



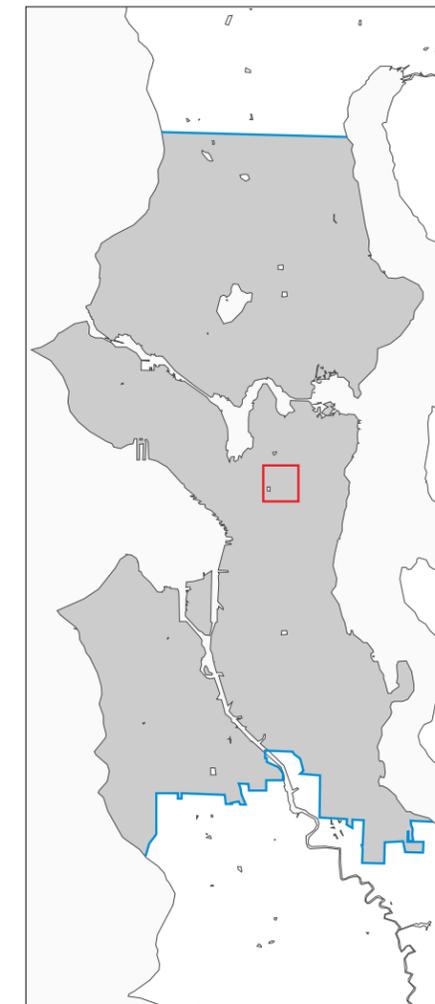
Jewish Family Service Office Building

Initial Recommendation Meeting

February 2010



JEWISH FAMILY SERVICE



Property Address:

1601 16th Avenue

Owner Name:

Jewish Family Service

Contacts:

Ed Weinstein, FAIA

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Weinstein A|U LLC

Architects + Urban Designers

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Existing Site Conditions

1. Please describe the existing site, including location, existing uses and/or structures, topographical or other physical features, etc.

Location

The subject property is located at the corner of 16th Avenue and E Pine Street, just to the north of the intersection with E Madison Street. Its primary frontage is along 16th Avenue.

Existing Uses

The Jewish Family Service (JFS) currently occupies a small two-story office building on the southern half of the property. This building is known as the Jessie Danz Building. The proposed building would occupy an existing surface parking lot on the north half of the property.

The properties to the north and west of the site are residential (L-3), while those to the south and east are mixed-use (NC3-65).

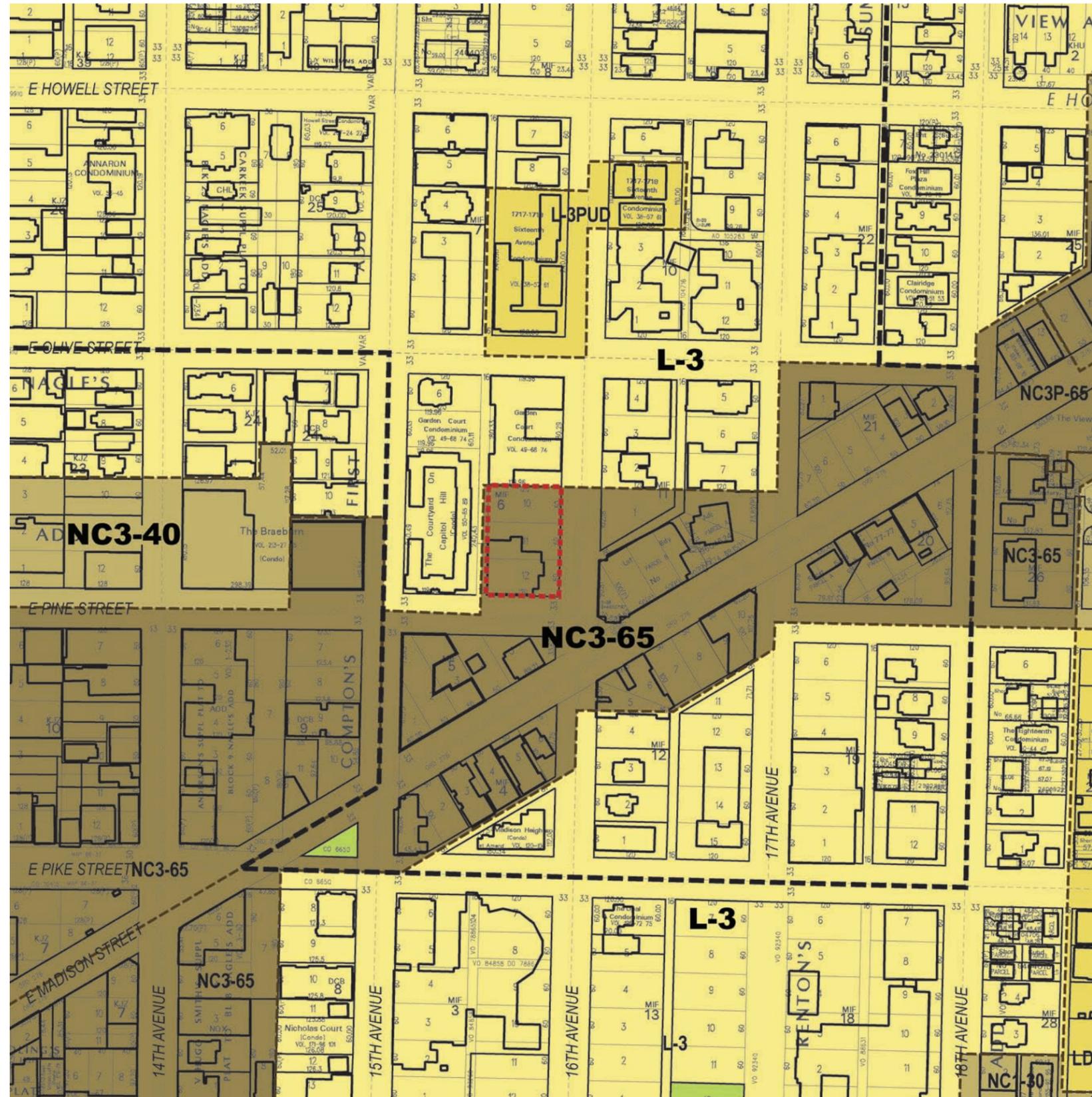
Physical Features

The property slopes from east to west, while remaining relatively flat from north to south over most of the site. The change in elevation along E Pine Street is dramatic, falling approximately eleven feet.

The City's Environmentally Critical Area mapping indicates that a portion of the site located on the north half of the west property line is considered a Steep Slope condition. An ECA Exemption was approved for this Steep Slope in February of 2008.

An undeveloped and discontinuous alley exists at either end of the block, interrupted by the aforementioned Steep Slope condition.





2. Please indicate the site's zoning and any other overlay designations.

The zoning is NC3 with a 65-foot height limit. The adjacent properties to the north and west are zoned L-3, and those to the east and south are zoned NC3. The site is within the Capitol Hill Urban Center Village (indicated by dashed line). The City's Environmentally Critical Area mapping indicates that a portion to the extreme west side of the site, at the center of the block along the undeveloped alley, is a Steep Slope.

Pertinent zoning issues are as follows:

Steep Slope Areas

Section 25.09.180

Development limitations in steep slope areas can be exempted provided applicant demonstrates the steep slope area in question is less than 20 feet in vertical rise and more than thirty feet from other steep slope areas. An ECA exemption request was approved for this Steep Slope in February of 2008.

Use

Section 23.47A.004, Chart A

Office uses are permitted outright in a NC3 zone with no limitations on area. Eating and drinking establishments, and Retail sales and services, general are also permitted outright. No residential use is planned.

Envelope

Section 23.47A.013B, Chart A

The new building is expected to be approximately 21,000-sf occupying the north half of a 21,600-sf lot. An existing building of approximately 13,500 occupies the southern half of the lot. The allowable FAR is 4.25. The program is unlikely to result in utilizing the full 65-foot height available.

Section 23.47A.014

Commercial zones adjacent to residential zones require setbacks at the rear and side.

- A 15-foot triangular setback is required at the intersection of the side lot and front lot lines
- A 10-foot rear and side setback is required above thirteen feet. One-half the alley width can be counted as part of the setback

Parking

Section 23.45.015.B.2

In commercial zones located in urban centers, no parking is required by the Land Use code. JFS will need parking for the staff, clients and volunteers. There will be approximately 25 parking spaces located at street level. JFS also has dedicated parking spaces in a surface parking lot across 16th Avenue.

Landscaping & Screening Standards

Section 23.47A.016

Green Factor requirements apply to the site. Because the site is relatively tight, street trees in the right-of-way along 16th Avenue and green walls will likely be employed to meet the requirements for the site. The landscaping around the existing Jesse Danze Building is substantial and will be retained as much as possible.

Alley

Section 23.53.030 C

In order to be considered improved, an alley must be paved. The alley is not paved and therefore is not improved.

Section 23.53.030 E.1

Improvement of an alley is required when the alley is used for access to parking spaces, open storage, or loading berths on a lot. None of these items are accessed from the alley. Therefore, alley improvement is not required.

Parking Access

Section 23.47A.032

Because the existing alley is not improved as defined by Section 23.53.030C, parking is not required to be accessed from the alley. The Seattle Municipal Code mandates that if a lot does not abut an improved alley but abuts two or more streets, access to parking must be from the street with the fewest lineal feet of commercially zoned frontage. In this case, 16th Avenue is the appropriate street for parking access.



3. Please describe neighboring development and uses, including adjacent zoning, physical features, existing architectural and siting patterns, views, community landmarks, etc.

E Pine Street & 15th Avenue

The surrounding street grid is interrupted by E Madison Street, with E Pine Street effectively beginning again at the intersection with Madison Street and 16th Avenue. East Pine Street and nearby 15th Avenue are both arterial roads, serving as entry portals to the Pike/Pine and Capitol Hill neighborhoods respectively. Both streets lead to burgeoning neighborhood commercial centers, resulting in substantial foot, bus and automobile traffic.

E Pine Street to the west is zoned almost entirely NC3, resulting in recent mixed-use additions to the neighborhood, such as the Braeburn Condominiums and the Pearl Apartments. An eclectic mix of older buildings characterizes this area, primarily small apartment buildings and condominiums, as well as lower scale commercial spaces, such as the Jessie Danz Building.

The architectural character of these streets is likewise eclectic, with a mix of old and new residential and commercial buildings, displaying a range of materials and differing levels of detail.



