alley to minimize adverse impacts on pedestrian safety and allowing for greater continuity of the sidewalk. The Porte Cochere, enhanced with special paving and high quality building materials and lighting, is sized to accommodate the anticipated arrival and departure traffic to eliminate congestion due to back ups on the street itself. Landscaping located between the tower and the sidewalk on 8th Ave. will serve to soften and shield the presence of the car at the Porte Cochere. Accessory parking is accessed from two entry / exits at either end of the alley to a below grade garage. Loading is situated at the turn in the alley, allowing large trucks to navigate the alley safely. Retail wraps the alley at both ends on Howell and 8th Avenue.



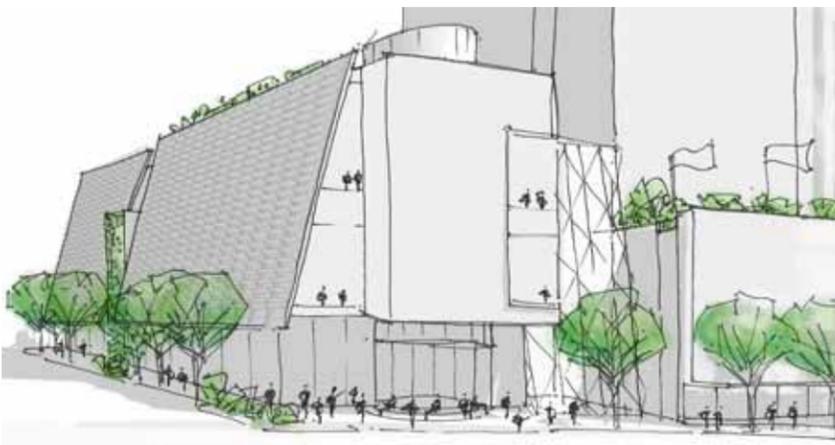
FROM NORTH



FROM SOUTH



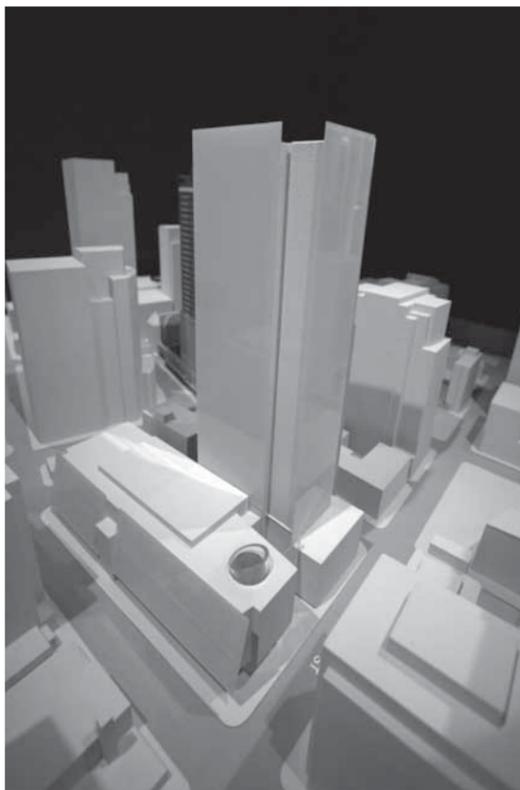
FROM WEST



Massing Option 3: Vignettes

Analysis

- Spaces are organized at the ground level to invigorate the street. Street level retail and dining is allowed to spill outside in the covered walkway and plaza areas, allowing people to become engaged with the activities of the building.
- The plaza space on 9th and Stewart provides a safe and welcoming opportunity for people to gather, infusing the street with activity.
- A water feature, artwork and or landscaping will serve to further invigorate the public plaza space, and help create a sense of place within the neighborhood.
- Significant pockets of landscaping, special paving, planters and site furniture are planned, with a special emphasis on 9th Ave, a green street.
- Building entries are provided at numerous locations at the ground level to allow a greater level of interaction with the interior spaces from the street.
- The alley façade is developed with retail storefront wrapping the corners to promote safety and to create a more interesting façade.
- A large percentage of rooftop area will be planted with green roofs, as well as large expanses of vertical gardens on 9th Avenue and the south facing Howell St. façade.
- A total of one curb cut in addition to the proposed new alley is provided for the entire $\frac{3}{4}$ block project. This design focuses on the safety of pedestrians, and allows the street façade to be continuous along Stewart, 9th and Howell.
- Parking, loading and service facilities are all access from the proposed new alley which connects Howell to 8th Ave, removing these functions and their adverse impacts from the Green street entirely.



Departures & Explanation

1. SMC 23.49.009 Street Level Use Requirements

75% of the length adjacent to 8th Ave. and Stewart St. is required to have Street level use within 10' of the property line, or a space that qualifies as a public amenity.

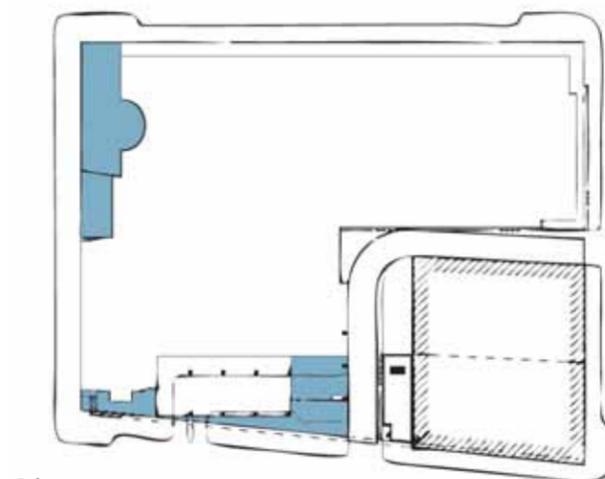
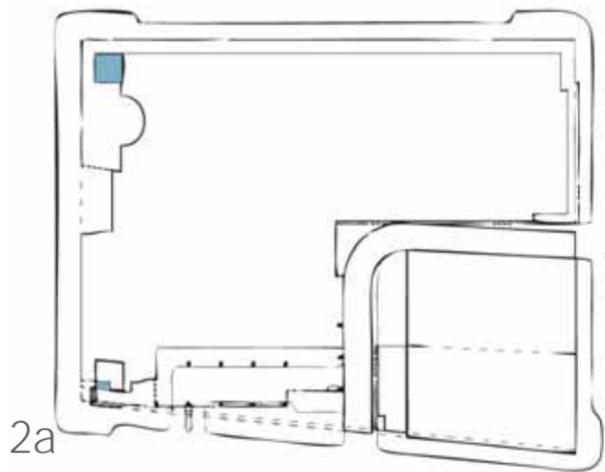
- Approximately 15% of the area parallel to 8th Ave. is proposed as Street level use within 10' of the property line. This departure is minimized by wrapping the retail storefront down the alley. 75% of the area parallel to Stewart St. is provided as Street Level Use, although a portion of the area is provided adjacent to the public plaza which does not conform to the Seattle Land Use definition of a public amenity, thus qualifying as a departure. 95% of the area along 9th Avenue and approximately 50% of the area fronting Howell is allocated to street level use within 10' of the property line, neither of which are required to have any by code.



2. SMC 23.49.56 Façade Setback Limits

Setback limits apply to the portion of the building between 15' and the min. setback height required for each street. Area that meets the Amenity Standards is not considered part of a setback. The max. area and max. width of setbacks greater than 15' from the property line are regulated. Facades shall be within 10' of a property line adjacent to an intersection for a min. length of 20'.

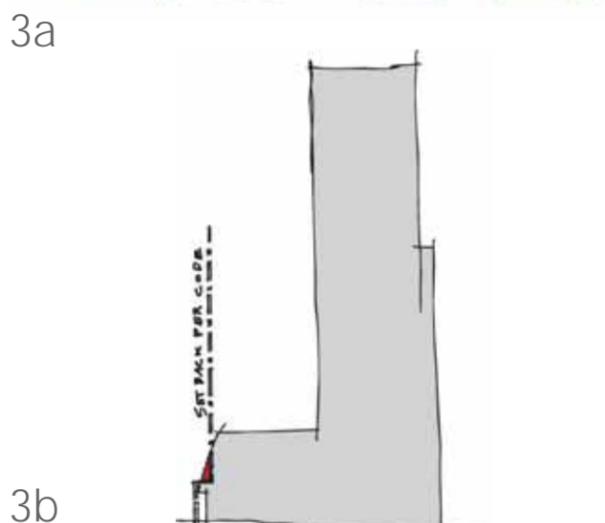
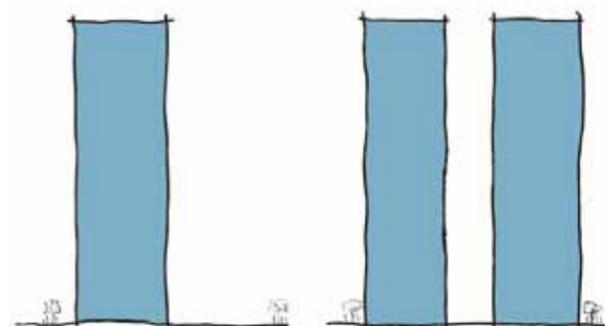
- The façades at the intersection of Stewart St. and 9th Ave. do not meet the code requirement to be within 10' of the property line, due to the public plaza. The façades at the intersection of Stewart St. and 8th Ave. do not meet the code to allow a graceful transition from sidewalk grade to the hotel lobby at the main entry.
- The area of the façade setback along Stewart St. (4028 SF) exceeds the allowable amount of 1300 SF due to the public plaza. There is no departure if the plaza area is not counted in the calculation. The area of the façade setback along 8th Ave. (3452 SF) exceeds the allowable amount of 1040 SF to allow the tower structure to meet the ground in a meaningful way. This setback also provides opportunity for additional landscaping adjacent to the Porte Cochere, screening the area from the sidewalk.
- The max. width of the façade setback provided (127') exceeds the allowable by Code (77') on Stewart. If the public plaza area is not included in the calculation, there is no departure.



