

u r b a n s i g h t

www.urbansight.net

urbansight is the fruitful collision of passions and talents of gProjects, the builder, and b9 architects, the project architect.



g Projects LLC
contemporary homes with a conscience

206.297.1284 p
206.284.7572 f
www.b9architects.com

b9 architects

27 June, 2007

1818 E Yesler Way

recommendation meeting 1818 e yesler way



① View looking northwest at 19th Avenue and E Yesler Way.



② Looking northwest on 19th Ave



③ Looking southwest on 19th Ave



④ Looking southwest from corner of site on 19th Ave

1. Please describe the existing site, including location, existing uses and/or structures, topographical or other physical features, etc.

EXISTING SITE

Uses

The site is located at the corner of E Yesler Way and 19th Avenue, and is approximately 83 feet east-west and 123 feet north-south. The lot contains an 83' by 49' rectangular, vacant, dilapidated commercial building that fronts E Yesler. The rest of the lot is vacant. The zoning on the lot is split, parallel to E Yesler Way, with NC1-40 facing Yesler and L-3 facing 19th.

Topography

The site drops approximately 6 to 7 feet from north to south along 19th Avenue and approximately 3 feet along E Yesler Way from east to west. The site has territorial views to the west of downtown and perhaps the sound with possible territorial views to the south as well.

Access

The site is bounded by E Yesler Way to the south and 19th Avenue to the east. E Yesler Way is a minor arterial that connects to Pioneer Square. 19th Avenue is a residential street with a pattern of traffic circles heading north from Yesler.



⑤ Looking east on Yesler

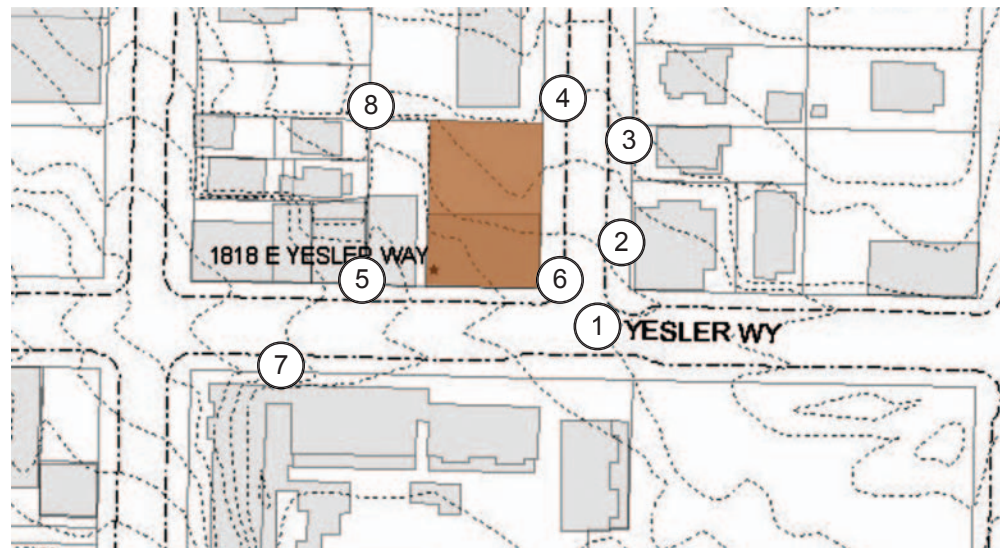


⑥ Looking west on Yesler



⑦ Looking northeast on Yesler at site

⑧ Looking southeast from neighboring lot



existing site conditions

g Projects LLC
contemporary homes with a conscience

206.297.1284 p
206.284.7572 f
www.b9architects.com

b9 architects

27 June, 2007

1818 E Yesler Way

recommendation meeting 1818 e yesler way

Site analysis summary:

E Yesler topography

- Slopes slightly downhill to the west immediately in front of the site, then slopes down steeper to Puget Sound.

19th avenue topography

- Slopes slightly downhill to the south, ending at E Yesler Way.

E Yesler Way traffic

- Minor arterial connecting Central District to downtown.

Solar access

- The 19th Avenue, south, and east sides of the site have good solar orientation. The north side has excellent ambient lighting.

Building mass

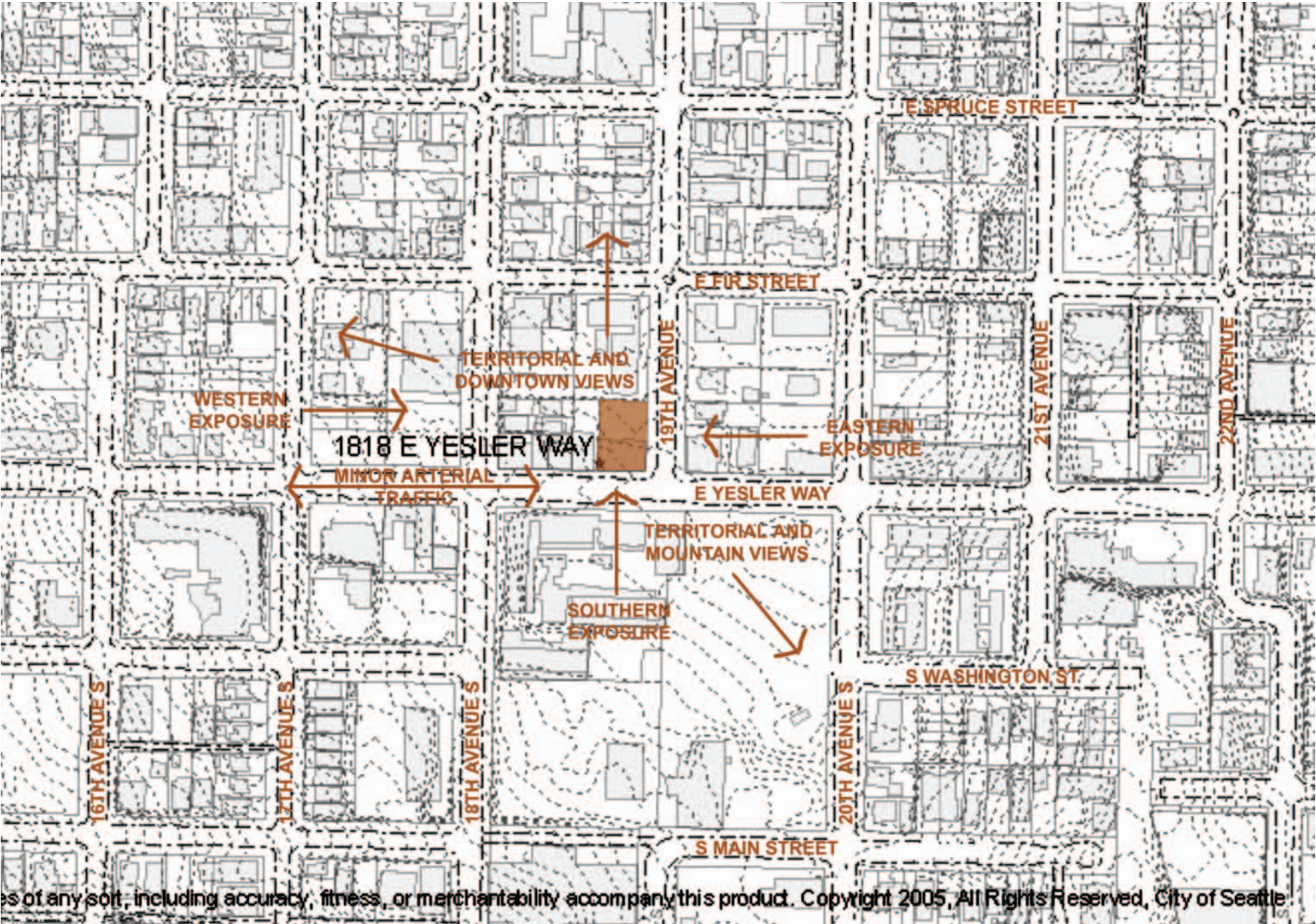
- Desire to address both E Yesler Way and 19th Avenue.