- 1 Courtyard on Capitol Hill Condominiums
- 2 Braeburn Condominiums
- 3 Jessie Danz Building
- 4 Madison Market
- 5 The Pearl Apartments
- 6 7-Eleven, intersection of E Pine Street and 16th Avenue



E Madison Street

Less than a block south of the site is E Madison Street, running on a northeast-southwest line and providing a direct connection to downtown Seattle. Also classified as an arterial, Madison Street is dominated by vehicular traffic, primarily passing through Capitol Hill.

The Madison Street corridor is a mix of uses, predominantly commercial. Many lots along Madison are also irregularly shaped due to the angle of Madison across the city's north-south street grid. Often, the irregular lots are underdeveloped, with small retail uses and at grade parking. Examples include the adjacent 7-Eleven store/gas station and Taco Time across 15th Avenue to the west.

A number of mixed use buildings have been built recently to the east along Madison, with large plate uses such as Madison Market, Trader Joes, and Safeway.

The mix of uses also includes several institutions. Temple de Hirsch Sinai, and the co-located middle school of Seattle Academy of Arts and Sciences (SAAS) are visible to the south of Madison. Other SAAS uses are nearby, and the north edge of the Seattle University campus is at 12th Avenue and Madison.

The architectural character of Madison, like the land uses, varies widely. The older buildings typically utilize a finer-grained palette of materials and higher level of detail than the newer buildings.



- 1 1605 E Madison, apartment building
- 2 Trader Joe's Market
- 3 1625 E Madison, apartment Building
- 4 Madison Market
- 5 7-Eleven
- 6 View to downtown along E Madison Street
- 7 The Pearl Apartments



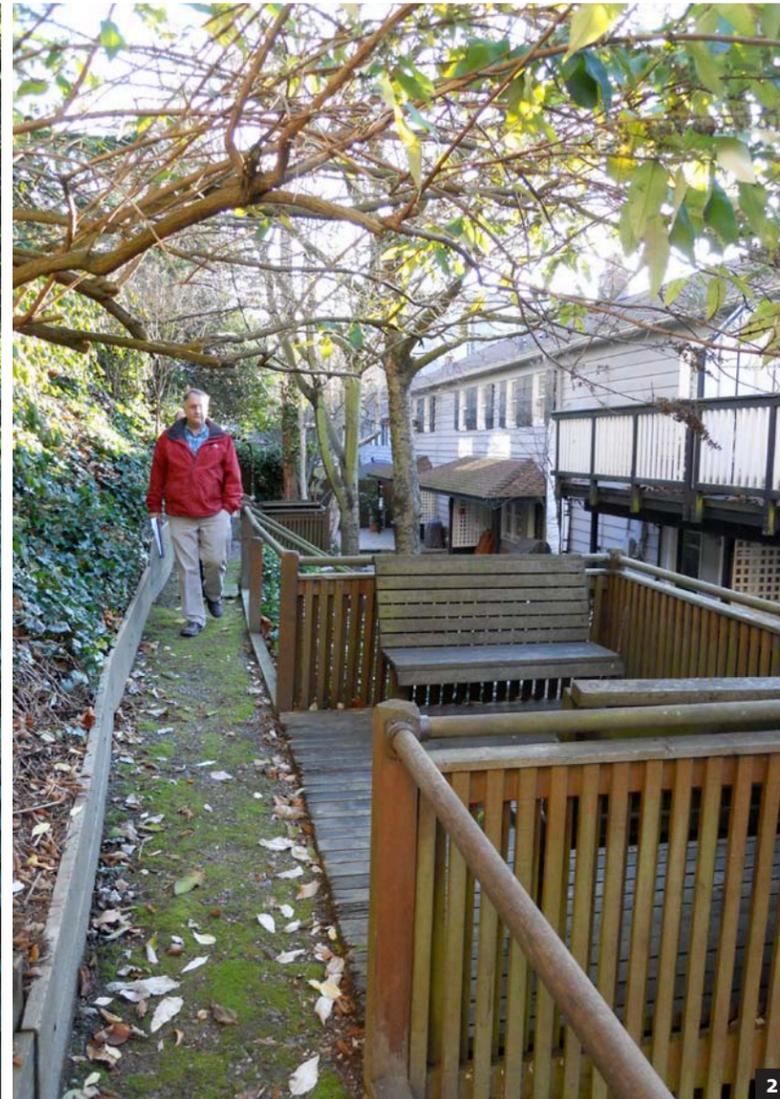
16th Avenue & E Olive Street

Unlike the previously described surrounding streets, 16th Avenue and E Olive Street are primarily residential. The buildings along 16th Avenue quickly diminish in scale north of Olive. Save for the areas adjacent to Madison, the zoning is primarily L-3.

Immediately to the north of the site are older four-story brick residential buildings with lushly planted entry courts. Continuing further to the north, large street trees dominate, blending a mix of newer condominiums, older single family residences, and masking the presence of Sound Mental Health. The character of 16th Avenue is comparatively tranquil when compared to 15th Avenue, E Pine and E Madison Streets.

The architectural character of 16th Avenue changes with the intensity of the development. The smaller scale buildings a block or more north of the site are either single family residences or larger buildings attempting to emulate the single family residences. Closer to the site, the buildings are larger and more urban in character, building to the street and utilizing more substantial materials.

- 1 1632 E Olive, condominiums
- 2 Garden Court Condominiums
- 3 View along 16th Avenue, looking south
- 4 1620 16th Avenue, single family residence
- 5 1632 E Olive, condominiums
- 6 View along 16th Avenue, looking north
- 7 Sound Mental Health
- 8 Madison Market at the end of E Pine Street
- 9 Entry to the Jessie Danz Building off of 16th Avenue.
- 10 View of the site from 16th Avenue, looking northwest. Note Jessie Danz Building to the left with surface parking lot immediately to the north.
- 11 View of surface parking lot looking west with non-exceptional vine-maples in the foreground.



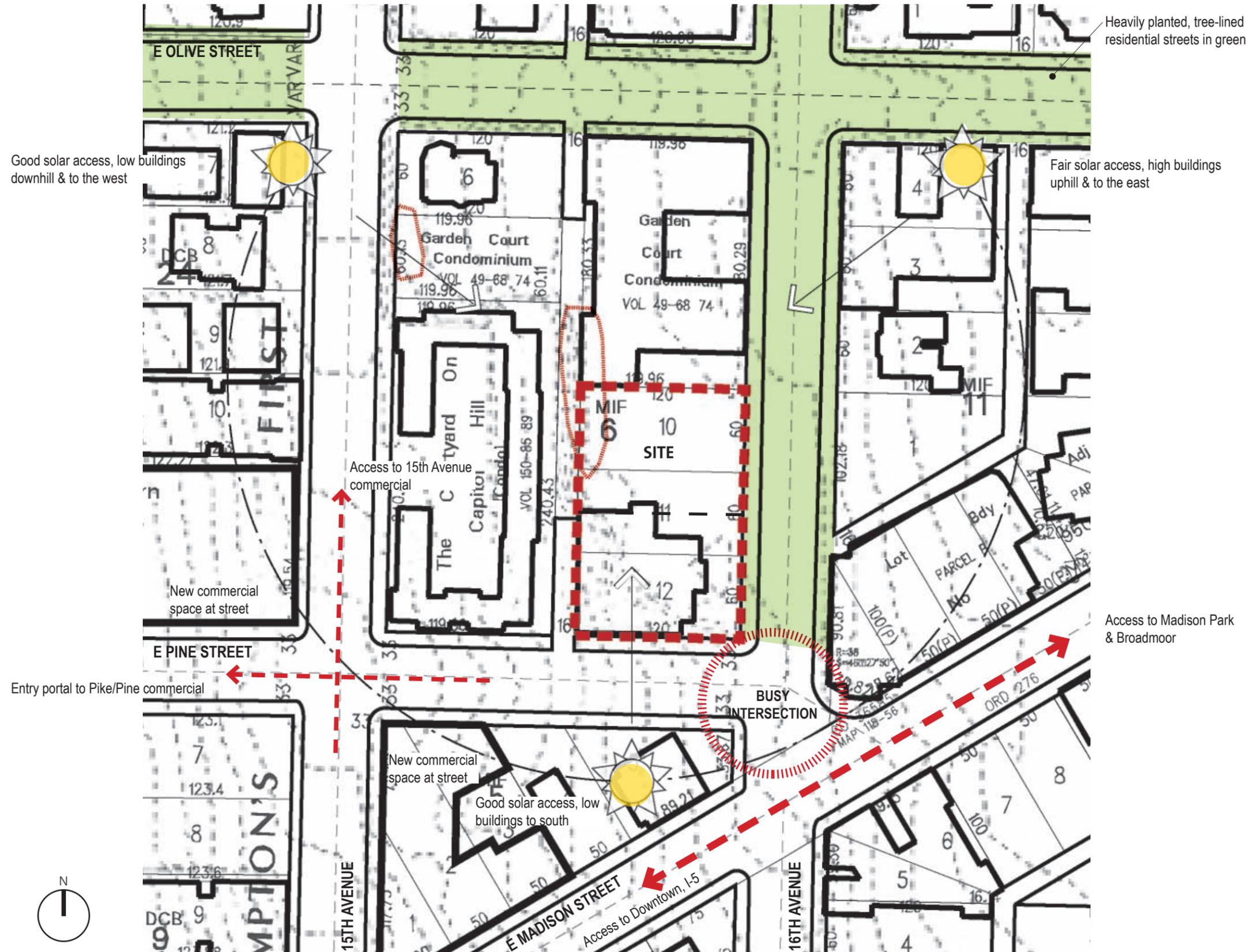
The Alley

The alley that abuts the west property line of the site is unimproved. The presence of electrical power lines in the alley right-of-way and a steep cross-slope present significant challenges for future development of the alley. There is partial vehicular access at the north and south ends of the block where the topography is least problematic. In its unimproved state, the alley is heavily vegetated and has become a valued green amenity for the neighboring condominiums.