3. SMC 23.49.58 Upper Level Development Standards

Tower width above 240' is restricted to 145' parallel to 8th and 9th Avenues. A continuous setback of 15' above 45' is required on 9th Ave.

- The proposed tower width is approximately 150', exceeding the restriction by 5'. The intent of the code is to minimize the impact that tall buildings have on blocking views westward to the Puget Sound. This departure is mitigated in that this is the only high rise planned on the block. A massing divided into two towers of 145' and 125' separated by the required 80' would be code compliant, but would block a far greater portion of the westerly view.
- The design proposes an angled façade on 9th that provides an increasing setback to allow greater access to sunlight at the street level. This departure is further mitigated in that the tower is located away from 9th Ave., providing an anticipated building height of approximately 90' at 15' from the property line and 105' overall on 9th Ave. compared to an allowable 500' at 15' from the property line.



4. SMC 23.54.030 Off Street Loading

9 stalls are required by Code.

- This option does not meet the code requirement of 9 loading berths. 6 berths are provided at or above code size. 3 berths will be provided in the parking garage measuring 9'x20'.

LANDSCAPING

The project will be enhanced by landscaping throughout, not only at street level, but also on rooftops and vertical surfaces. The main features are:

- 9th Avenue Green Street.
- Water Garden
- Porte Cochere
- Green Roofs/Elevated Terraces
- Green Walls

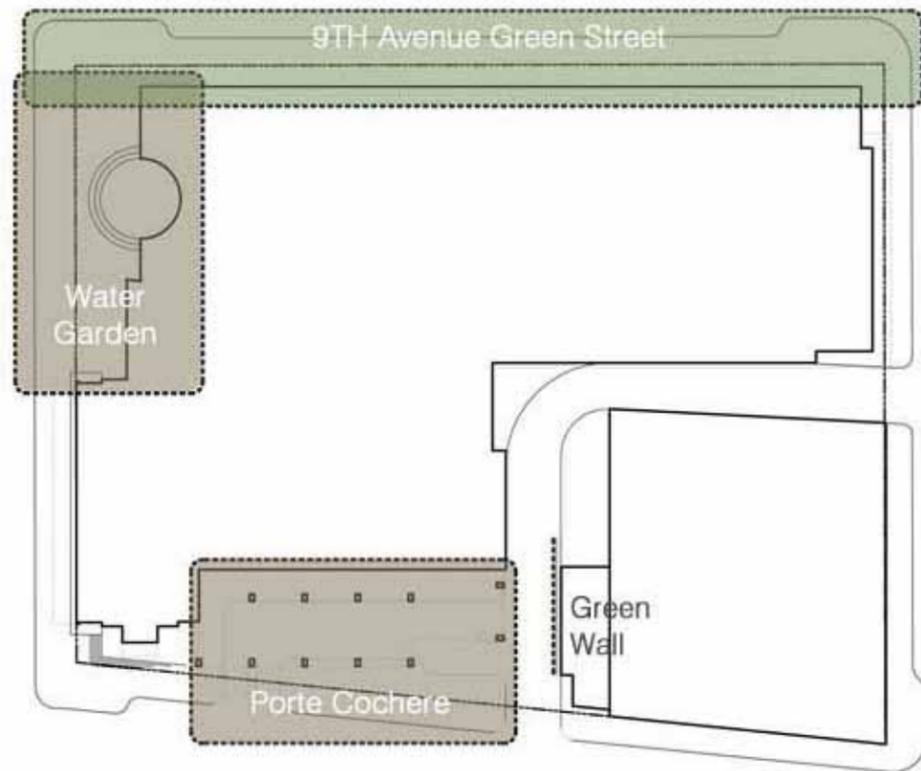
9th Avenue Green Street: The pedestrian experience will be emphasized along this frontage and serve as a continuation of the existing 9th Ave green street. Benches, expanded planting areas, and street trees will complement the ground floor retail. Planting areas may also serve as bioretention areas, reducing or eliminating the need for on-site stormwater detention.

Water Garden: This art/water feature will serve as a public amenity, a gesture to the city that highlights the site's connection to its environment. Rain water from the roof will be directed through the oculus into the street level water feature, which is open to the public. Generous seat walls and space for outdoor tables will also be provided.

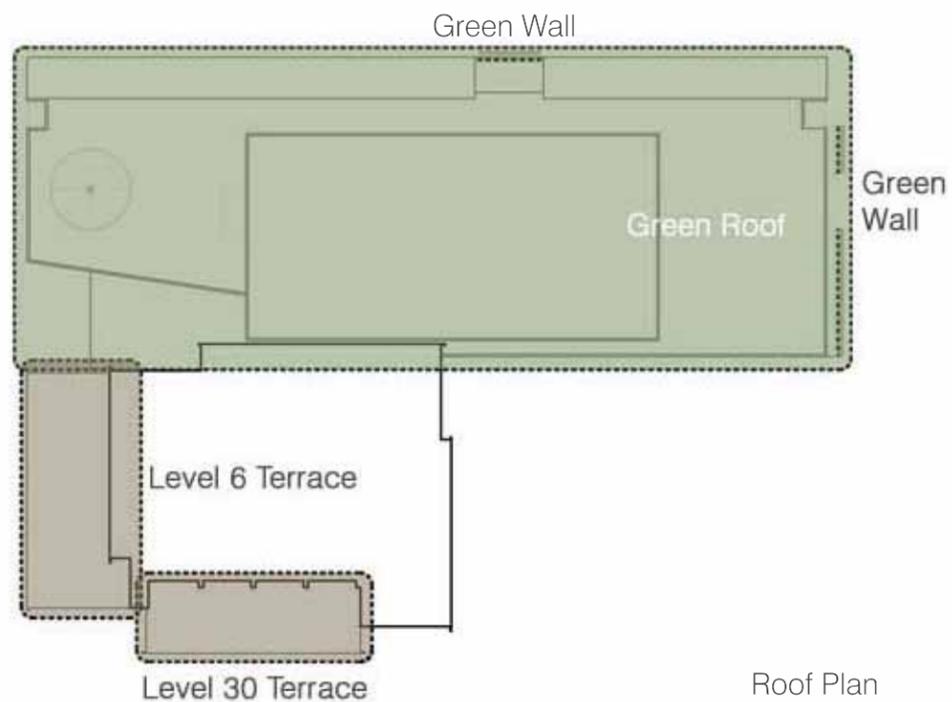
Porte Cochere: This will be the primary vehicular access point for the hotel. Special paving will run from the hotel lobby to the porte cochere and the street beyond, providing a visual connection that guides hotel guests to the entry.

Green Roofs/Elevated Terraces: Hotel events have the opportunity to expand outdoors above street level. A combination of extensive and intensive green roofs, in combination with outdoor terraces, will provide usable outdoor space, mitigate urban heat islands and filter storm water. Above the convention center and in conjunction with the spa/fitness center, a terrace will provide a connection to the outdoors and an expansive green roof will provide some visual relief to both guests and occupants of the surrounding buildings. There are also large outdoor terraces on levels 6 and 30, which will feature gathering space and containerized planting for ease of maintenance.

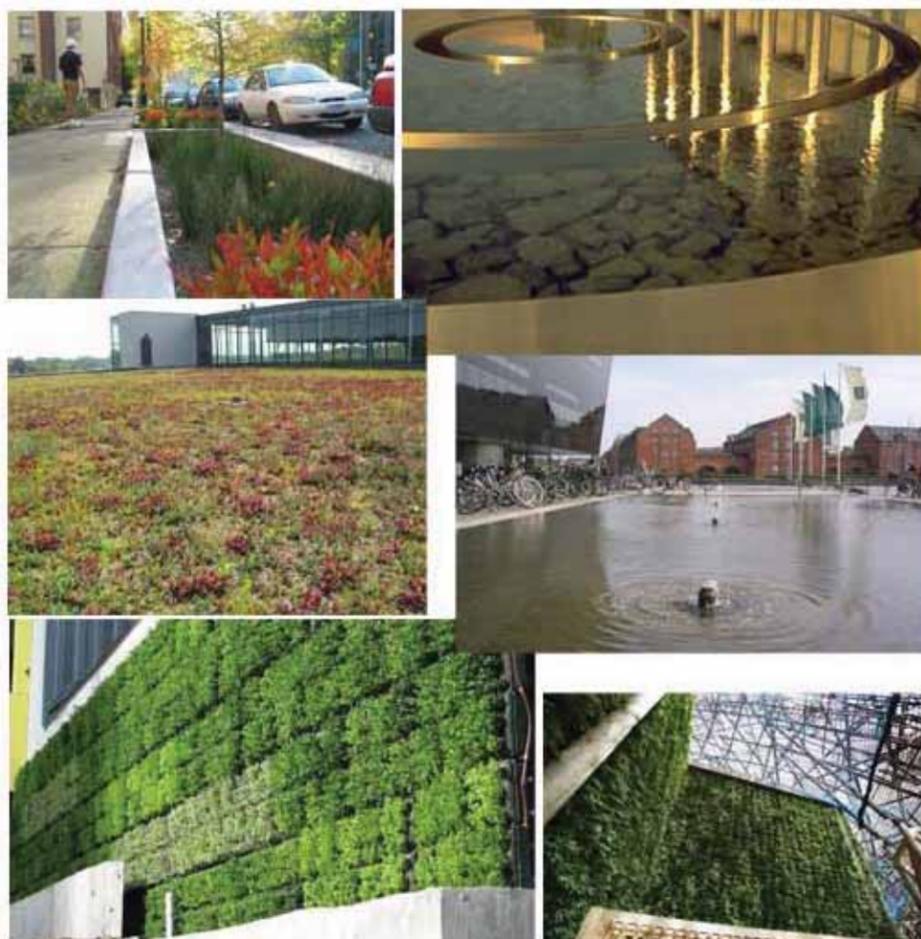
Green Walls: To provide visual relief and articulation to blank facades on Howell St., 9th Avenue and on the alley side of the small retail building, a green wall system is proposed. This will be one of the most outward, public displays of this project's commitment to sustainable building practices. The benefits, besides aesthetic, include mitigation of urban heat islands, CO2 absorption, energy efficiency and public education.