Views

- Territorial and possible sound views to the west towards downtown and potential views to the south.

Parking access

- Propose new, 10 foot curb cut at 19th Avenue for a two-way drive aisle 50 feet from the corner.



es of any sort, including accuracy, fitness, or merchantability accompany this product. Copyright 2005, All Rights Reserved, City of Seattle

27 June, 2007

1818 E Yesler Way

recommendation meeting 1818 e yesler way



① Storefront on E Yesler Way, Seattle Neighborhood Group



⑦ Kawabe Memorial House behind single family residence



② New mixed-use development at 1918 E Yesler Way



⑥ Edwin Pratt Park from 20th Avenue



⑤ Automobile entrance to Midori Condominiums



③ Langston Hughes Theater



④ Storefront on 18th Avenue with grasscrete parking area



No warranties of any sort, including accuracy, fitness, or merchantability accompany this product. Copyright 2005, All Rights Reserved, City of Seattle.

context

g Projects LLC
contemporary homes with a conscience

206.297.1284 p
206.284.7572 f
www.b9architects.com

b9 architects

27 June, 2007

1818 E Yesler Way

recommendation meeting 1818 e yesler way



① panoramic view along e yesler way, looking north



② panoramic view along e yesler way, looking south



③ panoramic view along 19th avenue, looking east



④ panoramic view along 19th avenue, looking west

27 June, 2007

1818 E Yesler Way

recommendation meeting 1818 e yesler way

3. Please describe neighboring development and uses, including adjacent zoning, physical features, existing architectural and siting patterns, views, community landmarks, etc.

Neighborhood Context and Adjacent Zoning

Land Use

The site sits in the middle of commercially zoned district, with recreational and residential uses to the north and south. The site is split zoned, with Neighborhood Commercial 1-40 along E Yesler Way and Lowrise 3 at the back of the site fronting 19th Avenue. This creates a distinct opportunity to merge two seemingly disparate zones at their boundaries. There are several new developments in the Multi-family and Commercial zones adjacent to the site that coexist with the adjacent single family housing stock. Across E. Yesler Way is Edwin T Pratt Park, a very accessible and large green space.

Architecture

The architecture varies dramatically in this neighborhood with a wide range of building types. Along E Yesler Way, there are older apartment buildings, a new mixed-use condominium, small one-story commercial storefronts, a large housing project, a vacant lot, a cultural performing arts center and a parking lot. The neighborhood fabric surrounding our site includes several recently built townhome projects, single family homes, several civic buildings and places of worship. The housing type along E. Yesler Way is predominantly large apartment buildings with smaller apartment buildings and multi- and single-family residences perpendicular to Yesler.

Topography and Views

The land slopes down hill to the west, which will create opportunities for territorial, skyline, and possibly Olympic mountain views. Territorial and possibly mountain views will be available to the southeast over Pratt Park.



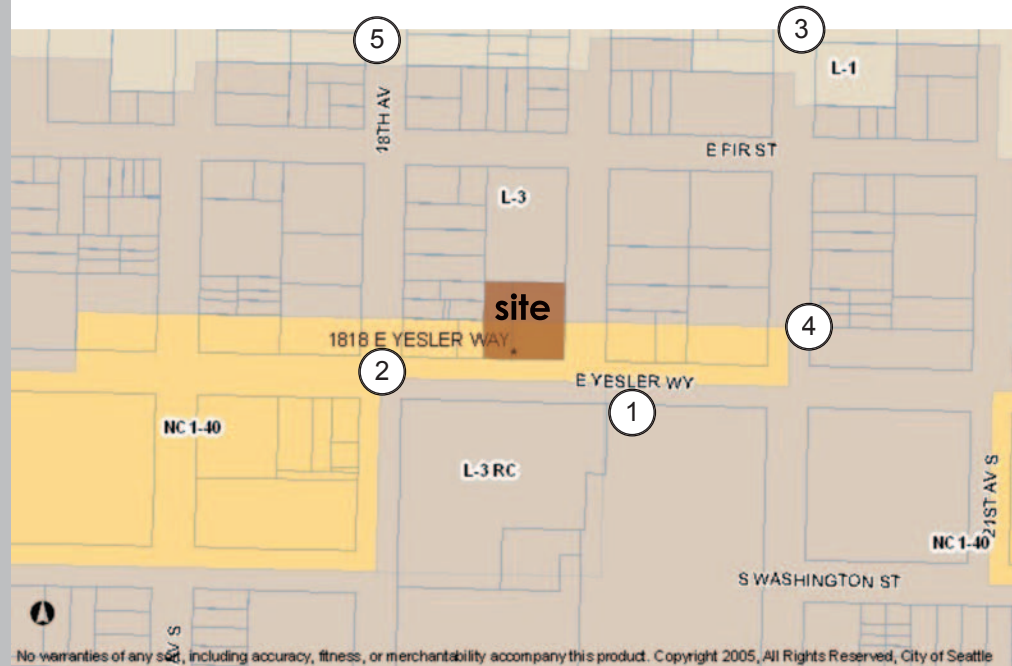
① Edwin Pratt Park from E Yesler Way



② view of Bryant Manor on Yesler



③ view of Rotary Boys and Girls Club on 19th Ave



context - neighborhood

④ view of mixed use development at boundary line of NC1-40 and L-3 at 1918 E Yesler