- 1 Alley along subject property, looking north
Note power pole in foreground
- 2 Alley along subject property, looking south
- 3 Alley north of property, looking north
Note power pole in background

Urban Design Analysis: Site



Site Analysis Summary

16th Avenue Neighbors:

- Area to north primarily residential, very quiet and heavily vegetated
- Scale of buildings quickly decreases north of Olive Street

Street Traffic:

- E Pine Street, and nearby 15th Avenue and E Madison Street are heavily travelled arterials
- Busy intersection with E Madison Street
- Entry point to Pike/Pine and Capitol Hill
- Additional growth in Pike/Pine and further north on 15th Avenue

Unimproved Alley:

- Undeveloped with partial access at each end of block
- Not realized due to topographical conditions and presence of electrical power lines
- Heavily planted, serves as a green amenity to the neighboring condominiums

Existing Jessie Danz Building

- 2-story modernist building on south half of site with lush and well-maintained entry garden
- Building is distinctive in style and continues to be serviceable

Garage Access for proposed building:

- Parking access from alley not required as alley is unimproved and not desirable due to traffic on E Pine Street, limited width, topographical constraints, and security issues
- Preferred parking access from 16th Avenue is safer and minimizes potential traffic impact

Solar Access:

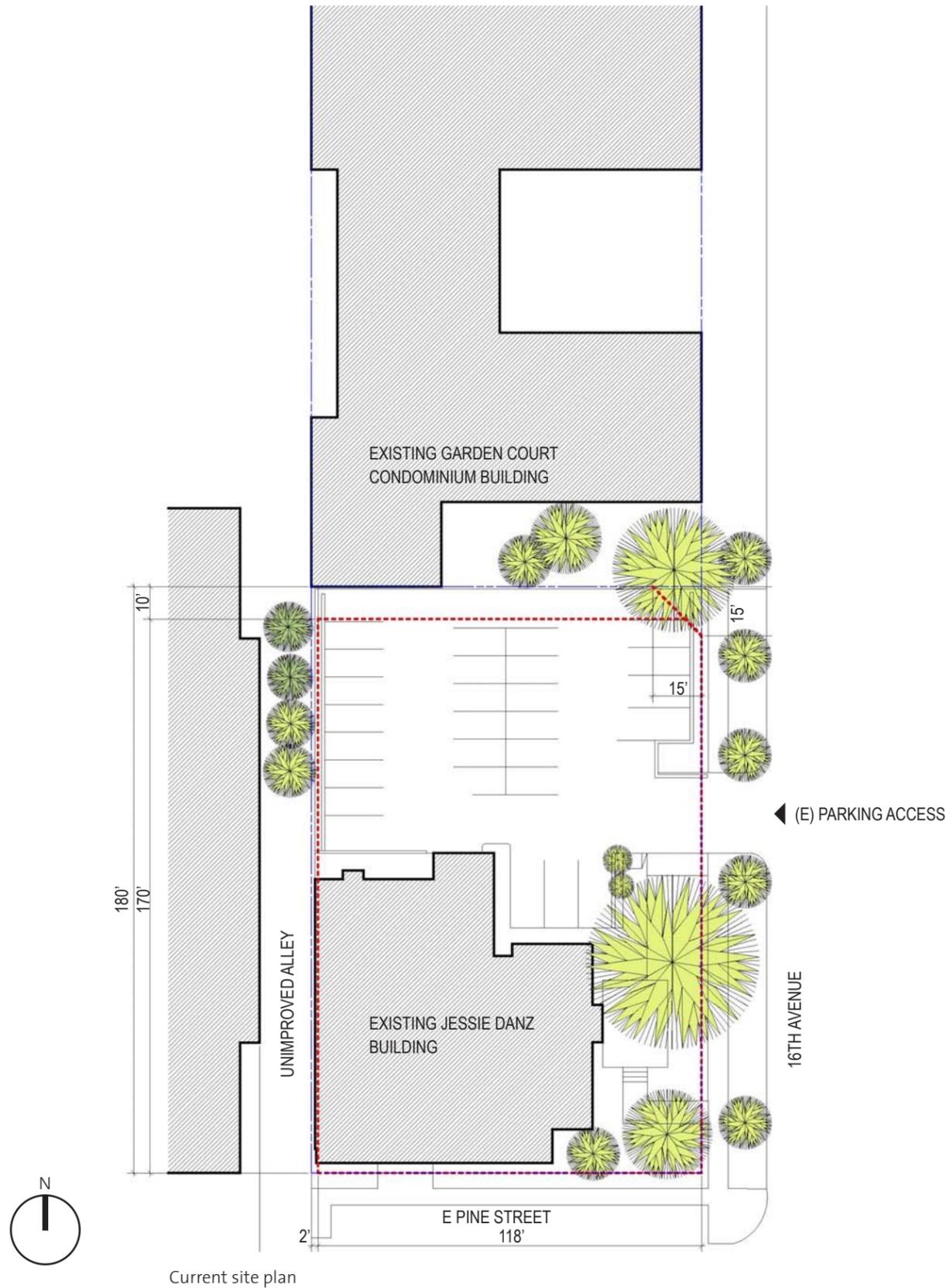
- Access to light very good to the south and west

Building Mass:

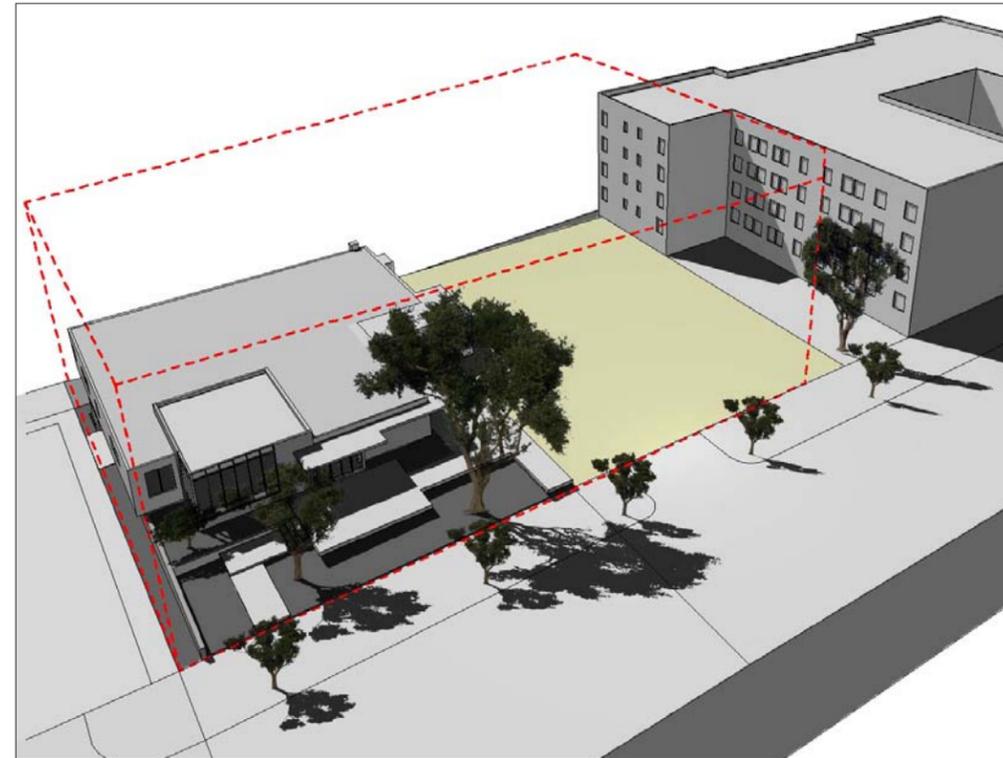
- Desire to address scale of residential buildings immediately to north and northeast
- Desire to respond to existing Jessie Danz Building on south half of site
- Transition to larger scale buildings on E Madison Street

Views:

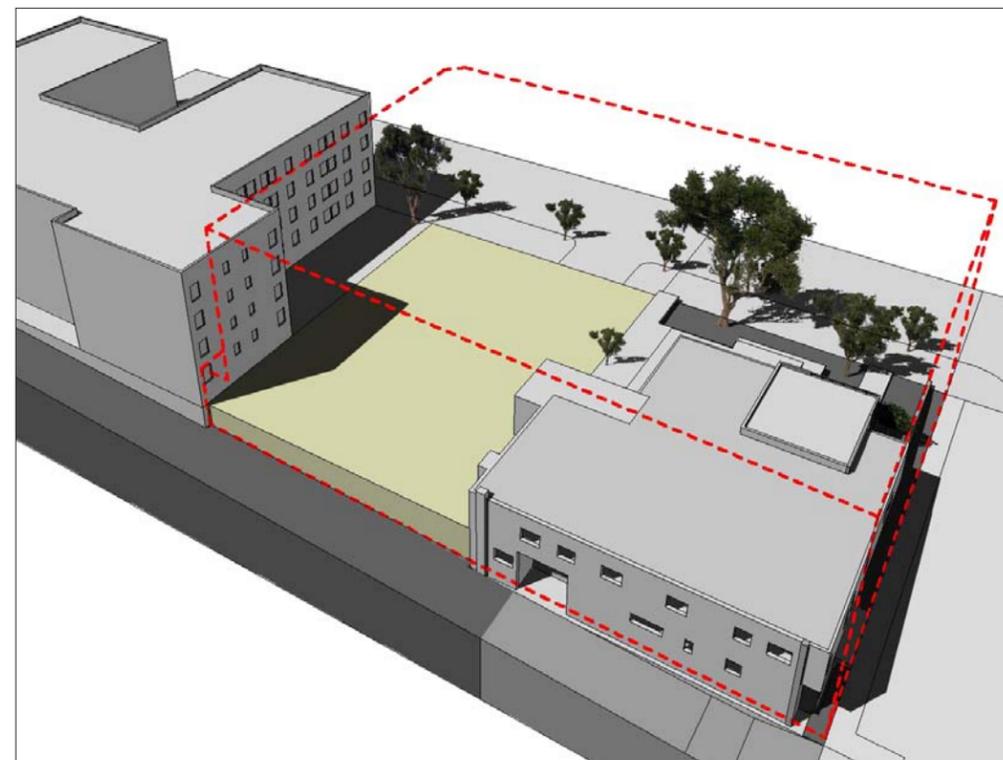
- Views primarily to the west over existing condominiums (mountains)



Current site plan



Existing site, viewed from east above 16th Avenue



Existing site, viewed from west above alley

Envelope Analysis

Structure Height

- The maximum building height is 65' above grade (23.47A.012 A)
- A marginal height bonus is available since the lot is sloped (23.47A.012 B)