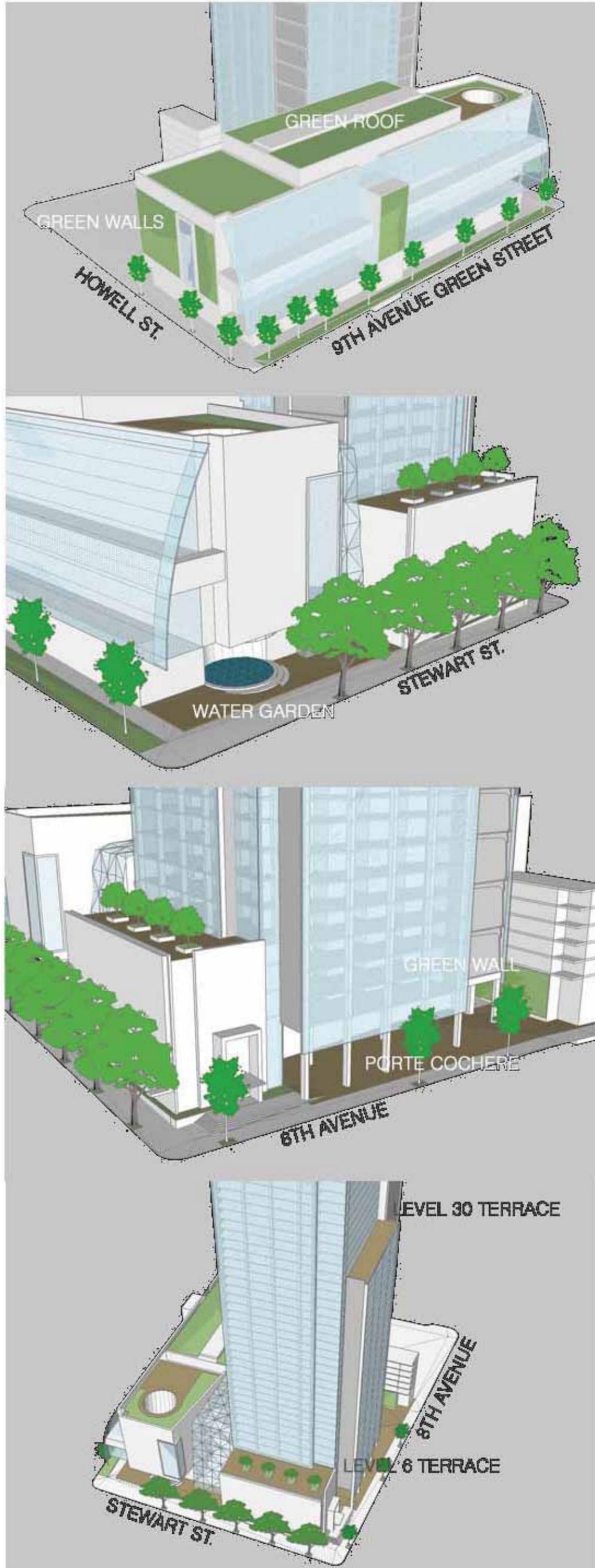
Street Level



Roof Plan

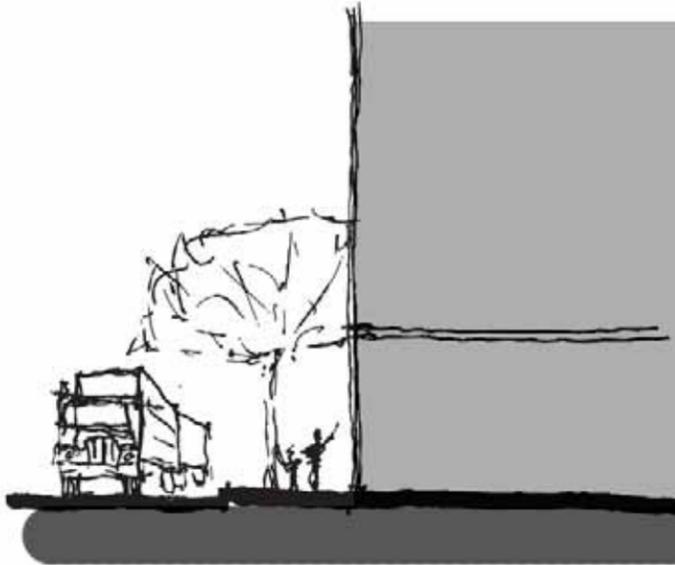


PERSPECTIVE VIEWS

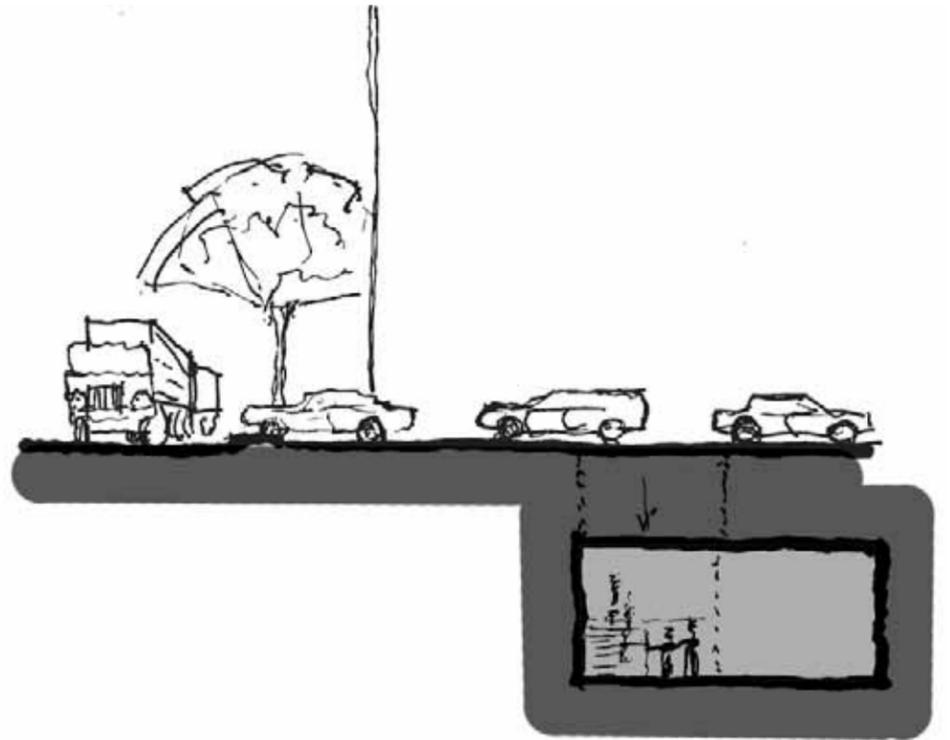


OPTION 1

A. 9TH AT STEWART

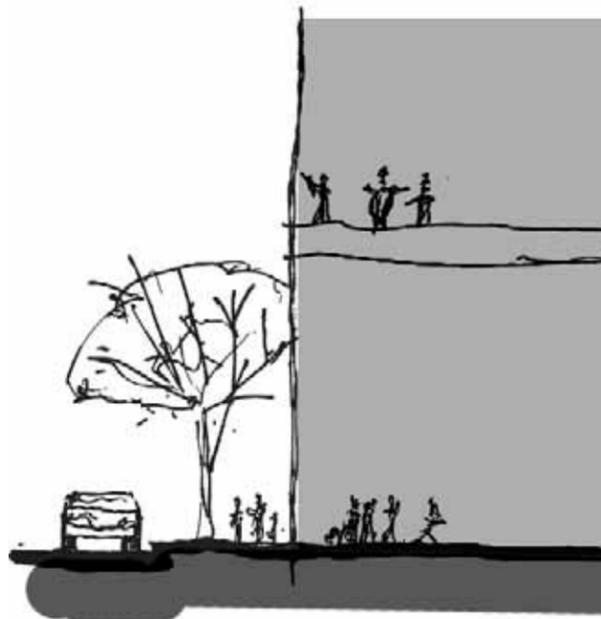


B. 9TH AT MIDBLOCK

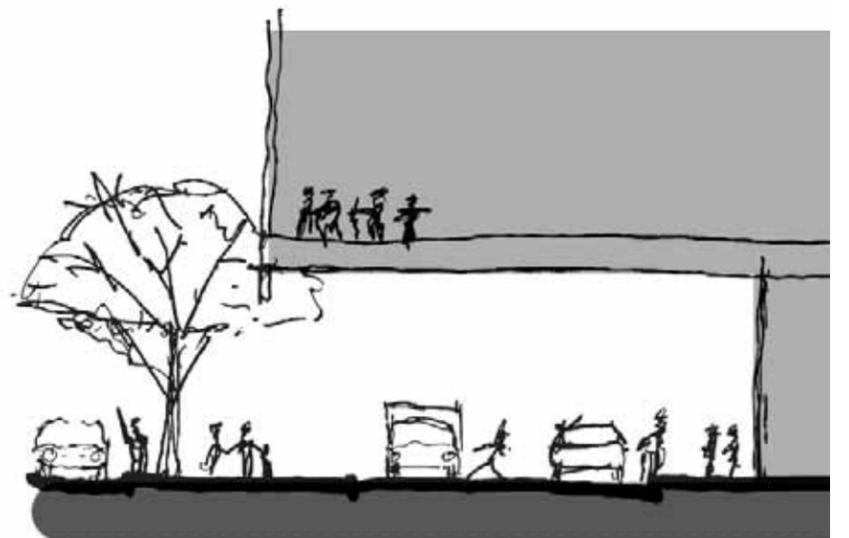


OPTION 2

A. 9TH AT STEWART

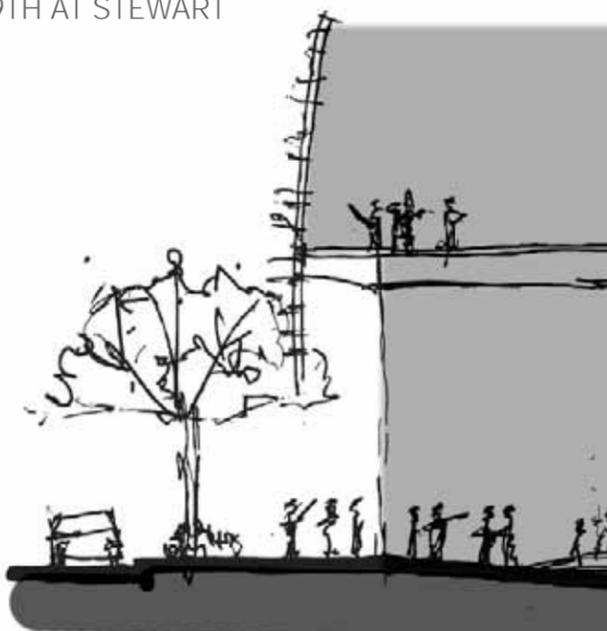