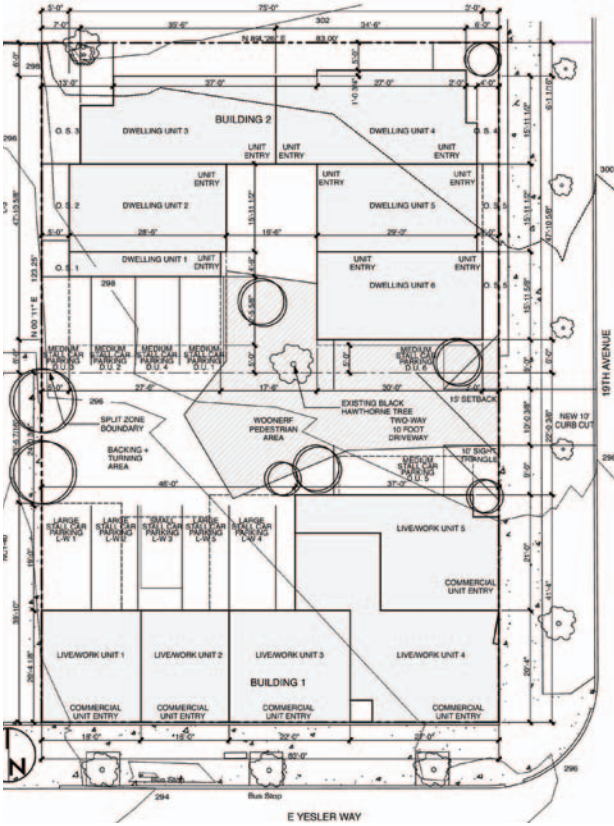


⑤ view of new townhomes on 18th Ave

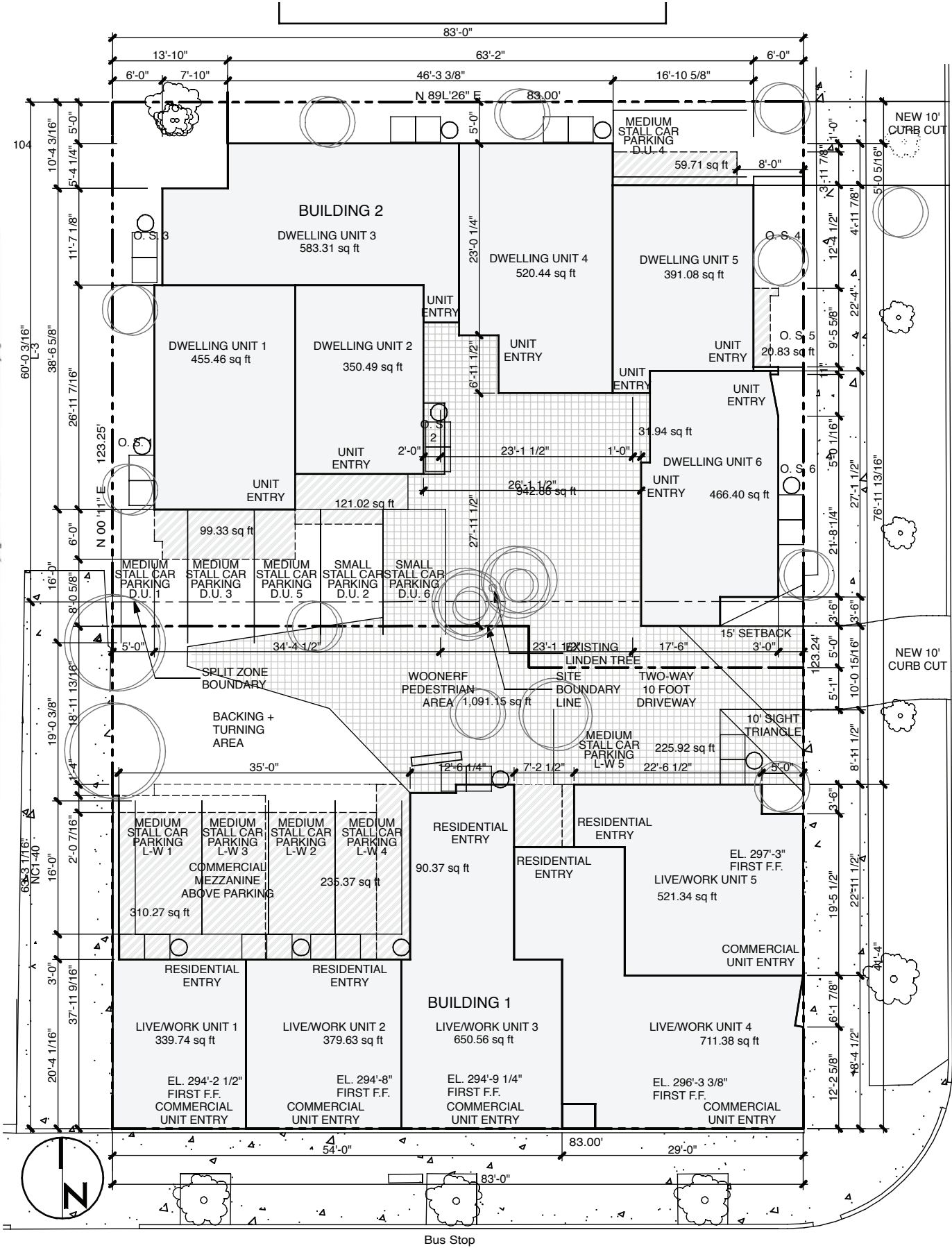




- site responses to early design guidance
- courtyard shifted to east and widened significantly
 - modulation increased
 - stronger resolution of design concept for entire site
 - parking rearranged
 - commercial spaces made more viable
 - courtyard and woonerf daylight increased



EDG site plan



E YESLER WAY

MUP site plan

site response to early design guidance

1. Please describe the proposal in detail, including types of uses, size of structure(s), location of structure(s), amount, location and access to parking, special design treatment of any particular physical site feature, etc.

Project Description

The proponents' objective is to activate the southeast corner of E Yesler Way and 19th Avenue by constructing a thoughtful project that includes five attached, live work homes and six residential townhouses. Bisecting the southern live work half from the residential north half of the project will be a green drive aisle or woonerf providing access to parking, but more importantly providing a treed place for owners to mingle and gather, centered on the existing Black Hawthorne tree. Use of this outdoor space will encourage social interaction and connections among people.

1. Live Work Homes (NC1-40)

Live work homes will provide an opportunity for a business owner to work on the ground floor, while maintaining a personal residence on the two to three floors above. Each of the homes will have a variety of square footages and floor plans, allowing for a variety of street level business to thrive while accommodating different family sizes above. The range of sizes for the street level portion of each home is 600 square feet to 830 square feet. The upper floors will be used for residential purposes, ranging in size from 1415 square feet to 1670 square feet. The preferred design will allow opportunities for the home and business owners to express their individuality through the architecture of their home and business. Environmental consciousness will be expressed through material choice, building practices, and site planning. Natural light will reach deep into the homes and the woonerf because of thoughtful fenestration, double height spaces, and massing choices. Thoughtful architecture and green building practices will demonstrate that these homes are informed by time and place. These homes will be certified built-green and we are exploring a LEEDs certification.

2. Residential Homes (L-3 Portion)

Like the live work homes, the six townhouses style homes will be built to reflect time and place by incorporating green building practices and certification. While addressing 19th Avenue with glass and doors, for security reasons the homes will be focused onto an interior courtyard from which people will come and go. Beyond security, this courtyard will serve as a pedestrian only extension of the woonerf. The home size will vary from 1100 square feet to 1650 square feet, allowing for a variety of family sizes.

A woonerf, according to Walkinginfo.org, is "a Dutch term for a common space created to be shared by pedestrians, bicyclists, and low-speed motor vehicles. They are typically narrow streets without curbs and sidewalks, and vehicles are slowed by placing trees, planters, parking areas, and other obstacles in the street. Motorists become the intruders and must travel at very low speeds below 16 km/h (10 mi/h). This makes a street available for public use that is essentially only intended for local residents."

Approximate Structure SizeAccess and Parking

1. Live Work Homes (NC1-40) Zoning allows 40 feet in height, with additional 4 feet of height with a floor-to-floor height of 13 feet for nonresidential uses at street level as long as the additional height does not allow an additional story beyond the number that could be built under the otherwise applicable height limit. Additionally, a parapet is permitted, up to 4 feet above the allowable height.

2. Residential Homes (L-3 Portion) Zoning allows 30 feet to the plate of a gabled roof with a height bonus for certain gable slopes above 30 feet or 30 feet to the top of a shed roof.

It is our intent to utilize the full height permitted in each respective zone at the street edge of the project along 19th Avenue and E Yesler Way with some variation for modulation of volumes, reducing the height at parts of the woonerf in the interior of the site. We intend to reduce the height of one home along E Yesler Way to allow natural light to permeate the woonerf. The structure size will be consistent with the eclectic building massing rhythm along 19th and E. Yesler Way while providing a solution specific to the corner. Live Work and Residential Homes

Access and Parking

All parking except for one space will take access off the woonerf. Parking for one of the residential homes will be under part of the building on the north side of the project in the side setback on 19th Avenue and the others will park perpendicular to the woonerf under the overhang of two residential homes.

Parking for the Live Work homes will be under the commercial mezzanine of several of the live work homes. One of the units will park in a parallel spot off the woonerf. The proposed access is the single width woonerf, which will be paved with grass-crete or similar, allowing vegetation to paint the area green. The access will be treed to the extent possible, and taking design cues from a successful Pearl District project where people, trees, and cars happily co-exist. The parking will be mostly shielded when viewing the woonerf from 19th Avenue South.