Floor Area Ratio

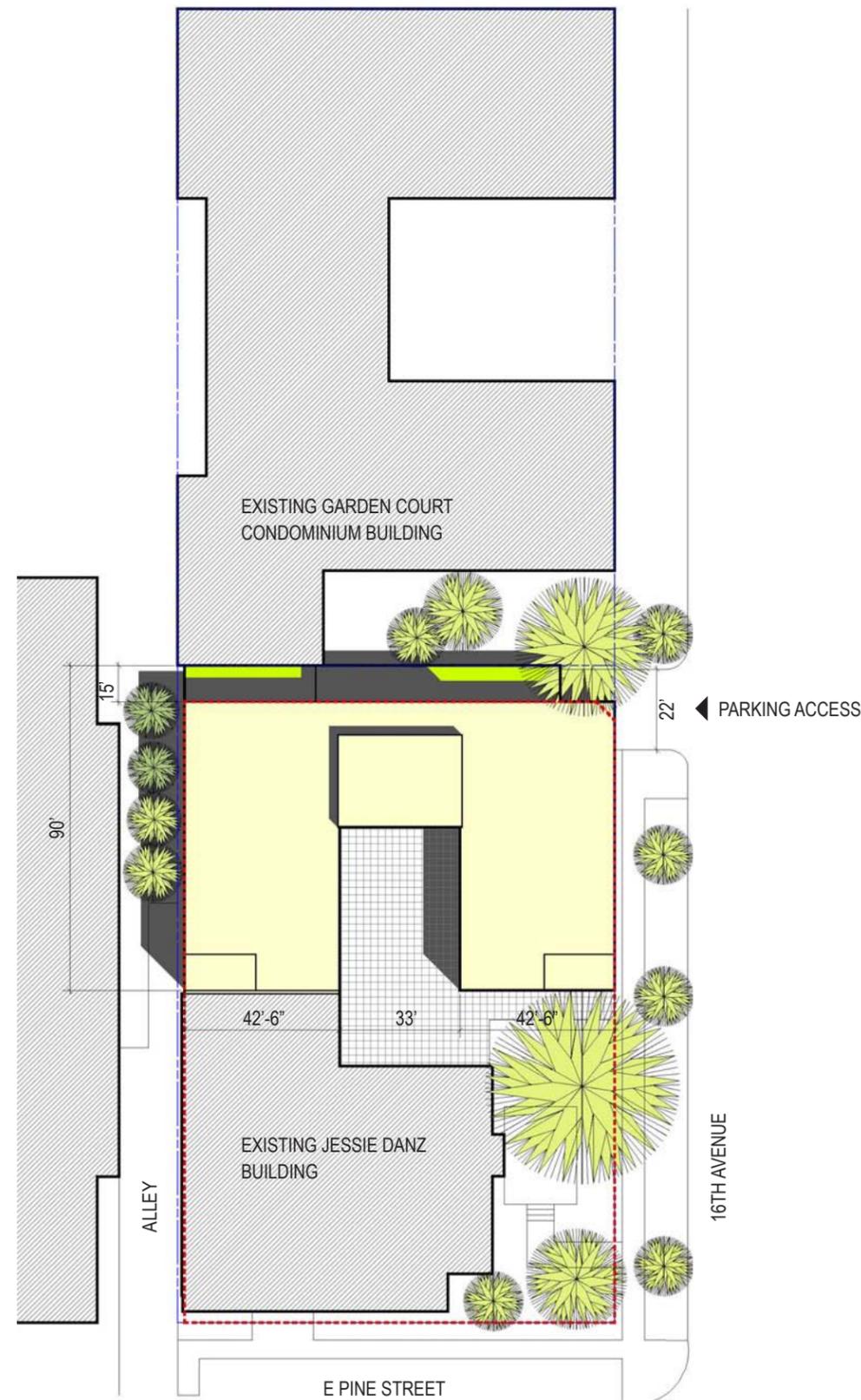
- The maximum FAR is 4.25 (23.47A.013)
- The lot area is 21,600 sf
- The allowable gross area is 98,800 sf

Setback Requirements

- A 15-foot triangular setback is required at the intersection of the side and the front lot lines at the northeast corner (23.47A.014 B1a)
- A 10-foot side setback is required above thirteen feet along the north property line (23.47A.014 B2a)
- A 10-foot rear setback is required above thirteen feet along the alley. One-half the alley width can be counted as part of the setback (23.47A.014 B2a). Because the alley is 16' wide, the additional setback from the rear property line is two feet.

Non-Conforming North Neighbor

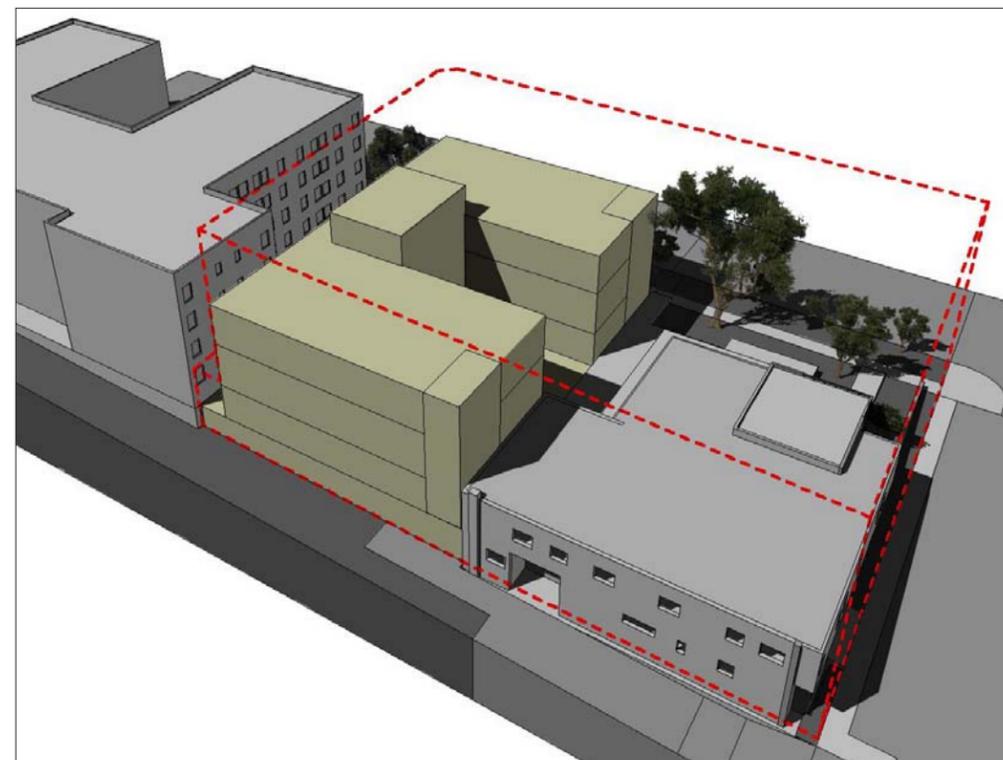
- The neighboring condominium building to the north extends all the way to the shared property line and has windows in the property line wall. This condition does not conform to current land-use and building code requirements. Full realization of the allowable building envelop on the subject property would block some of these property line windows.



2007 Scheme, site plan



2007 Scheme, 16th Avenue view



2007 Scheme alley view

The applicant previously went through Early Design Guidance in December of 2007 and submitted a Master Use Permit application in February of 2008. The project went on hold prior to issuance of a Master Use Permit. Since then, the owner has significantly reduced the scale of the project in order to more realistically align with JFS's fundraising capabilities. The following is a summary of the 2007 EDG Scheme.

The project was to include 23,000-sf of office space and below-grade parking for 70 cars. The proposed massing strategy organized the building around a south-facing, street-level courtyard. The building was held away from the north property line at the northeast corner to accommodate non-conforming windows in the adjacent condominium. At three stories, the scheme was well below the maximum allowable building height. Access to the parking garage was to be off of 16th Avenue.

The proposal to access parking from 16th rather than from the alley was treated as a departure request. An alley garage entry was considered undesirable for a number of reasons. As previously indicated, alley improvement is challenging due to topography and the presence of power poles. In addition, access to the parking garage from the alley would present a safety hazard due to the difficulty of maneuvering a 17'-0" wide, two-way, dead end lane. Noise and light pollution from vehicles entering, exiting, and, most especially, turning around would adversely affect residents of the existing condominiums to the west whose living spaces front directly onto the alley. Finally, alley access would require an unsupervised, recessed garage entry, which presents security concerns for a Jewish organization.

The Board was supportive of the preferred scheme, the departure request for parking access from 16th rather than the alley, and various other departure requests. While many of the objectives remain the same for the reduced-scale project, a somewhat different massing strategy has evolved.

4. Please describe the proponent's development objectives for the site, indicating types of desired uses and approximate structure sizes, as well as any potential requests for departures from development standards.

Objectives:

- Continue and expand JFS's capacity to carry out its mission of serving of the community
- Maintain a location that provides ease of access from within the City and from the Greater Seattle Area, and is proximate to other Jewish institutions
- Present an architectural image of an organization that supports families and those in need
- Create a positive environment for staff and clients reinforced by ample access to daylight and inviting outdoor spaces
- Be a good neighbor
- Preserve and connect to the existing Jessie Danz Building, which occupies the south half of the site
- Create an inviting entry space that serves both the new building and the existing building
- Provide shared elevator access for both buildings
- Preserve existing landscape
- Create a building that is compatible with the character of the existing building and the scale of the neighboring buildings
- Create a sustainable building that achieves LEED Silver Certification. This will likely inform the project in terms of:
 - Solar control
 - Daylighting
 - Storm water harvesting
 - Landscape design

Desired Uses:

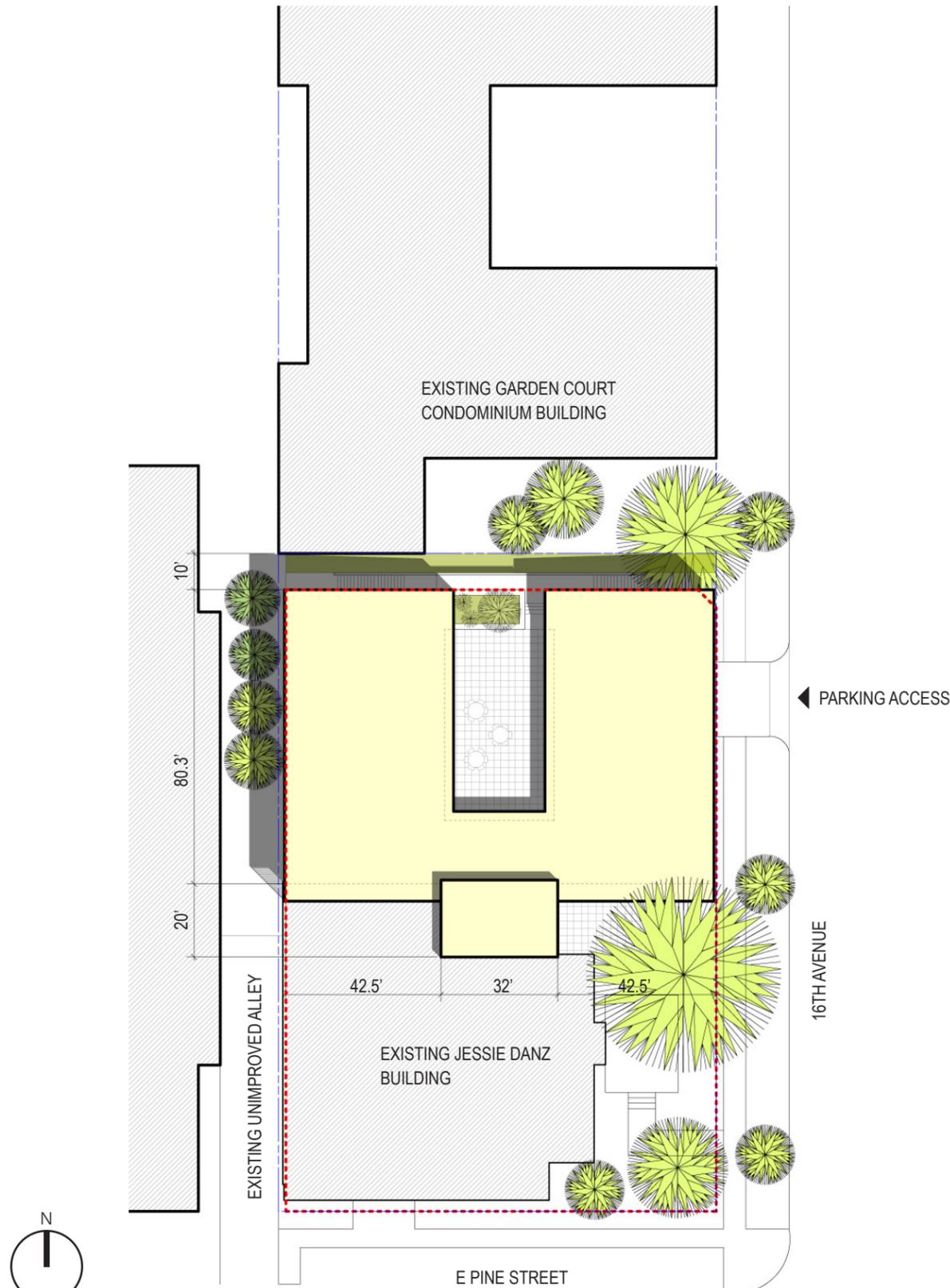
- Non-profit offices and common spaces
- Covered surface parking for 25 cars

Approximate Structure Size:

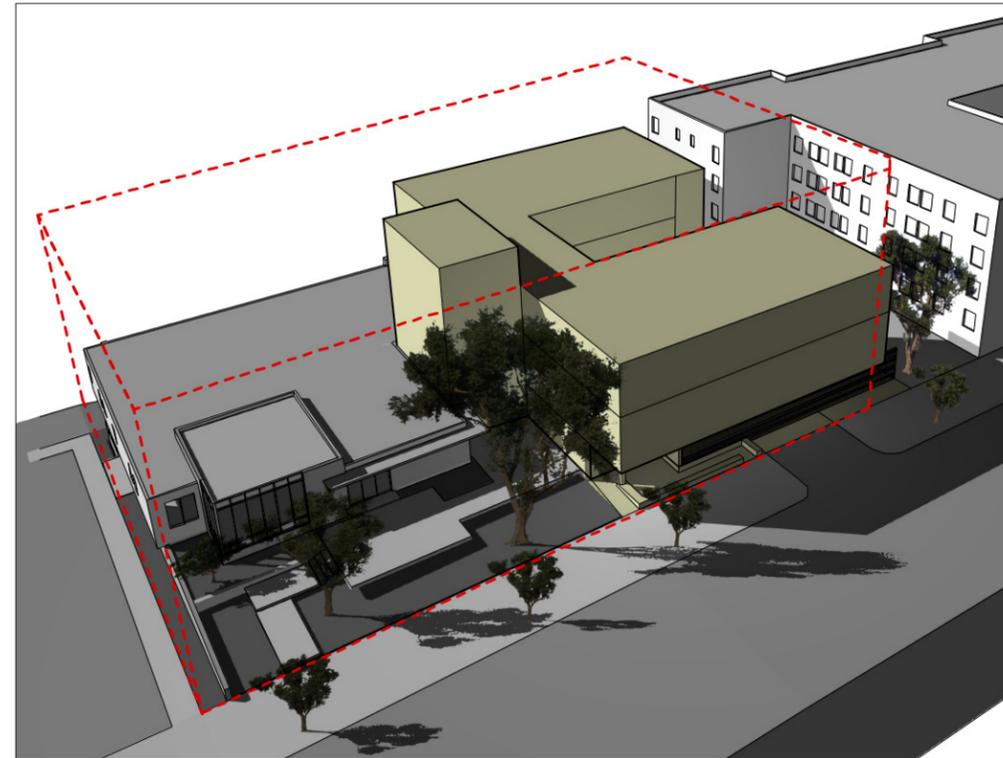
The project includes minor alterations to an existing two-story 13,500-sf office building and the construction of a new 21,000-sf office building on the northern half of the site. The structure is anticipated to be three stories in height with covered open parking comprising much of the first level.

Potential Requests for Design Departures:

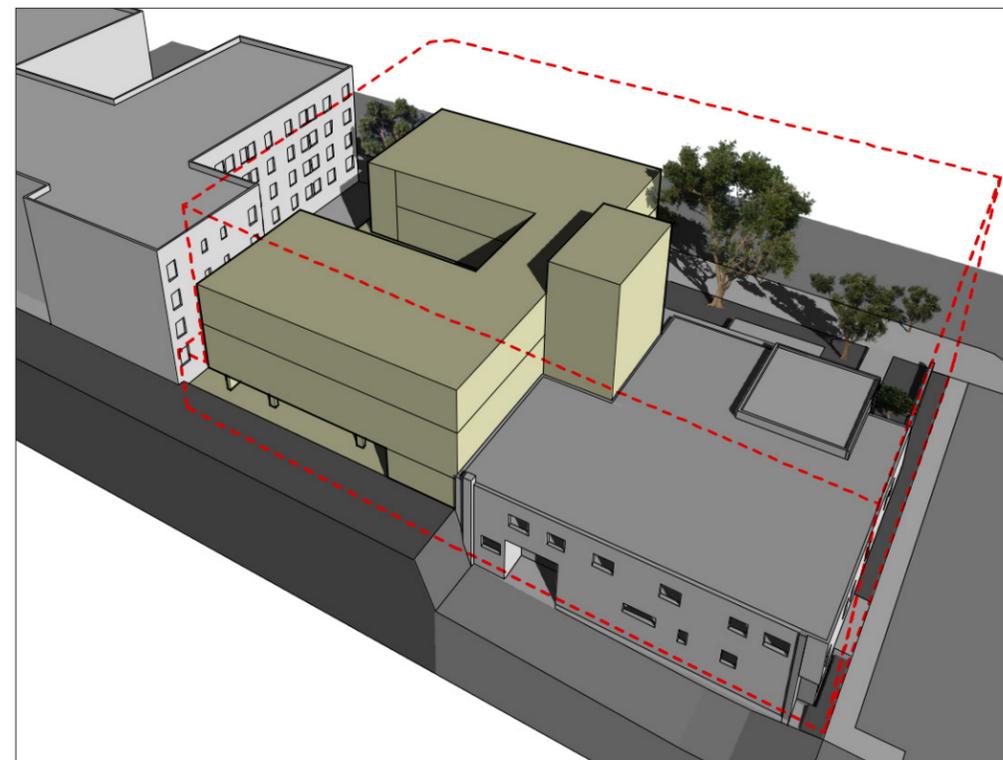
- **23.47A.014 B1: Setback requirements**
We request a departure from the 15' triangular set back required at the intersection of the side lot line and front lot line of the neighboring residential zone. The existing building on the neighboring property is a well-loved and well-maintained four-story brick condominium, which is unlikely to be redeveloped. Because this building is buffered from the subject property by a greater than 20' side setback, we believe the intent of this code section to be satisfied.
- **23.47A.032 B1: Intervening use between parking and sidewalk**
We request a departure from the requirement to separate parking within a structure from the street-level, street facing facades for two segments as described on page 14 of this document.
- **23.54.030 D2a.1: Driveway width**
We may request a reduction in the required width for a non-residential 2-way driveway from the standard minimum of 22'. The proposed driveway would be no less than 18' in width.
- **23.54.030 G3: Sight triangle requirements**
We may request a reduction in the required sight triangle from the 10'-0" standard. The proposed site triangle will likely be 5'-0" to 5'-6", and the applicant is willing to provide supplemental warning devices as recommended.



Massing Alternative 1, site plan



Massing, 16th Avenue view



Massing Alternative 1, alley view

Massing Alternative 1: Courtyard Scheme (Preferred Alternative)

Description

Massing Alternative 1 strives to maximize access to daylight and natural ventilation by organizing the building around a central courtyard. An elevator and restroom core is shared by both the new building and the existing Jessie Danz Building, which was previously not served by an elevator. The street level consists of an open parking garage and a transparent, layered entry zone that provides a common security check, lobby and reception area for both buildings. The courtyard sits atop the parking and is flanked by two levels of offices on either side.