B. 9TH AT MIDBLOCK

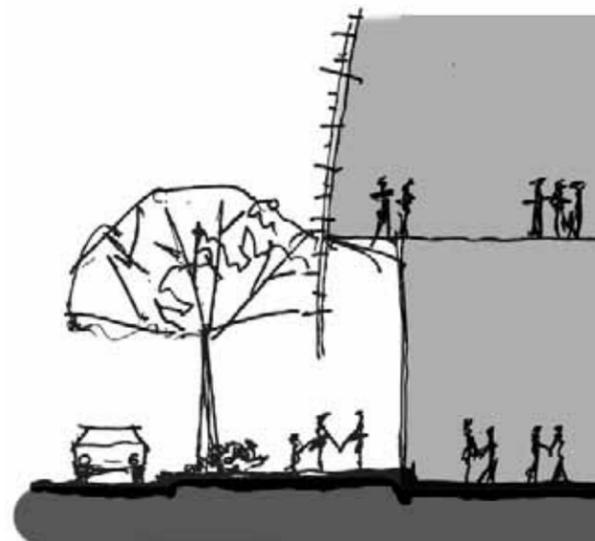


OPTION 3

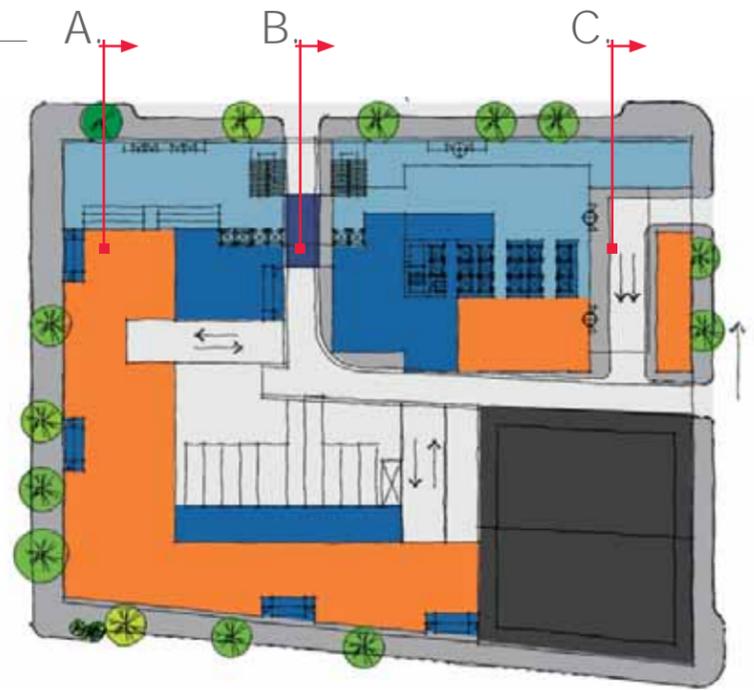
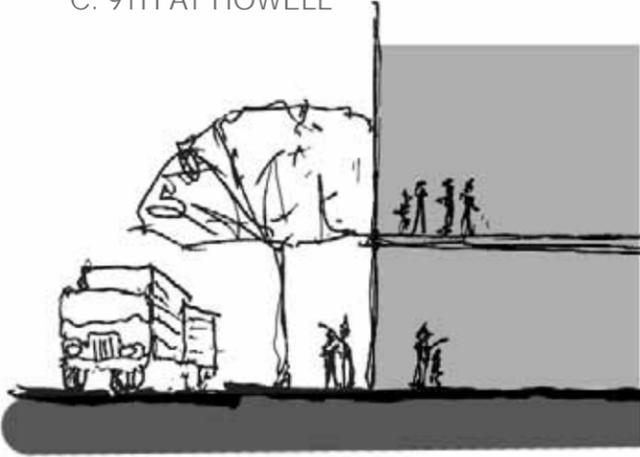
A. 9TH AT STEWART



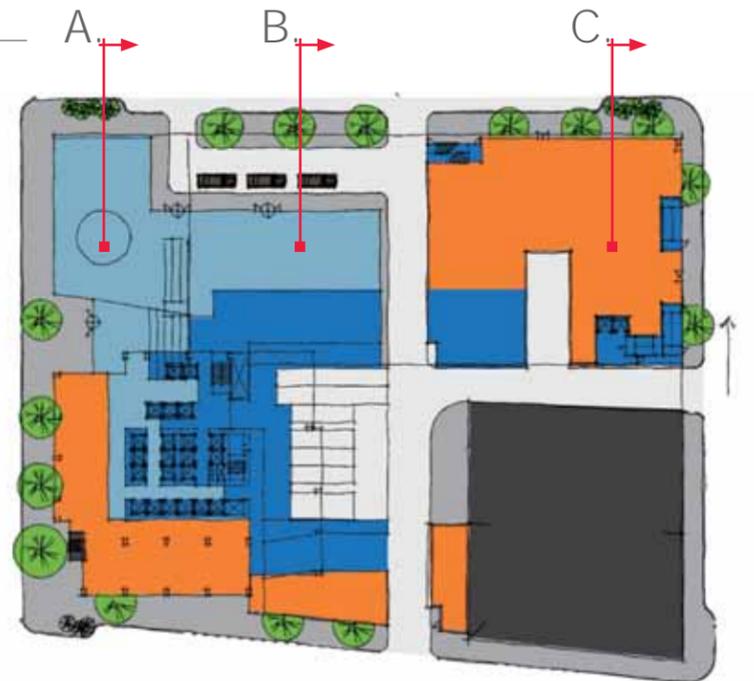
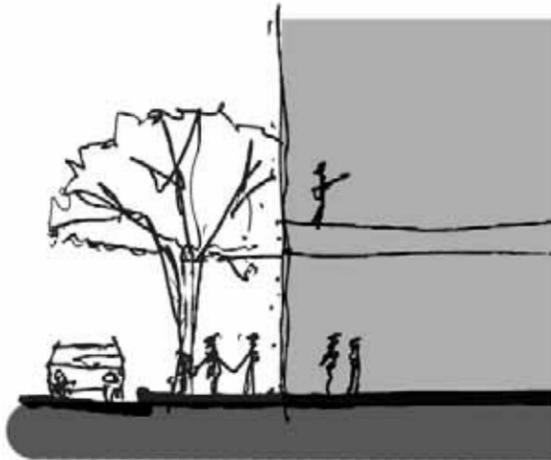
B. 9TH AT MIDBLOCK



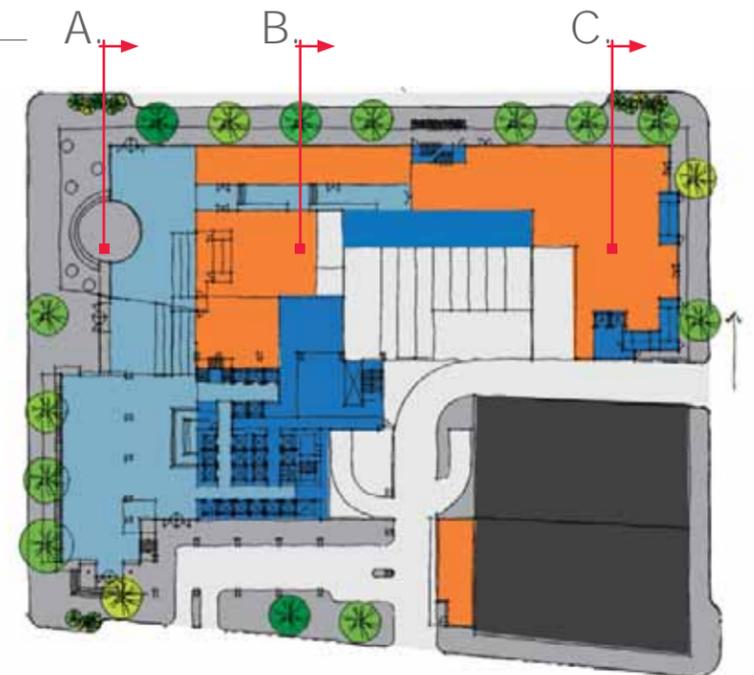
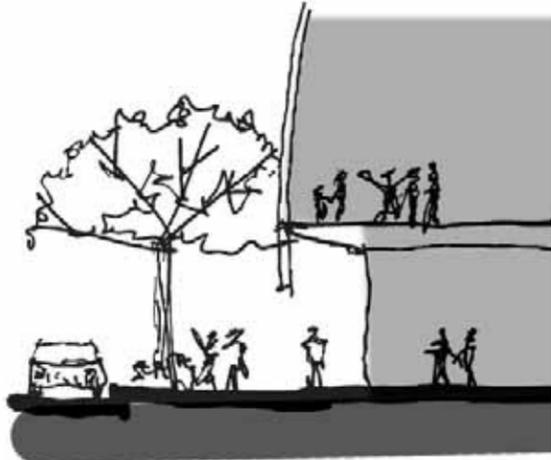
C. 9TH AT HOWELL