① view from southeast at corner of 19th Avenue and E Yesler Street



② view from southwest on E Yesler Street



③ view from northeast on 19th Avenue

3. Please describe how the proposed design responds to the early design guidance by the Design Review Board:

The Design Review Board's guidance has significantly strengthened this project in many key ways. The two structures are more cohesive design wise and physically closer, strengthening their ties. The common outdoor space is more useable because it shifted east, is larger, and will feature high quality, soulful materials, lighting, and landscaping. Private outdoor spaces are minimized, and where appropriate, fences are kept low. The commercial portion of the live work homes is enhanced because the mezzanines directly connect to the ground floor and the ceiling heights are increased.

The character of the commercial building has informed the architecture of the entire project. At street level, the commercial spaces have an abundance of glass to encourage activity and visual connection between the street and the interiors. While the transparent facade at street level prioritizes the ground floor, it also informs decisions about fenestration for the remainder of the building. Large openings are created in the façade in the form of decks, with the wall setback from the street, and with glazing. This design treats the façade as a single commercial building with live-work homes.

As illustrated on the elevations, the façades are composed of a series of three-dimensional interlocking shapes clad in stained cedar and colored cement fiber boards (James Hardie Panel or Hardie Panel). Both the cedar and Hardie Panel will be attached over a rain screen system with gaps between boards and panels.

While the new design of the Multifamily building takes architectural guidance from the Live-work building, its scale is reduced to three stories. The parapets that characterize the nature of the commercial building are picked up on the multifamily building, directly across the woonerf, before evolving to a shed roof with overhangs. Architectural manipulation of the multifamily façade is similar to the commercial building while transitioning to the neighboring multifamily lots. The physical gap between the buildings is now reduced, stitching the project closer together, and reducing the street frontage hole on 19th Avenue.

After our sun studies we shifted the common outdoor space to the east to receive more afternoon sunlight. In addition the space, which was approximately 16 x 50 feet, or 800 square feet, and oriented north-south, has been rearranged to 27 x 50 feet, or 1100 square feet. While there are no actual parking stalls in the key pedestrian areas, we anticipate and will promote activity to spill out into the woonerf.

The ground surfaces in the woonerf and common outdoor space will be covered with recycled cobblestones, hand set in sand to encourage natural infiltration. The homes' entrances will be marked by large, cast in place rectangular pavers, with strawberries between the rectangular shaped pavers. Turfcrete will be utilized in more car oriented areas, and concrete will be poured in areas where grass cannot grow. The woonerf and outdoor space will be activated by the placement of tables, benches, mailboxes and landscaping, and softened by the choice of wood siding along the lower levels of the building facades. With the Board's insights, the heart of the project is now more useable and light filled.

The formal entries for each home are still found off the woonerf, however, additional doors for units facing 19th Avenue have been added. The additional doors will sponsor activity and safety in the space between the buildings and the sidewalk, and we believe that some homeowners will use the street parking adjacent to their home and enter through this door.

While each home's primary outdoor open space is the common area, each multifamily home will have small private outdoor retreat area. These will be marked with a 4 to 6 foot cedar fences, made of horizontal 1 x 2 members with 3/8 inch gaps. Where separating the individual open spaces from the right-of-way, however, these fences will be 3 feet tall, to promote interaction with the sidewalk.

The homes have been designed with two different kinds of outdoor lighting: a front door light, signifying entry at the woonerf and at the public streets and additional lights strategically placed throughout the woonerf. In order to provide sufficient light for security in the woonerf, these lights will automatically come on at dark, and become brighter when motion is detected. The lighting is strategically placed to avoid light spilling onto adjacent properties.

In regards to the common outdoor space, the overall design, and the cohesion between the buildings, the project is greatly strengthened after digesting and incorporating the Board's Early Design Guideline Priorities.



27 June, 2007

1818 E Yesler Way

recommendation meeting 1818 e yesler way

DEPARTURE MATRIX FROM DEVELOPMENT STANDARDS:

Development Standard	Requirement	Proposed	Comment/Rational BY Architect	Board Recommendation
1. Lot coverage. 23.45.010.A	Maximum lot coverage for townhouses is 50%	3140 sf/5389 sf = 58%	The two lots are designed as a single project and the total lot coverage over both is 3140 sf (L3) + 3674 sf (NC) = 6814 sf / 10233 = 67%	The board will continue to entertain this departure but needs to show clear evidence of how the overall design meets these priority guidelines.
2. Structure width. 23.45.011.A	Maximum building width without modulation 30 feet; or 40 feet with a principal entrance facing a street. Maximum building width with modulation 45 feet	40'-7" with 3 foot modulation. 52'-11 3/8" with 5 foot modulation	The project proposes multiple steps in the front building wall over its total length.	The board will continue to entertain this departure but needs to show clear evidence of how the overall design meets these priority guidelines.
3. Structure depth. 23.45.011.A	Maximum building depth 65% of depth of lot.	75 feet or 90% of lot depth	In order to create an amenity space at the project's center, the building pushes to the sidewalk and mitigates the transition from NC1-40 to its L3 neighbor.	The board will continue to entertain this departure but needs to show clear evidence of how the overall design meets these priority guidelines.
4. Front setback 23.45.014.A	Required front setback shall be the average of structures on either side or not to exceed 15 feet and not less than 5 feet.	6 to 3 feet.	Again, in order to create the common central space, the building pushes to the sidewalk and mitigates the transition from NC1-40 to its L3 neighbor.	The board will continue to entertain this departure but needs to show clear evidence of how the overall design meets these priority guidelines.
5. Rear setback 23.45.014.B	Required 25 feet or 15 percent of lot depth whichever is less, but in no case less than 15 feet.	13'-10" to 5 feet.	Again, in order to create the common central space, the building pushes to the sidewalk and mitigates the transition from NC1-40 to its L3 neighbor.	The board will continue to entertain this departure but needs to show clear evidence of how the overall design meets these priority guidelines.
6. Open space 23.45.016	An average of 300 square feet of private usable open space per unit, but no unit shall have less than 200 square feet.	Less than 200 square feet per unit and less than 300 square foot average per unit.	The common amenity space is prioritized instead of individual open spaces. Reduction in open space is justified due to proximity to Pratt Park.	The board will continue to entertain this departure but needs to show clear evidence of how the overall design meets these priority guidelines.
7. Driveways 23.54.030.D	A minimum driveway width of 22 feet shall be required for two-way driveways serving commercial uses (live-work)	10 feet proposed.	A woonerf, a living street, is proposed at the center of the project, for pedestrian and vehicular access. To reduce automobile speed, create a single design for both buildings and minimize the parking access the driveway is reduced.	The board will continue to entertain this departure but needs to show clear evidence of how the overall design meets these priority guidelines.
8. Parking Aisle Dimensions Exhibit 23.54.020 D	For 19-foot stall length at 90 degrees, a 24-foot aisle is required (live-work). For 16-foot stall length at 90 degrees, a 22-foot aisle is required. (multifamily)	19 feet proposed.	The woonerf creates a place for vehicles and pedestrians. Reducing the parking aisle addresses the intent of the woonerf.	The board will continue to entertain this departure but needs to show clear evidence of how the overall design meets these priority guidelines.
9. Setbacks in Commercial Zone 23.47.014	Setback is required at the intersection of a side lot line and front lot line in a residential zone. 2 sides of the triangle must each extend along the street lot line and side lot line fifteen 15 feet from the intersection of the street lot line and the side lot line abutting the residentially zoned lot. A setback is required along any rear or side lot line that abuts a lot in a residential zone of 10 feet for portions of structures above thirteen 13 feet in height to a maximum of sixty-five 65 feet	Proposed from the residentially zoned structure, not from the lot line. 5'-1" at 19th Avenue, 19'-0 1/2" at the interior of the site	A 10-foot minimum setback is maintained at the driveway entrance from the street between building facades. This is required for building code. At the interior of the site the setback is 19 feet to the property line and is modulated between opposite building faces to enhance the woonerf and the outdoor living room.	The board will continue to entertain this departure but needs to show clear evidence of how the overall design meets these priority guidelines.
10. Parking Space Requirements 23.54.030 B	2. Nonresidential Uses and Live-work Units. a. When ten (10) or fewer parking spaces are provided, a maximum of twenty-five (25) percent of the parking spaces may be striped for small vehicles. A minimum of seventy-five (75) percent of the spaces shall be striped for large vehicles.	Medium stalls proposed for all five required parking spaces.	A departure is required for parking stall size for the live-work units to provide space at the nose of the stall under the mezzanine for the garbage/recycling containers for three of the live-work units. This is necessary as we address the board's concern regarding the impacts associated with internal trash pick-ups. We believe that the woonerf experience will be improved by providing individual cans to each unit.	The board will continue to entertain this departure but needs to show clear evidence of how the overall design meets these priority guidelines.