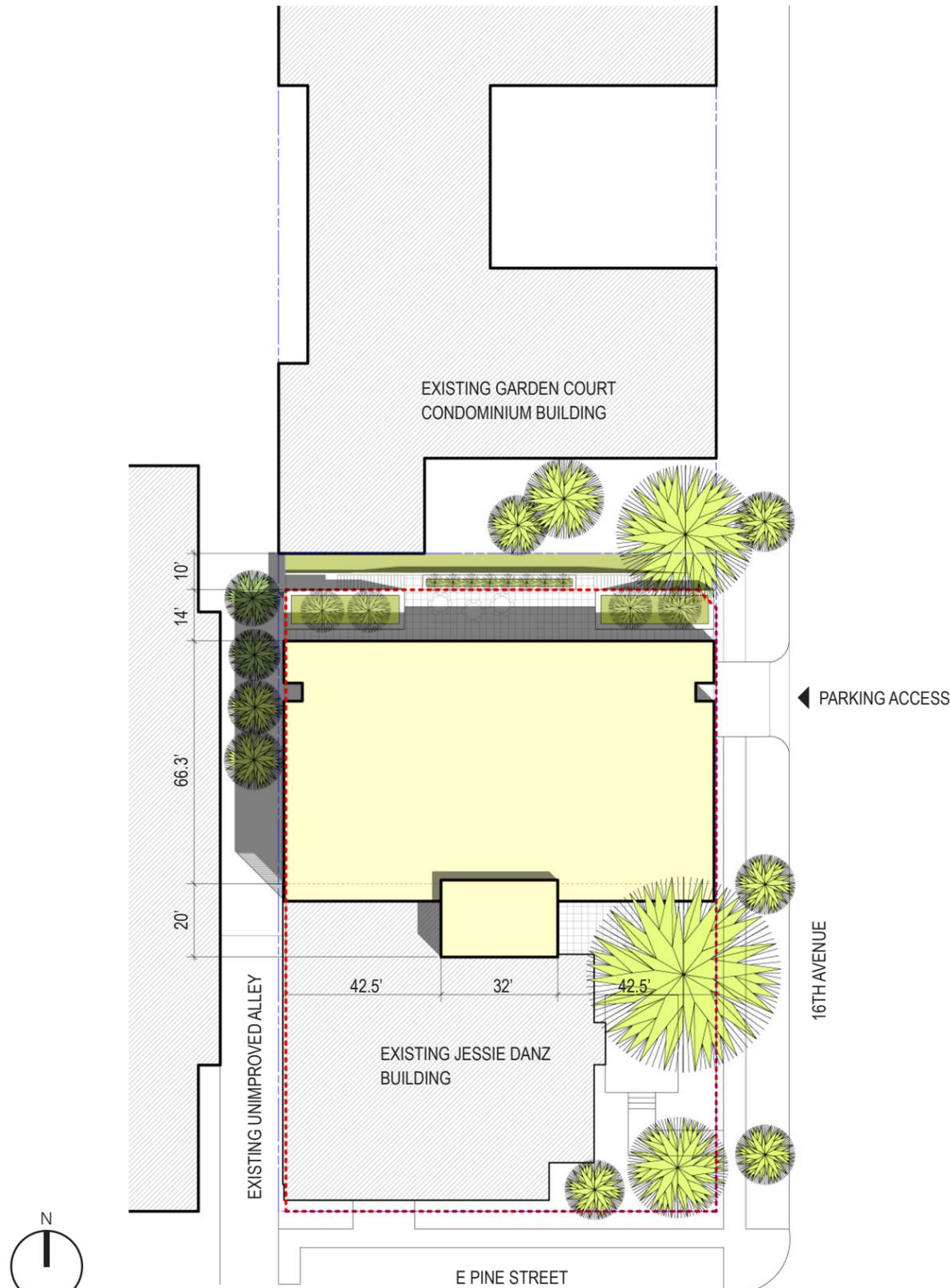
Two alternatives for the street level plan and sidewalk frontage follow (see Street-front Alternatives on pages 14 and 15).

Advantages

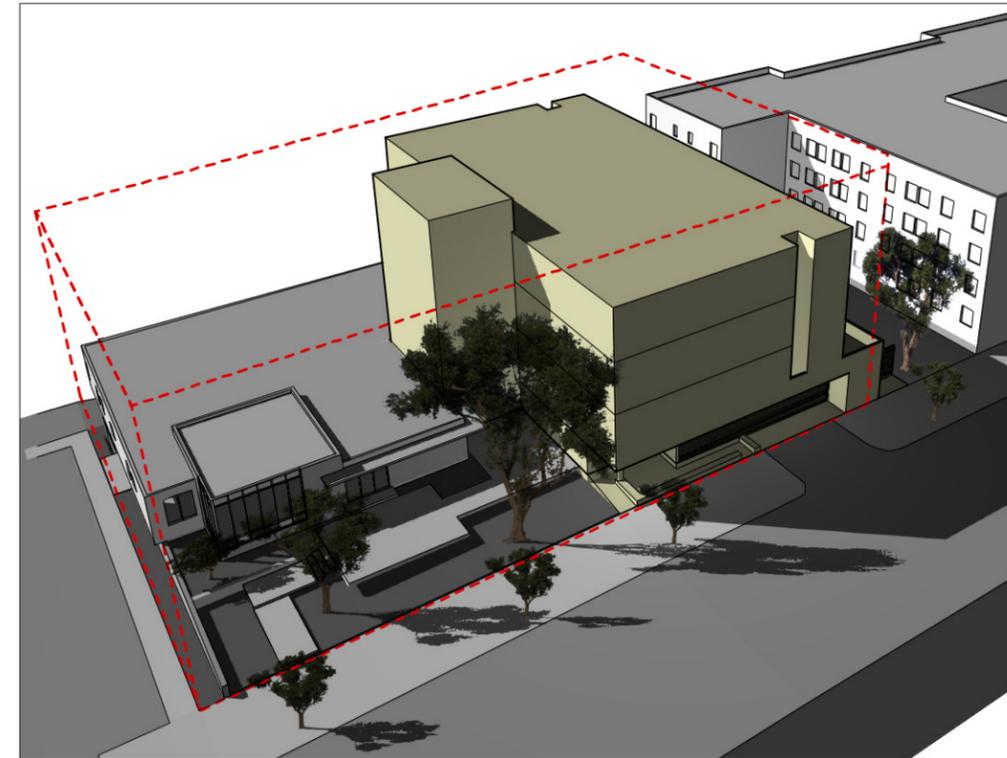
- Creates optimally sized floor plates and maximizes access to light and ventilation
- Preserves access to light and air for non-conforming windows at condominium to the north
- Courtyard is accessible from public spaces within JFS, making it an amenity for clients and visitors as well as employees
- Courtyard provides massing relief for neighbors to the north and maintains ample access to daylight and air.
- Orients office spaces facing east and west, minimizing privacy issues for condominium to the north

Disadvantages

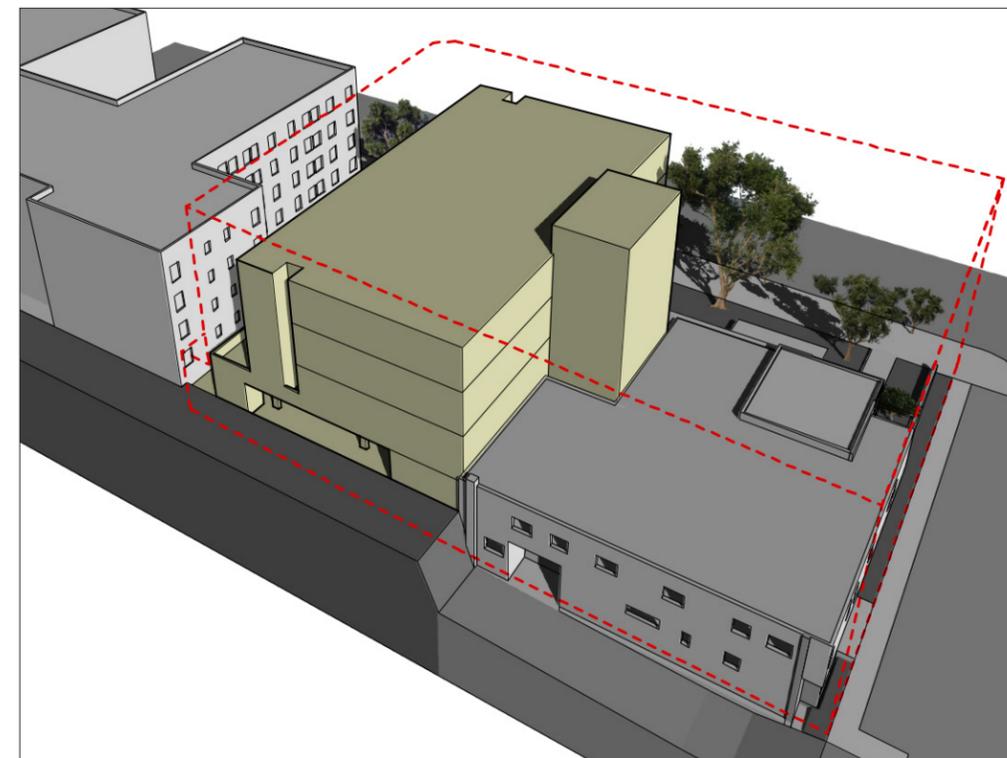
- Increased building perimeter translates to higher cost of construction



Massing Alternative 2, site plan



Massing Alternative 2, 16th Avenue view



Massing Alternative 2, alley view

Massing Alternative 2: Simple Plate Scheme

Description

Massing Alternative 2 seeks to accommodate the program with a simple floor plate. The reduced building perimeter per floor relative to Alternative 1 requires that the building be four stories rather than three stories in order to provide the desired number of closed-walled offices. A terrace along the north end of the 2nd floor provides outdoor space and a privacy buffer between the office uses and the residential condominium to the north.