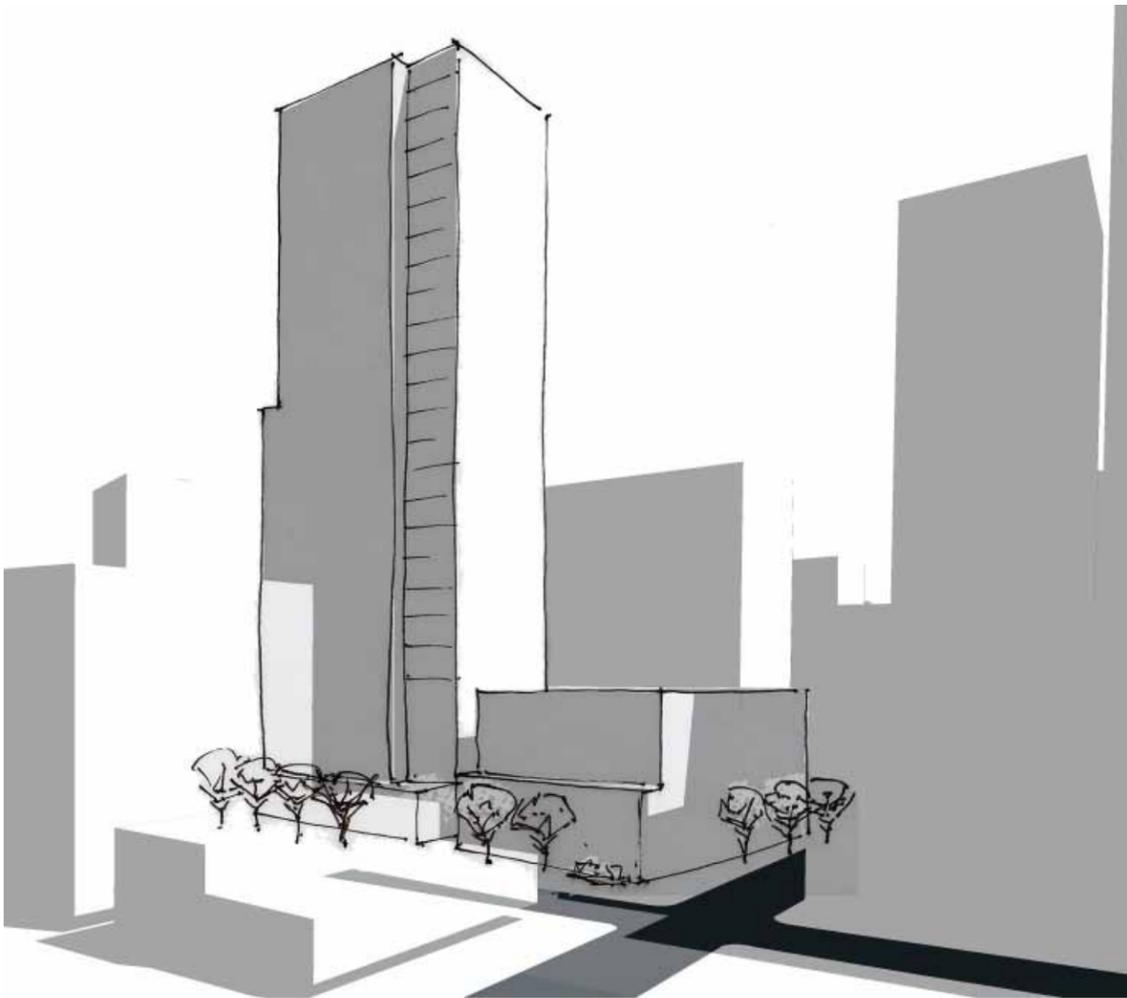
C. 9TH AT HOWELL



C. 9TH AT HOWELL



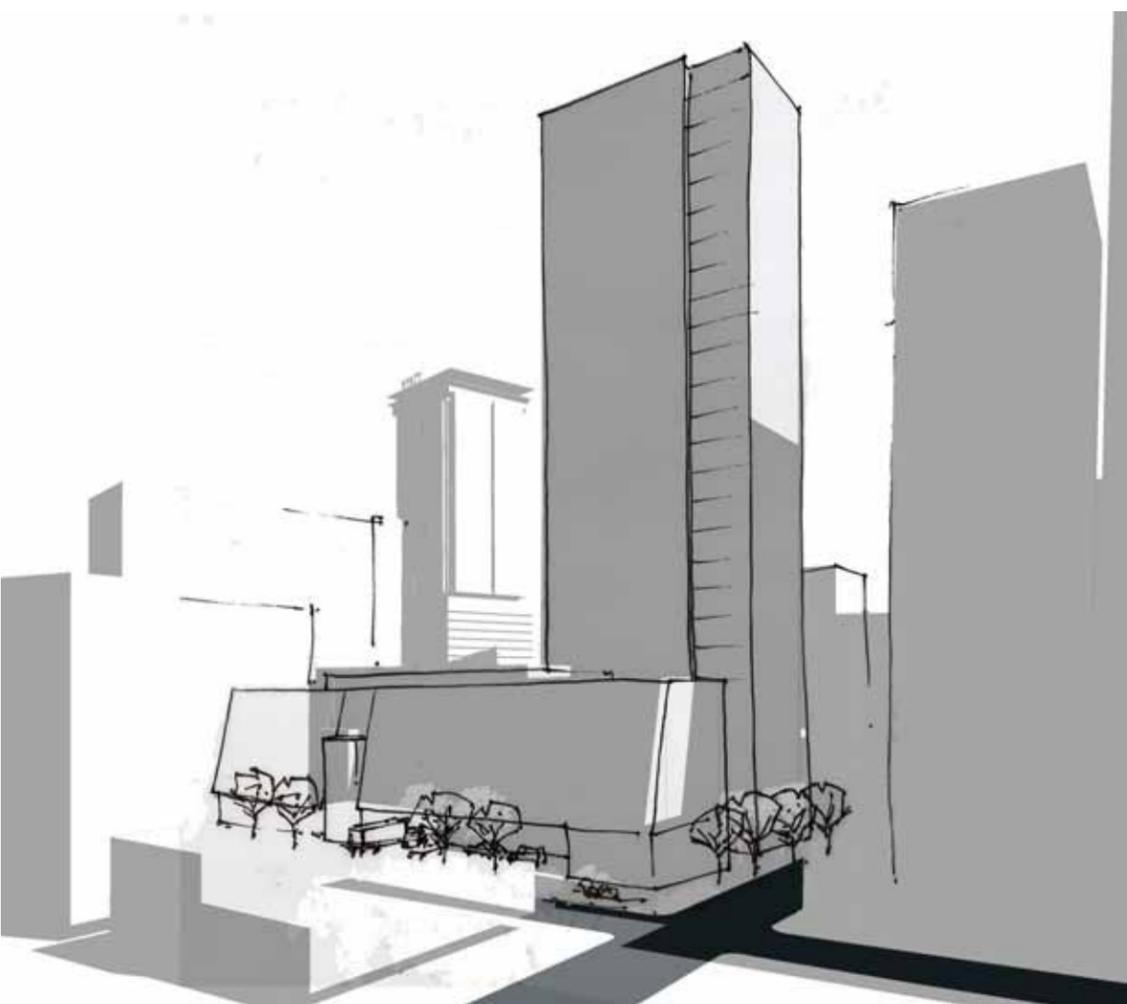
Option Comparison



View from corner of 9th and Stewart.

Option 1

- The tower is located on south east corner of the block on 9th and Howell. The remainder of the site contains the convention center.
- This code compliant option does not allow the design to adequately address the green street environment in a meaningful way due to the location of the alley, the presence of loading vehicles and automobiles accessing the parking garage.
- The subterranean tunnel creates logistic complications for the program, and deadens the streetscape environment.
- Program capacity is greatly diminished for exhibition space in this configuration.



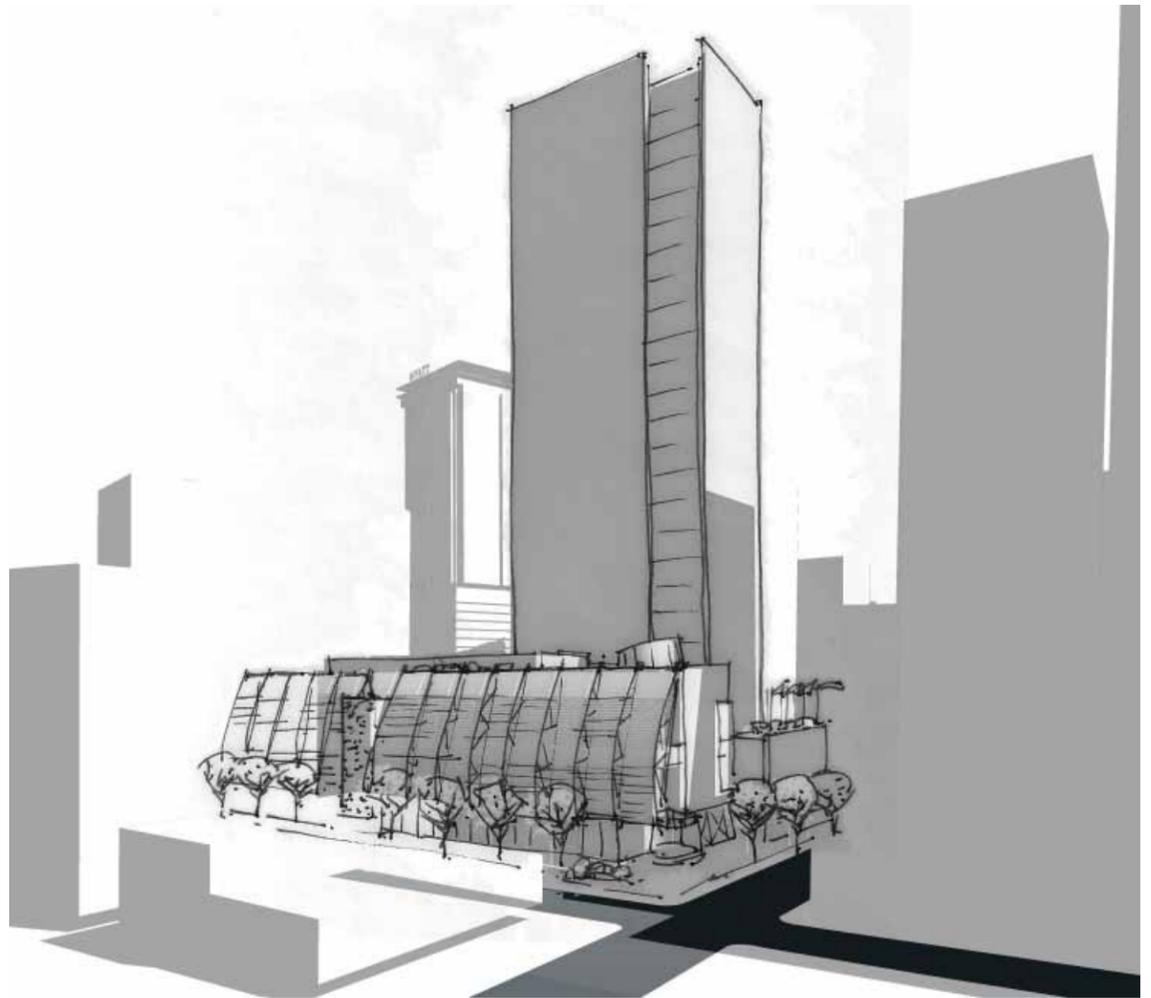
View from corner of 9th and Stewart.