- A

ALLOWABLE STRUCTURE DEPTH
- B

STRUCTURE IN ALLOWABLE FRONT AND REAR SETBACKS, ADDITIONAL STRUCTURE DEPTH REQUIRED, DEPARTURE REQUIRED
- C

COMMON AMENITY SPACE ALONG WITH REDUCED PRIVATE OPEN SPACES, DEPARTURE REQUIRED
- D

3-FOOT MODULATION, DEPARTURE REQUIRED
- E

DRIVEWAY AT REQUIRED WIDTH FOR ONE-WAY RESIDENTIAL ACCESS.
- F

PARKING AISLE WIDTH, DEPARTURE REQUIRED
- G

COMMERCIAL ZONE SETBACK AT THE INTERSECTION OF A SIDE LOT LINE AND FRONT LOT LINE IN A RESIDENTIAL ZONE, DEPARTURE REQUIRED
- H

COMMERCIAL ZONE SETBACK ALONG ANY REAR LOT LINE THAT ABUTS A LOT IN A RESIDENTIAL ZONE
- I

LOT COVERAGE, DEPARTURE REQUIRED

AREAS LABELED "A" — THESE AREAS ILLUSTRATE THE ALLOWABLE STRUCTURE DEPTH WITHOUT A DEPARTURE.

AREA LABELED "B" — ADDITIONAL STRUCTURE DEPTH IS REQUIRED TO CREATE THE PUBLIC AMENITY SPACE, THE SHARED OUTDOOR LIVING ROOM, AT THE CENTER OF THE PROJECT. BY PUSHING THE BUILDING TO THE CLOSER TO THE EDGES OF THE PROPERTY, THE PROJECT PROVIDES A 28-FOOT BY 27-FOOT, 756 SF OUTDOOR AMENITY SPACE (AREA LABELED 'C' ON PLAN).

AREA LABELED "C" — A SHARED OUTDOOR LIVING ROOM SIZED AT 28-FOOT BY 27-FOOT, 756 SF, ALONG WITH THE IMMEDIATE PROXIMITY TO PRATT PARK, SUPPORTS THE DECISION TO REDUCE PRIVATE OPEN SPACE. THIS SPACE EXTENDS INTO THE WOONERF AND GRABS ANOTHER AREA, 27 FEET BY AS MUCH AS 13 FEET IN ADDITIONAL DEPTH, INCREASING THE TOTAL AREA TO 1100 SF. THE LAND USE CODE FOR MULTIFAMILY ZONES IS BEING REWRITTEN TO INCLUDE AMENITY SPACES INSTEAD OF PRIVATE USABLE OPEN SPACE AND THIS DESIGN ANTICIPATES THAT.

AREA LABELED "D" — ALTHOUGH MODULATION AT THIS LOCATION IS ONLY 3 FEET, THE TOTAL MODULATION ALONG THE WIDTH OF THE FRONT OF BUILDING 2 IS 5 FEET, STEPPING BACK FROM A 3-FOOT SETBACK TO AN 8-FOOT SETBACK. THE FACADE IS FURTHER REDUCED IN SCALE AND MODULATION WITH A DECK AND RECESS ALONG THE 40'-7" LENGTH BEFORE THE 3-FOOT MODULATION OCCURS.

AREA LABELED "E" — A DEPARTURE HERE IS CRITICALLY IMPORTANT TO THE DESIGN APPROACH OF THE ENTIRE PROJECT. IN ORDER FOR THE TWO BUILDINGS AND THE WOONERF TO BECOME ONE PROJECT, THE DRIVEWAY AND SETBACK BETWEEN THE BUILDINGS MUST BE TO THE MINIMUM REQUIRED FOR BUILDING CODE, 10 FEET. THIS PROVIDES ARCHITECTURAL CONTINUITY ALONG THE 19TH AVENUE FACADE, A CONCERN OF THE BOARD FROM EARLY DESIGN GUIDANCE. ADDITIONALLY THE REDUCED DRIVEWAY WIDTH CREATES THE APPROPRIATE SCALE AND SPEED FOR THE WOONERF. THE OPEN GROUND SPACE AT THE DRIVEWAY IS WIDER THAN 10 FEET, TO INCLUDE ONE PARALLEL PARKING SPACE FOR THE LIVE WORK UNIT ADJACENT TO IT. BY REDUCING THE DRIVEWAY WIDTH TO THE RESIDENTIAL MINIMUM WIDTH UNDER THE CODE, THE WOONERF STARTS NARROW AT 19TH AVE AND WIDENS INTO THE SITE TO INCLUDE AREAS 'C' AND 'F' ON THE PLAN.

AREA LABELED "F" — A DEPARTURE FOR PARKING AISLE WIDTH IS REQUIRED TO MAINTAIN THE GREATEST POSSIBLE DEPTH FOR THE COMMERCIAL SPACES FRONTING E YESLER WAY. AS PART OF A PUBLIC OUTREACH PROCESS, NEIGHBORS VOICED CONCERNS FOR VALBIE COMMERCIAL SPACES IN THIS LOCATION SO EVERY FOOT OF COMMERCIAL DEPTH IS CRITICAL. DUE TO THE ANAOMOLY OF THE SPLIT ZONE AT THE SITE, THE PARKING ACCESS IS GREATLY LIMITED BY THE CODE. THIS DEPARTURE ALSO PROVIDES SUFFICIENT SQUAREFOOTAGE FOR THE MULTIFAMILY TOWNHOMES, DWELLING UNITS 1, 2 AND 3, WHOSE WIDTHS AND DEPTHS ARE IMPACTED BY THE SIZE AND LOCATION OF THE OUTDOOR AMENITY SPACE, AREA 'C'. BY REDUCING THE PARKING AISLE REQUIREMENT FROM 24 FEET FOR LARGE PARKING STALLS, REQUIRED FOR THE LIVE-WORK UNITS, AND FROM 22 FEET FOR MEDIUM PARKING STALLS REQUIRED FOR THE MULTIFAMILY UNITS, IT REINFORCES THE INTENTION OF THE WOONERF: A PLACE WHERE THE PEDESTRIAN IS PRIORITIZED, WHILE STILL PROVIDING A REASONABLE DEPTH TO MANUEVER THE CAR.