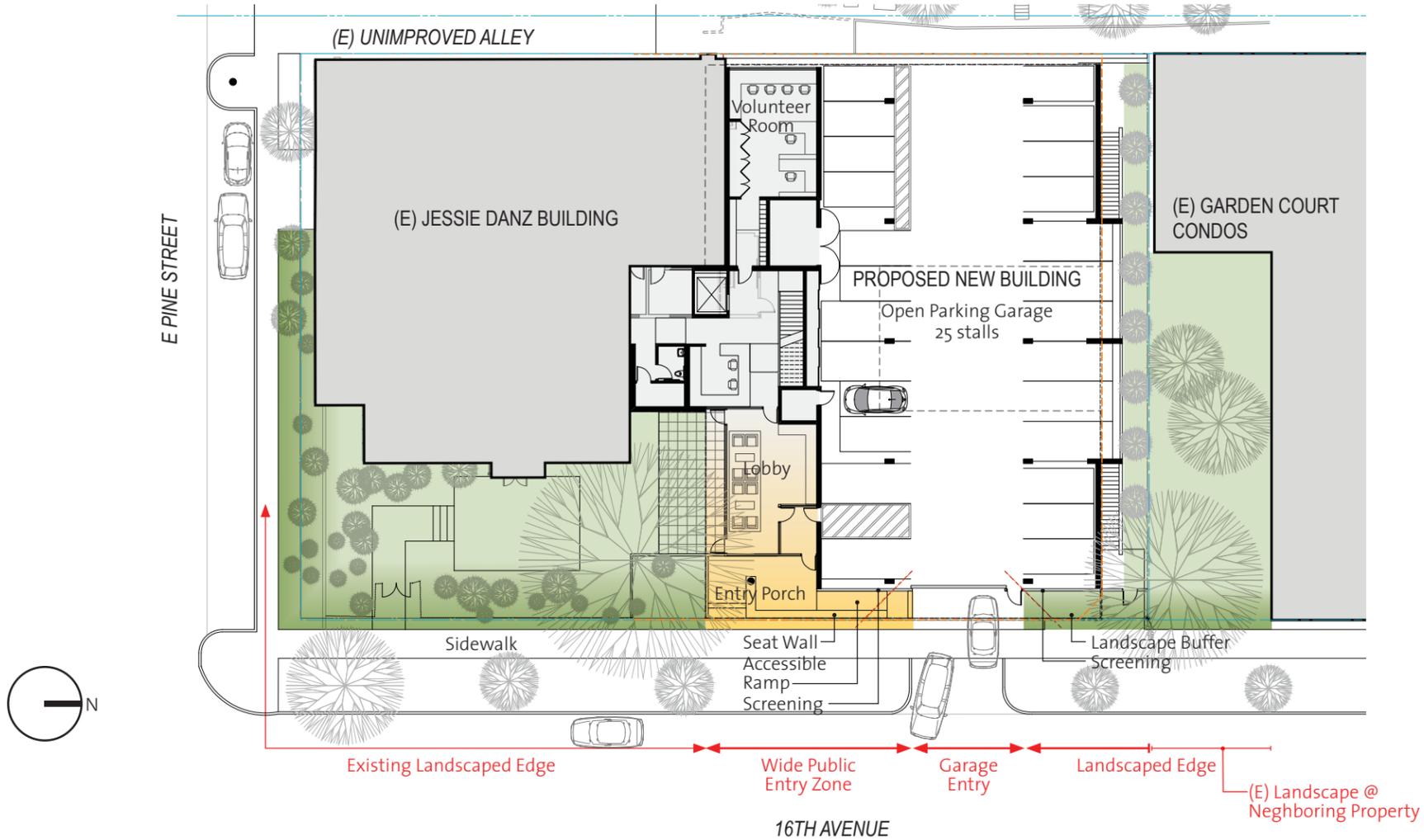
As with Alternative 1, this massing scheme assumes a common elevator and stair core shared by both the new and existing buildings, a street level open parking garage, and a common lobby and security check point. The street level alternatives described on page 14 and 15 also apply to this massing alternative.

Advantages

- Provides greater than required setback from condominium to the north
- Avoids blocking non-conforming windows at condominium to the north
- Provides more office space for JFS

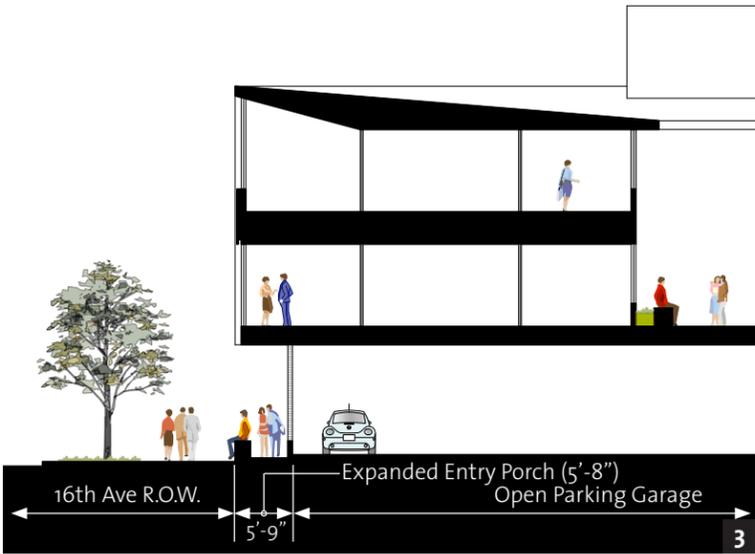
Disadvantages

- Taller building creates more shade and view blockage for condominium to the north
- Orientation of offices encroaches on privacy of residential neighbors to the north
- Narrow north-facing terrace will always be shaded, creating a less appealing outdoor space than the central courtyard
- Terrace space is not well integrated into building mass resulting in limited access
- Wider floor plate is less effective for daylighting and natural ventilation

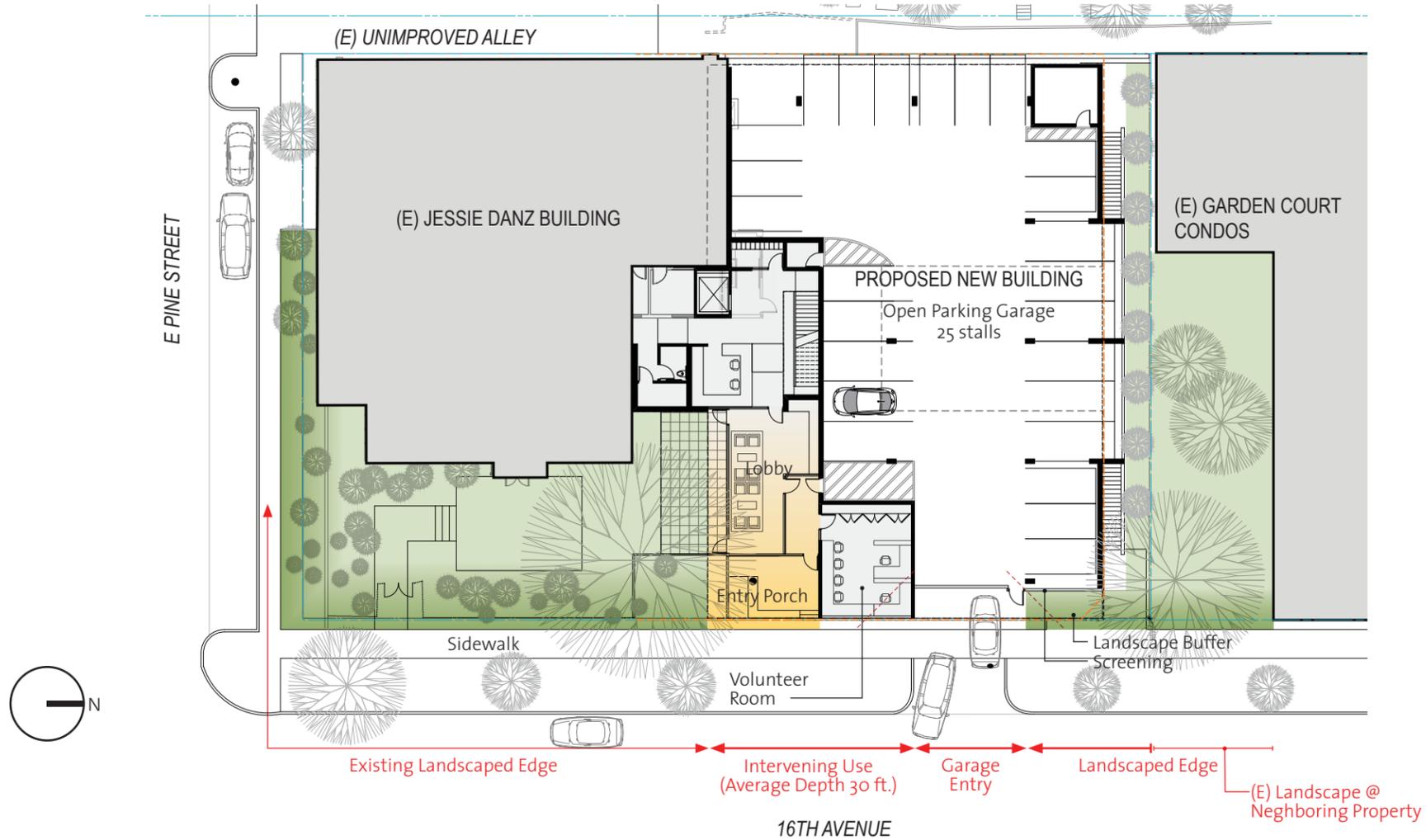


Street-Front Alt. 1: Expanded Entry Porch (Preferred Alternative)

Street-front Alternative 1 proposes a departure from the Land-Use standard that requires an intervening use to separate structured parking from a street-level, street-facing façade. The applicant recognizes the intent of this code requirement to minimize the impact of parking on the pedestrian environment, but believes that the intent can be achieved through other means. By pulling the edge of parking garage back from the property line a buffer zone can be created. To the south of the parking driveway, this buffer zone becomes an extension of the building entry with a continuous seat wall along the sidewalk edge and an accessible walkway tucked behind. An attractive and well-detailed screen wall would enclose the parking garage and form a textured backdrop to the layers of activity generated by the expanded entry porch. The screen wall might be composed of perforated or louvered materials and may even be a suitable signage opportunity. To the north of the garage driveway, a landscape buffer would occupy the 5'-9" setback and extend the vegetated screen wall of the neighboring property onto the site. The existing green edges at the sidewalk on either side of the proposed building are continued, breaking only for the pedestrian and vehicle entries to the building.