Option 2

- Alley is relocated to 8th Ave.
- Tower is located on the northwest corner of the block on 8th and Stewart. Convention center is organized in a linear building on 9th Ave.
- The Porte Cochere is located on 9th Ave, and ties back to the alley, challenging the continuity and vitality of the green street with the dominant presence of the automobile.

Option 3

- Alley is relocated to 8th Ave.
- The streetscape along 9th Ave. takes a prominent role in this scheme, allowing generous room for enhanced landscaping.
- The tower is setback from Stewart approx. 40', shouldered by a 5 story building at the property line. Convention center is organized in a linear building on 9th Ave.
- A public plaza is located at corner of 9th and Stewart. This sense of place is further defined with a grand gesture of cutting a hole through the convention center building to connect people vertically to each other and to the sky. The oculus will also be a celebration of 'Seattle rain' as runoff from the roof area is brought down to the water feature located below in the plaza in an elegant and beautiful manner. The water feature may act as retention before it is treated and re-circulated through the building, educating the public on sustainable practices.



View from corner of 9th and Stewart.

High Priority Guidelines

A-2 Enhance The Skyline

Design the upper portion of the building to promote visual interest and variety in the downtown skyline.

B-4 Design a well proportioned & unified building.

Compose the massing and organize the publicly accessible interior and exterior spaces to create a well-proportioned building that exhibits a coherent architectural concept. Design the architectural elements and the finish details to create a unified building, so that all the components appear integral to the whole.

C-1 Promote pedestrian interaction.

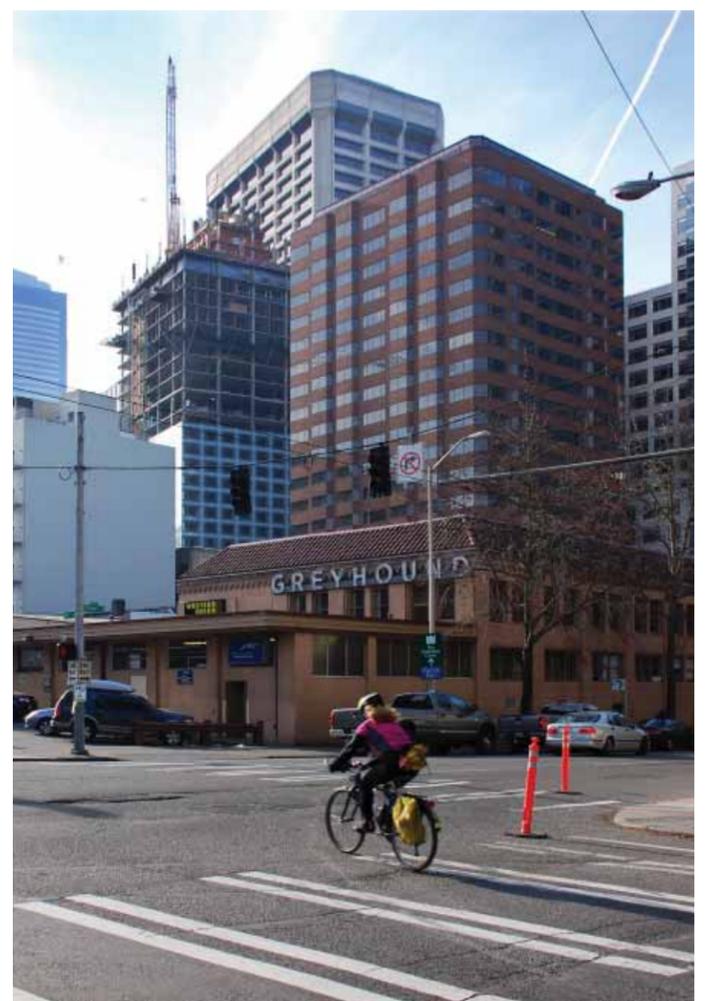
Spaces for street level uses should be designed to engage pedestrians with the activities occurring within them. Sidewalk-related spaces should be open to the general public and appear safe and welcoming.

D-2 Enhance the building with landscaping.

Enhance the building and site with substantial landscaping - which includes special pavements, trellises, screen walls, planters, and site furniture, as well as living plant material.

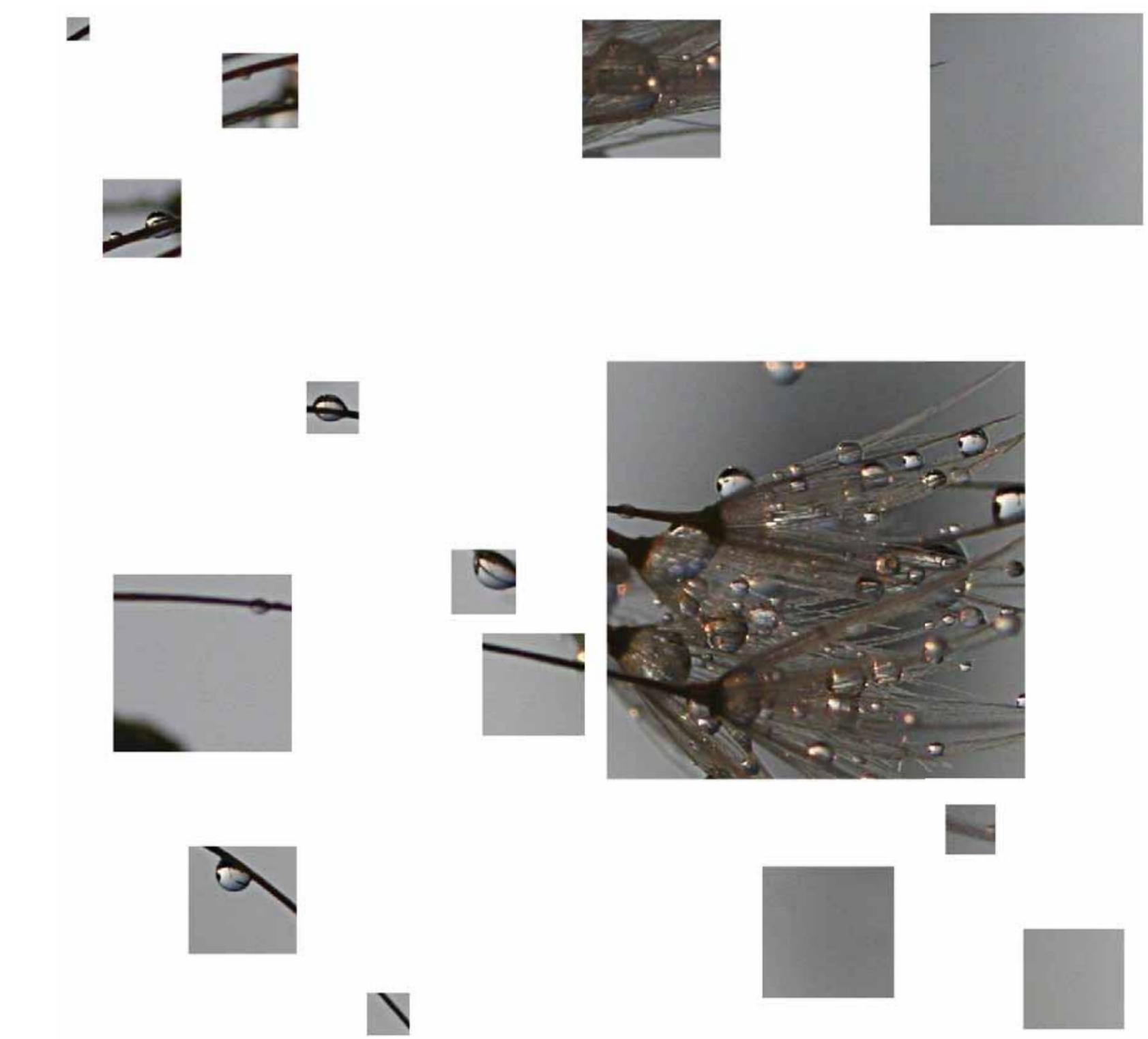
D-3 Provide elements that define the place.

Provide special elements on the facades, within public open spaces, or on the sidewalk to create a distinct, attractive, and memorable "sense of place" associated with the building.