AREAS LABELED "G" — THE REQUIRED SETBACK IS PROVIDED BETWEEN THE BUILDING WALLS. THE CODE REQUIRES IT TO THE PROPERTY LINE. IN ORDER TO MAINTAIN ARCHITECTURAL CONTINUITY ALONG 19TH AVE, A CONCERN OF THE BOARD FROM EARLY DESIGN GUIDANCE, THE SETBACK HAS BEEN COMPRESSED. ADDITIONALLY THE REDUCED SETBACK CREATES THE APPROPRIATE SCALE FOR THE WOONERF. THE OPEN GROUND SPACE AT THE DRIVEWAY IS WIDER THAN 10 FEET, TO INCLUDE ONE PARALLEL PARKING STALL AND LANDSCAPE.

AREA LABELED "H" — THE REQUIRED SETBACK IS PROVIDED 29 FEET INTO THE SITE. IN REALITY, THE PROJECT IS DESIGNED AS ONE DESIGN AND IS LIMITED BY THE SPLIT ZONE DESIGNATION. FOR ALL THE REASONS DESCRIBED ABOVE IN AREA 'G', THE SETBACK MUST BE REDUCED AT THE ENTRY TO THE WOONERF.

AREA LABELED "I" — BUILDING 2 EXCEEDS THE ALLOWABLE 50% LOT COVERAGE ON THE MULTIFAMILY ZONED LOT. THIS DEPARTURE IS CREATED BY ALL OF THE DESIGN DECISIONS THAT HAVE BEEN DESCRIBED ABOVE. WITHOUT ADDITIONAL LOT COVERAGE, AREA 'C' WOULD NOT BE ABLE TO EXIST. AREA 'C' EXTENDS FROM THE WOONERF TO CREATE THE HEART OF THE PROJECT. IN ADDITION, THE BOARD GUIDELINES SPECIFICALLY DIRECTED THE PROJECT TO PROVIDE A MORE SUCCESSFUL TRANSITION BETWEEN THE TWO LOTS. IN ORDER TO MEET THIS DIRECTION, BUILDING 2 IS DESIGNED TO BE MORE SIMILAR TO BUILDING 1, REDUCING ITS FRONT SETBACK AND EXTENDING ITS ARCHITECTURAL APPROACH TO CREATE A CONTINUOUS PROJECT.

A DEPARTURE IS REQUIRED FOR PARKING STALL SIZE FOR THE LIVE-WORK UNITS TO PROVIDE SPACE AT THE NOSE OF THE STALL UNDER THE MEZZANINE FOR THE GARBAGE/RECYCLING CONTAINERS FOR THREE OF THE LIVE-WORK UNITS. THIS IS NECESSARY AS WE ADDRESS THE BOARD'S CONCERN REGARDING THE IMPACTS ASSOCIATED WITH INTERNAL TRASH PICK-UPS. WE BELIEVE THAT THE WOONERF EXPERIENCE WILL BE IMPROVED BY PROVIDING INDIVIDUAL CANS TO EACH UNIT.





① sun study — 1 pm

aerial views — july 4



② sun study — 3 pm



③ sun study — 5 pm



④ sun study — 6 pm



⑤ sun study — 1 pm

aerial views — september 1



⑥ sun study — 3 pm



⑦ sun study — 5 pm



⑧ sun study — 6 pm



① sun study — march 21

aerial views — 5 pm



② sun study — april 21



③ sun study — may 21



④ sun study — june 21



⑤ sun study — march 21

woonerf views — 5 pm



⑥ sun study — april 21



⑦ sun study — may 21



⑧ sun study — june 21



① sun study — july 21

aerial views — 5 pm



② sun study — august 21



③ sun study — september 21



④ sun study — october 21



⑤ sun study — july 21

woonerf views — 5 pm



⑥ sun study — august 21



⑦ sun study — september 21

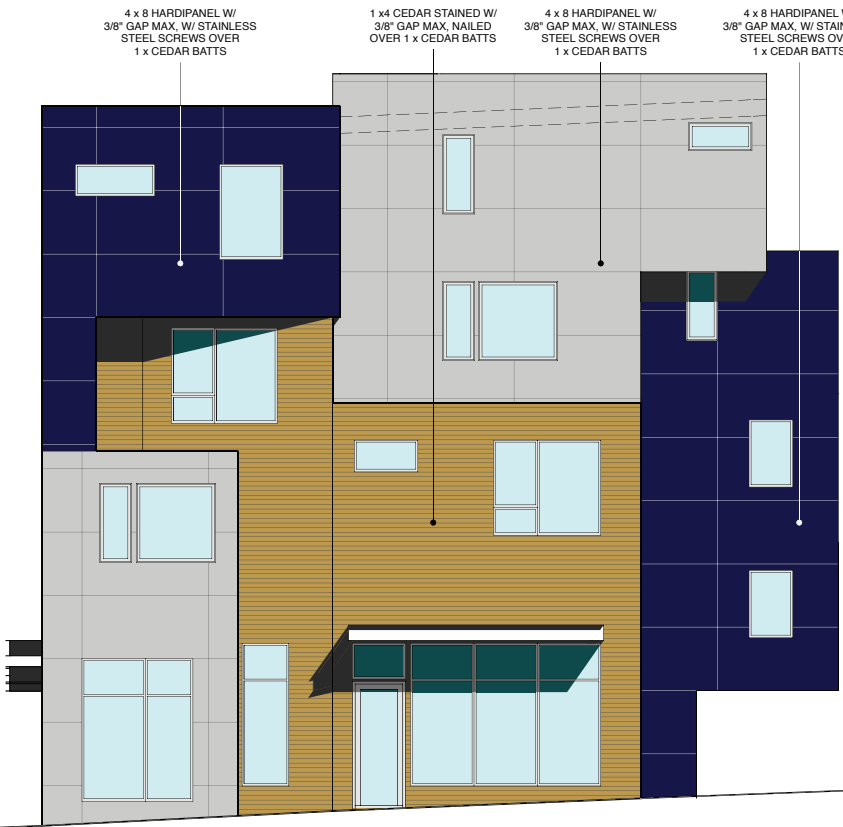


⑧ sun study — october 21



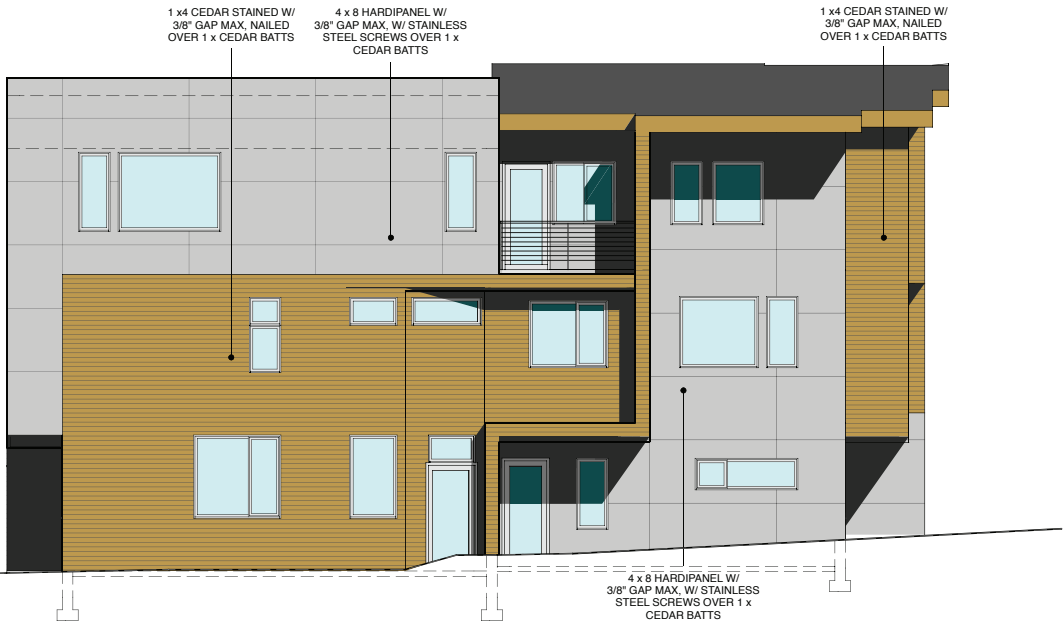
1 SOUTH ELEVATION BUILDING 1

SCALE: 0' 2' 4' 8' 16'