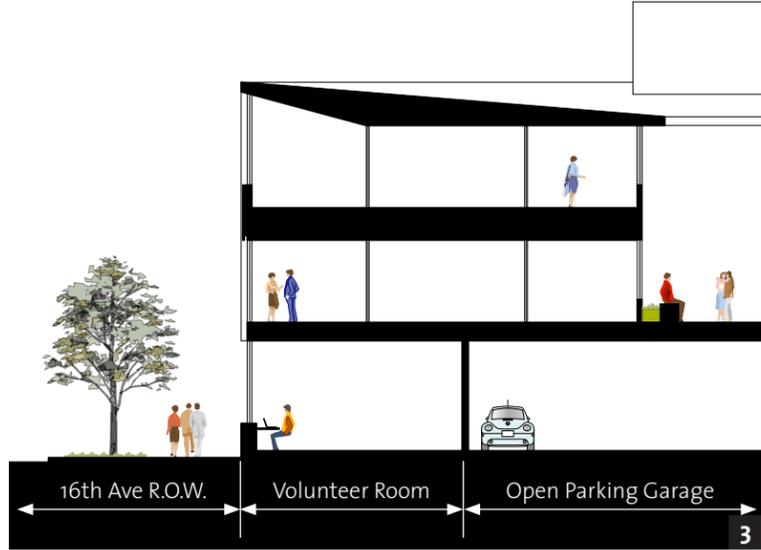


- 1 Aerial view showing 16th Avenue elevation and relationship to the sidewalk. Bench along entry porch is extended to activate the sidewalk edge all the way to garage driveway.
- 2 View from sidewalk looking at entry porch. Entry porch has been extended with a continuous bench. Low-slope entry ramp slips behind bench and in front of perforated or louvered screen wall. Special detailing to ensure attractive treatment of screen wall would be prioritized and signage may be incorporated as shown here.
- 3 Partial Building Section

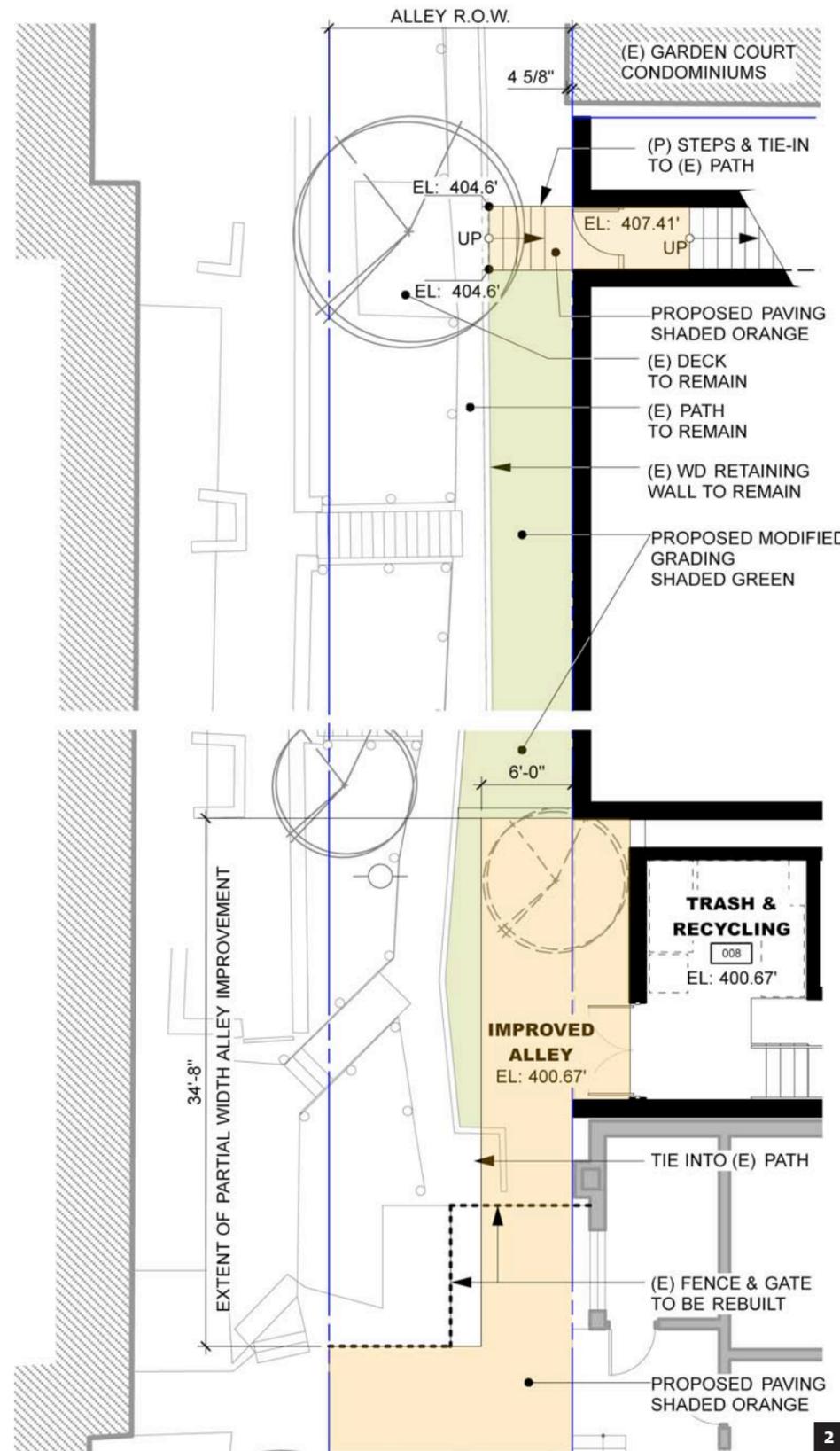
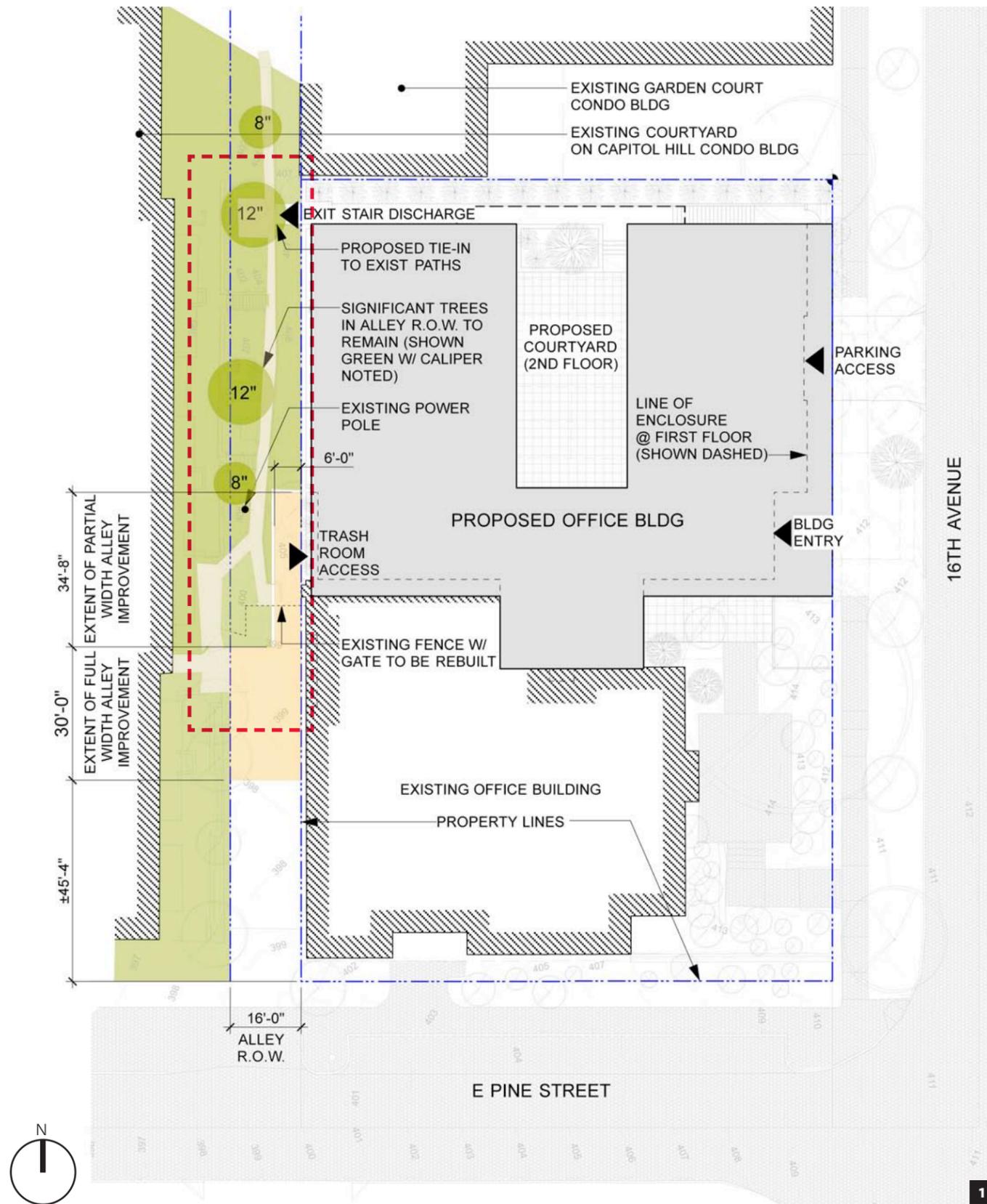


Street-Front Alt. 2: Intervening Use

Street-front Alternative 2 provides a code-complying intervening use between at-grade structured parking and the street-level, street-facing façade. This alternative maintains the same strategy for buffering the parking with landscape to the north of the garage entry as described in Street-front Alternative 1, but locates an office use between the parking and the sidewalk to the south of the driveway. The resultant parking garage is structurally and spatially less efficient than the preferred scheme. The intervening use, an open-office volunteer room, is unlikely to enhance the sidewalk vitality to a greater degree than the preferred alternative. In fact, blinds will likely be drawn for privacy and either bullet resistant glass or some amount of façade opacity will be necessary to address the organization’s security concerns. The location of the Volunteer Room in this scheme orphans it from the rest of the new building and from the existing building. In addition, this scheme narrows the entry zone at the sidewalk, making for a less welcoming street-front presence.



- 1 Aerial view showing 16th Avenue elevation and relationship to the sidewalk. Volunteer Room buffers the sidewalk from the garage.
- 2 View from sidewalk looking at entry porch. Note that office windows are shown with blinds drawn, as proximity to sidewalk affords little privacy to occupants.
- 3 Partial Building Section



As described on page 7 of this packet, the alley that abuts the subject property is largely unimproved. The Seattle Municipal Code requires full alley improvement of an unimproved alley when the alley is used for access to parking spaces, open storage or loading berths on a lot. Because none of the design alternatives propose accessing these items from the alley, improvement of the alley is not required.

The applicant endeavors to preserve much of the vegetation, existing paths and decks that currently serve as amenities for the neighbors. Nevertheless, the design proposal includes some minor areas of improvement to the alley. At the north end of the property, a small paved area will tie in to the existing path in order to allow egress from an exit stair. At the south end of the proposed building, a 6'-wide paved path will link the proposed trash and recycling room with the existing developed portion of the alley so that dumpsters may be carted out to the alley on collection days. Trash collection currently occurs in the alley and the proposed strategy has been reviewed with SPU and received preliminary approval. Unlike the current condition, the owner's dumpsters will not remain in the alley on non-collection days.

Currently, the west edge of the alley is steeply embanked up to the property line. The applicant proposes re-grading these areas more gently in order to reduce areas of unnecessarily steep slope and avoid extensive new retaining walls in the right of way.

The proposed alley improvements have been preliminarily reviewed with SDOT and appear to be within the parameters allowed by a "Field Review Application".

- 1 Site Plan view showing Proposed Alley Improvements shaded orange.
- 2 Enlarged plan showing areas of alley improvement