SUSTAINABLE DESIGN COMMITMENT

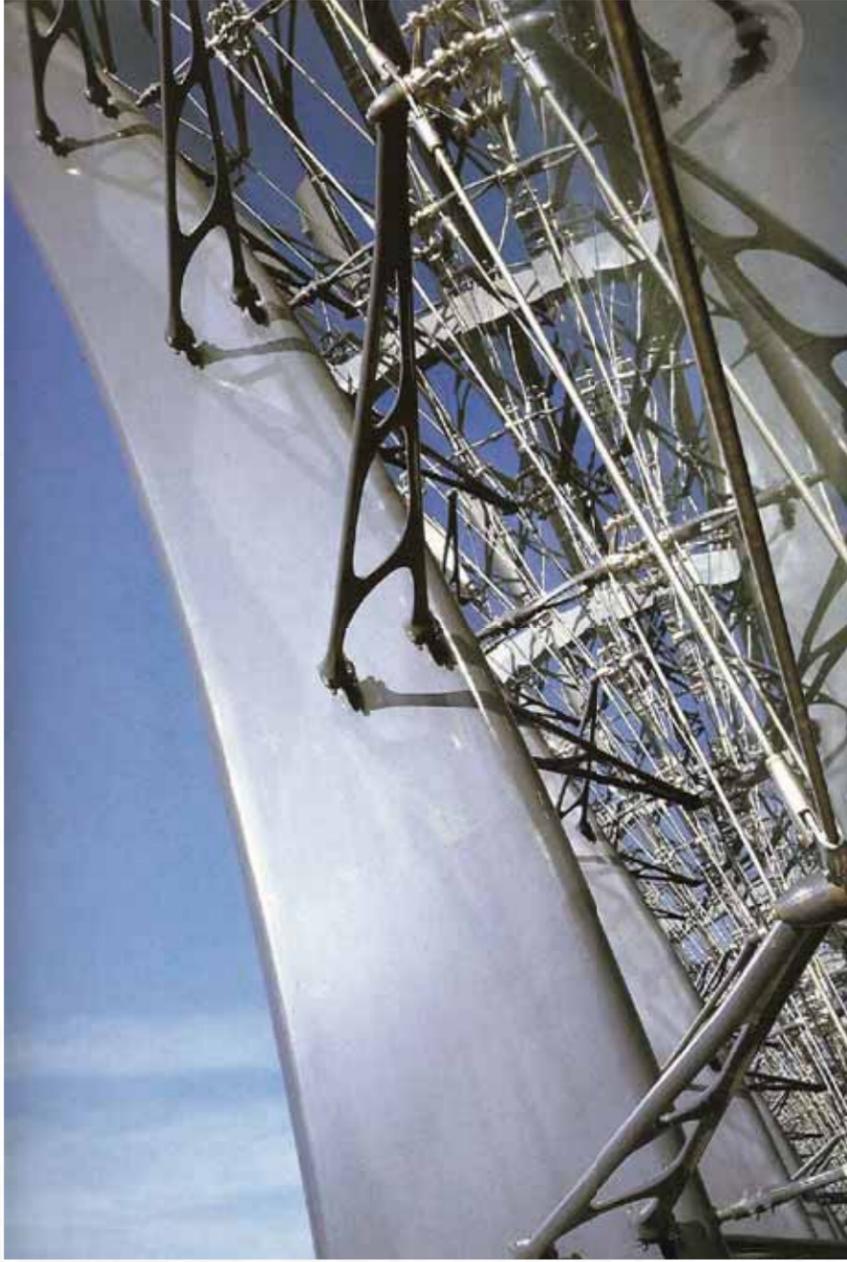
R.C. HEDREEN & CO. IS COMMITTED TO A DESIGN INCORPORATING SUSTAINABLE PRINCIPLES. GREEN PRACTICES REQUIRE A MULTI-DISCIPLINARY COMMITMENT THROUGH INTEGRATED BUILDING SYSTEMS.





- Bio retention green roof material to reduce or eliminate storm water retention requirements.
- Integrate Green roof to reduce the urban heat island effect. The green roof reflects the heat, reducing the amount of energy needed for refrigeration and/or air conditioning. It also slows the rain runoff and insulates the rooftop, keeping the building cooler overall.
- Bicycle Storage & Changing rooms
- Landscape design with water efficient native plantings
- Low Flow plumbing fixtures
- Storm water recycling for irrigation, mechanical cooling and other greywater needs
- Generate electricity from elevators using a regenerative drive incorporating magnet motors.
- Utilize abundant natural lighting with large energy-efficient windows in guest rooms.
- Allow increased levels of filtered natural light to be reflected into the convention spaces through design and massing, reducing the requirement for artificial lighting.
- Investigate design of intelligent facades where appropriate with regards to: daylight controllers; intelligent lighting; sun controllers; electrical generators; heating and temperature controllers; ventilation controllers; the double skin; cooling devices; building management systems; learning ability; and occupant control.
- Improve air quality by circulating large amounts of outside air into guestrooms in an energy efficient way by employing energy recovery technology where the outside air is tempered by the air being exhausted from the hotel.
- Utilize low-emitting volatile organic compound (VOC) paints, adhesives, carpets, etc to reduce indoor air contamination.
- Use regional vendors and artists for materials to reduce transportation and packaging.
- Endeavor to create pockets of green relief in the urban fabric, creating places for people to gather in an outdoor living room setting.
- Utilize Sustainable Hotel management procedures, such as low VOC cleaning products and optional linen changes for water and energy conservation.
- Investigate geothermal energy for the refrigeration equipment, instead of a standard water-cooled system, saving significant amounts of water.
- Become an Education Center for sustainable practices with tours for guests.





EXPRESS STRUCTURE
-N. GRIMSHAW.



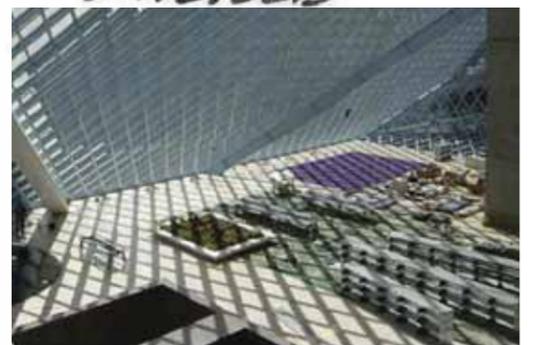
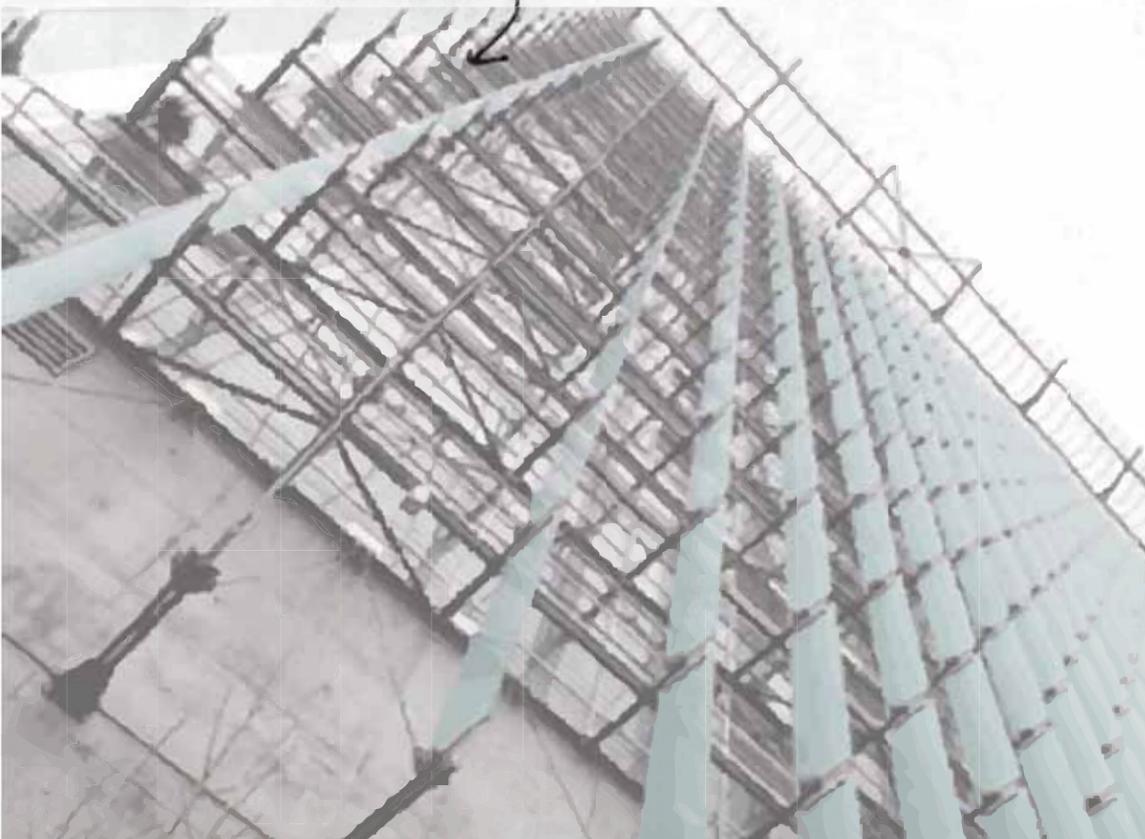
GLUCKMAN