2 EAST ELEVATION

SCALE: 0' 2' 4' 8' 16'





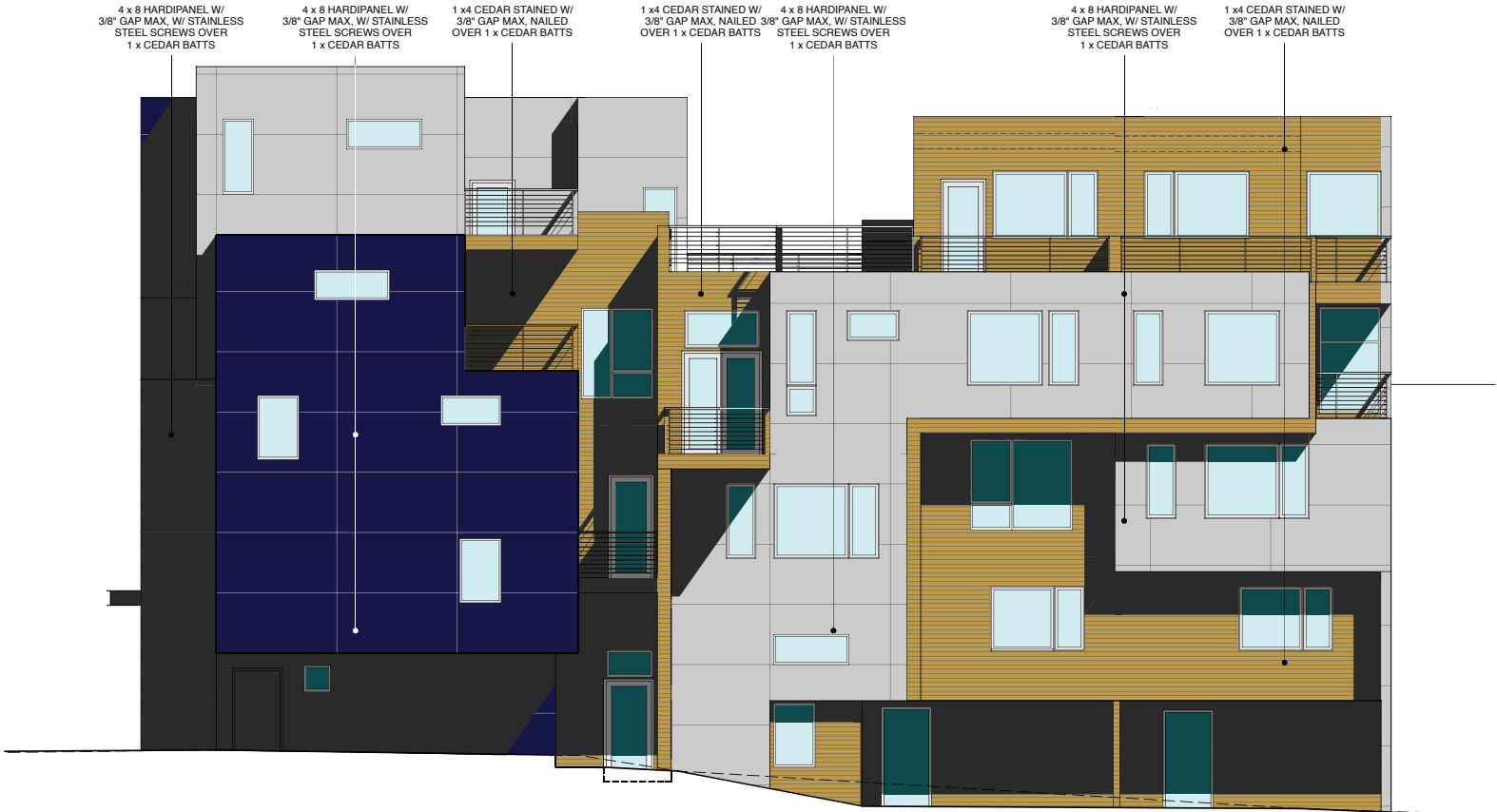
4 | COURTYARD EAST ELEVATION

SCALE: 0' 2' 4' 8' 16'



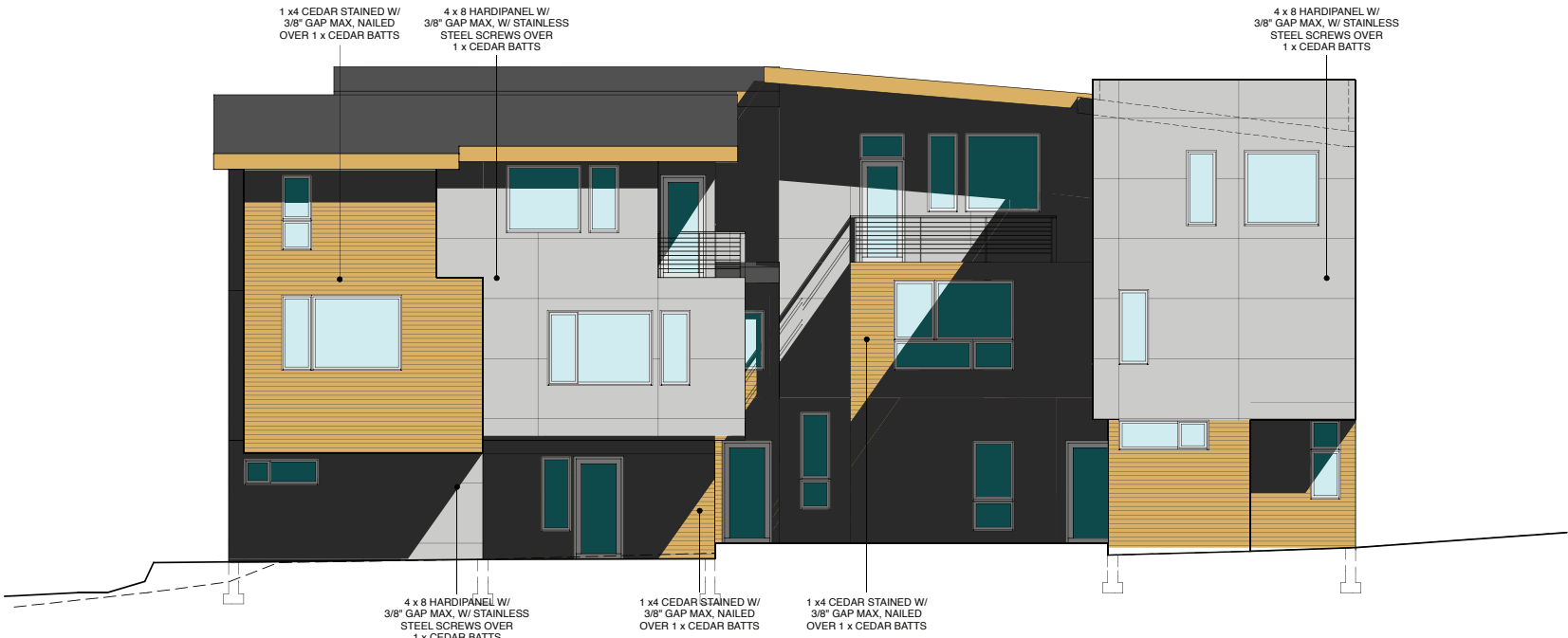
3 | COURTYARD WEST ELEVATION

SCALE: 0' 2' 4' 8' 16'



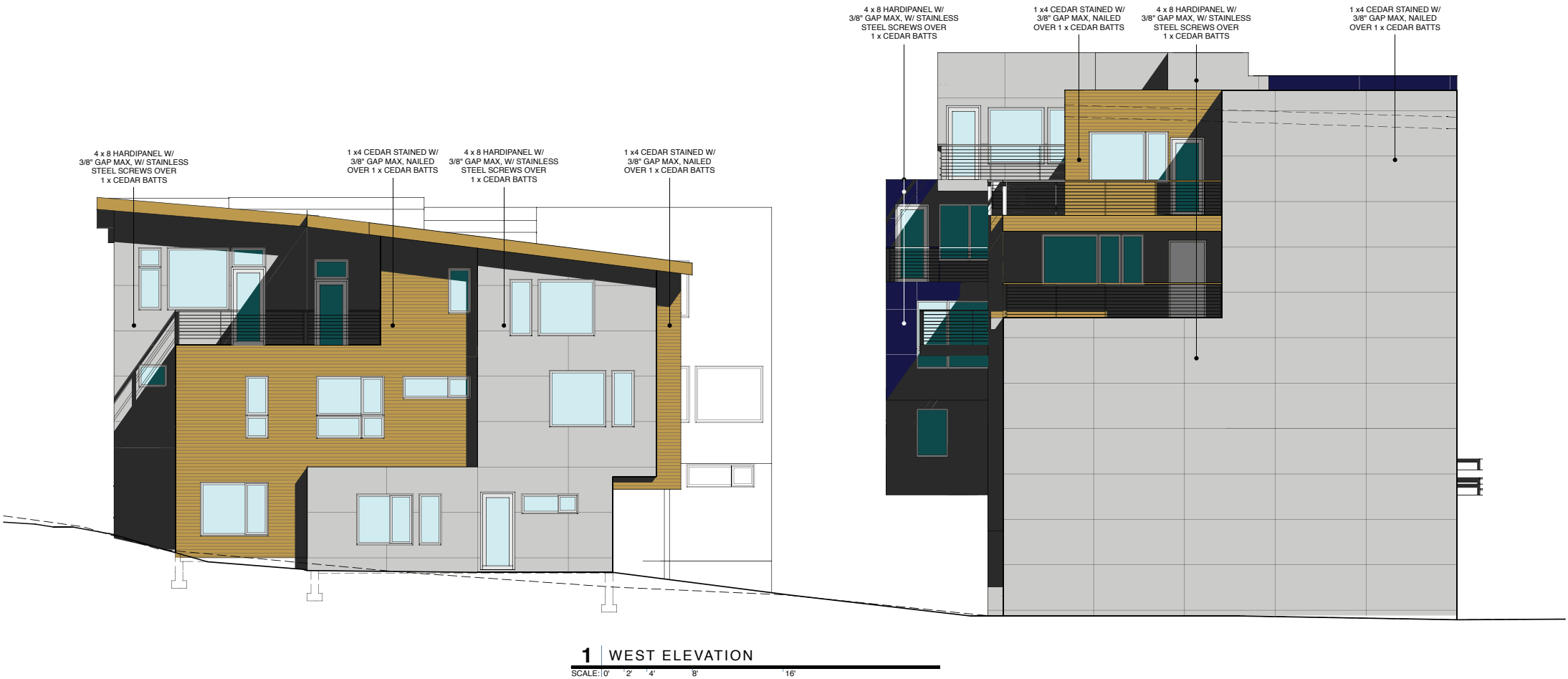
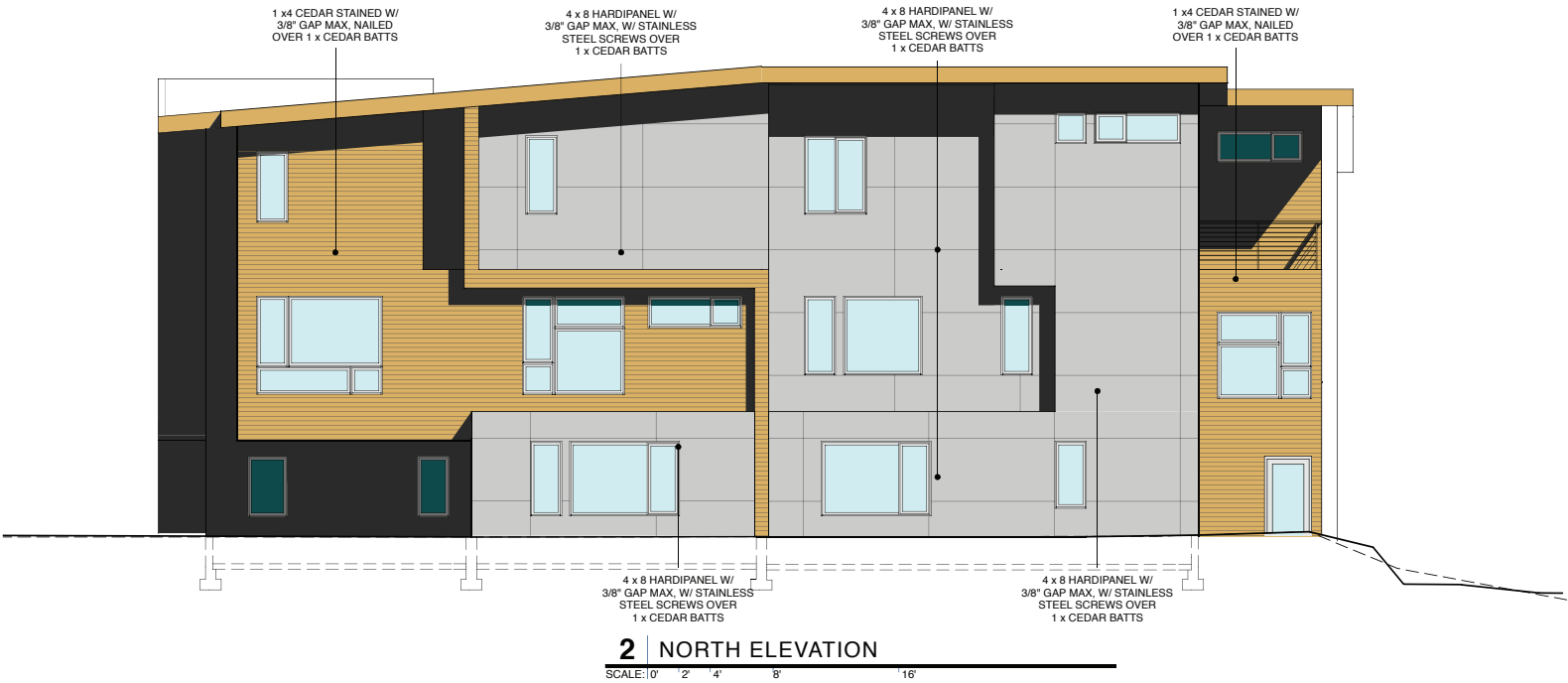
2 | NORTH ELEVATION BUILDING 1

SCALE: 0' 2' 4' 8' 16'



1 | WOONERF SOUTH ELEVATION

SCALE: 0' 2' 4' 8' 16'





① view of woonerf from outdoor living room, looking southwest



② view of outdoor living room from woonerf, looking northeast



③ view of woonerf from House 2 deck



④ view of woonerf from House 4 deck



⑤ view of woonerf from Live-work unit 3 deck