CABLE NET WALL



PASSIVE ENERGY UTILIZED

LAYERED FACADE



STRUCTURAL SKIN

M.N. FEDERAL BLDG.
SPENCER DECKER ARCHITECTS

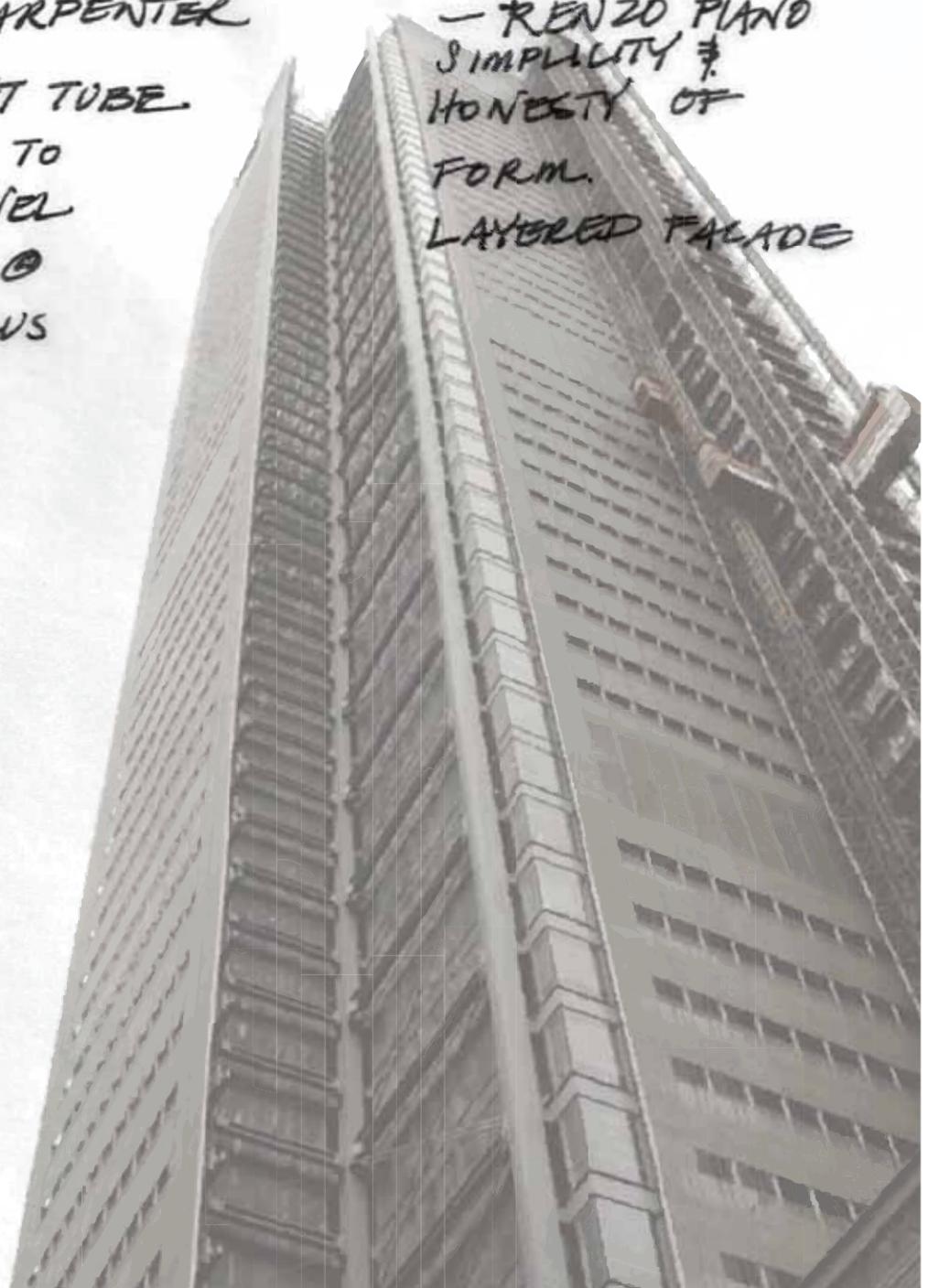


MOMA - PLAZA/WATER



J. CARPENTER
LIGHT TUBE
-SIM. TO
FUNNEL
IDEA @
OCULUS

- RENZO PIANO
SIMPLICITY &
HONESTY OF
FORM.
LAYERED FACADE



STRUCTURAL GLASS



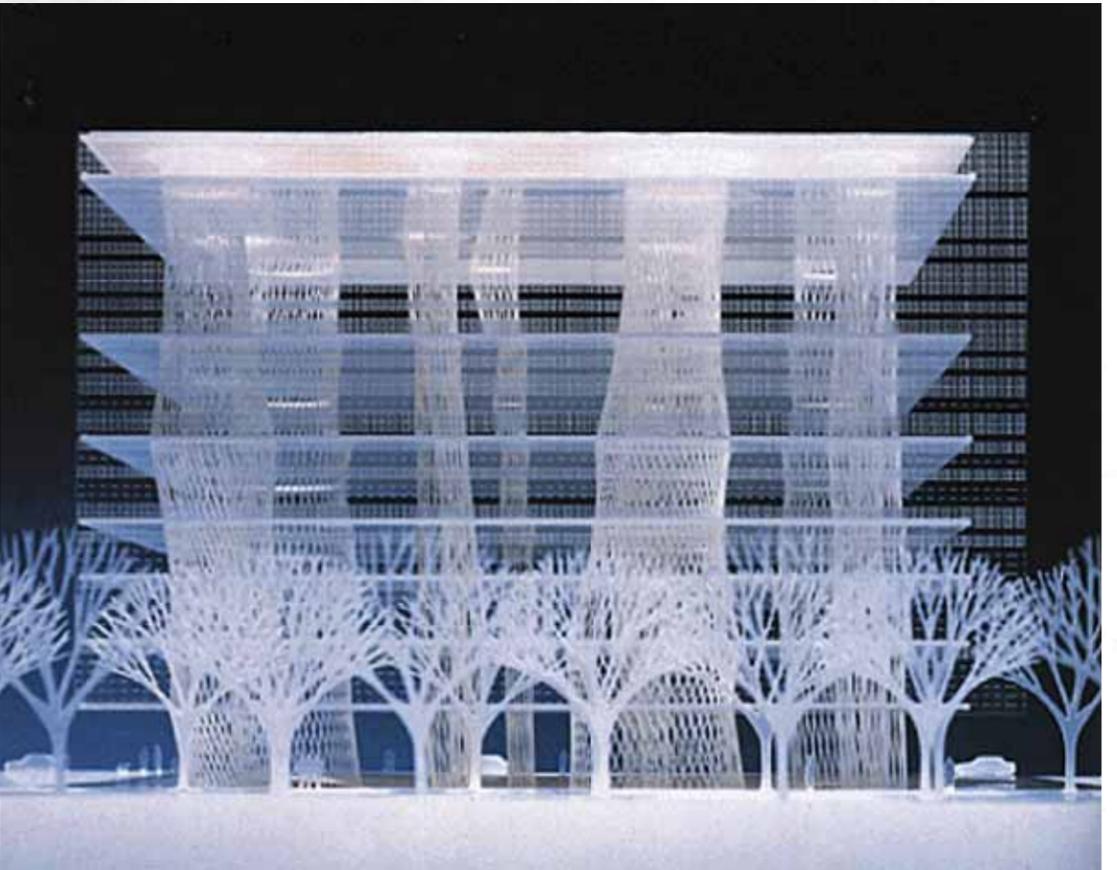
H. JAHN - REFLECTIVE SOFFIT



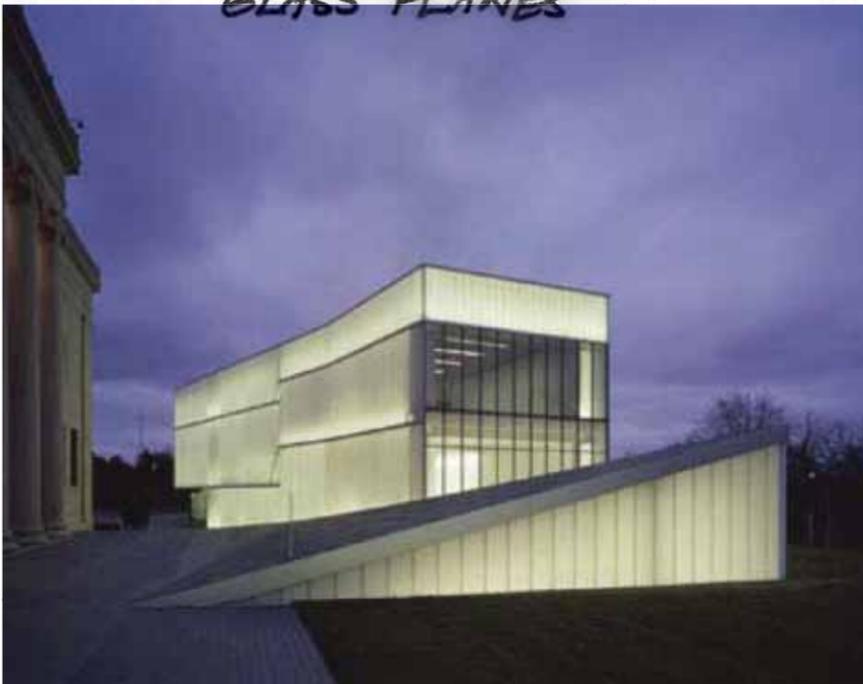
DOUBLE SKIN FACADES



H. JAHN - EXTENSION OF GLASS PLANES



TOYO ITO - TUBES SLICING THROUGH SPACE



LANTERN



SCALE OF PLAZA.



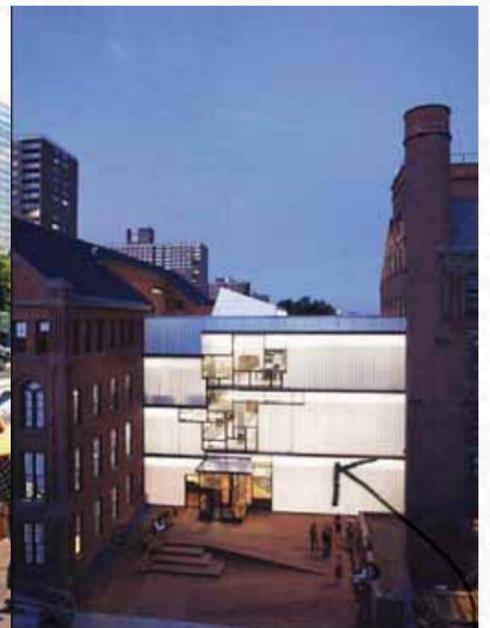
FLOATING ABOVE
TRANSPARENT BASE

INTELLIGENT FACADES



POLSHEK - SUN SHADING
& TRANSPARENT STAIR

J. NOUVEL
EXO-SKELETON
@ TOWER



VERTICAL
GARDEN
- POLSHEK

IDEA FOR
OCULUS
- GLUCKMAN

CURTAIN WALL
AS PLANE
EXTENDING BEYOND

LANTERN
@ NIGHT.